
Archaeological Research Design and Excavation Methodology

Pymont Station

April 2022




Sydney Metro West – Pyrmont Station Construction Sites

Archaeological Research Design and
Excavation Methodology

Report to Sydney Metro

April 2022



 artefact

Artefact Heritage
ABN 73 144 973 526
Suite 56, Jones Bay Wharf
26-32 Pirrama Road
Pyrmont NSW 2009
Australia

+61 2 9518 8411
office@artefact.net.au

EXECUTIVE SUMMARY

Project overview

Sydney Metro is Australia's biggest public transport program. Sydney Metro West (the project) is a new 24-kilometre metro line that will connect Greater Parramatta with the Sydney CBD. Confirmed stations include Westmead, Parramatta, Sydney Olympic Park, North Strathfield, Burwood North, Five Dock, The Bays, Pyrmont and Hunter Street (Sydney CBD). This infrastructure investment will double the rail capacity of the Greater Parramatta to Sydney CBD corridor with a travel time target between the two centres of about 20 minutes.

Sydney Metro West was assessed as a staged infrastructure application under section 5.20 of the Environmental *Planning & Assessment Act 1979* (EP&A Act).

The Concept and major civil construction work for Sydney Metro West between Westmead and The Bays (Stage 1 of the planning approval process, application number SSI-10038), was approved by the Minister for Planning and Public Places on 11 March 2021.

Stage 2 of the planning approval process includes all major civil construction work including station excavation and tunnelling between The Bays and Sydney CBD.

The Non-Aboriginal Heritage Technical Paper prepared by Artefact Heritage (Artefact) in 2021¹ concluded that the Pyrmont Station construction sites have the potential to contain locally significant non-Aboriginal archaeological resources. Consequently, it was recommended that an Archaeological Research Design (ARD) be prepared to guide excavation works and provide strategies to mitigate impact to potential archaeological resources.

This document provides the ARD and Excavation Methodology (ARDEM) for the Pyrmont Station construction sites.

Archaeological assessment

Historical and archival research to prepare a detailed site-specific history of the Pyrmont Station construction sites has identified the following potential non-Aboriginal archaeological items in the study area.

Site	Phase	Predicted remains	Potential	Significance
Pyrmont Station western construction site		Archaeological remains related to this phase may include brick and stone footings, hearths, yard and workshop surfaces, rubbish scatters and isolated rubbish pits/bottle dumps.	Moderate	Local
	Phase 2 (1879-1914)	Dependant on the extent of former basement excavation impacts in the northernmost portion of the site, the study area has some potential to contain deeper subsurface structures such as wells, privies and cisterns. These structures may contain artefact bearing deposits.	Low	Local

¹ Artefact Heritage, October 2021, Sydney Metro West – Major civil construction work between The Bays and Sydney CBD, report to Sydney Metro Authority

Site	Phase	Predicted remains	Potential	Significance
		The site has limited potential to contain underfloor deposits (discarded ceramic, pins, buttons and glass).		
Pyrmont Station eastern construction site	Phase 2 (1883-1930)	Archaeological remains related to this phase may include brick and stone footings and hearths.	Moderate	Local
		The site has limited potential to contain underfloor deposits (discarded ceramic, pins, buttons and glass), artefacts deposited within decommissioned deeper subsurface structures such as wells, cisterns or privies. Yard and workshop surfaces and isolated rubbish deposits	Low	Local

Archaeological management recommendations

Portions of the Pyrmont Station construction sites have been assessed as having moderate potential to contain locally significant archaeological remains in the form of structural evidence and artefacts associated with the late 19th century residential and commercial development of Pyrmont. If high-yield artefact-bearing deposits, such as those often found in backfilled cisterns or rubbish dumps, survive intact, they may reach the local significance threshold through their ability to provide information regarding the life-ways of the previous occupants of the site.

It is proposed that management of the potential archaeological resource include the following processes. These have been defined in Table 22 and illustrated in Figure 115:

- Heritage induction (completed prior to demolition; see Section 5.2)
- Unexpected Finds Procedure during demolition of extant structures and excavation within areas previously impacted by basements as illustrated on Figure 114 (see Section 5.5)
- Eastern construction site - Monitoring and salvage (if required) of excavation work within the area of moderate archaeological potential (see Figure 115)
- Western construction site - Testing and salvage (if required) of areas proposed for excavation within the area of moderate archaeological potential (see Figure 115).

CONTENTS

1.0	Introduction.....	19
1.1	Sydney Metro West	19
1.2	Pyrmont Station construction sites	20
1.2.1	Site location	20
1.2.2	Heritage listings	21
1.3	ARDEM methodology	24
1.4	Report limitations	24
1.5	Authorship and acknowledgements	24
2.0	Historical Context	25
2.1	Introduction	25
2.2	Aboriginal Country	25
2.3	Summary History of the Pyrmont area	27
2.4	Early Ownership and Development of the study Area	30
2.5	Western construction site land-use overview	36
2.5.1	Phase 1 (1788-1879: Early land clearance and grazing	36
2.5.2	Phase 2 (1879-1914) – Subdivision and urban development of commercial and residential premises	36
2.5.3	Phase 3 (1914-1947) – Resumption, clearance and relocation of commercial activity ...	48
2.5.4	Phase 4 (1947-present) – Gilbey’s Store, ‘Frankel House’ and redevelopment	59
2.6	Eastern Construction Site land-use overview.....	81
2.6.1	Phase 1 (1788-1878) – Early land clearance and grazing	82
2.6.2	Phase 2 (1883-1930) – Subdivision and mixed-use development.....	82
2.6.3	Phase 3 (1930-1986) – Timber and builders’ yard, car park and service station.....	110
2.6.4	Phase 4 (1986 - present) – Commercial development.....	122
3.0	Assessment of Archaeological Potential and Significance	126
3.1	Assessment of archaeological potential	126
3.2	Research potential and archaeological significance	126
3.3	Overview of historic context.....	128
3.4	Previous Archaeological Investigations	129
3.4.1	Pyrmont Peninsula Place Strategy (Godden Mackay Logan, October 2020)	129
3.4.2	26-32 Pyrmont Bridge Road, Pyrmont – Archaeological Research Design (Godden Mackay Logan, 2000).....	130
3.4.3	50-72 Union Street, Pyrmont –Archaeological Investigation for Charter Hall Holdings (Casey and Lowe May 2010)	131
3.4.4	55 Pyrmont Bridge Road, Pyrmont –Archaeological Monitoring of Former McWilliams Wines Premises for City West Centre (Edward Higginbotham 1994).....	131
3.4.5	1-9 Pyrmont Bridge Road, Pyrmont – Archaeological Monitoring Program for redevelopment of site for Powalu Pty Ltd (Godden MacKay Logan 1994-1996).....	131

3.4.6	“Falling through the Cracks: Method and Practice at the CSR Site, Pyrmont” (Casey M., 2004)	132
3.5	Known impacts	133
3.5.1	Western construction site	133
3.5.2	Eastern construction site	133
3.6	Assessment of archaeological potential	133
3.6.1	Pyrmont Station western construction site	133
3.6.2	Pyrmont Station eastern construction site	136
3.7	Assessment of archaeological significance	139
3.7.1	Summary of archaeological potential and significance at the Pyrmont Station construction sites	140
4.0	Research Design	143
4.1	Historic themes	143
4.1.1	Summary of relevant themes	143
4.2	Archaeological research questions	144
4.2.1	Methodology	144
4.2.2	Research questions	145
4.2.3	Industrial archaeology	146
4.2.4	Public utilities and services	147
5.0	Archaeological Methodology	148
5.1	Introduction	148
5.2	Heritage induction	150
5.3	Archaeological team	150
5.3.1	Excavation Director	150
5.3.2	On site archaeologists	150
5.4	Archaeological excavation methodologies	150
5.4.1	Archaeological monitoring – Eastern construction site	151
5.4.2	Test excavation – Western construction site	151
5.4.3	Salvage excavation methodology	154
5.5	Unexpected finds procedure	155
5.6	Archaeological recording and documentation procedures	156
5.6.1	Introduction	156
5.6.2	Survey Control	156
5.6.3	Recording of Contexts	156
5.6.4	Recording of Archaeological Features	157
5.6.5	Photography	157
5.6.6	Collection of Artefacts	157
5.6.7	Long term management of recovered artefacts	158
5.7	Clearance	158

5.8	Preliminary results reporting.....	158
5.9	Post-excavation analysis and reporting.....	159
5.10	Interpretation.....	159

FIGURES

Figure 1: Location of major civil construction work between The Bays and Sydney CBD	20
Figure 2: Pyrmont Station construction sites	21
Figure 3: Location of heritage items within the study area for the Pyrmont Station construction sites	23
Figure 4: c1839 – Detail from ‘Plan of Pyrmont Estate (...)’, showing the subdivision of the Pyrmont Estate (Source: State Library of NSW Z/M2 811.173/1839/1, https://collection.sl.nsw.gov.au/record/74VvM6VbD4Pd/AVRya4rQa6kQk)	29
Figure 5: c1839 – Detail from Map of the Parish of St Andrew, County of Cumberland, showing the early land grants made in the Pyrmont and Ultimo areas (Source: State Library of NSW Z/M2 811.173/1839/1, https://collection.sl.nsw.gov.au/record/74VvM6VbD4Pd/AVRya4rQa6kQk)	30
Figure 6: 1854 – Detail from ‘Woolcott & Clarke’s map of the City of Sydney’, noting houses on the north side of Union Street and on the west side of Darling Harbour, including the ‘Goods Station’ (Source: State Library of NSW, Z/M2 811.15/1854/1, https://collection.sl.nsw.gov.au/record/74VMbBv5kAGA)	32
Figure 7: 1857 – Detail from ‘A plan of part of Sydney and its environs’ by Allan & Wigley, showing the development of roadways, including the proposed new Pyrmont Bridge Road, connecting the two new bridges (Source: State Library of NSW, Z/M2 811.15/1854/1, https://collection.sl.nsw.gov.au/record/74VMbBv5kAGA)	33
Figure 8: c1859 – Detail from tracing of plan of the ‘Ultimo Estate’, Pyrmont, showing the original street grid with Block 57 at the top. The outline of the future Pyrmont Bridge Road is pencilled in, cutting across Blocks 59 a & b, Block 58 and Block 57. (Source: State Library of NSW, Z/M4 811.173/1866/1, https://collection.sl.nsw.gov.au/record/74VKmpo8gpdg)	33
Figure 9: 1865 – Detail from the Trigonometrical Survey of Sydney, Block W1, showing the vacant land on the northern side of Pyrmont Bridge Road, between Union and Harris Streets, with Mrs Bunn’s house noted at the bottom right. (Source: City of Sydney Archives, City of Sydney - Trigonometrical Survey, 1855-1865: Block W1 (01/01/1865 - 31/12/1865), [A-00880414]. https://archives.cityofsydney.nsw.gov.au/nodes/view/1709341)	34
Figure 10: 1873 – Detail from ‘Aerial view west from Town Hall clock tower, 1873,’ showing Pyrmont in the background. Mrs. Bunn’s house is visible on the left, with the newly laid out Pyrmont Bridge Road running behind it, forking off Union Street on the right. The land on the northern side of Pyrmont Bridge Road was still undeveloped (Source: City of Sydney Archives, (01/01/1873 - 31/12/1873), [A-00024418]. https://archives.cityofsydney.nsw.gov.au/nodes/view/585063)	35
Figure 11: 1875 – Detail from ‘Map of Port Jackson and city of Sydney’ by S. T. Leigh & Co. (Source: State Library of NSW, Z/M2 811.12/1875/1, https://collection.sl.nsw.gov.au/record/74Vv0KjLdxZM/qwzpw6kZkepJ)	35
Figure 12: 1879 – Plan of the ‘Subdivision of Block 57 of the Ultimo Estate, Pyrmont,’ showing initial details of proposed subdivision, with allotments facing Pyrmont Bridge Road, Paternoster Row and Pyrmont Street (Source: State Library of NSW, SP/811.1737/91, https://collection.sl.nsw.gov.au/record/74VvaqQxeEeX/QLZdR3WXdvkdb#viewer)	38
Figure 13: 1885 – Plan of Block 57 of the Ultimo Estate, Pyrmont, showing the various leases, including that to George Pile, for the block bounded by Pyrmont Bridge Road, Pyrmont Street, Union Street and Paternoster Row, which was noted as ‘subdivided’ (Source: State Library of NSW, SP/811.1737/94, https://collection.sl.nsw.gov.au/record/74VvaqQxeEeX/xAy2M8vXov7Ak#viewer).	39
Figure 14: 1881 – Impression of the new buildings constructed by Mills & Pile, showing the Union Street frontage of the development which encompassed Pyrmont Bridge Road, Pyrmont Street and	

Union Street (Source: Sydney Improvements, Illustrated Sydney News, 6 August 1881, 16, https://trove.nla.gov.au/newspaper/page/5406469)	41
Figure 15: 1888 – Detail from City of Sydney Section (Metropolitan Detail Series), Sheet H3 (Source: State Library of NSW, Z/M Ser 4 811.17/1, https://collection.sl.nsw.gov.au/record/74VKqv5pXe4b/bO70aB3gy0DAk#viewer)	44
Figure 16: 1895 – Sales plan for Block 57 of the Harris Estate, sold as city freeholds, showing details of buildings (Source: City of Sydney Archives, Harris Estate sale No. 12 [area between Pyrmont, Union, Gipps, Harris, Pyrmont Bridge Road] (30/09/1895), [A-00530399]. https://archives.cityofsydney.nsw.gov.au/nodes/view/10698863709-2)	45
Figure 17: Detail from Blackwattle Sheets, prepared by the Board of Water Supply and Sewerage, 1908 (with later updates) (Source: Sydney Water Archives, BLKWTL 3709-2).....	45
Figure 18: 1911 – Photograph of the terrace shop row on the northern side of Pyrmont Bridge Road, including the Pyrmont Mont de Piete Loan Office on the corner of Pyrmont Street. From Demolition Books. (Source: City of Sydney Archives, Pyrmont Bridge Road (01/12/1911), [A-00038946]. https://archives.cityofsydney.nsw.gov.au/nodes/view/676872)	46
Figure 19: 1911 – Photograph of the shops on Pyrmont Bridge Road, including Excelsior Dining Rooms (No. 32), Frederick Bricknell, saddler (No. 30), and Henry Lee, newsagent (No. 28). From Demolition Books. (Source: City of Sydney Archives, Pyrmont Bridge Road (01/12/1911), [A- 00038945]. https://archives.cityofsydney.nsw.gov.au/nodes/view/676862).....	46
Figure 20: 1911 – Photograph of the building at the corner of Paternoster Row (on left) and Pyrmont Bridge Road. Located at 36 Pyrmont Bridge Road, this was from 1881 until the early 1900s in use as a bakery. From Demolition Books. (Source: City of Sydney Archives, Pyrmont Bridge Road (01/12/1911), [A-00038944]. https://archives.cityofsydney.nsw.gov.au/nodes/view/676856)	47
Figure 21: 1922 – City Council Freehold properties for sale, including Section 6 on the northern side of Pyrmont Bridge Road (Source: SLNSW, SP/811.1737/2, https://collection.sl.nsw.gov.au/record/74VvaqQxeEeX/IX4Xd3rAk6vGV#viewer)	50
Figure 22: 1920s – ‘Aerial view of Pyrmont Bridge and wharves’, with the Western Construction site area outlined (Source: NLA, PIC/15611/15348, Fairfax archive, https://nla.gov.au/nla.obj-163003956)	51
Figure 23: c1920s – Aerial photograph by Milton Kent, said to date from 1932 but based on the context it was taken during the 1920s (all buildings at the Eastern Construction Site are still standing). Note the western portion of the site appears to contain deposits (Source: SLNSW, Milton Kent aerial views of Sydney, 1932, FL8812118, https://archival.sl.nsw.gov.au/Details/archive/110367483).....	51
Figure 24: August 1930 – View towards the vacant site, with the Quarryman’s Hotel on the left. A person appears to be standing on an elevated level, and the frontage to the street also appears to be elevated (Source: Noel Butlin Archives Centre, Tooth’s Yellow Card, Quarryman’s Hotel Pyrmont, Card 2 Side 2, https://openresearch-repository.anu.edu.au/handle/1885/104550)	52
Figure 25: 1930 – Aerial view by Milton Kent, showing the vacant site as outlined (Source: SLNSW, Milton Kent aerial views of Sydney, 1932, FL8812121, https://archival.sl.nsw.gov.au/Details/archive/110367483)	52
Figure 26: 1932 – Aerial view by Milton Kent, showing the vacant site and adjoining terraces as outlined (Source: SLNSW, Milton Kent aerial views of Sydney, 1932, FL8812119, https://archival.sl.nsw.gov.au/Details/archive/110367483)	53
Figure 27: 1932 – Aerial view by Milton Kent, showing the vacant site and adjoining terraces as outlined (Source: SLNSW, Milton Kent aerial views of Sydney, 1932, FL8812120, https://archival.sl.nsw.gov.au/Details/archive/110367483)	53

Figure 28: January 1939 – View towards the vacant site, with the Quarryman’s Hotel just visible on the left. The site appears to have been largely cleared, with a path likely cutting across (Source: Noel Butlin Archives Centre, Tooth’s Yellow Card, Quarryman’s Hotel Pyrmont, Card 2 Side 2, https://openresearch-repository.anu.edu.au/handle/1885/104550)	54
Figure 29: 1938-39 – Detail from City of Sydney Civic Survey, Map 17, showing the Pyrmont Bridge Road site as vacant (Source: City of Sydney Archives, City of Sydney - Civic Survey, 1938-1950: Map 17 – Pyrmont, [A-00880354]. https://archives.cityofsydney.nsw.gov.au/nodes/view/1709281)	54
Figure 30: c1940-41 – Detail from City of Sydney Detail Survey, originally surveyed in January 1923 and updated to 1940-41, showing the Pyrmont Bridge Road site as vacant (Source: Sydney Water Archives, City of Sydney Detail Survey Sheet H3, DTS 403)	55
Figure 31: 1943 – Aerial photograph showing the hoarding erected in 1941, as well as a long structure along Paternoster Row, the purpose of which could not be identified (Source: NSW LRS, SIX Maps, 1943 aerial)	55
Figure 32: 1914 – Plan showing details for the proposed new shopfront for Henry Lee’s newsagency at 133 Pyrmont Street, carried out by E. Spackman for the Scottish Australian Investment Co Ltd (Source: City of Sydney Archives, Pyrmont St (133) E Spackman alterations (01/01/1914 - 31/12/1914), [A-00551981]. https://archives.cityofsydney.nsw.gov.au/nodes/view/1420016)	57
Figure 33: 1914 – Plan showing details for the proposed new shopfronts 129 and 131 Pyrmont Street, carried out by E. Spackman for the Scottish Australian Investment Co Ltd. The plan notes the wrong address but was submitted as part of BA 609/14. These buildings were occupied by a restaurant (No. 131, on left) and hairdresser (No. 129, on right) (Source: City of Sydney Archives, Pyrmont St (129-131) E Spackman alterations & additions (01/01/1914 - 31/12/1914), [A-00571870]. https://archives.cityofsydney.nsw.gov.au/nodes/view/1436278)	58
Figure 34: 1916 – Plan showing details for proposed alterations to the restaurant at 131 Pyrmont Street, by J. H. Carden. (Source: City of Sydney Archives, Pyrmont St (131) J H Carden alterations (01/01/1916 - 31/12/1916), [A-00550357]. https://archives.cityofsydney.nsw.gov.au/nodes/view/1431879)	58
Figure 35: 1947 – Elevations and Sections for the proposed factory premises for W & A Gilbey Ltd by C. Orme, architect, Melbourne, approved 10 July 1947. These were later amended to include a third floor. The approximate ground level was noted by the bottom line in Section B-B and Section A-A (Source: City of Sydney Archives, Pyrmont Bridge Rd NW cnr & Pyrmont St (adjacent No 135), new building (01/01/1947 - 31/12/1947), [A-00572010]. https://archives.cityofsydney.nsw.gov.au/nodes/view/1437047)	61
Figure 36: 1947 – Roof and foundation plans for the proposed factory premises for W & A Gilbey Ltd by C. Orme, architect, Melbourne, approved 10 July 1947 and amended in 1948 (Source: City of Sydney Archives, Pyrmont Bridge Rd NW cnr & Pyrmont St (adjacent No 135), new building (01/01/1947 - 31/12/1947), [A-00572010]. https://archives.cityofsydney.nsw.gov.au/nodes/view/1437047)	62
Figure 37: 1947 – Ground and first floor plans for the proposed factory premises for W & A Gilbey, by C. Orme, architect, Melbourne, approved 10 July 1947 and amended in 1948 (Source: City of Sydney Archives, Pyrmont Bridge Rd NW cnr & Pyrmont St (adjacent No 135), new building (01/01/1947 - 31/12/1947), [A-00572010]. https://archives.cityofsydney.nsw.gov.au/nodes/view/1437047)	62
Figure 38: 1948 – Amended ground and first floor plans for the proposed factory premises for W & A Gilbey Ltd by C. Orme, architect, Melbourne, approved 31 March 1948 (Source: City of Sydney Archives, Pyrmont Bridge Rd NW cnr & Pyrmont St (adjacent No 135), new building (01/01/1947 - 31/12/1947), [A-00572010]. https://archives.cityofsydney.nsw.gov.au/nodes/view/1437047)	63

Figure 39: 1948 – Ground floor plan and structural details for W & A Gilbey Ltd Factory, by C. Orme, architect, and A. S. Nicholson & H. R. Hammond, consulting engineers, approved 18 June 1948 (Source: City of Sydney Archives, Pyrmont Bridge Rd NW cnr & Pyrmont St (adjacent No 135), new building (01/01/1947 - 31/12/1947), [A-00572010]. https://archives.cityofsydney.nsw.gov.au/nodes/view/1437047)	63
Figure 40: 1948 – Plan of footings for W & A Gilbey Ltd Factory, by C. Orme, architect, and A. S. Nicholson & H. R. Hammond, consulting engineers, approved 18 June 1948. The plan shows the locations of 'brickwork to solid rock' (Source: City of Sydney Archives, Pyrmont Bridge Rd NW cnr & Pyrmont St (adjacent No 135), new building (01/01/1947 - 31/12/1947), [A-00572010]. https://archives.cityofsydney.nsw.gov.au/nodes/view/1437047)	64
Figure 41: 1950 – Amended elevations and sections for proposed second floor (third storey) for W & A Gilbey Ltd Factory Premises, by C. Orme, architect, approved 27 January 1950 (Source: City of Sydney Archives, Pyrmont Bridge Rd NW cnr & Pyrmont St (adjacent No 135), new building (01/01/1947 - 31/12/1947), [A-00572010]. https://archives.cityofsydney.nsw.gov.au/nodes/view/1437047)	64
Figure 42: 1950 – Second floor plan and roof plan for proposed second floor (third storey) for W & A Gilbey Ltd Factory Premises, by C. Orme, architect, approved 27 January 1950 (Source: City of Sydney Archives, Pyrmont Bridge Rd NW cnr & Pyrmont St (adjacent No 135), new building (01/01/1947 - 31/12/1947), [A-00572010]. https://archives.cityofsydney.nsw.gov.au/nodes/view/1437047)	65
Figure 43: 15 December 1949 – Aerial photograph showing the building under construction (Source: City of Sydney Archives, A-00879983, City of Sydney Aerial Photographic Survey 1949: Image 38)	65
Figure 44: 1949-60s – City of Sydney Building Surveyor's Detail Sheets, showing the W. & A. Gilbey Ltd premises and adjacent terraces (Source: City of Sydney Archives, A-008880206, City of Sydney Building Surveyor's Detail Sheets, 5 - Pyrmont)	66
Figure 45: 1959 – Plan included in the Certificate of Title issued for Lot 1 in Plan 48128 (L), showing the subdivision and outline of buildings on Pyrmont Street, with 129 to 135 Pyrmont Street outlined. (Source: NSW LRS, CT Vol 7858 fol 133)	68
Figure 46: 1962 – Elevations and sections for proposed new bookstore and offices at 133-135, by Harry Seidler, architect, working drawing dated 8 February 1961. The BA was approved on 20 March 1962 and the existing terraces at the site were demolished in 1964, but the building was never constructed (Source: City of Sydney Archives, Pyrmont St (133-135) (01/01/1962 - 31/12/1962), [A-00547430]. https://archives.cityofsydney.nsw.gov.au/nodes/view/1411167)	68
Figure 47: 1962 – Floor plans for proposed new bookstore and offices at 133-135, by Harry Seidler, architect, working drawing dated 8 February 1961 (Source: City of Sydney Archives, Pyrmont St (133-135) (01/01/1962 - 31/12/1962), [A-00547430]. https://archives.cityofsydney.nsw.gov.au/nodes/view/1411167)	69
Figure 48: 1966 – Sketch plan included in DA 974/66, showing the location of the existing Gilbey's Building in blue, with the area of the proposed new car parking area outlined in red. At the time, that site still contained terraced houses at 129-131 Pyrmont Street (Source: City of Sydney Archives, DA 974/66, '129-135 Pyrmont Street – Gilbey's Aus P/L – Proposes use of premises as a car park and for unloading the company's truck and use the remainder as a parking station')	70
Figure 49: 1972 – Working drawing prepared by Anderson and Thomas, architects, dated 19 January 1972, as part of DA 465/71 (Source: City of Sydney Archives, DA 465/71, '129-135 Pyrmont Street, Pyrmont – Gilbeys Australia P/L – Use premises for parking and off-street loading')	71

Figure 50: 1978 – Aerial photograph showing Gilbey’s Building with car parking to the rear. The roof over the loading dock is also clearly visible (Source: NSW Spatial Services, aerial photograph, 6 May 1978, 2713-16-064)	73
Figure 51: 1979 – The Pyrmont Street elevation of Gilbey’s Distillers’ factory and warehouse, with a small section of the rear car parking area and loading dock visible (Source: City of Sydney Archives, W&A Gilbey Ltd, 26-32 Pyrmont Bridge Road Pyrmont, 1979 (01/01/1979 - 31/12/1979), [A-00008101]. https://archives.cityofsydney.nsw.gov.au/nodes/view/568746).....	74
Figure 52: 6 August 1979 – The Pyrmont Bridge Road elevation of W. & A. Gilbey Ltd’s factory and store (Source: City of Sydney Archives, W&A Gilbey Ltd, Bridge Road Pyrmont, 1979 (06/08/1979), [A-00007942]. https://archives.cityofsydney.nsw.gov.au/nodes/view/568587)	74
Figure 53: 1986 – Aerial photograph showing the site at 26-32 Pyrmont Bridge Road largely unchanged (Source: NSW Spatial Services, aerial photograph, 3 August 1986, 3528-22-061)	75
Figure 54: 1988 – Site plan prepared by Jackson Teece Chesterman Willis & Partners as part of BA 1877/88, dated August 1988 (Source: City of Sydney Archives, Pyrmont Bridge Rd (26-32) Pyrmont. Gilbeys Australia. Refurbish existing warehouse with alts. (01/01/1988 - 31/12/1988), [A-00574747]. https://archives.cityofsydney.nsw.gov.au/nodes/view/1457240)	75
Figure 55: 1999 – ‘Frankel House’ as seen from Pyrmont Bridge Road, looking northwest. (Source: City of Sydney Archives, D1999/00812, ‘Development Applications/26-32/Pyrmont Bridge Road/Pyrmont/Alterations & Additions to Existing Building – Bulawayo Holdings Pty Ltd – 14 10 99 - \$5000000’).....	77
Figure 56: 1999 – Looking southeast from Paternoster Row towards the north elevation of the former Gilbey’s Building, with the awning over the loading dock visible at centre. (Source: City of Sydney Archives, D1999/00812, ‘Development Applications/26-32/Pyrmont Bridge Road/Pyrmont/Alterations & Additions to Existing Building – Bulawayo Holdings Pty Ltd – 14 10 99 - \$5000000’)	77
Figure 57: 1999 – Looking along Pyrmont Street towards the former Gilbey’s Building and car parking area. (Source: City of Sydney Archives, D1999/00812, ‘Development Applications/26-32/Pyrmont Bridge Road/Pyrmont/Alterations & Additions to Existing Building – Bulawayo Holdings Pty Ltd – 14 10 99 - \$5000000’)	78
Figure 58: 1999 – Render of the proposed new development, by Allen Jack + Cottier for Bulawayo Holdings, showing the Pyrmont Street frontage (Source: City of Sydney Archives, D1999/00812, ‘Development Applications/26-32/Pyrmont Bridge Road/Pyrmont/Alterations & Additions to Existing Building – Bulawayo Holdings Pty Ltd – 14 10 99 - \$5000000’)	78
Figure 59: 1999 – Basement Level 2 plan, prepared by Allen Jack + Cottier, architects, dated 8 December 1999 (Source: City of Sydney Archives, D1999/00812, ‘Development Applications/26-32/Pyrmont Bridge Road/Pyrmont/Alterations & Additions to Existing Building – Bulawayo Holdings Pty Ltd – 14 10 99 - \$5000000’).....	79
Figure 60: 1999 – Basement Level 1 plan, prepared by Allen Jack + Cottier, architects, dated 8 December 1999 (Source: City of Sydney Archives, D1999/00812, ‘Development Applications/26-32/Pyrmont Bridge Road/Pyrmont/Alterations & Additions to Existing Building – Bulawayo Holdings Pty Ltd – 14 10 99 - \$5000000’).....	79
Figure 61: 1999 – Ground Floor plan, prepared by Allen Jack + Cottier, architects, dated 8 December 1999 (Source: City of Sydney Archives, D1999/00812, ‘Development Applications/26-32/Pyrmont Bridge Road/Pyrmont/Alterations & Additions to Existing Building – Bulawayo Holdings Pty Ltd – 14 10 99 - \$5000000’)	80
Figure 62: 1999 – West and south elevations, prepared by Allen Jack + Cottier, architects, dated 8 December 1999 (Source: City of Sydney Archives, D1999/00812, ‘Development Applications/26-	

32/Pyrmont Bridge Road/Pyrmont/Alterations & Additions to Existing Building – Bulawayo Holdings Pty Ltd – 14 10 99 - \$5000000')	81
Figure 63: 2000 – East and north elevations, prepared by Allen Jack + Cottier, architects, as part of the Section 96 application, dated 7 April 2000 (Source: City of Sydney Archives, D1999/00812, 'Development Applications/26-32/Pyrmont Bridge Road/Pyrmont/Alterations & Additions to Existing Building – Bulawayo Holdings Pty Ltd – 14 10 99 - \$5000000')	81
Figure 64: Plan from Primary Application Search Sketch for PA 30115, noting the land titles that relate to the study area, being the northern portion of Block 59b of the Ultimo Estate. (Source: NSW LRS, Historical Land Records Viewer, PASS 30115)	82
Figure 65 Detail from Public Works Department plan for Pyrmont, Sheet K3, submitted to the Surveyor General on 9 October 1886. This shows the original numbering of the buildings from 103 to 113 Union Street, 68 to 74 Edward Street, and 164 to 170 Pyrmont Bridge Road. (Source: Sydney Water Archives, PWDS1544-S235)	84
Figure 66: c1888 – Detail from City of Sydney Section (Metropolitan Detail Series), Sheet K3, showing the development of the block, with Union Street on the right. The plan was lithographed and printed in May 1888, based on an earlier survey (Source: State Library of NSW, Z/M Ser 4 811.17/1, https://collection.sl.nsw.gov.au/record/74VKqv5pXe4b/bBWNZQV5XWRll#viewer)	85
Figure 67: 1930 – Detail from aerial view showing the Eastern Construction Site fully developed, including the 1896 building on the corner of Union Street and Pyrmont Bridge Road, at lower right. The timber buildings to the rear of the shops at the corner of Union and Edward Streets have been demolished and the original hall replaced by residential terraces. Note the gabled buildings on Edward Street, and the single storey semi-detached cottages on Pyrmont Bridge Road, which are about to be demolished (Source: Fairfax Corporation. (1930). Aerial view of Darling Harbour showing Pyrmont Bridge closing following passage of a ship, Sydney, ca. 1930s. http://nla.gov.au/nla.obj-163204711)	85
Figure 68: c1886 – Detail from Public Works Department plan for Pyrmont, Sheet K3, submitted to the Surveyor General on 9 October 1886. This shows the buildings on the Union Street frontage, with the outline of 'Elder's Hall' clearly visible on the left portion. (Source: Sydney Water Archives, PWDS1544-S235)	89
Figure 69: c1888 – Detail from City of Sydney Section (Metropolitan Detail Series), Sheet K3, showing the development on Union Street, largely identical with the plan in Figure 68. This plan was lithographed and printed in May 1888, based on an earlier survey. (Source: State Library of NSW, Z/M Ser 4 811.17/1, https://collection.sl.nsw.gov.au/record/74VKqv5pXe4b/bBWNZQV5XWRll#viewer)	89
Figure 70: 22 September 1888 – Sales advertisement for the property at 89-113 Union Street, Pyrmont (Source: Advertising, Sydney Morning Herald, 22 September 1888, 18, https://trove.nla.gov.au/newspaper/article/13697481/1389741)	92
Figure 71: 24 October 1897 – Sales advertisement for Josiah White's corner shop (Source: Advertising, Sunday Times, 24 October 1897, 11, https://trove.nla.gov.au/newspaper/article/126249216/13493081)	93
Figure 72: Late 1890s to 1900s – The buildings at 51 to 69 Union Street as outlined in red, with the decorative parapet of the double storey shop at 55 Union Street visible at lower right. Photography by Charles H. Kerry (Source: Powerhouse Collection, Sydney from Pyrmont, 85/1284-1778. https://collection.maas.museum/object/28399)	94
Figure 73: 1 December 1911 – Corner of Union Street and Pyrmont Bridge Road, with the Wine Bar at 69 Union Street on the right and small cottages to the rear (Source: City of Sydney Archives, Pyrmont (01/12/1911), [A-00038966]. https://archives.cityofsydney.nsw.gov.au/nodes/view/676948)	94

- Figure 74: Detail from Blackwattle Sheet, prepared by the Board of Water Supply and Sewerage, 1908 (with later updates), showing that the hall has been demolished and replaced with double storey terraces (now five in total, area outlined). The blacksmith's shop to the rear of 37-45 Union Street was still standing by then. (Source: Sydney Water Archives, BLKWTL 3716) 98
- Figure 75: 8 September 1916 – Photograph of dwellings at 47-59 Union Street, Pyrmont, with workers working on the parapet. From Demolition Books. In c1905, these terraces replaced the former hall which was once located at 49/49a Union Street, before being gutted by fire in 1903. It is unclear what works were carried out to the terraces at that time, as double storey houses continued to be located at the site until the early 1930s (Source: City of Sydney Archives, Dwellings, Union St (47-59), Pyrmont, 8 September 1916 (08/09/1916), [A-00036813]. <https://archives.cityofsydney.nsw.gov.au/nodes/view/663180>) 99
- Figure 76: c1920s – Looking south from Edward Street, across Union Street, with the verandah structure of the corner building at 37 Union Street just visible on the right. Said to date from 1931, however, the verandah structure was already gone by 1929, see Figure 81 (Source: City of Sydney Archives, Edward Street, Pyrmont (01/01/1931 - 31/12/1931), [A-00011914]. <https://archives.cityofsydney.nsw.gov.au/nodes/view/572559>) 99
- Figure 77: c1920s – Detail from 'Panorama of Darling Harbour and Pyrmont bridge from the city', by A. E. Foster, ca 1916-1947, with the Union Street buildings outlined. Gillespie Brothers Flour Mill, on the right, was constructed c1921 (Source: SLNSW, Series 06: Sydney Views, Box 32 No. 360, FL410934. <https://collection.sl.nsw.gov.au/record/nX6OPA8Y/04wINBGeVg0OD#viewer>) 100
- Figure 78: c1920s – Detail from 'Panorama of Darling Harbour and Pyrmont bridge from the city', by A. E. Foster, ca 1916-1947, with the Union Street buildings outlined. Gillespie Brothers Flour Mill, on the right, was constructed c1921 (Source: SLNSW, Series 06: Sydney Views, Box 32 No. 359, FL410934. <https://collection.sl.nsw.gov.au/record/nX6OPA8Y/r3RDxyrpxR7BZ>) 100
- Figure 79: 1920s – 'Aerial view of Pyrmont Bridge and wharves', with the Eastern Construction Site area at centre (Source: NLA, PIC/15611/15348, Fairfax archive, <https://nla.gov.au/nla.obj-163003956>) 101
- Figure 80: c1920s – Aerial photograph by Milton Kent, said to date from 1932 but all buildings are still standing, suggesting a date prior to 1930 (Source: SLNSW, Milton Kent aerial views of Sydney, 1932, FL8812118, <https://archival.sl.nsw.gov.au/Details/archive/110367483>) 101
- Figure 81: 2 August 1929 – The store at the corner of Union and Edward Streets is for sale (Source: City of Sydney Archives, 37-45 Union Street, Pyrmont (02/08/1929), [A-00037606]. <https://archives.cityofsydney.nsw.gov.au/nodes/view/663973>) 102
- Figure 82: 1930 – Detail from aerial view showing the buildings on Union Street shortly prior to demolition. The blacksmith's workshop to the rear of 37 to 45 Union Street is no longer standing (Source: Fairfax Corporation. (1930). Aerial view of Darling Harbour showing Pyrmont Bridge closing following passage of a ship, Sydney, ca. 1930s. <http://nla.gov.au/nla.obj-163204711>) 102
- Figure 83: 20 October 1930 – Houses being demolished at Union Street, near corner of Edward Street. The shop on the left was located at 55 Union Street, however, the numbering of houses to the west (on right) was not always consistent (Source: City of Sydney Archives, Houses being demolished (20/10/1930), [A-00037644]. <https://archives.cityofsydney.nsw.gov.au/nodes/view/664011>) 103
- Figure 84: 20 October 1930 – Shops and houses being demolished at Union Street, near corner of Edward Street. Shop at 55 Union Street bearing date 1881 (Source: City of Sydney Archives, Shop and houses being demolished (20/10/1930), [A-00037643]. <https://archives.cityofsydney.nsw.gov.au/nodes/view/664010>) 103

- Figure 85: 1930 – Aerial view by Milton Kent, showing the site after demolition of the buildings on the corner of Union and Edward Streets. The image is said to have been taken in 1932, however, by then all the buildings had been demolished (Source: SLNSW, Milton Kent aerial views of Sydney, 1932, FL8812121, <https://archival.sl.nsw.gov.au/Details/archive/110367483>) 104
- Figure 86: c1920s – Detail from ‘Panorama of Darling Harbour and Pyrmont bridge from the city’, by A. E. Foster, ca 1916-1947, with two of the gabled buildings on Edward Street noted by the arrow. (Source: SLNSW, Series 06: Sydney Views, Box 32 No. 360, FL410934. <https://collection.sl.nsw.gov.au/record/nX6OPA8Y/04wINBGevG0OD#viewer>) 106
- Figure 87: 2 August 1929 – Corner of Union and Edward Streets, showing buildings on Edward Street on the right (Source: City of Sydney Archives, 37-45 Union Street, Pyrmont (02/08/1929), [A-00037607]. City of Sydney Archives, accessed 17 Jan 2022, <https://archives.cityofsydney.nsw.gov.au/nodes/view/663974>) 106
- Figure 88: 1930 – Detail from aerial view showing the four gabled buildings on Edward Street shortly prior to demolition (Source: Fairfax Corporation. (1930). Aerial view of Darling Harbour showing Pyrmont Bridge closing following passage of a ship, Sydney, ca. 1930s. <http://nla.gov.au/nla.obj-163204711>) 107
- Figure 89: 1920s – Detail from ‘Panorama of Darling Harbour and Pyrmont bridge from the city’, by A. E. Foster, ca 1916-1947, with the buildings at 8 to 14 Pyrmont Bridge Road indicated by the arrow (Source: SLNSW, Series 06: Sydney Views, Box 32 No. 360, FL410934. <https://collection.sl.nsw.gov.au/record/nX6OPA8Y/04wINBGevG0OD#viewer>) 109
- Figure 90: 1930 – Detail from aerial view showing the single storey semi-detached cottages on Pyrmont Bridge Road, indicated by the red arrows, shortly prior to demolition (Source: Fairfax Corporation. (1930). Aerial view of Darling Harbour showing Pyrmont Bridge closing following passage of a ship, Sydney, ca. 1930s. <http://nla.gov.au/nla.obj-163204711>) 110
- Figure 91: 1932 – Aerial view by Milton Kent, showing the subject block at centre largely vacant and surrounded by fencing and hoarding, with three buildings remaining standing on the corner of Union Street and Pyrmont Bridge Road (Source: SLNSW, Milton Kent aerial views of Sydney, 1932, FL8812119, <https://archival.sl.nsw.gov.au/Details/archive/110367483>) 111
- Figure 92: 1932 – Aerial view by Milton Kent, showing the subject block at centre largely vacant and surrounded by hoarding, with three buildings remaining standing on the corner of Union Street and Pyrmont Bridge Road (Source: SLNSW, Milton Kent aerial views of Sydney, 1932, FL8812120, <https://archival.sl.nsw.gov.au/Details/archive/110367483>) 112
- Figure 93: 1938-39 – Detail from City of Sydney Civic Survey, Map 17, with the triangular site noted as a timber yard (Source: City of Sydney Archives, City of Sydney - Civic Survey, 1938-1950: Map 17 – Pyrmont, [A-00880354]. <https://archives.cityofsydney.nsw.gov.au/nodes/view/1709281>) 112
- Figure 94: 1940 – Detail from City of Sydney Detail Sheets, K3, based on a survey dated September 1940, with the site noted as a timber yard surrounded by hoarding (Source: Sydney Water Archives, DTS 2674) 113
- Figure 95: circa early 1940s – Streetscape at the corner of Union Street (at front) and Pyrmont Bridge Road, showing existing corner buildings and hoarding (Source: Sydney Living Museums, Caroline Simpson Library & Research Collection, Record No. 31219, <http://collection.hht.net.au/firsthttpictures/picturerecord.jsp?recno=31219>) 113
- Figure 96: 1943 – Aerial photograph showing the site in use as a timber and storage yard. Note that access appears to have been from Edward Street. Hoarding is in place around the site, with the buildings on the corner of Union Street and Pyrmont Bridge Road still standing (Source: NSW LRS, SIX Maps, 1943 aerial imagery) 114

Figure 97: 1940s – MWS & DB survey made in September 1940 and renewed in April 1949, showing the location of the extant buildings on the corner of Union Street and Pyrmont Bridge Road, and the hoarding around the timber yard, with a building noted on the Pyrmont Bridge Road frontage (Source: Sydney Water Archives, BLKWTL 3716-4) 114

Figure 98: 15 December 1949 – Aerial photograph showing the site with several buildings, including those shown in the 1940s survey above. The building in the upper left (north-western) corner of the site was likely a corrugated iron and timber workshop. Several other, smaller sheds are located at the site (Source: City of Sydney Archives, A-00879983, City of Sydney Aerial Photographic Survey 1949: Image 38)..... 115

Figure 99: 1950 – Sketch plan noting the existing buildings at the site, including three sheds on the Pyrmont Bridge Road frontage, however, this was likely extremely indicative only (Source: City of Sydney Archives, 2866/50, ‘Premises, Site bounded by Pyrmont Bridge Rd, Union St & Edward St. M. Fiorelli Pty Ltd – Demolition of two (2) dwellings & sheds & erection of building for use as factory, store & offices.’)..... 115

Figure 100: 1950 – Sketch plan for the proposed new factory, submitted by Fiorelly Ltd Ptd as part of IDA 349/50. This development did not go ahead (Source: City of Sydney Archives, IDA 349/50 Correspondence File, ‘Cnr Bridge Rd., Union St & Edward St, Pyrmont – Erection of factory & office building – M. Fiorelli Pty Ltd – Interim Development Application.’) 116

Figure 101: March 1954 – Photograph of the Pyrmont Power Station, by Max Dupain, showing part of the hoarding at the corner of Union and Edward Streets (Source: SLNSW, FL19484880, <https://collection.sl.nsw.gov.au/record/nvgPBWJ1>) 116

Figure 102: 1953 – Plan showing the subdivision of the site owned by Arthur Leslie Harris into two allotments (A and B), excluding the corner allotment at top right, for lease purposes (Source: NSW LRS, Deed Bk 2396 No 85) 117

Figure 103: 1954 – Plan submitted as part of DA 804/54, for a new service Station (Source: City of Sydney Archives, DA 804/54, ‘Pyrmont Bridge Road & 65/69 Union Street, Pyrmont. Erection of a two level Service Station. Vacuum Oil Company Pty Ltd.’) 119

Figure 104: 1954 – Elevations submitted as part of DA 804/54, for a new service Station (Source: City of Sydney Archives, DA 804/54, ‘Pyrmont Bridge Road & 65/69 Union Street, Pyrmont. Erection of a two level Service Station. Vacuum Oil Company Pty Ltd.’)..... 119

Figure 105: 1979 – Mobil Service Station, showing elevation to Union Street (Source: Mobil Service Station, Union Street Pyrmont, 1979 (01/01/1979 - 31/12/1979), [A-00008061]. <https://archives.cityofsydney.nsw.gov.au/nodes/view/568706>) 120

Figure 106: 1978 – Aerial photograph showing the Eastern Construction site, containing the Pyrmont Parking Station and Mobil Service Station (Source: NSW Spatial Services, aerial photograph, 6 May 1978, 2713-16-064) 122

Figure 107: c1985-86 – Photograph showing the Service Station at centre prior to demolition, with a portion of the fence surrounding the parking station on the right, facing Union Street. (Source: City of Sydney Archives, BA 86/1056, ‘37 - 61 63 - 65 67 & 69 Union St Pyrmont - Construct New Office Warehouse Building \$1800000 22 05 86 Cedra Construction Pty Ltd (22/05/1986 - 14/02/1992).’). 123

Figure 108: 1986 – Elevations prepared by Butler & Sky Pty Ltd, architects, dated May 1986, for 37-69 Union Street, approved on 11 July 1986. Minor later amendments were also approved (Source: City of Sydney Archives, BA 86/1056, ‘37 - 61 63 - 65 67 & 69 Union St Pyrmont - Construct New Office Warehouse Building \$1800000 22 05 86 Cedra Construction Pty Ltd (22/05/1986 - 14/02/1992).’) 124

Figure 109: 1986 – Ground floor plan prepared by Butler & Sky Pty Ltd, architects, dated May 1986, for 37-69 Union Street, as amended, approved on 10 April 1987 (Source: City of Sydney Archives, BA

86/1056, '37 - 61 63 - 65 67 & 69 Union St Pyrmont - Construct New Office Warehouse Building \$1800000 22 05 86 Cedra Construction Pty Ltd (22/05/1986 - 14/02/1992).')	124
Figure 110: August 1986 – Aerial photograph showing the site already cleared (Source: NSW Spatial Services, Historical Aerial Imagery, 3 August 1986, 3528-22-061)	125
Figure 111: August 1991 – Aerial photograph showing the new buildings completed (Source: NSW Spatial Services, Historical Aerial Imagery, 14 August 1991, 4029-10-133)	125
Figure 112: Overlay of potential archaeological remains within Pyrmont Station western construction site.	135
Figure 113: Overlay of potential archaeological remains within the Pyrmont Station eastern construction site.	138
Figure 114: Significant non-Aboriginal archaeological resources at the Pyrmont Station construction sites.	142
Figure 115: Overview of archaeological management	149
Figure 116: Proposed test trench locations – Western construction site	153

TABLES

Table 1: Heritage items within 25 metres of Pyrmont Station western construction site	21
Table 2: Heritage items within 25 metres of Pyrmont Station eastern construction site	22
Table 3: Overview of historic land use phases of western construction site	36
Table 4: Occupants 22-36 Pyrmont Bridge Road, 1895 - 1915	43
Table 5: Occupants o 129-135 Pyrmont Street, 1895 - 1914.....	48
Table 6: Occupants of 129-135 Pyrmont Street, 1921-1931.....	57
Table 7: Building and development applications, 26-32 Pyrmont Bridge Road, 1951-1999.....	71
Table 8: Overview of historic land use phases	81
Table 9: Occupants of 41-53 Union Street, 1907	95
Table 10: Occupants of 57-69 Union Street, 1911 - 1929.....	96
Table 11: Occupants 68-74 Edward Street, 1883-1895.....	104
Table 12: Occupants 8-14 Pyrmont Bridge Road, 1883-1895	108
Table 13. Grades of archaeological potential.....	126
Table 14. NSW Heritage criteria for assessing significance related to archaeological sites and relics	127
Table 15: Summary of historic phases for western construction site	128
Table 16: Summary of historic phases for eastern construction site	128
Table 17: Summary of archaeological remains and potential at western construction site	133
Table 18: Summary of archaeological remains and potential at eastern construction site	136
Table 19: Assessment of archaeological significance for Pyrmont Station western construction site phase 2 (1883 –1914 –commercial and residential development) and the Pyrmont Station eastern construction site phase 2 (1878 –1940 –commercial and residential development.....	139
Table 20: Summary of significant non-Aboriginal archaeological remains within the Pyrmont Station construction sites	140
Table 21: Historic themes relevant for the predicted archaeological remains	143
Table 22: Definition of archaeological methodologies.....	148

1.0 INTRODUCTION

1.1 Sydney Metro West

Sydney is expanding and the NSW Government is working hard to deliver an integrated transport system that meets the needs of customers now and in the future.

Sydney Metro is Australia's biggest public transport program. Sydney Metro West (the project) is a new 24-kilometre metro line that will connect Greater Parramatta with the Sydney CBD. Confirmed stations include Westmead, Parramatta, Sydney Olympic Park, North Strathfield, Burwood North, Five Dock, The Bays, Pyrmont and Hunter Street (Sydney CBD). This infrastructure investment will double the rail capacity of the Greater Parramatta to Sydney CBD corridor with a travel time target between the two centres of about 20 minutes.

The delivery of Sydney Metro West is critical to keeping Sydney moving and is identified in a number of key strategic planning documents including the Greater Sydney Region Plan: A Metropolis of Three Cities – connecting people (Greater Sydney Commission, 2018a), Building Momentum: State Infrastructure Strategy 2018-2038 (Infrastructure NSW, 2018) and Future Transport Strategy 2056 (Transport for NSW, 2018).

Sydney Metro West was assessed as a staged infrastructure application under section 5.20 of the Environmental *Planning & Assessment Act 1979* (EP&A Act).

The Concept and major civil construction work for Sydney Metro West between Westmead and The Bays (Stage 1 of the planning approval process, application number SSI-10038), was approved by the Minister for Planning and Public Places on 11 March 2021.

Stage 2 of the planning approval process (this proposal) includes all major civil construction work including station excavation at the Pyrmont Station and Hunter Street Station (Sydney CBD) construction sites, and tunneling between The Bays and Sydney CBD.

The proposed major civil construction work between The Bays and Sydney CBD would include:

- Enabling work such as demolition, utility supply to construction sites, utility adjustments, and modifications to the existing transport network
- Tunnel excavation including tunnel support activities between The Bays and Sydney CBD
- Station excavation for new metro stations at Pyrmont and at Hunter Street (Sydney CBD)

This proposal would be located largely underground in twin tunnels. Indicative locations of the proposed alignment and stations are shown in Figure 1.

The Non-Aboriginal Heritage Technical Paper prepared by Artefact Heritage (Artefact) in 2021² concluded that the Pyrmont Station construction sites have the potential to contain locally significant non-Aboriginal archaeological resources. Consequently, it was recommended that an Archaeological Research Design (ARD) be prepared to guide excavation works and provide strategies to mitigate impact to potential archaeological resources (Recommendations NAH6).

This document provides the ARD and Excavation Methodology (ARDEM) for the Pyrmont Station construction sites.

² Artefact Heritage, October 2021, Sydney Metro West – Major civil construction work between The Bays and Sydney CBD, report to Sydney Metro Authority



Figure 1: Overview of Sydney Metro West between The Bays and Sydney CBD

1.2 Pyrmont Station construction sites

1.2.1 Site location

The location of the Pyrmont Station construction sites is shown in Figure 2. The eastern site would be located in the block bound by Pyrmont Bridge Road, Union Street and Edward Street. The western site would be located in the block bound by Pyrmont Bridge Road, Paternoster Row and Pyrmont Street.



Figure 2: Pyrmont Station construction sites

1.2.2 Heritage listings

For the purposes of undertaking a search of statutory registers, the Pyrmont Station construction sites study area consists of the two proposed construction sites and a 25 metre buffer zone. Listed and potential heritage items located within the Pyrmont Station construction sites or buffer are provided in Table 1 and Table 2 respectively, and curtilages are illustrated in Figure 3.

Table 1: Heritage items within 25 metres of Pyrmont Station western construction site

Item	Address	Listing	Significance	Proximity
Pyrmont Heritage Conservation Area	Pyrmont	<ul style="list-style-type: none"> SLEP 2012 Item no. C52 	Local	Within western construction footprint
Quarryman's Hotel	214-216 Harris Street	<ul style="list-style-type: none"> SLEP 2012 Item no. I1232 SREP 2005 (City West) Item no. 56 RNE Place ID 100706 	Local	15 metres south-west
Former Industrial Building 'Waite and Bull' Also known as 'John Taylor Wool Stores'	137 Pyrmont Street, Pyrmont	<ul style="list-style-type: none"> SLEP 2012 Item no. I1263 SREP 2005 (City West) Item no. 54. RNE Place ID2036 NTR no. 10858 	Local	25 metres south

Item	Address	Listing	Significance	Proximity
Gilbey's Distillery	26-32 Pyrmont Bridge Road, Pyrmont	<ul style="list-style-type: none"> • Unlisted 	Potential local heritage item	Within western construction footprint

Table 2: Heritage items within 25 metres of Pyrmont Station eastern construction site

Item	Address	Listing	Significance	Proximity
Former New York Hotel	50 Union Street, Pyrmont	<ul style="list-style-type: none"> • SLEP 2012 Item no. I1275 • SREP 2005 (City West) Item no. 67 • RNE Place ID 100709 	Local	15 metres north
Former Warehouse 'Bank of NSW Stores'	17-21 Pyrmont Bridge Road	<ul style="list-style-type: none"> • SLEP 2012 Item no. I1256 • SREP 2005 (City West) Item no. 53 • RNE Place ID 100704 	Local	20 metres south
Corner Shop and Residence 'Charmelu'	63-67 Edward Street, Pyrmont 'Charmelu' street address on 35 Union Street, Pyrmont	<ul style="list-style-type: none"> • SLEP 2012 Item no. I1213 • SREP 2005 (City West) Item no. 68 	Local	20 metres west
Terrace Group	31-33 Union Street, Pyrmont	<ul style="list-style-type: none"> • SLEP 2012 Item no. I1274 • SREP 2005 (City West) Item no. 122 	Local	20 metres west
Pyrmont Bridge Road Hotel	11 Pyrmont Bridge Road, Pyrmont	<ul style="list-style-type: none"> • SLEP 2012 Item no. I1255 • SREP 2005 (City West) Item no. 52 • RNE Place ID100703 	Local	20 metres south

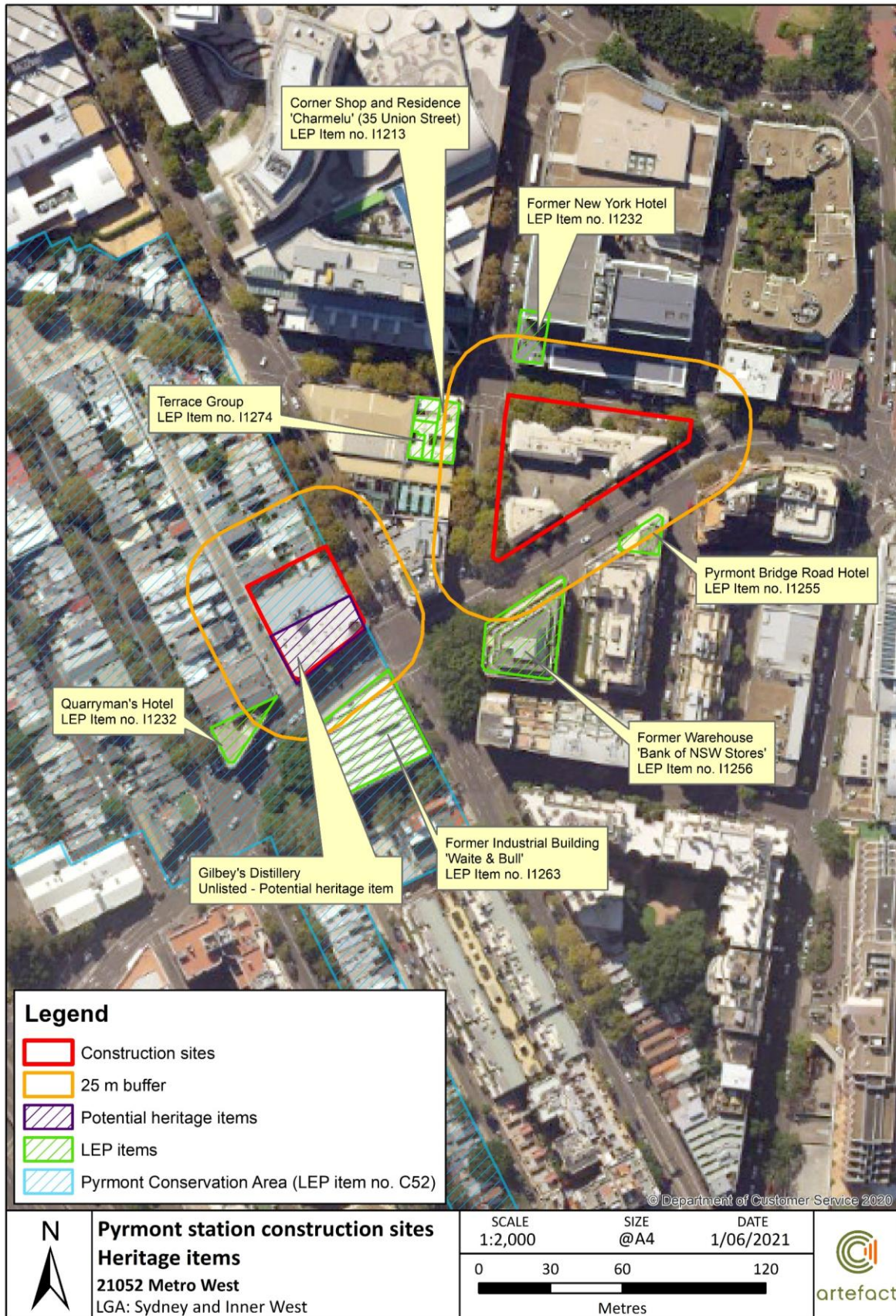


Figure 3: Location of heritage items within the study area for the Pyrmont Station construction sites

1.3 ARDEM methodology

This ARDEM has been developed utilising additional historical archival research into the Pyrmont Station construction sites in addition to the historical background prepared for the 2021 Technical Paper, as presented in Section 2.0. An updated assessment of archaeological potential and significance is presented in this document.

The archaeological management strategies recommended in this ARDEM are based on this revised assessment.

1.4 Report limitations

This Archaeological Methodology and Research Design has been prepared to provide an assessment of Non-Aboriginal (Historical) archaeological heritage values only.

1.5 Authorship and acknowledgements

This archaeological research design and excavation methodology report was prepared by Sammuell Sammut (Heritage Consultant) and Jenny Winnett (Principal). A detailed historical summary of the Pyrmont Metro station was prepared by Dr. Martina Muller. Management input and review was provided by Dr Sandra Wallace (Managing Director).

2.0 HISTORICAL CONTEXT

2.1 Introduction

This section presents a history of the study area. The aim of the research in the broadest sense is to determine the sequence of historical events that occurred on the site and their context. The emphasis in this history is on events that would create, modify, and remove archaeological evidence.

In order to better understand the course or pattern of the study area's history; the historical evidence has been organised into discrete periods or phases of time. This site history is then considered in the light of the Australian and NSW State historical themes in order to place the site history into broader contexts of the course or pattern of Australian or NSW's history.³

A detailed discussion of historic themes relevant for this assessment is provided in Section 4.1.

The following discussion of land-use has been divided into the western and eastern construction sites.

2.2 Aboriginal Country

The following summary has largely been copied verbatim from Section 7.1 in 'Technical Paper 4: Aboriginal Cultural Heritage Assessment Report', prepared by Artefact Heritage in October 2021:⁴

The Aboriginal people who lived in the Sydney region were part of the Eora Nation. The word Eora, meaning 'here' or 'of this place', is not a term traditionally used by Aboriginal communities pre-contact but arose during the first encounters with non-Aboriginal settlers.⁵ It is now accepted as an appropriate term for the coastal Aboriginal peoples in the broader Sydney area.

Phillip Gidley King recorded in 1793 that:

*The tribe of Cadi inhabit the south side, extending from the south head to Long-Cove; at which place the district of Wanne, and the tribe of Wangal, commences, extending as far as Par-ra-mata, or Rose-Hill.*⁶

Modern streets of Sydney, including George Street, are thought to have originated as Aboriginal tracks connecting areas of plentiful food or fishing opportunities.⁷ These tracks were worn into the landscape through years of use and managed with cultural burning.⁸

The Pyrmont Peninsula appears to form part of the boundary between the Gadigal (also known as Cadigal) and the Wangal tribes. Early recordings by Governor Phillip describe the Wangal lands as:

³ C.f. New South Wales Heritage Office. Assessing Historical Importance a Guide to State Heritage Register : Criterion A. Heritage Office (Parramatta, N.S.W.: 2006)

⁴ With minor changes and updated references, for the original version see Artefact Heritage, 'Sydney Metro West - Major Civil Construction Work between The Bays and Sydney CBD. Technical Paper 4: Aboriginal Cultural Heritage Assessment Report. Report to Sydney Metro', October 2012, 35–38.

⁵ Anita Heiss and Melodie-Jane Gibson, 'Aboriginal People and Place | Barani. Sydney's Aboriginal History | City of Sydney', 2013, <https://www.sydneybarani.com.au/sites/aboriginal-people-and-place/>.

⁶ Quoted in Dominic Steele Consulting Archaeology, 'Application for a S87 Preliminary Research Permit Aboriginal Archaeological Assessment, Research Design, and Excavation Methodology – 320-328 George Street PAD, George Street, Sydney NSW. Report to Caverstock Group', 2005, 35.

⁷ *Ibid.*

⁸ 'Redfern Oral History: Gadigal People', accessed 10 February 2022, <http://redfernoralthistory.org/Timeline/Gadigalpeople/tabid/240/Default.aspx/>.

*The south of the harbour, from the above-mentioned cove (present day Darling Harbour) to Rose Hill, which the natives call Paramatta.*⁹

Phillip continues to describe the district as Wann, and the clan as Wangal. The Darug (alternatively Darruk) language would have been spoken by both Gadigal and Wangal clans, as it was spoken across the Sydney region, from the Hawkesbury River in the north to Appin in the south, west of the George's River to the Blue Mountains, as well as along the coast between Port Jackson and Botany Bay.¹⁰ It is likely that dialects would have differed between the coast and inland areas, although documentary evidence of this is thin.

Ethnohistorical sources suggest that, despite differences in dialect and customs, the Wangal, Gadigal, and other Darug clans of the Eora Nation would have interacted for ceremonies, intermarriage, dispute resolution, trade, and access to resources. Early accounts report large gatherings of clans for social or religious events, or to share access to abundant resources. For example, Tench records that in September 1790 at least 200 Aboriginal people came together to harvest a whale beached in Manly Cove.¹¹ He notes that the gathering included members from the Wangal, Gadigal, and Broken Bay clans.

Archaeological and historical records indicate that marine and estuarine resources formed an important part of the Port Jackson area. Aboriginal communities consumed large amounts of marine animals, evidenced by the large number of shell middens in the Sydney area, where the shellfish were processed onsite for the meat to be consumed or used as fishing bait.¹² Watkin Tench noted that "fishing, indeed, seems to engross nearly the whole of their time, probably forming the chief part of their subsistence".¹³ Tench recorded observing fish, including "bass, mullets...and sharks of an enormous size",¹⁴ populating the waters of Port Jackson, indicating the wide variety of marine animals available for consumption. Shellfish not only formed an important subsistence resource, but also were utilised as tools. Shell tools included fishhooks, shell hafted onto spears in various forms, as a tool to repair spears, and as a cutting edge.¹⁵ Other locally available raw materials, including quartz, were also favoured for cutting edges, and in some areas bordering readily abundant shellfish in inner Sydney, quartz may have actually been favoured as a cutting edge.¹⁶

The Aboriginal people of Cockle Bay utilised the wide range of animals native to the region for food and tool making. Mammals such as kangaroo and wallabies, as well as arboreal mammals such as possums were in plentiful supply, with cultural burning used to facilitate hunting. Ethnographic observations by early non-Aboriginal colonists noted that Aboriginal people used a variety of tools and non-utilitarian functions. Plant products like gums and sap were used for binding and hafting stone hatchets and plant fibres were woven into baskets, nets, ropes, and hammocks. Locally sourced

⁹ A. Phillip, 'Letter from Governor Phillip to Lord Sydney, Government House, Sydney Cove, February 12th, 1790', *Historical Records of NSW* 1, no. 2 (1892): 309.

¹⁰ Val Attenbrow, 'Pre-Colonial Aboriginal Land and Resource Use in Centennial, Moore, and Queens Parks – Assessment of Historical and Archaeological Evidence for Centennial Parklands Conservation Management Plan. Report for Incorporation into Overall Conservation Management Plan Prepared by Beyond Consulting for Conybeare Morrison and Partners', 2002, 34.

¹¹ W. Tench, *Complete Account of the Settlement of Port Jackson* (London: G. Nicol and J. Sewell, 1793).

¹² Attenbrow, Val, 'Archaeological Evidence of Aboriginal Life in Sydney | The Dictionary of Sydney', 2012, https://dictionaryofsydney.org/entry/archaeological_evidence_of_aboriginal_life_in_sydney.

¹³ Cited in Kelleher Nightingale Consulting, 'Pyrmont Peninsula Place Strategy: Indigenous Cultural Heritage Report. Report to DPIE', 2020.

¹⁴ Watkin Tench, *1788, Edited and Introduced by Tim Flannery* (Melbourne: Text Publishing, 2009).

¹⁵ Attenbrow, Val, 'Archaeological Evidence of Aboriginal Life in Sydney | The Dictionary of Sydney'.

¹⁶ N Baker, 'Archaeological Salvage of an Aboriginal Site at Williams Street, East Sydney. Report to Zonie Construction and Design Pty Ltd', 2004, 31.

plants were also used in the manufacture of shelters, shields, and coolamons used to carry food and water.

2.3 Summary History of the Pyrmont area

The following summary has largely been copied verbatim from Section 5.3.1 in 'Technical Paper 3: Non-Aboriginal Heritage', prepared by Artefact Heritage in October 2021:¹⁷

Following the establishment of the colony at Port Jackson, Governor Arthur Phillip declared the boundaries of the township of Sydney in 1792.¹⁸ At this time, despite its close proximity to the settlement at Port Jackson, Pyrmont had been excluded from the township due to the lack of European land grants, subdivision and settlement. During the early days of European settlement, Pyrmont was depicted as an isolated place, remaining largely undeveloped until into the c1840s. Early accounts recall a distinct Aboriginal presence into the c1830s, particularly centring around 'Tinker's Well', a natural spring located on the south-western side of Pyrmont Hill facing Glebe Island.¹⁹ A 1912 *Evening News* article details mussel shells and shellfish remains within vicinity of this natural spring.²⁰

The first land grants in the area occurred from 1794 and were granted to soldiers of the New South Wales Corps - John Malone, William Mitchell and Thomas Jones, who received 24, 18 and 55 acres each respectively (Figure 5).²¹ Another significant landowner in the area was John Harris, who acquired four land grants from 1803-1818.²² Harris, the Corps Surgeon, was the owner of the Ultimo Estate upon which his grand residence 'Ultimo House' was built.²³

Access into Pyrmont during these early years of settlement was likely carried out by boat. Captain John Macarthur made an excursion to the area in December 1806, picnicking near Tinkers Well.²⁴ The area comprised gentle hills covered in grass, a result of Aboriginal fire-stick farming. These green hills, coupled with the natural spring reminded Macarthur and his excursion party of a German spa town named 'Bad Pyrmont', subsequently providing the suburb its present name.²⁵ By 1818 Macarthur had acquired Jones' grant while the grants made to Mitchell and Malone had been transferred to Harris. Macquarie's property was in 1839-40 sold as the Pyrmont Estate (Figure 4).²⁶

Substantial development did not occur in the area until the 1840s, which saw the establishment of Pyrmont as an industrial community. The main improvements carried out by Macarthur were the construction of a windmill for the processing of grain, which operated for a few years before closing to the competition of larger mills closer to the heart of the new settlement.²⁷ Interestingly, the lack of development – and the view of Pyrmont in the minds of the settlement as a peripheral, isolated

¹⁷ With minor changes to text and figures, for the original version see Artefact Heritage, 'Sydney Metro West - Major Civil Construction Work between The Bays and Sydney CBD. Technical Paper 3: Non-Aboriginal Heritage. Report to Sydney Metro Authority', October 2021, 35–42.

¹⁸ Weir Phillips Architects and Heritage Consultants, 'Heritage Impact Statement: 212 Harris Street, Pyrmont', March 2012, 3.

¹⁹ Shirley Fitzgerald, 'Pyrmont | The Dictionary of Sydney', 2008, <https://dictionaryofsydney.org/entry/pyrmont>.

²⁰ 'Tinker's Well', *Evening News*, 12 December 1912, 11, <http://nla.gov.au/nla.news-article113775147>.

²¹ Heritage 21, 'Statement of Heritage Impact for Proposed Development at Chambers Cellars, 1/17-21 Pyrmont Bridge Road, Pyrmont', May 2013, 8.

²² Weir Phillips Architects and Heritage Consultants, 'Heritage Impact Statement: 212 Harris Street, Pyrmont', 4.

²³ Weir Phillips Architects and Heritage Consultants, 5.

²⁴ Fitzgerald, 'Pyrmont | The Dictionary of Sydney'.

²⁵ Sydney Harbour Foreshore Authority, 'Heritage and Conservation Register - Old Pyrmont Cottages', New South Wales (NSW), accessed 10 February 2022, http://www.shfa.nsw.gov.au/sydney-About_us-Heritage_role-Heritage_and_Conservation_Register.htm&objectid=150.

²⁶ Shirley Fitzgerald and Hillary Golder, *Pyrmont & Ultimo: Under Siege* (Sydney: Hale & Iremonger, 1994), 24–25.

²⁷ Fitzgerald, 'Pyrmont | The Dictionary of Sydney'.

location – meant that the Aboriginal population of Pyrmont were able to remain in the area until around the 1930s, when accounts of their presence diminish.²⁸

In the 1840s, nearby Darling Island was built up and incorporated into the mainland of Pyrmont, becoming the home of the Australian Steam Navigation Company.²⁹ Shipbuilding thus became a key land use in the Darling Island Area of Pyrmont, with the company approved to repair ships up to 2,000 tons in 1855.³⁰ Alongside this, quarrying emerged as a key industry within the Ultimo Estate from the 1840s onwards. The rich Pyrmont Sandstone resources of the area were hewn until the turn of the century.³¹ The legacy of this quarry is seen throughout the city in the rich locally sourced sandstone used to construct key government and civic buildings. In addition, abattoirs were established on Glebe Island in 1860, representing another key source of employment in the area.³²

Pyrmont village grew out of the introduction of industry in the area and increased its desirability for living due to its proximity to places of work.³³ In 1844, Pyrmont was officially incorporated into the City of Sydney area as a permanent population grew.³⁴ Many stone workers cottages were constructed at the time. Pyrmont emerged and matured as a shared space between residents and industry.³⁵ Maps show the village of Pyrmont had been subdivided and partly developed by 1854 (Figure 6), though little development had occurred in the proposal area on the south side of Union Street at this time. Alongside industry and the movement of people to Pyrmont, key civic buildings began to emerge to cater to the growing population, including the first church, a Scottish Presbyterian Church, erected in 1842.³⁶ The first school was established in 1850, a Catholic Church in the 1860s, and Methodist and Congregational churches in the 1870s.

The accessibility and desirability of the Pyrmont area further increased when the Pyrmont Bridge opened in 1858 allowing industrial workers direct access from the city. Subdivision of the area increased with the death of Harris and the subdivision of Ultimo Estate, and the further growth within the Pyrmont Estate and village.³⁷ In line with this growth, nearby streets such as Pyrmont Street, Harris Street and Union Street were officially formed and ballasted, with kerb and guttering constructed around 1862 on these major streets.³⁸ Some streets like Harris and John Streets were also partially metalled according to Council Improvement Books.

In the following decades, the emerging Pyrmont appeared as a mixed-use precinct combining industrial, commercial and residential developments. Industry continued to move into the area, with the CSR sugar refining factory established as a key landmark to the north from 1875.³⁹ Other

²⁸ Fitzgerald.

²⁹ Heritage 21, 'Statement of Heritage Impact for Proposed Development at Chambers Cellars, 1/17-21 Pyrmont Bridge Road, Pyrmont', 8.

³⁰ Fitzgerald, 'Pyrmont | The Dictionary of Sydney'.

³¹ Weir Phillips Architects and Heritage Consultants, 'Heritage Impact Statement: 212 Harris Street, Pyrmont', 5.

³² Fitzgerald, 'Pyrmont | The Dictionary of Sydney'.

³³ Weir Phillips Architects and Heritage Consultants, 'Heritage Impact Statement: 212 Harris Street, Pyrmont', 4.

³⁴ 'Samuel Hordern Fountain Including Base and Setting | Heritage NSW', accessed 10 February 2022, <https://apps.environment.nsw.gov.au/dpcheritageapp/ViewHeritageItemDetails.aspx?ID=2424682>.

³⁵ Fitzgerald, 'Pyrmont | The Dictionary of Sydney'.

³⁶ Heritage 21, 'Statement of Heritage Impact for Proposed Development at Chambers Cellars, 1/17-21 Pyrmont Bridge Road, Pyrmont', 9.

³⁷ Weir Phillips Architects and Heritage Consultants, 'Heritage Impact Statement: 212 Harris Street, Pyrmont', 6.

³⁸ 1862 reports of city improvements, held by City of Sydney Archives, see 'Reports - Improvement Committee, Vol. 1, 1862', City of Sydney Archives, accessed 10 February 2022, <https://archives.cityofsydney.nsw.gov.au/nodes/view/952162>; 'Reports - Improvement Committee, Vol. 2, 1862', City of Sydney Archives, accessed 10 February 2022, <https://archives.cityofsydney.nsw.gov.au/nodes/view/952164>.

³⁹ Weir Phillips Architects and Heritage Consultants, 'Heritage Impact Statement: 212 Harris Street, Pyrmont', 7; Heritage 21, 'Statement of Heritage Impact for Proposed Development at Chambers Cellars, 1/17-21 Pyrmont Bridge Road, Pyrmont', 8.

factories and workshops were built in the area, including the significant John Taylor Wool Stores in Pyrmont Street.⁴⁰

By the 1890s, Pyrmont had a population of over 19,000 people with just under 4,000 dwellings, representing a densely populated township. At this time, with the booming population and growing industry, much of the early housing stock was rebuilt and replaced with brick terraces to make more efficient use of space.⁴¹ Competition also ensued between industry and extant residents in some locations over increasingly desirable and scarce land, with industry ultimately winning out and more and more land consumed by industrial activities like the Pyrmont Power Station.⁴²

Into the twentieth century, the growing pressure on land in proximity to the city led to the post-war shift of industry out of the area. Pyrmont took on an increasingly residential character from this point forward.⁴³ This loss of industry led to decreases in population as workers moved to follow employment. Subsequently, the area became less affordable, and schools and other services also left the area.⁴⁴ The later decades of the twentieth century saw the retention and adaptive reuse of warehouses to new residential and commercial purposes, but also the demolition and replacement of older development with larger scale development including new office buildings, apartment blocks, and the major Star Casino to the east of the subject site. Pyrmont retains evidence of its original undulating land form and key evidence of its working class and industrial past, while the Pyrmont Conservation Area partially encompassing the site serves to retain the built remains of this former history.



Figure 4: c1839 – Detail from ‘Plan of Pyrmont Estate (...)’, showing the subdivision of the Pyrmont Estate (Source: State Library of NSW Z/M2 811.173/1839/1, <https://collection.sl.nsw.gov.au/record/74VvM6VbD4Pd/AVRya4rQa6kQk>)

⁴⁰ Heritage 21, ‘Statement of Heritage Impact for Proposed Development at Chambers Cellars, 1/17-21 Pyrmont Bridge Road, Pyrmont’, 8; Heritage 21, ‘Statement of Heritage Impact for Proposed Development at 214-216 Harris Street, Pyrmont, Quarryman’s Hotel’, December 2017, 8.

⁴¹ Heritage 21, ‘Statement of Heritage Impact for Proposed Development at Chambers Cellars, 1/17-21 Pyrmont Bridge Road, Pyrmont’, 8.

⁴² Fitzgerald, ‘Pyrmont | The Dictionary of Sydney’.

⁴³ Fitzgerald; Heritage 21, ‘Statement of Heritage Impact for Proposed Development at 214-216 Harris Street, Pyrmont, Quarryman’s Hotel’, 8.

⁴⁴ Fitzgerald, ‘Pyrmont | The Dictionary of Sydney’.



Figure 5: c1839 – Detail from Map of the Parish of St Andrew, County of Cumberland, showing the early land grants made in the Pyrmont and Ultimo areas (Source: State Library of NSW Z/M2 811.173/1839/1, <https://collection.sl.nsw.gov.au/record/74VvM6VbD4Pd/AVRya4rQa6kQk>)

2.4 Early Ownership and Development of the study Area

Both study areas are located on land that was part of a Crown Grant made to John Malone on 10 October 1794 (Figure 5).⁴⁵ Malone's property was one of three grants made in the same locality by Lieutenant-Governor Grose, the others being William Mitchell's 18-acre grant of December 1794 and Thomas Jones' 55-acre grant of March 1795. Although Grose made the grants under the condition that the grantees would have to reside on their properties, and improve and cultivate the land, Malone "speedily parted with his interest in the property."⁴⁶ Several transfers were noted on the original deed of grant, some of which were illegible, but one of which was from Sergeant Sherwin to John Fitzpatrick Jefferie on 30 September 1800.⁴⁷

On 1 January 1806, Malone's property was purchased by Surgeon John Harris, together with Mitchell's grant to the south.⁴⁸ Harris, who had been granted the nucleus of the 'Ultimo' estate (34 acres) in 1803 and who received another 135 adjacent acres in 1806, now owned the majority of the Ultimo and Pyrmont peninsula. His residence, 'Ultimo House' (built in 1804), was for a long time the

⁴⁵ NSW LRS, Certificate of Title (CT) Vol 3670 fol 90.

⁴⁶ Sydney Morning Herald and Edward Hordern & Sons, 'XVIII. - Pyrmont and Ultimo', *Old and New Sydney*, no. 6 (1882): 4, <http://nla.gov.au/nla.obj-754630143>.

⁴⁷ Sydney Morning Herald and Edward Hordern & Sons, 4.

⁴⁸ NSW LRS, Primary Application (PA) 9768, cf. Michaela Ann Cameron and Alexander Cameron-Smith, 'A "Raw, Ignorant Boy": John Harris, Esquire', *St. John's* (blog), 29 April 2020, <https://stjohnsonline.org/bio/john-harris/>.

only house on the Ultimo Estate, and Harris used the property largely for grazing, extending his landholding to 233 acres by 1818.⁴⁹

In the late 1820s, John Harris gifted a parcel of land on the western side of Cockle Bay, on the eastern side of the peninsula, to Anna-Maria Bunn, née Murray.⁵⁰ Harris was a friend of Anna-Maria Bunn's father, Captain Terence Murray, and the property was a wedding present to her. She had married George Bunn in 1828, and together with her husband, she took up residence on the property, with George building a wharf in 1832 and constructing a sandstone residence in 1833. George Bunn died in 1834, and until 1841, Anna-Maria and her two young sons continued to live at "Mrs Bunn's Cottage", also known as 'Newstead House'. In 1838, her novel *The Guardian: a Tale*, written at Newstead House, was published. It was the first novel published in mainland Australia, and the first by a woman. The house, which was still the only major building in that area by the 1870s, is shown on early maps (Figure 4 and Figure 6) and an 1873 photograph (Figure 10).

Meanwhile, Thomas Jones' 55-acre grant at the tip of the Pyrmont peninsula had been transferred to Captain John Macarthur, and after Macarthur's death in 1834, his son Edward offered the Pyrmont Estate for subdivision in 1839 and 1840.⁵¹ This first subdivision sale in 1839, which was very successful, was for the land north of Union Street and to the east of the northern portion of Harris Street, laid out in 1836 (Figure 4).

After John Harris' death in 1838, one year after the death of his wife Eliza, the Ultimo estate was transferred to his brothers, William and George, who arrived from Ireland in about 1841.⁵² In his will, John Harris' noted that in the event of his brothers' death, the estate was to pass on to their respective sons, both named John, and subsequently to any other heirs named John Harris. After the death of George in 1843, that of George's son John in 1846, and of William in 1856, the estate was subdivided in 1859 and distributed among John Harris, the son of William Harris who lived at Shane's Park (today known as Harris Park) at the time, and the wife and children of John Harris, the son of George Harris.⁵³

As part of the partition, Block 57, on which the Western Construction Site is located, was transferred to John Harris of Shane's Park, while Block 59b, containing the Eastern Construction Site, was transferred to the family of John Harris, son of George Harris.⁵⁴ The eventual allocation of parcels saw Block 59b initially allocated to Mary Ann and Matthew Harris, as shown on the plan in Figure 8, which was a tracing of the original plan of partition made in 1859. However, in 1867 the northern portion (including the Pyrmont Station eastern construction site) was also transferred to John Harris of Shane's Park, who now owned both study areas.⁵⁵

John Harris was primarily occupied with his sheep station at Murrumburrah and his cattle station at Callagoon, as well as with other pastoral land holdings, and was in 1891 described as not being "very intimate with any of his relatives" who owned the other half of the Ultimo Estate. John Harris died at Harris Park Station in December 1891, aged 81, when he was accidentally hit by a train.⁵⁶

⁴⁹ Sydney Morning Herald and Edward Hordern & Sons, 'XVIII. - Pyrmont and Ultimo', 4; Mark Dunn, 'Ultimo House | The Dictionary of Sydney', 2010, https://dictionaryofsydney.org/entry/ultimo_house.

⁵⁰ 'Bunn, Anna Maria', Pyrmont History Group, accessed 16 January 2022, <https://pyrmonthistory.net.au/anna-maria-bunn>; Colin Fowler, 'Bunn's Cottage' (n.d.).

⁵¹ Fitzgerald and Golder, *Pyrmont & Ultimo: Under Siege*, 24–25.

⁵² C. H. Bertie, 'Pioneer Families of Australia. No. 42 - The Harris Family', *The Home: An Australian Quarterly*, 1 June 1933, 58, 42. 58, <http://nla.gov.au/nla.obj-371998287>.

⁵³ The partition is noted in the Indenture, Bk 62 no. 705, which was a deed of partition between James Fullerton (trustee of the will of John Harris), John Harris of Shane's Park at South Creek, and Henry Burton Bradley, solicitor, likely acting on behalf of the heirs of John Harris, son of George Harris.

⁵⁴ NSW LRS, Indenture Bk 62 No 705.

⁵⁵ The original plan of partition is included in Indenture Bk 62 No 705. See NSW LRS, Primary Application (PA) 12759 and PA 56103 for further details.

⁵⁶ 'Death of Mr. John Harris.', *Bathurst Free Press and Mining Journal*, 23 December 1891, 2, <http://nla.gov.au/nla.news-article64224614>; Bertie, 'Pioneer Families of Australia. No. 42 - The Harris Family', 58.

After his death, Block 57 was sold to new owners outside the Harris family, while block 59b remained in the ownership of the Harris family until well into the 20th Century. Both blocks remained undeveloped until the late 1870s and early 1880s, although they appear to have been cleared and may have been used for grazing, potentially containing small sheds and associated structures that have not been documented.

By the time the plan of partition had been drawn up in 1859, the Pymont Bridge had been constructed and Pymont Bridge Road formed. Pymont Bridge, opened on 17 March 1857, provided a much-needed connection with the city, and Pymont Bridge Road connected this bridge with another bridge that was planned across Blackwattle Swamp (Figure 7).⁵⁷ The alignment of the Pymont Bridge Road, constructed in c1858, had a major impact on Blocks 57 and 59b of the Ultimo Estate, as it cut right across both blocks (see Figure 7 and Figure 8).

While Pymont Bridge Road was constructed by 1859, the northern side between Union Street and Harris Street remained undeveloped, as shown on the c1865 Trigonometrical Plan of Sydney (Figure 9) and in a photograph dated 1873 (Figure 10). The *Sands Sydney Directory* did not list Pymont Bridge Road until 1879, when it noted four persons and vacant land to the east of Harris Street. This was also the date when John Harris leased the block of land containing the Western Construction Site to Mills & Pile Co, initiating the first major development of the land on the corner of Pymont Bridge Road and Pymont Street. Development of the Eastern Construction Site began at around the same time.

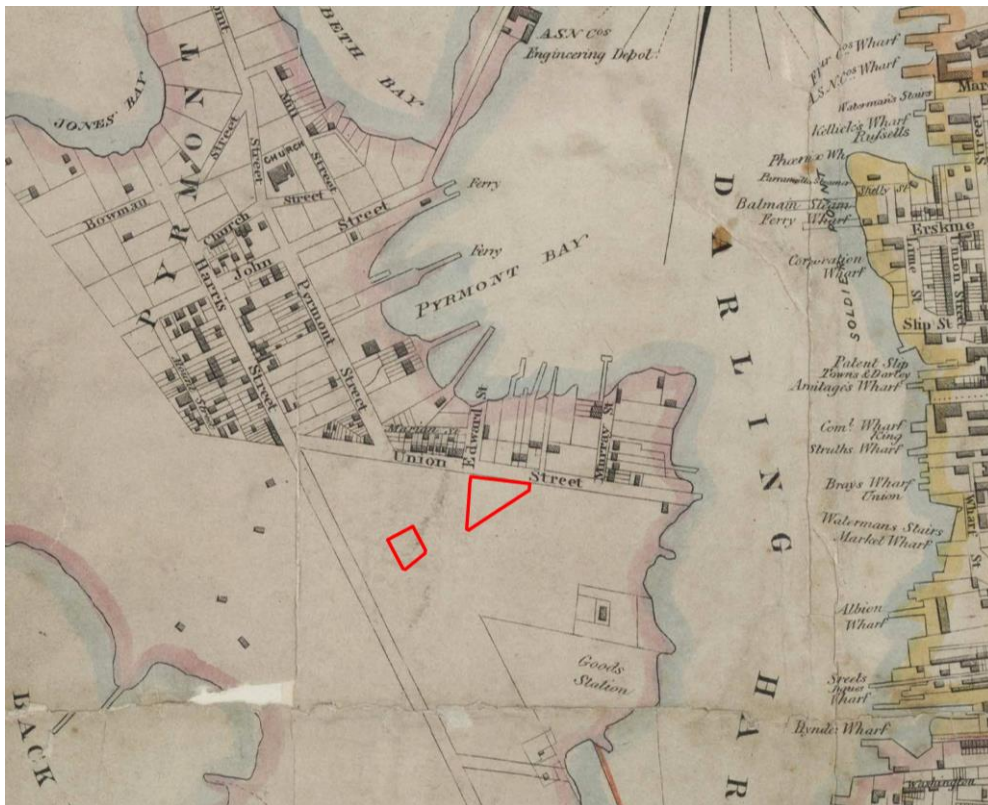


Figure 6: 1854 – Detail from 'Woolcott & Clarke's map of the City of Sydney', noting houses on the north side of Union Street and on the west side of Darling Harbour, including the 'Goods Station' (Source: State Library of NSW, Z/M2 811.15/1854/1, <https://collection.sl.nsw.gov.au/record/74VMbBv5kAGA>)

⁵⁷ 'Pymont Bridge', Pymont History Group, accessed 17 January 2022, <https://pymonthistory.net.au/pymont-bridge>; Christa Ludlow, 'Pymont Bridge | The Dictionary of Sydney', 2010, https://dictionaryofsydney.org/entry/pymont_bridge.



Figure 7: 1857 – Detail from ‘A plan of part of Sydney and its environs’ by Allan & Wigley, showing the development of roadways, including the proposed new Pyrmont Bridge Road, connecting the two new bridges (Source: State Library of NSW, Z/M2 811.15/1854/1, <https://collection.sl.nsw.gov.au/record/74VMbBv5kAGA>)

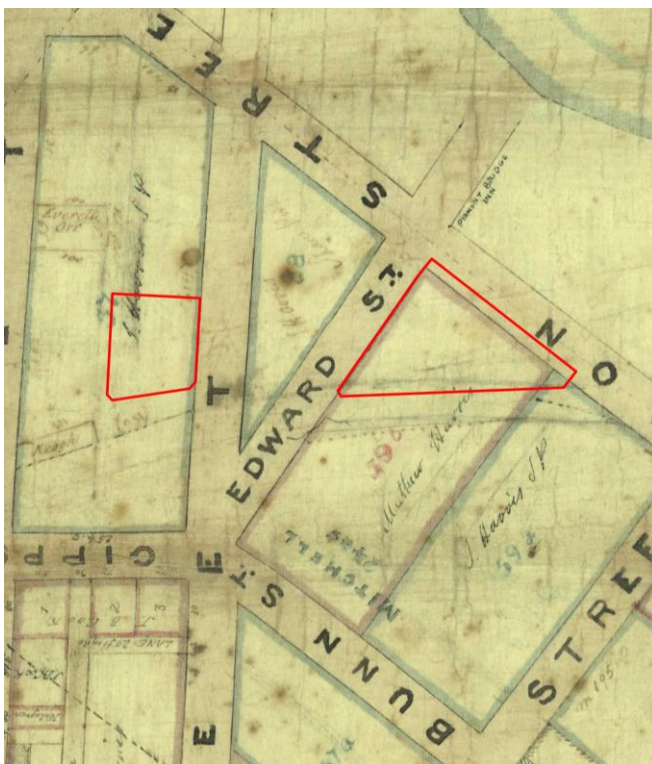


Figure 8: c1859 – Detail from tracing of plan of the ‘Ultimo Estate’, Pyrmont, showing the original street grid with Block 57 at the top. The outline of the future Pyrmont Bridge Road is pencilled in, cutting across Blocks 59 a & b, Block 58 and Block 57. (Source: State Library of NSW, Z/M4 811.173/1866/1, <https://collection.sl.nsw.gov.au/record/74VKmpo8gpdg>)

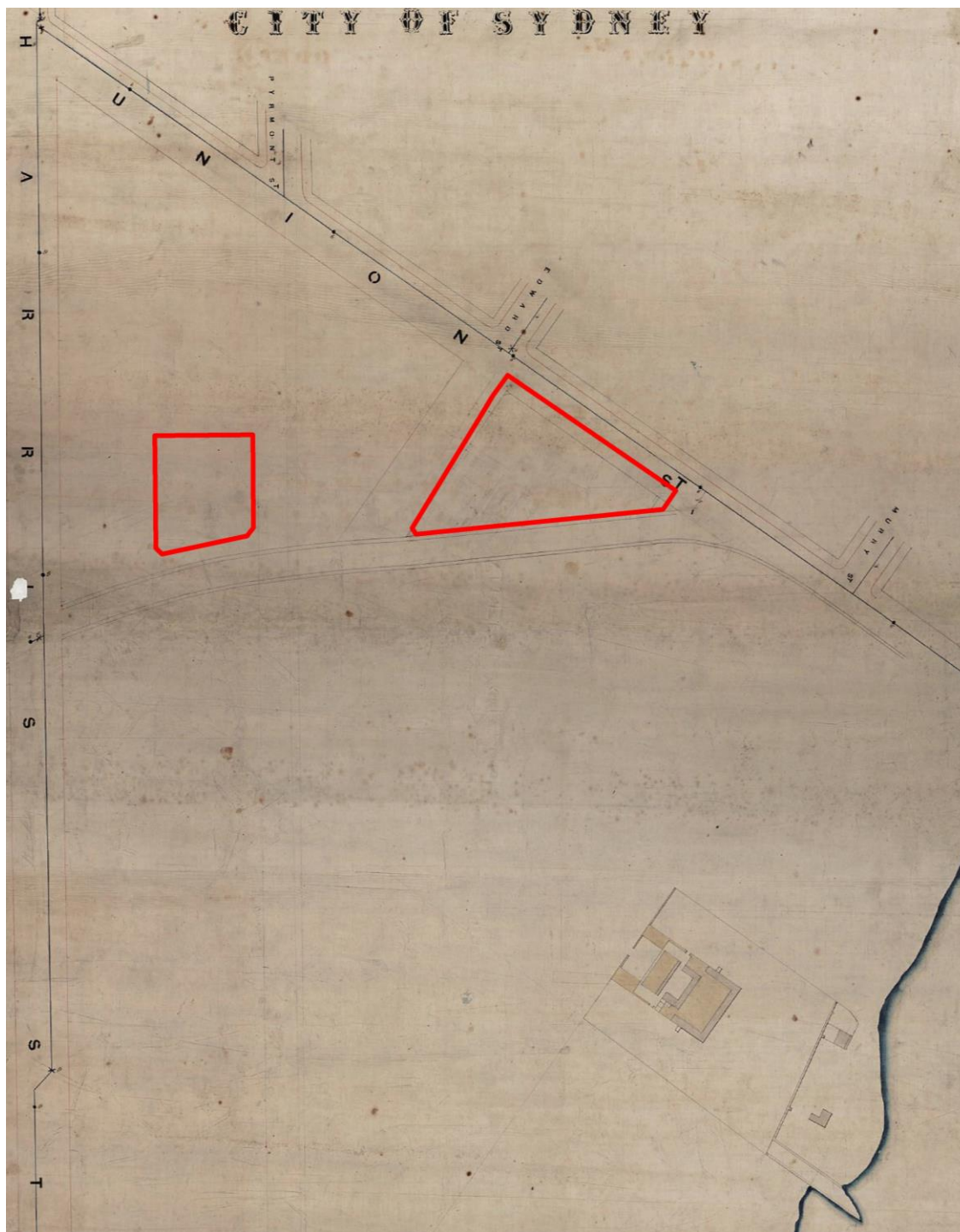


Figure 9: 1865 – Detail from the Trigonometrical Survey of Sydney, Block W1, showing the vacant land on the northern side of Pyrmont Bridge Road, between Union and Harris Streets, with Mrs Bunn's house noted at the bottom right. (Source: City of Sydney Archives, City of Sydney - Trigonometrical Survey, 1855-1865: Block W1 (01/01/1865 - 31/12/1865), [A-00880414]. <https://archives.cityofsydney.nsw.gov.au/nodes/view/1709341>)

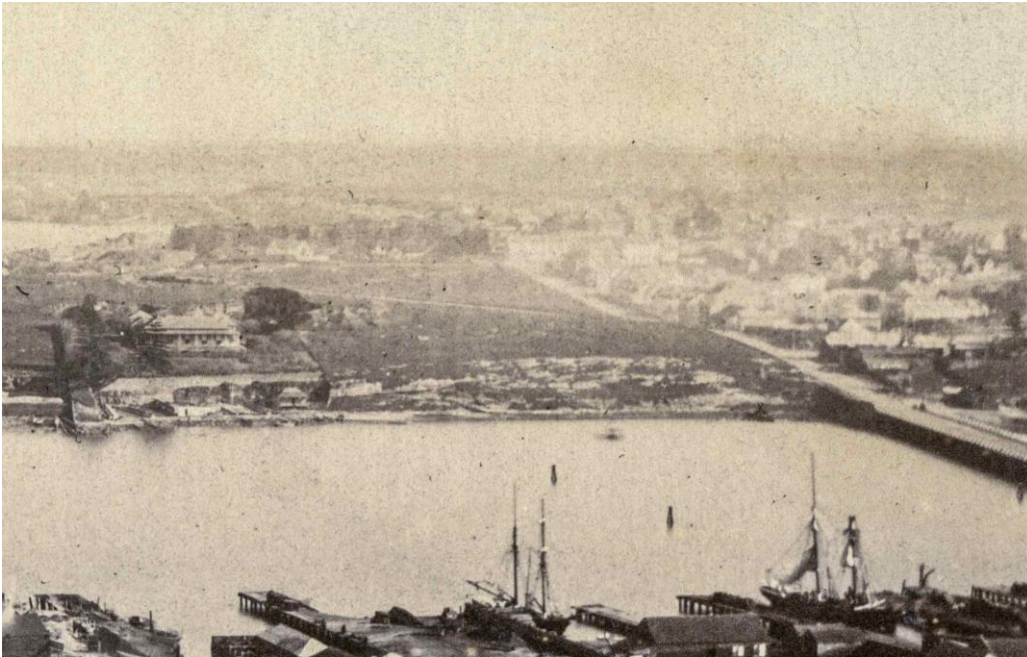


Figure 10: 1873 – Detail from ‘Aerial view west from Town Hall clock tower, 1873,’ showing Pyrmont in the background. Mrs. Bunn’s house is visible on the left, with the newly laid out Pyrmont Bridge Road running behind it, forking off Union Street on the right. The land on the northern side of Pyrmont Bridge Road was still undeveloped (Source: City of Sydney Archives, (01/01/1873 - 31/12/1873), [A-00024418]. <https://archives.cityofsydney.nsw.gov.au/nodes/view/585063>)

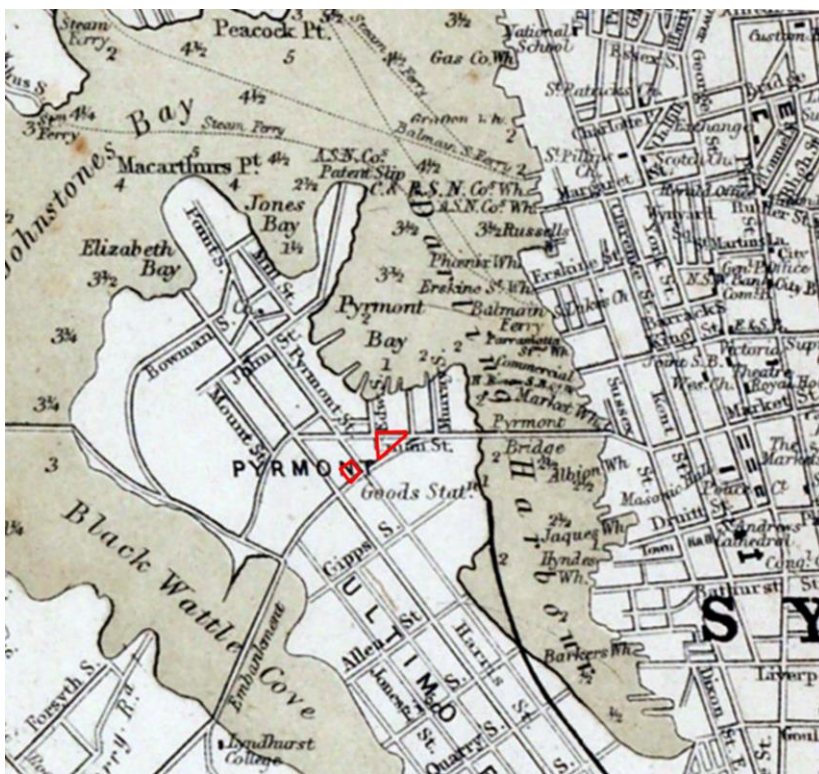


Figure 11: 1875 – Detail from ‘Map of Port Jackson and city of Sydney’ by S. T. Leigh & Co. (Source: State Library of NSW, Z/M2 811.12/1875/1, <https://collection.sl.nsw.gov.au/record/74Vv0KjLdxZM/qwzpwpm6kZkepJ>)

2.5 Western construction site land-use overview

A summary of the historical use of the study area discussed in this chapter is provided in Table 3 below.

Table 3: Overview of historic land use phases of western construction site

Phase	Date	Use of site
Phase 1	1788-1879	Part of the Ultimo Estate, potentially used for livestock grazing
Phase 2	1879-1914	Residential housing and commercial properties
Phase 3	1914-1947	Resumed for road widening, potential storage area for building material or soil, residential housing and commercial properties
Phase 4	1947-present	Gilbey's Distillery and connected carpark, 'Frankel House' and connected basement carpark

2.5.1 Phase 1 (1788-1879): Early land clearance and grazing

The Western construction site at 26-31 Pyrmont Bridge Road, Pyrmont, is located on Block 57 of the Ultimo Estate subdivision and remained undeveloped until the late 1870s, potentially being used for grazing.

During the partition of the Ultimo Estate in the 1850s, Block 57 was transferred to John Harris of Shane's Park, who retained ownership until his death in 1891.⁵⁸ Development on Block 57 only began after John Harris offered it as a leasehold in the late 1870s.⁵⁹

2.5.2 Phase 2 (1879-1914) – Subdivision and urban development of commercial and residential premises

2.5.2.1 'Heights of Pyrmont' subdivision (1879)

On 1 July 1879, John Harris transferred almost all of Block 57 of the Ultimo Estate to George Pile as a leasehold.⁶⁰ The lease was for the land bounded by Harris Street, Pyrmont Bridge Road, Pyrmont Street and Union Street. Exempt from the lease were three smaller sites, which had been leased earlier to James Orr (16 May 1874), Joseph Everett (22 July 1875) and Anthony Keogh (30 December 1875). Also exempt from the lease was the reserved road which cut Block 57 into two rectangular blocks, and which was subsequently named Paternoster Row.

Keogh's allotment was located on the Pyrmont Bridge Road frontage, on the block to the west of Paternoster Row, where the Quarryman's Arms was built by 1879 (see Figure 12). Likewise, Orr's and Everett's leases were on the subdivided block to the west of Paternoster Row, while the block to the east of the newly formed road was entirely in the possession of George Pile (see Figure 13). This contained the Western Construction Site.

George Pile was a director of Mills & Pile, also known as Mills, Pile and Gilchrist, estate agents, who took out three advertisements in the *Evening News* of 6 September 1879 for the sale of 86 leasehold

⁵⁸ NSW LRS, Indenture Bk 62 No 705.

⁵⁹ NSW LRS, Indenture Bk 62 No 705.

⁶⁰ NSW LRS, Primary Application (PA) 9768.

building sites at the 'Heights of Pyrmont'.⁶¹ The plan for the subdivision is included in Figure 12 below. It suggests that the plan was initially to sell the eastern block, bounded by Paternoster Row, Pyrmont Bridge Road, Pyrmont Street and Union Street as 53 small allotments, with frontages to all streets, including eight allotments to Pyrmont Bridge Road, 22 allotments to Pyrmont Street and 17 allotments to Paternoster Row.

The largest advertisement noted the following:

Heights of Pyrmont

Eighty-six Leasehold Building Sites, bounded by Pyrmont Bridge Road, Pyrmont-street, Union-street, Harris-street.

Convenient to Sydney and all the Great Suburban Manufactories and Workshops.

Good Clear Air, Consequently Good Health, the Cheapest Plan for the Working Man, and the Best Subdivision so Accessible yet offered.

Mills and Pile are favoured with instructions to sell by public auction, on the Ground, Heights of Pyrmont, To-Morrow, Saturday, the 6th September, at 3 o'clock sharp,

All these Eighty-six Allotments of Land, as subdivided per plan, obtainable at the Rooms, on the Heights of Pyrmont, in the most easily accessible position in a locality offering all the advantages of pure air, consequent health, and, as a natural consequence, the greatest economy with the greatest home comforts.

To the Working Man these leaseholds, which run for a period of 50 years, supply the greatest of all wants, viz., the opportunity of securing, at a Small Rental, the necessary ground on which to erect a home, which to many who cannot afford the cost of a freehold, must prove a boon of the utmost importance. These subdivisions will be offered but once by public sale, attendance at which is specially invited by the auctioneers. The title is perfect, and the terms of payment are liberal. Intending purchasers are requested to get a plan, to visit the ground, and judge for themselves whether all the advantages stated are not incontrovertible facts.⁶²

However, it appears that the leasehold sale did not go ahead as planned. Instead of selling these allotments as vacant leasehold sites, Mills & Pile subsequently developed the entire parcel with terrace rows, with frontages only to Pyrmont Bridge Road, Pyrmont Street and Union Street.

⁶¹ 'Advertising', *Evening News*, 5 September 1879, 1, <http://nla.gov.au/nla.news-article107167432>; 'No. 1642, A.D. 1883.', *New South Wales Government Gazette*, 15 June 1883, 3304, <http://nla.gov.au/nla.news-article225728887>.

⁶² 'Advertising', 5 September 1879.

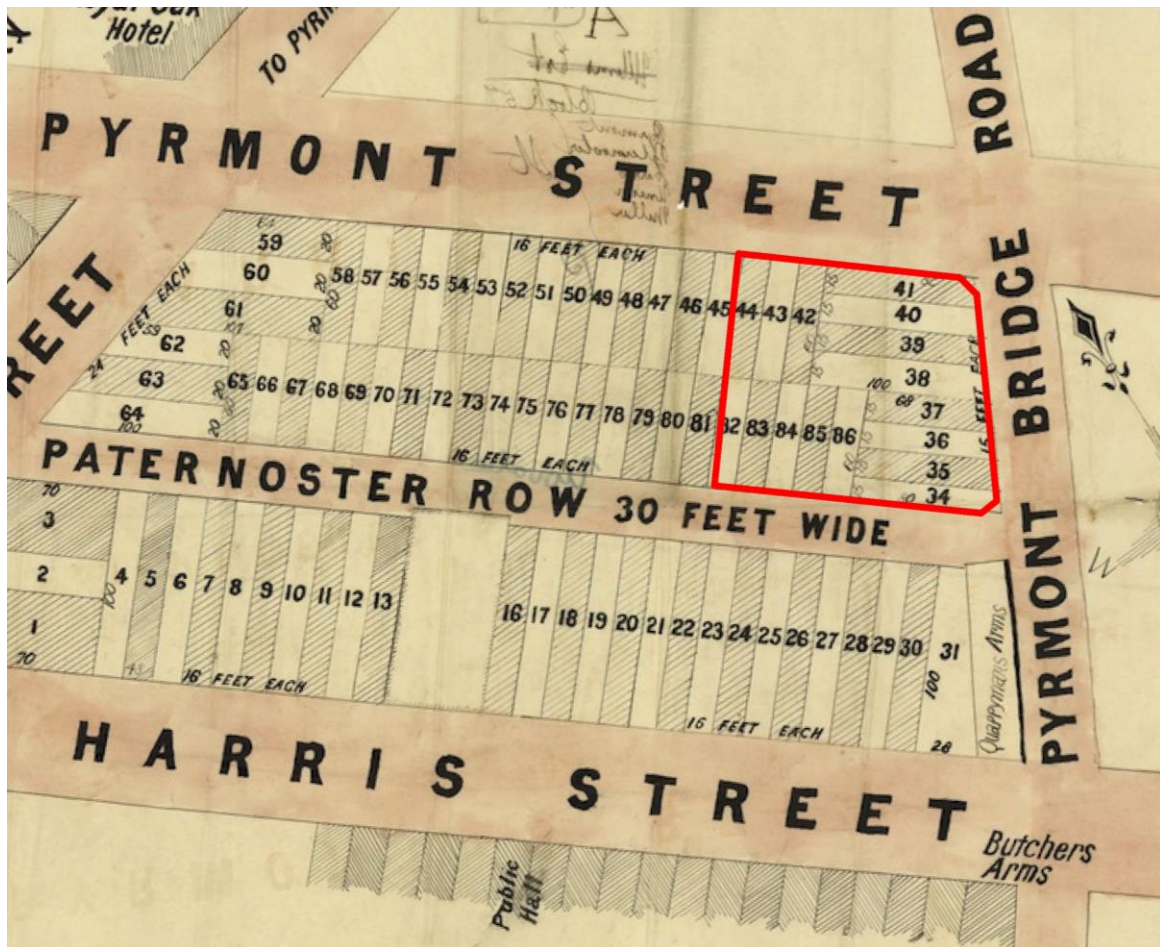


Figure 12: 1879 – Plan of the ‘Subdivision of Block 57 of the Ultimo Estate, Pyrmont,’ showing initial details of proposed subdivision, with allotments facing Pyrmont Bridge Road, Paternoster Row and Pyrmont Street (Source: State Library of NSW, SP/811.1737/91, <https://collection.sl.nsw.gov.au/record/74VvaqQxeEeX/QLZdR3WXdvkdb#viewer>)

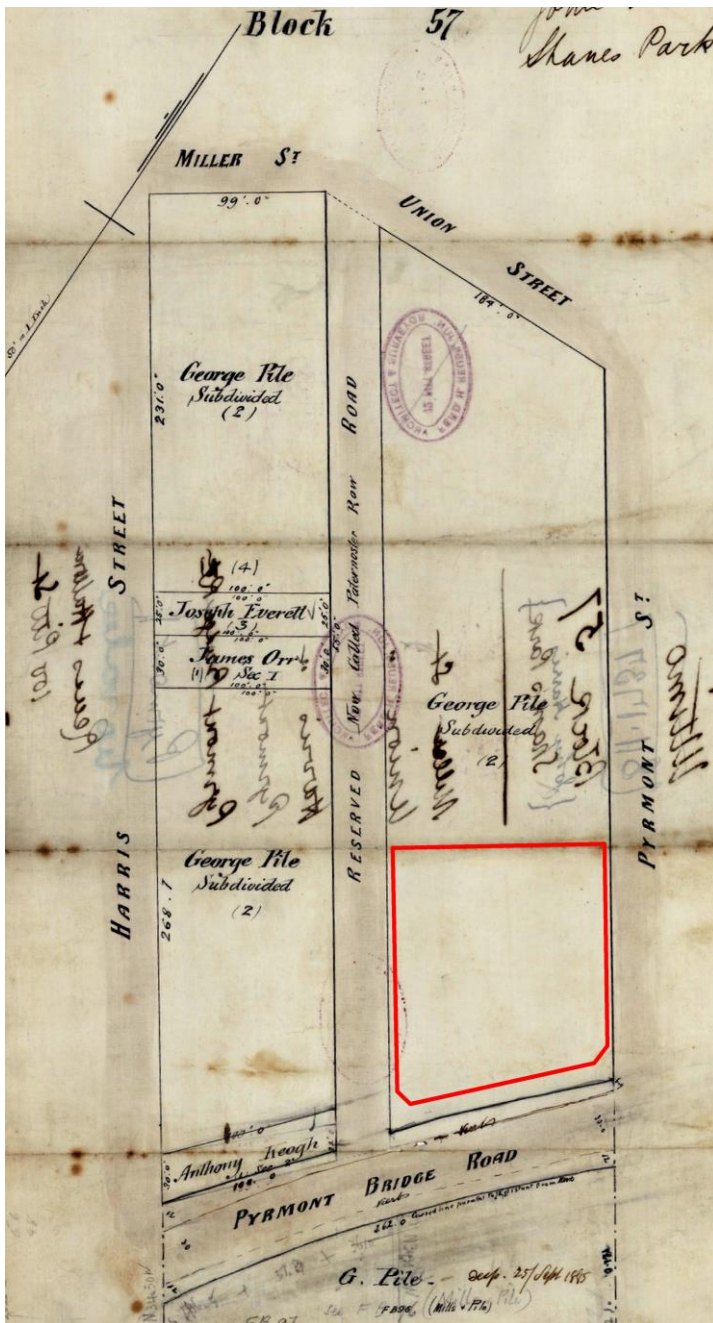


Figure 13: 1885 – Plan of Block 57 of the Ultimo Estate, Pyrmont, showing the various leases, including that to George Pile, for the block bounded by Pyrmont Bridge Road, Pyrmont Street, Union Street and Paternoster Row, which was noted as ‘subdivided’ (Source: State Library of NSW, SP/811.1737/94, <https://collection.sl.nsw.gov.au/record/74VvaqQxeEeX/xAy2M8vXov7Ak#viewer>)

2.5.2.2 Construction of Terrace Rows and Shops by Mills & Pile (1881)

On 15 October 1881, the *Freeman's Journal* noted that Mills, Pile and Gilchrist, the owners of a block of land bounded by Union Street, Pyrmont Street, Paternoster Row and Pyrmont Bridge Road, had “commenced the erection of six shops and dwellings in Union-street, eight on the Bridge-road, and seventeen other dwellings.”⁶³ One of the buildings on Union Street was intended for use as a bank, and the laying of the foundation stone had been attended by “a large number of friends” of the firm on Monday, 10 October 1881.

⁶³ ‘General News’, *Freeman's Journal*, 15 October 1881, 9, <http://nla.gov.au/nla.news-article115455022>.

However, it appears that the laying of the foundation stone was for the buildings on the Union Street frontage, and those buildings on Pyrmont Bridge Road and Pyrmont Street had already been nearly completed by August 1881, as the *Illustrated Sydney News* of 6 August 1881 noted the following:

New Business Premises at Ultimo

Considerable attention has become directed to the neighbourhood of Pyrmont and Ultimo in consequence of the Government having resumed a large amount of property in those localities for the purpose of enabling the railway to be brought down to the deep water at Pyrmont Bridge, the result being, not only a considerable increase in the value of surrounding property, but the introduction of improvements frequently of the most expensive character, and which are the most interesting by reason of the fact that, although so near the centre of the city, Pyrmont and Ultimo have, until quite recently, been almost unknown to the larger number of the citizens.

It is practically a new city, and the immense number of buildings which have gone up in every direction within the last few years would astonish any who have not been there recently. Our illustration represents portion of a square fronting Union Street, Pyrmont Street, and Pyrmont Road, which has been built for the well-known Sydney auctioneers, Messrs. Mills, Pile, and Gilchrist. The Union Street portion consists of six large shops and a building suitable for banking purposes, which will probably be occupied as a branch establishment of one of the leading Sydney banks. The Pyrmont [Bridge] Road frontage consists of eight large shops, which are nearly finished, and the Pyrmont Street frontage is formed by seventeen good houses, built upon allotments each 120 feet in depth. The cost of the entire block will be over £15,000. Mr. W. Barley is the architect, and Mr. D. Jarman the contractor.⁶⁴

The *Illustrated Sydney News* contained a depiction of the “new business premises at Pyrmont”, showing the new buildings facing Union Street (Figure 14). Like the two corner buildings on Pyrmont Bridge Road, the corner building on Union Street had the construction date of 1881 inscribed on the parapet (cf. Figure 18 and Figure 20).

The *Sands Sydney Directory* listed the buildings on the Pyrmont Bridge Road frontage for the first time in 1882, noting them as “unfinished shops”.⁶⁵ Considering that the 1882 *Sands Directory* was based on the 1881 survey of streets, this confirms that the buildings were indeed under construction in 1881. By 1883, six of the shops were noted as occupied, with the following occupants listed in the *Sands*, from the corner of Paternoster Row: Alexander Robertson, baker (No. 50); Robert Hutchinson, grocer (No. 52); Mary Scarlett, fancy repository (No. 54); Horatio Clark, draper (No. 56); Daniel Murphy, greengrocer (No. 58); and Lawrence McGrath, draper (No. 60).⁶⁶

The numbering in the *Sands* does not entirely correspond with that used in the 1882 *Assessment Books* for the Denison Ward, which noted Alexander Robinson in the double storey brick shop at No. 50 Pyrmont Bridge Road, followed by Robert Hutchinson at No. 52.⁶⁷ The three buildings at 54 to 58 Pyrmont Bridge Road were noted as ‘not occupied’. Horatio Clark, (No. 60), Daniel Murphy (No. 62) and Laurence McGrath (No. 64) were all listed as the occupants of the last three houses within the

⁶⁴ ‘Sydney Improvements.’, *Illustrated Sydney News and New South Wales Agriculturalist and Grazier*, 6 August 1881, 3, <http://nla.gov.au/nla.news-article64974493>.

⁶⁵ City of Sydney Archives, *Sands Sydney Directory*, City Directory – Pyrmont Bridge road – North side, 1882.

⁶⁶ City of Sydney Archives, *Sands Sydney Directory*, City Directory – Pyrmont Bridge road – North side, 1883.

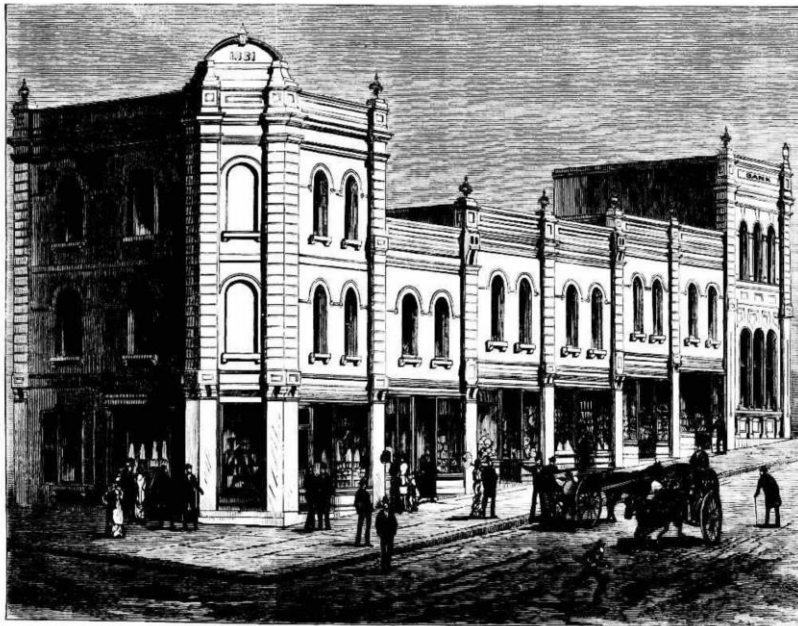
⁶⁷ City of Sydney Archives, *Assessment Books*, Denison Ward – Pyrmont Bridge Road, 1882.

row. All buildings were described as double storey brick shops with iron roofs and six rooms, rated at £65, except the corner shop on Paternoster Row, which had seven rooms and was rated at £109. The owners were noted as Mills & Pile. The subsequent occupants and leasehold owners of the subject shops on Pyrmont Bridge Road are detailed in Section 2.5.2.3 below.

As part of the overall development, seventeen terraces on Pyrmont Street were constructed and completed in 1881, at the same time as the Pyrmont Bridge Road shops. The 1882 *Assessment Books* noted all 17 terrace houses as occupied by individual residents, with the owners noted as Mills & Pile.⁶⁸ They were located at 101 to 131 Pyrmont Street and were all described as double storey brick houses with iron roofs and 4 rooms. The numbering shifted in c1885, after the construction of another shop (a butcher's shop) at the Union Street end of the row, which now became 101 Pyrmont Street, while the last house became 135 Pyrmont Street.

The first occupants of the four terraced houses that were located within the Western Construction Site were in 1882 noted as Thomas Allan at No. 127 (later No. 129), William Glenn at No. 129 (later No. 131), W. H. Millmot at No. 131 (later No. 133) and James Christie at No. 133 (later No. 135). Thomas Allan and William Glenn were identified as engineers in the 1883 *Sands Directory*, while William H. Millman (note the slightly different name) was a master mariner, and John Chrystal (also slightly different) was a piano tuner.⁶⁹

The following year, in 1884, the *Sands* listed the terrace row as 'Temora Terraces', with the numbering having changed and now starting at No. 1 near Union Street and ending at No. 17 near Pyrmont Bridge Road. The occupants of No. 14 (129 Pyrmont Street) to No. 17 (135 Pyrmont Street) were noted as Thomas Allan (No. 14), William Glen (No. 15), Mrs Margaret Parkins (No. 16), and Mrs. Charlotte Smith (No. 17). The subsequent occupants and leasehold owners of the subject terraced houses on Pyrmont Street are detailed in Section 2.5.2.4 below.



SYDNEY IMPROVEMENTS. - NEW BUSINESS PREMISES AT PYRMONT.

Figure 14: 1881 – Impression of the new buildings constructed by Mills & Pile, showing the Union Street frontage of the development which encompassed Pyrmont Bridge Road, Pyrmont Street and Union Street (Source: Sydney Improvements, Illustrated Sydney News, 6 August 1881, 16, <https://trove.nla.gov.au/newspaper/page/5406469>)

⁶⁸ City of Sydney Archives, *Assessment Books*, Denison Ward – Pyrmont Bridge Road, 1882.

⁶⁹ City of Sydney Archives, *Sands Sydney Directory*, City Directory – Pyrmont Street – West side, 1883.

2.5.2.3 Subsequent Occupants of the Pyrmont Bridge Road Shops (1884-1914)

By 1885, the *Sands* listed all shops on Pyrmont Bridge Road as occupied, with Mary Scarlett's fancy repository occupying 54-56 Pyrmont Bridge Road, Henry Barnett, tobacconist, at No. 58, and William Kitchen, general storekeeper, at No. 60 Pyrmont Bridge Road.⁷⁰ John Nicholls, tinsmith, was listed at No. 62 and Solomon Cohen, pawnbroker, was noted for the first time at No. 64, at the corner of Pyrmont Bridge Road and Pyrmont Street. A bakery was still located on the corner of Paternoster Row and Pyrmont Bridge Road (No. 50), and Robert Hutchinson still ran a grocery store at 52 Pyrmont Bridge Road. All of these buildings were in mixed-use as commercial premises (on ground floor level) and dwellings (on first floor level).⁷¹

The corner properties were naturally a little more prominent than the shops in the centre of the row, and they were occupied by a bakery on the corner of Paternoster Row and a pawnbroker on the corner of Pyrmont Street from around 1882, with the bakery operating until the mid-1900s and the pawnbroker located on the corner of Pyrmont Street until the shops were demolished in the mid 1910s.

The bakery on the corner of Paternoster Row was the largest building within the row, extending all the way to the lane running alongside the rear of the shops (Figure 15). In September 1886, a fire broke out at Henry Maker's bakery on the corner of Paternoster Row, which "burned the wooden ceiling and joisting of the structure."⁷² Maker was noted as a baker and confectioner in the 1887 *Sands Directory*.⁷³ By then, Mary Scarlett's fancy repository and furniture warehouse was noted at No. 54, while William E. Cross ran a restaurant at 56 Pyrmont Bridge Road.

By 1889, the former grocery store at No. 52 was occupied by Williams Brothers, wire mattress makers, and the following year they were listed at 32 Pyrmont Bridge Road, after renumbering of the street.⁷⁴ Salomon Cohen's corner pawnbroker shop was now located at 22 Pyrmont Bridge Road, while Henry Marker's bakery was located at 36 Pyrmont Bridge Road, with the street numbering now increasing towards the west rather than the east. The 1888 Metropolitan Detail Series Map (Figure 15) shows the outline of the shops at that time, with the later plans in Figure 16 and Figure 17 showing the properties essentially unchanged.

The 1891 *Sands* listed the following persons at 22 to 36 Pyrmont Bridge Road, largely corresponding with the 1891 *Assessment Books*: G. Berger, baker (No. 36 on the corner of Paternoster Row); Joseph Struck (No. 34); Williams Brothers, mattressmakers (No. 32); Charles Bartch, boardinghouse (No. 30); William Vose, grocer (No. 28); W. C. Waller, fruiterer (No. 26); William Gaden, carpenter (No. 24); and, Solomon Cohen, pawnbroker (No. 22).

The pawnbroker on the corner of Pyrmont Street was from around the late 1890s known as the Pyrmont Mont de Piété & Loan Office (see Figure 18).⁷⁵ This was one of several Australian offices of the institution which traced its history back to Medieval Europe, where it had been established as a money lending operation for the poor, as a "charitable pawn office".⁷⁶ The first such office had been established in Perugia in 1464, where it was known as Mons Pietatis, spreading across Germany, Belgium, France and Ireland in subsequent years. First attempts to introduce Mont de Piété in Australia had been made in around 1845, and in August 1861, the first office based on the Mont de

⁷⁰ City of Sydney Archives, *Sands Sydney Directory*, City Directory – Pyrmont Bridge road – north side, 1885.

⁷¹ They were in 1895 sold as shops and dwellings, see 'Advertising', *Daily Telegraph*, 21 September 1895, 3, <http://nla.gov.au/nla.news-article238541670>.

⁷² 'Fires in Sydney.', *Maitland Mercury and Hunter River General Advertiser*, 23 September 1886, 3, <http://nla.gov.au/nla.news-article18900137>.

⁷³ City of Sydney Archives, *Sands Sydney Directory*, City Directory – Pyrmont Bridge road – north side, 1887.

⁷⁴ *Ibid.*, 1889.

⁷⁵ 'Advertising', *Evening News*, 25 April 1898, 8, <http://nla.gov.au/nla.news-article109648605>; 'Advertising', *Truth*, 30 September 1900, 3, <http://nla.gov.au/nla.news-article200507907>.

⁷⁶ 'Mont de Piete, or Charitable Pawn Office', *Morning Chronicle*, 5 July 1845, 2, <http://nla.gov.au/nla.news-article31745809>.

Piété principle had been established by the Victoria Equitable Loan Investment Company in Melbourne.⁷⁷ The first Mont de Piété office in Sydney had been constructed in 1883 at Castlereagh Street.⁷⁸

Between 1895 and 1914, the occupants of the shops at 22 to 36 Pyrmont Bridge Road included the following, based on sample editions of the *Sands Directories*:⁷⁹

Table 4: Occupants 22-36 Pyrmont Bridge Road, 1895 - 1915

No	1895	1901	1905	1911	1914
36 (ex 50)		Hinds Brothers, bakers	Riggs A., bag merchant	Riggs A., bag merchant	Riggs A., bag merchant
34 (ex 52)	Phillips S. J., commission agent	Phillips S. J., commission agent	Phillips S. J., house and estate agent		
32 (ex 54)	Watson H., hairdresser	Hendy Harry, restaurant	Hendy Harry, restaurant	Condon and Court, restaurant	
30 (ex 56)	Bolton William, grocer	Vose Mrs. William, grocer	Langton John Bricknell Frederick, saddler	Bricknell Frederick, saddler Hawks Bert.	Baker Mrs. Martina Burnham Arthur, manufacturer of patent keys
28 (ex 58)	Vose William, grocer	Vose Mrs. William, grocer	Lee Henry, newsagent Hawks Miss H. A., stationer	Lee Henry, newsagent	Lee Henry, newsagent
26 (ex 60)	Farman Frank, basketmaker	Cremen P. J.	Cremen P. J.	Anderson Mrs. Flenberg Harold, pawnbroker	Flenberg Harold, pawnbroker
24 (ex 62)	Ross William, confectionery	Sloman William, pawnbroker	Sloman William, pawnbroker	Flenberg Harold, pawnbroker	Flenberg Harold, pawnbroker
22 (ex 64)	Marks Lazar, pawnbroker	Sloman, William, pawnbroker	Sloman William, pawnbroker	Flenberg Harold, pawnbroker	Flenberg Harold, pawnbroker

This suggests that the corner bakery at 36 Pyrmont Bridge Road was later used as a bags store, while 32 Pyrmont Bridge Road became a restaurant, which was by 1911 known as ‘Excelsior Dining Rooms’ (Figure 19). The grocery store at 28 Pyrmont Bridge Road became a newsagency, while the pawnbrokers on the corner of Pyrmont Street slowly enlarged their business and eventually occupied the three shops at 22 to 26 Pyrmont Bridge Road. The 1914 *Sands* listing was the last one for the row of shops on Pyrmont Bridge Road, and they were demolished not long afterwards (see Section 2.5.3). Photographs taken in 1911 show the shops prior to demolition (Figure 18 - Figure 20). The pawnbroker on the corner was still in use as a Mont de Piété loan office by that time.

⁷⁷ ‘Original Correspondence.’, *Weekly Register of Politics, Facts and General Literature*, 18 October 1845, 184, <http://nla.gov.au/nla.news-article228135542>; ‘Victoria Equitable Loan Investment Company’, *Argus*, 23 August 1861, 5, <http://nla.gov.au/nla.news-article5703385>.

⁷⁸ ‘The Mont de Piete.’, *Evening News*, 3 February 1883, 5, <http://nla.gov.au/nla.news-article107236295>.

⁷⁹ City of Sydney Archives, *Sands Sydney Directory*, City Directory – Pyrmont Bridge road – north side, 1895, 1901, 1905, 1911 and 1914.

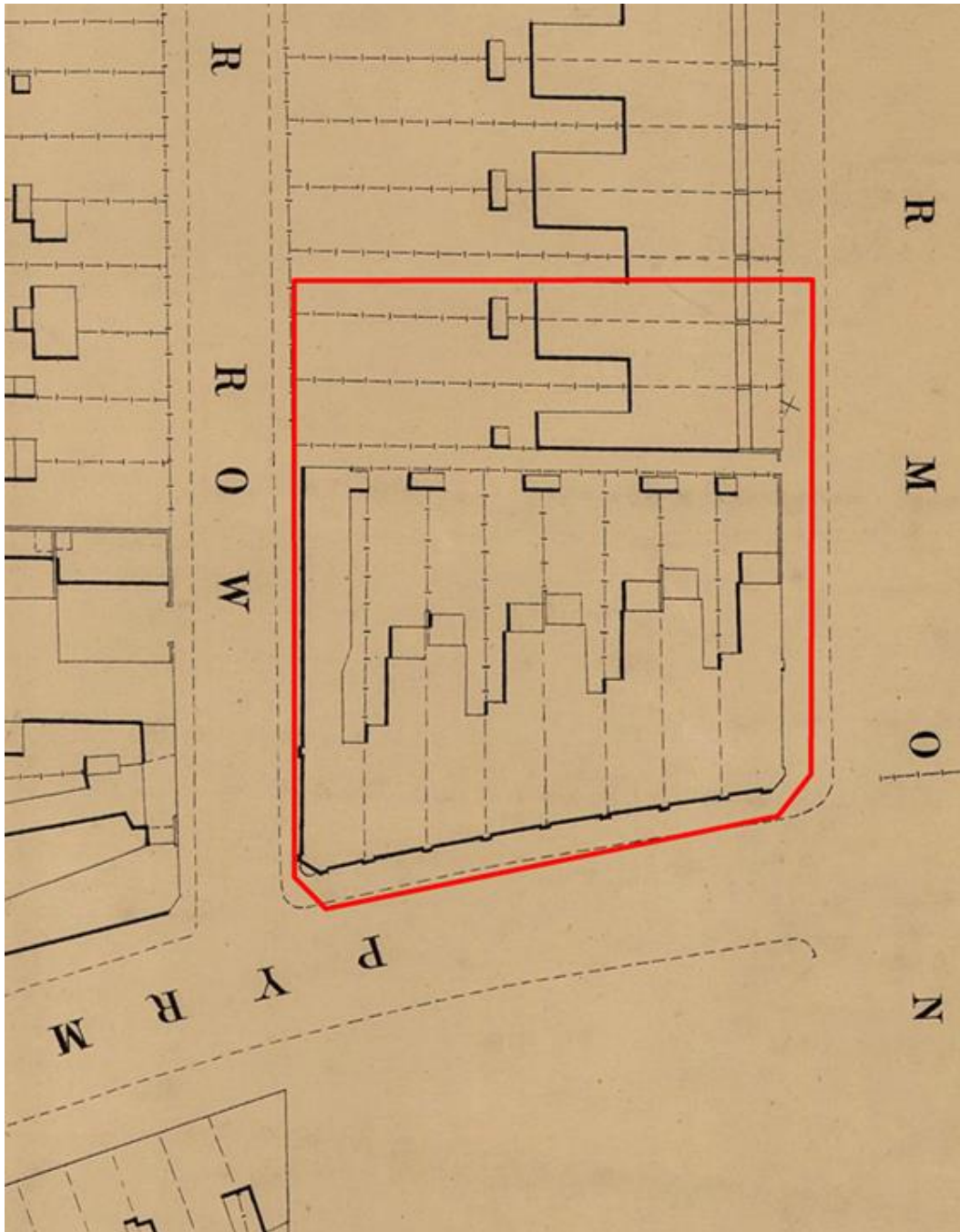


Figure 15: 1888 – Detail from City of Sydney Section (Metropolitan Detail Series), Sheet H3
(Source: State Library of NSW, Z/M Ser 4 811.17/1,
<https://collection.sl.nsw.gov.au/record/74VKqv5pXe4b/bO70aB3gy0DAk#viewer>)

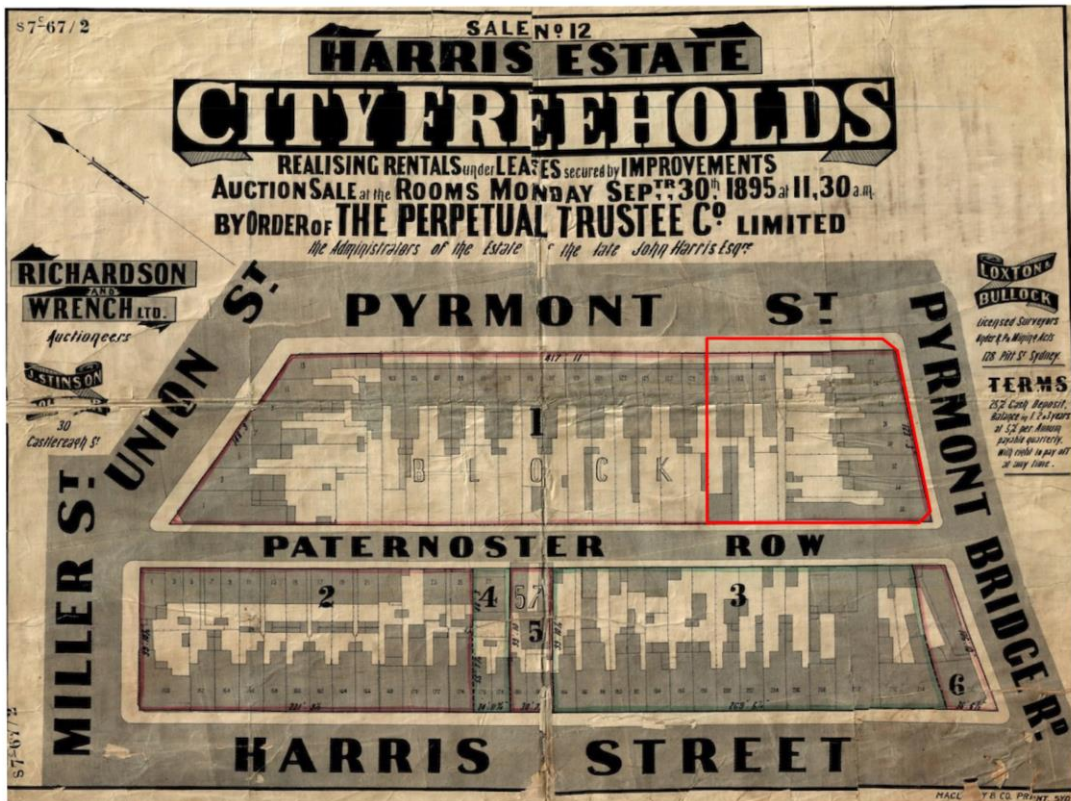


Figure 16: 1895 – Sales plan for Block 57 of the Harris Estate, sold as city freeholds, showing details of buildings (Source: City of Sydney Archives, Harris Estate sale No. 12 [area between Pyrmont, Union, Gipps, Harris, Pyrmont Bridge Road] (30/09/1895), [A-00530399]. <https://archives.cityofsydney.nsw.gov.au/nodes/view/10698863709-2>)

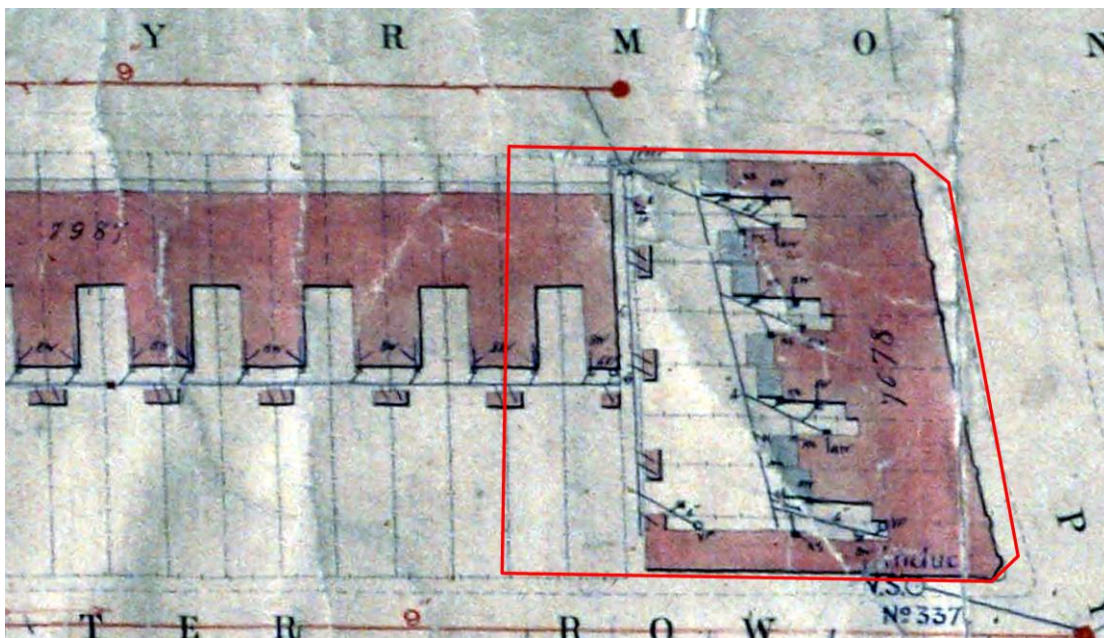


Figure 17: Detail from Blackwattle Sheets, prepared by the Board of Water Supply and Sewerage, 1908 (with later updates) (Source: Sydney Water Archives, BLKWTL 3709-2)



Figure 18: 1911 – Photograph of the terrace shop row on the northern side of Pyrmont Bridge Road, including the Pyrmont Mont de Piete Loan Office on the corner of Pyrmont Street. From Demolition Books. (Source: City of Sydney Archives, Pyrmont Bridge Road (01/12/1911), [A-00038946]. <https://archives.cityofsydney.nsw.gov.au/nodes/view/676872>)



Figure 19: 1911 – Photograph of the shops on Pyrmont Bridge Road, including Excelsior Dining Rooms (No. 32), Frederick Bricknell, saddler (No. 30), and Henry Lee, newsagent (No. 28). From Demolition Books. (Source: City of Sydney Archives, Pyrmont Bridge Road (01/12/1911), [A-00038945]. <https://archives.cityofsydney.nsw.gov.au/nodes/view/676862>)



Figure 20: 1911 – Photograph of the building at the corner of Paternoster Row (on left) and Pyrmont Bridge Road. Located at 36 Pyrmont Bridge Road, this was from 1881 until the early 1900s in use as a bakery. From Demolition Books. (Source: City of Sydney Archives, Pyrmont Bridge Road (01/12/1911), [A-00038944]. <https://archives.cityofsydney.nsw.gov.au/nodes/view/676856>)

2.5.2.4 Subsequent occupants of the Pyrmont Street Terraces (1884-1914)

The 'Temora Terraces' were occupied by various residents, usually short term, during the 1880s.⁸⁰ Occupants continued to include engineers, shipwrights and master mariners; people who were likely attracted by the proximity to their workplace around the harbour. Mrs Durrant was one of the more longer-term occupants of 17 Temora Terrace, later known as 135 Pyrmont Street, even though the *Sands Directory* variously listed her either to the north or south of Pyrmont Bridge Road.

However, the 1891 *Assessment Books* firmly place Mrs Durrant at 135 Pyrmont Street, as the last house within the block owned by Mills & Pile, just before Pyrmont Bridge Road.⁸¹ Her next-door neighbour at that time, at No. 133, was Solomon Cohen, who ran the pawnbroker store on the corner of Pyrmont Street and Pyrmont Bridge Road. George Palmer lived at 131 Pyrmont Street, and C. Insoll was noted at 129 Pyrmont Street. All houses were described as double storey brick and stone houses with iron roofs, containing 5 rooms, except Solomon Cohen's house which had 6 rooms. The occupants of No. 131 and No. 129 Pyrmont Street were noted as George Farmer, basketmaker and Charles Messell, master mariner, respectively, in the 1891 *Sands*.⁸²

Between 1895 and 1914, the occupants of the terraced houses at 129 to 135 Pyrmont Street included the following, based on sample editions of the *Sands Directories*:

⁸⁰ City of Sydney Archives, *Sands Sydney Directory*, City Directory – Pyrmont Street – West side, 1883-1890.

⁸¹ City of Sydney Archives, *Assessment Books*, Denison Ward – Pyrmont Street, 1891.

⁸² City of Sydney Archives, *Sands Sydney Directory*, City Directory – Pyrmont Street – West side, 1891.

Table 5: Occupants o 129-135 Pyrmont Street, 1895 - 1914

No	1895	1901	1905	1911	1914
129	Messell Charles, master mariner	Messell Charles, master mariner	Messell Charles, master mariner	Messell Charles, master mariner	Condon Henry
131	Jennings George Thomas	Jennings George Thomas	Sheriff William	Sheriff William	Brewer C.
133	Carter Henry T., sailmaker Nicholls Henry	Cleaves Edward	O'Neill Michael	Moore Edward	Hunt Mrs. Mary
135	Stewart Hugh	Allen Stephen	O'Grady Barney	O'Grady Barney	Fienberg Harold, pawnbroker

It is likely that the terraced houses were largely used as residences between 1895 and 1914, with Charles Messel being a long-term resident of 129 Pyrmont Street. However, with the resumption and subsequent demolition of the nearby shops on Pyrmont Bridge Road from 1914, this appears to have changed and the properties near Pyrmont Bridge Road became commercially used, as detailed in Section 2.5.3.2 below.

2.5.3 Phase 3 (1914-1947) – Resumption, clearance and relocation of commercial activity

2.5.3.1 Resumption and demolition of Pyrmont Bridge Road Shops (1914-15)

By 1914, the block bounded by Pyrmont Bridge Road, Pyrmont Street, Union Street and Paternoster Row had been transferred to new owners twice. Having been advertised as freehold properties in September 1895 (see Figure 16),⁸³ the block was transferred from the Perpetual Trustee Company Ltd to the Scottish Australian Investment Company Ltd in 1896, together with the rest of Block 57.⁸⁴ The lease to George Pile and all subleases were noted on the Certificate of Title issued to the Scottish Australian Investment Company on 19 June 1896. This ended the Harris family ownership of the site.

The second ownership transfer was registered on 9 April 1913, when the property at 22 to 36 Pyrmont Bridge Road was resumed by the Municipal Council of Sydney under the *Sydney Corporation Amendment Act 1906*, as part of the planned street widening of Pyrmont Bridge Road and Union Streets. A public notice for the resumption of properties for the widening as well as for “carrying out improvements in and remodelling that portion of the said City” had officially been gazetted on 27 September 1911.⁸⁵

On 20 May 1914, the two-storeyed brick shops at 22, 24 and 26 Pyrmont Bridge Road, and those at 32 and 36 Pyrmont Bridge Road were offered as two separate lots, with the heading of the sales advertisement noting: “Pyrmont Resumption Scheme – Important Unreserved Auction Sale of Buildings – For Demolition and Removal.”⁸⁶

It appears that the buildings could not be sold but were demolished soon afterwards, as from 1915, the *Sands Directory* no longer listed any occupants or vacant buildings at that site, and the 1918 *Assessment Book* for the Pyrmont Ward noted the site as ‘land’ owned by the Municipal Council of

⁸³ ‘Advertising’, 21 September 1895.

⁸⁴ NSW LRS, CT Vol 1193 fol 248.

⁸⁵ ‘Notice of Resumption’, *Government Gazette of the State of New South Wales*, 27 September 1911, 5280, <http://nla.gov.au/nla.news-article227592668>.

⁸⁶ ‘Advertising’, *Daily Telegraph*, 20 May 1914, 3, <http://nla.gov.au/nla.news-article238799928>.

Sydney.⁸⁷ The vacant site remained in the ownership of Council which offered the site for sale once more in November 1922, when it was noted as Section 6 of the City Council Freehold (Figure 21).

The site was transferred to Richard Morris Williams on 30 October 1924, as a parcel of land containing 23 perches.⁸⁸ Morris was noted as a master butcher from Pymont, but he did not appear to have done anything with the site, trying to sell his property one year later, describing the site as being “suitable for factory, store, warehouse, etc.”⁸⁹ The sales advertisement in the *Sydney Morning Herald* of 30 September 1925 noted that the site had about 118ft 6in frontage to Pymont Bridge Road, 54ft frontage to Pymont Street, and 76ft 2in to Paternoster Row.⁹⁰

Williams transferred his property to Herbert John Nichols (also known as Nicholls) two years later, on 1 September 1927.⁹¹ One year later, in 1928, a new scheme was submitted for widening of Pymont Bridge Road and Union Street to 84 feet.⁹² Although no action was ever taken, because other solutions were found to ease the congestion of both roads, it would appear the proposal would have impacted on any plans by Nicholls to develop the site. The photographs in Figure 22 to Figure 28 show the site during the 1920s and 1930s, suggesting that it was largely vacant, without any fixed structures, but may have been used to deposit building materials or soil, particularly during the late 1920s and early 1930s, when the site did not appear to have been level.

On 6 January 1936, Nicholls submitted an application for “new building (motor show room)” at 22/36 Pymont Bridge Road.⁹³ The following day, the *Sydney Morning Herald* noted that Crane and Scott, architects, had already let a contract “to H. J. Nicholls, Camperdown, for erection of motor truck showroom in Pymont Bridge Road.”⁹⁴ However, even though the building was swiftly approved, with Nicholls’ address listed as Comet Street, Ashfield,⁹⁵ the City’s *Planning Street Cards* noted that the work did not go ahead.⁹⁶ It appears that Nicholls, who was noted as a retired livery stables proprietor of Ashfield in the transfer document of 1927,⁹⁷ had gone into business as a builder, as he was noted as the contractor for several buildings, suggesting he may have used the vacant site to dump or store building materials or soil.⁹⁸

The 1936 and 1939 *Assessment Books* for the Phillip Ward noted Herbert John Nichols as the owner of ‘land’ that was unoccupied and rated at £150 for both years. The City of Sydney Civic Survey plan included in Figure 29 shows the site in 1938-39, where it was noted as vacant. Likewise, the survey in Figure 30, prepared in January 1923 and updated based on a survey dated October 1940, does not show any structures at the site.

⁸⁷ City of Sydney Archives, *Sands Sydney Directory*, City Directory – Pymont Bridge Road – North side, 1915; *Assessment Books*, Pymont Ward – Pymont Bridge Road, 1918.

⁸⁸ NSW LRS, CT Vol 2357 fol 186.

⁸⁹ ‘Advertising’, *Sydney Morning Herald*, 30 September 1925, 21, <http://nla.gov.au/nla.news-article16245606>.

⁹⁰ *Ibid.*

⁹¹ NSW LRS, CT Vol 3670 fol 90.

⁹² City of Sydney Archives, Town Clerk Correspondence Files – 2566/47, Agenda Paper Item No. 36, dated 16 July 1947.

⁹³ City of Sydney Archives, *Planning Street Cards*, Pymont Bridge Road, 22/36 Pymont Bridge Rd, Pymont St & Paternoster Road, No. 12/36

⁹⁴ ‘Contracts Let’, *Sydney Morning Herald*, 7 January 1936, 6, <http://nla.gov.au/nla.news-article17207997>.

⁹⁵ ‘Buildings and Works Approved’, *Construction and Real Estate Journal*, 15 January 1936, 4, <http://nla.gov.au/nla.news-article222918001>.

⁹⁶ City of Sydney Archives, *Planning Street Cards*, Pymont Bridge Road, 22/36 Pymont Bridge Rd, Pymont St & Paternoster Road, No. 12/36

⁹⁷ NSW LRS, CT Vol 3670 fol 90.

⁹⁸ See, for instance: ‘Buildings and Works Approved’, *Construction and Real Estate Journal*, 1 December 1937, 5, <http://nla.gov.au/nla.news-article222924118>; ‘Tenders Accepted or Received’, *Construction*, 20 July 1938, 16, <http://nla.gov.au/nla.news-article222852320>.

On 22 July 1941, Hay Publishing Co. (or Hay Publicity Ltd) submitted an application for hoarding at 22/36 Pyrmont Bridge Road.⁹⁹ After approval was granted on 24 July 1941, the hoarding was erected on all three street frontages, with the posts bolted into the rock and concreted in. The hoarding was complete on 22 September 1941. It is visible in the 1943 aerial photograph showing the site, which also shows what appears to be a building constructed alongside the Paternoster Row frontage. No evidence as to the purpose and construction of this building could be found, however, it would appear that it was a simple building of a temporary nature, potentially of timber construction and maybe erected in connection with the hoarding.

H. J. Nicholls retained ownership of the site until 21 February 1945, when the property was transferred to W. and A. Gilbey Ltd.¹⁰⁰

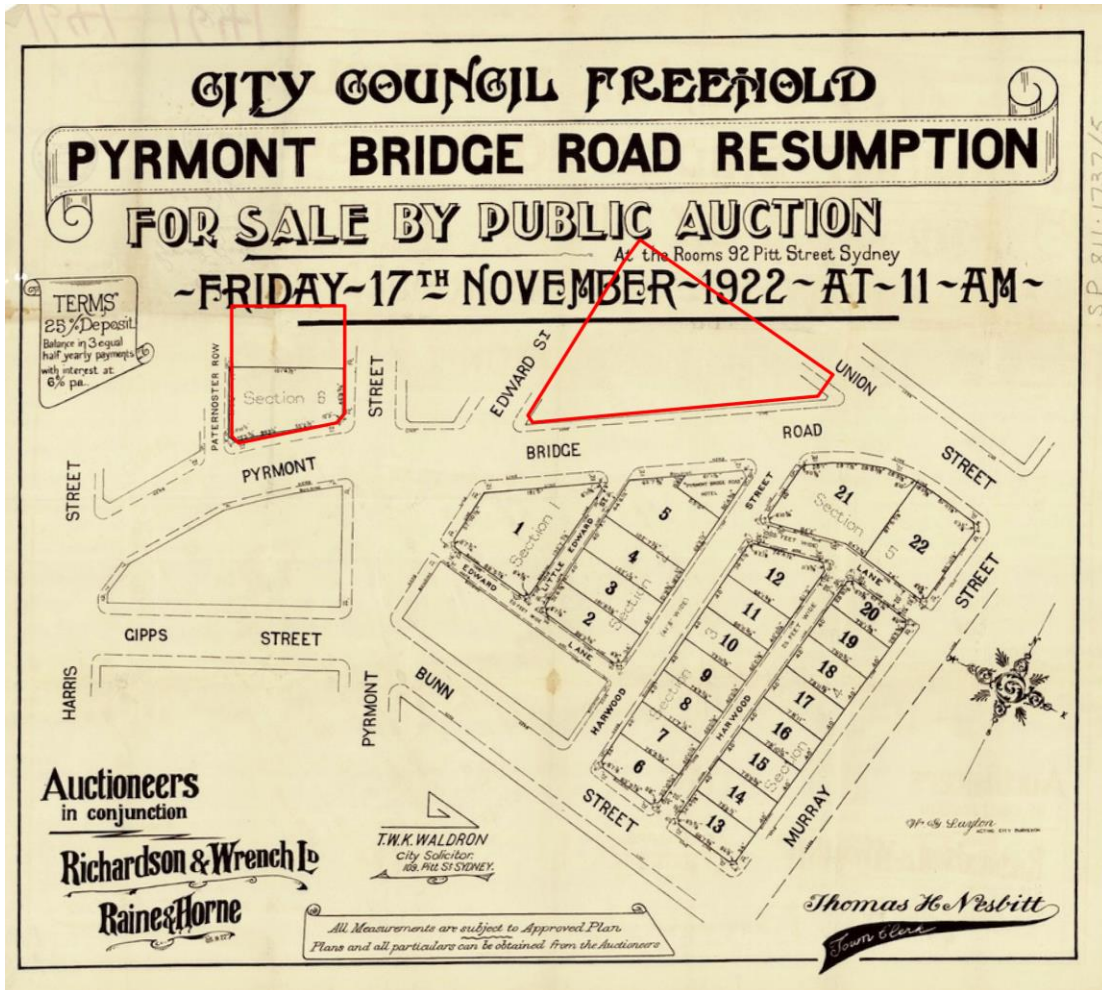


Figure 21: 1922 – City Council Freehold properties for sale, including Section 6 on the northern side of Pyrmont Bridge Road (Source: SLNSW, SP/811.1737/2, <https://collection.sl.nsw.gov.au/record/74VvaqQxeEeX/IX4Xd3rAk6vGV#viewer>)

⁹⁹ 'Building Inspectors Card: 22/36 Pyrmont Bridge Rd Pyrmont St & Paternoster Row. Hoardings. BA 478/41', City of Sydney Archives, accessed 31 January 2022, <https://archives.cityofsydney.nsw.gov.au/nodes/view/1564438>; 'Tenders Accepted or Received', *Construction*, 30 July 1941, 14, <http://nla.gov.au/nla.news-article222861626>.

¹⁰⁰ NSW LRS, CT Vol 3670 fol 90.

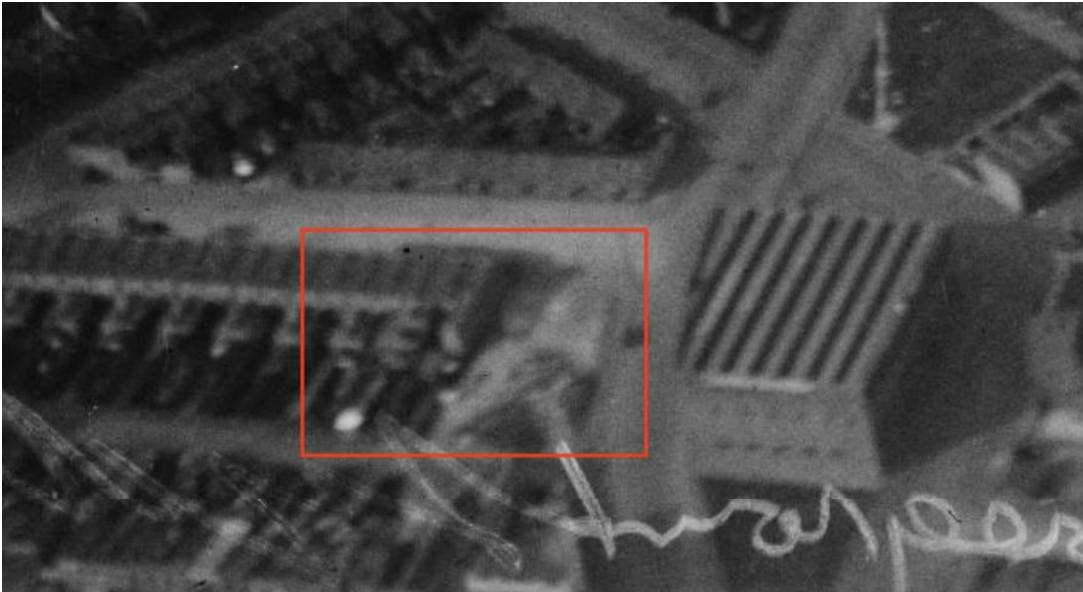


Figure 22: 1920s – ‘Aerial view of Pyrmont Bridge and wharves’, with the Western Construction site area outlined (Source: NLA, PIC/15611/15348, Fairfax archive, <https://nla.gov.au/nla.obj-163003956>)

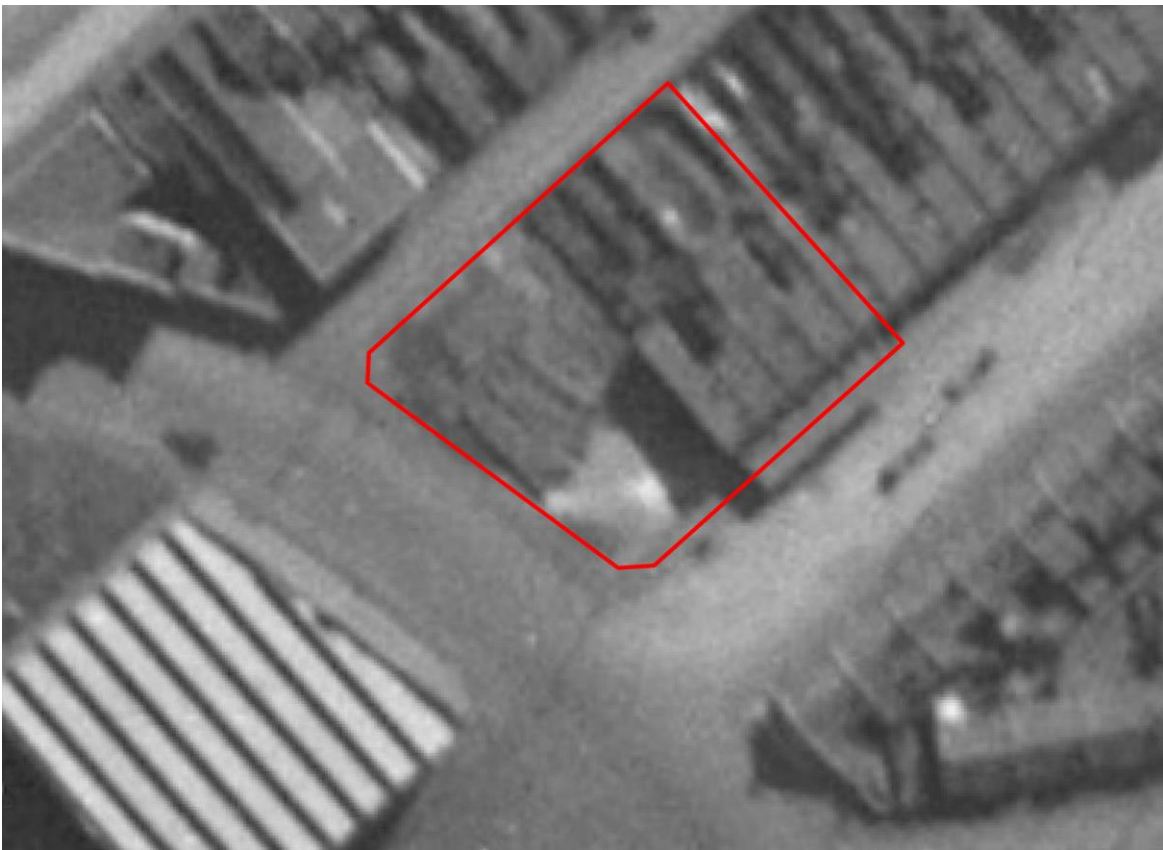


Figure 23: c1920s – Aerial photograph by Milton Kent, said to date from 1932 but based on the context it was taken during the 1920s (all buildings at the Eastern Construction Site are still standing). Note the western portion of the site appears to contain deposits (Source: SLNSW, Milton Kent aerial views of Sydney, 1932, FL8812118, <https://archival.sl.nsw.gov.au/Details/archive/110367483>)



Figure 24: August 1930 – View towards the vacant site, with the Quarryman's Hotel on the left. A person appears to be standing on an elevated level, and the frontage to the street also appears to be elevated (Source: Noel Butlin Archives Centre, Tooth's Yellow Card, Quarryman's Hotel Pyrmont, Card 2 Side 2, <https://openresearch-repository.anu.edu.au/handle/1885/104550>)



Figure 25: 1930 – Aerial view by Milton Kent, showing the vacant site as outlined (Source: SLNSW, Milton Kent aerial views of Sydney, 1932, FL8812121, <https://archival.sl.nsw.gov.au/Details/archive/110367483>)



Figure 26: 1932 – Aerial view by Milton Kent, showing the vacant site and adjoining terraces as outlined (Source: SLNSW, Milton Kent aerial views of Sydney, 1932, FL8812119, <https://archival.sl.nsw.gov.au/Details/archive/110367483>)



Figure 27: 1932 – Aerial view by Milton Kent, showing the vacant site and adjoining terraces as outlined (Source: SLNSW, Milton Kent aerial views of Sydney, 1932, FL8812120, <https://archival.sl.nsw.gov.au/Details/archive/110367483>)



Figure 28: January 1939 – View towards the vacant site, with the Quarryman’s Hotel just visible on the left. The site appears to have been largely cleared, with a path likely cutting across (Source: Noel Butlin Archives Centre, Tooth’s Yellow Card, Quarryman’s Hotel Pyrmont, Card 2 Side 2, <https://openresearch-repository.anu.edu.au/handle/1885/104550>)

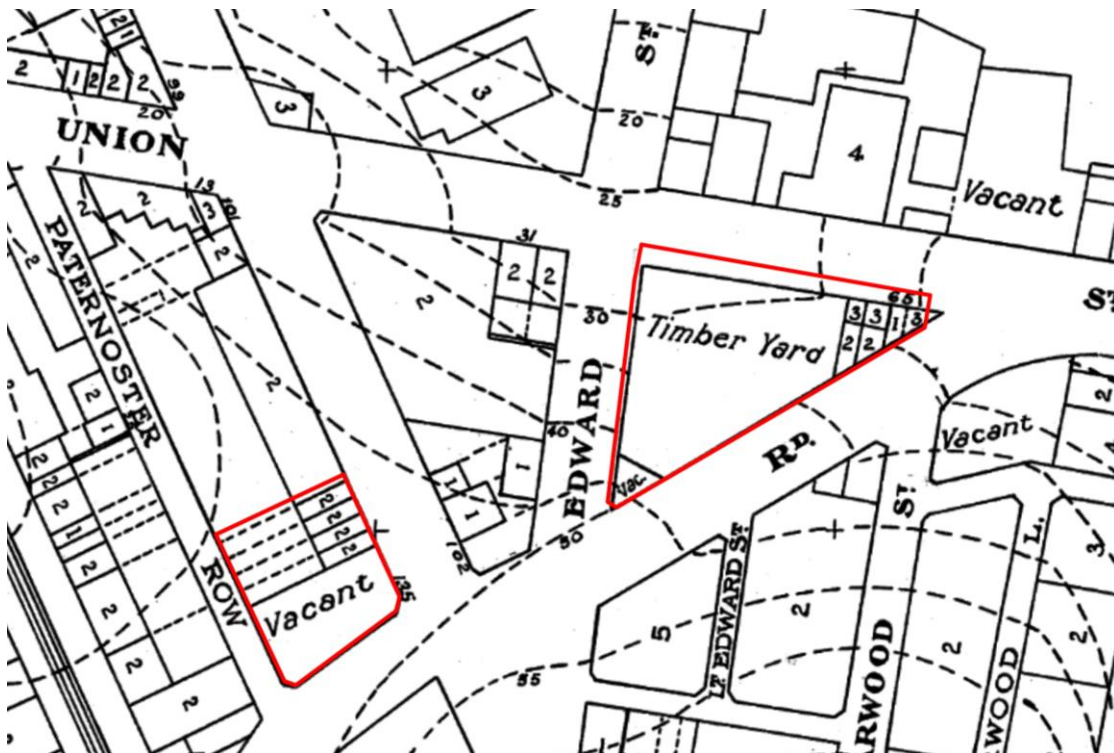


Figure 29: 1938-39 – Detail from City of Sydney Civic Survey, Map 17, showing the Pyrmont Bridge Road site as vacant (Source: City of Sydney Archives, City of Sydney - Civic Survey, 1938-1950: Map 17 – Pyrmont, [A-00880354]. <https://archives.cityofsydney.nsw.gov.au/nodes/view/1709281>)

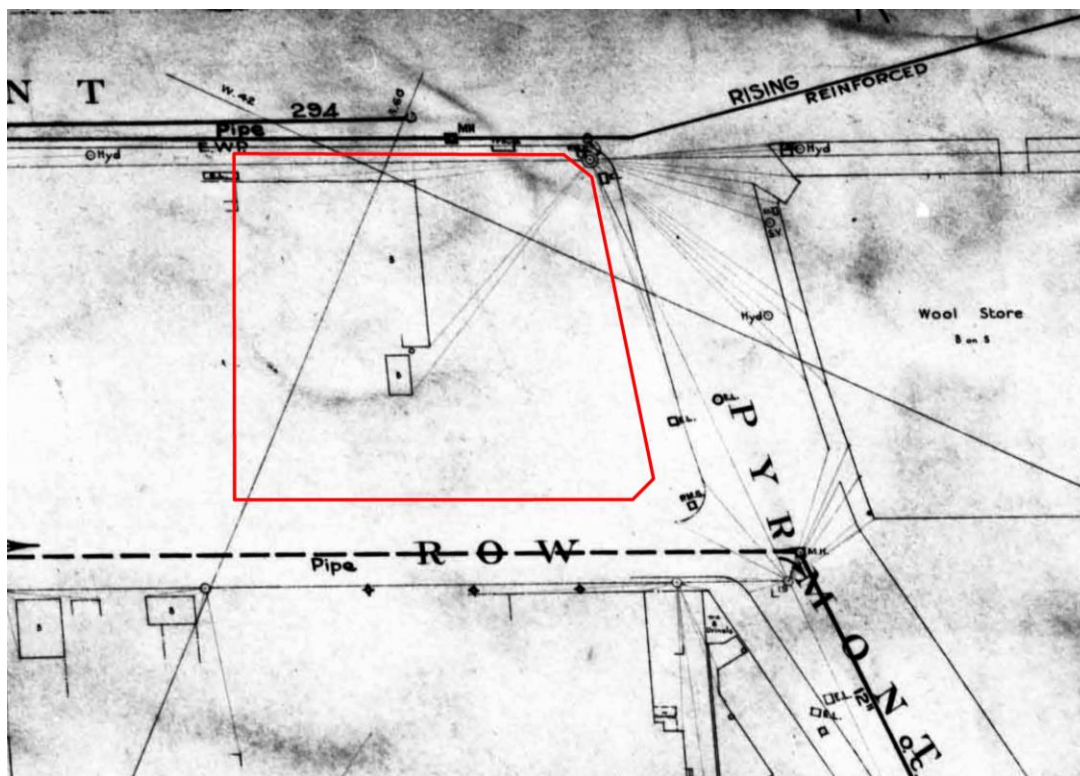


Figure 30: c1940-41 – Detail from City of Sydney Detail Survey, originally surveyed in January 1923 and updated to 1940-41, showing the Pyrmont Bridge Road site as vacant (Source: Sydney Water Archives, City of Sydney Detail Survey Sheet H3, DTS 403)



Figure 31: 1943 – Aerial photograph showing the hoarding erected in 1941, as well as a long structure along Paternoster Row, the purpose of which could not be identified (Source: NSW LRS, SIX Maps, 1943 aerial)

2.5.3.2 Relocation of commercial activity to Pyrmont Street (1914-1947)

With the resumption and sale of the shops on Pyrmont Bridge Road in 1914, and subsequent demolition of all buildings, some of the previous shop keepers on Pyrmont Bridge Road decided to relocate to the terraced houses at 129 to 135 Pyrmont Street, which were still owned by the Scottish Australian Investment Company at that time.

Harold Fienberg, for instance, who had run the Mont de Piété loan office at 22 Pyrmont Bridge Road during the 1910s, was in 1914 noted at both 22 Pyrmont Bridge Road and 135 Pyrmont Street, and in 1915, when the Pyrmont Bridge Road shops were no longer listed, Fienberg continued to run his pawnbroker business from 135 Pyrmont Street.¹⁰¹ Being the terrace immediately adjacent to the vacant site, it became the new Mont de Piété loan office. This was from c1917 onwards run by M. or F. Lewis, who commissioned a large advertising sign to be painted on the south wall of his premises, for 'Lewis's Pyrmont' (see Figure 24 and Figure 28).¹⁰²

Henry Lee moved his newsagency from 28 Pyrmont Bridge Road to 133 Pyrmont Street, where he was listed in the *Sands Directory* from 1915 onwards.¹⁰³ It appears that the buildings at 129 and 131 Pyrmont Street underwent alterations during 1914, as these buildings were not listed in the 1915 *Sands*, however, by 1916, 129 Pyrmont Street had become a hairdresser and was occupied by George Austin, and 131 Pyrmont Street had become a restaurant run by Mrs. M. Hummell.¹⁰⁴

Indeed, four building applications for Pyrmont Street had been submitted to Council in 1914, three of which were for the properties at 129 to 135 Pyrmont Street.¹⁰⁵ E. Spackman, architect and builder, submitted two applications, one on 9 June for alterations to 133 Pyrmont Street, involving a new shop front (BA 438/14, see Figure 32),¹⁰⁶ and one on 6 August 1914 for 129-131 Pyrmont Street (BA 609/14). The latter was again for new shopfronts and internal alterations, with the plan submitted as part of BA 609/14 noting the wrong address (see Figure 33).

Johns & Carden, architects, submitted an application on 16 December 1914, to erect a supported awning for 129-135 Pyrmont Street, extending along the frontages of all four buildings (BA 876/14).¹⁰⁷ The awning is visible in Figure 23 - Figure 28. A further development application for internal alterations to 131 Pyrmont Street were submitted in 1916, again by J. Carden.¹⁰⁸ This involved internal reconfiguration of the rear kitchen of the restaurant.

By 1918, hairdresser George Austin was still located at No. 129, Mrs. H. Humble ran the restaurant at No. 131, Lee Henry's newsagency was still located at No. 133, and M. Lewis, pawnbroker, was noted at No. 135.¹⁰⁹ By 1921, No. 129 was no longer listed separately, but had become part of the restaurant which was now located at 129-131 Pyrmont Street, occupying both buildings.¹¹⁰

¹⁰¹ City of Sydney Archives, *Sands Sydney Directory*, City Directory – Pyrmont Street (Pyrmont) – West side, 1910-1915; Pyrmont Bridge Road – north side, 1910-1915.

¹⁰² City of Sydney Archives, *Sands Sydney Directory*, City Directory – Pyrmont Street (Pyrmont) – West side, 1918; cf. 'Advertising', *Sydney Morning Herald*, 20 September 1918, 3, <http://nla.gov.au/nla.news-article15803416>.

¹⁰³ City of Sydney Archives, *Sands Sydney Directory*, City Directory – Pyrmont Street (Pyrmont) – West side, 1915.

¹⁰⁴ *Ibid.*, 1916.

¹⁰⁵ City of Sydney Archives, *Planning Street Cards*, Pyrmont Street (1907-1928), Card 3.

¹⁰⁶ Cf. 'City Applications', *Sun*, 15 June 1914, 12, <http://nla.gov.au/nla.news-article224650138>.

¹⁰⁷ City of Sydney Archives, *Planning Street Cards*, Pyrmont Street (1907-1928), Card 3; cf. 'City Council Applications', *Construction: Weekly Supplement to Building*, 21 December 1914, 12, <http://nla.gov.au/nla.news-article234764546>.

¹⁰⁸ City of Sydney Archives, *Planning Street Cards*, Pyrmont Street (1907-1928), Card 3.

¹⁰⁹ City of Sydney Archives, *Sands Sydney Directory*, City Directory – Pyrmont Street (Pyrmont) – West side, 1918.

¹¹⁰ *Sands Directory*, 1919.

Between 1921 and 1931, the occupants of the terraced houses at 129 to 135 Pyrmont Street included the following, based on sample editions of the *Sands Directories*:

Table 6: Occupants of 129-135 Pyrmont Street, 1921-1931

No	1921	1926	1929	1931
129-131	Patman H., restaurant	Patman H., restaurant	Blackwell Mrs. S., restaurant Blackwell, A.D. (No. 131)	Blackwell Mrs. S., restaurant Blackwell, A.D. (No. 131)
133	Dackett Helen Ada, newsagent	Dackett Helen Ada, newsagent	Carroll Mrs. I., newsagent	Carroll Mrs. I., newsagent
135	Lewis Fanny, pawnbroker	Lewis Fanny, pawnbroker	Sanders, Richard, carrier	Childs William

During the 1920s and 1930s, the only building and development applications that were submitted for the shops at 129 to 135 Pyrmont Street, as noted in the City's *Planning Street Cards* for Pyrmont Street, were for rear fencing at No. 135 (BA 589/29), for repairs and re-roofing of the awning of Nos. 133 and 135 (DA notice 107/31 and BA 620/31), for a dividing fence at Nos. 133 and 135 (DA notice 1612/34), and for repair or renewal of the awning gutter at Nos. 133 and 135 (DA notice 3213/36).¹¹¹ No applications were listed for the 1940s.

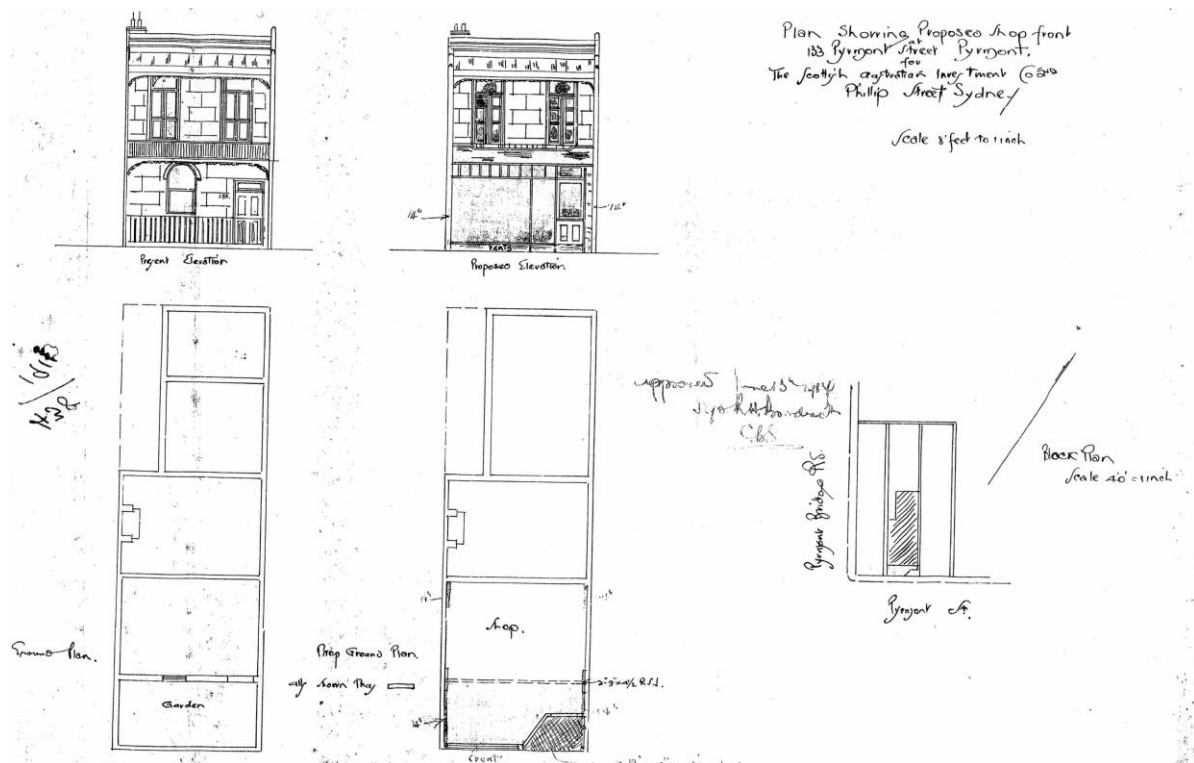


Figure 32: 1914 – Plan showing details for the proposed new shopfront for Henry Lee’s newsagency at 133 Pyrmont Street, carried out by E. Spackman for the Scottish Australian Investment Co Ltd (Source: City of Sydney Archives, Pyrmont St (133) E Spackman alterations (01/01/1914 - 31/12/1914), [A-00551981]. <https://archives.cityofsydney.nsw.gov.au/nodes/view/1420016>)

¹¹¹ City of Sydney Archives, *Planning Street Cards*, Pyrmont Street (1907-1928) and (1928-1994).

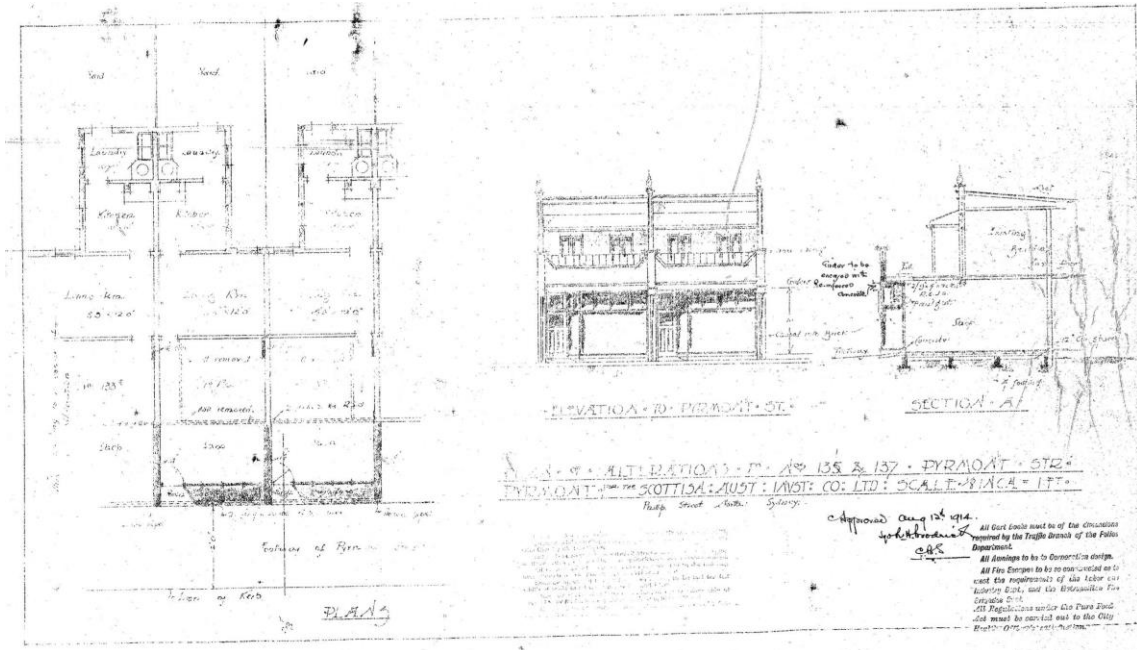


Figure 33: 1914 – Plan showing details for the proposed new shopfronts 129 and 131 Pyrmont Street, carried out by E. Spackman for the Scottish Australian Investment Co Ltd. The plan notes the wrong address but was submitted as part of BA 609/14. These buildings were occupied by a restaurant (No. 131, on left) and hairdresser (No. 129, on right) (Source: City of Sydney Archives, Pyrmont St (129-131) E Spackman alterations & additions (01/01/1914 - 31/12/1914), [A-00571870]. <https://archives.cityofsydney.nsw.gov.au/nodes/view/1436278>)

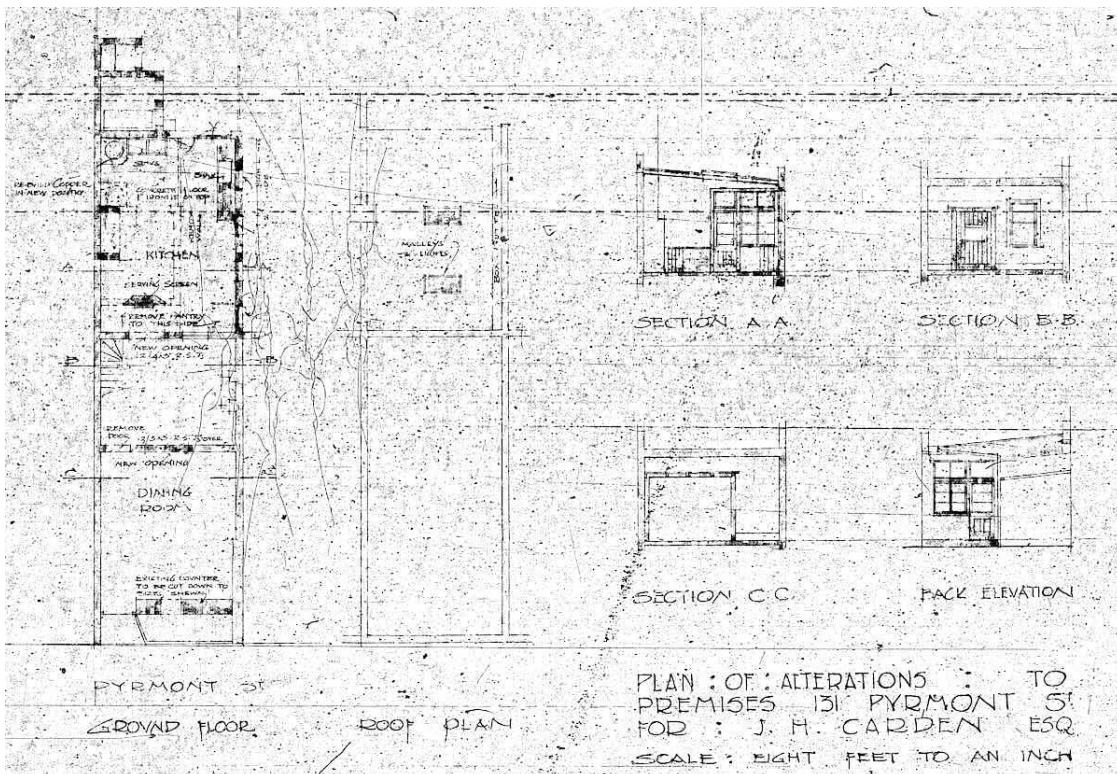


Figure 34: 1916 – Plan showing details for proposed alterations to the restaurant at 131 Pyrmont Street, by J. H. Carden. (Source: City of Sydney Archives, Pyrmont St (131) J H Carden alterations (01/01/1916 - 31/12/1916), [A-00550357]. <https://archives.cityofsydney.nsw.gov.au/nodes/view/1431879>)

2.5.4 Phase 4 (1947-present) – Gilbey’s Store, ‘Frankel House’ and redevelopment

2.5.4.1 Construction of warehouse and store for W & A Gilbey Ltd (1947-50)

Two years after acquisition of the vacant site fronting Pyrmont Bridge Road, C. J. Orme, architect, submitted a building application to Sydney Council for erection of a new building for W & A Gilbey Ltd on 11 July 1947 (BA 614/47).¹¹² An interim development application (IDA 83/1947) had been submitted to Council a couple of days earlier, which was granted on 11 August 1947.¹¹³ Consulting engineers were Sydney based A. S. Nicholson & H. R. Hammond, and the builders were Howie Moffat & Co Ltd. The building was estimated to cost £25,000 and £8,600. The initial plans outlined in the interim development application were for a double-storey reinforced concrete structure consisting of a free store, and a bond and bottlings store, as well as providing the NSW offices of Gilbey’s, and amenities for factory and office staff. It is possible that Nicholson and Hammond were largely responsible for the project as the Sydney based experts, while the architect responsible for the design, Charles J. Orme, was based in Melbourne.

C. J. Orme had been educated at Michael House in Durban, South Africa, and after migrating to Victoria, he had served his articles with Gibbs and Finlay, architects.¹¹⁴ While working in the office of Sidney Smith, Ogg and Serpell for nine years, he also studied art at the Victorian Artists’ Society. In 1934, Orme entered into partnership with Eric Hughes, however, he appears to have worked on his own again by 1947.

W. & A. Gilbey had been founded in London in 1857 by brothers Walter and Alfred Gilbey, who established a business as wine and spirits merchants, largely importing wines from the Cape of South Africa, and later producing their own gin, mainly London dry gin.¹¹⁵ Expanding quickly across England and Scotland, they soon entered the Australian market, importing and exporting spirits and wine, and in c1925 established their first Australian distillery in West Melbourne.¹¹⁶ From 1905, they had added grain whisky to their blend. W & A Gilbey merged with United Wine Traders Ltd in 1962, forming International Distillers and Vintners, and were later acquired by Grand Metropolitan which became part of Diageo PLC, together with Guinness PLC.¹¹⁷

As part of the approvals process for the 1947 IDA, a report was prepared by the City Architect and Building Surveyor’s Department in July 1947, which noted that the site was vacant and did not contain any buildings. The question of the widening of Pyrmont Bridge Road was also addressed, and the Town Clerk noted the following:

“In the year 1928 a scheme was submitted to the Commissioners providing for the widening of Pyrmont Bridge Road and Union Street to Eighty four feet (84’), but no action was taken to carry out the widening of these two (2) thoroughfares.

Since that date the proposal by the Maritime Services Board of New South Wales to remove Glebe Island Bridge and the proposal for the reconstruction of Pyrmont Bridge, involving the relocation of access to that Bridge in a more Southerly

¹¹² ‘Building Inspectors Card: NW Cnr Pyrmont Br Road & Pyrmont St. Gilbeys Ltd. Application to Erect New Building, BA 614/47’, City of Sydney Archives, accessed 31 January 2022, <https://archives.cityofsydney.nsw.gov.au/nodes/view/915644>.

¹¹³ City of Sydney Archives, IDA 83/47, ‘Pyrmont St, Pyrmont Bridge Road and Paternoster Row, Pyrmont – Proposed New Factory for W & A Gilbey Ltd’.

¹¹⁴ ‘Descriptive Details’, *Age*, 23 January 1934, 9, <http://nla.gov.au/nla.news-article203375086>.

¹¹⁵ ‘W&A Gilbey | Scotch Whisky’, accessed 2 February 2022, <https://scotchwhisky.com/whiskypedia/5989/w-a-gilbey/>.

¹¹⁶ ‘Sydney.’, *Darling Downs Gazette and General Advertiser*, 11 December 1880, 3, <http://nla.gov.au/nla.news-article85754111>; ‘W. and A. Gilbey’s Plans’, *Newcastle Morning Herald and Miners’ Advocate*, 28 December 1929, 5, <http://nla.gov.au/nla.news-article137793719>.

¹¹⁷ ‘W&A Gilbey | Scotch Whisky’.

direction than Union Street, when consummated, will have the effect of considerably relieving traffic in both Pyrmont Bridge Road and Union Street.

Further, the scheme for providing an arterial expressway from the City to the West, in the line of Goulburn Street, will, it is felt, have the effect of removing a considerable amount of through traffic which now normally uses Pyrmont Bridge Road.

In all the circumstances, it is considered that any widening of Pyrmont Bridge Road and Union Street will not be necessary, and the Officers of both Councils recommend accordingly.”¹¹⁸

The building application was approved on 13 August 1947, but it was not until 23 February 1948 that works began with the erection of a builders shed and hoarding.¹¹⁹ By 10 March 1948, all hoarding had been completed and preparations for excavation works began, with amended plans submitted that day. Two days later, builders hoarding was erected on Pyrmont Street excavation started. The building inspector noted on 12 March 1948: “excavation for foundation shows rock at varying depths below ground level.”¹²⁰

On 23 March, excavation for column foundation was in progress, being noted as completed two months later. On 1 June, concreting of the foundation bases began, “with Glebe Island ready mixed concrete poured for column foundations on north side.”¹²¹ The nature of the foundation was noted as ‘rock’ in the Building Inspector’s Card, and the depth of the footings was noted as 4 feet. Progress was extremely slow, and it took nearly three years for the building to be completed.

Amended plans were submitted on 1 July 1948, when the foundation beams had all been poured and the brickwork was in progress on the ground floor. One week later, the brickwork up to ground floor beams was completed, and excavations for drainage had started. The amended plans were approved on 23 July 1948, and one week later, the site was levelled in preparation for concreting of the ground floor. By early September, the first portion of the ground floor was concreted, and reinforcements laid for the second portion.

The building inspector continued to observe the progress of the building with great care, noting in detail the various steps taken until completion of the building, halting works whenever plans had yet to be approved, and inspecting the structural details at short intervals throughout the construction process. In late September 1949, amended plans for the gin tank and tower section were submitted and approved on 6 October 1949. Although work on the parapet was nearing completion in January 1950, with the ground floor windows finally glazed in February, the project changed again on 25 January 1950, when amended plans for an additional, third storey were submitted and approved two days later. It was not until 15 September 1950 that the building was noted as having been completed to approval, with wet weather hampering the completion of the roof.

Details of plans submitted with the building application, including later and amended plans for the third floor, are included in Figure 35 - Figure 42 below.

¹¹⁸ Item No. 36 on Agenda Paper, TC No. 2566/47, dated 16 July 1949, contained in the IDA 83/47 file held by the City of Sydney Archives.

¹¹⁹ This and the following is based on ‘Building Inspectors Card’.

¹²⁰ ‘Building Inspectors Card’.

¹²¹ ‘Building Inspectors Card’.

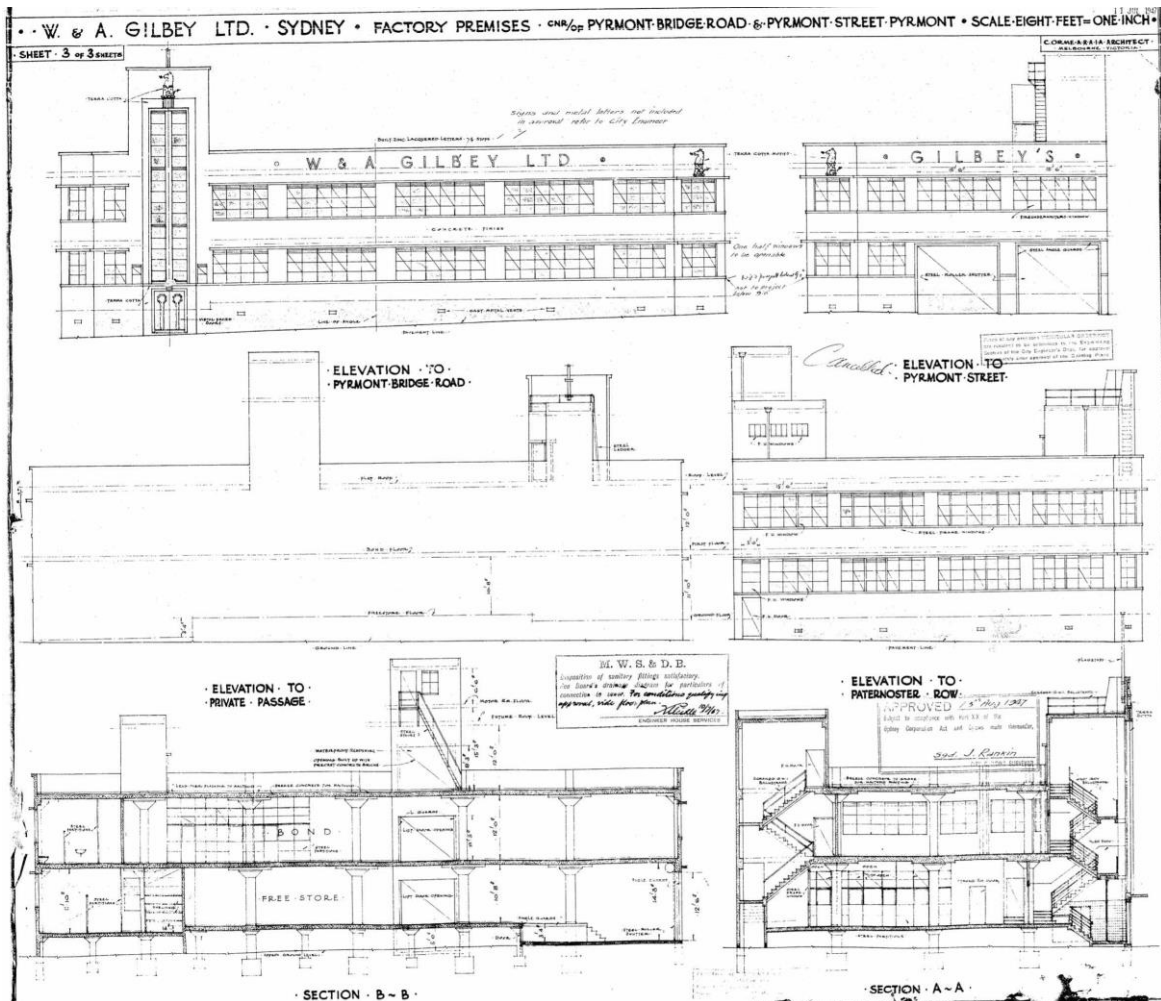


Figure 35: 1947 – Elevations and Sections for the proposed factory premises for W & A Gilbey Ltd by C. Orme, architect, Melbourne, approved 10 July 1947. These were later amended to include a third floor. The approximate ground level was noted by the bottom line in Section B-B and Section A-A (Source: City of Sydney Archives, Pyrmont Bridge Rd NW cnr & Pyrmont St (adjacent No 135), new building (01/01/1947 - 31/12/1947), [A-00572010]. <https://archives.cityofsydney.nsw.gov.au/nodes/view/1437047>)

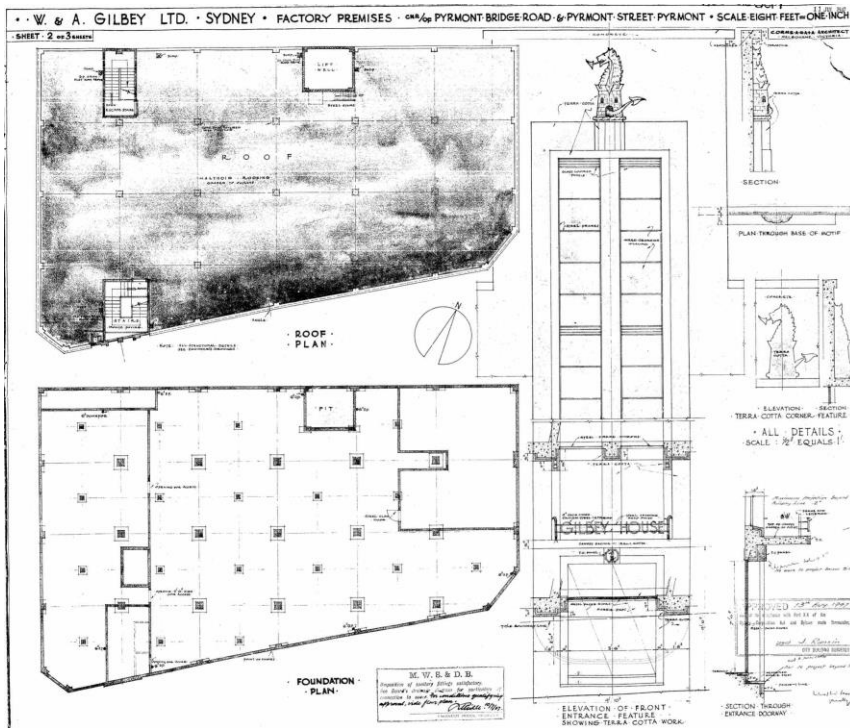


Figure 36: 1947 – Roof and foundation plans for the proposed factory premises for W & A Gilbey Ltd by C. Orme, architect, Melbourne, approved 10 July 1947 and amended in 1948 (Source: City of Sydney Archives, Pyrmont Bridge Rd NW cnr & Pyrmont St (adjacent No 135), new building (01/01/1947 - 31/12/1947), [A-00572010]. <https://archives.cityofsydney.nsw.gov.au/nodes/view/1437047>)

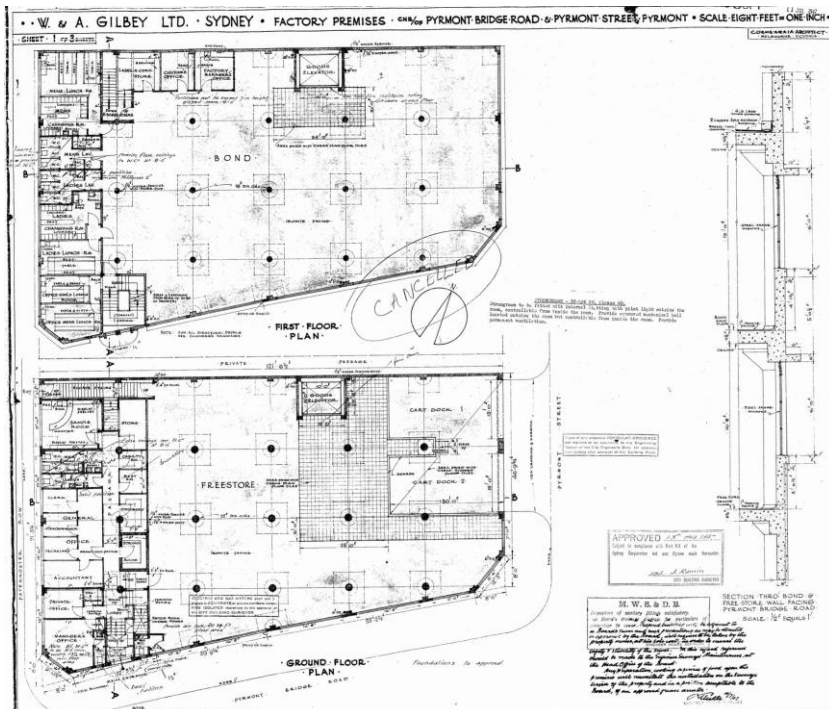


Figure 37: 1947 – Ground and first floor plans for the proposed factory premises for W & A Gilbey, by C. Orme, architect, Melbourne, approved 10 July 1947 and amended in 1948 (Source: City of Sydney Archives, Pyrmont Bridge Rd NW cnr & Pyrmont St (adjacent No 135), new building (01/01/1947 - 31/12/1947), [A-00572010]. <https://archives.cityofsydney.nsw.gov.au/nodes/view/1437047>)

Sydney Metro West – Pyrmont Station Construction Sites
Archaeological Research Design and Excavation Methodology

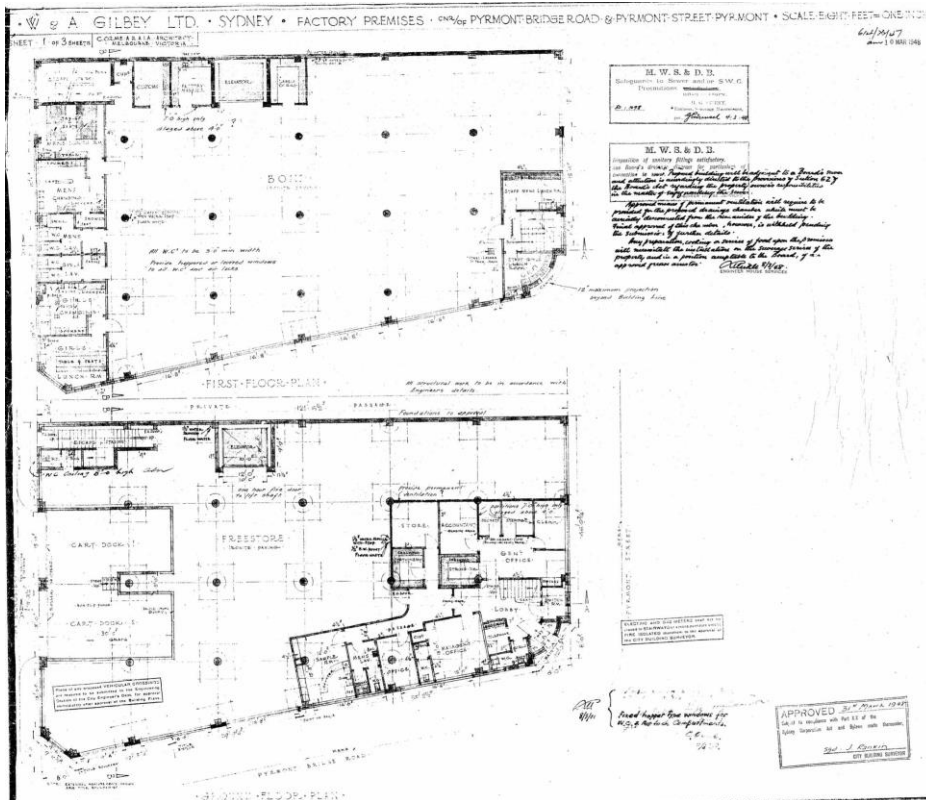


Figure 38: 1948 – Amended ground and first floor plans for the proposed factory premises for W & A Gilbey Ltd by C. Orme, architect, Melbourne, approved 31 March 1948 (Source: City of Sydney Archives, Pyrmont Bridge Rd NW cnr & Pyrmont St (adjacent No 135), new building (01/01/1947 - 31/12/1947), [A-00572010]. <https://archives.cityofsydney.nsw.gov.au/nodes/view/1437047>)

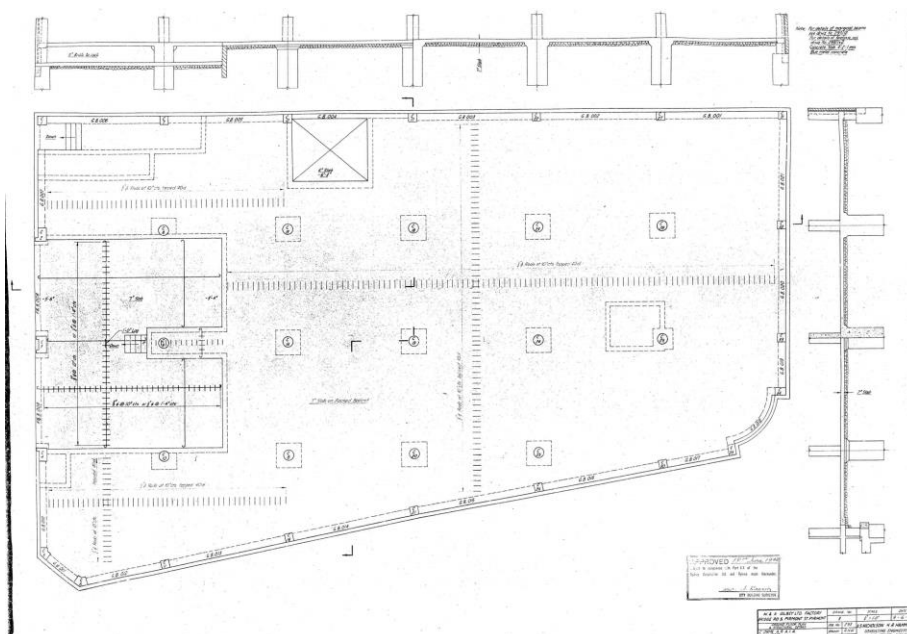


Figure 39: 1948 – Ground floor plan and structural details for W & A Gilbey Ltd Factory, by C. Orme, architect, and A. S. Nicholson & H. R. Hammond, consulting engineers, approved 18 June 1948 (Source: City of Sydney Archives, Pyrmont Bridge Rd NW cnr & Pyrmont St (adjacent No 135), new building (01/01/1947 - 31/12/1947), [A-00572010]. <https://archives.cityofsydney.nsw.gov.au/nodes/view/1437047>)

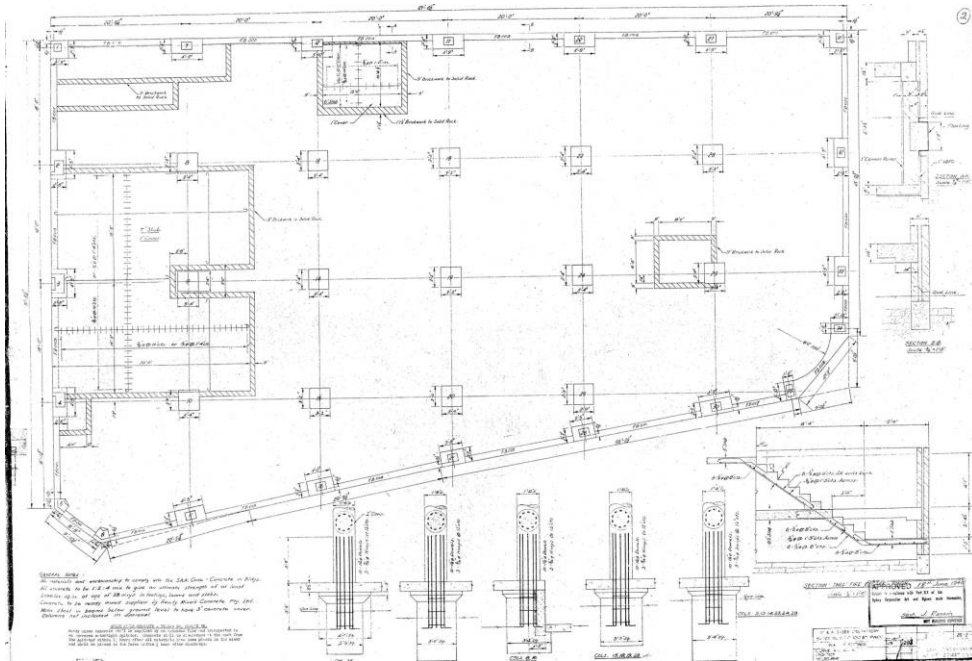


Figure 40: 1948 – Plan of footings for W & A Gilbey Ltd Factory, by C. Orme, architect, and A. S. Nicholson & H. R. Hammond, consulting engineers, approved 18 June 1948. The plan shows the locations of 'brickwork to solid rock' (Source: City of Sydney Archives, Pyrmont Bridge Rd NW cnr & Pyrmont St (adjacent No 135), new building (01/01/1947 - 31/12/1947), [A-00572010]. <https://archives.cityofsydney.nsw.gov.au/nodes/view/1437047>)

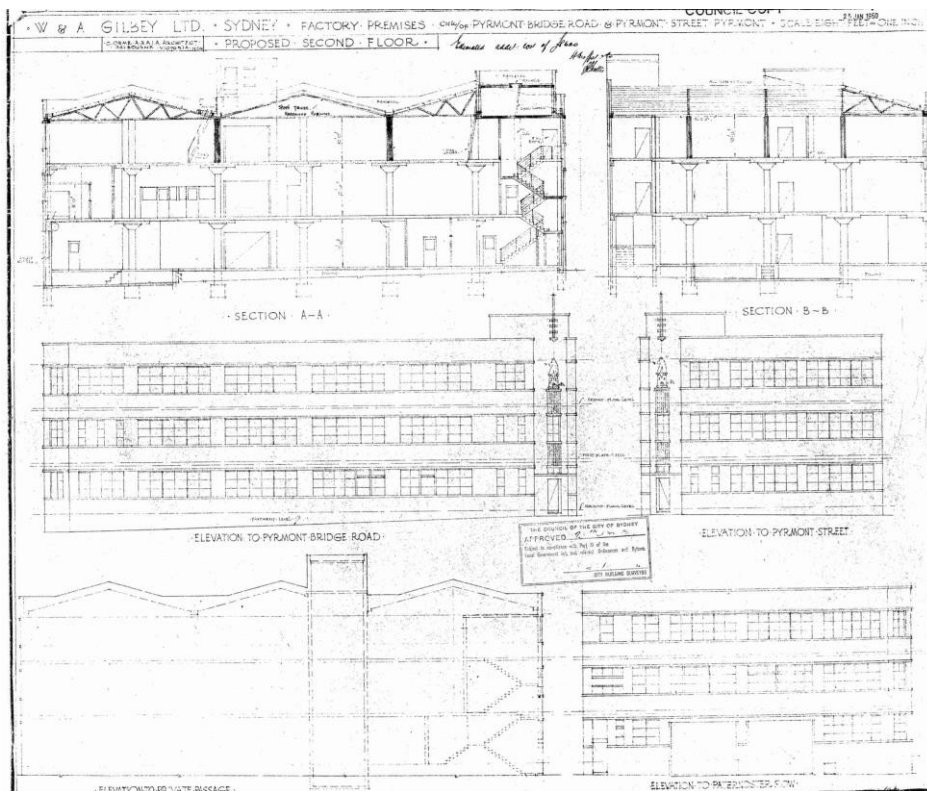


Figure 41: 1950 – Amended elevations and sections for proposed second floor (third storey) for W & A Gilbey Ltd Factory Premises, by C. Orme, architect, approved 27 January 1950 (Source: City of Sydney Archives, Pyrmont Bridge Rd NW cnr & Pyrmont St (adjacent No 135), new building (01/01/1947 - 31/12/1947), [A-00572010]. <https://archives.cityofsydney.nsw.gov.au/nodes/view/1437047>)

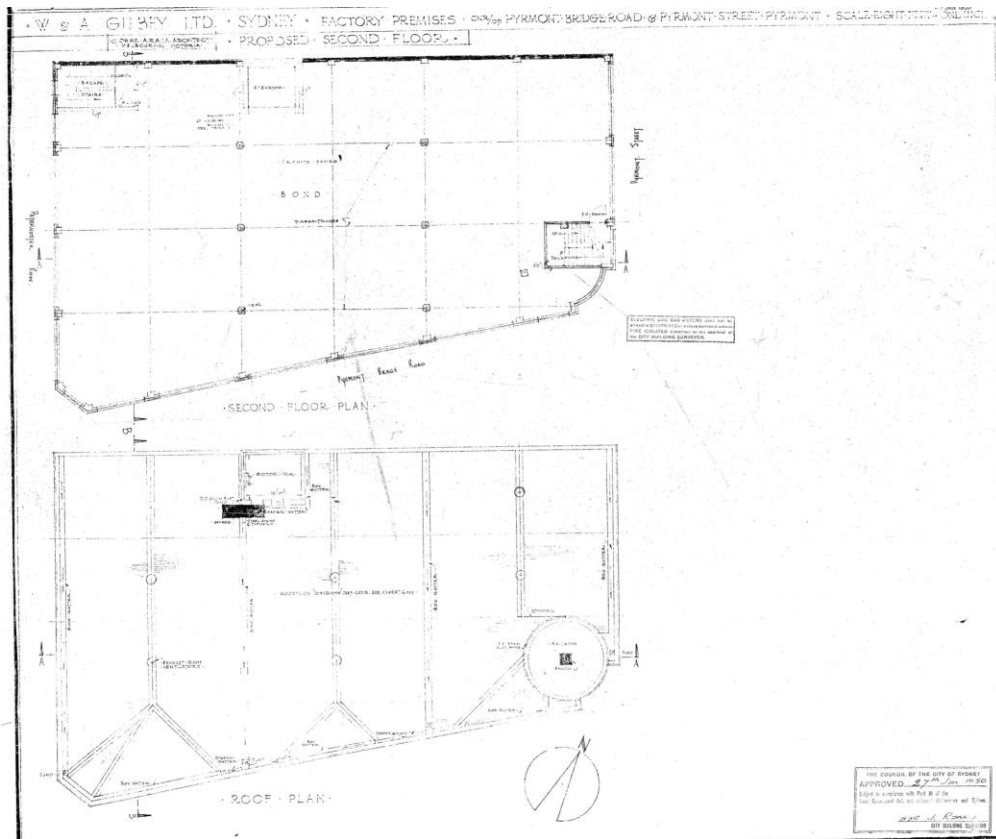


Figure 42: 1950 – Second floor plan and roof plan for proposed second floor (third storey) for W & A Gilbey Ltd Factory Premises, by C. Orme, architect, approved 27 January 1950 (Source: City of Sydney Archives, Pyrmont Bridge Rd NW cnr & Pyrmont St (adjacent No 135), new building (01/01/1947 - 31/12/1947), [A-00572010]. <https://archives.cityofsydney.nsw.gov.au/nodes/view/1437047>)



Figure 43: 15 December 1949 – Aerial photograph showing the building under construction (Source: City of Sydney Archives, A-00879983, City of Sydney Aerial Photographic Survey 1949: Image 38)

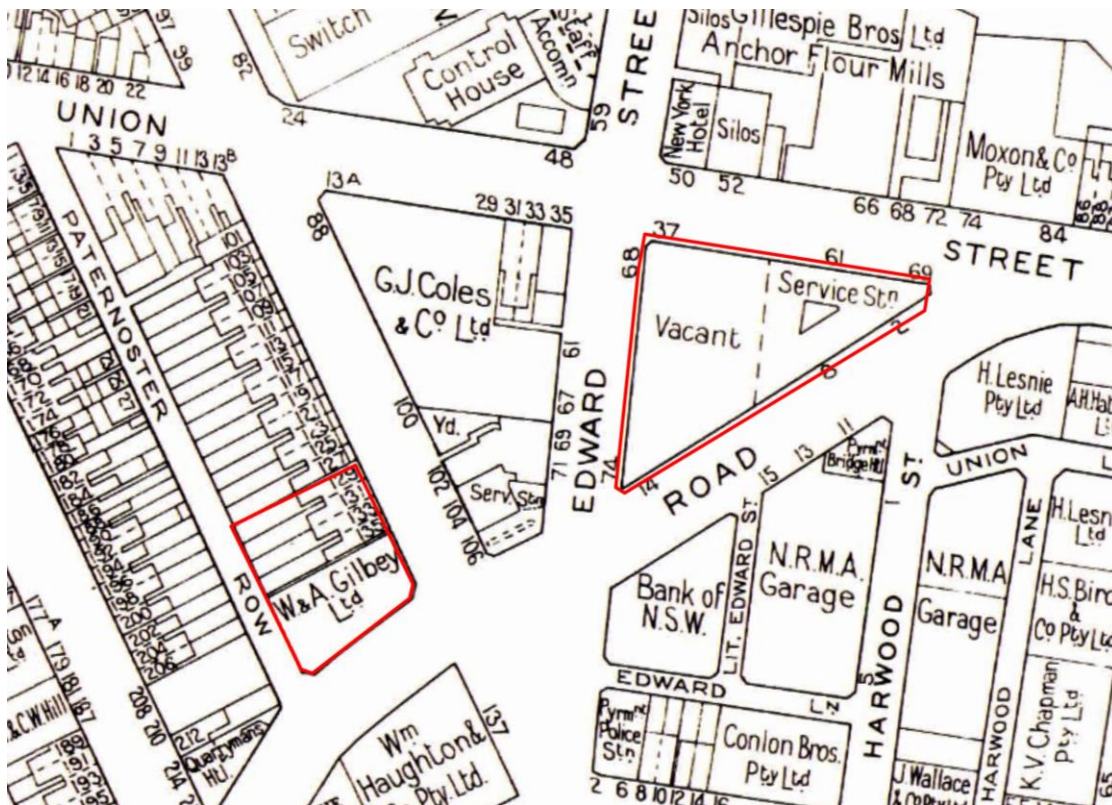


Figure 44: 1949-60s – City of Sydney Building Surveyor’s Detail Sheets, showing the W. & A. Gilbey Ltd premises and adjacent terraces (Source: City of Sydney Archives, A-008880206, City of Sydney Building Surveyor’s Detail Sheets, 5 - Pyrmont)

2.5.4.2 Development of 129 to 135 Pyrmont Street (1947-1966)

While no building and development applications were lodged in relation to the terraced houses at 129-135 Pyrmont Street during the 1940s, several applications were submitted during the 1950s, all for 133 and 135 Pyrmont Street. Those noted in the *Planning Street Cards* under 133-135 Pyrmont Street were:

- DA for use of premises for manufacture of electronic components by Resistor Engineering Co Pty Ltd (1268/51), dated 10 December 1951 (Permit No. 412);
- Proposal to erect a temporary canvas awning over the front verandah, by Resistor Engineering Co Pty (512/1/53), dated 25 August 1953;
- Unauthorised erection of a roof cover (444/56), dated 28 November 1956; and
- Labour and Industry Notification – Welding wire brackets, by W. A. L. Building Supplies Pty Ltd (DA 840/57?), dated January 1957.¹²²

The only building application that was submitted during the 1950s was for 135 Pyrmont Street, dated 1 December 1952, for alterations to the first floor at a cost of £100, by F. P. Noonan.¹²³

From September 1958, the Scottish Australian Investment Company subdivided their property, which still contained the majority of Block 57 of the Harris Estate, with the block bounded by Paternoster Row, Union Street, Pyrmont Street and Gilbey’s Building on Pyrmont Bridge Road being part of a plan

¹²² City of Sydney Archives, *Planning Street Cards*, Pyrmont Street (1928-1994), 133-135 Pyrmont St.

¹²³ City of Sydney Archives, *Planning Street Cards*, Pyrmont Street (1928-1994), 135 Pyrmont St.

deposited as 48128 (L).¹²⁴ As part of the subdivision sale, 129 to 135 Pyrmont Street were sold to individual owners as Lots 1 to 3, and were all acquired by Horwitz Corporation Pty Ltd during the early to mid-1960s, before Gilbey's Pty Ltd became the owners of all of these properties in 1968. A plan showing the outlines of the terraces on their individual allotments, as existing by 1959, is included in Figure 45 below.

133 Pyrmont Street, located on Lot 2, was acquired by Horwitz Publications Inc. Pty Ltd as part of the 1958 subdivision sale, with the transfer noted on 4 February 1959.¹²⁵ Later changing their name to Horwitz Corporation Pty Ltd, the company also became the owner of the adjacent 135 Pyrmont Street (Lot 1) on 8 February 1961.¹²⁶ This allotment had been transferred by the Scottish Australian Investment Company to Murray Hull Pty Ltd on 7 April 1959.

On 1 May 1961, Horwitz enquired with Council regarding the potential demolition of the existing terraces at 133-135 Pyrmont Street, as an alternative to remodelling and alterations (357/1/61).¹²⁷ In June 1961, the company submitted a development application for alterations and additions for printing works, offices and store (DA 379/61), suggesting that the decision had been made to retain the terraces.¹²⁸

However, this decision was later reversed. On 22 February 1962, a building application (BA 400/62) for a new store and office building was submitted by prominent Sydney architect, Harry Seidler.¹²⁹ The plans (Figure 46 - Figure 47), which involved demolition of the existing terraces, were approved on 20 March 1962. After approval was granted, the project stalled for a year until the BA lapsed. An extension was granted on 20 March 1963, but it took almost another year until work started. Demolition of the existing terraces at 129-131 Pyrmont Street was noted as being in progress on 3 February 1964 and continued for several weeks. By June 1964, the BA had lapsed again, but a further extension was granted. With the terraces demolished, no new building work was ever carried out and the BA lapsed for good on 15 February 1965 and was marked off the Building Inspector's list.

With their property at 133-135 Pyrmont Street now effectively vacant, Horwitz Corporation Pty Ltd acquired the adjacent two terraces at 129-131 Pyrmont Street; the official transfer was noted on 20 May 1965.¹³⁰ Located on Lot 3 of the 1958 subdivision, this had been sold as an allotment containing two terraced houses instead of one, and it had been transferred from the Scottish Australian Investment Company to Pyrmont shopkeepers, Anna and Paul Kutasi, on 16 October 1958. Just prior to the 1965 sale to Horwitz, a development application (DA 187/65) had been submitted by Christine Lynch Pty Ltd on 4 March 1965, on behalf of Paul and Anna Kutasi, to use the premises for manufacturing of jewellery.¹³¹ However, shortly later, on 28 April 1965, Sydney real estate agent Wilbur Humphrey Cowled had acquired the site, transferring it to Horwitz one month later.¹³²

Although Horwitz presumably acquired the terraces at 129-131 Pyrmont Street with the intention to propose a larger scheme for the overall site, the next development application submitted to Council,

¹²⁴ NSW LRS, CT Vol 5321 fol 194.

¹²⁵ NSW LRS, CT Vol 5321 fol 194; Vol 7708 fol 141.

¹²⁶ NSW LRS, CT Vol 7858 fol 133.

¹²⁷ City of Sydney Archives, *Planning Street Cards*, Pyrmont Street (1928-1994), 135 Pyrmont St.

¹²⁸ Also see '133/135 Pyrmont St. Horwitz Publications Inc. Pty Ltd. To Use Premises for Purpose of a Printing Factory with Associated Offices & Store & to Carry out Alterations (TC 4198/61)', City of Sydney Archives, accessed 2 February 2022, <https://archives.cityofsydney.nsw.gov.au/nodes/view/749473>.

¹²⁹ For the following see 'Building Inspectors Card: 133/135 Pyrmont St, Pyrmont. Store & Office Building DA379/61, Approved 20/03/1962 (BA 400/62)', City of Sydney Archives, accessed 2 February 2022, <https://archives.cityofsydney.nsw.gov.au/nodes/view/1587296>.

¹³⁰ NSW LRS, CT Vol. 7708 fol 142.

¹³¹ '129-131 Pyrmont Street Pyrmont. Alts to Premises and Use for the Mfg of Fashion Jewellery (DA 187/65)', City of Sydney Archives, accessed 2 February 2022, <https://archives.cityofsydney.nsw.gov.au/nodes/view/790629>.

¹³² City of Sydney Archives, *Planning Street Cards*, Pyrmont Street (1928-1994), 129-131 Pyrmont St; NSW LRS, CT Vol 7708 fol 142

dated 3 February 1965, was for 135 Pyrmont Street (DA 82/65).¹³³ The applicant was Sher Power Tools who proposed the erection of offices and showroom.¹³⁴ This did not go ahead, and instead, Gilbeys Australia Pty Ltd began to develop schemes for use of the site which it subsequently acquired.

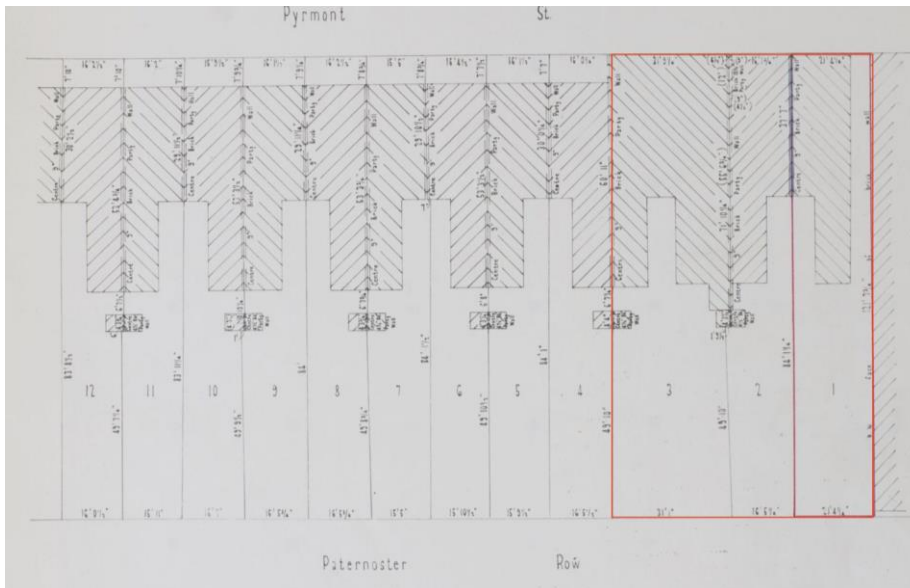


Figure 45: 1959 – Plan included in the Certificate of Title issued for Lot 1 in Plan 48128 (L), showing the subdivision and outline of buildings on Pyrmont Street, with 129 to 135 Pyrmont Street outlined. (Source: NSW LRS, CT Vol 7858 fol 133)

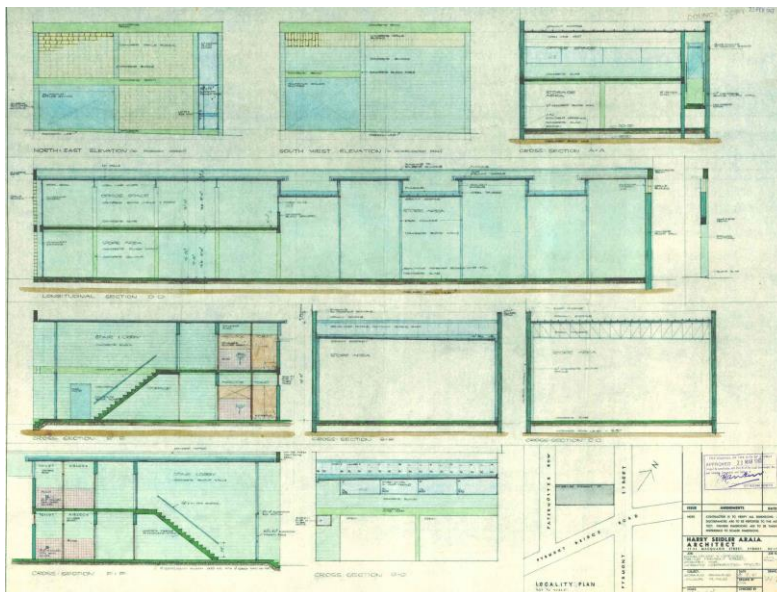


Figure 46: 1962 – Elevations and sections for proposed new bookstore and offices at 133-135, by Harry Seidler, architect, working drawing dated 8 February 1961. The BA was approved on 20 March 1962 and the existing terraces at the site were demolished in 1964, but the building was never constructed (Source: City of Sydney Archives, Pyrmont St (133-135) (01/01/1962 - 31/12/1962), [A-00547430]. <https://archives.cityofsydney.nsw.gov.au/nodes/view/1411167>)

¹³³ City of Sydney Archives, *Planning Street Cards*, Pyrmont Street (1928-1994), 135 Pyrmont St Pyrmont.

¹³⁴ '133-135 Pyrmont St. Pyrmont. Proposed Erection of Office Warehouse. Sher Power Tools Pty Ltd. (DA 82/65)', City of Sydney Archives, accessed 2 February 2022, <https://archives.cityofsydney.nsw.gov.au/nodes/view/787638>.

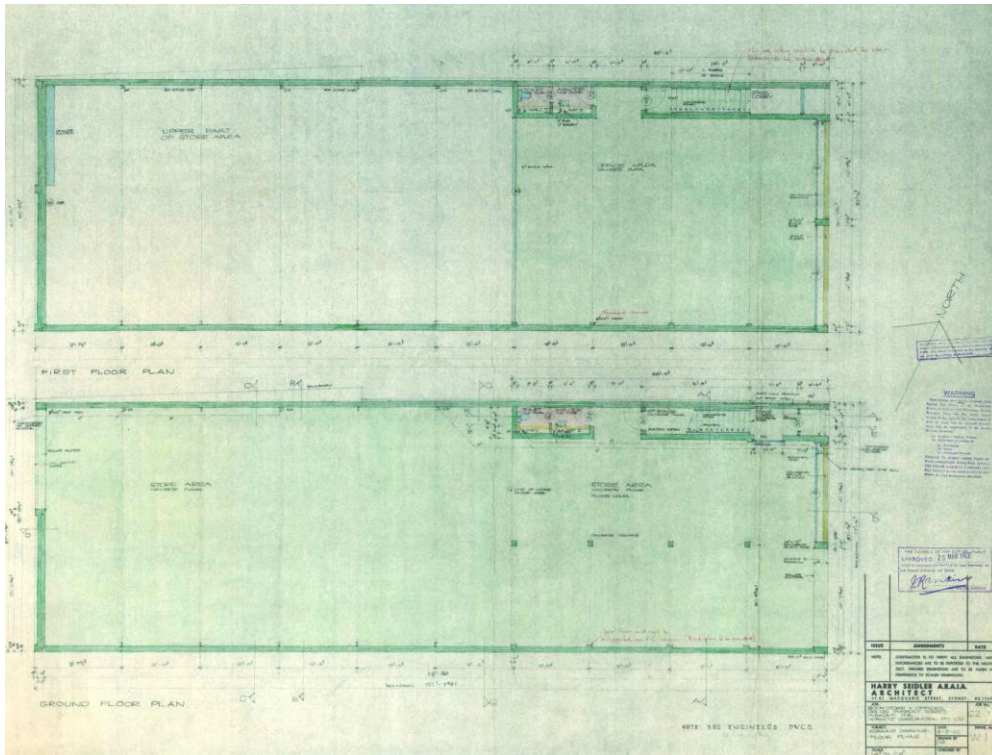


Figure 47: 1962 – Floor plans for proposed new bookstore and offices at 133-135, by Harry Seidler, architect, working drawing dated 8 February 1961 (Source: City of Sydney Archives, Pyrmont St (133-135) (01/01/1962 - 31/12/1962), [A-00547430]. <https://archives.cityofsydney.nsw.gov.au/nodes/view/1411167>)

2.5.4.3 Car park for Gilbey's at 129-135 Pyrmont Street (1967)

On 20 December 1966, Gilbey's Australia Pty Ltd submitted a development application (DA 974/66), for “proposed use of premises [at 129-135 Pyrmont Street, Pyrmont,] as a car park and for unloading the company's trucks and use the remainder of land as a parking station.”¹³⁵ In October 1966, Horwitz Corporation Pty Ltd, the owners of the site, had given consent to Gilbey's to make the application.¹³⁶ The development application stated that Gilbey's had the intention to purchase the site which still contained two vacant shops at 129-131 Pyrmont Street as well as vacant land at 133-135 Pyrmont Street. The plan in Figure 48 shows the site of the new car parking premises.

In relation to the proposed use of the new premises, the application form noted: “after demolition of Nos. 129-131 to temporarily use the whole of the land for the parking and unloading the Company's vehicles, parking of Company and staff cars in conjunction with the Applicants' adjoining bottling plant and warehouse and the balance as a parking station.”¹³⁷ The site was to be paved and fenced, and it was proposed that the parking station would provide spaces for about ten cars.

The city building surveyor wrote in his assessment report that 129-131 Pyrmont Street had been used as a cafe and residence since before 1951, while 133-135 had been used as a printing factory and for the storage of paper prior to being demolished, suggesting that Horowitz had indeed occupied the premises as part of their business. Consent for the new car park was granted on 17 April 1967, under the condition that two separate driveway crossings were provided on Pyrmont Street, one for entry

¹³⁵ City of Sydney Archives, *Planning Street Cards*, Pyrmont Street (1928-1994), 135 Pyrmont St Pyrmont.

¹³⁶ If not otherwise noted, this and the following is based on the file held by the City of Sydney Archives, DA 974/66, ‘129-135 Pyrmont Street – Gilbeys Aus P/L – Proposes use of premises as a car park and for unloading the company's truck and use the remainder as a parking station.’

¹³⁷ *Ibid.*

and one for exit of vehicles. By 9 August 1967, demolition of the two remaining terraces at 129-131 Pyrmont Street was complete.

However, by December 1967, negotiations between Horwitz and Gilbey's had stalled and work on the new car parking area stopped, and as the land was not fenced off it appears that the public had started using the vacant land as a free parking area. However, not much later, on 14 February 1968, all three allotments, containing 129-135 Pyrmont Street, were officially transferred to Gilbey's Australia Pty Ltd.¹³⁸ From then on, no public parking was allowed, and no fees were collected on the land which had been paved and fenced by early March 1968.

On 20 July 1971, Gilbey's Australia Pty Ltd submitted another development application for 129-135 Pyrmont Street, for change of use.¹³⁹ The land, which had been used for staff and company car parking for around three years, was to be used for car parking of 19 staff vehicles, and as a loading dock for Gilbey's adjoining premises, involving the construction of a new loading dock as well as erection of new fences and gates for the vacant land. Consent was granted on 29 September 1971.

A working drawing prepared in January 1972 by Anderson and Thomas, architects, shows the location of the new entrance gates to Paternoster Row and Pyrmont Street, the car parking spaces along the northern side boundary and the building line of the factory, and the new loading dock, which contained an existing fuel tank outlet that had likely been installed in 1968 (Figure 49, see Section 2.5.4.4). A new awning over the loading dock was approved as part of the same application on 31 April 1972, by which time the development had not yet commenced. The work was carried out from September 1972 and appears to have been completed in early 1973.

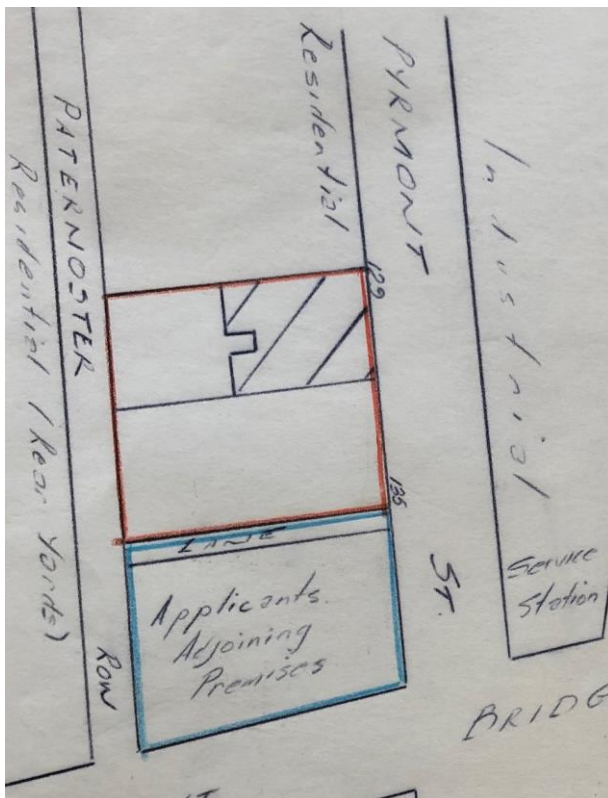


Figure 48: 1966 – Sketch plan included in DA 974/66, showing the location of the existing Gilbey's Building in blue, with the area of the proposed new car parking area outlined in red. At the time, that site still contained terraced houses at 129-131 Pyrmont Street (Source: City of Sydney Archives, DA 974/66, '129-135 Pyrmont Street – Gilbey's Aus P/L – Proposes use of premises as a car park and for unloading the company's truck and use the remainder as a parking station')

¹³⁸ NSW LRS, CT Vol 7858 fol 133; Vol 7708 fol 141 and fol 142.

¹³⁹ City of Sydney Archives, DA 465/71, '129-135 Pyrmont Street, Pyrmont – Gilbeys Australia P/L – Use premises for parking and off-street loading'.

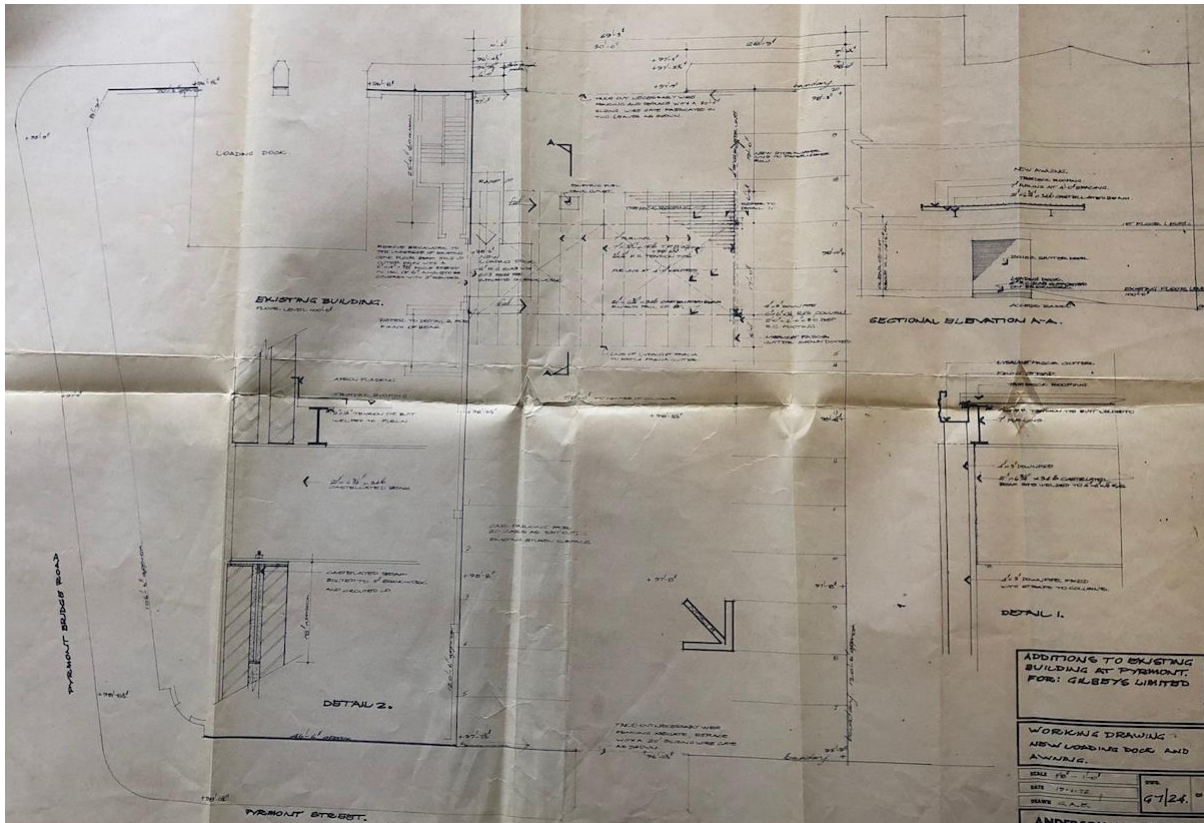


Figure 49: 1972 – Working drawing prepared by Anderson and Thomas, architects, dated 19 January 1972, as part of DA 465/71 (Source: City of Sydney Archives, DA 465/71, ‘129-135 Pyrmont Street, Pyrmont – Gilbeys Australia P/L – Use premises for parking and off-street loading’)

2.5.4.4 Alterations to Gilbey’s Building at 26-32 Pyrmont Bridge Road (1947-1999)

After its completion in September 1950, the building on the corner of Pyrmont Bridge Road, Paternoster Row and Pyrmont Street underwent numerous alterations over the years, however, these were largely of a minor nature and limited to changes to the internal fabric of the building. The following building and development applications were noted for the building in the City’s *Planning Street Cards* from 1951 to 1999:¹⁴⁰

Table 7: Building and development applications, 26-32 Pyrmont Bridge Road, 1951-1999

Reference	Date	Subject
73-19/51	30 Mar 1951	Labour and Industry Notification – Bottling wine and spirits, W. & A. Gilbey Ltd, ground, 1 st and 2 nd floors
BA 1596/55	6 Sep 1955	Demolish & re-erect toilet. Applicant: Howie & Moffat Co Pty Ltd
381-1/57	9 May 1957	Enquiry re zoning & Cumberland Planning Scheme
BA 1985/58	23 Sep 1958	Alterations and additions to sterile department, £10,000. Applicant: H. W. Thompson Bldg P/L
BA 2013/59	2 Sep 1959	Alterations and additions, £16,500. Applicant: Golding Constructions P/L
BA 1135/61	31 May 1961	Conveyor from Loading Dock to 1 st floor, £11,659. Applicant: Gilbeys Ltd

¹⁴⁰ City of Sydney Archives, *Planning Street Cards*, Pyrmont Bridge Road (1928-1994), 22-36 Pyrmont Bridge Road (several cards).

Sydney Metro West – Pyrmont Station Construction Sites
Archaeological Research Design and Excavation Methodology

Reference	Date	Subject
BA 1725/61	13 Aug 1961	Roof Sign Structure, £290. Applicant: Neon Signs Pty Ltd
BA 1674/64	17 Jul 1964	Alterations and additions, £10,000. Applicant: Leighton
BA 1539/67	19 Jul 1967	Partitions, £12,000. Applicant: George Ward & Co
BA 1612/67	28 Jul 1967	Mechanical ventilation, £6243. Applicant: McNeill Air Conditioning P/L
BA 2265/67	20 Oct 1967	Internal Partitions, £28,000. Applicant: George Ward & Co
BA 2398/67	6 Nov 1967	Mechanical ventilation, £7543. Applicant: McNeill Air Conditioning P/L
DA 350-1/68	20 Jul 1968	Installation of one 1000-gallon petrol storage tank (Paternoster Row). This was likely connected with the fuel tank outlet shown in the 1972 plan in Figure 49
BA 429/70	23 Mar 1970	Mechanical ventilation, £5000. Applicant: Thos Clark & Son P/L
BA 869/70	30 Jun 1970	Partitions 1 st floor, £4000. Applicant: Anderson & Thomas
BA 345/71	22 Mar 1971	Partitions 1 st floor, £700. Applicant: George Ward Pty Ltd
BA 97/72	2 Feb 1972	New loading dock, awning & shutter door, £2000. Applicant: George Ward Pty Ltd
BA 1521/72	29 Nov 1972	Alterations to dock, £1500. Applicant: George Ward Pty Ltd
DA 123/73	20 Feb 1973	Establish new loading dock in existing warehouse building. Applicant: George Ward P/L
BA 19/75	14 Jan 1975	Certificate of Compliance: Applicant: Perkins Stevenson & Linton
BA 406/78		Alterations, £23,000. Applicant: Anderson & Thomas
BA 832/78	11 Jul 1978	Air Conditioning 1 st Floor, £11,599. Applicant: McNeill Air Conditioning P/L
DA 44/83/0312	9 May 1983	Use ground floor for parking. Applicant: Remobe P/L
DA 88/1166	25 Aug 1988	Alter window to Existing Warehouse. Applicant: Jackson Teece Chesterman
BA 45/88/1877	25 Aug 1988	Refurbish existing warehouse with alterations, £876,000. Applicant: Jackson, Teece, Chesterman, Willis
BA 45/88/2598	15 Nov 1988	Erect Awning (cost included in BA 1877/88). Applicant: Jackson, Teece, Chesterman, Willis
DA 24/89	16 Jan 1989	Use the whole of the premises as commercial. Applicant: Masterplan Consultants
BA 45/89/1032	20 Oct 1989	Partitions and alterations to services 2 nd floor, £35,000. Applicant: Cemac Interiors P/L
BA 45/89/1139	15 Nov 1989	Partitions and alterations to services, £35,000. Applicant: Cemac Interiors P/L
BA 482/92	27 May 1992	Partitions ground floor, £35,000. Applicant: Firth Lee P/L
DA 344/93	25 Jun 1993	Erect 2 illuminated fw signs, 2.8 x 1.1m 'Otis', £6200, Pyrmont Bridge Rd & Harris St Ground. Applicant: Otis Elevator Company Pty Ltd
BA 739/93	30 Jul 1993	Erect Flush Wall Sign, £6200. Applicant: Otis Elevator

The property was on 9 March 1984 transferred to Rosenthal & Sons Pty Ltd, followed by a transfer to Darane Pty Ltd on 29 October 1986.¹⁴¹ Only two years later, the ownership changed again, as the site was transferred to Bulawayo Holdings Pty Ltd on 14 July 1988. Immediately upon assuming ownership of the site, Bulawayo Holdings Pty Ltd embarked on a major refurbishment of the building, including alterations to all floor levels and to window openings which were proposed to be replaced with double glazed aluminium windows.¹⁴² Most of the internal partitions were removed at that time, and new fire rated concrete floors appear to have been installed. New partitions were installed in 1989 and 1992, as noted in the table above. It is likely that the name of the building was changed to 'Frankel House' at that time, likely after one of the directors of the company, Oliver Frankel (see Section 2.5.4.5). The site plan in Figure 54, prepared in 1988 as part of BA 1877/88, suggests that the parking area and loading dock largely remained unchanged but were used by the builders as sheds and storage areas.

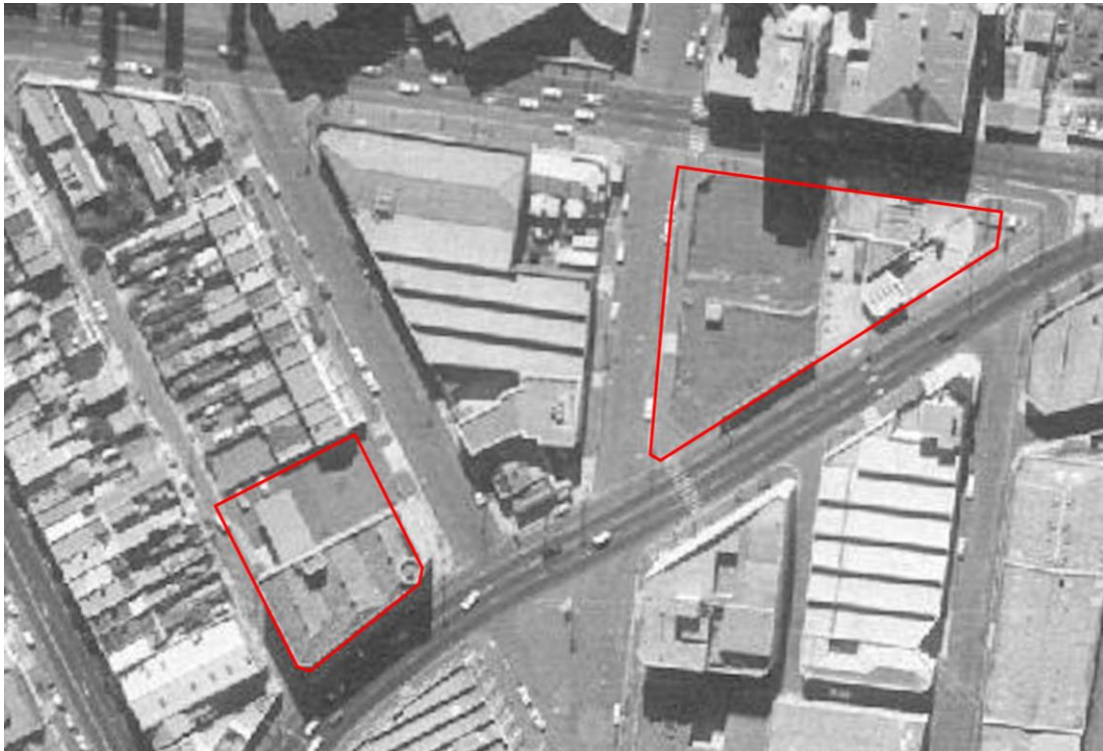


Figure 50: 1978 – Aerial photograph showing Gilbey's Building with car parking to the rear. The roof over the loading dock is also clearly visible (Source: NSW Spatial Services, aerial photograph, 6 May 1978, 2713-16-064)

¹⁴¹ NSW LRS, CT Vol 7708 fol 142.

¹⁴² 'Pyrmont Bridge Rd (26-32) Pyrmont. Gilbeys Australia. Refurbish Existing Warehouse with Alts.', City of Sydney Archives, accessed 24 December 2021, <https://archives.cityofsydney.nsw.gov.au/nodes/view/1457240>.

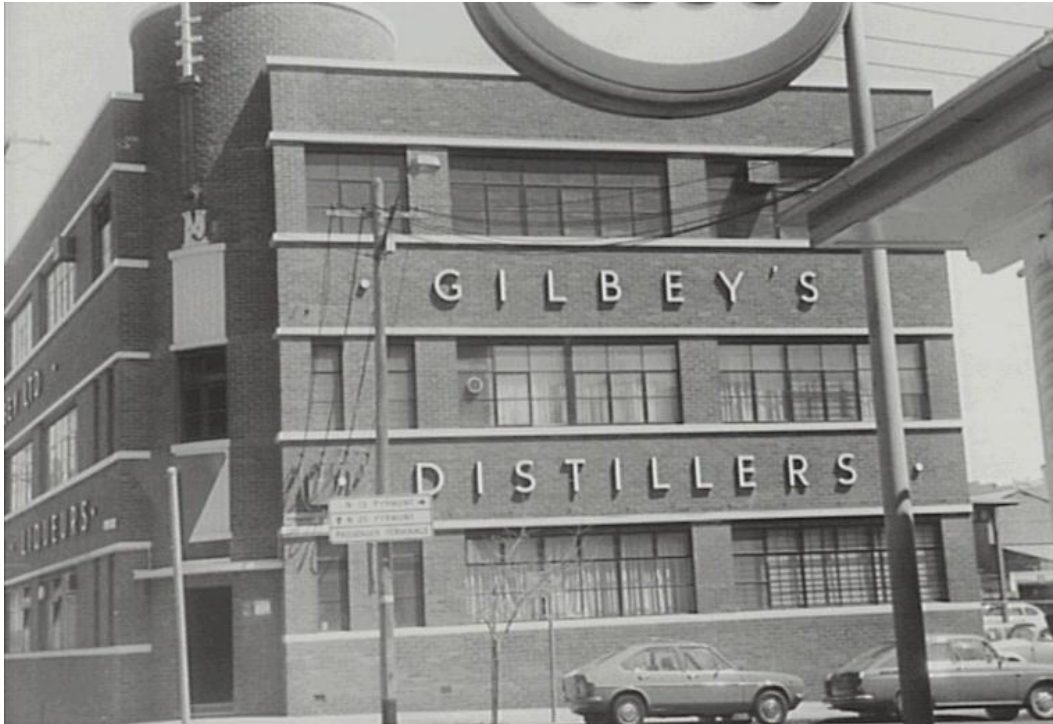


Figure 51: 1979 – The Pyrmont Street elevation of Gilbey's Distillers' factory and warehouse, with a small section of the rear car parking area and loading dock visible (Source: City of Sydney Archives, W&A Gilbey Ltd, 26-32 Pyrmont Bridge Road Pyrmont, 1979 (01/01/1979 - 31/12/1979), [A-00008101]. <https://archives.cityofsydney.nsw.gov.au/nodes/view/568746>)



Figure 52: 6 August 1979 – The Pyrmont Bridge Road elevation of W. & A. Gilbey Ltd's factory and store (Source: City of Sydney Archives, W&A Gilbey Ltd, Bridge Road Pyrmont, 1979 (06/08/1979), [A-00007942]. <https://archives.cityofsydney.nsw.gov.au/nodes/view/568587>)



Figure 53: 1986 – Aerial photograph showing the site at 26-32 Pyrmont Bridge Road largely unchanged (Source: NSW Spatial Services, aerial photograph, 3 August 1986, 3528-22-061)

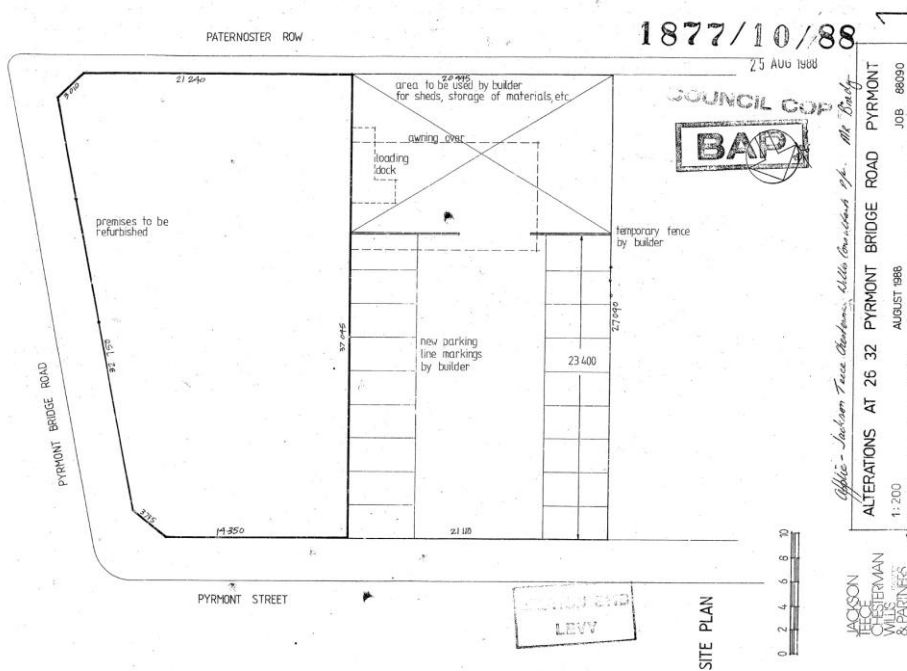


Figure 54: 1988 – Site plan prepared by Jackson Teece Chesterman Willis & Partners as part of BA 1877/88, dated August 1988 (Source: City of Sydney Archives, Pyrmont Bridge Rd (26-32) Pyrmont. Gilbeys Australia. Refurbish existing warehouse with alts. (01/01/1988 - 31/12/1988), [A-00574747]. <https://archives.cityofsydney.nsw.gov.au/nodes/view/1457240>)

2.5.4.5 Commercial redevelopment 'Frankel House' site (1999)

On 14 October 1999, Bulawayo Holdings Pty Ltd submitted a development application (D99/00812) for alterations to and extension of the existing Gilbey's building at 26-32 Pyrmont Bridge Road, Pyrmont, including the provision of a new entrance to Pyrmont Street, new stairs and lift access, and construction of a new three storey building/extension on the northern side incorporating a two level basement car park for 24 cars.¹⁴³ The new extension was located on the site of the former car parking and loading dock area and involved a double storey section with rear courtyard along the northern boundary of the site. The new basement car park was accessed from Paternoster Row.

The Central Sydney Planning Committee was the consent authority for the development, as the new addition exceeded the maximum permissible height and Floor Space ratio standards. Documents included in the DA file held by the City of Sydney Archives referred to the building as 'Frankel House' and noted that there had been no other relevant development application history after the 1971 application for use of the northern portion of the site as car parking. Oliver Frankel was a director of Bulawayo Holdings which had owned the site since 1988, which may explain the name change for the building.¹⁴⁴

The application was approved on 16 February 2000. As part of the conditions of consent, all land titles within the site had to be consolidated into one lot. As part of the development, an Archaeological Research Design Report was prepared by Godden Mackay Logan in May 2000, for Property Partnerships Pacific and the NSW Heritage Council.¹⁴⁵

A Section 96 (2) Modification application was approved on 18 August 2000, which was for minor alterations to the original design, including alterations to the corner 'drum' element, minor repairs to the existing building and a new 'vergola' in the rear courtyard.

The redevelopment was designed by Allen Jack + Cottier, architects, who prepared plans in December 1999, details of which are included below. Also included below are photographs taken in 1999, showing the site prior to redevelopment, with signage on the building's corner naming it as 'Frankel House'.

Apart from a Section 96 (1A) amendment "to modify condition 12(c) to say 'Parking for no more than 23 cars' and add new condition 12(G) to install shower facilities and lockers adjacent to existing bicycle parking area," submitted under D/1999/812/D approved in July 2017, no further applications appear to have been made for the property.¹⁴⁶

¹⁴³ The following is based on the file held by the City of Sydney Archives, D1999/00812, 'Development Applications/26-32/Pyrmont Bridge Road/Pyrmont/Alterations & Additions to Existing Building – Bulawayo Holdings Pty Ltd – 14 10 99 - \$5000000.'

¹⁴⁴ As per written correspondence contained in the DA file for D99/00610

¹⁴⁵ Godden Mackay Logan, '26-31 Pyrmont Bridge Road, Pyrmont. Archaeological Research Design. Prepared for Property Partnerships Pacific and the NSW Heritage Council', May 2000.

¹⁴⁶ City of Sydney, Development Application Tracker, accessed 11 February 2022, <https://online2.cityofsydney.nsw.gov.au/DA/IndividualApplication?tpklapappl=1301186>.



Figure 55: 1999 – ‘Frankel House’ as seen from Pyrmont Bridge Road, looking northwest. (Source: City of Sydney Archives, D1999/00812, ‘Development Applications/26-32/Pyrmont Bridge Road/Pyrmont/Alterations & Additions to Existing Building – Bulawayo Holdings Pty Ltd – 14 10 99 - \$5000000’)



Figure 56: 1999 – Looking southeast from Paternoster Row towards the north elevation of the former Gilbey's Building, with the awning over the loading dock visible at centre. (Source: City of Sydney Archives, D1999/00812, ‘Development Applications/26-32/Pyrmont Bridge

Road/Pyrmont/Alterations & Additions to Existing Building – Bulawayo Holdings Pty Ltd – 14
10 99 - \$5000000')

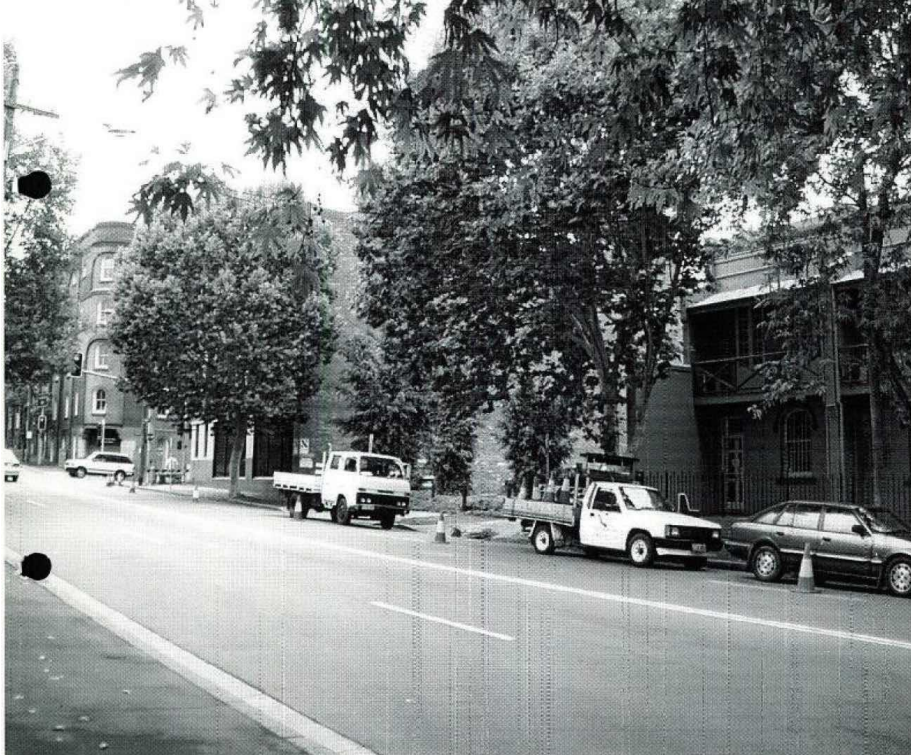


Figure 57: 1999 – Looking along Pyrmont Street towards the former Gilbey's Building and car parking area. (Source: City of Sydney Archives, D1999/00812, 'Development Applications/26-32/Pyrmont Bridge Road/Pyrmont/Alterations & Additions to Existing Building – Bulawayo Holdings Pty Ltd – 14 10 99 - \$5000000')

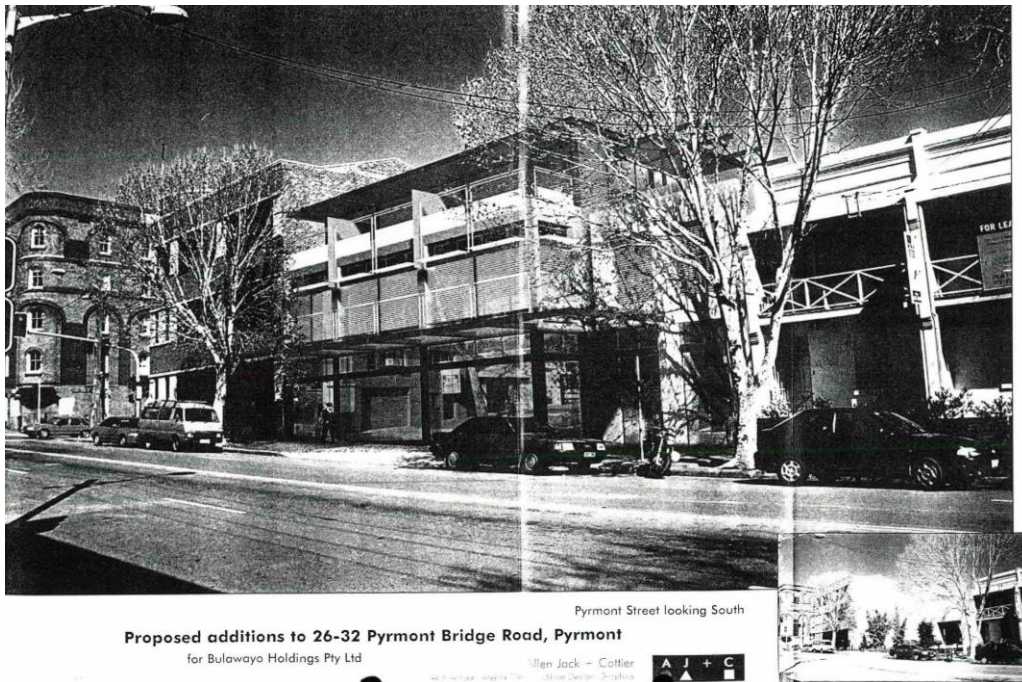


Figure 58: 1999 – Render of the proposed new development, by Allen Jack + Cottier for Bulawayo Holdings, showing the Pyrmont Street frontage (Source: City of Sydney Archives, D1999/00812, 'Development Applications/26-32/Pyrmont Bridge Road/Pyrmont/Alterations & Additions to Existing Building – Bulawayo Holdings Pty Ltd – 14 10 99 - \$5000000')



Figure 59: 1999 – Basement Level 2 plan, prepared by Allen Jack + Cottier, architects, dated 8 December 1999 (Source: City of Sydney Archives, D1999/00812, 'Development Applications/26-32/Pyrmont Bridge Road/Pyrmont/Alterations & Additions to Existing Building – Bulawayo Holdings Pty Ltd – 14 10 99 - \$5000000')

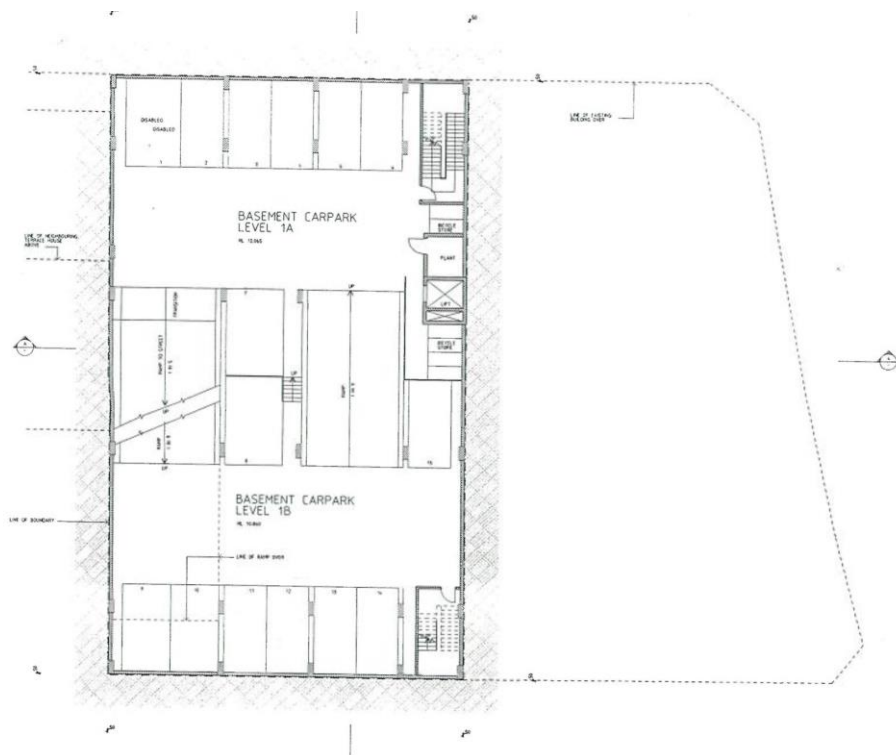


Figure 60: 1999 – Basement Level 1 plan, prepared by Allen Jack + Cottier, architects, dated 8 December 1999 (Source: City of Sydney Archives, D1999/00812, 'Development Applications/26-32/Pyrmont Bridge Road/Pyrmont/Alterations & Additions to Existing Building – Bulawayo Holdings Pty Ltd – 14 10 99 - \$5000000')

Applications/26-32/Pyrmont Bridge Road/Pyrmont/Alterations & Additions to Existing Building – Bulwayo Holdings Pty Ltd – 14 10 99 - \$5000000’)

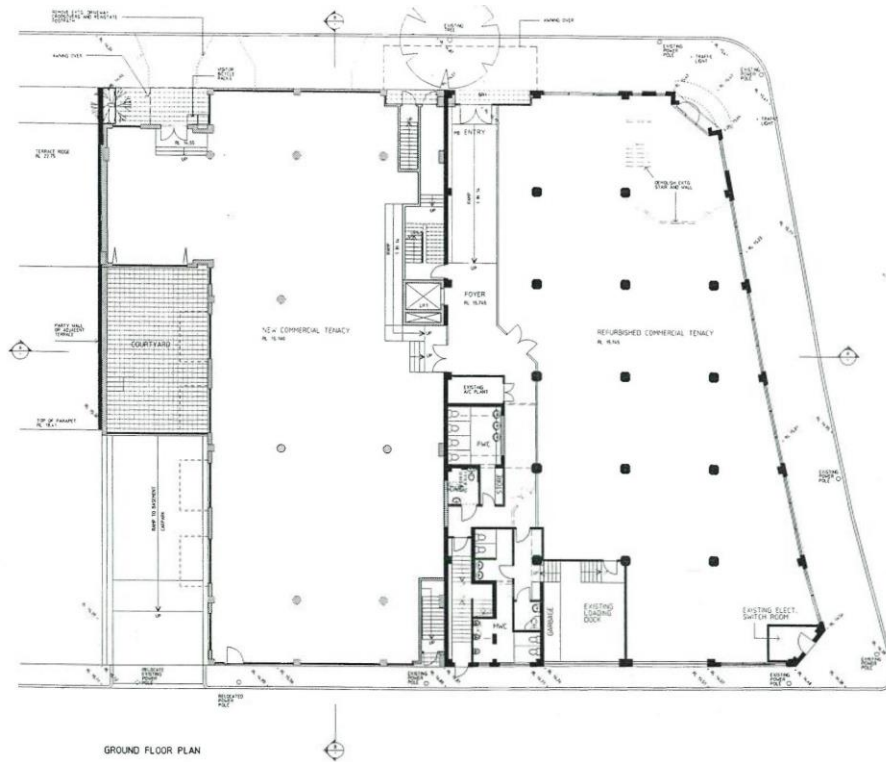


Figure 61: 1999 – Ground Floor plan, prepared by Allen Jack + Cottier, architects, dated 8 December 1999 (Source: City of Sydney Archives, D1999/00812, ‘Development Applications/26-32/Pyrmont Bridge Road/Pyrmont/Alterations & Additions to Existing Building – Bulwayo Holdings Pty Ltd – 14 10 99 - \$5000000’)

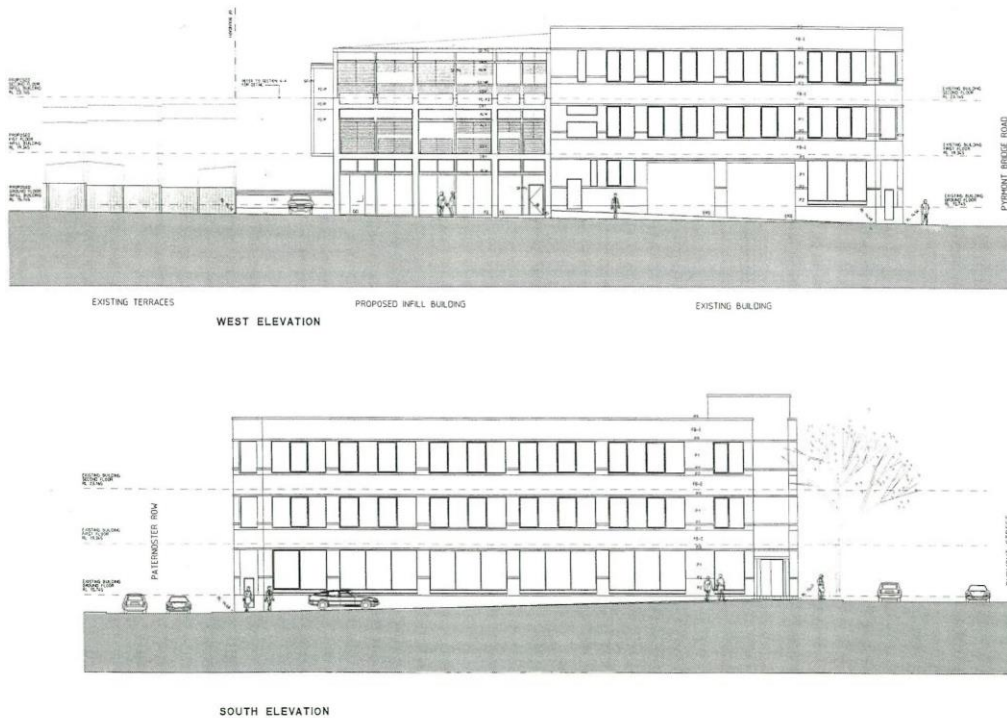


Figure 62: 1999 – West and south elevations, prepared by Allen Jack + Cottier, architects, dated 8 December 1999 (Source: City of Sydney Archives, D1999/00812, ‘Development Applications/26-32/Pyrmont Bridge Road/Pyrmont/Alterations & Additions to Existing Building – Bulawayo Holdings Pty Ltd – 14 10 99 - \$5000000’)

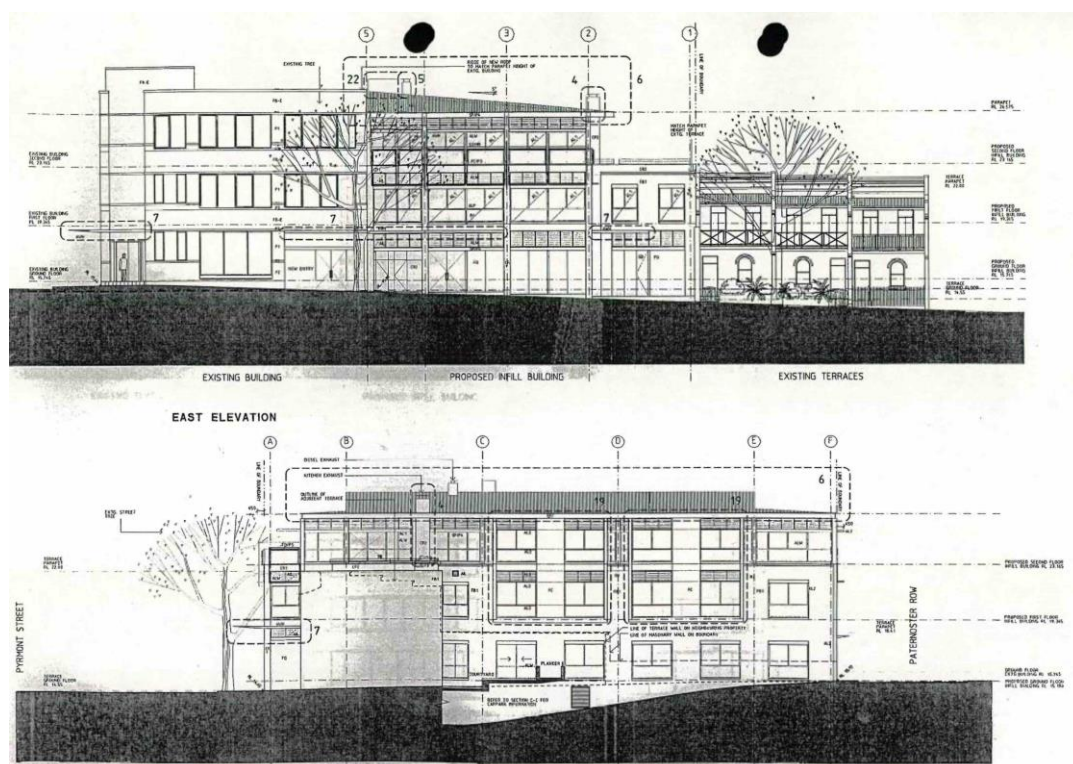


Figure 63: 2000 – East and north elevations, prepared by Allen Jack + Cottier, architects, as part of the Section 96 application, dated 7 April 2000 (Source: City of Sydney Archives, D1999/00812, ‘Development Applications/26-32/Pyrmont Bridge Road/Pyrmont/Alterations & Additions to Existing Building – Bulawayo Holdings Pty Ltd – 14 10 99 - \$5000000’)

2.6 Eastern Construction Site land-use overview

A summary of the historical use of the study area discussed in this chapter is provided in Table 3 below.

Table 8: Overview of historic land use phases

Phase	Date	Use of site
Phase 1	1788-1878	Part of the Ultimo Estate, potentially used for livestock grazing
Phase 2	1883-1930	Residential housing, commercial properties, ‘Elder’s Hall’
Phase 3	1930-1986	Timber and building materials storage yard, and associated sheds and offices, fuel service station, car park
Phase 4	1986-present	Commercial properties

2.6.1 Phase 1 (1788-1878) – Early land clearance and grazing

The Eastern Construction Site at 37-69 Union Street, Pyrmont, is located on Block 59b of the Ultimo Estate subdivision and remained undeveloped until the late 1870s, being potentially used for grazing.

During the partition of the Ultimo Estate in the 1850s, Block 59b was transferred to Mary Ann Harris, the daughter of George Harris, but she died on 24 September 1857, two years before the subdivision was finalised and the property was tentatively transferred to Matthew Harris instead, as shown in Figure 8. However, the official transfer was postponed, and Matthew Harris transferred the northern portion of Block 59b to John Harris of Shane's Park on 21 May 1867, retaining the southern portion of Block 59b, to the south of Pyrmont Bridge Road.¹⁴⁷

The plan in Figure 9 and the 1873 photograph in Figure 10 show the site as a vacant block of land that was bounded by Union Street, Pyrmont Bridge Road and Edward Street. The plan in Figure 64 shows the division of the original Block 59b, with details of land titles that are relevant to the northern and southern portion, divided by Pyrmont Bridge Road.

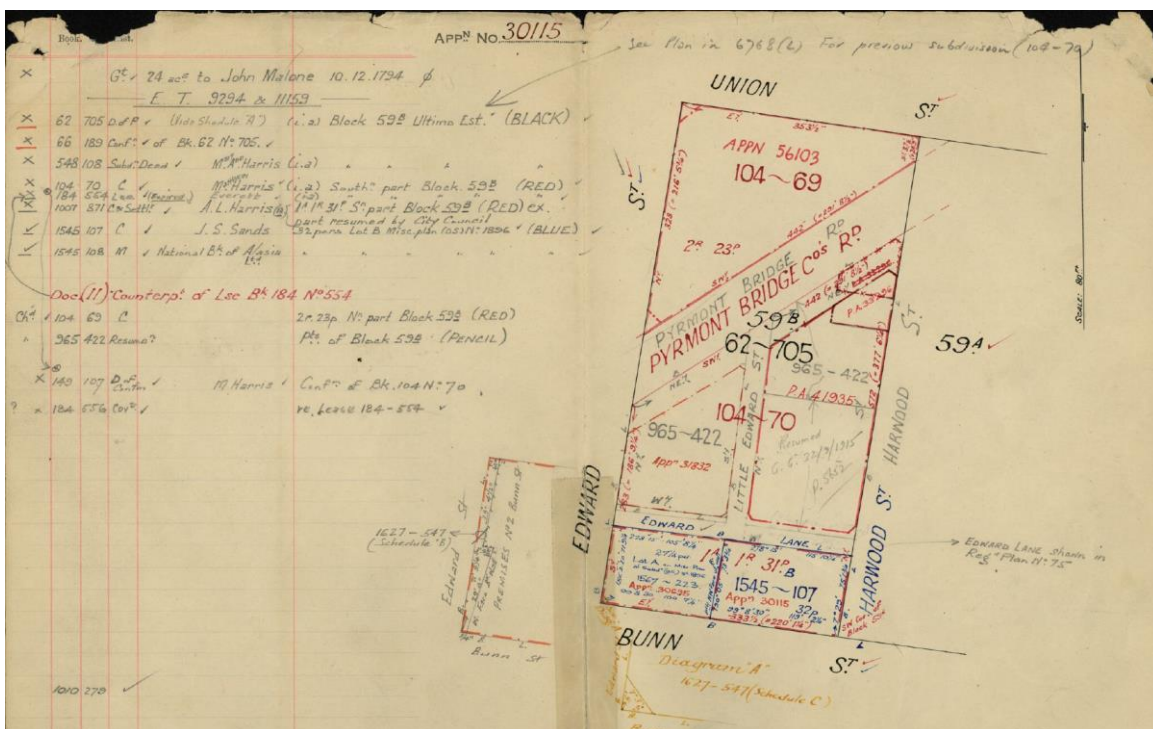


Figure 64: Plan from Primary Application Search Sketch for PA 30115, noting the land titles that relate to the study area, being the northern portion of Block 59b of the Ultimo Estate. (Source: NSW LRS, Historical Land Records Viewer, PASS 30115)

2.6.2 Phase 2 (1883-1930) – Subdivision and mixed-use development

2.6.2.1 Summary of Early Development at Eastern Construction Site (1878-1930)

Development on the block bounded by Union Street, Edward Street and Pyrmont Bridge Road began in 1878, with the *Sands Sydney Directory* edition of 1879 listing Pyrmont Bridge Road for the first time, noting at least two occupants within the study area, and noting the first occupants on the south

¹⁴⁷ See NSW LRS, Conveyance Bk 104 no. 69.

side of Union Street in the same year. Likewise, the first four occupants of buildings on the eastern side of Edward Street were listed in the same 1879 edition of the *Sands*.

From 1877, the land was part of the same leasehold by Mills & Pile, estate agents, who may have been responsible for some of the development on the land. However, it appears that several undocumented subleases were subsequently made, as well as various subsequent changes to the headlease, potentially leading to subdivision of the site for lease purposes over time. As these leases are mainly documented in the Old Title System, it has not been feasible to investigate all of these changes. Most of the information included below has been based on the analysis of the *Sands Sydney Directory* and the *Assessment Books*, as well as maps and photographs, and newspaper articles.

While it has not been possible to determine the exact sequence of development and nature of occupation for each individual building on the block during this first phase of development, due to inconsistent numbering and renumbering of houses, and suspected (as well as proven) demolition and rebuilding activity, the block appears to have been fully developed by the mid- to late 1880s, with further development carried out at the north-eastern corner in the late 1880s and 1890s. Maps and aerial photographs showing the block and its buildings from c1888 to 1930 are included below, to illustrate the density and configuration of development within the block, as well as the early street numbering of buildings (Figure 65 - Figure 67).

The terraced buildings on Union Street were largely of stone and brick construction and consisted of double storey shops or residences, with shops concentrated around the corners of Edward and Pyrmont Bridge Road. Most notably, the frontage contained a c1883 hall that was in heavy use during the 1880s and 1890s, until it burnt down in 1903 and was subsequently redeveloped into residential terraces. Many of the buildings, including the hall, were associated with William Elder, who was listed as the occupant of a timber blacksmith's workshop at the corner of Edward and Union Streets in the 1879 *Sands*. The workshop occupied the land directly behind the brick buildings at 37 to 45 Union Street, on the corner of Edward and Union Streets.

The c1883 hall, which was initially constructed as a Dance Academy and was later known as a skating rink and gymnasium, was generally known as 'Elder's Hall'. The name, 'W. Elder, Ironmonger', was also inscribed below the parapet of the building shown in Figure 81 (at 41 Union Street, originally known as 93 Union Street). Elder later moved to 113 Union Street (61 Union Street), where he lived until c1887, before he was declared bankrupt in c1890.

After John Harris' death in 1891, the Eastern Construction Site was transferred to the Perpetual Trustee Company Ltd which sold the north-eastern corner of the site to Josiah White, grocer, in 1896. That same year, White constructed a three-storey building with basement on the corner of Union Street and Pyrmont Bridge Road, as a prominent landmark in the area. By June 1913 at the latest, the remainder of the site had been transferred to Matthew Harris.¹⁴⁸

The development on Pyrmont Bridge Road and on Edward Street was largely residential. There were four addresses on the Edward Street frontage, all noted as timber buildings and consisting of two detached houses and one semi-detached house. These buildings were double storey structures with gables facing the street. Similarly, the two pairs of semi-detached cottages on Pyrmont Bridge Road were noted as single storey timber buildings. A creek may have run across the middle of the block during the 1880s, as suggested by an irregularly shaped line in the plans in Figure 65 and Figure 66.

The below sections provide a more detailed overview of the development and nature of buildings on each street frontage during the late 19th and early 20th century, including details of occupants, until the buildings were demolished from the late 1920s.

¹⁴⁸ See NSW LRS, Primary Application (PA) 56103.

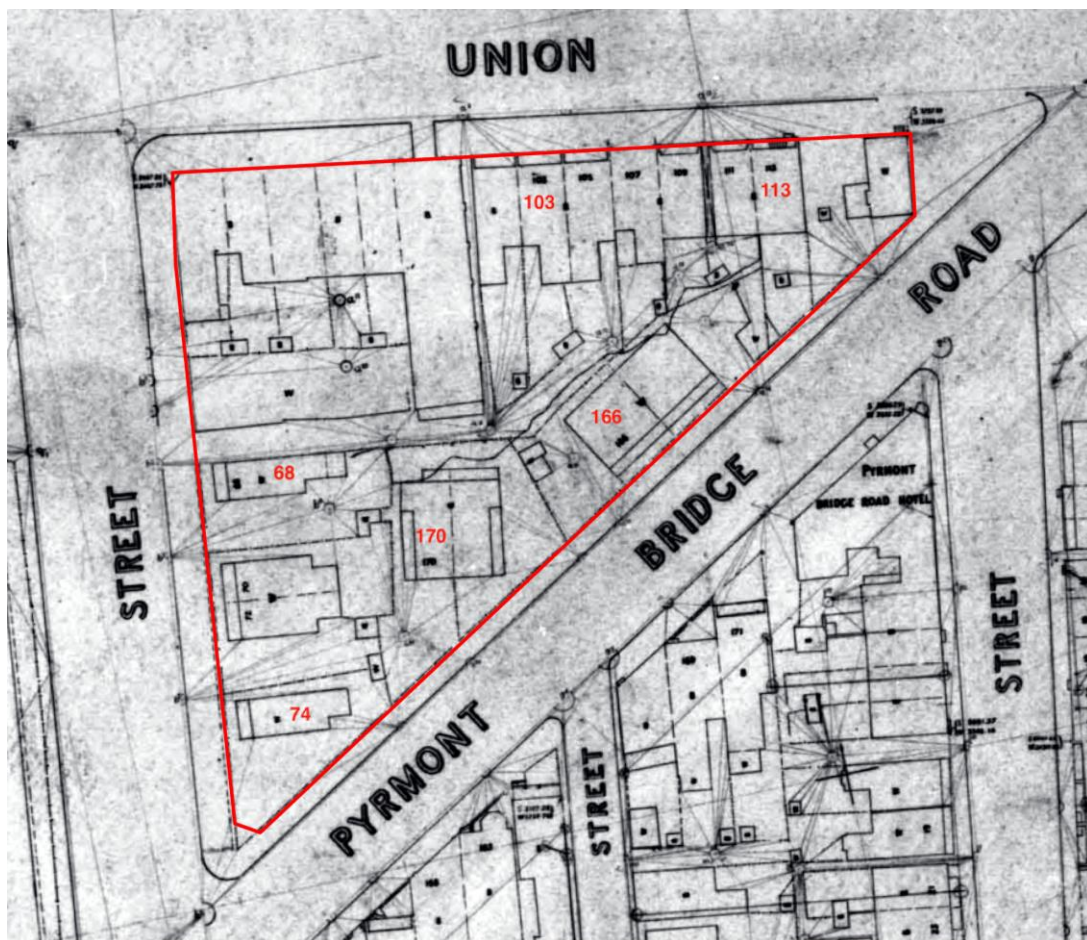


Figure 65 Detail from Public Works Department plan for Pymont, Sheet K3, submitted to the Surveyor General on 9 October 1886. This shows the original numbering of the buildings from 103 to 113 Union Street, 68 to 74 Edward Street, and 164 to 170 Pymont Bridge Road. (Source: Sydney Water Archives, PWDS1544-S235)

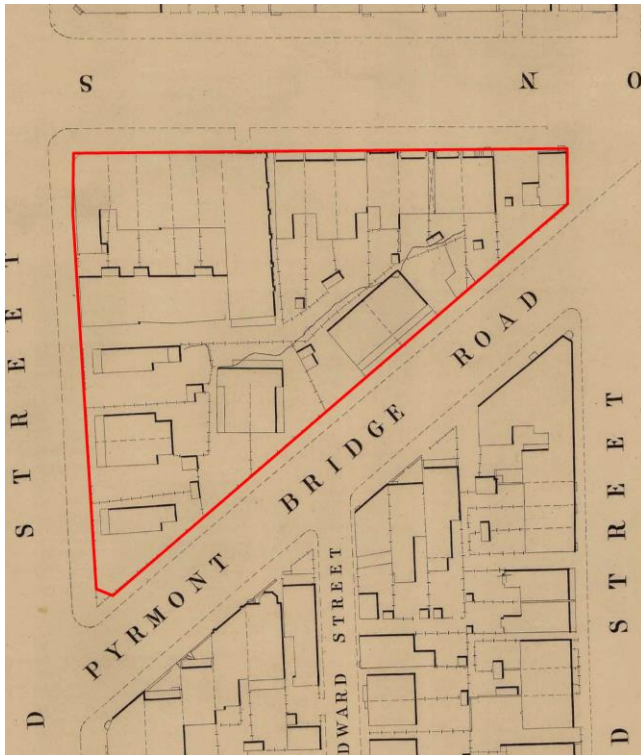


Figure 66: c1888 – Detail from City of Sydney Section (Metropolitan Detail Series), Sheet K3, showing the development of the block, with Union Street on the right. The plan was lithographed and printed in May 1888, based on an earlier survey (Source: State Library of NSW, Z/M Ser 4 811.17/1, <https://collection.sl.nsw.gov.au/record/74VKqv5pXe4b/bBWNZQV5XWRll#viewer>)



Figure 67: 1930 – Detail from aerial view showing the Eastern Construction Site fully developed, including the 1896 building on the corner of Union Street and Pymont Bridge Road, at lower right. The timber buildings to the rear of the shops at the corner of Union and Edward Streets have been demolished and the original hall replaced by residential terraces. Note the gabled buildings on Edward Street, and the single storey semi-detached cottages on Pymont Bridge Road, which are about to be demolished (Source: Fairfax Corporation. (1930). Aerial view of Darling Harbour showing Pymont Bridge closing following passage of a ship, Sydney, ca. 1930s. <http://nla.gov.au/nla.obj-163204711>)

2.6.2.2 Development of 37-67 Union Street (1878-1930s)

2.6.2.2.1 Construction of Shops and Dwellings (1878-1882)

Development on the south side of Union Street, between Edward Street and Pyrmont Bridge Road, began in 1878, with the construction of a blacksmith's workshop occupied by William Elder and a general store by Robert Berry. Both were noted as the first buildings on Union Street in the *Sands Sydney Directory* of 1879,¹⁴⁹ and they were located at either end of the block's Union Street frontage: Elder's workshop was situated on the corner of Union and Edward Streets and Berry's store on the corner of Union Street and Pyrmont Bridge Road.

The following year, in 1880, Horatio Walpole, butcher, was listed as the third occupant on the street, between Elder and Berry.¹⁵⁰ The *Assessment Books* for the Denison Ward of 1880 also noted all three, at No. 89 (Elder), No 91 (Walpole) and No. 117 (Berry).¹⁵¹ Robert Edward Berry, who was a sawyer, was the occupant and owner of a double storey timber shop with iron roof and four rooms. William Elder was described as the occupant and owner of a single storey timber workshop with iron roof and 1 room, and Horatio Walpole was the occupant and owner of a single storey brick shop with iron roof and 2 rooms.

Elder was likely located to the rear of Walpole,¹⁵² whose butcher shop occupied the prime location at what was from 1882 until c1886 noted as 89 Union Street (this later became 37 Union Street). Walpole was seeking "help from a boy" on 12 February 1879, noting his address as "opposite the New York Hotel, Pyrmont".¹⁵³ That building remained a butcher's shop until well into the 1920s, before it was used as dining rooms. By 1882, it was described as a double storey brick shop with shingled roof and was noted as being owned by Mills & Pile and occupied by David Barnett.¹⁵⁴

The lease from John Harris to George Pile, on 1 August 1877, was officially registered on 1 October 1877, suggesting that part of the development on Union Street was potentially carried out by the estate agents who also subdivided and developed other land parcels in the area at that time, including the Western Construction Area (see Section 2.5.2).¹⁵⁵

That Mills & Pile had indeed taken over the head lease for the site was confirmed by John Y. Mills, partner of Mills & Pile, estate agents, who, when he gave evidence as a witness in June 1882, noted that in June 1881 he had acquired the leasehold of a property "which had a frontage of 230 feet to Union-street, 291 feet to Pyrmont Bridge-road, and 216 feet to Edward-street. The lease had 50 years to run, and the rent was £100 a year."

The 1882 *Assessment Book* entry suggests that the corner building/butcher shop at 89 Union Street had been extended in c1880-81, to include an upper storey above the shop as well as further shops immediately adjacent to the east, forming a unified street elevation for the three shops that contained a grocer's store at 91 Union Street (later No. 39) and William Elder's official ironmongery premises at 93 Union Street (later No. 41). Only these three addresses were noted as being owned by Mills & Pile. Elder's name was inscribed below the parapet of the building at 93 Union Street (see Figure 81), even though he did not seem to occupy it for long. He was only listed there once in the *Sands Directory*, in 1882, and in November 1881 he offered the shop for lease without stock, noting it as "doing a good trade in ironmongery."¹⁵⁶ The building was later used as a confectioner's shop, and

¹⁴⁹ City of Sydney Archives, *Sands Sydney Directory*, City Directory – Union Street (south side), 1879.

¹⁵⁰ City of Sydney Archives, *Sands Sydney Directory*, City Directory – Union Street (south side), 1880.

¹⁵¹ City of Sydney Archives, *Assessment Books*, Denison Ward – Union Street, 1880.

¹⁵² 'Advertising', *Evening News*, 22 August 1885, 6, <http://nla.gov.au/nla.news-article111019206>.

¹⁵³ 'Advertising', *Sydney Morning Herald*, 12 February 1879, 12, <http://nla.gov.au/nla.news-article13429148>.

¹⁵⁴ Mills' leasehold ownership appears to be confirmed in this article in the *Sydney Daily Telegraph*: 'Law Intelligence', *Sydney Daily Telegraph*, 15 June 1882, 4, <http://nla.gov.au/nla.news-article238469310>.

¹⁵⁵ NSW LRS, Surrender and Confirmation Bk 175 No 686; cf. Conveyance Bk 579 No 567. 'Advertising', *Sydney Morning Herald*, 22 September 1888, 18, <http://nla.gov.au/nla.news-article13697481>.

¹⁵⁶ 'Advertising', *Evening News*, 14 November 1881, 3, <http://nla.gov.au/nla.news-article107223982>.

much later as a tobacconist.¹⁵⁷ It appears to have originally had a supported verandah structure running around the corner of the building (Figure 76).

As part of the same development, in 1880-81, two terraced residences were built immediately adjacent to the shops. They were located at 95 and 97 Union Street (later known as No. 43 and No. 45) and were by 1882 occupied by Matthias Andrews, master mariner (at 95) and Richard Thrussell (at 97), with the (leasehold) owner noted as George Snooks. Both buildings were double storey brick houses with iron roofs and 5 rooms each.¹⁵⁸ The houses appear to have been used as residences and shops, with a bootmaker later noted at 95 Union Street.¹⁵⁹

By 1882, the *Sands* listed a further six new addresses at 103 to 113 Union Street, leaving two vacant lots between the shops and terraces in the west (89 to 97 Union Street) and these houses.¹⁶⁰ The occupant of this was William Elder, who shortly later erected a hall on this parcel which had a frontage of 20 feet.¹⁶¹

The street numbering in the *Sands Directory* and the Assessment Books does not correspond for 1882, and the new houses to the east of the vacant land were listed at 101 to 109 in the *Assessment Books*; Berry's timber shop was now noted as being located at No. 113, while the *Sands* still noted it at No. 117. However, it appears that there were three new double storey brick houses to the east of the vacant land, followed by a double storey brick shop, a single storey timber stable, and another double storey brick house. "5 large roomed houses, balcony, verandah" had been advertised for lease in November 1880, at 17s per week, with the address noted as 103 Union Street.¹⁶²

The brick shop at No. 107 (corresponding numbers in the *Sands* and the *Assessment Book*) was occupied by John Perumall, a hairdresser, while his next-door neighbour to the west was patternmaker John Walker (at 105 in the *Sands*), and joiner William Norris further to the west (at 103 in the *Sands*). Fuel merchant Edward Gray occupied the house and stables at 109 and 111 (107 and 107a in the *Assessment Books*), and a Peter Johnson occupied the house at 113 Union Street (111 in the *Sands*). Gray's stables were later noted as 'pulled down', as was William Norris' house at 101 (103) Union Street, suggesting further alterations to these sites after 1882. Berry's timber shop was the last house before the intersection with Pyrmont Bridge Road, at 113 Union Street (No. 117 in the *Sands*).

2.6.2.2.2 Construction and development of 'Elder's Hall' (1883-1887)

By September 1883, William Elder had constructed a hall on the vacant allotment near the western corner shops.¹⁶³ It was noted for the first time in the 1884 *Sands Directory* and was in subsequent editions listed as 'Elder's Hall' at 99 Union Street. The hall was a popular meeting place for various associations and groups and became "Pyrmont's favourite public space, favoured by Protestants and Catholics, political aspirants, boxers and people keeping fit. This two-storey building, fronted by shops and partly occupied by residents, had a gymnasium and a supper-room. It was – according to the *Australian Star* 26 October 1903 – 'the principal meeting place in Pyrmont'."¹⁶⁴ It underwent various alterations over time – the gymnasium appears to have been a later change – until it was destroyed by fire in 1903.

¹⁵⁷ City of Sydney Archives, *Sands Sydney Directory*, City Directory – Union Street (south side), 1890ff. 1926.

¹⁵⁸ City of Sydney Archives, *Sands Sydney Directory*, City Directory – Union Street (south side), 1882; *Assessment Books*, Denison Ward – Union Street, 1882.

¹⁵⁹ City of Sydney Archives, *Sands Sydney Directory*, City Directory – Union Street (south side), 1886. 1890-1892.

¹⁶⁰ City of Sydney Archives, *Sands Sydney Directory*, City Directory – Union Street (south side), 1882.

¹⁶¹ City of Sydney Archives, *Assessment Books*, Denison Ward – Union Street, 1882.

¹⁶² 'Advertising', *Sydney Morning Herald*, 16 November 1880, 16, <http://nla.gov.au/nla.news-article13473713>.

¹⁶³ 'Advertising', *Evening News*, 24 September 1883, 1, <http://nla.gov.au/nla.news-article108840127>.

¹⁶⁴ 'Elder's Hall: A Neutral Public Place', Pyrmont History Group, accessed 20 January 2022, <https://pyrmonthistory.net.au/elders-hall>.

In 1885, Huntley and Barnard, property auctioneers, offered the leasehold of the buildings on the Union Street frontage for sale, including the butcher shop and dwelling at the corner of Edward and Union Streets (89 Union Street).¹⁶⁵ The adjoining property to the rear, on Edward Street, still occupied by the blacksmith's shop, was also offered for sale, as were "all those 10 Dwellings, together with premises used as a brewery, stores, and public hall, being Nos. 91, 93, 95, 97, 99, 101, 103, 105, 107, 107a, and 113, Union-street, producing a gross rental of £974 per annum",¹⁶⁶ offered for sale as leaseholds of 44 years, unexpired. The sale was to proceed either as a block or in three or more lots.

In 1885 and 1886, William Elder still occupied the blacksmith's shop to the rear of 37 Union Street, as he was listed as the occupant of the first building on Edward Street in the 1886 and 1887 *Sands Directories*.¹⁶⁷ It is unclear whether this was still a single storey timber building, as initially noted in the *Assessment Book* of 1880, or whether it had been altered.¹⁶⁸ The c1886 and c1888 plans in Figure 65 and Figure 66 show this building as one single structure, extending all the way to Elder's Hall, with an open space between the workshop and the buildings on the Union Street frontage.

By 1889, the blacksmith's shop was known as Elder, Erskine and Co., and was listed after Henry Hackman at 50 Edward Street and boilermaker Archibald Ferguson at 52 Edward Street, who had been listed at the same addresses in 1888.¹⁶⁹ Ferguson was also noted at 97 Union Street and adjacent to Elder's Hall in 1888, suggesting that 52 Edward Street was located to the rear of 97 Union Street. However, the 1888 and 1889 entries were the only ones for these rear premises in the *Sands*, even though the building appears to have remained standing well into the 21st century (see Figure 74).

The 1885 advertisement for the sale of the Union Street buildings, published in the *Evening News* of 22 August 1885, mentioned premises used as a brewery.¹⁷⁰ This was the Pyrmont Brewery Co. Ltd's wine and spirit store which potentially occupied the shop fronting the hall at 99 Union Street, or premises immediately adjacent. The *Sands Directory* listed the brewery's store only from 1885 to 1887, always together with the hall. This appears to have been the public store for the nearby Pyrmont Brewery, which had been established in 1880 on the corner of Union and Murray Streets, by J. G. H. Swain and Co..¹⁷¹ Although it was said to have produced "above average" beer, it was only a short-lived enterprise, as the Pyrmont Brewery Company (Ltd) was wound up in February 1887.¹⁷²

The Brewery must have moved out in c1886, because William Elder extended the hall in 1886 and altered the premises for 'Elder's Pyrmont Academy of Music', which opened on 24 December 1887. At the time of the opening, the "building was not quite completed."¹⁷³ It was to contain "a large hall for meetings, &c., in addition to the academy" and was expected to "present a rather attractive appearance."¹⁷⁴ The opening entertainment "was entirely of the negro minstrel and variety order, and comprised ballads, comic songs, songs and dances, horizontal bar performance, and a farce, entitled 'The Rival Waiters'."¹⁷⁵

The 1886 Public Works Department Plan (Figure 68) and the 1888 Metropolitan Detail Series Plan (Figure 69) show the outline of the hall at that time, and of the three double storey dwellings to its

¹⁶⁵ 'Advertising', 22 August 1885.

¹⁶⁶ 'Advertising'.

¹⁶⁷ City of Sydney Archives, *Sands Sydney Directory*, City Directory – Edward Street (east side), 1886-87.

¹⁶⁸ Cf. City of Sydney Archives, *Assessment Books*, Denison Ward – Union Street, 1880.

¹⁶⁹ City of Sydney Archives, *Sands Sydney Directory*, City Directory – Edward Street (east side), 1888-89.

¹⁷⁰ 'Advertising', 22 August 1885.

¹⁷¹ City of Sydney Archives, *Sands Sydney Directory*, City Directory – Union Street (north side), 1882-1887; cf. 'The Pyrmont Brewery', *Sydney Morning Herald*, 5 August 1880, 7, <http://nla.gov.au/nla.news-article13465534>.

¹⁷² 'The Pyrmont Brewery Company (Limited)', *New South Wales Government Gazette*, 25 February 1887, 1355, <http://nla.gov.au/nla.news-article219935997>.

¹⁷³ 'Academy of Music', *Sydney Morning Herald*, 26 December 1887, 6, <http://nla.gov.au/nla.news-article28347584>.

¹⁷⁴ 'Academy of Music'.

¹⁷⁵ 'Academy of Music'.

east, followed by the double storey shop at 107 Union Street. The buildings to the east of this shop, at 109 and 111 Union Street, appear to have been separated by a laneway providing access to the rear. These were later replaced by three storey buildings with a shared party wall. 113 Union Street formed the eastern end of the terrace row, with an unnumbered timber building located on the corner of Union Street and Pyrmont Bridge Road. This was likely Robert E. Berry's old grocer's shop, which had been listed as the last building before Pyrmont Bridge Road in the *Sands* until 1884. The shop was no longer listed after 1884, with 113 Union Street being noted as the last building from 1885 until 1887.

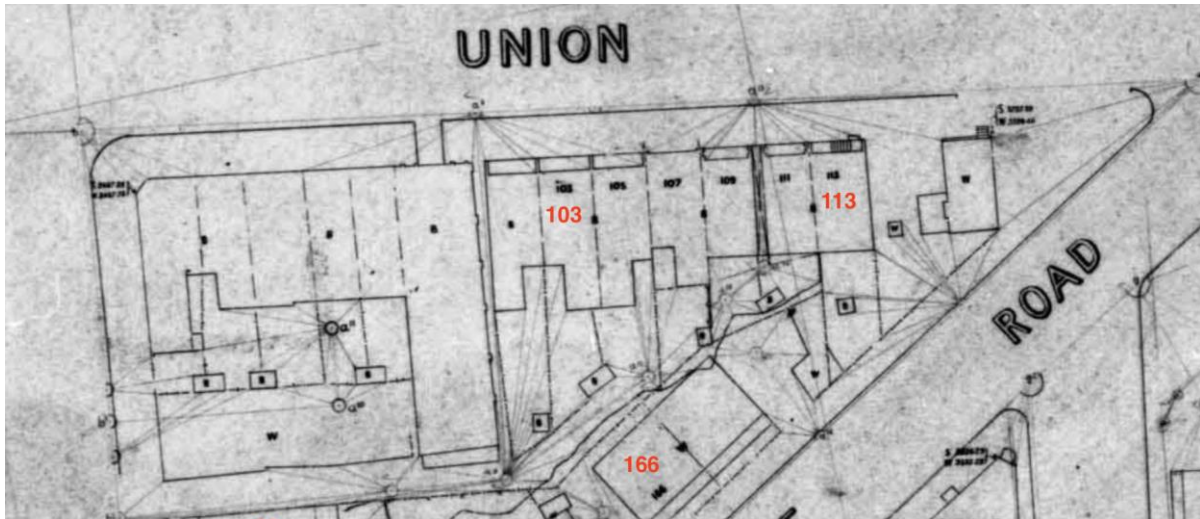


Figure 68: c1886 – Detail from Public Works Department plan for Pyrmont, Sheet K3, submitted to the Surveyor General on 9 October 1886. This shows the buildings on the Union Street frontage, with the outline of 'Elder's Hall' clearly visible on the left portion. (Source: Sydney Water Archives, PWDS1544-S235)

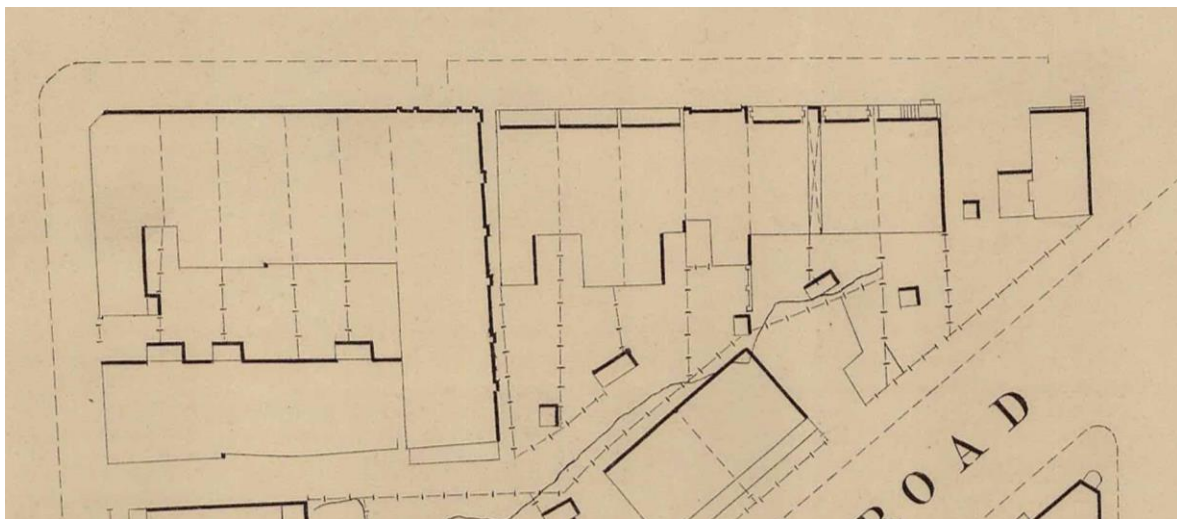


Figure 69: c1888 – Detail from City of Sydney Section (Metropolitan Detail Series), Sheet K3, showing the development on Union Street, largely identical with the plan in Figure 68. This plan was lithographed and printed in May 1888, based on an earlier survey. (Source: State Library of NSW, Z/M Ser 4 811.17/1, <https://collection.sl.nsw.gov.au/record/74VKqv5pXe4b/bBWNZQV5XWRll#viewer>)

2.6.2.2.3 Further development and W. Elders' bankruptcy (1887-1892)

It appears that in c1887, two new buildings were added on the eastern side of the terrace row, even though that is not reflected in the 1888 plan in Figure 69 above. The *Sands* for 1888 noted the

following occupants in the buildings on Union Street, from west to east: Hall, T. W. butcher (No. 89); Ridgewell, John, fruiterer (No. 91); Taplen, Mrs., storekeeper (No. 93); Skues, Mrs. (No. 95); Ferguson, Archibald, blacksmith (No. 97); Elder's hall – William Elder, proprietor (No. 99); Ferguson, Archibald, boiler maker (unnumbered, under No. 99); Davis Mrs., boardinghouse (No. 103); Smith, James (No. 105); Brew, James (No. 109); Fagan, Thomas, boardinghouse (No. 111); Elder William, blacksmith (No. 113); Peromall, John, hairdresser (unnumbered, under No. 113); Fursey, Samuel, bootmaker (No. 115); Irwin, Ellen, grocer (No. 117).¹⁷⁶

The last two addresses, being 115 and 117 Union Street, were new listings, and it appears that it was at this time that two new buildings had been completed to the east of 113 Union Street, replacing the old timber building at No. 117 (this was still noted in the 1888 plan in Figure 69 which was printed in May 1888). While from that time onward, 115 and 117 Union Street appeared on the western side of Pyrmont Bridge Road, confusingly, both numbers had previously been noted on the eastern side of the road, and one of these had also been occupied by Samuel Fursey (although he had previously been listed at No. 117 and was now listed at No. 115). To add to the confusion, the numbering on the eastern side of Pyrmont Bridge Road began with No. 117, leaving two Nos. 117 on Union Street in 1888. However, the numbering was continuous again from 1889 onwards, with 117 noted on the western side.

William Elder had moved to 113 Union Street in c1883, and the address had been noted as his residence in 1886, when his wife died.¹⁷⁷ Mary Jane Elder was only 29 when she died on 3 April 1886.¹⁷⁸ Elder was listed at the address for the last time in 1888, when he appeared alongside John Peromall, the hairdresser who had previously been located at 107 Union Street.¹⁷⁹ Peromall subsequently occupied the building at 113 Union Street which was from 1890 onwards listed as 61 Union Street, after renumbering of the street.

In 1888, not long after completion of the hall extensions for 'Elder's Pyrmont Academy of Music', there were even grander plans for the hall. By then, William Elder had entered a partnership with Edward John Erskine, trading as Elder, Erskine & Co., Manufacturers of Agricultural Implements, and the company enquired at the City Council about the approximate cost of "laying on the salt water and also the after cost of water through meter", as they had "the intention of making a swimming bath under the hall known as Elder's Academy Union".¹⁸⁰ The letter was dated 18 May 1888 and signed by Erskine, but it appears that the plans for the indoor swimming pool did not go ahead.

The partnership was dissolved on 29 October 1888, likely because Elder had run into financial difficulties, and the business was from that time headed by E. J. Erskine who continued trading under the old name at the same address, which was given as 99 Union Street, placing it with the hall.¹⁸¹ By March 1889, C. P. Stephen had become the proprietor of the Academy, and he opened the new 'West Sydney Skating Rink and Dancing Academy' on St. Patrick's Day, 16 March 1889. The advertisement for the opening noted that "the skating and dancing room has been largely extended, and every convenience made to suit the public generally. The floors have been renewed, the skates are in first-class order. Good instructors in attendance. Commencing, 2 to 5 p.m. end 7.30 to 10.30. Admission to rink, gents 1s 6d, ladies 1s. Admission to dancing, gents 1s 6d, ladies free."¹⁸²

¹⁷⁶ City of Sydney Archives, *Sands Sydney Directory*, City Directory – Union Street (south side), 1888.

¹⁷⁷ 'Family Notices', *Daily Telegraph*, 5 April 1886, 1, <http://nla.gov.au/nla.news-article237242020>.

¹⁷⁸ 'Family Notices', *Sydney Morning Herald*, 5 April 1886, 1, <http://nla.gov.au/nla.news-article13638980>.

¹⁷⁹ City of Sydney Archives, *Sands Sydney Directory*, City Directory – Union Street (south side), 1888.

¹⁸⁰ 'Letter: Elder, Erskine & Co, Importers and Manufacturers of Agricultural Machinery, Electrical & City of Sydney Archives, accessed 20 January 2022, <https://cosaprod.recollect.net.au/nodes/view/1549998>. Elder, Erskine & Co. still occupied the blacksmith's shop to the rear of the Union Street buildings on the corner of Edward Street, see City of Sydney Archives, *Sands Sydney Directory*, City Directory – Edward Street (east side), 1889.

¹⁸¹ 'Advertising', *Sydney Morning Herald*, 31 October 1888, 3, <http://nla.gov.au/nla.news-article13701703>.

¹⁸² 'Advertising', *Evening News*, 18 March 1889, 1, <http://nla.gov.au/nla.news-article108788188>.

In September 1888, the lease for the Union Street buildings had been offered for sale again by Richardson and Wrench, with the advertisement (Figure 70) noting that the overall property, which comprised a 185 feet frontage to Union Street as well as frontage to Pyrmont Bridge Road, contained “4 shops, 6 houses, large hall premises, and skating rink, and blacksmith’s shop, being No. 89 to 105 and 107 to 113.”¹⁸³ The lease term was 50 years from 1 July 1877.

William Elder soon had to declare bankruptcy. It appears that he still held the (sub-)lease for some of these sites by May 1890, as the lease for several houses as well as the Assembly Hall was sold by E. M. Stephen as the official assignee in the bankrupt estate of William Elder.¹⁸⁴

The 1890 sales advertisement for Elder’s bankrupt estate described the land to be sold as two separate parcels, in addition to land at Glebe and Guildford. The first parcel at Pyrmont was for the “Second Mortgage of £1500 (subject to a first charge of £2830) over two dwelling houses, Nos. 51 and 53 Union-street, Assembly Hall adjoining thereto, and two dwelling houses, Nos. 57 and 59 Union-street, erected on land having frontage to Union-street, Pyrmont, of about 98ft. Title, leasehold; 50 years from July, 1877. Ground rent, £ - per annum.”¹⁸⁵

The second parcel was for “Pyrmont. – Land having 15ft. frontage to Union-street, on which is erected business premises of brick, iron roof, containing shop and 4 dwelling rooms, occupied as hairdressig establishment at 30s per week. Title, leasehold; 50 years, from 1877.”¹⁸⁶ This was likely for 61 Union Street, occupied by John Perumall.

The 1891 *Assessment Books* suggest that the leasehold for the buildings at 51 and 53, and 59 Union Street had been sold to a person named “Kurtzer” who also owned the buildings at 43 to 51 Union Street, including the shop and hall at 49 and 49a Union Street. The shop at 55 Union Street (formerly No. 107), occupied by fruiterer J. Ridgewell, was owned by C. Delohery, while the two shops at 57 and 61 (formerly at 109 and 113) Union Street were owned by two different estate agents.

59 to 67 Union Street were all noted as three storey brick and stone shops in 1891, with 67 Union Street being a new number. The lease for 63 to 67 Union Street was owned by A. Streatfield (also noted as ‘Stratfield’), with the occupants being listed as A. Bowan (No. 63), H. Rosenberg (No. 65) and W. G. Furzey (No. 67). However, the numbering in the *Sands* for that period was inconsistent and the next *Assessment Books*, for 1896, noted 63/65 as one address, followed by 67 Union Street. This may suggest that there were still only two building to the east of 61 Union Street (formerly 113 Union Street), i.e. those constructed in c1887.

A photograph dated late 1890s to 1900s (Figure 72) seems to confirm this, suggesting that 67 Union Street was in fact a double storey building, located to the east of four three storey buildings. 63/65 Union Street was by 1896 occupied by Josiah White, grocer, who was listed at 63 and 63/65 in the *Sands* from 1892 onwards. In 1896, White had a new building constructed for himself at the corner of Union Street and Pyrmont Bridge Road, known as 69 Union Street.

¹⁸³ ‘Advertising’, 22 September 1888.

¹⁸⁴ ‘Advertising’, *Sydney Morning Herald*, 3 May 1890, 16, <http://nla.gov.au/nla.news-article13769001>.

¹⁸⁵ ‘Advertising’.

¹⁸⁶ ‘Advertising’.

LARGE RENTAL INCOME.

UNION-STREET, PYRMONT.

An enormous traffic past the property, and considering that on two large enterprises alone the start has been made with expenditure estimated at about £1,000,000, the position in this main road ought to receive attention from
SPECULATORS, INVESTORS, and OTHERS.

185 FEET Frontage UNION-STREET,
corner of Edward-street, also to Pyrmont Bridge-road.

4 SHOPS, 6 HOUSES, LARGE HALL PREMISES, and
SKATING RINK, and BLACKSMITH'S SHOP,
being Nos. 89 to 105 and 107 to 113.
A Splendid Situation, near the City Wharf and Railway.

Estimated Annual RENTAL, £1331 4s.
Ground Rental is only £83 10s per annum.

The Term is 50 years, from 1st July, 1877.

On the completion of the extensive railway works and the numerous private undertakings close by, a much larger and more active trade may be expected to flow to Pyrmont, and the front position of this property must naturally prove attractive and procure enhanced values.

RICHARDSON and WRENCH will sell by public auction, at the Rooms, Pitt-street, on **FRIDAY, 12th October,** at 11 o'clock,
The above good rental property, Pyrmont. (3404)

Figure 70: 22 September 1888 – Sales advertisement for the property at 89-113 Union Street, Pyrmont (Source: Advertising, Sydney Morning Herald, 22 September 1888, 18, <https://trove.nla.gov.au/newspaper/article/13697481/1389741>)

2.6.2.2.4 Construction of corner shop at 69 Union Street (1896)

On 14 June 1896, a fire broke out at Josiah White's grocer shop and dwelling at 63 Union Street. The *Daily Telegraph* provided the following account of the building at that time:

*"The building was occupied by Mrs. Josiah White, and owned by Mrs. Streatfield, of Livingstone-road, Marrickville. The front part of the ground floor was the shop, while the back rooms on this floor and the back rooms on the floor above were used as store rooms. The remainder of the first floor and the whole of the second floor were occupied as a dwelling. The fire broke out in the lower storerooms, and this, together with the storerooms above, were gutted, and the contents completely destroyed. The remainder of the premises were damaged by the water used in quenching the conflagration. An empty shop adjoining was also damaged by fire and water. The origin of the fire is unknown. The grocer's shop was insured in the Imperial Insurance Company for £800. The other premises were uninsured."*¹⁸⁷

While his old premises were partly destroyed by fire, White's new premises were being built at the corner of Union Street and Pyrmont Bridge Road. Josiah White had bought this corner site in early 1896, and the land had officially been transferred to him on 28 May 1896.¹⁸⁸ By the end of August 1896, the new three storey building with basement was completed, with the *Sunday Times* providing the following description:

¹⁸⁷ 'Fires', *Daily Telegraph*, 15 June 1896, 6, <http://nla.gov.au/nla.news-article238593602>.

¹⁸⁸ NSW LRS, Primary Application 12754.

“Mr. Sam Hordern very generously presented the residents of Pyrmont last week with a handsome drinking fountain, which is undoubtedly an improvement to Union-street, and an equally striking addition to that portion of the borough is the building just completed for Mr. Josiah White, shipping and family grocer, at the corner of Union-street and Pyrmont Bridge-road. The peculiar ground formation at that corner required all the architect’s skill to utilise space and give ample room for Mr. White’s increasing business. This has, however, been successfully accomplished. The building, which was opened for business yesterday, is four stories high, with ample basement room for storage. A spiral staircase ascends to the upper floors, and a lift from basement to top floor has been conveniently placed. Taken altogether, a prominent and imposing structure meets the view from Pyrmont Bridge, and Mr. J. White’s new outlay should be rewarded by increased business.”¹⁸⁹

Josiah White used a depiction of the building in advertisements for his new premises, such as the one included in Figure 71 below. The building is also shown in Figure 72 and Figure 73. In September 1897, White was involved in a court case, defending himself against accusations by his former landlord, A. Streatfield, who claimed that White was owing 16 weeks rental for the premises at 63 and 65 Union Street. White pleaded that “the plaintiff did not let him the property on the terms alleged, and that the premises were surrendered before the said rent became due.”¹⁹⁰

Development in this portion of the Eastern Construction Site may have included additions to the rear of 61, 63-65 and 67 Union Street. From 1899, the *Sands Directory* intermittently listed occupants at 2, 4 and 6 Pyrmont Bridge Road, immediately adjacent to White’s corner shop.¹⁹¹ However, it was not until 1924 that 2 Pyrmont Bridge Road was for the first time listed as a separate building in the *Assessment Books*, described as a double storey brick building with cellar.¹⁹² A double storey building in this location is visible in Figure 73. In 1930, the *Assessment Books* noted 6 Pyrmont Bridge Road with 61 Union Street and 4 Pyrmont Bridge Road as part of 63/65 Union Street, suggesting that these buildings had access to both, Pyrmont Bridge Road and Union Street.¹⁹³

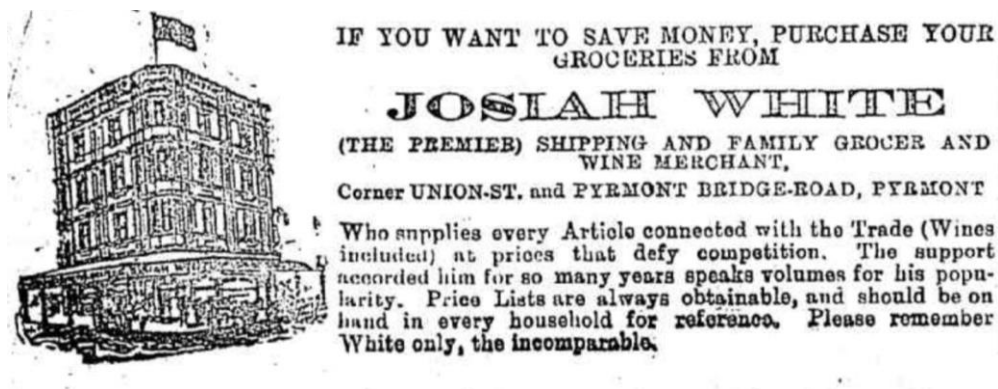


Figure 71: 24 October 1897 – Sales advertisement for Josiah White’s corner shop (Source: Advertising, Sunday Times, 24 October 1897, 11, <https://trove.nla.gov.au/newspaper/article/126249216/13493081>)

¹⁸⁹ ‘No Title’, *Sunday Times*, 30 August 1896, 7, <http://nla.gov.au/nla.news-article130405487>.

¹⁹⁰ ‘No. 2 Jury Court’, *Sydney Morning Herald*, 3 September 1897, 3, <http://nla.gov.au/nla.news-article14118088>.

¹⁹¹ City of Sydney Archives, *Sands Sydney Directory*, City Directory – Pyrmont Bridge Road – North side, for instance, 1899, 1901, 1911.

¹⁹² City of Sydney Archives, *Assessment Books*, Pyrmont Ward – Pyrmont Bridge Road, 1924.

¹⁹³ City of Sydney Archives, *Assessment Books*, Pyrmont Ward – Pyrmont Bridge Road, 1930.



Figure 72: Late 1890s to 1900s – The buildings at 51 to 69 Union Street as outlined in red, with the decorative parapet of the double storey shop at 55 Union Street visible at lower right. Photography by Charles H. Kerry (Source: Powerhouse Collection, Sydney from Pyrmont, 85/1284-1778. <https://collection.maas.museum/object/28399>)



Figure 73: 1 December 1911 – Corner of Union Street and Pyrmont Bridge Road, with the Wine Bar at 69 Union Street on the right and small cottages to the rear (Source: City of Sydney Archives, Pyrmont (01/12/1911), [A-00038966]. <https://archives.cityofsydney.nsw.gov.au/nodes/view/676948>)

2.6.2.2.5 Redevelopment of Elder's Hall (c1905)

In 1899, the *Sands Directory* listed the following occupants of the buildings at 37 to 67 ½ (69) Union Street: No. 37 – Cassidy Joseph, butcher; No. 39 – Newell Henry, J., fish and oyster saloon; No. 41 – Baird Steward, bootmaker; No. 43 – Sheriff William; No. 45 – Gray John; No. 49 – Athenaeum Hall; No. 49a – Higson, W., property agent; No. 51 – Bourke Patrick; No. 53 – Brogan Mrs. Catherine; No. 55 – Greenhalph J. H., ham and beef shop; No. 57 – Gordon Mrs. A.; No. 59 – Cousens William, tailor; No. 61 – McLennan, Alexander V., hairdresser; No. 67 Lobo R. F., fruiterer; No. 67 ½ - White Josiah, grocer.¹⁹⁴ This shows that the shop at 39 Union Street was now in use as a fish and oyster saloon, while 55 Union Street was now a ham and beef shop and 61 Union Street continued to be used as a hairdresser.

Elder's Hall at 49 (also sometimes listed at 49a) Union Street, was now known as 'Athenaeum Hall', having been renamed in around 1895.¹⁹⁵ It continued to be used for various performances and meetings, as well as dancing events.¹⁹⁶

However, in October 1903, a fire destroyed the hall, with the *Australian Star* describing the building and the event that destroyed it, as follows:

*"Elder's Hall, the principal meeting place in Pyrmont, was destroyed by fire about mid-night. The hall was situated in Union-street, and was a two-storied building, fronted by two shops, and also occupied partly as a dwelling. The alarm was sent to the M.F.B., and Deputy-Superintendent Sparks ordered out a big detachment of men, with steamers. After some little time the flames were overcome, the firemen being successful in keeping the outbreak practically confined to the one building, though the roof of No. 45, adjoining, was partly damaged. On the way to the fire the axle of the steamer from Redfern Station broke, the men were thrown off, but not seriously injured, though the steamer was badly wrecked."*¹⁹⁷

Alfred Thomas Barnes, hairdresser, who stated that he had rented the shop at 61 Union Street from Mr. Cassidy for nearly five years, was subsequently charged for setting the hall on fire, and he was subsequently committed for trial.¹⁹⁸ During the trial, Barnes mentioned that the hall contained a gymnasium with adjacent supper-room, with the gymnasium used for card games on Sundays and let for boxing.

The hall was subsequently demolished, and until 1905 the *Sands* did not list any occupants between 41 and 53 Union Street. From 1906 onwards, the numbering was again continuous all the way, suggesting that the hall had been replaced by double storey residences in c1905. The 1907 *Assessment Books* for the Pyrmont Ward noted the following buildings:

Table 9: Occupants of 41-53 Union Street, 1907

No	Occupant	Owner	Type	Material	Roof	Storeys	Rooms
37	Joseph Cassidy	Eden George	Shop & house	Brick	Iron	2	10
39	John Martin	Eden George	Shop & house	Brick	Iron	2	6

¹⁹⁴ City of Sydney Archives, *Sands Sydney Directory*, City Directory – Union Street – South side, 1899.

¹⁹⁵ 'Advertising', *Daily Telegraph*, 28 September 1895, 2, <http://nla.gov.au/nla.news-article238537455>.

¹⁹⁶ 'Advertising', *Evening News*, 19 August 1896, 1, <http://nla.gov.au/nla.news-article111034507>.

¹⁹⁷ 'Fire at Pyrmont', *Australian Star*, 26 October 1903, 3, <http://nla.gov.au/nla.news-article228611179>.

¹⁹⁸ 'Pyrmont Fire', *Australian Star*, 11 November 1903, 6, <http://nla.gov.au/nla.news-article228613191>; 'Pyrmont Fire', *Australian Star*, 12 November 1903, 6, <http://nla.gov.au/nla.news-article228612288>.

No	Occupant	Owner	Type	Material	Roof	Storeys	Rooms
41	Charles Hart	Eden George	Shop & house	Brick	Iron	2	6
43	Dupue	Eden George	House	Brick	Iron	2	6
45	J Cunningham	Eden George	House	Brick	Iron	2	6
47	Lever	Australian Mutual Provident Soc	House	Brick	Iron	2	6
49	W Candish	Australian Mutual Provident Soc	House	Brick	Iron	2	6
49a	Alfred Foys	Australian Mutual Provident Soc	House	Brick	Iron	2	6
51	Mary Leaver	Australian Mutual Provident Soc	House	Brick	Iron	2	5
53	August Newman	Australian Mutual Provident Soc	House	Brick	Iron	2	5
55	William Percy	H Reynolds	Shop & house	Brick	Iron	2	5
57	Margaret Wilson	Australian Mutual Provident Soc	House	Brick	Iron	2	5
59	Carl Nelson	Australian Mutual Provident Soc	House	Brick	Iron	2	5
61	Alfred Barnes	H H King	shop & house	Brick	Iron	2	6
63/65	Alice Rush	H H Streatfield	shop & house	Brick	Iron	2	10
67	Frank Lobo	Adam Wilson	shop & house	Brick	Iron	2	4
69	Edward Keogh	Union Bank of Aust	shop & house	Brick	Iron	2	4

While the 1907 *Assessment Books* wrongly noted all buildings as double storey structures, even though the buildings at 57 to 69 Union Street contained three, the entry shows that the site of the former hall now formed part of a row of five double storey terraced houses. This corresponds with the plan in Figure 74 and the photographs in Figure 75, Figure 77-Figure 78, and Figure 83. Various photographs showing the buildings during the 1910s and 1920s are also included below.

During that time, the occupants of buildings were the following, based on sample editions of the *Sands Directories*:

Table 10: Occupants of 57-69 Union Street, 1911 - 1929

No	1911	1915	1921	1926	1929
37	Young James, butcher	Young Arthur, butcher	Leimasar John, dining rooms	Gibbon Mrs. May, cafe	
39	Swanson Mrs. Susan, newsagent	Swanson, Mrs. Susan, newsagent	Swanson Mrs. Susan, newsagent	Dillon Mrs. S., smallgoods	Jorgensen Christian, smallgoods

Sydney Metro West – Pyrmont Station Construction Sites
Archaeological Research Design and Excavation Methodology

No	1911	1915	1921	1926	1929
41	Callaghan Jack, tobacconist	Callaghan Jack, tobacconist	Callaghan Jack, tobacconist	Callaghan Mrs. J., tobacconist	
43	Shaw Henry, laundry	Shaw Henry, laundry	Cunningham John, bootmaker	McGregor Robert	
45		Shaw Henry, laundry	Abrahams Barnett, outfitter	Rivers Charles	
47	King William	Williams Charles	Balks John J.	Cunningham John	Cunningham John, boot repairer
49	Callaghan Mrs. Jane	Callaghan Mrs. Jane	Hackett John	Hawthorne Eric H.	
49a	Fors Mrs. Maria	Fors Mrs. Maria	Appleby George	Dye James	Williams Mrs. J.
51	Leaver Mrs. Mary	Leaver Mrs. Mary	Wilson Percy	Wilson Percy	Harvie Mrs. M.
53	Martin Alexander	Plummer Charles	Gibbons Miss Caroline	Kermode Mrs. G.	Kermode Mrs. O.
55	Macartney H. B., grocer	Plumber Charles, grocer		Tarrent George F.	
57	Bonnie Mrs. Susan	Alkins Frederick	Carter James	Carter Mrs. Helen	Borg J.
59	Osborne James	Plummer William	Carter Henry		Vaughan Mrs. J.
61	Clarke George, trunk manufacturer	Clarke George, dealer	Perry E., bag and sack dealer	Perry George, sack store	
63/65	Irvine Henry, bootmaker	Maier Miss E., confectioner (63) McCarthy M., bootmaker (65)	Carr James, storekeeper	Carr James, storekeeper	McInerney John, grocer
67	Gregerio Dinario, fruiterer	O'Connor Arthur, hairdresser	Defries W. A., watchmaker		
69	Smart Alfred, Woonona Wine Bar	Clancy J. J., wine bar	Sullivan L., wine bar	Dunn David B., wine bar	Rowley Cecil, wine bar

On 28 June 1913, Sir Matthew Harris, who had by then become the owner of Block 59b, had transferred the site to his son, Arthur Leslie Harris.¹⁹⁹ Despite that, Matthew Harris was still noted as the owner of 37 to 69 Union Street in the 1914 *Assessment Book*.²⁰⁰

On 1 August 1929, J. D. Cowling, builder, applied to Council for consent to demolish the premises at 37/45 Union Street and Edward Street (BA 821/29).²⁰¹ The same applicant also submitted applications for demolition of the eight buildings on the Pyrmont Bridge Road frontage, while the buildings at 47 to 69 Union Street were not part of the site to be demolished at that time.²⁰²

¹⁹⁹ NSW LRS, PA 56103; Conveyance Bk 1007 no 871.

²⁰⁰ City of Sydney Archives, *Assessment Books*, Pyrmont Ward – Union Street, 1914.

²⁰¹ City of Sydney Archives, *Planning Street Cards*, Union Street (1928-1994), Union St 37/45 Edward St.

²⁰² City of Sydney Archives, *Planning Street Cards*, Pyrmont Bridge Road (1928-1994), Cnr Pyrmont Bridge Rd & Edward St.

The buildings at 37 to 45 Union Street appear to have been demolished in early 1930, as the 1930 *Assessment Books* noted the site as vacant land.²⁰³ The buildings at 47 to 69 Union Street remained occupied, however, on 17 October 1930, S. J. Harrison submitted a building application for demolition of the premises at 47 to 55 Union Street (BA 1015/30), followed by an application for a fence on 17 November 1930 (BA 1104/30). A “monster building material sale” of building materials from five demolished houses at 47 to 53 Union Street was held on 16 October 1930.²⁰⁴ The photographs in Figure 83 and Figure 84 show demolition of these buildings underway in October 1930. On 12 December 1930, Harrison also applied for demolition of 57 and 59 Union Street (BA 1172/30), with demolition likely carried out in early 1931, as the 1932 *Sands Directory* no longer listed these buildings.

By 1933, the *Assessment Books* only noted the four buildings at 61, 63-65, 67 and 69 Union Street as extant, as well as the double storey shop at 2 Pyrmont Bridge Road. The wine saloon on the corner was noted as being owned by Caldwell’s Wines Limited in 1933.



Figure 74: Detail from Blackwattle Sheet, prepared by the Board of Water Supply and Sewerage, 1908 (with later updates), showing that the hall has been demolished and replaced with double storey terraces (now five in total, area outlined). The blacksmith’s shop to the rear of 37-45 Union Street was still standing by then. (Source: Sydney Water Archives, BLKWTL 3716)

²⁰³ City of Sydney Archives, *Assessment Books*, Pyrmont Ward – Union Street, 1930.

²⁰⁴ ‘Advertising’, *Sydney Morning Herald*, 11 October 1930, 10, <http://nla.gov.au/nla.news-article16721541>.



Figure 75: 8 September 1916 – Photograph of dwellings at 47-59 Union Street, Pyrmont, with workers working on the parapet. From Demolition Books. In c1905, these terraces replaced the former hall which was once located at 49/49a Union Street, before being gutted by fire in 1903. It is unclear what works were carried out to the terraces at that time, as double storey houses continued to be located at the site until the early 1930s (Source: City of Sydney Archives, Dwellings, Union St (47-59), Pyrmont, 8 September 1916 (08/09/1916), [A-00036813]. <https://archives.cityofsydney.nsw.gov.au/nodes/view/663180>)



Figure 76: c1920s – Looking south from Edward Street, across Union Street, with the verandah structure of the corner building at 37 Union Street just visible on the right. Said to date from 1931, however, the verandah structure was already gone by 1929, see Figure 81 (Source: City of Sydney Archives, Edward Street, Pyrmont (01/01/1931 - 31/12/1931), [A-00011914]. <https://archives.cityofsydney.nsw.gov.au/nodes/view/572559>)

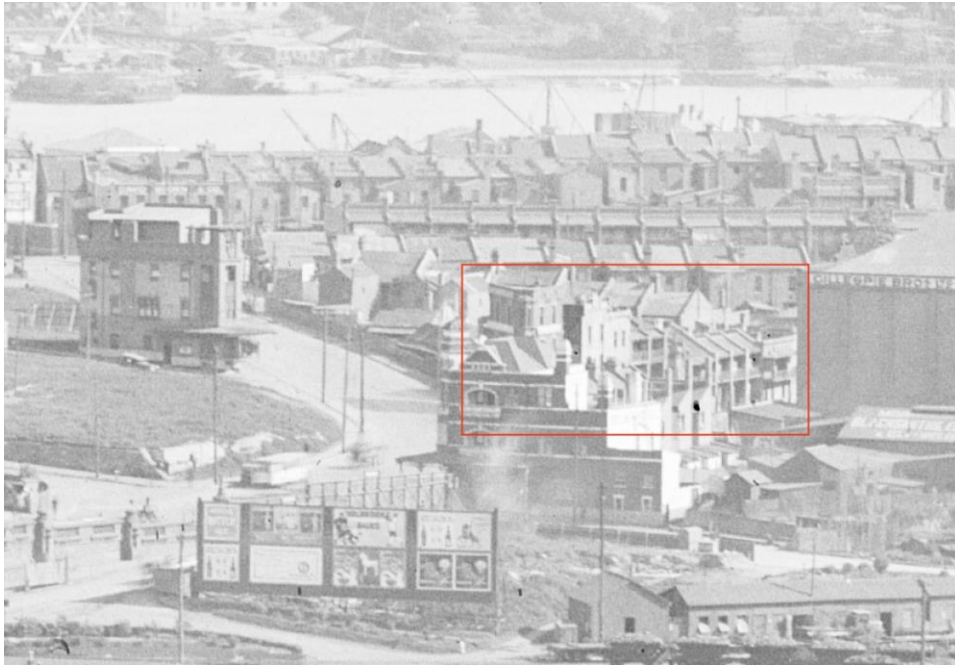


Figure 77: c1920s – Detail from ‘Panorama of Darling Harbour and Pyrmont bridge from the city’, by A. E. Foster, ca 1916-1947, with the Union Street buildings outlined. Gillespie Brothers Flour Mill, on the right, was constructed c1921 (Source: SLNSW, Series 06: Sydney Views, Box 32 No. 360, FL410934. <https://collection.sl.nsw.gov.au/record/nX6OPA8Y/04wINBGeVg0OD#viewer>)

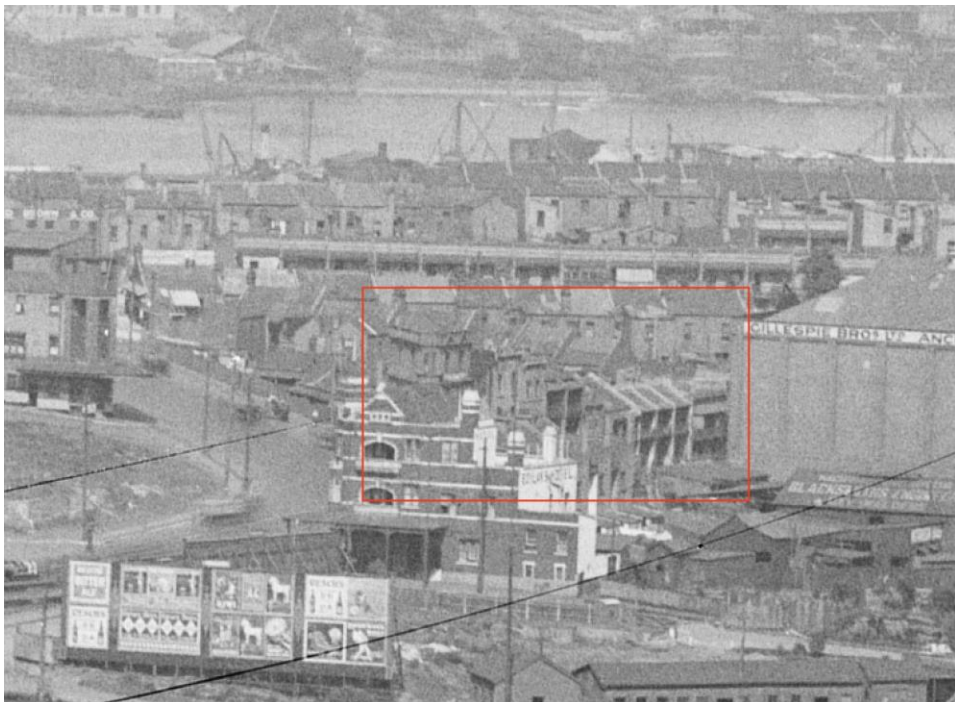


Figure 78: c1920s – Detail from ‘Panorama of Darling Harbour and Pyrmont bridge from the city’, by A. E. Foster, ca 1916-1947, with the Union Street buildings outlined. Gillespie Brothers Flour Mill, on the right, was constructed c1921 (Source: SLNSW, Series 06: Sydney Views, Box 32 No. 359, FL410934. <https://collection.sl.nsw.gov.au/record/nX6OPA8Y/r3RDxyrXP7BZ>)



Figure 79: 1920s – ‘Aerial view of Pyrmont Bridge and wharves’, with the Eastern Construction Site area at centre (Source: NLA, PIC/15611/15348, Fairfax archive, <https://nla.gov.au/nla.obj-163003956>)



Figure 80: c1920s – Aerial photograph by Milton Kent, said to date from 1932 but all buildings are still standing, suggesting a date prior to 1930 (Source: SLNSW, Milton Kent aerial views of Sydney, 1932, FL8812118, <https://archival.sl.nsw.gov.au/Details/archive/110367483>)



Figure 81: 2 August 1929 – The store at the corner of Union and Edward Streets is for sale (Source: City of Sydney Archives, 37-45 Union Street, Pyrmont (02/08/1929), [A-00037606]. <https://archives.cityofsydney.nsw.gov.au/nodes/view/663973>)



Figure 82: 1930 – Detail from aerial view showing the buildings on Union Street shortly prior to demolition. The blacksmith's workshop to the rear of 37 to 45 Union Street is no longer standing (Source: Fairfax Corporation. (1930). Aerial view of Darling Harbour showing Pyrmont Bridge closing following passage of a ship, Sydney, ca. 1930s. <http://nla.gov.au/nla.obj-163204711>)



Figure 83: 20 October 1930 – Houses being demolished at Union Street, near corner of Edward Street. The shop on the left was located at 55 Union Street, however, the numbering of houses to the west (on right) was not always consistent (Source: City of Sydney Archives, Houses being demolished (20/10/1930), [A-00037644]. <https://archives.cityofsydney.nsw.gov.au/nodes/view/664011>)



Figure 84: 20 October 1930 – Shops and houses being demolished at Union Street, near corner of Edward Street. Shop at 55 Union Street bearing date 1881 (Source: City of Sydney Archives, Shop and houses being demolished (20/10/1930), [A-00037643]. <https://archives.cityofsydney.nsw.gov.au/nodes/view/664010>)



Figure 85: 1930 – Aerial view by Milton Kent, showing the site after demolition of the buildings on the corner of Union and Edward Streets. The image is said to have been taken in 1932, however, by then all the buildings had been demolished (Source: SLNSW, Milton Kent aerial views of Sydney, 1932, FL8812121, <https://archival.sl.nsw.gov.au/Details/archive/110367483>)

2.6.2.3 Development of 68-74 Edward Street (c1878-1930)

On the Edward Street frontage of the triangular block, four double-storey timber houses were constructed in c1878, and the *Sands Directory* listed boatbuilder P. O’Sullivan, engineer W. Middleton, schoolmaster S. Pegum and engineer Joseph Rose for the first time on the eastern side of Edward Street in 1879.²⁰⁵

The Assessment Books for 1880 noted the four houses at 68 to 74 Edward Street as double storey timber buildings with iron roofs, each containing five or six rooms.²⁰⁶ Photographs and plans show that 68 and 74 Edward Street were freestanding buildings, while 70-72 Edward Street were semi-detached (Figure 65 -Figure 67 and Figure 85-Figure 87). Outbuildings were located to the rear.

By 1882, the *Assessment Books* noted the (leasehold) owner of all four buildings as Mills & Pile, corresponding with entries for Union Street and Pyrmont Bridge Road (see Sections 2.6.2.2.1 and 2.6.2.4).²⁰⁷ It is likely that the timber buildings were primarily used as residences. While some commercial activity may have been conducted from these sites, occupations of the persons listed at these addresses were only rarely given in the relevant *Sands Directories*. Occupancies changed frequently over time, with the following occupants listed at 68 to 74 Edward Street in the *Sands Directories* from 1883 to 1895 (occupations are noted, where known):

Table 11: Occupants 68-74 Edward Street, 1883-1895

No	Occupant	Year
	Not listed	1883
	Calcraft, George	1884
68	Miller, John, carpenter	1885-1886
	Webb, John	1887-1888
	Brook, Robert, sawyer	1889-1895

²⁰⁵ City of Sydney Archives, *Sands Sydney Directory*, City Directory – Edward Street, Pyrmont – East side, 1879.

²⁰⁶ City of Sydney Archives, *Assessment Books*, Denison Ward – Edward Street, 1880.

²⁰⁷ City of Sydney Archives, *Assessment Books*, Denison Ward – Edward Street, 1882.

No	Occupant	Year
70	Bulgar, George	1883
	O'Sullivan, Peter	1884
	Calcraft, William, stonemason	1885-1887
	Jarvis John	1888-1892
	Johnston Nixon J.	1893-1894
	Anderson Andrew	1895
72	Goursat, Ulysses, engineer	1883-1890
	Not listed	1891-1892
	Claesson, Johan T.	1892-1895
74	James, Henry	1883-1886
	Heaton James	1887-1893
	Potter, Thomas	1894
	Heaton James W.	1895

While the 1891 *Assessment Book* still noted Mills & Pile as the owners of the leasehold, by 1896, the owner was noted as a certain 'Hancock'.²⁰⁸ There appears to have been no change to the buildings, and from the late 1890s, the *Sands* did not list any occupations of persons listed, suggesting that any commercial uses, if present earlier, had ceased by then, and the buildings were primarily used as dwellings.²⁰⁹

In the 1907 edition of the *Assessment Books*, the leasehold owner was given as Cecil W. Darley, who was still noted as the lessee in 1914, when the owner of the four houses was listed as Matthew Harris.²¹⁰ Darley was still the lessee in 1918, when Arthur L. Harris was noted as the owner of the four double storey timber houses with iron roofs.

On 10 September 1929, J. D. Cowling, builder, applied to Council for consent to demolish 8 houses on the corner of Pyrmont Bridge Road and Edward Street (BA 1005/29).²¹¹ This included the four cottages on Pyrmont Bridge Road as well as the four houses on Edward Street. The 1930 *Assessment Book* still noted all four buildings on Edward Street, however, only the house at No. 74 was occupied.²¹² The *Sands Directory* no longer listed the buildings in its 1930 edition and the 1933 *Assessment Book* no longer listed the site, instead referring to it as part of 'land' at 37-59 Union Street.²¹³

On 12 September 1930, Blundell and Brown submitted a building application (BA 911/30) for a fence at the site on the corner of Pyrmont Bridge Road and Edward Street, suggesting that the demolition of the buildings was carried out at around that time and the fencing was required to secure the vacant site from intruders.²¹⁴ Photographs showing the buildings in the 1920s and shortly prior to demolition in 1930 are included in Figure 86-Figure 88 below.

²⁰⁸ City of Sydney Archives, *Assessment Books*, Denison Ward – Edward Street, 1891 and 1896.

²⁰⁹ From around 1899, the *Sands Directory* intermittently listed additional persons immediately to the east of the four houses, at numbers 2 to 6. However, the 1924 *Assessment Books* noted 2 Pyrmont Bridge Road, listing it for the first time, as a double storey brick house with cellar, suggesting that this was part of the corner building on Union Street, see Section 2.6.2.2.4.

²¹⁰ City of Sydney Archives, *Assessment Books*, Pyrmont Ward – Edward Street, 1907 and 1914.

²¹¹ City of Sydney Archives, *Planning Street Cards*, Pyrmont Bridge Road (1928-1994), Cnr Pyrmont Bridge Rd & Edward St.

²¹² City of Sydney Archives, *Assessment Books*, Pyrmont Ward – Edward Street, 1930.

²¹³ City of Sydney Archives, *Assessment Books*, Phillip Ward – Edward Street and Union Street, 1933.

²¹⁴ City of Sydney Archives, *Planning Street Cards*, Pyrmont Bridge Road (1928-1994), Cnr Pyrmont Bridge Rd & Edward St.

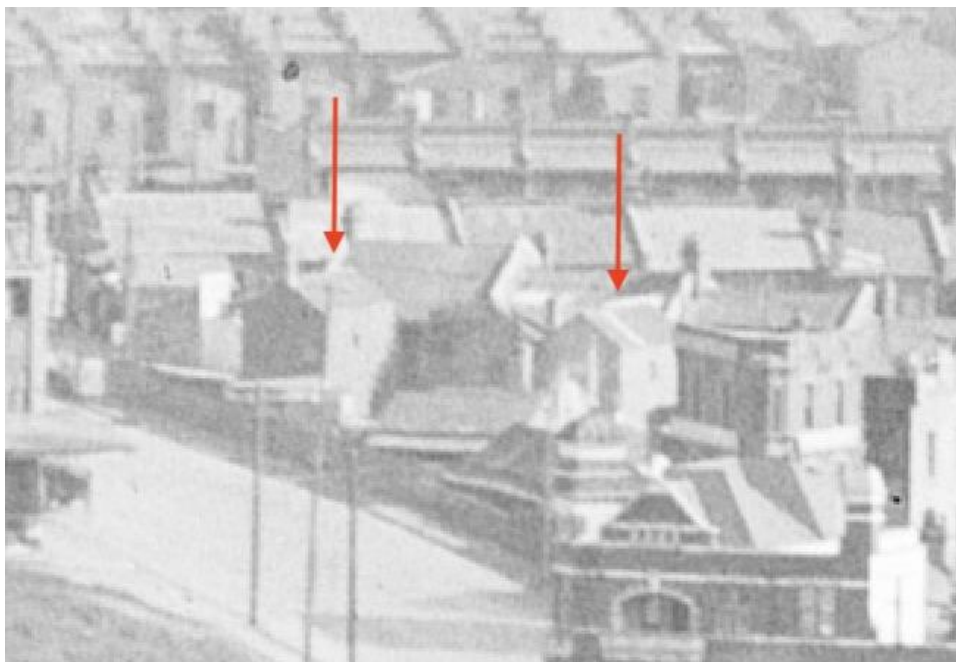


Figure 86: c1920s – Detail from ‘Panorama of Darling Harbour and Pyrmont bridge from the city’, by A. E. Foster, ca 1916-1947, with two of the gabled buildings on Edward Street noted by the arrow. (Source: SLNSW, Series 06: Sydney Views, Box 32 No. 360, FL410934. <https://collection.sl.nsw.gov.au/record/nX6OPA8Y/04wINBGeVg0OD#viewer>)



Figure 87: 2 August 1929 – Corner of Union and Edward Streets, showing buildings on Edward Street on the right (Source: City of Sydney Archives, 37-45 Union Street, Pyrmont (02/08/1929), [A-00037607]. City of Sydney Archives, accessed 17 Jan 2022, <https://archives.cityofsydney.nsw.gov.au/nodes/view/663974>)



Figure 88: 1930 – Detail from aerial view showing the four gabled buildings on Edward Street shortly prior to demolition (Source: Fairfax Corporation. (1930). Aerial view of Darling Harbour showing Pyrmont Bridge closing following passage of a ship, Sydney, ca. 1930s. <http://nla.gov.au/nla.obj-163204711>)

2.6.2.4 Development of 8 to 14 Pyrmont Bridge Road (c1878-1930)

On the Pyrmont Bridge Road frontage of the Eastern Construction Site, four single-storey timber houses were constructed in c1878, and the *Sands Directory* listed constable Thomas Cootes, butcher William H. Steel, master mariner Jacob Condio, and toll collector Richard Peisley as the only occupants on the northern side of Pyrmont Bridge Road in 1879, as the first listing for the street.²¹⁵

The next *Sands* listing in 1880 noted Thomas Coates (note the different spelling) as a drayowner, followed by William Steele (also with different spelling) and Horatio Walpole, who was also a butcher, trading from the corner shop on Union and Edward Streets at that time (see Section 2.6.2.2.1).

The *Assessment Books* for 1880 still noted the four houses as the only four buildings on Pyrmont Bridge Road. Numbered at 158 to 164 Pyrmont Street, starting at Union Street, these were single storey buildings with iron roofs, each containing four rooms.²¹⁶ Plans and photographs suggest that they were in fact two pairs of semi-detached cottages with verandahs (Figure 65 - Figure 67). They were located on irregularly shaped allotments, separated by fences, and an irregular black line running behind the buildings may have denoted the presence of a creek. The (leasehold) owner of the buildings was noted as Bull & Co in the 1880 *Assessment Books*, and the occupants were listed as Thomas Coates (No. 164); H. W. Steel (No. 162); Horatio Walpole (No. 160); and Richard Peisley (No. 158).

By 1881, the numbering had changed and was now increasing from Union Street, from 164 to 170 Pyrmont Bridge Road. While Thomas Coate (different spelling again), van proprietor, was still located at No. 164, James Heaton now occupied No. 166 to the west, and William Duval, master mariner, occupied No. 168, while No. 170 was not listed in the 1882 *Sands Directory*.²¹⁷

The 1882 *Assessment Books* noted the (leasehold) owner of all four buildings as Mills & Pile, corresponding with the entries on Union and Edward Streets (see Sections 2.6.2.2.1 and 2.6.2.3).²¹⁸ It

²¹⁵ City of Sydney Archives, *Sands Sydney Directory*, City Directory – Pyrmont Bridge Road – North side, 1879.

²¹⁶ City of Sydney Archives, *Assessment Books*, Denison Ward – Pyrmont Bridge Road, 1880.

²¹⁷ The 1882 *Sands Directory* listed the buildings on the wrong (south) side of the road.

²¹⁸ The old numbering was still used in the 1882 *Assessment Books* for the Denison Ward.

is likely that the four timber buildings at 164 to 170 Pyrmont Bridge Road were primarily used as residences, however, some commercial activity may have been conducted from these sites. Occupancies changed frequently over time and the following persons were listed in the *Sands Directories* from 1883 to 1895, with a change of numbering occurring in 1889 (occupations are given, where known):

Table 12: Occupants 8-14 Pyrmont Bridge Road, 1883-1895

No (until 1889)	No (from 1890)	Occupant	Year
164	8	Elder William, blacksmith	1883
		Finn, John	1884-1885
		Burns, Patrick, van proprietor	1886-1888
		Burlew, Charles	1889
		Smith, David	1890-1892
		Potter, Thomas	1893
		Heaton, James	1894
		Taylor, James	1895
166	10	Eaton, John and Turner, William, engineer	1883
		Heaton, James, carter	1884-1886
		Johnston, Nixon, carter	1887
		Kelly, Joseph	1888
		Nicholas, Henry, woodcarver	1889-1894
		Reid, George	1895
168	12	Loventure, - and Reid, George, boatbuilder	1883
		Johnston, N., [carter]	1884-1886
		Lewis, Jack	1887-1892
		Jack, Mrs. A.	1893-1894
		Johnson Charles	1895
170	14	Phillips, Stephen J., commission agent	1884-1891
		Miller, William	1892-1895

While the 1891 *Assessment Book* still noted Mills & Pile as the owners of the leasehold, by 1896, the Australian Joint Stock Bank had taken over the lease.²¹⁹ There appears to have been no change to the buildings, and from the late 1890s, the *Sands* did not list any occupations of persons listed, suggesting that any commercial uses, if present earlier, had ceased by then, and the buildings were primarily used as dwellings.²²⁰

In the 1907 edition of the *Assessment Books*, the leasehold owner was given as Cecil Darley, who was still noted as the lessee in 1911 and 1914, when the owner of the four houses was listed as Matthew Harris.²²¹ By 1918, the lessees were George and Rebecca Morris, with Arthur L. Harris noted as the owner of the four cottages.

On 10 September 1929, J. D. Cowling, builder, applied to Council for consent to demolish 8 houses on the corner of Pyrmont Bridge Road and Edward Street (BA 1005/29).²²² This included the four cottages on Pyrmont Bridge Road as well as the four houses on Edward Street. The 1930

²¹⁹ City of Sydney Archives, *Assessment Books*, Denison Ward – Pyrmont Bridge Road, 1891 and 1896.

²²⁰ From around 1899, the *Sands Directory* intermittently listed additional persons immediately to the east of the four houses, at numbers 2 to 6. However, the 1924 *Assessment Books* noted 2 Pyrmont Bridge Road, listing it for the first time, as a double storey brick house with cellar, suggesting that this was part of the corner building on Union Street, see Section 2.6.2.2.4.

²²¹ City of Sydney Archives, *Assessment Books*, Pyrmont Ward – Pyrmont Bridge Road, 1907, 1911 and 1914.

²²² City of Sydney Archives, *Planning Street Cards*, Pyrmont Bridge Road (1928-1994), Cnr Pyrmont Bridge Rd & Edward St.

Assessment Book still noted all four buildings on Pyrmont Bridge Road, with those at No. 8 and No. 10 occupied, however, the *Sands Directory* no longer listed the buildings in its 1931 edition and the 1933 *Assessment Book* no longer listed the site, instead referring to it as part of 'land' at 37-59 Union Street.²²³

On 12 September 1930, Blundell and Brown submitted a building application (BA 911/30) for a fence at the site on the corner of Pyrmont Bridge Road and Edward Street, suggesting that the demolition of the buildings was carried out at around that time and the fencing was required to secure the vacant site from intruders.²²⁴ Photographs showing the buildings in the 1920s and shortly prior to demolition in 1930 are included in Figure 89 and Figure 90 below.

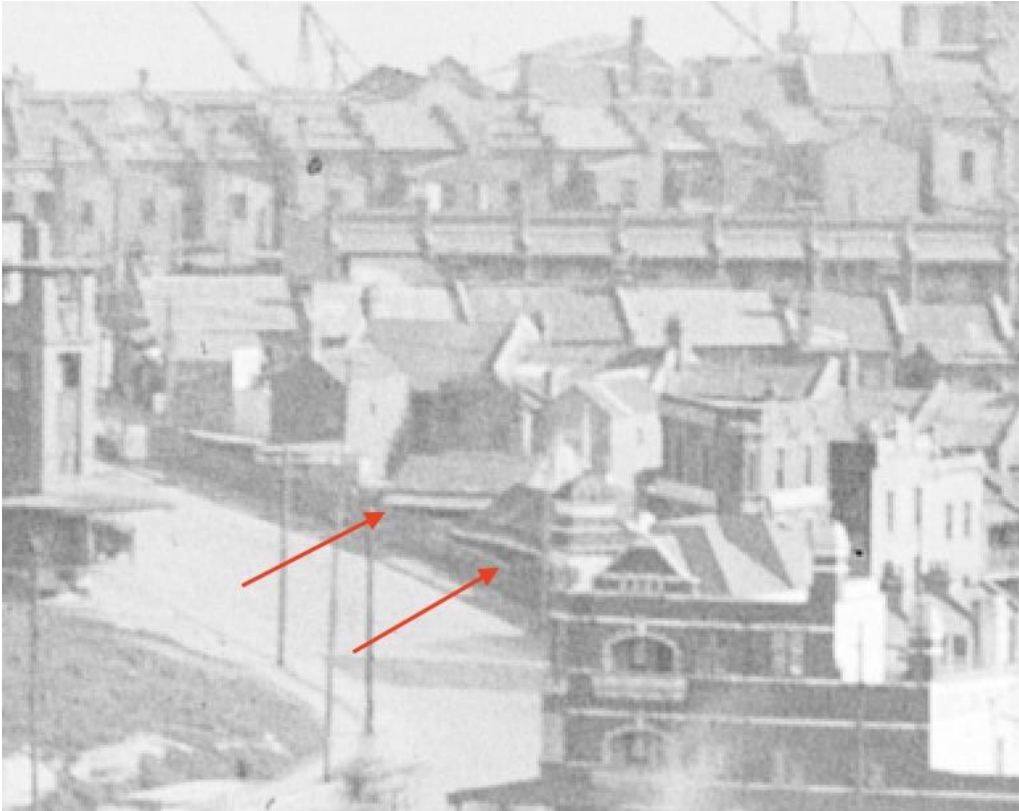


Figure 89: 1920s – Detail from 'Panorama of Darling Harbour and Pyrmont bridge from the city', by A. E. Foster, ca 1916-1947, with the buildings at 8 to 14 Pyrmont Bridge Road indicated by the arrow (Source: SLNSW, Series 06: Sydney Views, Box 32 No. 360, FL410934. <https://collection.sl.nsw.gov.au/record/nX6OPA8Y/04wINBGeVg0OD#viewer>)

²²³ City of Sydney Archives, *Assessment Books*, Pyrmont Ward – Pyrmont Bridge Road, 1930; Phillip Ward – Union Street and Pyrmont Bridge Road, 1933.

²²⁴ City of Sydney Archives, *Planning Street Cards*, Pyrmont Bridge Road (1928-1994), Cnr Pyrmont Bridge Rd & Edward St.



Figure 90: 1930 – Detail from aerial view showing the single storey semi-detached cottages on Pyrmont Bridge Road, indicated by the red arrows, shortly prior to demolition (Source: Fairfax Corporation. (1930). Aerial view of Darling Harbour showing Pyrmont Bridge closing following passage of a ship, Sydney, ca. 1930s. <http://nla.gov.au/nla.obj-163204711>)

2.6.3 Phase 3 (1930-1986) – Timber and builders’ yard, car park and service station

2.6.3.1 Development of 37-61 Union Street as timber and builders’ yard (1930-1954)

After the buildings at 37 to 59 Union Street, and the houses on Edward Street and Pyrmont Bridge Road had been demolished by 1931, that portion of the site became a timber yard. On 5 October 1932, a building application was submitted to erect fence hoarding around the site (BA 540/32).²²⁵ While in 1933 the site was unoccupied, the 1936 and 1939 *Assessment Books* noted H. C. Corfe, or Herbert Corfe, as the occupant of the land and office at 37-61 Union Street.²²⁶ It is possible that Corfe used the existing three-storey building at 61 Union Street as his office, while using the vacant land to store timber. This timber yard use is noted on the plans included in Figure 93-Figure 94 and Figure 97.

By 1945, Corfe was no longer listed, and in 1947, United Distillers Pty Ltd submitted an interim development application for a three or four storey factory at the site, for their bottling department (IDA 75/1947).²²⁷ At that time, in June 1947, the site was noted as being mainly used as a storage yard and workshop by Hornibrook McKenzie & Clark, contractors for the New Pyrmont Power Station. They were also said to occupy a corrugated iron and timber workshop on the corner of Union and Edward Streets, likely erected on the site during the 1930s. Although the interim development application was approved, construction of the new factory building did not go ahead. The corrugated iron and timber workshop in the north-western corner is shown in the 1949 aerial in Figure 98. By 1949, another building had been constructed on the Pyrmont Bridge Road frontage, likely used as a workshop or storage shed. It is shown in the plan in Figure 97 and in the 1949 aerial in Figure 98.

²²⁵ City of Sydney Archives, *Planning Street Cards*, Pyrmont Bridge Road (1928-1994), Pyrmont Bridge Rd, 65/69 Union St & Edward St.

²²⁶ City of Sydney Archives, *Assessment Books*, Phillip Ward – Union Street, 1933, 1936 and 1939.

²²⁷ City of Sydney Archives, IDA 75/1947, ‘Proposed Factory Building, Pyrmont Bridge Rd, Union St & Edward St for United Distillers Pty Ltd. Cf. City of Sydney Archives, *Assessment Books*, Phillip Ward – Union Street, 1945.

By 1948, Herbert Corfe was noted as the occupant of the site again, however, two years later, on 19 May 1950, another interim development application (IDA 349/50) was submitted for the property.²²⁸ The application was for a proposed cellar and macaroni factory and offices for M. Fiorelli Pty Ltd. The application form noted “a few old timber sheds and 2 very old houses unfit for habitation” on the otherwise vacant site which excluded the corner shop at 69 Union Street. One of the two remaining (three storey) buildings on Union Street was said to be four storeys, which suggests that it may have contained a basement/cellar. The ground floor shops of this four-storey part were said to be disused, while the remaining portion was used for residential purposes. A sketch plan identifying these buildings is included in Figure 99.

A formal lease agreement between Arthur Leslie Harris and Fiorelli Pty Ltd for a lease term of 99 years was registered on 1 March 1951.²²⁹ A sketch plan showing the footprint of the proposed new factory is shown in Figure 100. Although approval was granted on 13 June 1950, the work did not go ahead. Instead, the building inspector noted on 23 January 1951 that the vacant land was used by James Wallace & Sons, builders, in connection with the construction of the new power station nearby, and the situation was unchanged in August 1951. In February 1953, a development application was submitted for temporary sheds and builders’ offices in timber yard, which was again used by Hornibrook McKenzie Clark Pty Ltd who had a major contract for the Pyrmont ‘B’ Power Station.²³⁰ This suggests that the site continued to be used as a builders’ yard and for storage of machinery and materials at least until the site was subdivided in 1954.



Figure 91: 1932 – Aerial view by Milton Kent, showing the subject block at centre largely vacant and surrounded by fencing and hoarding, with three buildings remaining standing on the corner of Union Street and Pyrmont Bridge Road (Source: SLNSW, Milton Kent aerial views of Sydney, 1932, FL8812119, <https://archival.sl.nsw.gov.au/Details/archive/110367483>)

²²⁸ City of Sydney Archives, IDA 349/50 Correspondence File, ‘Cnr Bridge Rd., Union St & Edward St, Pyrmont – Erection of factory & office building – M. Fiorelli Pty Ltd – Interim Development Application.’; cf. 2866/50, ‘Premises, Site bounded by Pyrmont Bridge Rd, Union St & Edward St. M. Fiorelli Pty Ltd – Demolition of two (2) dwellings & sheds & erection of building for use as factory, store & offices’; City of Sydney Archives, *Assessment Books*, Phillip Ward – Union Street, 1948.

²²⁹ NSW LRS, PA 56103; Deed Bk 2165 No 193.

²³⁰ City of Sydney Archives, *Planning Street Cards*, Union Street (1928-1994), 37/61 Union Street & 68/71 Edward St & 14 Pyrmont Bridge Rd. Cf. ‘A Major Building Job: Hornibrook Mackenzie Clark’s Work at Pyrmont “B” Power Station’, *Building, Lighting and Engineering*, 24 February 1954, 46, <http://nla.gov.au/nla.obj-299925342>.



Figure 92: 1932 – Aerial view by Milton Kent, showing the subject block at centre largely vacant and surrounded by hoarding, with three buildings remaining standing on the corner of Union Street and Pyrmont Bridge Road (Source: SLNSW, Milton Kent aerial views of Sydney, 1932, FL8812120, <https://archival.sl.nsw.gov.au/Details/archive/110367483>)

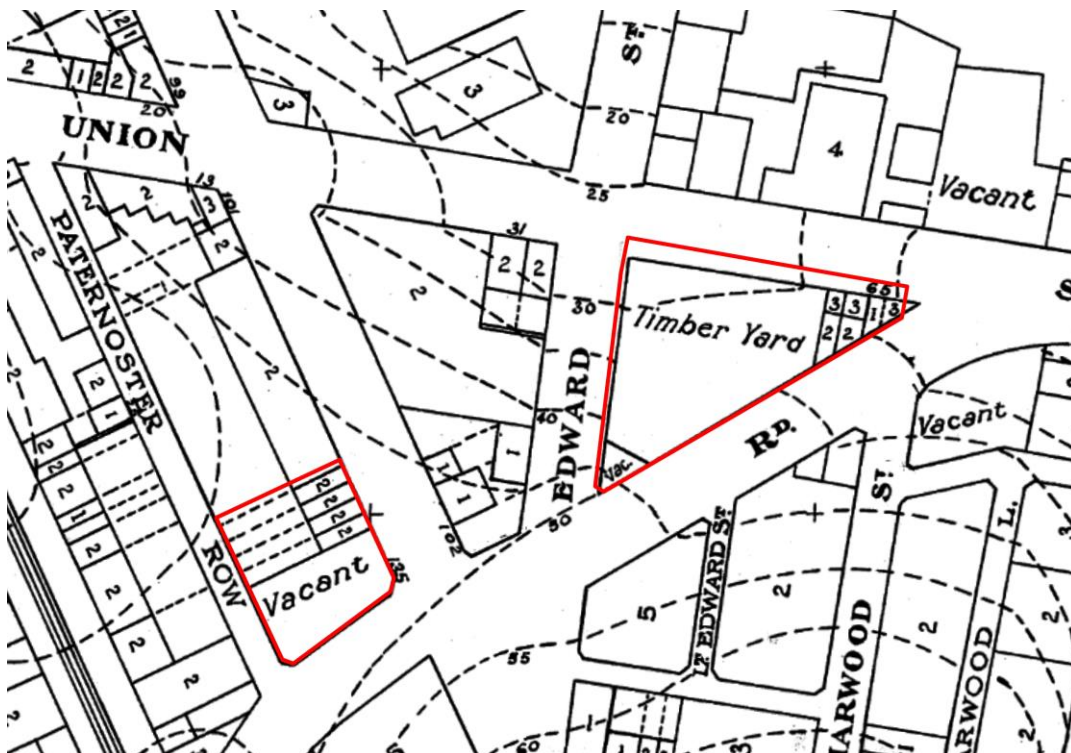


Figure 93: 1938-39 – Detail from City of Sydney Civic Survey, Map 17, with the triangular site noted as a timber yard (Source: City of Sydney Archives, City of Sydney - Civic Survey, 1938-1950: Map 17 – Pyrmont, [A-00880354]. <https://archives.cityofsydney.nsw.gov.au/nodes/view/1709281>)

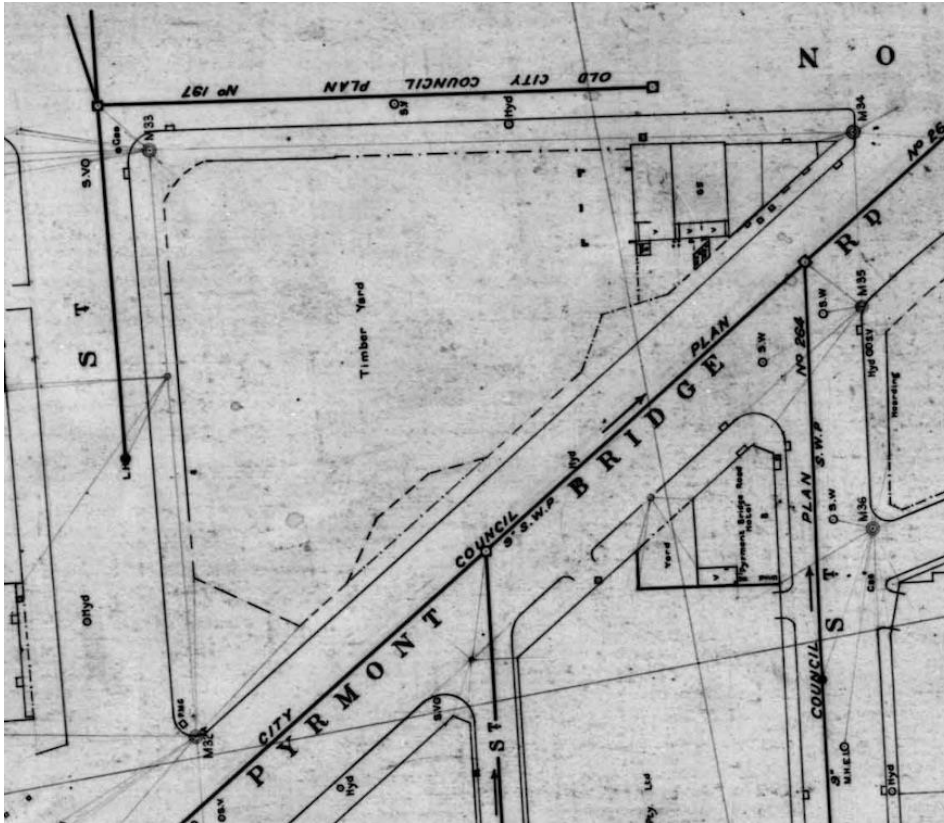


Figure 94: 1940 – Detail from City of Sydney Detail Sheets, K3, based on a survey dated September 1940, with the site noted as a timber yard surrounded by hoarding (Source: Sydney Water Archives, DTS 2674)



Figure 95: circa early 1940s – Streetscape at the corner of Union Street (at front) and Pyrmont Bridge Road, showing existing corner buildings and hoarding (Source: Sydney Living Museums, Caroline Simpson Library & Research Collection, Record No. 31219, <http://collection.hht.net.au/firsthhtpictures/picturerecord.jsp?recno=31219>)

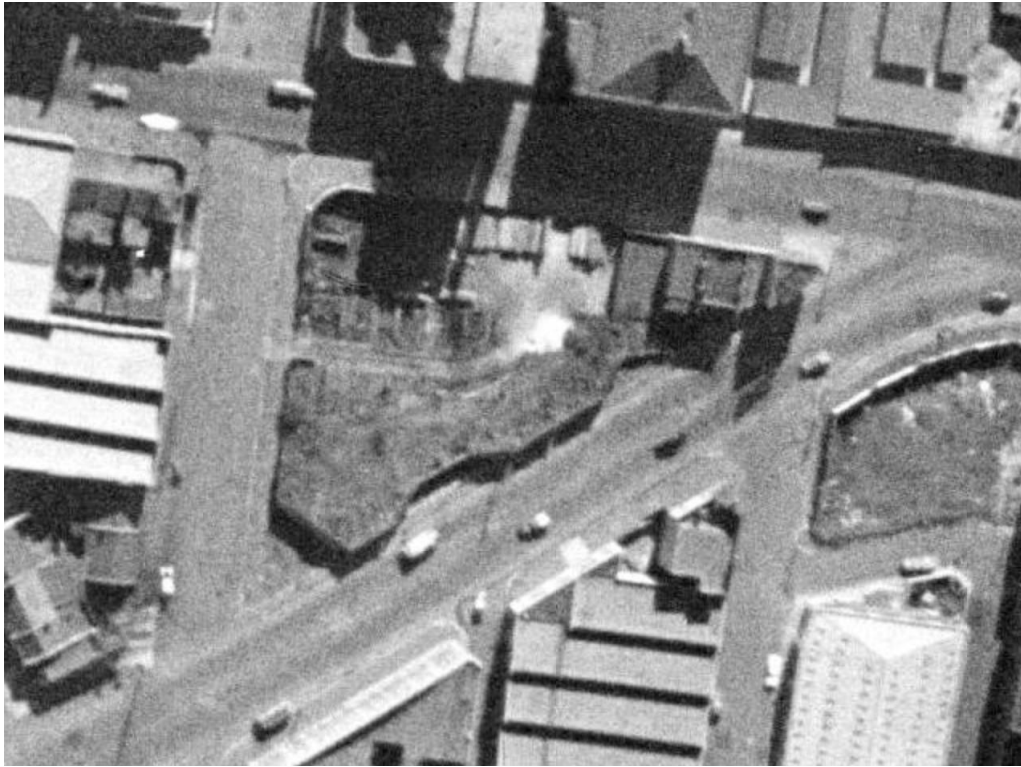


Figure 96: 1943 – Aerial photograph showing the site in use as a timber and storage yard. Note that access appears to have been from Edward Street. Hoarding is in place around the site, with the buildings on the corner of Union Street and Pyrmont Bridge Road still standing (Source: NSW LRS, SIX Maps, 1943 aerial imagery)

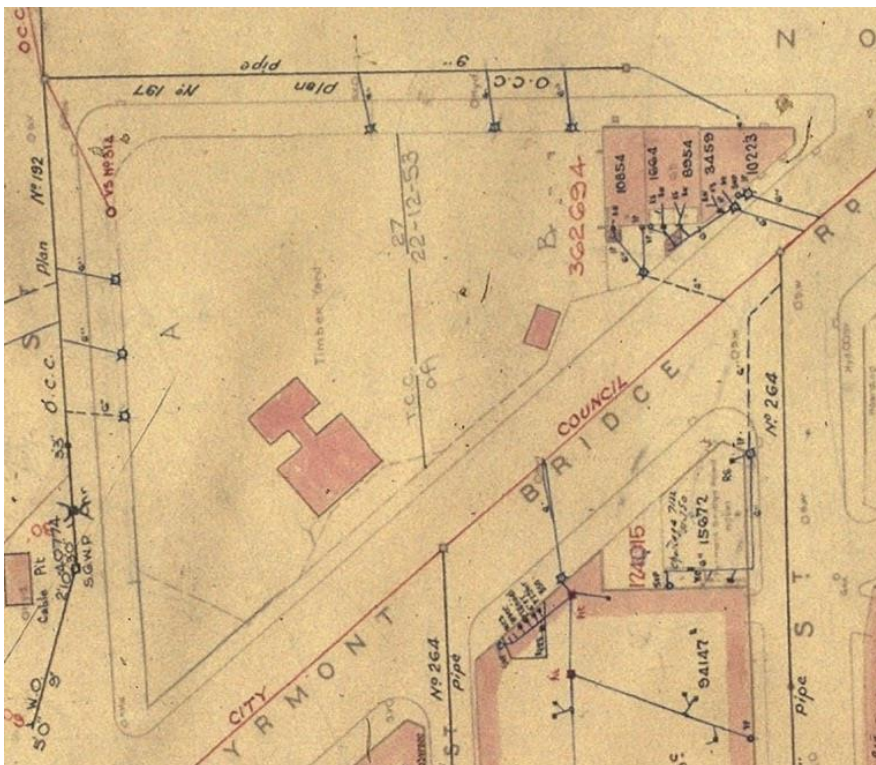


Figure 97: 1940s – MWS & DB survey made in September 1940 and renewed in April 1949, showing the location of the extant buildings on the corner of Union Street and Pyrmont Bridge Road, and the hoarding around the timber yard, with a building noted on the Pyrmont Bridge Road frontage (Source: Sydney Water Archives, BLKWTL 3716-4)

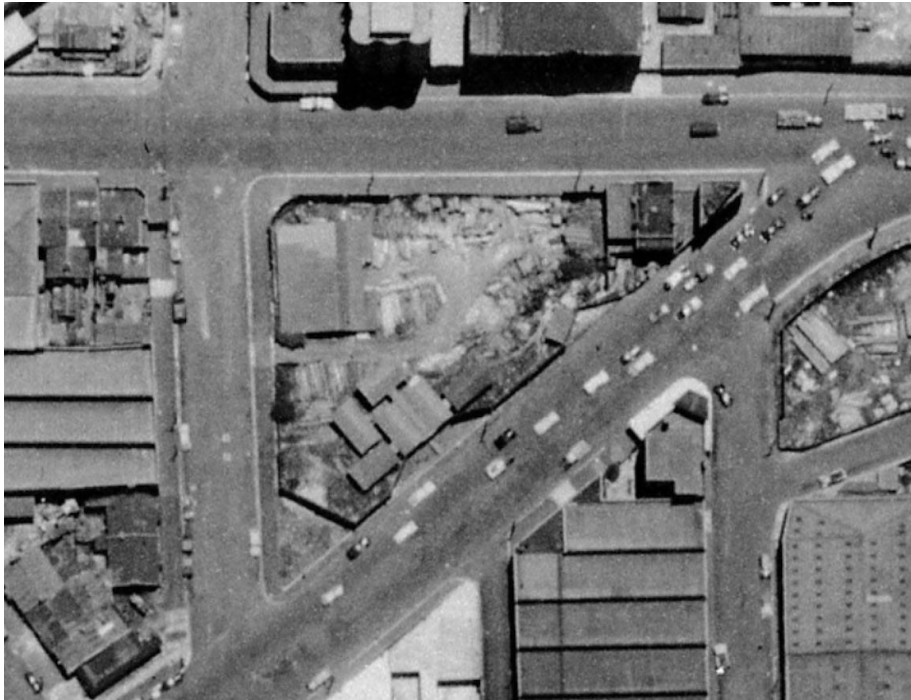


Figure 98: 15 December 1949 – Aerial photograph showing the site with several buildings, including those shown in the 1940s survey above. The building in the upper left (north-western) corner of the site was likely a corrugated iron and timber workshop. Several other, smaller sheds are located at the site (Source: City of Sydney Archives, A-00879983, City of Sydney Aerial Photographic Survey 1949: Image 38)

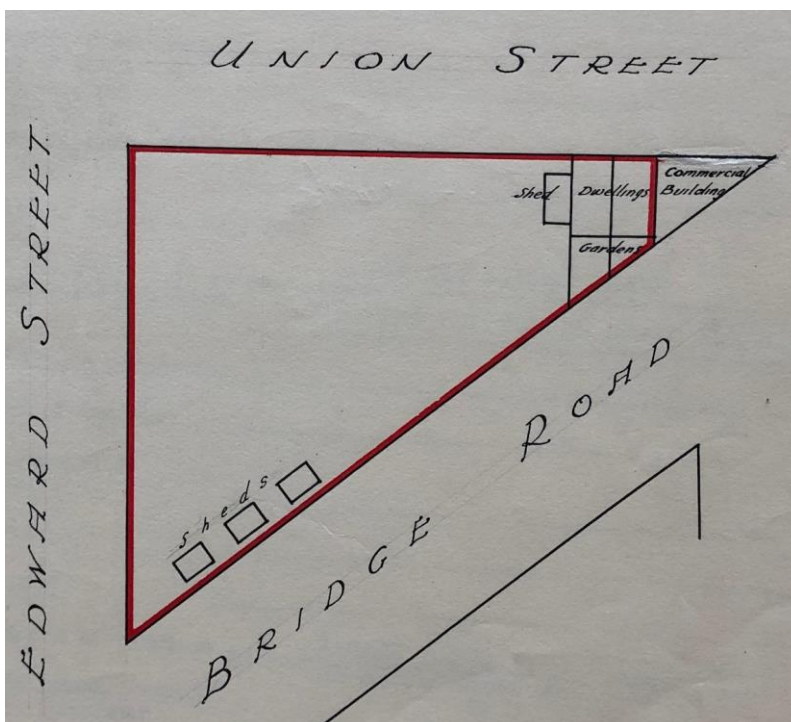


Figure 99: 1950 – Sketch plan noting the existing buildings at the site, including three sheds on the Pyrmont Bridge Road frontage, however, this was likely extremely indicative only (Source: City of Sydney Archives, 2866/50, 'Premises, Site bounded by Pyrmont Bridge Rd, Union St & Edward St. M. Fiorelli Pty Ltd – Demolition of two (2) dwellings & sheds & erection of building for use as factory, store & offices.')

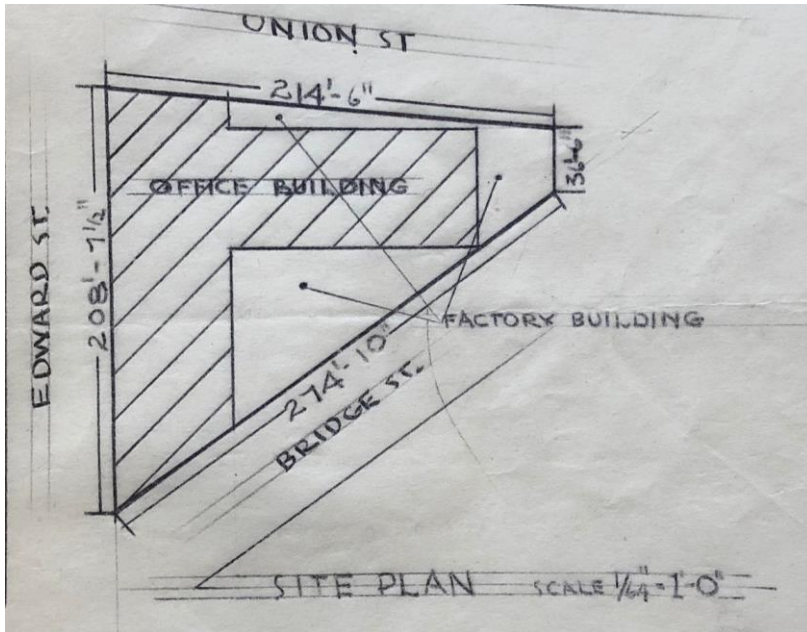


Figure 100: 1950 – Sketch plan for the proposed new factory, submitted by Fiorelly Ltd Ptd as part of IDA 349/50. This development did not go ahead (Source: City of Sydney Archives, IDA 349/50 Correspondence File, 'Cnr Bridge Rd., Union St & Edward St, Pyrmont – Erection of factory & office building – M. Fiorelli Pty Ltd – Interim Development Application.')



Figure 101: March 1954 – Photograph of the Pyrmont Power Station, by Max Dupain, showing part of the hoarding at the corner of Union and Edward Streets (Source: SLNSW, FL19484880, <https://collection.sl.nsw.gov.au/record/nvgPBWJ1>)

2.6.3.2 Site subdivision for lease purposes (1953)

On 22 December 1953, a certificate was issued by the Town Clerk for the subdivision of the site owned by Arthur Leslie Harris.²³¹ The subdivision created two allotments, known as Lot A and Lot B. This excluded the site occupied and owned by Caldwell's Wines on the corner of Union Street and

²³¹ NSW LRS, Deed Bk 2396 No 85.

Pyrmont Bridge Road that remained on its own, much smaller allotment. The plan of subdivision by Colwell & Larcombe, registered surveyors, is included in Figure 102.

The subdivision was specifically for lease purposes only. While the overall leasehold remained in the ownership of Fiorelli Pty, a sublease to Vacuum Oil Co Pty Ltd was registered from 1 July 1955, for a term of 30 years.²³² Vacuum Oil Co Pty Ltd acquired 69 Union Street, on the corner of Union Street and Pyrmont Bridge Road, which had been owned by Caldwells Wines Ltd since 1920, on 15 September 1955.²³³ By that time, two development applications for the remainder of the site had been submitted to Council.

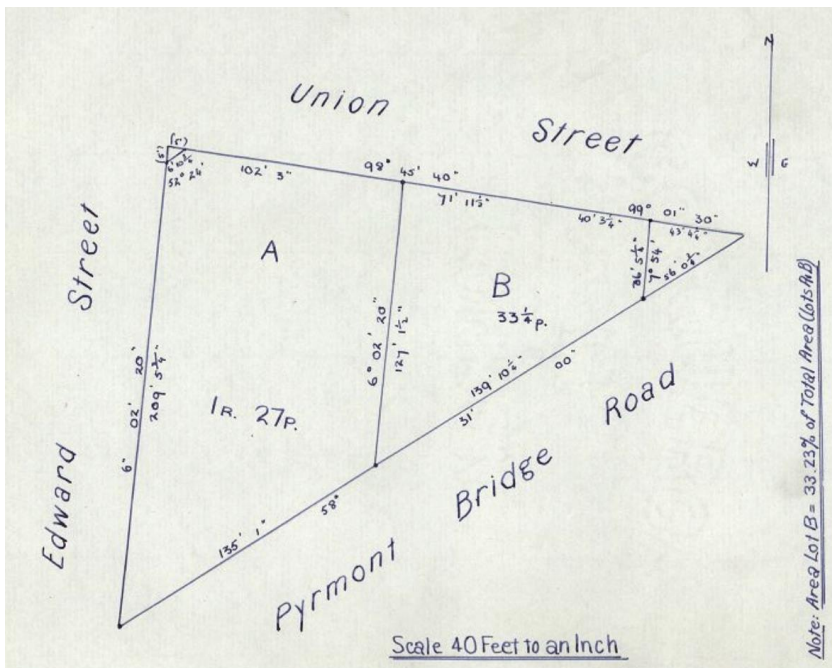


Figure 102: 1953 – Plan showing the subdivision of the site owned by Arthur Leslie Harris into two allotments (A and B), excluding the corner allotment at top right, for lease purposes (Source: NSW LRS, Deed Bk 2396 No 85)

2.6.3.3 Car park, parking station and service station (1954-1986)

On 27 February 1954, Lyell Boyd Pratt, of Randwick, sought permission to use both allotments (A and B) as a car park for privately owned vehicles (DA 189/1954).²³⁴ This was the first development application after the 1953 subdivision. The application form noted that Pratt intended to lease the land and use the vacant site “until such time as owner may require same for building.”²³⁵ No buildings were proposed for the land which was described as having previously been used as a timber yard but was no longer in use. The existing buildings were to be retained as part of that scheme. The application was approved on 3 May 1954, but the site remained unoccupied until at least March 1955. Instead, another development application was submitted for part of the site, for use as a petrol station. With this application, the development of the site known as 61-69 Union Street and Pyrmont Bridge Road and that of the western portion fronting Edward Street became separate.

2.6.3.3.1 Mobil service station at 61-69 Union Street and Pyrmont Bridge Road (1954-1986)

On 15 September 1954, Vacuum Oil Company submitted a development application (DA 804/54) for the erection of a two-level service station at Pyrmont Bridge Road and 65/69 Union Street,

²³² Cf. NSW LRS, PA 56103. Cf. Bk 2396 No 85

²³³ NSW LRS, CT Vol 1485 fol 7.

²³⁴ City of Sydney Archives, DA 189/54, ‘Union St, Pyrmont Bridge Rd & Edward St, Pyrmont. Use land for the purpose of a car park. L. B. Pratt.’

²³⁵ *Ibid.*

Pyrmont.²³⁶ This was to be located on Lot B of the site, including the corner of Union Street and Pyrmont Bridge Road, requiring the demolition of the existing corner buildings.

The plans submitted with the application (Figure 103 and Figure 104) still noted the existing wine store on the corner, proposing a new double storey building with a lubricatorium, store and sales counter, with service islands on both the Union Street and Pyrmont Bridge Road frontages. Each service island was connected to a 3,000-gallon underground tank, with the location of each of these three tanks noted on the plan. The whole site received a bitumen surface, and a new retaining wall was constructed near the ramp leading down from the Pyrmont Bridge Road frontage to Union Street. The plans noted that “retaining walls & filling [were] required to develop site for a two level station.”

By 25 January 1955, the two old buildings on the site had been vacated. While Council initially refused the proposal due to another proposed widening of Pyrmont Bridge Road (which never affected the subject site), approval was granted on 25 July 1955, subject to conditions. A building application (1421/55) was submitted on 9 August 1955 and approved on 19 September 1955.²³⁷ Structural engineers were P.O. Miller and the brick structure with reinforced concrete floors was built by Howe & Bamber. Work on the service station began in November 1955 and was noted as having been completed to approval on 2 July 1956.

After official acquisition of the corner site (69 Union Street) by Vacuum Oil Co Pty Ltd on 15 September 1956, the north-easternmost tip was resumed by the Council of the City of Sydney on 11 May 1956 for the creation of a splay.²³⁸

After construction of the building, further alterations were carried out during subsequent years, including the following applications listed in the City's *Planning Street Cards*:²³⁹

- BA 2168/4, dated 8 September 1964, for structural support for roof signs, by Mobil Oil Australia, estimated at £50;
- BA 2169/64, dated 8 September 1964, for alterations and additions, by Mobil Oil Australia, at £1250;
- DA 189/67, dated 15 March 1967, for proposed erection of a steel canopy to be used in conjunction with existing use, by Mobil (Aust) Ltd;
- BA 292/67, dated 13 December 1967, for canopy by Mobil Oil P/L, at £3,500;
- DA 965/68, dated 18 October 1968, for using the premises as existing and erection of sign by Mobil (Aust) Ltd; and
- BA 1844/69, dated 31 February 1969, for canopy, by Mobil Oil P/L, at £3,500.

²³⁶ City of Sydney Archives, DA 804/54, 'Pyrmont Bridge Road & 65/69 Union Street, Pyrmont. Erection of a two level Service Station. Vacuum Oil Company Pty Ltd.'

²³⁷ City of Sydney Archives, BA 1421/55, 'Cnr Union St & Pyrmont Bridge Rd, Pyrmont, New Building (service station).'

²³⁸ NSW LRS, CT Vol 1485 fol 7.

²³⁹ City of Sydney Archives, *Planning Street Cards*, Union Street (1928-1994), 61/69 Union St, Edward Street.

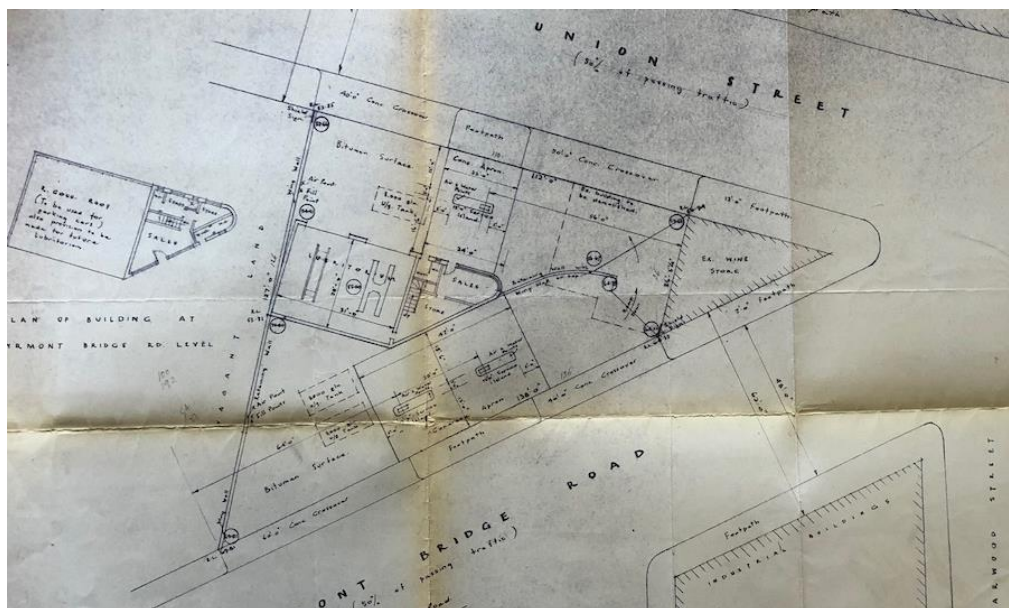


Figure 103: 1954 – Plan submitted as part of DA 804/54, for a new service Station (Source: City of Sydney Archives, DA 804/54, ‘Pyrmont Bridge Road & 65/69 Union Street, Pyrmont. Erection of a two level Service Station. Vacuum Oil Company Pty Ltd.’)

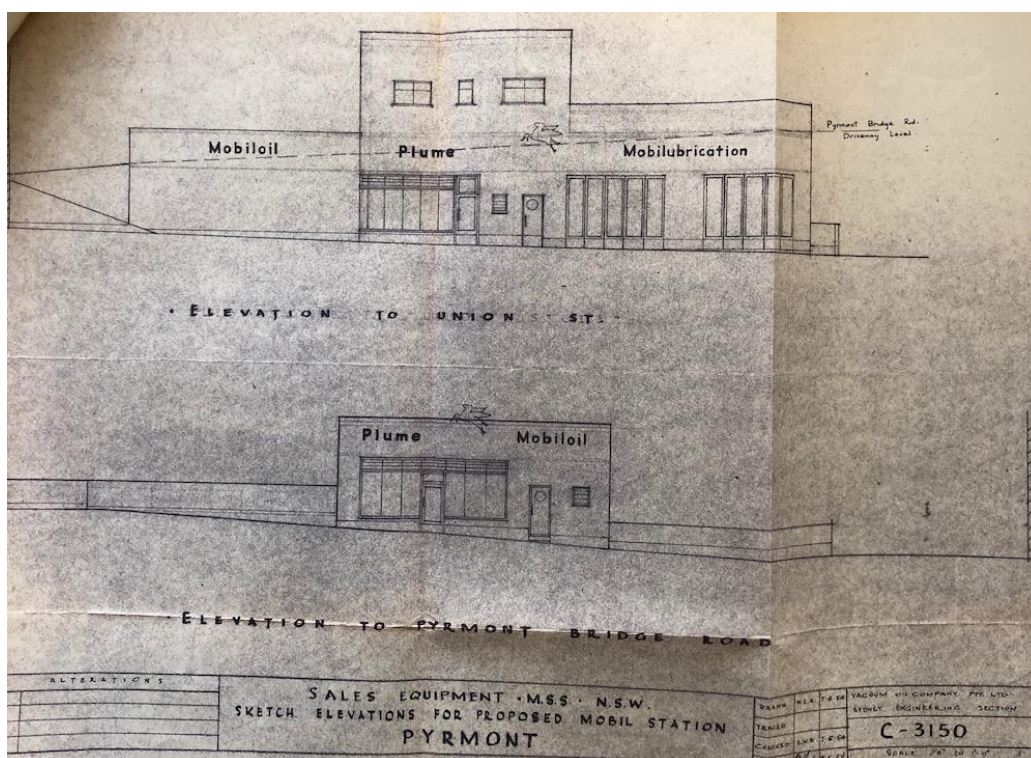


Figure 104: 1954 – Elevations submitted as part of DA 804/54, for a new service Station (Source: City of Sydney Archives, DA 804/54, ‘Pyrmont Bridge Road & 65/69 Union Street, Pyrmont. Erection of a two level Service Station. Vacuum Oil Company Pty Ltd.’)



Figure 105: 1979 – Mobil Service Station, showing elevation to Union Street (Source: Mobil Service Station, Union Street Pyrmont, 1979 (01/01/1979 - 31/12/1979), [A-00008061]. <https://archives.cityofsydney.nsw.gov.au/nodes/view/568706>)

2.6.3.3.2 Car park at 37-59 Union Street, Edward Street and Pyrmont Bridge Road (1956-1969)

The site at the corner of Union and Edward Streets, noted as 37-59 Union Street, Edward Street and Pyrmont Bridge Road, remained unused long after approval had been granted to L. B. Pratt on 3 May 1954 for use of the site as a car park.²⁴⁰

On 27 August 1956, Yellow Express Carriers Ltd, headquartered at nearby 103 Murray Street, Pyrmont, applied to Council to use the land for the parking of trucks and trailers (DA 4309/56).²⁴¹ The company leased the land from M. Fiorelli Pty Ltd, and intended to clear and level the land to give easy access for trucks. This was approved on 17 September 1956.

It appears that Yellow Express Carriers indeed used the land for parking of their vehicles, as this was the current use noted when the next development application was submitted on 3 June 1963 (DA 599/63).²⁴² The new applicant was William Harold Parton, who rented the land from M. Fiorelli Pty Ltd by then. The owner of the site was noted as Arthur L. Harris and the Perpetual Trustee Company. As part of the new use, the land was to be levelled and asphalted, with the private car park to operate from 7am to 7pm daily. Approval was granted on 22 July 1963.²⁴³

However, no work was carried out and the site remained vacant and unused, and on 29 January 1965, the Building Inspector made a note that a 'for sale notice' had been erected on site.²⁴⁴ The development was still pending in July 1968. On 9 October 1968 the Building Inspector noted that

²⁴⁰ City of Sydney Archives, DA 189/54, 'Union St, Pyrmont Bridge Rd & Edward St, Pyrmont. Use land for the purpose of a car park. L. B. Pratt.'

²⁴¹ City of Sydney Archives, DA 4309/56, 'Pyrmont Bridge Rd., 68/74 Edward St. & Union St. Yellow Express Carriers Ltd. To use land for the parking of trucks and trailers.'

²⁴² City of Sydney Archives, DA 599/1963, '37 Union St., 68-74 Edward St., 14 Pyrmont Bridge Rd, Pyrmont. Use vacant land as a private car parking area. William H. Parton.'

²⁴³ City of Sydney Archives, *Planning Street Cards*, Union Street (1928-1994), 37/61 Union Street & 68/75 Edward St & 14 Pyrmont Bridge Rd.

²⁴⁴ City of Sydney Archives, DA 4309/56, 'Pyrmont Bridge Rd., 68/74 Edward St. & Union St. Yellow Express Carriers Ltd. To use land for the parking of trucks and trailers.'

“there were 5 cars parked on subject land at time of inspection. Inquiries to business premises in area revealed that cars are parked on property every day but that no person runs site as a car park.”²⁴⁵

Cars accessed the site via an approved vehicular crossing from Edward Street, and the use of the site as an unsupervised car park by the public continued until July 1969. Cars were mostly parked in the northern portion of the site. On 2 July 1969, John Alexander Draper applied for a Parking Station License, followed by a blasting licence on 6 August 1969, likely for works in connection with the levelling of the site.²⁴⁶ On 18 August 1969, a licence was issued, and the file for DA 599/63 closed.

2.6.3.3.3 Pyrmont Parking Station at 37-59 Union Street, Edward Street and Pyrmont Bridge Road (1969-1986)

It appears that in spring 1969, the site was indeed prepared for use as a paid parking station, known as the ‘Pyrmont Parking Station’. On 24 September 1969, another development application was lodged for the site, to supplement the approved parking station with a car wash and 8 petrol bowsers (DA 655/69).²⁴⁷ The application form noted an existing company office building at the site, described as a single storey steel building with a concrete floor and a square footprint of 9’ 6” x 9’ 6”. It appears that this building had been constructed shortly prior to lodgement of the application, in around August or September 1969, as the files in DA 599/63 had described the site as vacant.

The application form noted that the present use of the site would begin on 25 September 1969, the day after the application was lodged by Pyrmont Parking Station, headquartered in Cammeray.²⁴⁸ The company intended to lease the parking station site subject to the DA being approved. Five new single storey buildings were proposed as part of the car wash, as well as a vehicular turntable and fuel pumps with associated fuel tanks. Although approval was granted, the proposed car wash and fuel stations were never constructed. However, the site was from September 1969 in use as an official, fenced in car parking station.

In October 1969, as part of DA 655/69, the City of Sydney’s Engineer’s Department entered into negotiations with the owner regarding the acquisition of land on the corners of the site, to increase the existing splay on the corner of Edward and Union Streets from 5 to 10 feet and create a new splay of 10 feet on the corner of Edward Street and Pyrmont Bridge Road. This was noted in the *Planning Street Cards* on 6 December 1971, suggesting that the splays were transferred or created around that time.²⁴⁹

Further development proposals, all for the overall site, were submitted during the 1970s and early 1980s, none of which went ahead. They included the approved proposal by James Wallace Pty Ltd for a multi-storey car parking development to provide over 800 car parking spaces, submitted in September 1970 (DA 533/70), and the proposed construction of two residential buildings by Goodway and Southwell, submitted on 12 June 1981 (DA 44-81-0470).²⁵⁰

²⁴⁵ *Ibid.*

²⁴⁶ City of Sydney Archives, *Planning Street Cards*, Union Street (1928-1994), 37/61 Union Street & 68/74 Edward St & 14 Pyrmont Bridge Rd.

²⁴⁷ City of Sydney Archives, DA 655/69, ‘37-61 Union Street & 68-74 Edward Street Pyrmont. Pyrmont Parking Station. Use premises for car parking and washing and also for selling petrol.’

²⁴⁸ The lease was noted in Bk 2396 No 85.

²⁴⁹ City of Sydney Archives, *Planning Street Cards*, Union Street (1928-1994), 37/61 Union Street & 68/74 Edward St & 14 Pyrmont Bridge Rd. Noted as 661/1/71.

²⁵⁰ City of Sydney Archives, *Planning Street Cards*, Union Street (1928-1994), 37/part 61 Union St, 68/74 Edward St & 14 Pyrmont Bridge Rd. Part 61/63/65 Union Street & 2/14 Pyrmont Bridge Rd



Figure 106: 1978 – Aerial photograph showing the Eastern Construction site, containing the Pyrmont Parking Station and Mobil Service Station (Source: NSW Spatial Services, aerial photograph, 6 May 1978, 2713-16-064)

2.6.4 Phase 4 (1986 - present) – Commercial development

In 1982, the triangular site was sold to Hardale Pty Ltd, ending the site's long association with the Harris family.²⁵¹ Hardale had been a lessee at the time of acquisition. On 26 October 1983, Susila Dharma Pty Ltd was registered as the owner, together with Project Sunrise Pty Ltd. Three years later, on 30 June 1986, Copeland Mercantile Ventures Pty Ltd took ownership of the site, and it was under these new owners that the block containing the service station and parking station was redeveloped, resulting in the demolition of all existing structures at the site.

On 23 April 1986, development approval was granted to Cedra Constructions Pty Ltd under DA 44-86-0031 to construct a new building of ground, mezzanine, and two upper levels, for use as eleven light industrial units, with associated office, parking and loading facilities, on the triangular block bounded by Union Street, Edward Street and Pyrmont Bridge Road.²⁵²

This was followed by a building application (BA 86/1056) approved on 11 July 1986.²⁵³ The redevelopment of the site, for Copeland Mercantile Ventures Pty Ltd, involved demolition of all existing structures at the site, including the existing service station. Prior to demolition a photograph was taken of the petrol station, which is included in Figure 107. The ground floor plan and elevations for the redevelopment, designed by Butler & Sky Pty, architects, and approved on 10 April 1987, are included in Figure 108 and Figure 109. The relevant BA file does not contain any site plan showing the location of buildings prior to redevelopment.

After completion of the building, the development underwent various alterations for subsequent commercial tenancies, however, these were largely minor and limited to internal alterations and

²⁵¹ NSW LRS, CT Vol 14782 fol 182.

²⁵² City of Sydney Archives, DA 44-86-0031, '37 - 69 Union St Pyrmont - Erect 4 Storey Industrial Complex Copeland Mercantile Venture Pty Ltd.'

²⁵³ City of Sydney Archives, BA 86/1056, '37 - 61 63 - 65 67 & 69 Union St Pyrmont - Construct New Office Warehouse Building \$1800000 22 05 86 Cedra Construction Pty Ltd (22/05/1986 - 14/02/1992).' BA plans were accessed online in January 2022 at <https://archives.cityofsydney.nsw.gov.au/nodes/view/1720703> (PART 01 [A-00590385]) and <https://archives.cityofsydney.nsw.gov.au/nodes/view/1720704> (PART 02 [A-00590386]).

fitouts.²⁵⁴ On 8 April 1988, the site was transferred again, to Donald Gordon Mackay and Adrian John Lane as joint tenants, with the site being subject to a number of subsequent ownership transfers and various leases.



Figure 107: c1985-86 – Photograph showing the Service Station at centre prior to demolition, with a portion of the fence surrounding the parking station on the right, facing Union Street. (Source: City of Sydney Archives, BA 86/1056, '37 - 61 63 - 65 67 & 69 Union St Pyrmont - Construct New Office Warehouse Building \$1800000 22 05 86 Cedra Construction Pty Ltd (22/05/1986 - 14/02/1992).')

²⁵⁴ As documented in the City of Sydney Archives, *Planning Street Cards*, Union Street (1928-1994), the City of Sydney Archives' online catalogue, and the City of Sydney's online DA Tracker.

Sydney Metro West – Pyrmont Station Construction Sites
Archaeological Research Design and Excavation Methodology

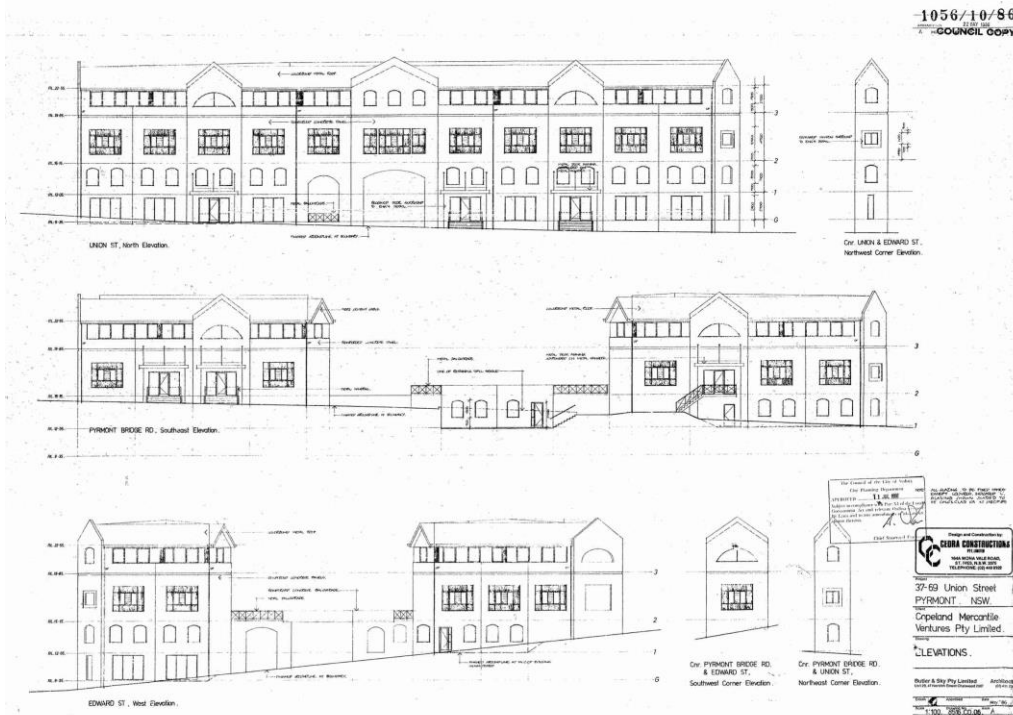


Figure 108: 1986 – Elevations prepared by Butler & Sky Pty Ltd, architects, dated May 1986, for 37-69 Union Street, approved on 11 July 1986. Minor later amendments were also approved (Source: City of Sydney Archives, BA 86/1056, '37 - 61 63 - 65 67 & 69 Union St Pyrmont - Construct New Office Warehouse Building \$1800000 22 05 86 Cedra Construction Pty Ltd (22/05/1986 - 14/02/1992).')

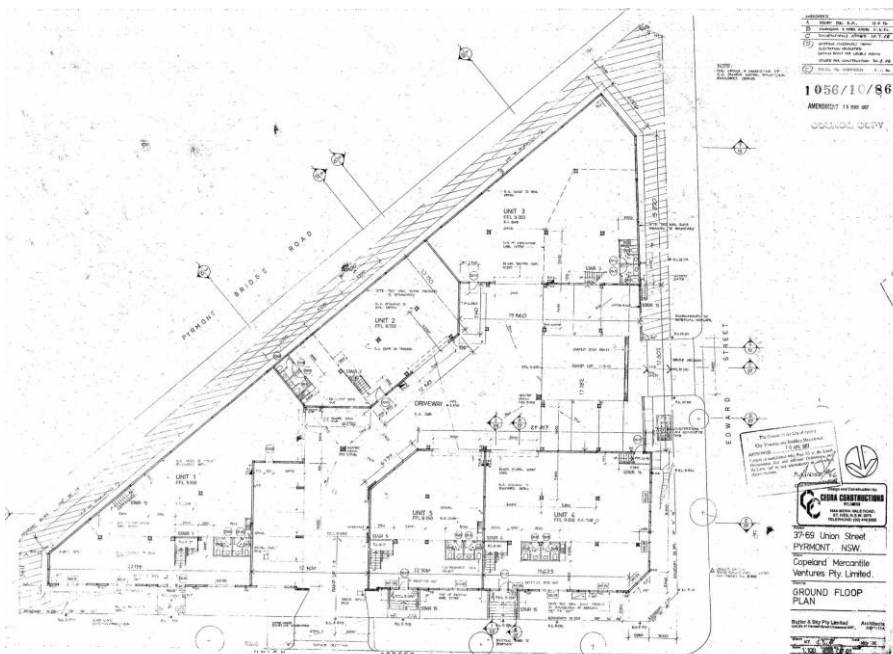


Figure 109: 1986 – Ground floor plan prepared by Butler & Sky Pty Ltd, architects, dated May 1986, for 37-69 Union Street, as amended, approved on 10 April 1987 (Source: City of Sydney Archives, BA 86/1056, '37 - 61 63 - 65 67 & 69 Union St Pyrmont - Construct New Office Warehouse Building \$1800000 22 05 86 Cedra Construction Pty Ltd (22/05/1986 - 14/02/1992).')



Figure 110: August 1986 – Aerial photograph showing the site already cleared (Source: NSW Spatial Services, Historical Aerial Imagery, 3 August 1986, 3528-22-061)



Figure 111: August 1991 – Aerial photograph showing the new buildings completed (Source: NSW Spatial Services, Historical Aerial Imagery, 14 August 1991, 4029-10-133)

3.0 ASSESSMENT OF ARCHAEOLOGICAL POTENTIAL AND SIGNIFICANCE

3.1 Assessment of archaeological potential

The assessment of historical archaeological potential discusses the study area’s potential to contain historical archaeological resources. This assessment is based on consideration of historic land use, current ground conditions, analysis of the historical development of the study area, and considering whether subsequent actions (either natural or human) may have impacted on archaeological evidence for these former land uses.

‘Archaeological potential’ refers to the likelihood that an area contains physical remains associated with an earlier phase of occupation, activity or development of that area. This is distinct from ‘archaeological significance’ and ‘archaeological research potential’. ‘Archaeological significance’ refers to the cultural value of the known items within an area, whilst ‘archaeological research potential’ refers to the cultural value of the potential archaeological remains in an area.

Knowledge of previous archaeological investigations, and an understanding of the potential archaeological remains have been assessed with due consideration of the historic land use and previous ground disturbance that may have impacted any subsurface archaeological remains. This is presented using the grades of archaeological potential provided in Table 13.

Table 13. Grades of archaeological potential

Grading	Justification
Nil	No evidence of historical development or use, or where previous impacts such as deep basement structures would have removed all archaeological potential.
Low	Little or low intensity historical development, or where there have been substantial previous impacts, disturbance and truncation in locations where some archaeological remains such as deep subsurface features (privies, cesspits or wells) may survive.
Moderate	Known historical development and some previous impacts, but it is likely that archaeological remains survive with some localised truncation and disturbance.
High	Evidence of multiple phases of historical development and structures with minimal or localised later development impacts, and it is likely the archaeological resource would be largely intact.

3.2 Research potential and archaeological significance

In 1984, Bickford and Sullivan²⁵⁵ examined the concept and assessment of archaeological research potential; that is, the extent to which archaeological resources can address research questions. They developed three questions which can be used to assess the research potential of an archaeological site:

- Can the site contribute knowledge that no other resource can?
- Can the site contribute knowledge that no other site can?

²⁵⁵ Bickford, A. and S. Sullivan, 1984. “Assessing the research potential of historic sites” in *Sites Surveys and Significance Assessment in Australian Archaeology*.

- Is this knowledge relevant to:
 - General questions about human history?
 - Other substantive questions relating to Australian history?
 - Other major research questions?

In the 2009 guidelines *Assessing Significance for Historical Archaeological Sites and 'Relics'*, the NSW Heritage Division has since provided a broader approach to assessing the archaeological significance of sites, which includes consideration of a site's intactness, rarity, representativeness, and whether many similar sites have already been recorded, as well as other factors. This document acknowledges the difficulty of assessing the significance of potential subsurface remains, because the assessment must rely on predicted rather than known attributes.²⁵⁶ A site can have high potential for archaeological remains, and yet still be of low research potential if those remains are unlikely to provide significant information or make contributions to our current state of knowledge.

In addition, these archaeological guidelines address the potential significance of archaeological remains against the NSW Heritage Assessment Criteria. A discussion of how these criteria apply to assessments of archaeological remains has been extracted from the 2009 guidelines and provided in Table 14.

Table 14. NSW Heritage criteria for assessing significance related to archaeological sites and relics²⁵⁷

Criteria	Discussion
Archaeological research potential (criterion E)	Archaeological research potential is the ability of archaeological evidence, through analysis and interpretation, to provide information about a site that could not be derived from any other source and which contributes to the archaeological significance of that site and its 'relics'.
	The integrity of the site, the state of preservation of archaeological material and deposits will also be relevant.
Associations with individuals, events or groups of historical importance (criteria A, B & D)	Archaeological remains may have particular associations with individuals, groups and events which may transform mundane places or objects into significant items through the association with important historical occurrences.
Aesthetic or technical significance (criterion C)	Whilst the technical value of archaeology is usually considered as 'research potential' aesthetic values are not usually considered to be relevant to archaeological sites. This is often because until a site has been excavated, its actual features and attributes may remain unknown. It is also because aesthetic is often interpreted to mean attractive, as opposed to the broader sense of sensory perception or 'feeling' as expressed in the Burra Charter.
	Nevertheless, archaeological excavations which reveal highly intact and legible remains in the form of aesthetically attractive artefacts, aged and worn fabric and remnant structures, may allow both professionals and the community to connect with the past through tangible physical evidence.

²⁵⁶ NSW Heritage Branch 2009

²⁵⁷ *ibid*

Criteria	Discussion
Ability to demonstrate the past through archaeological remains (criteria A, C, F & G)	<p>Archaeological remains have an ability to demonstrate how a site was used, what processes occurred, how work was undertaken and the scale of an industrial practice or other historic occupation. They can demonstrate the principal characteristics of a place or process that may be rare or common.</p> <p>A site may best demonstrate these aspects at the time of excavation. It may also be possible to explain the nature of the site and demonstrate past practices via public interpretation either before, during, or after excavation.</p>

3.3 Overview of historic context

The history of the construction sites has been broken into discrete historical phases to discuss potential archaeological remains. Within each historic phase specific remains are assessed for their archaeological potential and archaeological significance in accordance with the methodology provided in Section 3.1.

A summary of the historical phases of the two construction sites is outlined in Table 15 and Table 16.

Table 15: Summary of historic phases for western construction site

Phase	Date Range	Use of site
Phase 1	1788-1879	Part of the Ultimo Estate, potentially used for livestock grazing
Phase 2	1879-1914	Residential housing and commercial properties
Phase 3	1914-1947	Resumed for road widening, potential storage area for building material or soil, residential housing and commercial properties
Phase 4	1947-present	Gilbey's Distillery and connected carpark, 'Frankel House' and connected basement carpark

Table 16: Summary of historic phases for eastern construction site

Phase	Date Range	Use of site
Phase 1	1788-1878	Part of the Ultimo Estate, potentially used for livestock grazing
Phase 2	1883-1930	Residential housing, commercial properties, 'Elder's Hall'
Phase 3	1930-1986	Timber and building materials storage yard, and associated sheds and offices, fuel service station, car park
Phase 4	1986-present	Commercial properties

3.4 Previous Archaeological Investigations

Non-Aboriginal archaeological excavations which have been previously conducted in the Pyrmont area, specifically those sites which are either near to the proposed construction sites or that may feature similar archaeological remains as those which are predicted at the Pyrmont Station construction sites, are discussed below. The purpose of this section is to provide an indication of the archaeological potential for the Pyrmont Station construction sites and to explore comparative research design questions which may provide insight into the material culture of the Pyrmont neighbourhood at various phases through the nineteenth and early twentieth centuries.

3.4.1 Pyrmont Peninsula Place Strategy (Godden Mackay Logan, October 2020)

The Pyrmont Peninsula Place Strategy contained a non-Indigenous cultural heritage study prepared by GML²⁵⁸. This study aimed to understand the non-Indigenous heritage of the study area and immediate surrounds and gain an appreciation of the roles and value on non-Indigenous heritage for place identity, cohesion and character today, particularly the role of heritage to help create an authentic, attractive and vibrant place. As part of the study, GML also made recommendations on how matters of non-Indigenous heritage could be considered as part of the ongoing governance of the study area.

As stated with then study, the Pyrmont Peninsula has:

...outstanding heritage significance for its ability to demonstrate human intervention and modification of a visually prominent, distinctive harbour landform. Since colonisation the peninsula has been modified and transformed, it is a significant historic cultural landscape that demonstrates a distinctive evolutionary pattern of large land grants, subdivision, agriculture and industrial development, quarrying, land reclamations, industry, rail lines and wharf construction, urbanisation, technological development, industrial decline, and government-led urban planning, consolidation and renewal writ large. Associated with major shifts in Australia's industrial and political economy, these significant historical shaping forces have resulted in dramatic and significant changes in community and working life expressed through an urban landscape and a complex amalgam of significant public infrastructure, industrial, residential, and commercial development and public space... Pyrmont-Ultimo has substantial potential to yield archaeological information that will contribute to an understanding of NSW's cultural and natural history fabric and Sydney's early development and industrial history, along with buried deposits relating to those activities.

The Place Strategy commended that a detailed, area-wide Archaeological Zoning or Management Plan is required to better identify potential archaeological sites and locations where more area-specific archaeological assessment is warranted within the Pyrmont Peninsula. The Place Strategy also identified the area occupied by the Gilbey's Distillery as an "area of archaeological potential that is partially disturbed or destroyed".

²⁵⁸ Godden Mackay Logan, 2020. *Pyrmont Peninsula Place Strategy – Non-Indigenous Cultural Heritage Study – Final Report*.

3.4.2 26-32 Pyrmont Bridge Road, Pyrmont – Archaeological Research Design (Godden Mackay Logan, 2000)

Godden Mackay and Logan prepared an Archaeological Research Design report for proposed basement carpark excavation work at the 26 –32 Pyrmont Bridge Road site²⁵⁹. This site is located within the Pyrmont Station western construction site. Basement work at the site was to be confined to the northern half of the property.

The Archaeological Research Design report identified that the site was not known to have been developed beyond its use for cattle grazing prior to the 1880s, when terrace housing was constructed on both the Pyrmont Bridge Road and Pyrmont Street frontages. Terrace housing was identified as both residential and commercial in nature. The following statement of significance of provided for potential archaeological remains at the site:

The study site at 26 –32 Pyrmont Bridge Road, Pyrmont is a significant archaeological resource both in terms of the later development of Pyrmont and the local community. The site is likely to contain intact archaeological resources including structural remains, outbuildings and occupation deposits which yield information about the residential and trade occupations of inhabitants of the 1880s to 1915 terraces as well as the site's earlier occupation and uses which contribute to the history of Pyrmont.

Relevant site-specific research questions for this site were developed during preparation of the Archaeological Research Design, including:

- How does the material culture of the site compare with urban sites of a similar period and site use?
- What evidence exists of European occupation of the site before it was developed [in the 1880s]?
- What evidence exists of uses of the buildings which occupied the site?
- Can individuals be identified in the archaeological record?
- What evidence exists of uses of exterior spaces?
- Construction of the privies –do the rear yards of the terraces contain privies, or were sewer services incorporated into their construction?
- Are there any wells within the study site?

While this archaeological research design recommended a program of archaeological monitoring, the results of any archaeological investigation at the site are not publicly available. The construction of the basement carpark occurred in the northern portion of the Pyrmont Station western construction site, meaning that the southern portion containing the location of the 1880s terraces was not impacted by these works. Consequently, the above research design questions are still applicable to this area of the western construction site and are addressed in this archaeological research design.

²⁵⁹ Godden Mackay Logan, 26-32 Pyrmont Bridge Road, Pyrmont – Archaeological Research Design. Prepared for Property Partnerships Pacific (May 2000).

3.4.3 50-72 Union Street, Pyrmont –Archaeological Investigation for Charter Hall Holdings (Casey and Lowe May 2010)

Between November and December 2003, Casey and Lowe conducted a program of archaeological excavation at 50-72 Union Street, Pyrmont ahead of the redevelopment of the site²⁶⁰. The site, which is situated directly north of the eastern construction site on the opposite side of Union Street, was previously occupied by c1840s terraces and a 1920s flour mill, which was demolished in the 1980s.

Archaeological investigations found that while much of the pre-1920s evidence of occupation had been destroyed by the construction of the flour mill, evidence of the terraces houses, which included stone footings, a fireplace, drain and floor surfaces were still present. Furthermore, building materials, ceramics, glass, metal, and animal bone artefacts were located in deposits around the site. Pollen remains from soil samples were also used to reconstruct vegetation from the site. The investigation of the site provided an interesting insight into the early working-class history of Pyrmont and the lives of the individuals who resided and worked there.

The findings of this investigation provide a potential indication of the type of artefactual remains that may be encountered during works at the Pyrmont Station eastern construction site. Given the proximity of these two sites and the presence of contemporaneous terraces in both locations, Casey and Lowe's investigation provides an indication of the likely archaeological remains that may be present at the Pyrmont Station site.

3.4.4 55 Pyrmont Bridge Road, Pyrmont –Archaeological Monitoring of Former McWilliams Wines Premises for City West Centre (Edward Higginbotham 1994)

During March 1994, archaeological monitoring was carried out at 55 Pyrmont Bridge Road, Pyrmont, during the redevelopment of the Former McWilliams Wines Premises²⁶¹. The factory was purchased by the company in 1941, and a new building and underground wine tanks had been constructed which incorporated the previous premises of the early twentieth century ironworks on the site, expanded in the 1960s with a set of concrete tanks. Bulk excavations at this site revealed evidence of the underground tanks for the storage of bulk wine, with a large portion of the southern end of the excavated site occupied by disturbed to sandstone bedrock. Crushed sandstone overlaying bedrock was encountered under the northern end of the site. No evidence of the former foundry was encountered, likely having been removed due to the substantial excavations and levelling which took place for the new building and wine tanks in the 1940s, as well as for the additional wine tanks in the 1960s.

This site is located approximately 50m to the southwest of the Pyrmont Station western construction site. It demonstrates the lack of potential associated with sites which have experienced extensive below-ground excavation and disturbance.

3.4.5 1-9 Pyrmont Bridge Road, Pyrmont – Archaeological Monitoring Program for redevelopment of site for Powalu Pty Ltd (Godden MacKay Logan 1994-1996)

In July 1996, GML carried out archaeological monitoring at 1-9 Pyrmont Bridge Road, Pyrmont during the removal of a concrete slab and excavation of the underlying fill²⁶². Initial test pits and coring

²⁶⁰ Casey and Lowe, *Archaeological Investigation 50-72 Union Street, Pyrmont. Volume 1: Main Report* (May 2010).

²⁶¹ Edward Higginbotham and Associates, *Report on the archaeological monitoring programme during redevelopment of the former McWilliams Wines Premises, 55 Pyrmont Bridge Road, Pyrmont, N.S.W* (August 1994).

²⁶² Godden Mackay Logan, *1-9 Pyrmont Bridge Road, Pyrmont – Archaeological Monitoring Report* (September 1996).

suggested that intact archaeological deposits may have survived in the eastern portion of the site. This was supported by the identification of a stratigraphic layer containing bricks, gravel, and artefacts, which were associated with the levelling of the site during construction work in the 1950s.

Ultimately the site was found to be highly disturbed though and limited evidence for land use prior to the 1880s could be retrieved. However, evidence of former structures on the site was located, including the remains of a brick, north-south wall with evidence of sandstone footings below the concrete slab floor of the 1950s building. Additionally, sandstone footings and a slate damp course were located and associated with neighbouring structures, as well as a rectangular brick outbuilding complete with sewer pipes. These remains provided evidence on the post-1878 development of the area, likely indicating the existence of a row of terraces built along Pyrmont Bridge Road in 1880.

The study area for this monitoring program is in close proximity to the Pyrmont Station eastern construction site, directly to the southeast on the opposite side of Pyrmont Bridge Road. The findings uncovered in this investigation offer some comparative insights into the neighbourhood of the Pyrmont Station eastern construction site and reflect indicative archaeological deposits that may be associated with the 1870s terrace housing in the northern portion of the Pyrmont Station construction site.

3.4.6 “Falling through the Cracks: Method and Practice at the CSR Site, Pyrmont” (Casey M., 2004)

Casey and Lowe undertook archaeological excavation at the former Colonial Sugar Refining (CSR) site in Pyrmont in 1996. The former refinery, which was complete with terrace housing for workers, was constructed in 1878, with the workers’ houses being demolished in the 1950s to make way for expansions to the refinery. The archaeological excavations focused on the workers’ residences, and a detailed assessment of the domestic artefact deposits from the under-floor spaces of multiple terrace houses was conducted.

Mary Casey, who acted as Excavation Director for the investigation prepared a journal article in 2004 which outlined a detailed methodology for comparative analyses of domestic archaeological deposits based on the findings from the excavation program²⁶³. Casey discussed appropriate standards of artefact cataloguing, which allow for detailed scrutiny of artefact distribution patterns between domestic archaeological deposits. Casey also specified a detailed methodology for understanding the depositional process which contribute to the formation of underfloor deposits, highlighting the greater propensity for these deposits to form underneath kitchens as opposed to sleeping areas. Moreover, Casey also provided a detailed spatial excavation methodology which was utilised during the CSR site investigations, and then correlated this to the occupants of the terraces in order to provide more focused and detailed social and material information about the inhabitants.

This paper details specific approaches that can be adopted when investigating places where working-class people lived such as the terraces housing seen in both the Pyrmont Station construction sites. In particular, a focus on the underfloor deposits of the kitchen areas can provide comparative insights into the “typical’ profile of lives as lived within working-class housing” across other later nineteenth century housing in Pyrmont and the broader urban Sydney area. The site of the former CSR workers terrace houses is about 300m to the northwest of the Pyrmont Station western construction site, and dates to within 20 years of construction.

²⁶³ Casey, M. 2004. “Falling through the Cracks: Method and Practice at the CSR Site, Pyrmont”, *Australasian Historical Archaeology*, 22: 27-43.

3.5 Known impacts

3.5.1 Western construction site

The southernmost portion of the western construction site was purchased by W & A Gilbey in 1947. Excavation for a new building commenced in 1948. The building inspector noted on 12 March 1948: “excavation for foundation shows rock at varying depths below ground level.”²⁶⁴ The nature of the foundation was noted as ‘rock’ in the Building Inspector’s Card, and the depth of the footings was noted as 4 feet. It is likely that this work has resulted in considerable truncation of the western construction site in areas containing the concrete foundations for this building.

The northern portion of the western construction site contains a substantial basement, excavated c.1999. Godden Mackay and Logan prepared an Archaeological Research Design report for proposed basement carpark excavation work at the 26 –32 Pyrmont Bridge Road site.²⁶⁵

3.5.2 Eastern construction site

Josiah White obtained the eastern construction site in 1896. White built a three-storey building with basement on the corner of Union Street and Pyrmont Bridge Road in the same year. Newspaper articles from the time refer to the ample basement room and lifts.²⁶⁶

A fuel service station was constructed in the eastern corner of the construction site in the 1960s. Later excavation for the construction of the 1980s office building has likely removed all remnants of this structure with the possible exception of any residual deep fuel storage tanks.

3.6 Assessment of archaeological potential

3.6.1 Pyrmont Station western construction site

A summary of the predicted archaeological remains at the Pyrmont Station western construction site and their potential is presented in Table 17. This summary is informed by the historical background contained within this report and an analysis of previous archaeological assessments.

Table 17: Summary of archaeological remains and potential at western construction site

Phase	Site use	Predicted archaeological remains	Potential
Phase 1 (1788-1879)	Part of the Ultimo Estate, potentially used for livestock grazing	Land clearing is presumed to have occurred during this time, but no known construction occurred within the Pyrmont Station western construction site during this time. Land utilised for informal grazing or farming may have left ephemeral archaeological remains such as animal bones (cattle or sheep), tree boles, fencing post holes or field drains. However, with the later development of the site and with significant basements in the northern portion of the site, these remains are not anticipated to be present.	Nil
Phase 2 (1879-1914)	Residential housing and commercial properties	Terrace housing used for commercial and residential tenants was constructed in 1883 in the southern portion of the Pyrmont Station western construction site. These	Moderate for truncated remains

²⁶⁴ ‘Building Inspectors Card’.

²⁶⁵ Godden Mackay Logan, *26-32 Pyrmont Bridge Road, Pyrmont – Archaeological Research Design*. Prepared for Property Partnerships Pacific (May 2000).

²⁶⁶ ‘No Title’, *Sunday Times*, 30 August 1896, 7, <http://nla.gov.au/nla.news-article130405487>.

Phase	Site use	Predicted archaeological remains	Potential
		<p>terraces were the living and working spaces of a variety of retail trades, including bakers, grocers, drapers, newsagencies, and restaurants. Archaeological remains related to these premises would only be situated within the southern portion of the site due to basement excavation present in the north. The location of the former terrace houses with respect to the construction site and known basement excavation is provided in Figure 112.</p> <p>The area of archaeological potential within the construction site is associated with the central and rear portions of the terrace buildings, which would include rear kitchens. Privies were located within the rear yards however these are situated within the area of later basement excavation.</p> <p>Archaeological resources relating to development within these subdivisions include:</p> <ul style="list-style-type: none"> • Building foundations—sandstone or brick—associated with documented and undocumented structures; • Demolition fills, postholes and rubbish deposits; • Undocumented features including basements, drains, cesspits, utilities, cisterns and wells • Yard and workshop surfaces • Evidence of gardens and yard layouts. <p>The study area has generally low potential to contain artefact-bearing occupation/underfloor deposits.</p>	
Phase 3 (1914-1947)	Resumed for road widening, potential storage area for building material or soil, residential housing and commercial properties	<p>During this phase the northern portion of the construction site still housed terraces used in both a residential and commercial capacity. However, the ground disturbance caused by the construction of the basement carpark will have undoubtedly destroyed the archaeological remains associated with this section. It is possible that some of the same potential artefacts predicted for Phase 2 may have been discarded in the southern portion of the site, or been redeposited by subsequent construction works.</p> <p>The southern portion of the site' was used for storing building materials at this time. There will likely be minimal archaeological remains associated with this use. Any such remains will likely be limited to building materials, rubbish deposits, and introduced soil deposits. Historical evidence indicates that structures related to the storage yard were likely impermanent sheds, possibly of timber construction. This would suggest that remnants of such structures will be limited to postholes, remnants of the structures' construction materials, and possibly items that were being stored within.</p>	Low
Phase 4 (1947-present)	Gilbey's Distillery and connected carpark, 'Frankel House' and connected basement carpark	<p>The current building was constructed at the beginning of this phase. Although the building was constructed on pile footings, it is likely that excavation work has resulted in considerable truncation of the western construction site in areas containing the concrete foundations for this building. Remains associated with the former use of the building as a distillery may be present in the sub-floor cavity of the building from chance discard, including glass bottles / bottle fragments, metal equipment and rubbish deposits.</p>	Moderate



Figure 112: Overlay of potential archaeological remains within Pyrmont Station western construction site.

3.6.2 Pyrmont Station eastern construction site

A summary of the predicted archaeological remains at the Pyrmont Station western construction site and their potential is presented in Table 18. This summary is informed by the historical background contained within this report and an analysis of previous archaeological assessments.

Table 18: Summary of archaeological remains and potential at eastern construction site

Phase	Site use	Predicted archaeological remains	Potential
Phase 1 (1788-1878)	Part of the Ultimo Estate, potentially used for livestock grazing	Land clearing is presumed to have occurred during this time, but no known construction occurred within this Pyrmont Station western construction site during this time. Land utilised for informal grazing or farming may have left ephemeral archaeological remains such as animal bones (cattle or sheep), tree boles, fencing post holes or field drains. However, with the later development of the site and with significant basements in the northern portion of the site, these remains are not anticipated to be present.	Nil
Phase 2 (1883-1930)	Residential housing, commercial properties, 'Elder's Hall'	<p>Terrace housing used for commercial and residential tenants were constructed in 1878. These terraces were the living and working spaces of a variety of retail trades, including butchers, grocers, hairdressers, and general stores, as well as "Elder's Hall" a local community centre at the time. Archaeological remains related to these premises would only be situated within the northern portion of the site due to basement excavation present in the south. The location of the former terrace houses with respect to the construction site and known basement excavation is provided in Figure 113.</p> <p>The area of archaeological potential within the construction site is associated with the front rooms of these former terraces and only for a small section of the original Union Street row of buildings. Privies and kitchens are likely not preserved due to their presence in the rear of the properties which have been removed from excavation for basement parking.</p> <p>Archaeological resources relating to development within these subdivisions include:</p> <ul style="list-style-type: none"> • Building foundations—sandstone or brick—associated with documented and undocumented structures; • Demolition fills, postholes and rubbish deposits; • Undocumented features including basements, drains, cesspits, utilities, cisterns and wells • Yard and workshop surfaces • Evidence of gardens and yard layouts. <p>The study area has generally low potential to contain artefact-bearing occupation/underfloor deposits.</p>	Moderate for truncated remains

Sydney Metro West – Pyrmont Station Construction Sites
Archaeological Research Design and Excavation Methodology

Phase	Site use	Predicted archaeological remains	Potential
Phase 3 (1930-1986)	Timber and building materials storage yard, and associated sheds and offices, fuel service station, car park	<p>This phase saw the western portion of the construction site used for lumber storage which would preclude the likely accumulation of significant archaeological remains. Chance archaeological remains from this phase could include timber and brick footings for walls or fences, as well as isolated rubbish deposits, however later disturbance to construct office buildings in the 1980s involved ground excavation would have likely removed all of these relatively ephemeral remains.</p> <p>The fuel service station was constructed in the eastern corner of the construction site in the 1960s. Later excavation for the construction of the 1980s office building has likely removed all remnants of this structure with the possible exception of any residual deep fuel storage tanks.</p>	<p>Nil for storage yard</p> <p>Low for deep deposits (fuel tanks) only</p>
Phase 4 (1986-present)	Commercial properties	Subsurface archaeological remains are not anticipated to be present from the construction of the 1980s office building on the site	Nil for subsurface remains



Figure 113: Overlay of potential archaeological remains within the Pyrmont Station eastern construction site.

3.7 Assessment of archaeological significance

The following assessments of significance for archaeological remains are provided for remains predicted to be located within the construction sites only. Land use phases for which archaeological remains have not been predicted are not included in this discussion of archaeological significance. Archaeological remains related to later twentieth century phases are also not included in these assessments of significance.

Predicted archaeological deposits relating to late-nineteenth and early twentieth century remains within both Pyrmont Station construction sites are assessed in accordance with the methodology outlined in Section 3.2. in Table 19.

Table 19: Assessment of archaeological significance for Pyrmont Station western construction site phase 2 (1883 –1914 –commercial and residential development) and the Pyrmont Station eastern construction site phase 2 (1878 –1940 –commercial and residential development).

Criteria	Discussion
Research potential (criterion E)	<p>Archaeological remains related to small-scale late-nineteenth and early twentieth century domestic and commercial industries, if a robust dataset, has research potential for its ability to investigate the domestic and working practices of the people who resided in these buildings. Robust domestic and commercial artefactual deposits, when spatially interrogated, can respond to questions regarding the trades, class, gender, ethnicity, and habits of the people who discarded them. Artefactual datasets can be compared with similar comparative deposits from other Pyrmont late-nineteenth archaeological sites. This could further answer questions regarding to respectability and material aspirations of the residents and workers in what was a predominantly working class suburb for much of its history.</p> <p>If substantial and intact archaeological resources associated with Phase 2 are present within the study area, these remains would reach the threshold for local significance under this criterion.</p>
Association with individuals, events or groups of historical importance (criteria A, B & D)	<p>While the Ultimo Estate was originally the possession of John Harris and was subdivided by his family, the archaeological remains of late nineteenth terrace houses (and associated artefactual deposits) would not be associated with Harris or his family. No individual, events or groups of historic importance are associated with the predicted remains.</p> <p>Archaeological remains would not reach the threshold for local significance under these criteria.</p>
Aesthetic or technical significance (criterion C)	<p>Artefactual remains related to trades and commercial practices have the potential to provide insight into the activities that took place. Artefact assemblages may have aesthetic significance, and be able to be used in interpretive displays, dependant on the intactness of the objects recovered. Structural remains associated with the former terraces and shopfronts have the potential to exhibit the layout, form and function of a former late nineteenth commercial high street. Domestic artefactual remains, particularly those related to families, child rearing or education, may be of aesthetic value.</p> <p>Archaeological remains would be considered of local significance under this criterion.</p>

Criteria	Discussion
Ability to demonstrate the past through archaeological remains (criteria A, C, F & G)	<p>The use and operation of combined commercial and residential terrace houses would be able to demonstrate the working-and middle-class lives of the former occupants of these buildings. Former terrace buildings on Pyrmont Bridge Road and Union Street would have formed a small commercial 'high' street as Pyrmont grew with increased traffic after the construction of Pyrmont Bridge. The operation of this commercial area, which may show the material aspirations of the largely working class residents of Pyrmont, would likely display past practices and lifeways of these inhabitants.</p> <p>Artefacts associated with Phase 2 of residential development, if recovered from intact occupation deposits, may provide information about prior uses of the study area and its residents. Artefacts from secure contexts may further contextualise the lifeways of the residents who occupied the study area in the mid to late 19th century, providing insight into personal preferences, cottage industry and/or trade networks. Such artefacts may prove useful as comparative material for previous similar archaeological investigations in the area. Expanding the corpus of available archaeological information may highlight heretofore unknown variation or homogeneity in domestic lives through future intra-regional studies.</p> <p>If substantial and intact archaeological resources associated with Phase 2 are present within the study area, these remains would reach the threshold for local significance under these criteria.</p>
Statement of significance	<p>Archaeological remains associated with late-nineteenth and early-twentieth century commercial and residential terrace houses (and associated artefactual deposits) are of local significance for their ability to answer research questions about trades, class, gender and aspiration. Archaeological remains recovered from these sites can be interrogated against comparative datasets of other Pyrmont archaeological investigations. These remains may provide information on the manufacture, operation and retail of a variety of commercial trades. Remains would be demonstrative of the practices and lifeways of a burgeoning local commercial community which tended to the daily needs of a busy maritime and industrial working-class precinct.</p>

3.7.1 Summary of archaeological potential and significance at the Pyrmont Station construction sites

A summary of predicted significant archaeological remains located within the Pyrmont Station construction sites is provided in Table 20 and the anticipated location of these remains is shown in Figure 114.

Table 20: Summary of significant non-Aboriginal archaeological remains within the Pyrmont Station construction sites.

Site	Phase	Predicted remains	Potential	Significance
Pyrmont Station western construction site	Phase 2 (1879-1914)	Archaeological remains related to this phase may include brick and stone footings, hearths, yard and workshop surfaces, rubbish scatters and isolated rubbish pits/bottle dumps.	Moderate	Local
		Dependant on the extent of former basement excavation impacts in the northernmost portion of the site, the study area has some potential to contain deeper subsurface structures such as wells, privies and cisterns. These structures may contain artefact bearing deposits.	Low	Local
		The site has limited potential to contain underfloor deposits (discarded ceramic, pins, buttons and glass).		

Site	Phase	Predicted remains	Potential	Significance
Pyrmont Station eastern construction site		Archaeological remains related to this phase may include brick and stone footings and hearths.	Moderate	Local
	Phase 2 (1883-1930)	The site has limited potential to contain underfloor deposits (discarded ceramic, pins, buttons and glass), artefacts deposited within decommissioned deeper subsurface structures such as wells, cisterns or privies. Yard and workshop surfaces and isolated rubbish deposits	Low	Local



Figure 114: Significant non-Aboriginal archaeological resources at the Pyrmont Station construction sites.

4.0 RESEARCH DESIGN

4.1 Historic themes

Contextual analysis is undertaken to place the history of a particular site within relevant historical contexts, in order to gauge how typical or unique the history of a particular site actually is. This is usually ascertained by gaining an understanding of the history of a site in relation to the broad historical themes characterising Australia at the time. Such themes have been established by the Australian Heritage Commission and the NSW Heritage Office and are outlined in synoptic form in *New South Wales Historical Themes*, issued by the NSW Heritage Office.²⁶⁷

4.1.1 Summary of relevant themes

The Pyrmont Station construction sites have the potential to contain locally significant archaeological resources associated with late-nineteenth century residences and early 20th century small-scale light industry. Archaeological remains associated with the history of the site may include building foundations, demolition rubble and fill and isolated artefact deposits. The potential archaeological resource may relate to the NSW Historic Themes of 'Industry', 'Accommodation' and 'Domestic Life'. Each theme will be discussed in turn to contextualise the site history and identify potential archaeological evidence. Historic themes and their descriptions have been derived from the Heritage Council of NSW Historical themes guidelines.²⁶⁸

Table 21: Historic themes relevant for the predicted archaeological remains

Australian Theme	NSW Theme	Discussion
3. Developing local, regional and national economies	Industry	The study area has the potential to contain an archaeological resource associated with the construction and use of industrial spaces and factories through artefactual resources and yard/workshops layout. Artefact bearing deposits may provide insight into the goods manufactured and/or traded through the study area.
4. Building settlements, towns and cities	Towns, suburbs and villages	Archaeological remains relating to the settlement of the study area may provide evidence of activities associated with creating, planning, and managing urban functions, landscapes and lifestyles in towns, suburbs and villages. This may provide information regarding streetscape, subdivision pattern and town plan. Evidence may include building foundations, undocumented subsurface features, former fence lines and property boundaries.
4. Building settlements, towns and cities	Land tenure	Archaeological remains relating to the settlement of the study area are likely to be present, potentially providing evidence of activities and processes for identifying forms of ownership and occupancy of land. Evidence may include former fence lines and property boundaries.
4. Building settlements, towns and cities	Utilities	Archaeological remains relating to the settlement of the study area are likely to be present, potentially providing

²⁶⁷ Heritage Council of NSW 2001. *New South Wales Historical Themes*. Heritage Office guidelines. Accessed online 20 September 2021: <https://www.heritage.nsw.gov.au/assets/Uploads/a-z-publications/g-i/Historical-Themes.pdf>

²⁶⁸ Heritage Council of NSW 2001. *New South Wales Historical Themes*. Heritage Office guidelines. Accessed online 20 September 2021: <https://www.heritage.nsw.gov.au/assets/Uploads/a-z-publications/g-i/Historical-Themes.pdf>

Australian Theme	NSW Theme	Discussion
		evidence of activities associated with the provision of services, especially on a communal basis. Evidence may include water pipelines, sewerage tunnels, wells, cesspits, and rubbish pits.
4. Building settlements, towns and cities	Accommodation	Archaeological remains relating to the residential development of the study area are likely to be present, potentially providing evidence of the provision of accommodation. Evidence may include building foundations, undocumented subsurface features and artefact scatters.
5. Working	Labour	The study area was occupied by several businesses and light industry throughout the late nineteenth and into the mid twentieth century. Archaeological remains relating to their use of the site may provide information regarding labour practices and activities of that time.
8. Developing Australia's cultural life	Domestic life	Archaeological remains relating to the nineteenth century residences are likely to be present, potentially providing evidence of activities associated with creating, maintaining, living in and working around houses and institutions. Evidence may include domestic artefact scatters, building foundations and undocumented subsurface features.
8. Developing Australia's cultural life	Leisure	Archaeological remains relating to the Captain Cook / Bunnerong Hotel are likely to be present within the study area, potentially providing evidence of activities associated with recreation and relaxation. Evidence may include artefact scatters.

4.2 Archaeological research questions

4.2.1 Methodology

“Archaeological research potential is the ability of archaeological evidence, through analysis and interpretation, to provide information about a site that could not be derived from any other source, and which contributes to the archaeological significance of that site and its ‘relics’”²⁶⁹ .

The significance of a potential archaeological resource therefore lies predominantly in its ability to respond to research agendas in a meaningful way, rather than duplicating known information, or information that might be more readily available from other sources, such as documentary records or oral history. Therefore, the aim of the following research questions is to ensure that the proposed archaeological investigation is focused on genuine research needs.

The Statement of archaeological Significance, in combination with the NSW Historic Themes,²⁷⁰ have provided the basis for the research design framework. Archaeological resources within the construction sites have the potential to contribute to research areas such as:

- Domestic occupation and material culture in the late 19th and early 20th centuries
- Consumer behaviour and the household

²⁶⁹ Heritage Branch, Department of Planning NSW. Assessing Significance for Historical Archaeological Sites and ‘Relics’. Endorsed by the Heritage Council of NSW in December 2009 p11.

²⁷⁰ Heritage Council 2001

- Industrial archaeology and late 19th century material culture
- The intersection of commercial space and the household
- Comparative analysis of archaeological results in Pyrmont.

Additional research questions may be posed (and existing questions modified) as the archaeological excavation progresses and the extent and condition of the archaeological resource is revealed.

4.2.2 Research questions

4.2.2.1 Domestic occupation and material culture in the 19th century

Early plans indicate that a number of semi-detached residences and shopfronts were located at both sites, dating from the late 19th century. The historical record indicates that these residences were primarily occupied by working-class individuals and families. Evidence of the residences is likely to consist of stone footings and foundations, postholes associated with timber structures and outhouses, and deposits containing evidence of such as yard scatters. The age of the residences indicates that underfloor deposits are unlikely to be present. The site may contain evidence of cottage gardens, the layout and use of yard areas, and artefact scatters and refuse pits associated with former residents.

The western construction site, dependant on the extent of the basement constructed on the northern portion of the site, has some potential to contain former privies and/or undocumented water sources such as wells or cisterns. Privies dating to the later 19th century, and therefore after the introduction of mandatory sewerage installation enforced by the Sydney City Council from c. 1875, may contain artefacts discarded within the structures on decommissioning. Similarly, wells and cisterns can be backfilled with artefact-bearing fills.

Evidence of domestic occupation and identity obtained from artefacts recovered from the sites would relate to the NSW Historic Theme of 'Domestic life', 'Accommodation' and 'Utilities'.

The site is likely to contain an archaeological resource that is typical of the late Victorian period of development. Evidence of footings, yards surfaces and utilities are unlikely to provide additional information beyond that which has already been ascertained through archaeological excavation of sites of a similar date or can be obtained from the historical or cartographic record. However, should artefact bearing occupation deposits survive in situ, these have the potential to provide detailed insight into the lives of individuals residing and working in the study area. Artefacts may prove useful as comparative material for previous similar archaeological investigations in the area. Expanding the corpus of available archaeological information may highlight heretofore unknown variation or homogeneity in domestic lives through future intra-regional studies.

Potential research questions relating to the lifeways of these individuals include the following:

- Is there evidence that the residents were engaged in recreational activities? (gaming, smoking, sewing, etc)
- Is there evidence of the intersection of work and home life? Is there evidence of other small-scale industrial practises occurring in a seemingly domestic environment?
- What food were the residents of the cottages consuming? Is there evidence of the cooking methods, brand or food preferences?
- What evidence is there of gardens, workshops and the layout and use of yard areas?

- How do the archaeological remains compare to other sites excavated in Pyrmont and the inner suburbs of Sydney? are the study areas typical of a late 19th century light industrial and residential site, or do the remains provide information that is rare in the regional context?
- Does the archaeological resource provide insight into activities split along gender or age lines? Is there evidence for the presence of women and children? Does the resource provide information about family dynamics in early Sydney?
- Does the archaeological resource provide evidence of social standing and status? Does this support the notion that the area was working class? Is there evidence that former inhabitants of the site displayed their social standing or ethnicity through items of personal adornment or preferences for certain consumables?

4.2.3 Industrial archaeology

It is anticipated that the study area may contain archaeological remains associated with small-scale shopfronts of the later 19th and early 20th century. Archaeological remains associated with the layout of warehouses and other industrial buildings, construction materials and methods, are unlikely to be particularly significant or rare, as discussed in Section 3.7 of this report. However, there is some potential for the study area to contain an artefactual resource within former building cavities and refuse pits that may be associated with the individuals who worked and resided within the study area. Potential research questions relating to the lifeways of these individuals include the following:

- Does the artefact assemblage provide information on the daily life of employees and/or workers at the site? Do any intact deposits provide useful spatial information, identify discrete activity areas or provide spatial data on the range of tasks undertaken within the building over time?
- Is there artefactual or other evidence of the practising of trades and industry other than those recorded in the historic record, such as cottage industries associated with women and children?
- Is there evidence of the division of labour spaces, yards and sheds?
- Is there evidence of the conditions in which the employees worked?
- If evidence of rubbish disposal practises are encountered, that can be associated with industrial rather than domestic purposes, do they contain assemblages that differ dramatically from those in more domestic environments? Are those individuals working at the site disposing of the same rubbish/materials that they would at home in addition to industrial waste?
- Is there evidence of the types of foods consumed by the workers?
- Is there evidence of food production on-site?
- Do the artefacts provide some indication of the types of eating utensils and vessels being used? Do these conform to those found in typically 'industrial' archaeological settings?
- Can the archaeological resource provide us with information regarding the day-to-day life of workers in the late 1800s? Is there evidence of consumer choices and preferences or gender-

specific artefactual remains that may provide information on the lives of working class men and women?

4.2.4 Public utilities and services

Any drainage features, cisterns, outbuildings (including cesspits) and water infrastructure identified to date the nineteenth century have the potential to contribute to our understanding of the types of amenities available to the throughout the nineteenth century, as well as the adaptation of infrastructure to the local landscape. Solutions to landscape, drainage, water supply and sewage issues unique to this area can be explored through these types of remains. The Pyrmont area was somewhat unique, in that it was located in such close proximity to the city, but was almost entirely undeveloped until the later nineteenth century. Does the archaeological resource indicate that

The abandonment of the cesspit and pan toilet, and connection into the municipal sewerage system, was made compulsory by the City Council c.1875, although the rate at which it was adopted in working class neighbourhoods, typically those associated with lower socio-economic standing, is unknown. Archaeological resources present at the site may reveal answers to the following research questions:

- Can the archaeological resources at the site provide insight into when this new technology was adopted in this part of Pyrmont? Can they also inform us how this was achieved?

5.0 ARCHAEOLOGICAL METHODOLOGY

5.1 Introduction

The study area has been assessed as generally having moderate potential to contain locally significant archaeological remains in the form of structural evidence and artefacts associated with the late 19th century residential and commercial development of Pyrmont.

It is proposed that management of the potential archaeological resource include the following processes. These have been defined in Table 22 and illustrated in Figure 115:

- Heritage induction (completed prior to demolition; see Section 5.2)
- Unexpected Finds Procedure during demolition of extant structures and excavation within areas previously impacted by basements as illustrated on Figure 114 (see Section 5.5)
- Monitoring and/or Unexpected Finds Procedure for enabling works (see Section 5.4.1)
- Eastern construction site - Monitoring and salvage (if required) of excavation work within the area of moderate archaeological potential (see Figure 115)
- Western construction site - Testing and salvage (if required) of areas proposed for excavation within the area of moderate archaeological potential (see Figure 115).

It is recommended that the final archaeological program be developed by the ETP contractor and the Excavation Director engaged for the project.

Table 22: Definition of archaeological methodologies

Methodology	Definition
Monitoring	<p>Archaeological monitoring is where an archaeologist is in attendance and supervising construction excavation work with potential to expose or impact archaeological remains.</p> <p>Monitoring is generally undertaken where there is lower potential for significant archaeological remains and/or where minor excavation work is in an area of archaeological sensitivity.</p> <p>If archaeological remains are identified during monitoring, they would be excavated and recorded by the site archaeologist</p>
Test excavation	<p>Archaeological test excavation involves excavation of small sample trenches within a potential archaeological site. Testing is usually undertaken prior to construction to clarify the extent of the potential remains, archaeological significance, potential of a construction task to impact significant archaeology and inform requirements for further archaeological investigation, such as salvage excavation or monitoring.</p>
Salvage excavation	<p>Archaeological salvage refers to open-area archaeological excavation under the control of the Excavation Director undertaken prior to impact. Salvage includes the horizontal excavation of the entire historical archaeological site. Manual excavation would be undertaken using hand tools, by a qualified archaeological team.</p>

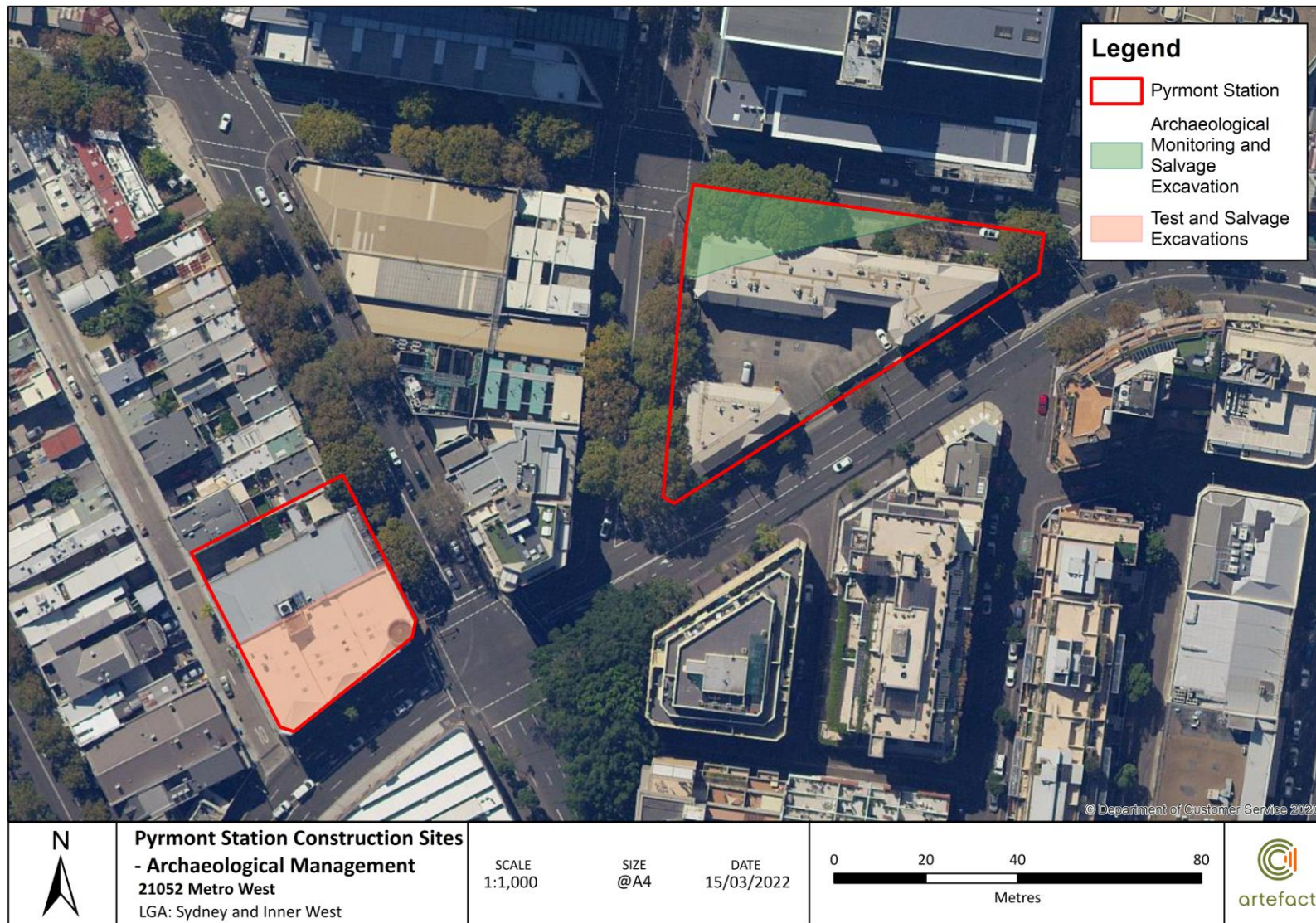


Figure 115: Overview of archaeological management

5.2 Heritage induction

Archaeological heritage would be included in the general project induction as described in the project CEMP or CEMP for each contract. Additional site specific archaeological inductions would be provided to staff and contractors. At a minimum this process would include an overview of the project obligations and archaeological areas of potential, the role of the archaeological team, and the project unexpected finds procedure including typical potential archaeological remains identified in this assessment.

Location maps for predicted areas of archaeological potential would be made available to all relevant contractor personnel along with a copy of this ARDEM. A visual guide of potential archaeological items would be provided as part of the heritage induction to all work crews to assist with potential unexpected heritage find identification.

Contact details for the Excavation Director and Site Director would be provided to all necessary work managers and environmental assessment personnel.

5.3 Archaeological team

5.3.1 Excavation Director

Archaeological investigations would be managed by a suitably qualified and experienced Excavation Director who would meet the NSW Heritage Council criteria. The Excavation Director would be responsible for overseeing the archaeological investigation program.

A Site Director may also be required to support the Excavation Director on site.

A suitably qualified and experienced Aboriginal Archaeological Excavation Director would be nominated as required.

5.3.2 On site archaeologists

Archaeological excavation and monitoring tasks would be conducted by appropriately trained archaeologists under the coordination of the Excavation Director and Site Director.

5.4 Archaeological excavation methodologies

5.4.1 Enabling works

Should excavation work be required prior to archaeological testing of the eastern construction site (see Section 5.4.2) or monitoring of the western construction site (see Section 5.4.3) appropriate management methodologies would be adopted by the ETP contractor in consultation with the nominated Excavation Director.

It is assumed that enabling works may include a combination of contamination and geotechnical testing and trenching to disconnect or relocate utilities. It is assumed that this work may take the form of auguring, Non-Destructive Digging and or machine excavation.

Dependant on the location and excavation methodology required, archaeological monitoring (as per the methodology outlined in Section 5.4.2) and/or implementation of the Unexpected Finds Procedure may be appropriate.

5.4.2 Archaeological monitoring – Eastern construction site

It is recommended that archaeological monitoring and salvage (if required) of project excavation work occur within the area of moderate archaeological potential in the eastern construction site (see Figure 115). Archaeological monitoring is considered to be the most appropriate management for this location, due to the limited size of the area of potential, likely disturbance through the introduction of utilities adjacent to the public footpath and low potential for intact artefact-bearing deposits to be present.

Archaeological monitoring would be conducted by on site archaeologists who would be coordinated by the Site Director and Excavation Director.

The on site archaeologist would supervise excavation but would also be able to direct machine excavation contractors in consultation with contractor supervisors, to excavate areas of interest under their direction, so long as excavation does not exceed the approved impact area for the scope of work. Should construction excavation work endanger potential archaeological deposits, the machine excavation contractor must cease excavation if advised by the monitoring archaeologist.

If archaeological remains are identified during archaeological monitoring, they would be recorded and assessed to determine if further investigation or salvage is required. Localised stoppages in the construction work would be required to facilitate this process. Works would not recommence until the monitoring archaeologist has completed the recording and the Excavation Director is satisfied that further investigation is not required.

If significant and intact archaeological remains are identified, then archaeological salvage would be required prior to construction impacts. Assessments of significance of all finds would be supervised and confirmed by the Excavation Director.

5.4.3 Test excavation – Western construction site

It is assumed that excavation impacts would be required throughout the construction site and that, therefore, any intact archaeological remains would be removed by the project.

The test excavation program would include the excavation of five (5) test trenches within the area of moderate potential (see Figure 116). The test excavation program would target locations deemed to have a higher likelihood of containing deeper subsurface structures and intact artefact-bearing deposits, such as yards, outbuildings and structural footings based on historic mapping and research. The primary aim of the testing is to verify the extent of any previous impacts and identify whether significant archaeological deposits, structures or features survive within the site.

Should the location of any test trench be found to be unsuitable due to the presence of additional underground services, overhead wires, tree roots or other practical reasons, it is proposed that the trenches be moved to a more suitable location, within 3 metres of the proposed location. Should initial testing results indicate that historic mapping / prepared overlays are incorrect, it is proposed that the location of the remaining trenches be moved to better accommodate this, to obtain the best outcome in terms of identifying the archaeological resource.

The 5 test trenches, measuring 1.5 metres by 6 metres, be excavated in the locations illustrated on Figure 116 and described below. The length of test trenches was selected to span the distance, wherever possible, between potential structural remains associated with neighbouring properties, therefore increasing the likelihood of encountering archaeological remains within any given trench and achieving a useful sample of the overall site.

Test Trench 1

Trench 1 is located within an area previously occupied by two Phase 2 residences and yards and a Phase 3 structure. Testing in this location will provide insights into disturbance that may have resulted from the construction of Phase 3 and 4 buildings and assess the survival of any potential Phase 2 archaeological remains such as underfloor deposits, services, footings and yard surfaces. The extent of Phase 3 demolition activities may also be identified during the program.

Test Trench 2

Trench 2 is located in an area previously occupied by a Phase 2 residence and extends into the former yard. Testing in this location would provide insight into the survival of stratigraphy in this location and identify the extent of survival of the Phase 2 structure. The trench has been placed to include the former rear yard threshold, a location where discarded artefacts often accumulate. The extent of Phase 3 demolition activities may also be identified during the program.

Test Trench 3

Trench 3 is located within an area previously occupied by two Phase 2 residences. The trench has been placed to encompass potential rear yard thresholds and the former property boundary. Testing in this location would provide insights into the survival of Phase 2 remains and features including underfloor deposits, footings, services and post holes and extent of Phase 3 demolition activities.

Test Trench 4

Trench 4 is located within an area that was utilised as yard through Phases 2 and 3. Testing in this location would provide insight into the survival of stratigraphy in this location and will allow for a comparison between the eastern and western portions of the site. This trench has also been placed to confirm that the privies and outbuildings located against the former property boundary are definitely outside the area of moderate potential (within the Phase 4 basement), as the property boundaries may have shifted over time.

Test Trench 5

Trench 5 is located within an area previously occupied by two Phase 2 residences and a Phase 3 structure. Testing in this location will provide insights into disturbance that may have resulted from the construction of Phase 3 and 4 buildings and assess the survival of any potential Phase 2 archaeological remains such as underfloor deposits, services, footings and yard surfaces. The extent of Phase 3 demolition activities may also be identified during the program.

5.4.3.1 Triggers for archaeological salvage

Should significant and intact archaeological remains be identified during test excavation, and these remains show they are part of a larger feature which extends beyond the limit of the test excavation area, salvage excavation may be proposed by the Excavation Director. Salvage excavation of a resource would be proposed if one or more of the following conditions are met:

- Substantially intact archaeological remains in association with *in situ* artefact bearing deposits that have the ability to address the research questions (see Section 4.2) and meet the local significance threshold are identified during testing
- The results of the test excavation indicate that the research questions can be addressed more thoroughly through continued excavation i.e. salvage excavation
- Excavation of the remains to their full extent had value for heritage interpretation or public outreach purposes.

Should salvage excavation be required, the methodology outlined in Section 5.4.3 would be adopted.



Figure 116: Proposed test trench locations – Western construction site

5.4.4 Salvage excavation methodology

The need for salvage excavation would be triggered by the discovery of considerably intact stratigraphy and/or locally significant archaeological remains identified during the archaeological monitoring and test excavation programs. Please note that the archaeological testing and/or monitoring programs may identify that only portions of the areas of archaeological potential require full salvage excavation.

Open area salvage excavation is a method of archaeological investigation in which the full horizontal extent of an area of site is investigated and cleared, whilst preserving the stratigraphic record.

Removal of modern fills and disturbance would be undertaken by an appropriately sized excavator and flat bucket, manned by an experienced operator, under direct archaeological supervision. On the identification of any historical/archaeological fills, detailed excavation would commence using hand by a qualified archaeological team. Archaeological features would be cleaned, investigated (excavated) and recorded in detail. In urban archaeological sites, careful machine excavation may also be employed to assist the detailed archaeological excavation process. Where contaminated deposits are identified, remote recording techniques may be utilised to minimise exposure to harmful materials.

Construction works would not proceed until the salvage excavation is completed and the Excavation Director has provided clearance for the area in question.

5.4.4.1 Deep subsurface structural remains

Structural remains of wells, cisterns and cesspits/privies can contain substantial amounts of backfilled material and artefactual remains. Artefacts find their way into these features through a number of actions, including deliberate placement and accidental loss. Structures of this type often contain a number of backfill or deposition events and are typically excavated using a combination of machine excavation and hand excavation at depth. Accumulated deposits are often useful for soil and pollen analysis.

If a feature is found to extend to a substantial depth, complete excavation of the fill may not be possible due to Occupational Health and Safety requirements. In this situation fill would be removed to a safe depth to allow for the recording of the structure and collection of a representative stratified sample of any fill or artefacts. It is possible that further excavation or monitoring of particularly deep structures, such as wells, may be able to be undertaken by machine at a later date. As this would involve the removal of substantial amounts of soil, the archaeological program would need to have been finalised in the immediate vicinity to avoid disturbance to any archaeological remains or deposits.

Should any intact and deep structural features be encountered it may be necessary to remove any demolition or fill material within by mechanical excavation under the supervision of an archaeologist. Any material removed by excavator would be examined for artefacts by the archaeologists.

5.4.4.2 Occupation deposits

The study area has generally low potential to contain underfloor or occupation deposits that may have accumulated beneath flooring. Deposits of this type are sensitive and are investigated via a methodical system utilising grid squares, careful excavation with hand tools and sample sieving.

Intact underfloor deposits would be excavated in a grid system, either 50 centimetre or 1 metre depending on extent of deposit. Excavation would be by context if stratigraphic layers are identifiable. If the deposit is homogenised, excavation would proceed in 5 or 10 centimetre spits. Excavated material would be wet or dry sieved if required. The range and percentage of archaeological material collected would be in accordance with a sieving strategy developed by the Excavation Director and Site Director.

This type of investigation can recover data that may be utilised in the analyses of interior spaces and in the identification of activities within those spaces.

5.4.4.3 Open days

Excavation schedules and planning should allow for public access to the site where appropriate. Open days, site tours and school access should all be considered where state significant archaeological resources are identified and when safe access can be provided.

5.5 Unexpected finds procedure

Unexpected archaeological finds would be managed under the Sydney Metro Unexpected Heritage Finds Procedure. The procedure should be implemented during all excavation work on site not subject to direct management by on-site archaeologists.

5.6 Contaminated materials

Due to the potential for contaminants across the Pyrmont Station construction sites, archaeological excavation would be undertaken in accordance with the specified WH&S and environmental protocols established for the site, prior to the commencement of works on site. The archaeological team is reliant on contamination and hygienist contractors to provide prompt analysis of potential contaminants and detail contamination controls for safe excavation.

Should the discovery of contaminants on site likely result in the potential harm to archaeological staff, there may be a requirement to deviate from the proposed archaeological methodology, in order to ensure the health and safety of onsite staff. This response may include the use of protective clothing, face masks, and specified gloves, additional washing protocols, through to the need to cease hand excavation on site. It is noted that whole-of-body PPE (such as contamination suits) exacerbate heat stress in workers and the routine use of this PPE to excavate through contaminated deposits is not recommended – rather, where archaeological remains are identified in contaminated deposits, remote recording techniques are recommended over risking worker safety. However, basic contamination control PPE would be present with the archaeological team at all times (specifically, N2-masks for preventing inhalation of airborne asbestos fibres as well as impermeable gloves for handling soil).

The Excavation Director would provide justification wherever this proposed excavation methodology is deviated from in order to protect human health during archaeological investigation and outline the revised archaeological methodology for investigating specific remains.

Should the requirement to employ mechanical excavation rather than hand excavation arise, archival photographic recording of archaeological material would be conducted from a safe distance (as specified in the WH&S requirements of the remediation specialists). Should significant and robust archaeological remains be identified within contaminated deposits, opportunities for photogrammetry and orthography would be prioritised.

Should artefactual material located within bulk contaminated soil deposits be identified, the Excavation Director would consider opportunities for wet sieving of artefacts, within contamination PPE, depending on environmental and hygienist controls for managing run-off and disposing of contaminated soil. Artefacts which are composed of hazardous materials, or which have been embedded in known contamination areas (and which therefore may have absorbed harmful contaminants) would also not be collected if they are not deemed to be safe or for which storage of these items would be hazardous off-site.

Manual excavation is not anticipated at a depth where the water table is anticipated to be encountered with the exception of benched test excavation for reclamation fills within the station box

excavation area. In circumstances where ground-water is present during archaeological excavation, archaeological staff would follow all OH&S advice provided to contractor crews. All opportunities to avoid handling or excavating within contaminated ground water would be undertaken, including vacuum suction of contaminated water if possible and remote recording of exposed archaeological remains with photogrammetry or drone photography where possible.

5.7 Archaeological recording and documentation procedures

5.7.1 Introduction

Archaeological recording would be undertaken by allocating each stratigraphic unit a context number and completing a record of each context on a context sheet.

Mapping, planning and recording would be coordinated through a GIS system that would combine data from varying sources and present it in the form of maps.

The Excavation Director and the Site Director would complete a daily journal outlining the aims for the work to be done each day, what is achieved and what the next task is. In this way, the progress of the excavation and the day-to-day work and decisions are captured for reference if required, and to inform the preliminary and final excavation reports.

5.7.2 Survey Control

A survey control for the site would be established, tied to the Geocentric Datum of Australia (GDA) 2020. For preference, survey data would be recorded with a DGPS and post-processed to sub 1cm accuracy. Alternatively, a Total Station would be used to establish the survey and record survey data. An automatic level could also be used to record depths and tied to known datum points.

Due to the likelihood of staged and sequential works, a physical site grid for the whole of the site is not likely to be able to be established or preserved. Surveyed datums would be established at the commencement of each excavation program, with these datums measured either with a DGPS or total station survey. Hand-drawn plans would then be measured off these datums, with at least 4 datums used for measurement control.

Within an archaeological excavation area, the archaeological team would set out a grid for ease of recording and, where required, and establish main and subsidiary datums based on survey information. Further datums for vertical control will be established to allow all trenches to be surveyed in to a nearby datum. These will be tied back to Australian Height Datum and the survey grid.

Where electronic surveying equipment is not available to the archaeological team, horizontal measurements and detailed scaled plans of excavation areas and features would be prepared. Vertical relative elevations would be taken with Dumpy level. These plans and levels would be tied to a previously surveyed main or subsidiary datum. Every level taken is assigned a number and is recorded on a level sheet (particularly for Dumpy readings).

Where dateable or otherwise special artefacts are located, they would be recorded in three dimensions with surveying equipment if available.

5.7.3 Recording of Contexts

All soil deposits and significant features would be given a unique context number without duplication. Context numbers will be recorded in a register of context numbers to ensure context numbers are not duplicated. Each context is numbered sequentially.

Rubble deposits would be recorded only where it provides specific information regarding masonry and construction (i.e., wall finishes, material etc.). Fills need to be described in detail as there are varying types of fills (e.g., demolition, levelling).

Contexts would be related to each other through the use of a Harris Matrix. The relationships between each of the contexts are recorded on the context sheet and these are also recorded in Stratify, a computer program used for producing Harris Matrices.

5.7.4 Recording of Archaeological Features

Archaeological features would be recorded through the preparation of plans and sections. Structural elements, such as brick walls or timber posts, would be recorded in situ to observe phases in construction, and then removed in stratigraphic sequence.

Plans and sections will be labelled with details of what is being recorded, context numbers and details of the recorder. Each plan, map or section will be catalogued and receive a number which is put on the plan and in the catalogue. The plan, map or section will be placed flat in an artist portfolio.

Plans need four control points on each plan that can then be surveyed in to georeference the plan. All records of vertical sections would include elevation data to ensure accurate measurement of stratigraphic layers at the site. Excavation open areas of significant features would include elevation levels throughout site, recorded either with a DGPS or total station, or with a dumpy level measured off surveyed datum control points for the site. The surface level and end of excavation elevation levels for all test excavation trenches, and all salvage excavation areas, would be recorded.

5.7.5 Photography

All photographic recording would be carried out in accordance with *Photographic Recording of Heritage Items Using Film or Digital Capture* (Heritage Office 2006), accepting that parts of these guidelines are technically obsolete. The team would use a digital SLR camera and shoot in raw format to capture the maximum amount of information from the camera sensors. The photograph number and direction would be recorded, and a description provided for all photographs and the locations from which each image was taken would be recorded on a site base plan. Photographs would be recorded in a register identifying the shot number, direction and a description of the scene.

The archaeological team should record the site using digital techniques such as photogrammetry and/or laser scanning. This will ensure that an augmented site archive will be available for creative digital interpretation and that interactive reconstructions can be produced during content development. In photographically recording archaeological features, the practice would be to shoot to the requirements for photogrammetry, which includes accurate scale bars, overlapping of images and recording with a colour card where required.

5.7.6 Collection of Artefacts

Artefacts would be collected and bagged with reference to the soil deposit's context number. Each artefact bag or individual artefact would be tagged with a Tyvek tag and sealed in plastic bags. Delicate materials such as bone or metal may need to be stored in a paper bag before being taken off site for conservation. All artefact bags would be catalogued prior to being stored in an archival quality storage container to prevent loss or misattribution of contextual data.

Retained artefacts would be cleaned, processed, catalogued and analysed by an archaeologist experienced in historical artefact assemblages. Artefact analysis would include production of a database in accordance with best practice archaeological data recording. The resulting information would be included in the final excavation report.

5.7.6.1 *Artefact discard policy*

It is not considered feasible to collect all artefactual remains from an archaeological site. Often artefacts lack stratigraphic integrity or represent a large number of almost identical items (such as bricks) or may be hazardous materials (such as asbestos cement sheeting).

Artefacts that are manufactured from hazardous material such as asbestos or may be contaminated due to being in contaminated deposits, would not be collected to minimise danger to human health. These items would be catalogued and recorded, and then disposed of in an appropriate manner according to contractor site protocols for managing hazardous and contaminated spoil.

Artefacts from fills and other bulk deposits that lack stratigraphic integrity would be recorded in the field without collection. A record of artefacts would be made to be included with the context record for the fill or deposit. Should diagnostic or significant artefacts be present within the fill layers (out-of-context), a sample would be retained as part of the archaeological record. Any discarded items would be recorded on context or discard sheets (in the case of sieving).

Building materials and other ubiquitous and non-diagnostic infrastructure items such as bricks or tiles would not be collected due to their limited diagnostic value and difficulty in storing and accessioning. Rather, a type collection would be collected for reference purposes if required, with records made in contextual records of the presence of these materials. If required, samples of building materials may be collected for further analysis.

5.7.7 Long term management of recovered artefacts

Archaeological remains collected and analysed from archaeological investigation would be stored safely by Sydney Metro following the completion of analysis of remains. Opportunities for artefacts to be interpreted would be considered in the Heritage Interpretation Plan for Pyrmont. Should recovered archaeological artefacts or material be considered unstable for long-term storage, assessment of their condition, integrity and significance would be undertaken in the first instance. This assessment should be undertaken by the artefact specialist and Excavation Director. Opportunities for interpretation should also be considered during the preparation of the assessment.

Should artefactual remains be considered to be suitably significant to justify long-term storage and conservation, a specialist conservator would be engaged. The material would be stabilised and stored securely.

Should unstable artefacts not be considered suitable for long-term storage and/or display, they would be subject to the discard policy outlined above.

5.8 Clearance

On immediate completion of each stage of archaeological investigation i.e. monitoring of the eastern construction site, testing of the western construction site or any salvage required, a brief clearance letter would be provided by the Excavation Director indicating next steps for the location or clearing the area for project works.

5.9 Preliminary results reporting

Preliminary archaeological results reports would be prepared for Sydney Metro within a month of the completion of each stage of archaeological investigation i.e. monitoring of the eastern construction site, testing of the western construction site or any salvage required.

These reports would be prepared using Sydney Metro templates and would outline the main archaeological findings, post-excavation and analysis requirements.

The preliminary results reports would also identify opportunities for interpretation as per the Heritage Interpretation Plan. Recommendations for the use of materials in design development would be provided if appropriate. The preliminary report would also provide high level information on any significant finds that might be suitable for incorporation into heritage interpretation planning.

5.10 Post-excavation analysis and reporting

Following the completion of on-site archaeological works, post-excavation analysis of the findings would be undertaken. This includes artefact analysis, environmental and building material sample analysis, stratigraphic reporting and production of Harris Matrices, production of detailed site survey plans, illustrations and interpretative drawings, generation of catalogues, data records and site registers.

Artefacts would be catalogued and analysed in a robust database in accordance with the EAMC (Exploring the Archaeology of the Modern City) catalogue architecture and methodology to facilitate inter-site artefactual comparative analysis.²⁷¹ The use of artefact recording techniques such as photogrammetry and laser scanning of significant or diagnostic finds to allow for use in digital interpretation would be considered and incorporated where appropriate.

A final excavation report detailing the archaeological program and results would be prepared. It would include the results of the archaeological excavation and analysis, additional historical information if needed, photographs, illustrations and plans, catalogue and analysis of artefacts, and also respond to the research questions in detail. The report would also include a reassessment of archaeological significance based on the investigation results.

Final excavation reporting would be prepared within 12 months of the completion of all archaeological investigation at the construction site, in accordance with relevant CoA for the project. Sydney Metro post-excavation reporting templates would be used for post-excavation reports. This report would be a standalone report. The report would be submitted to the Planning Secretary, Heritage NSW and the City of Sydney Council when it is completed.

5.11 Interpretation

There is the potential for recovered artefacts as well as final archaeological reporting results to be provided for future public interpretation opportunities. Archaeological records – particularly survey and photogrammetry – would be provided in the preliminary archaeological reporting and would be considered for use in future public interpretation products to be developed for the site.

The following interpretive requirements for the archaeological component of the project have been summarised from the draft Heritage Interpretation Strategy prepared for Stage 1 of the project by Extent Heritage:²⁷²

- During Archaeological Excavation
 - Excavation schedules and planning should allow for public access to the site where appropriate. Open days, site tours and school access should all be considered where

²⁷¹ Crook and Murray, 2006. *Guide to the EAMC Archaeology Database*. Archaeology of the Modern City Series, Volume 10. Historic Houses Trust of New South Wales.

²⁷² Extent Heritage Pty Ltd, March 2022, draft Heritage Interpretation Strategy, p.71

state significant archaeological resources are identified and when safe access can be provided (see Section 5.4.3.3)

- Archaeology teams should record the site using digital techniques such as photogrammetry and/or laser scanning. This will ensure that an augmented site archive will be available for creative digital interpretation and that interactive reconstructions can be produced during content development (see Section 5.7.5).
- Post Excavation
 - During design development consideration should be given to the post excavation schedule to allow for future use of materials. This may include setting aside areas for future displays based on preliminary excavation reporting, in the event that artefacts are not yet ready for incorporation (see Section 5.9)
 - Preliminary archaeological reports (submitted within three months of the excavation program being completed) will provide high level information on any significant finds that might be suitable for incorporation into heritage interpretation planning (see Section 5.9)
 - Artefact analysis should consider the use of recording techniques such as photogrammetry and laser scanning on significant or diagnostic finds, to allow for use in digital interpretation (see Section 5.10).

It is noted that the following archaeological recommendations of the draft Heritage Interpretation Strategy should be considered during the design project:²⁷³

- Installation
 - Archaeological objects, and potentially features, should be displayed on site where appropriate, practical and relevant to do so
 - Design of displays will need to consider the ongoing protection and conservation of archaeological materials. Appropriate display cases suitable for conservation, stabilisation of materials and security should be considered during the design stage and the advice of conservation experts should be sought
 - Ongoing maintenance, condition assessment, cleaning should be considered during installation and a management plan may be required to manage displays during once installed
 - Interpretation of archaeological remains should not necessarily be confined to physical remains. Digital and online interpretation should also be considered in instances where evidence is significant, but not suitable for display, and where animations, 3D renders and other solutions could enhance audience experience.

²⁷³ Extent Heritage Pty Ltd, March 2022, draft Heritage Interpretation Strategy, p.71



artefact

Artefact Heritage
ABN 73 144 973 526
Suite 56, Jones Bay Wharf
26-32 Pirrama Road
Pyrmont NSW 2009 Australia
+61 2 9518 8411
office@artefact.net.au
www.artefact.net.au