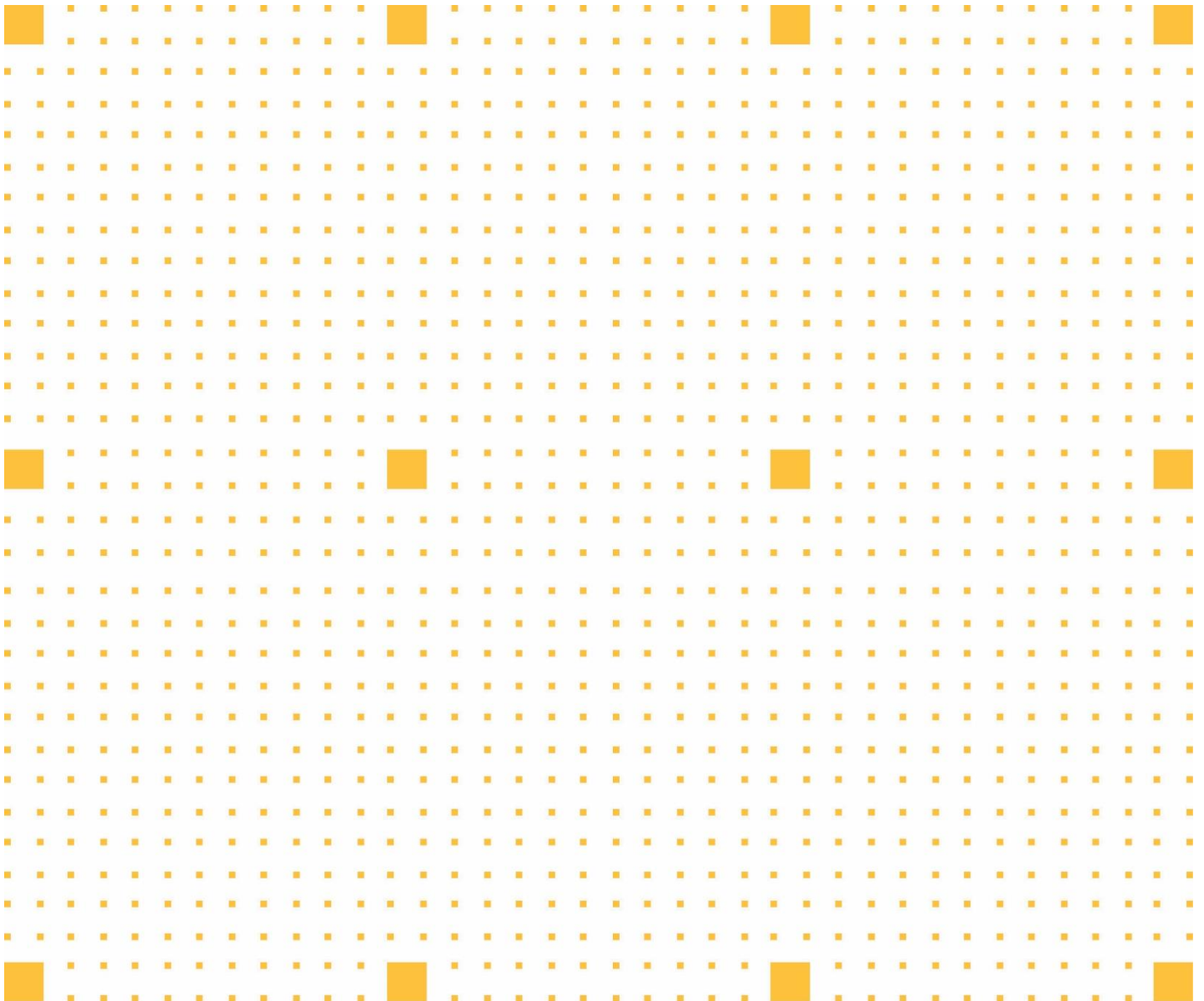


Traffic Management Plan

Project: new Sydney Fish Market Early Works

Job No: SC132



Rev F | Jan 2021

Uncontrolled Document in Hard Copy

Copies shall not be made without the written
permission of Hansen Yuncken Project Manager

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1 Introduction

1.1 Review & Approval

Refer to Project Management Plan Responsibility Matrix for traffic management responsibility, input and approval

Position	Name	Sign	Date
Review			
Project Manager	Daniel Yarrow		
Site Supervisor	Peter Dworacek		
HSE Manager	Tim Redmond		
Engineer / Project Administrator	Daniel Cessario		
Approval			
Construction Manager	Dean Marcon		

1.2 Revision history

Rev.	Date	Description of amendments	Author	Checked
0	Mar 2020	TENDER	PS	DY
A	Aug 2020	Issue for Consultation Review – Authorities	DY	DY
B	Aug 2020	IFC – Authorities Comments addressed	DY	DY
C	Sep 2020	IFC w DPIE	DY	DY
D	Oct 2020	IFC w T&T comments	DY	DY
E	Jan 2021	IFC w Cycleway Information	RP	DY
F	Jan 2021	IFC w TfNSW Approval	RP	DY

1.3 Definitions & Abbreviations

The following definitions and abbreviations have been used in this Traffic Management Plan. Further definitions and abbreviations are provided in referenced procedures and plans:

CORP	Hansen Yuncken Corporate
HSE	Health, Safety & Environment
HY	Hansen Yuncken Pty Ltd
PLN	HY Plan
PPE	Personal Protective Equipment
PR	Procedure
S/C	Subcontract(s) or Subcontractor(s) as the context requires

1.4 Legislation, Standards & Codes of Practice

Traffic shall be controlled in accordance with either of the following, depending on the contract or site conditions and requirements.

- **AS 1742.3** Manual for uniform traffic control devices, Part 3 – Traffic control devices for works on roads
- **SAA HB81.1 to HB81.6** Field guides for traffic control at works on roads. Part 1 to Part 6 cover various examples of work on different roads and under different conditions

2 Traffic Management Requirements

2.1 Traffic Management

The traffic management will be carried out and maintained by Liberty Industrial.

See Liberty Industrial's Construction Pedestrian and Traffic Management Plan – Appendix 3.1

2.2 Signs

The purpose of road signing or work site protection is:

- to provide a safe work area to work within; and
- to safely move traffic through, around and past a work site with minimum inconvenience.

2.3 Traffic Controllers

Only competent persons who possess the relevant state certification shall be appointed as traffic controllers and when a traffic management plan is to be implemented, they must possess the relevant competency to implement, and or audit and design the traffic management plans dependent on the competencies obtained.

2.4 Signage Placement or Modification

HY Site Manager is responsible to ensure that the placement of temporary signs and their location is placed as per Traffic Control Plan by a qualified Traffic Controller.

Any worker setting up temporary traffic control or modifying permanent traffic controls or directing traffic must have signed a SWMS which has been reviewed by Hansen Yuncken.

The traffic controllers must be wearing the required PPE for the activity which is required to be nominated in the SWMS.

Any existing signs that do not apply shall be covered as per the approved traffic management plan.

2.5 Pedestrian Paths

Paths shall be safe and at least 1.2 metres wide.

2.6 Maintenance of Existing Traffic Flow

Existing traffic flows shall be maintained and only modified for short periods when other alternatives have been exhausted.

2.7 Site Access

The main entrance to the site shall be from **Bridge Rd, Pyrmont**. Warning signs will be placed along approximately 150m from the main entrance in both directions to warn traffic that vehicles will be crossing.

The access points into the site are indicated on the attached Traffic Control Plan along with Traffic Control Devices which will be put in place for the duration of the project and Temporary Traffic Control which will take place from time to time to bring in long or wide loads for items such as Structural Steel and Roof Sheet.

All truck drivers will be directed to turn left out of site at Bridge Road gates using a forward movement. Entry to site will be via left in forward movements where possible. Access via Gate 2 will require a right in movement using the existing movements into the former Hanson Concrete Batch Plant gates adjacent to Wentworth Park Road. A qualified Traffic Controller will stop cyclists/pedestrians whilst vehicles enter and exit the site. Additional signage will be placed at entry and exit points. No parking is permitted on Bridge Road or neighbouring side roads.

2.7.1 Exiting Site

All traffic exiting the site will exit via the designated exit points onto Bridge Rd, Pyrmont.

2.7.2 Entering Site

Traffic that is to enter site will enter via the designated entry points off Bridge Rd, Pyrmont

2.7.3 On Site Traffic Management

All on site traffic management will be managed through the Daily Pre-Start Meetings.

2.8 Traffic Management Report

During the operation of a Traffic Guidance Scheme, a daily Traffic Management Report shall be completed using the *Traffic Management Report Checklist* in BIM360 or equivalent report by the Traffic Management Subcontractor. The Subcontractors Traffic Management Report must be supplied to the Site Manager for future reference.

During the operation of a Traffic Guidance Scheme, daily routine tasks shall be undertaken in accordance with Appendix A of 1742.3;

- Before Work Starts.
- During Work Hours.
- Closing Down at the end of the day.
- After hours.

2.9 Special Deliveries

Any trucks that are long or wide loads will have specific traffic management in place to control traffic on Bridge Rd, Pyrmont. These loads, depending on RTA requirements, may require support vehicles or police escorts.

3 Consultation with Authorities / Stakeholders

3.1 TfNSW - Sydney Coordination Office

From: Kaye Russell <Kaye.Russell@transport.nsw.gov.au>
Sent: Wednesday, 19 August 2020 11:48 AM
To: Daniel Yarrow <D.Yarrow@hansenyuncken.com.au>
Subject: FW: new Sydney Fish Market - Early Works | Traffic Management Plan Consultation

Morning Daniel

Thank you for providing TfNSW with the Construction Pedestrian Traffic Management Plan (CPTMP) for 1A, 1B & 1C Bridge Road Glebe.

Several construction projects, including the Sydney Light Rail Project are likely to occur at the same time as this development within the CBD. The cumulative increase in construction vehicle movements from these projects could have the potential to impact on general traffic and bus operations in the CBD, and the safety of pedestrians and cyclists within the CBD particularly during commuter peak periods.

TfNSW raises the follow matters that should be addressed –

- Section 4.5 make mention of no bus services along Bridge Rd – this is in correct. There are 2 school routes (692s & 740s) that operate along this section of roadway as does rail replacement services(1L1). Please ensure that these services are not impacted by the proposed works.
- City of Sydney Council and TfNSW have planned the installation of a pop-up cycleway along Bridge St to assist commuters to safely travel during Covid 19 restrictions. The date of installation is yet to be confirmed but as you can see from the attached plan the route will travel along the frontage of your site. The cycleway along the development frontage will not include any klem-flex devices and therefore will not impact or egress from the site. However measures should be taken to ensure safety of cyclists using the facility.
- Traffic controllers present at vehicle crossover points will need to manage interactions with pedestrians and cyclists.

Can you please confirm the above issues have been addressed in the TMP and provide me with an updated response.

If you would like to discuss any of these issues please call me on the number below.

Regards

Kaye Russell
Transport Planning Project Manager
Sydney Coordination Office
Transport Coordination, Greater Sydney
Transport for NSW

M 0435 961 672
Level 44 550 George Street Sydney NSW 2000

Comments	Action
Section 4.5 make mention of no bus services along Bridge Rd – this is incorrect. There are 2 school routes (692s & 740s) that operate along this section of roadway as does rail replacement services(1L1). Please ensure that these services are not impacted by the proposed works.	Liberty plan Section 4.5 updated to include
City of Sydney Council and TfNSW have planned the installation of a pop-up cycleway along Bridge St to assist commuters to safely travel during Covid-19 restrictions. Installation is expected to commence on the 10 th of January 2021 and the works will be undertaken at night, with a forecasted two-week duration. The route will travel along the frontage of the site. The cycleway along the development frontage will not include any klem-flex devices and therefore will not impact or egress from the site. However, measures should be taken to ensure safety of cyclists using the facility.	Liberty plan Section 3.6 updated to include

Comments	Action
Traffic controllers present at vehicle crossover points will need to manage interactions with pedestrians and cyclists.	Liberty plan Section 3.6 updated to include

3.2 TfNSW - Sydney Community & Place Office



C/O Jared Abrahamse
13 Verrell Street
Wetherill Park NSW 2164
Sent via email to jared@atstraffic.com.au

Important information about access to the construction site

Dear Jared,

Transport for NSW write to inform you of changes to the road environment on Bridge Road that will affect access arrangements to and from the construction site on Bridge Road, Glebe.

As you may be aware, Transport for NSW and the City of Sydney have been building pop up cycleways, as part of our response to the Covid-19 pandemic.

In September we completed the Bridge Road Cycleway, it now has close to 3,000 people a week utilising this infrastructure, reducing traffic and allowing people to commute more safely. Currently, the cycleway stops before Taylor Street and cyclists continue along on the roadway.

Transport for NSW with the City of Sydney, have finalised a design for the continuation of the cycleway, with plans for a two way cycleway located on the north side of Bridge Road, between Taylor Street and Wattle Street. At the Taylor Street intersection, cyclists will be able to cross and join the established cycleway.

Critical requirements

As part of this infrastructure, access to and from the construction site will require traffic controllers to undertake traffic management during the site operations.

All vehicle movements within the site, for entry and exits, must be under traffic control arrangements, with controllers stopping cyclists while vehicles enter and exit.

This traffic management will manage cyclists on the cycleway, along with pedestrians on the footpath and on-road traffic as normal.

It's our understanding that this is required in any event, as part of the development consent conditions. ***Transport for NSW and the City of Sydney will monitor these arrangements and ensure traffic control is in place, while site operations are underway.***

Transport for NSW
27-31 Argyle Street, Parramatta NSW 2150 | Locked Bag 5085, Parramatta NSW 2124
P (02) 8265 6962 | W transport.nsw.gov.au | ABN 18 804 239 602

Construction Details

The cycleway extension will be completed by the end of January, with work expected to commence on the 10th of January, subject to change. Work will be undertaken during nights, forecast currently for two weeks duration.

This will minimise interference with the ongoing work on site, enabling you to go about your operation as normal. Should any night work be planned on site that may conflict with this schedule, please contact our contractor, VBAJV, as soon as practicable.

All the pop-up cycleways will be monitored and evaluated for their uptake, use, and safety. While a decision has been made to implement the cycleway, we are still open to feedback from the community.

I understand Network Safety Officer, Nic Kocoski, has been in contact with you, to explain this in more detail. However, to avoid confusion, as workers come back in 2021, it may be best to circulate this advice to all.

It would also be helpful to remind people of cycling and pedestrian use near the site, with its confluence of active transport users from nearby. We encourage the community to utilise healthier ways to get to and from the city, and your site is no exception.

For additional information, please contact Nic Kocoski, Senior Manager, Network and Safety. 0437 049 189, alternatively contact myself on my details below.

Please pass this information on to the principal contractor / client to notify them of these changes.

Kind regards,



Tyson Kinnane
Communications & Stakeholder Engagement Officer
Community & Place
Greater Sydney
Transport for NSW

M 0407 282 589
Level 9, 27-31 Argyle Street Parramatta NSW 2150

Transport for NSW

27-31 Argyle Street, Parramatta NSW 2150 | Locked Bag 5085, Parramatta NSW 2124
P (02) 8265 6962 | W transport.nsw.gov.au | ABN 18 804 239 602

Comments	Action
All vehicles movements within the site, for entry and exits, must be under traffic control arrangements, with Traffic Controllers stopping pedestrians and cyclists in the cycleway while vehicles enter and exit.	Liberty plan Section 3.6 updated to include

3.3 TfNSW - Approval of TMP

From: Joshua Powell <Joshua.Powell@transport.nsw.gov.au>
Sent: Thursday, 14 January 2021 1:23 PM
To: Daniel Yarrow
Cc: Kaye Russell
Subject: RE: new Sydney Fish Market - Early Works | Traffic Management Plan Update - Consultation
Attachments: SFMS1-LIB-EN-PLAN-CPTMP-Rev02.pdf

Good afternoon Daniel,

Thank you for providing TfNSW with an updated Construction Pedestrian Traffic Management Plan (CPTMP) for the new Sydney Fish Market redevelopment.

I am satisfied that you have addressed the issues identified by Kaye dated 19 August 2020 and request no further changes.

TfNSW has endorsed the attached CPTMP. Please ensure this CPTMP is shared and adhered to by all contractors. If the CPTMP changes, please forward a copy to TfNSW for further review and endorsement.

TfNSW welcomes ongoing discussions on any issues that may arise during the development relating to traffic and transport.

Kind regards,
Josh Powell
Customer Journey Planning
Operations
Greater Sydney
Transport for NSW

M 0466 523 526
Level 8, 231 Elizabeth Street NSW 2000



4 APPENDICES

4.1 Liberty Industrial Construction Pedestrian and Traffic Management Plan



New Sydney Fish Market Stage 1

Construction Pedestrian and Traffic Management Plan

Prepared by
Liberty Industrial Pty Ltd
For

Hansen Yuncken Pty Ltd

1A, B, and C Bridge Road, Glebe NSW 2037

HANSEN YUNCKEN

Revision No.	Revision Date	Authority	Changes
A	31.07.2020	DD	Draft For Consultation
B	21.08.2020	MD	Information regarding bus routes on Bridge Rd. Information on pop-up cycleway.
0	11.09.2020	DR	First Issue
01	06.01.2020	SZ	Amendment made in regards to bike lane construction work along Bridge Road

PREPARED: Mohamad Darwish Date: 30 - 07 - 2020

Project Engineer

ACCEPTED: Antoine Delort Date: 11 - 09 - 2020

Project Manager

Specialist Deconstruction Services

■ Industrial demolition contractors ■ Mine closure consulting ■ 3D Modelling

■ Demolition consultants ■ Asbestos abatement

Liberty Industrial Pty Ltd A.B.N. 99 147 758 487



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1 INTRODUCTION

1.1 PURPOSE

The intended purpose of the Construction Pedestrian and Traffic Management Plan (CPTMP) is to describe how Liberty Industrial will implement the work in accordance with the requirements of the Project Approvals for the approved development SSD8924 for 1A,1B and 1C Bridge Road, Glebe (lots 3,4,5 DP 1064339).

1.2 SCOPE

This scope of work is for the demolition of existing buildings, structures wharves and jetties on the site including:

- Erection of perimeter fencing, hoarding and scaffolding (as required), site accommodation and environmental controls;
- Establishment of temporary access and pedestrian arrangements (where required);
- Services verification, relocations and installation of selected temporary services including capping and removal of in-ground services, diversions and terminations;
- Demolition of structures
- Localised remediation works;
- Selected early civil works (temporary works, drainage and other in ground services);
- Make good works to the existing sea wall and provision of revetment structures as required.

1.3 OBJECTIVES

The overall principles of pedestrian and traffic management during the New Sydney Fish Market Stage 1 works program include but are not limited to:

- Provide a convenient, safe and appropriate environment for pedestrians.
- Minimise effects on pedestrian movements and amenity.
- Manage and control vehicular movements to and from the site.
- Maintain current on street parking in the vicinity of the site where practical.
- Maintain access to other properties adjacent to the site.
- Restrict vehicle activity to designated truck routes in the vicinity of the site.
- Maintain safety for all workers, visitors and sub-contractors.
- Maintain appropriate access to the site for excavation and construction traffic.

2 LEGAL AND OTHER REQUIREMENTS

2.1 LEGISLATION

Legislation relevant to construction pedestrian and traffic management Includes:

- Roadworks Manual VO4
- *Roads Act 1993*
- *Transport Administration Act 1988*
- Work Health and Safety Act 2011;
- Work Health and Safety Regulation 2011;
- Occupational Health and Safety Act 2000;
- Occupational Health and Safety Regulation 2001;
- Australian Standard 2601-2001 Demolition of Structures

2.2 REQUIREMENT MATRIX

2.2.1 Table 1-Minister Conditions of Approval

Table 1 - Minister CoA

CoA No	Condition Requirement	Document Reference
Part B – Prior to Commencement of Stage 1 Demolition Works		
B14	Where construction/building works require the use of a public place including a road or footpath, approval under section 138 of the <i>Roads Act 1993</i> for a Barricade Permit. Road Occupancy Licence is to be obtained prior to the commencement of work. Details of the barricade construction, area of enclosure and period of work are required to be submitted to the satisfaction of the relevant road authority.	Where required, permits will be obtained and this plan updated.
B15	A separate application under section 138 of the <i>Roads Act 1993</i> is to be made to the relevant road authority to erect a hoarding and/or scaffolding in a public road (if required) and such application is to include: <ul style="list-style-type: none">• Architectural, construction and structural details of the design as well as proposed artwork;• Structural certification prepared and signed by an appropriately qualified practising	Where required, permits will be obtained and this plan updated where required for the erection of hoardings within the work area.

	<p>structural engineer.</p> <p>Evidence of the issue of a Structural Works Inspection Certificate and structural certification will be required prior to the commencement of works on site.</p>	
B16 i)	<p>Prior to the commencement of works, the Applicant shall prepare and implement an updated Construction Environmental Management Plan (CEMP) for the development and be submitted to the Certifier. The CEMP must:</p> <ul style="list-style-type: none"> d) detail how the environmental performance of the site preparation and construction works will be monitored, and what actions will be taken to address identified potential environmental impacts, including but not limited to noise, traffic and air impacts 	<p>This document forms part of the CEMP for the Stage 1 works</p>
B17	<p>Prior to the commencement of works, a Construction Pedestrian and Traffic Management Plan (CPTMP) shall be prepared in consultation with the Sydney Coordination Office within TfNSW. The CPTMP shall address (but not be limited to) the following:</p> <ul style="list-style-type: none"> (a) A description of the development; (b) Location of any proposed work zone(s); (c) Location of any crane(s); (d) Haulage routes; (e) A detailed plan identifying all construction vehicle access arrangements; (f) Estimated number of construction vehicle movements, including measures to reduce the number of movements during the AM and PM peak periods; (g) Measures to avoid construction worker vehicle movements; (h) Construction program; (i) Proposed construction hours; (j) Consultation strategy for liaison with surrounding stakeholders, including 	<p>This document:</p> <p>Section 1, 3</p> <p>Figure 1</p> <p>NA</p> <p>Section 3.4.1</p> <p>Appendix A TCP's</p> <p>Section 4.9</p> <p>Section 4</p> <p>Section 4</p> <p>Section 3.2</p> <p>Section 4.11</p>

	<p>other developments;</p> <p>(k) Any potential impacts to generate traffic, cyclists, pedestrian and bus and light rail services within the vicinity of the site from the construction of the development;</p> <p>(l) Cumulative construction impacts of the development: Bays Market District, Sydney Metro City and Southwest, Western Harbour Tunnel and Westconnex. Existing CPTMPs for developments within or around the development site should be referenced in the CPTMP to ensure that coordination of work activities are managed to minimise impacts on the road network; and</p> <p>(m) Proposed mitigation measures. Should any impacts be identified, the duration of the impacts and measures proposed to mitigate any associated general traffic, public transport, pedestrian and cyclist impacts should be clearly identified and included in the CPTMP.</p> <p>Prior to the commencement of works, details demonstrating compliance with the above requirements (Condition B18 (a)-(m)) must be submitted to the Certifier. A copy of the CPTMP must be submitted to the Certifier, TfNSW (RMS) and the Planning Secretary.</p>	<p>Section 4.9</p> <p>Section 4.11</p> <p>Section 5</p> <p>The CPTMP was submitted and approved by the certifier on XXX</p>
B19 d) and e)	<p>Prior to the commencement any construction (including demolition), a Waste Management Plan (WMP) must be prepared and submitted to the Certifier. The WMP must:</p> <p>d) require that the body of any vehicle or trailer used to transport waste or excavation spoil from the Subject Site, is covered before leaving the Subject Site to prevent any spill, or escape of any dust, waste, or spoil from the vehicle or trailer;</p> <p>e) require that mud, splatter, dust and other material likely to fall from or be cast off the wheels, underside or body of any vehicle, trailer or motorised plant leaving the Subject Site, is removed before the vehicle, trailer or motorised plant leaves the Subject Site.</p>	<p>WMP is part of CEMP. These matters are addressed in Section 5 of this document also.</p>

B32	<p>Prior to the commencement of works, the Applicant is to engage a suitably qualified professional(s) to prepare a Pre-Construction Dilapidation Report detailing the current structural condition of all retained and existing and adjoining building, infrastructure and roads with the zone of influence. The report must be submitted to the Certifier prior to the commencement of works. A copy of the report must be forwarded to the affected landowners.</p> <p>The Pre-Construction Dilapidation Report must also include a photographic recording of the public domain site frontages. The recording must include clear images of the foreshore, footpath, nature strip, kerb and gutter, driveway crossings and laybacks, kerb ramps, road carriageway, street trees and plantings, parking restriction and traffic signs, and all other existing infrastructure along the street. The form of the recording is to be as follows:</p> <ul style="list-style-type: none"> a) a PDF format report containing all the images that clearly demonstrates the existing site conditions; b) each image is to be labelled to identify the elements depicted, the direction that the image is viewed towards, and include the name of the relevant street frontage; c) each image is to be numbered and cross referenced to a site location; d) a summary report, prepared by a suitable professional, must be submitted in conjunction with the images detailing the project description, identifying any apparent existing defects, detailing the date and authorship of the photographic record, the method of documentation and limitations of the photographic record. 	
Part C – During Demolition Works		
C2	<p>Construction, including the delivery of materials to and from the site, may only be carried out between the following hours:</p> <ul style="list-style-type: none"> (a) between 7.00 am and 5.30 pm, Mondays to Fridays inclusive; and (b) between 7.30 am and 3.30 pm, Saturdays. 	Section 3.2

C4	Activities may be undertaken outside of these hours if required: (a) by the Police or a public authority for the delivery of vehicles, plant or materials; (b) in an emergency to avoid the loss of life, damage to property or to prevent environmental harm.	Section 3.2
C5	Notification of such activities must be given to affected residents before undertaking the activities or as soon as is practical afterwards.	Section 3.2, 4.11
C34	All vehicles involved in the excavation and / or demolition process and departing from the property with materials, spoil or loose matter must have their loads fully covered before entering the public roadway.	Section 5
C35	Prior to the commencement of work, suitable measures are to be implemented to ensure that sediment and other materials are not tracked onto the roadway by vehicles leaving the site. It is an offence to allow, permit or cause materials to pollute or be placed in a position from which they may pollute waters.	SWMP, CPTMP Section 5
C41	Unless otherwise authorised, the public way on Council owned land must not be obstructed by any materials, vehicles, refuse skips or the like, under any circumstances. Non-compliance with this requirement will result in the issue of a notice by the Planning Secretary to stop all work on site.	Section 3.3
C42	Any damage to the public way, including trees, footpaths, kerbs, gutters, road carriageway and the like, must immediately be made safe and functional by the Applicant.	Section 5.1
Part D – Post Completion of Demolition/Construction Works		
D3	All public footways, paving, sub-surface infrastructure, kerbs, gutters and road pavement damaged during the works are to be immediately repaired following the damage, to a satisfactory state that provides for safe use by pedestrians and vehicles.	

D4	<p>Post completion of the works:</p> <ul style="list-style-type: none"> a) the Applicant must engage a suitably qualified person to prepare a post-construction dilapidation report. This report must ascertain whether the construction works created any structural damage to adjoining buildings, infrastructure and roads. b) the report is to be submitted to the Certifier. In ascertaining whether adverse structural damage has occurred to adjoining buildings, infrastructure and roads, the Certifier must: c) compare the post-construction dilapidation report with the pre-construction dilapidation report required by these conditions; d) have written confirmation from the relevant authority that there is no adverse structural damage to their infrastructure and roads; and e) a copy of this report is to be forwarded to the Certifier, Council, the Planning Secretary and each of the affected property owners. 	Post Construction delay
Advisory Notes		
AN2	The Applicant shall apply to the relevant authority for all necessary permits including crane permits, road opening permits, hoarding or scaffolding permits, footpath occupation permits and/or any other approvals under section 68 (Approvals) of the Local Government Act 1993 or section 138 of the Roads Act 1993.	Appendix D contains all permits relating to these requirements.
AN3	A Road Occupancy Licence (ROL) should be obtained from Transport Management Centre for any works that may impact on traffic flows on the surrounding state road network during construction activities. A ROL can be obtained through https://myrta.com/oplinc2pages/security/oplincl oqin.isf .	Appendix D

3 PROJECT OVERVIEW

3.1 SITE INTRODUCTION

The New Sydney Fish Market development once completed will involve an improved public domain including the creation of a waterfront promenade with improved access to Blackwattle Bay and linking to surrounding areas and to public transport. The development will include wholesale facilities and auction rooms, offices and commercial space, culinary education (the Sydney Seafood School), retail premises including food and beverage premises (potentially with liquor licenses), back-of-house facilities and car and delivery vehicle parking. The new facility is to include a new bayside promenade

and wharves. The new fish market will be purpose built and will be supported by state of the art back-of-house plant and recycling/waste management facilities.

Works are also proposed to Bridge Road to provide improvements to its design and operation including improvements to the intersection of Bridge Road with Wattle Street and Wentworth Park Road.

The development will be undertaken in stages, with the first stage of the development the demolition of land and water-based structures on the site including removal of marine piles. This is the subject of this management plan.

The site is currently occupied by the following buildings and improvements:

- A finger jetty that protrudes from the main wharf;
- Within the northern area of the central portion of the site a wharf deck consisting of asphalt applied to reinforced concrete supported by timber beams and turpentine piles;
- Within the southern area of the central portion of the site a post tensioned slab supported by wharf beams and steel piles; and
- A single storey office building.
- The eastern portion of the site comprises the former Jones Brothers Coal loader and bins and weighbridge building.
- The site includes the existing SFM wharf and outdoor dining area which is located along the foreshore on the western side of the main fish market building.

The scope of works for Stage 1 involves:

- erection of perimeter fencing, hoarding and scaffolding (as required), site accommodation and environmental controls;
- establishment of temporary access and pedestrian arrangements (if required);
- Services verification, relocations and installation of selected temporary services including capping and removal of in-ground services, diversions and terminations;
- Localised remediation works;
- Selected early civil works (temporary works, drainage and other in ground services);
- Make good works to the existing sea wall and provision of revetment structures as required.

Demolition equipment required for the project will include:

- Excavators to demolish buildings and above ground structures fitted with hydraulic shears and hammer attachments
- Concrete saws and coring drills used to break the wharf structure into smaller sections for removal
- Excavators to load trucks
- Rigid body trucks to remove materials from the site including skip bins
- Electric and hydraulic hand tools (drills, hammers, welding equipment, oxy acetylene cutting equipment) to separate wastes and other building materials
- Barges and cranes to remove timber supports and piles
- Hoses and pumps to control water

- (b) 2.00 pm to 5.00 pm Monday to Friday; and
- (c) 9.00 am to 12.00 pm, Saturday.

Further approvals would be required for work outside of these hours and the process for gaining approval is specified in the CNVMP.

3.3 SITE ACCESS AND WORKZONES

Access to the construction site will be via the existing driveways and gates to the site located off Bridge Road as highlighted on Figure 2.

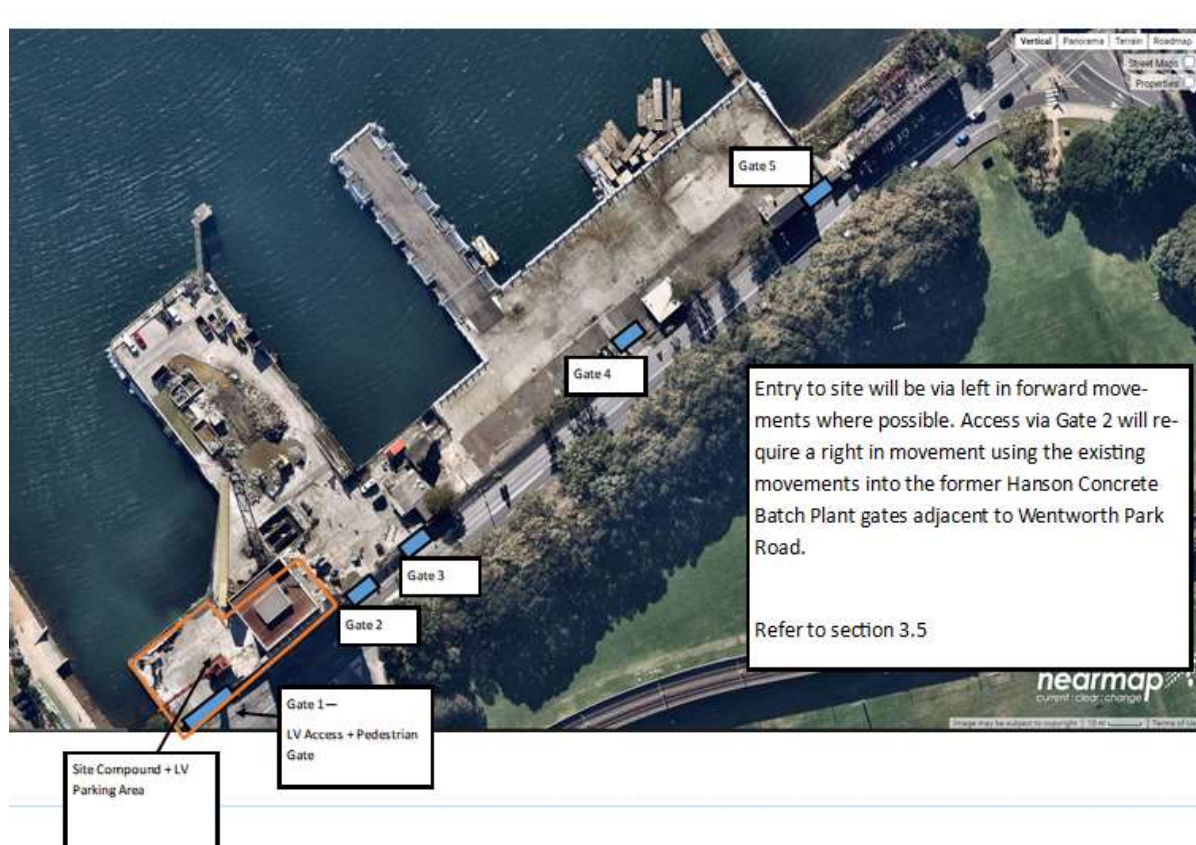


Figure 2 – New Sydney Fish Markets Stage 1 Works - Location of access points to the site

On-street work zones are not envisaged to be required on Bridge Road to facilitate the demolition works. The public way, on Council owned land, must not be obstructed by any materials, vehicles, refuse skips or the like, under any circumstances. A hardstand area north of the existing Bridge Road footpath (see Figure 3) will be utilised to store construction vehicles where required. A site hoarding will also be established when required, to separate this hardstand work zone when established from the adjacent Bridge Road footpath so not to impact the safety of pedestrians in the area as outlined in Figure 3. Traffic controllers will be present at the vehicle crossover points to manage interactions with pedestrians.

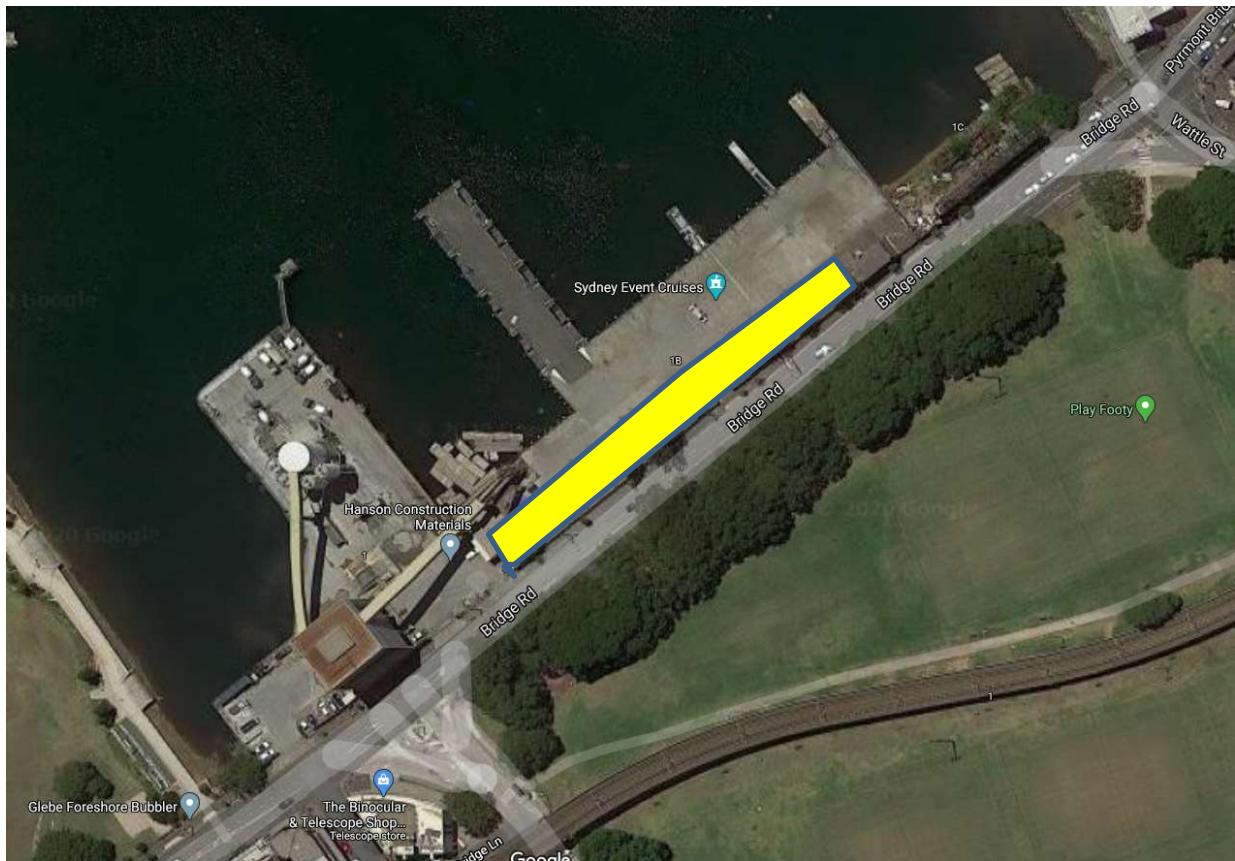


Figure 3 – New Sydney Fish Markets Stage 1 Works - Indicative location of hardstand area for construction use

Detailed traffic control plans (TCPs) will be prepared prior to the commencement of work on site and are contained in Appendix A.

3.4 TRAFFIC MANAGEMENT PLAN AND MONITORING PROCESS

Daily inspections on all traffic management set outs will be carried out by the site supervisor in line with CPTMP. Regular Safety Audits will be carried out throughout the project and reported accordingly through the project reporting requirements.

The site Supervisor will schedule vehicle movement to stagger movements through a “call up” system to prevent Heavy vehicle queuing on Bridge Road and local residential streets outside the Project boundaries.

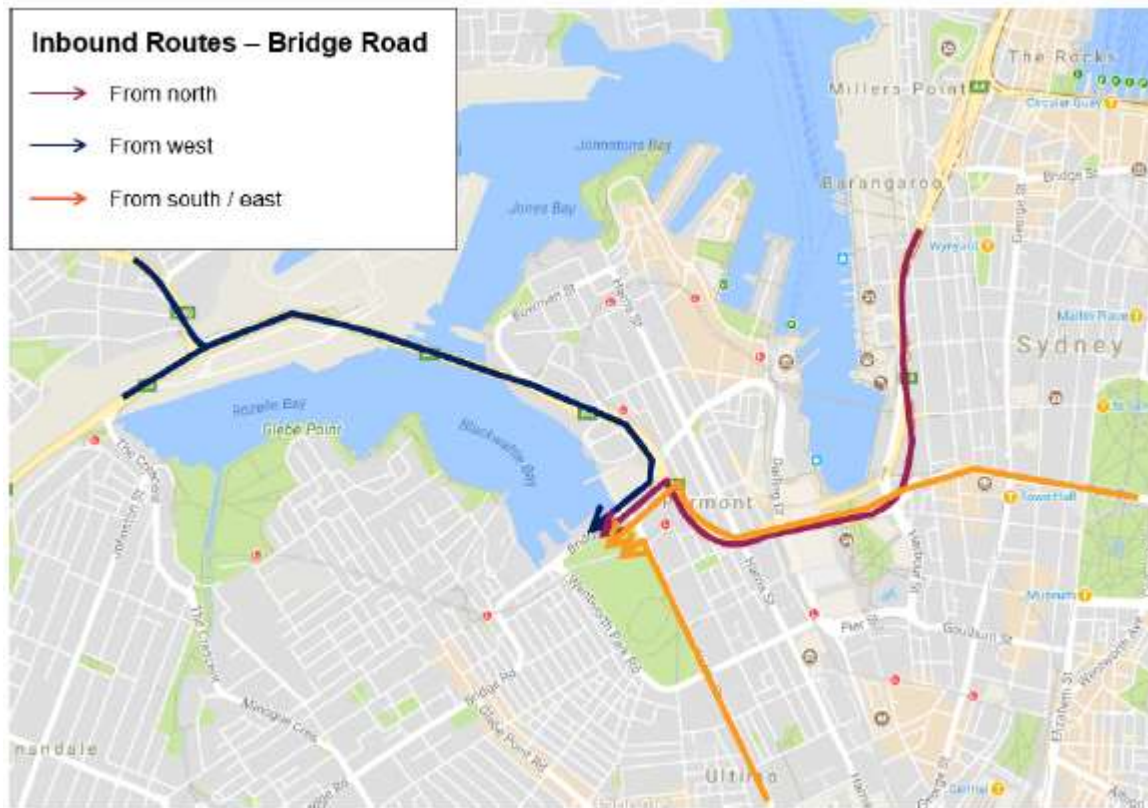
3.4.1 Construction Traffic routes

The main construction access will be via the state road network including the Western Distributor. Vehicles will likely originate from this network. It is expected that the majority of trips will likely be generated from the west and will access the site via either Victoria Road or the City West Link.

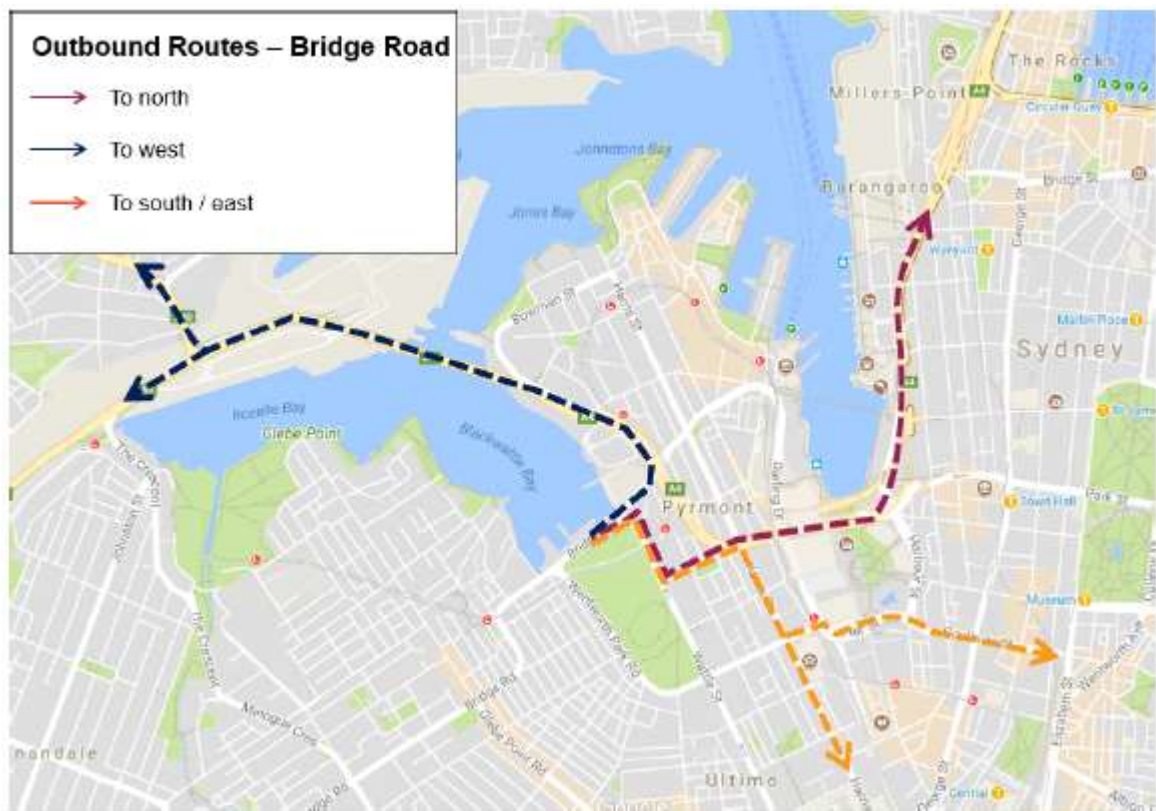
Trips from the east and south may access the site via the Cross-City Tunnel and Western Distributor, while those arriving from the north will use the Sydney Harbour Bridge and Western Distributor. Wattle Street and Harris Street will provide an alternative route for vehicles arriving from and departing to the south.

The primary access point to the site will be via the intersection of Wentworth Park Road and Bridge Road.

For part of the construction period, trucks may arrive and depart from Glebe Island, with materials barged across to the construction site. Primary access to this construction compound will be via James Craig Road, with similar arrival/departure routes to the construction access via Bridge Road used.

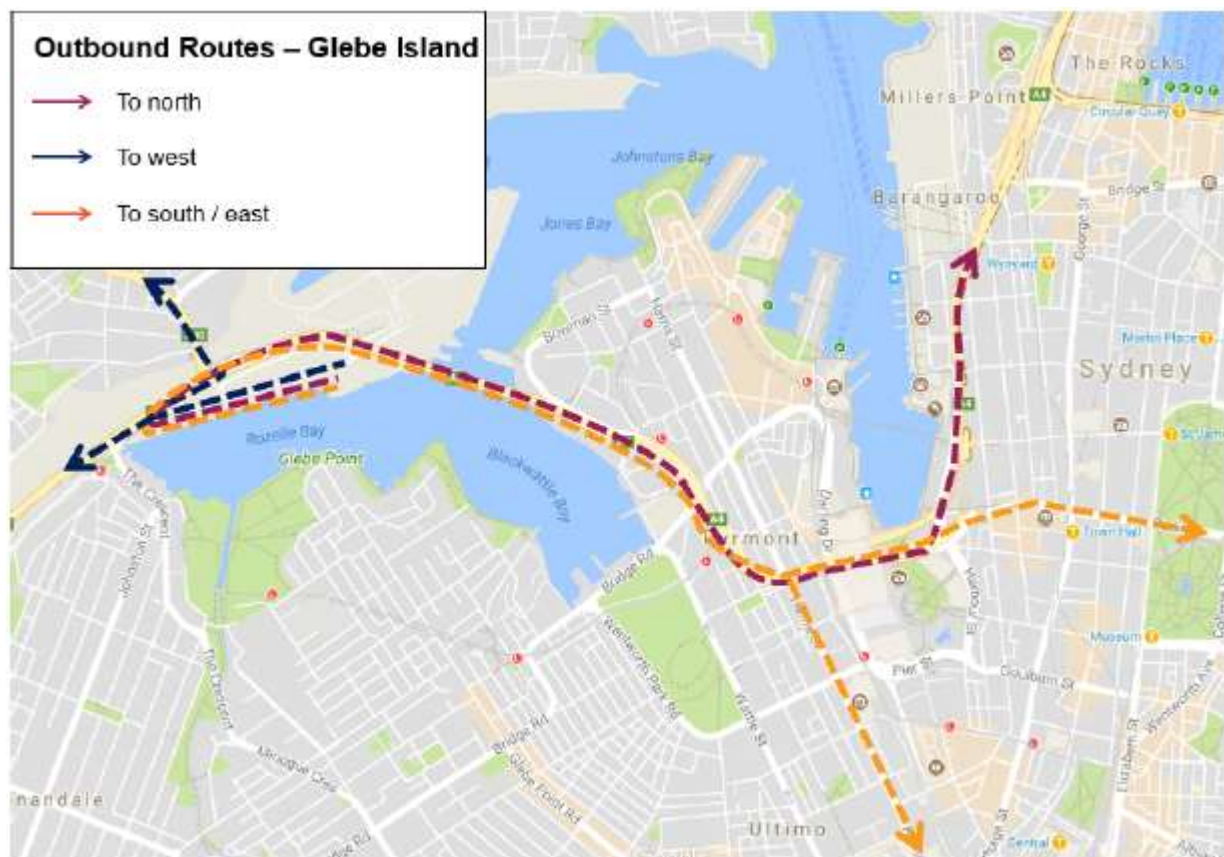


Source : EIS Appendix 11 TIA (SLR 2019)



Source : EIS Appendix 11 TIA (SLR 2019)





Source : EIS Appendix 11 TIA (SLR 2019)

Vehicles that will access the site during construction will likely mainly comprise of heavy vehicles including Articulated Vehicles (AV) such as precast delivery trucks and Heavy Rigid (HR) such as truck and dog. These different types of vehicles may access the site at the same time.

Other heavy machinery plants such as cranes will have to be delivered to site in the preliminary stage. All heavy goods such as girders or machinery plants are likely to be delivered outside of peak traffic hours.

3.5 INTERNAL COMMUNICATION PROCESS

Signage shall be provided to inform drivers of the access restrictions and locations in line with this CPTMP. In addition, all heavy vehicle transport companies will receive a copy of the site-specific traffic requirements, including access points along Bridge Road, site specific PPE requirements and a copy of the Traffic Control Plan (TCP) Appendix A.

All truck drivers will be directed to turn left out of site at Bridge Road gates using a forward movement. Entry to site will be via left in forward movements where possible. Access via Gate 2 will require a right in movement using the existing movements into the former Hanson Concrete Batch Plant gates adjacent to Wentworth Park Road. Additional signage will be placed at entry and exist points. No parking is permitted on Bridge Road or neighbouring side roads.

This information is included in the project induction and will be reiterated in pre-start meetings and tool box talks.

3.6 WORKSITE ACCESS

All access including entry and exit to the work site will be from existing gates on Bridge Road. All trucks will enter and exit the site in a forward motion only, no reversing will be permitted onto the site. Resident access will not be affected during works.

City of Sydney Council and TfNSW have planned the installation of a pop-up cycleway along Bridge Rd to assist commuters to safely travel during Covid-19 restrictions. The route will travel along the frontage of the site. The cycleway along the development frontage will not include any klem-flex devices and therefore will not impact or egress from the site. However, measures will be taken to ensure safety of cyclists using the cycleway.

Traffic controllers will be present at vehicle crossover points to assist in the management of vehicles entering/exiting the site, pedestrians and cyclists.

Establishment and enforcement of appropriate on-site vehicle speed limits (<20km/h), would be enforced and sign posted.

3.7 LOADING AND UNLOADING AREA

Loading and unloading of material and equipment (large or small) will be done within the boundaries of the site. There will be no loading and unloading of materials outside the site boundaries. There will be a dedicated hardstand area for unloading, loading and storing of materials on site. Authorised traffic controllers will be required for movements into and out of the site to ensure motorists, cyclists and pedestrian safety, details are provided on TCP's in Appendix A.

3.8 SITE OFFICE LUNCHROOM AND TOILETS

Site office, lunch room and toilets will be located within the property boundaries.

4 IMPACT OF WORK

4.1 PUBLIC CAR PARKING

Control measures will be put in place to prevent impacts on public parking during the works. To ensure minimal impact on local parking the following shall apply:

- Parking will be made available within the site for all early works personnel and vehicles;
- Trucks and delivery vehicles will load/unload on site;
- Certified traffic controllers will be made available for all major truck movements and deliveries, closing off the footpath and opening when deemed safe to do so; and
- Labourers where possible will car pool or use public transport to and from the site each day.

4.2 WORKERS/TRADESPERSONS PARKING

All construction workers will park within the Project site boundary. However, it is encouraged for all workers to car share or use public transport when possible to minimize the impact on surrounding streets.

Should vehicle parking be required on-site, it would be undertaken within the site boundaries. The site supervisor will be responsible for vehicle movements on site at all times.

4.3 OUT OF HOURS DELIVERIES

Out of hours deliveries will only be permitted as stated below in line with development consent (SSD8924) condition C4:

- by the Police or a public authority for the delivery of vehicles, plant or materials;
- in an emergency to avoid the loss of life, damage to property or to prevent environmental harm.

Where works are required to be undertaken outside of normal hours, which may involve traffic changes, affected stakeholders would be provided with 48 hours' notice (or as soon as practical after becoming aware of the need for the works) in line with the Community Consultation Strategy. A procedure for approval of OOHW is contained in the NVMP.

4.4 PEDESTRIANS/CYCLISTS

Some pedestrians and cyclists within the area may be impacted from walking past the site during construction, particularly along Bridge Road. Traffic controller with appropriate accreditation will manage pedestrians and cyclist at cross-over points when site vehicles are entering or exiting the site. Signage will be erected as required in TCP's including the examples in Figure 4. Temporary A/B Class hoardings, site fencing and gates will be installed on all Bridge Road access points to the boundaries of the extent of the project site area. Site accommodation areas will be established on site and temporary hoarding and signage will be adopted in all working areas at all times. Temporary closure of the adjoining footpath may be required during erection of class B hoarding in this area.

Ref Appendix A – TCP-



Figure 4- Pedestrian Signage

4.5 PUBLIC TRANSPORT SERVICES

Whilst there are light rail stations at Wentworth Park, Glebe and Fish Market within 300 to 400m from the site, there also two school routes (692s & 740s) that operate along this section of roadway. As does the rail replacement service 1L1. These routes will not be impacted by the proposed works. Also, bus routes along Harris Street are available as well as through Glebe. Trips to the Project site using existing public transport will require between 500m to 1.5km walk to the site.

4.6 EDUCATION FACILITIES

There are three facilities in close proximity (<1km) to the site as shown on Figure 5. Drivers will be made aware that the probabilities of pedestrian activity including that of children may occur.



Figure 5- Educational Facilities in proximity to the site

4.7 EMERGENCY SERVICES

Emergency Services will be advised of the works as required. Access will be made available for Emergency Services at all times within the vicinity of the work site. All emergency services will have priority throughout the worksite. Upcoming works will be regularly discussed and distributed to all relevant stakeholders.

4.8 LOCAL RESIDENTS AND ROAD CHANGES

Any resident(s) affected by the construction works and or road changes will be notified in accordance with the community consultation protocols for the project.

No road changes are proposed as part of the stage 1 works.

4.9 TRAFFIC IMPACT ON LOCAL ROADS

Traffic impacts will be minimised with truck movements will also be scheduled and sequenced in general to avoid peak times where intersection performance is already at a poor LOS. In the case of oversize over mass truck movements, they will occur as directed by police and the TMC. These will mostly occur outside of normal construction hours and may require short stoppages of traffic in both directions along Bridge Road.

Construction Traffic volumes have been estimated in the EIS Appendix 11 and are expected to be a worst case. Stage 1 works have been reviewed and identified not to exceed these volumes.

4.9.1 Light Vehicles

Typically, the stage 1 works are likely to have a workforce of between 50 and 100 personnel. Given the public transport availability to the site and limited on-site parking opportunities, the majority of construction workers will be required to take public transport to work. Typically, construction workers have a high vehicle occupancy of between 2-3 people per vehicle, particularly for sites with constrained parking environments. Therefore, the likely number of light vehicles generated by the project would be in the order of 50 to 100 per day.

Additionally, construction workers generally start earlier and finish earlier than the commuter peak periods, and would likely not coincide with the site's peak periods.

4.9.2 Heavy Vehicles

The number of daily construction vehicles accessing the site is forecast to vary from between 16 to 60 daily vehicles. The forecast daily volume of construction vehicles for each stage of the works is outlined in Table 2.

Table 2 - Forecast daily construction traffic volumes

Stage	Duration (months)	Heavy Vehicle Number Estimates
Site Mobilisation and establishment	1	16
Demolition	7	36
Marine Construction	9	30

Source Transport Impact Assessment - ARUP 2019

On average over the life of the construction project, 45 construction vehicles per day are forecast to access the site. A maximum of 50 trucks per day are likely to access the construction site. This volume of traffic is commensurate with the existing level of traffic (both light and heavy vehicle) that currently access the concrete plants on the site of the future Sydney Fish Market.

From the above the hourly breakdown of vehicle movement contained in the approved project EIS (Appendix 11 Table 18) has been reviewed and is generally in accordance with the proposed construction traffic for the Stage 1 works.

Table 3 - Hourly construction vehicle movements

Construction Stage	0600-0700	0700-0800	0800-0900	0900-1000	1000-1100	1100-1200	1200-1300	1300-1400	1400-1500	1500-1600	1600-1700	1700-1800	1800-1900
Site mobilisation and establishment	0	3	1	1	2	2	2	2	1	1	1	0	0
Demolition	1	7	3	3	4	4	4	3	3	3	2	1	0
Marine Construction	0	5	2	3	3	3	3	3	2	2	2	1	0
Construction	1	9	4	4	5	5	5	5	4	4	3	1	1
Fit-out	1	7	3	3	4	4	4	4	3	3	2	1	1
Commissioning	0	2	1	1	1	1	1	1	1	1	1	0	0

4.10 NOISE

Truck operators will be made aware of their responsibilities in creating excess noise. Measures to be adopted include:

- Restricted arrival and departure times to ensure operations are contained within the approved hours of work;
- Turning off equipment when not required to be operational;
- No loud radios or shouting, radio contact to be used
- No use of horns to signify loading completed

If there are any noise complaints from neighbouring businesses, steps shall be taken by the Site Manager to reduce noise output or change the methodology of work creating the noise.

Further information is contained within the Construction Noise and Vibration Management Plan.

4.11 COMMUNITY ENGAGEMENT

5 MANAGEMENT MEASURES

5.1 MONITORING

Traffic monitoring during the early works will be undertaken, generally in accordance with daily heavy vehicle movements. Monitoring will include

- Daily pre-start visual inspections of vehicles to ensure vehicles are maintained in good working order and in accordance with manufacturer specifications.
- Daily inspection of Bridge Road will be maintained by the Area Supervisor, including:
 - Road condition daily;
 - Traffic interactions daily

- Complaints and incident register.
- Any damage to the public way, including trees, footpaths, kerbs, gutters, road carriageway and the like, will be made safe and functional as soon as practical following being identified
- Noise controls such as silencers and low-noise mufflers will be installed and maintained.
- Where reasonably practicable trucks with broadband alarms as opposed to beepers will be used as they provide an effective warning while being more directional as well as less bothersome to the surrounding community.
- All trucks would be fitted with tarps prior to leaving the site to control dust and debris.
- Where dust and materials are tracked onto Bridge Road, A street sweeper or equivalent method will be used to remove loose materials as soon practical after the site becomes aware of the issue.
- Access points will be maintained in a clean state to control loose materials leaving the site. Detailed controls would be identified in the PESCP as part of the Soil and Water Management Plan for the project.

5.2 COMMUNITY CONSULTATION STRATEGY

In accordance with Hansen Yuncken’s Stakeholder Management & Communications Management Plan, an approved community letter will be distributed to local residents and businesses within the area. The community letter will include information of works taking place including dates, times and project contact details for any information, comments or concerns.

5.3 MONITORING, REVIEW AND AMENDMENT OF THIS PLAN

Weekly management meetings will take place with all key parties ensuring safety issues and program schedules are reviewed and minuted. Any revision to plans or documentation will be noted in the revision table on the header page and this plan up revved.

6 CONSULTATION

This CPTMP has been developed in consultation with the relevant Council, emergency services, road user groups. A summary of consultation undertaken during the preparation of this CPTMP is provided in Table 4. Appendix B contains records of stakeholder consultation undertaken on the project.

Table 4 – Consultation for preparation of the CPTMP

Organisation	Date	Outcome
INSW		
DPIE		

Organisation	Date	Outcome
TfNSW		

7 STAKEHOLDERS

Liberty Industrial recognises there is a broad range of stakeholders with an interest in the development and the need to maintain an ongoing relationship with the community, throughout the delivery of the Stage 1 Works.

Hansen Yuncken's Stakeholder Management & Communications Management Plan will be followed when engaging with stakeholders and the public

8 INCIDENT MANAGEMENT

Detailed processes for responses to any traffic, construction or other incident is outlined within the Incident and Complaints Management Procedure. Please refer to the WHS Mgt Plan.

9 REPORTING

A weekly report will be produced to summarise all monitoring results. This report will be reviewed by the Project Manager.

10 AUDITING/ REVIEW

Audits and reviews (both internal and external) will be undertaken to assess the effectiveness of environmental controls and compliance with this plan, EPBC and SSD approvals and other relevant guidelines. The following elements may be included in the audit:

- Compliance with statutory obligations.
- Compliance with the Construction Environment Management Plan (CEMP) and CPTMP.
- Adequacy of monitoring and operational reports.
- Completion of environmental actions.
- Adequacy of environmental training records.
- Adequacy of environmental records, checklists and document management systems.
- Preparation of environmental reports.
- Recording and completion of corrective actions following environmental incidents and complaints.
- Achievement of environmental performance objectives.
- Implementation of actions from previous audits.

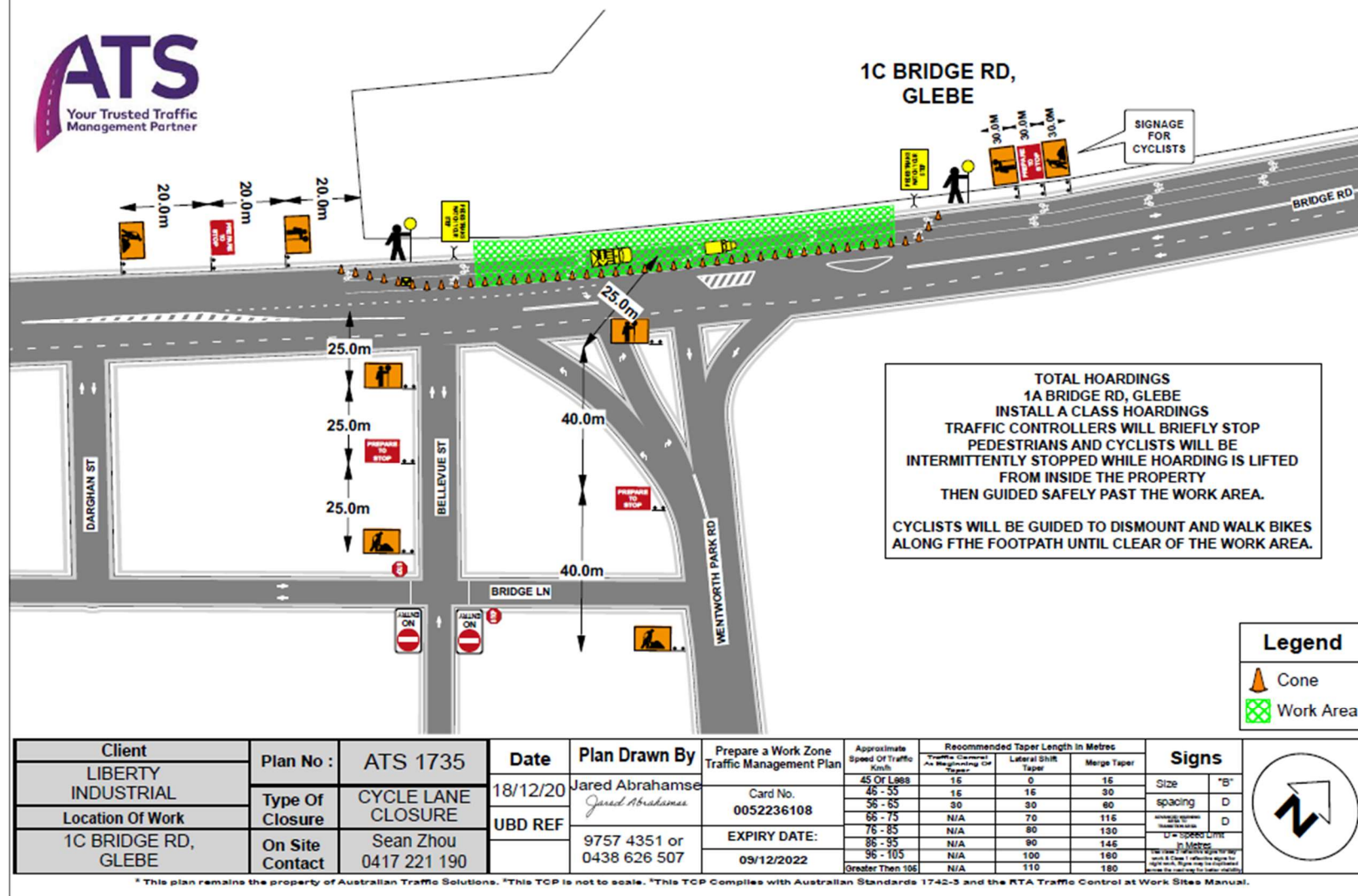
Audit/review of this plan will be part of an overall construction environmental management systems audit process defined in the CEMP.

Checklists used for regular monitoring of this plan are contained in Appendix C.

PPENDIX A - TRAFFIC CONTROL PLANS- TCP'S –



Figure 6 - Traffic Control Plans for general operating site conditions at 1a, b & c Bridge Road



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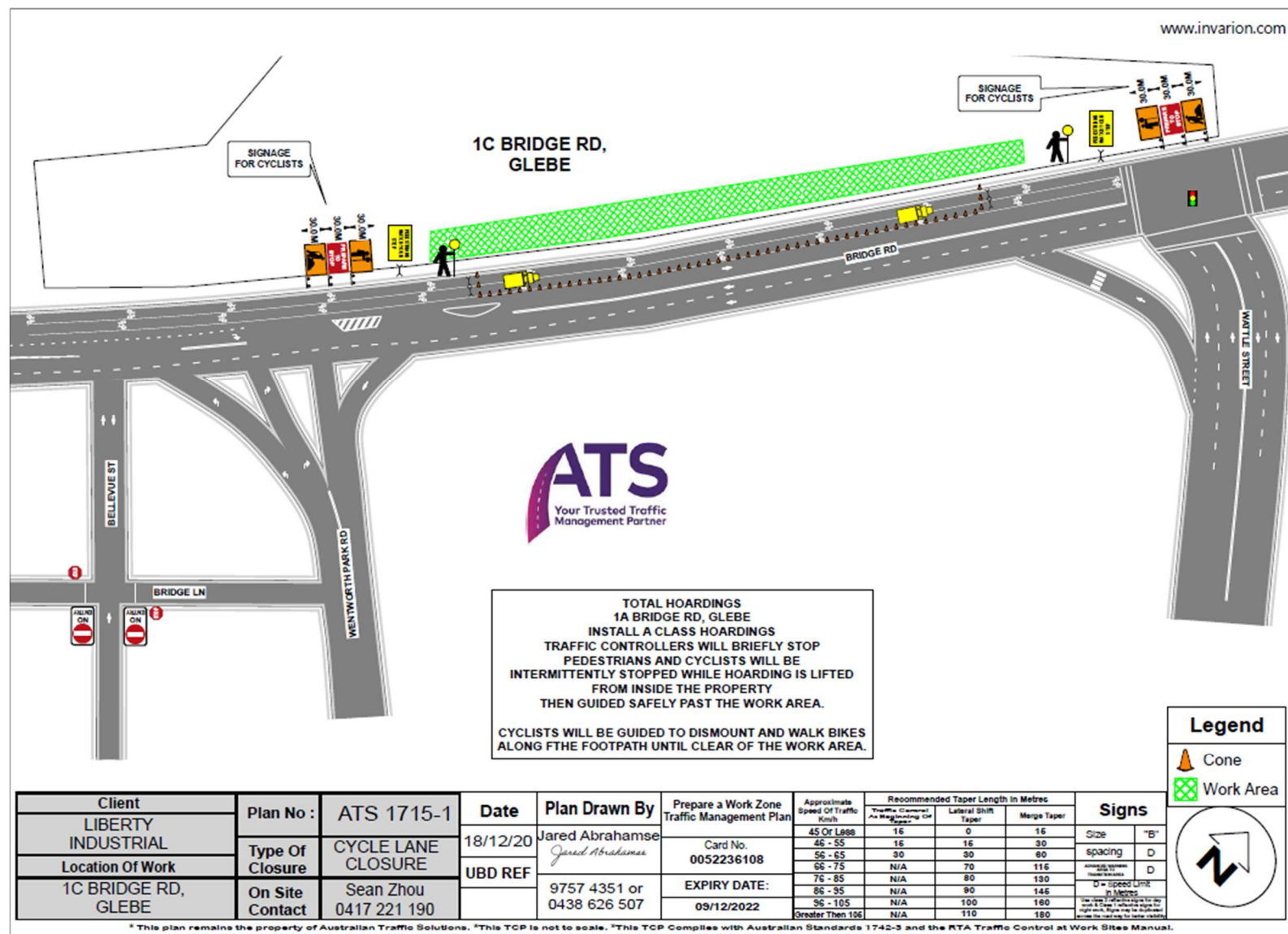


Figure 8 - Traffic Control Plan for work undertaking at 1b & c Bridge Road

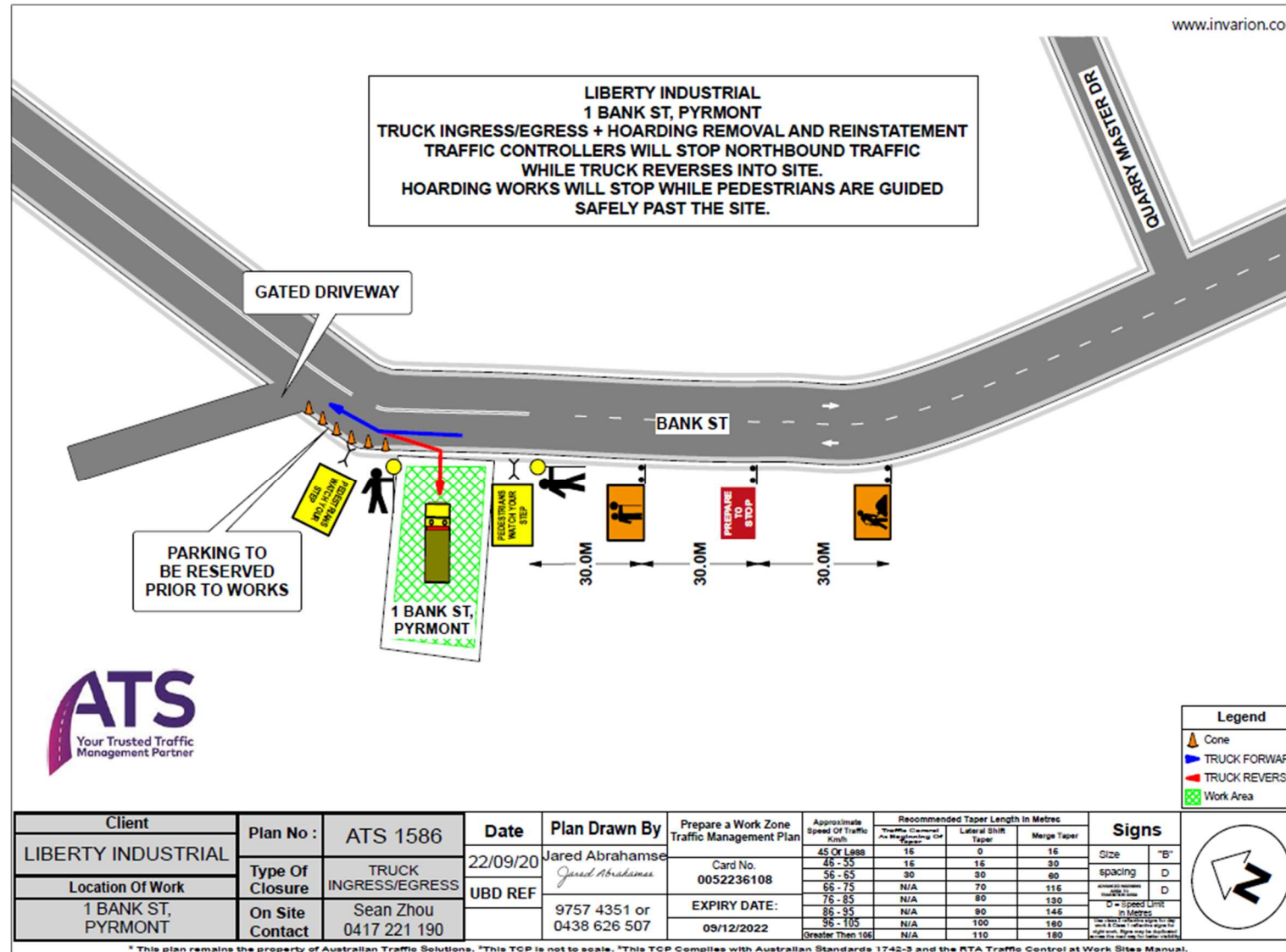


Figure 9 - Traffic Control Plan for work undertaking at 1 Bank Street

APPENDIX B – STAKEHOLDER CONSULTATION

New Sydney Fish Market - SSD 8924

XXX review comments

Co A	Comment	Proponent Response

Project: _____

Inspection Carried Out by: _____ Signed: _____

Date: _____ Area/Location: _____

Weather Conditions (Tick appropriate boxes):

Fine ☐ Light rain ☐ Heavy rain ☐ Light wind ☐ Strong wind ☐

ITEM	Y / N	COMMENTS
1. General Site		
Is the site in a generally tidy condition?		
Is all equipment, materials, etc contained within work area boundary?		
Are there any obvious signs of demolition/ remediation related disturbance outside of the demolition/ remediation area?		
Is the EMP readily accessible?		
Is an environmental incident response plan displayed in a prominent position?		
Is there an accessible complaints register?		
Is there documentation of any training undertaken since the last inspection?		
Is there minimal dirt on adjacent public roads?		
Have all required traffic control measures been implemented in accordance with the EMP (eg: warning signs, temporary road closures etc)?		
Is all demolition/ remediation plant parked on site?		
Are any private vehicles of demolition/ remediation personnel obstructing the passage of local traffic?		
Have local residence been notified 5 days prior to the commencement of works, for works outside of the normal working hours?		
Have local residents been notified 5 days prior to demolition/ remediation of activities that are likely to cause dust, offensive noise or access?		

ITEM	Y / N	COMMENTS
Are complaints being reported to the Principals Representative?		
Is the Complaints Register complete and have actions detailed been implemented?		
Is the access to any private properties being obstructed?		
Are pedestrian routes adjacent to site being obstructed (are appropriate alternative routes in place)?		
Has Environmental training been conducted over the last 3 months?		
2. Air Emissions		
Is dust suppression equipment readily accessible?		
Are there any obvious signs of dust deposition outside of demolition/ remediation area(s)?		
Is spoil being prevented from being tracked onto public roads?		
Are the haul roads being kept damp (if required)?		
Is the air quality monitoring equipment (if installed) operating correctly?		
Is there adequate procedures implemented for dust control?		
Is there stabilisation of stockpiles or erection of dust screens?		
Do any vehicles or machinery have visible exhaust for more than 10 seconds?		
3. Water Management		
Have required erosion control measure been correctly installed and are they functional? Check that there are/is; ○ no gaps in silt fences/barriers ○ no material lying across filter material or build up of silt no obvious signs of significant seepage through fences.		

ITEM	Y / N	COMMENTS
Are there any obvious signs of overflow from sediment detention basins?		
Are there obvious signs of uncontrolled drainage leaving the site?		
Are any materials, temporary structures/works in drainage lines?		
Where required, are drainage outlets provided with energy dissipaters to minimise erosion?		
Does water quality in down slope areas appear to be unaffected by demolition/ remediation works?		
Are diversion banks and drains located appropriately?		
Are there any apparent illegal discharges to sewers (cleaning of paint brushes, plaster, concrete)?		
Is the washdown of demolition/ remediation plant/vehicles restricted to a designated area (eg: truck wash out area)?		
Does the sediment basin require discharge? (10 days from last rain event?)		
4. Waste Management		
Is there appropriate documentation of any waste material disposed of offsite?		
Are waste receptacles accessible and clearly marked with regard to waste type?		
Is all recyclable material separated as per the waste management plan (are records available)?		
Are records of the type, amounts, date, transport, and disposal site of waste kept in a Waste Register?		
Do trucks removing material from the site have their loads covered?		
5. Hazardous Materials and Storage		
Are all hazardous materials (eg: fuels, chemicals etc) stored in an impervious bund which can contain 110% of the volume of the largest container stored in that bund?		
Are all hazardous materials stored in a covered area more than 20m away from waterways and drainage inlets?		

ITEM	Y / N	COMMENTS
Is the spill kit readily accessible?		
Is the on-site refuelling of demolition/ remediation plant restricted to a designated area more than 20m away from waterways and stormwater inlets?		
Are there any obvious signs of fuel spills, oil leakage, etc from demolition/ remediation plant) check both plant and ground?		
Are the relevant Safety Data sheets (SDS) available on site?		
Are containers labelled and stored correctly when not in use (i.e. in chemical storage areas or portable bunds)?		
6. Flora and Fauna Management		
Are there measures in place to minimise clearing eg: protective fencing, webbing, marked tape?		
Are all required vegetation protection measures in place and functional? TPZ fencing in place at locations #1, #2 and #3 along Moorebank Avenue?		
Is protection or delineation in place of Habitat Bearing Trees (HBTs)?		
Are staff trained to identify protected vegetation?		
Is there any historic evidence of soil and groundwater contamination?		
Is there evidence of noxious weed invasion?		
Has there been any soil and groundwater contamination surveys?		
Is there evidence of native fauna being encountered?		
Have any threatened Species been observed? (Grey Headed Flying Fox, Bent Wing Bat)?		
Nest boxes inspected and in good condition?		
7. Noise Control		
Is there documentary evidence that all required noise suppression measures have		

ITEM	Y / N	COMMENTS
been installed and operating in accordance with manufacturer's instruction and/or relevant environmental protection licence conditions?		
Is all noise monitoring equipment (if installed) operating correctly?		
Are all Plant/machinery switched off when not in use?		
Have the residents that are likely to be affected by offensive noise and/or vibration been notified?		
Have residents been notified of works to be undertaken outside of normal working hours?		
Have the siting of work areas, vehicle and plant parking areas, material stockpiles and equipment storage been arranged to minimise noise?		
Are there appropriate noise and vibration controls for activities adjacent to residents and other sensitive receivers?		
Are there any controls imposed on the Project by regulatory authorities?		
8. Resource Consumption		
Does the Project monitor water consumption?		
Does the Project monitor energy consumption?		
Are there any objectives and targets directed at resource consumption?		
Are there any recycling/reuse/redesign initiatives for products, materials and processes?		
9. Processing Areas		
Do stockpiles appear adequately maintained and managed (measures in place to prevent dust and soil run off)?		
Are there separate stockpiles for different material eg: ferrous/ non ferrous/ hand cut etc?		
Are any stockpiles located within the tree drip line (3m from tree base)?		
Are there dust control measures in place for the stockpile?		

ITEM	Y / N	COMMENTS
10. Demolition/ Remediation Areas		
Are areas where demolition/ remediation activities have ceased being stabilised and rehabilitated?		
Are any demolition/ remediation materials stored inside vegetation protection zones?		
Are there any obvious signs of demolition/ remediation activity within protected vegetation areas?		
Is contaminated land fenced off?		
Are disturbed areas stabilised?		
Are all required protection measures in place and functional?		
11. Heritage Management		
Are demolition/ remediation materials stored inside heritage protection zones?		
Are there any obvious signs of demolition/ remediation activity within protected areas?		

ACTION ITEM NO.	1. CORRECTIVE ACTION DETAILS	ACTION REQ'D BY	ACTION CLOSE-OUT DATE
1.			
2.			
3.			
4.			
5,			

2. Site Photos

SITE MANAGER:		DATE
AUDITOR:		DATE

APPENDIX D – PERMITS, LICENSES AND APPROVALS



29 November 2019

Australian Traffic Solutions Pty Ltd
13 Verrell Street
Wetherill Park NSW 2164

Attn: Michael Arthur
Operations Manager

Tel: (02) 9757 4351
Mob: 0414 963 997

Fax: (02) 9757 4301
Email: michael@atstraffic.com.au

REGISTRATION OF CONTRACTORS

Dear Sir,

I refer to your application for category G under the RMS Registration Scheme.

After the assessment, I would like to advise that your company has been registered with NSW RMS for:

Category G Provision of Traffic Control

The registration is valid for 3 years from the date of this letter and it will expire on 28 November 2022.

Yours faithfully,

Chris Martin
Manager Policy, Systems & Processes
Commercial Services Section
Technical & Project Services Branch
Infrastructure & Place Division