



Campbell's Stores
7-27 Circular Quay West, The Rocks
Statement of Heritage Impact



October 2015

Cover Image

Historic image - c.1904 Dawes Point
Source: NLA trove

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Executive Summary

The proposal for Campbell's Stores, a State Heritage Register item within *The Rocks Conservation Area*, represents a significant adaptive reuse of the building that will also signify a degree of urban renewal for the precinct.

Built by the Campbell family business in 1851-61, the Stores are a rare example of 19th century warehouse construction remaining on the shores of Sydney Cove.

Constructed of sandstone walls to the first two levels and brickwork for the later third level addition, the internal structure consists of hardwood columns and beams. The basic configuration is of ten gable fronted bays with slate roofing, and includes examples of industrial heritage.

The Campbell's Stores have evolved over 150 years to its current restaurant use and has reached the stage where another cycle of conservation works is required. The last thirty years have seen numerous restaurant fitouts that have obscured its integrity and impacted adversely on fabric condition.

The proposal for this DA submission, which has resulted from a consolidated and holistic design methodology, includes:

- Removal of identified intrusive elements
- Removal of internal fitout amenities and all services
- Conservation and reconstruction
- Installation of new amenities, kitchen infrastructure, lifts, stairs, voids and services
- A new free-standing canopy to a renewed landscaped forecourt to the east of the building
- Enlarged openings, including canopies, to the western elevation doorways of Bays 3, 6 and 9, as well as a glazed section of roof over the eastern end of Bay 6, as elements for a through site link from Hickson Road to the Campbells Cove foreshore
- The construction of a free-standing glass block clad building to the north end of the site, known as Bay 12.

The proposal has included integration with Public Domain initiatives, subject of separate DAs, which include:

- A Hickson Road upgrade and widening to allow for al fresco dining
- Reconfiguration of the stairway next to Bay 1
- The lowering of the promenade to be level with the Campbell's Stores' forecourt.

The proposal will provide significant benefit, not only for the conservation and sustained viability of Campbell's Stores, but will be instrumental in the revitalisation of the precinct within The Rocks.

Introduction

1.0

1.1 Background

This report has been prepared to accompany a development application for Campbell's Stores, Circular Quay West, The Rocks (Figure 1.1). The report evaluates the proposed development, designed by Johnson Pilton Walker (JPW), in terms of potential heritage impact on a NSW State Heritage Register item, as well as within *The Rocks Conservation Area*.

The general findings and analysis is that the project is taking a responsible, careful and creative approach to the conservation and reuse of the Campbell's Stores.

1.2 Report Objectives

The main objective of this report is to determine the suitability of the design and the heritage impact of the proposal in relation to the provisions established by the Sydney Harbour Foreshore Authority (SHFA) and by the NSW Office of Environment and Heritage, Heritage Division guidelines. The report will assess the proposal in terms of the Heritage Council endorsed *Campbell's Stores, The Rocks Conservation Management Plan* (CMP), prepared by Godden Mackay Logan (GML) in July 2014.

1.3 Methodology and Structure

This Statement of Heritage Impact has been prepared in accordance with guidelines outlined in the *Australia ICOMOS Charter for Places of Cultural Significance, 2013*, known as *The Burra Charter*, and the New South Wales Heritage Office (now the Heritage Division of the NSW Office of Environment and Heritage) publication, *NSW Heritage Manual*.

The Burra Charter provides definitions for terms used in heritage conservation and proposes conservation processes and principles for the conservation of an item. The terminology used, particularly the words *place*, *cultural significance*, *fabric*, and *conservation*, is as defined in Article 1 of *The Burra Charter*. The *NSW Heritage Manual* explains and promotes the standardisation of heritage investigation, assessment and management practices in NSW.

The term 'Stores' is used in this report as an abbreviation for Campbell's Stores.

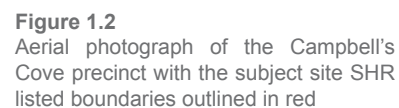


Figure 1.1
Location map showing the subject site circled in red

Source: street-directory.com

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The subject site is located on the east of Hickson Road (Figure 1.2), between the elevated roadway to the Overseas Passenger Terminal to the south, and the Hyatt Hotel to the north. The s170 listing refers to the site as 7-27 Circular Quay West. It is described by NSW Land and Property Information (LPI) as Lots 3-9 and Lot 12 in Plan Folio number DP 264534. The proposed works are confined to the area in the *Agreement for Lease*.



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1.5 Heritage Management Framework

The subject site at 7-27 Circular Quay West is listed as a heritage item under the *NSW Heritage Act 1977*:

- *Campbell's Stores*, State Heritage Register of New South Wales (Item 01536).
- *Campbell's Stores*, Section 170 of the SHFA Heritage Register as an agency of the NSW Government.

The Campbell's Stores are contained within:

- *The Rocks Conservation Area*, Section 170 of the SHFA Heritage Register

The subject item is not listed on Schedule 5 of the *Sydney LEP 2012* as an item of local heritage significance.

Accordingly the Minister of Planning is the consent authority, however SHFA are to be consulted, as the property owner, for any proposed DA submissions.

The property is a State Significant Development (SSD 7056) and is subject to the heritage provisions of the *NSW Heritage Act*, under the *Environmental Planning and Assessment Act 1979*. The consent authority must take into consideration the potential impact of any proposed development on the heritage significance of the heritage item.

1.6 Authorship

This report has been prepared by Garry McDonald, Senior Heritage Consultant, of Graham Brooks and Associates Pty Ltd and has been reviewed by the Director, Graham Brooks. Unless otherwise noted all of the photographs and drawings in this report are by Graham Brooks and Associates Pty Ltd.

1.7 Report Limitations

This report is intended as an initial commentary and is limited to a review of the European history of the site. Recommendations have been made on the basis of documentary evidence viewed, inspection of the existing fabric, and review of the current design documents to date.

This report only addresses the relevant heritage planning provisions and does not address general planning or environmental management considerations.

Archaeological assessment of the subject site is in the form of a separate report of both indigenous and non-indigenous archaeology prepared by *Austral Archaeology Pty Ltd*, and dated September 2015.

The author and Graham Brooks inspected the site on a number of occasions over the months from June to August 2015.

1.8 Copyright

Copyright of this report remains with the author, Graham Brooks and Associates Pty Ltd.

1.9 Reference Documents

In order to assess the potential heritage impacts on the proposed design by JPW, the following are the main documents consulted (refer to Section 8 Bibliography):

- Austral Archaeology Pty Ltd, *Aboriginal and Historical Archaeological Assessment, Statement of Heritage Impact & Research Design*, September 2015
- Design 5 Architects, *Condition Survey and Project Plan For Repairs to Building*, November 2005
- Design 5 Architects, *Investigations of Potential Causes of Ground Floor Rising Damp*, June 2008
- Design 5 Architects, *Stormwater Drainage Report Campbell's Storehouse*, December 2008
- Godden Mackay Logan, *Campbell's Stores, The Rocks Conservation Management Plan*, July 2014 (endorsed by Heritage Council 15/7/14). Reference has also been made to the previous CMPs by GML and dated 2011 (Adopted by SHFA March 2011), 2004 and 1996
- Jasper Swann, *Ground Level Stonework Conservation Report*, July 2011
- David West, International Conservation Services, *Campbell's Stores - Rising Damp Review*, July 2014
- Taylor Thompson Whitting, *Civil Engineering Schematic Design and Integrated Water Management Report*, August 2015

Historical Summary

2.0

2.1 Brief History of the Locality and Site

The following history is an outline of the subject site, and for a more detailed history refer to the CMP (2014) prepared by GML.

Sydney Cove has been occupied for millennia by the Darug language group of the Gadigal people, with the most recent and profound layers of change occurring within the last 200 years. Sydney Cove has been at the centre of this layering of the cultural landscape, with the foundation of the colony and from where development and exploration has emanated.

The place now known as Campbell's Cove, located on the western shore of Circular Quay, is easily recognised on the first published map of Sydney Cove (Figure 2.1). The April 1788 "sketch and description" is attributed to the convict, Francis Fowkes.¹

Robert Campbell, eventual merchant, pastoralist, politician and philanthropist, was born in Scotland in 1769 (Figure 2.3). The youngest surviving son of John Campbell, laird of Ashfield, Robert arrived in Sydney, via India, in 1798.² Rapidly assessing the potential of the newly established colony, he bought a parcel of prime water-front land on the west side of the harbour, and proceeded to set himself up as a trader.

The land where Robert Campbell built his first warehouses and wharfage facilities is located on Fowkes' map between the position of the General Hospital and Garden (CC) and Lieutenant Ball's House and Garden (BB). The development of Sydney Cove by about 1800 is also evident in a panorama of the western shoreline of Sydney town (Figure 2.2).



Figure 2.1
Francis Fowkes' 16 April 1788 sketch and description of the Settlement at Sydney Cove Port Jackson

Source: McCormick, Tim et al, *First Views of Australia: 1788–1825*, David Ell Press, 1987, p37.

¹ This map is the first published pictorial work relating to the English Settlement at Sydney. *First Views*, p. 37.

² Margaret Steven, 'Campbell, Robert (1769 - 1846)', *Australian Dictionary of Biography*, Volume 1, Melbourne University Press, (1966), pp 202-206

Beginning in 1801, and following his marriage in the same year to the eligible Miss Sophia Palmer, Robert Campbell began laying the foundations of what would become a thriving international trading business. He commenced with the construction of his family home (*Wharf House*), a series of storehouses, a stone retaining wall and a small wharf (Figures 2.4 & 2.5) The initial construction of his buildings would have involved the levelling or benching of the site, that is cutting into the sandstone topography.

Recognition by Governor Macquarie of Campbell's business acumen and valuable civic contribution to the infant colony led to Campbell being formally granted these 3 acres.³



Figure 2.2
Sydney Cove, c.1800, western shoreline, with Campbell's Cove on the right

Source: State Library of NSW, Mitchell Library, ML 443, artist unknown



Figure 2.4
1804 watercolour of showing Campbell's house, wharfage and retaining walls, plus the original stores

Source: Detail of an 1804 watercolour "possibly drawn by Eyre", *First Views*, p. 115.

³ The grant was dated 29 June, 1814. Conservation Plan (1996), Godden Mackay, p. 5, *Conservation Management Plan* (2004) Godden Mackay Logan, p. 5.



Figure 2.3
Robert Campbell (1769-1846), by an unknown artist

Source: *Australian Dictionary of Biography*

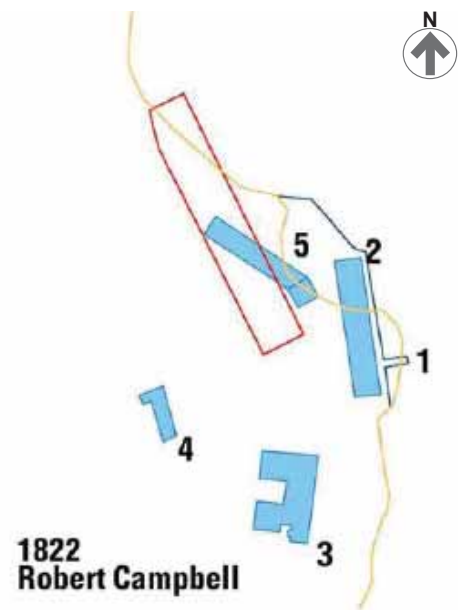


Figure 2.5
Stage 1: CMP figure with No 1 as the 1801 wharf; No 2, the 1801 storehouse; No 3, Wharf House; No 4, an unidentified building, and No 5, the storehouse shown on Joseph Lysett's 1820 watercolour.

Source: Built form sketches from Godden Mackay Logan's *Conservation Management Plan 2014*, Figure 3.2, page 97

Campbell's two sons, John and Robert, followed in their father's footsteps, successfully nurturing and expanding the family's highly successful merchant trading operations. Eventually, they created what was a vibrant industrial export hub, servicing an international trading empire.

2.2 Initial Construction of the Building 1851-1861

The following is an extract from the GML CMP that summarises the phases of construction for the Campbell's Stores:⁴

The first five bays to be constructed were built in 1851/52, and are the present Bays 6 - 10. A further three bays, which are the present Bays 3 - 5 were completed around 1858, and the final three bays were finished by 1860. Of these last three bays, the southern-most and the final one to be completed was demolished in 1958 to make way for the first overseas shipping terminal Circular Quay. This left two bays, now known as Bays 1 and 2, and the remaining eight to form the group known as the Campbell's Stores. The building was stepped up towards the south with a rise of about 0.3 metres between Bays 1 and 2 and Bays 4 and 5, the change in level being most evident by the string course at gable level, and the second level sill course. When originally completed the building was only two storeys high (Figure 2.6).

Note that the southern demolished bay is referred to as 'Bay X'.



Figure 2.6
c.1870 photograph that shows the Campbell family home, *Wharf House*, with the long verandah, to the left of the two storey, ten bay Campbell's Stores building, and the company's extensive industrial shipping and trading operations and facilities along the shoreline.

Source: (image provenance to be confirmed)

⁴ GML CMP 2004, p51

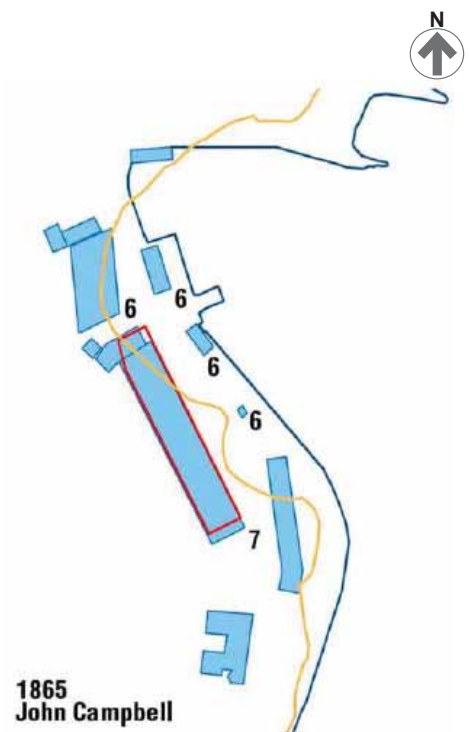


Figure 2.7
Stage 2: CMP figure with number 6 representing 'other structures' on the site (circa 1860/1870), and number 7, the completed Campbell's Stores plus the additional single storey structure to the north

Source: Built form sketches from Godden Mackay Logan's *Conservation Management Plan 2014*, Figure 3.2, page 97.

The original layout of the Stores was based on separate storage spaces to each bay and to each level, which explains the external stairways to the first floor, in front of every bay (Figure 2.8). When the third level was added, there were wall penetrations providing up to three bay stores, with a connecting internal stair located adjacent to a first floor landing from ground level.



Figure 2.8
c.1870 Campbells Cove from Dawes Point battery, with the two storey Campbell's Stores on the right. Note the original stores building closer to the wharf edge, and the roof of Wharf House visible just above the Campbell's Stores roof

Source: SLNSW 40180

In 1876, John Campbell, by then sole owner of the business (following the death of his younger brother, Robert 1859) sold the stores and wharfage, as well as *Wharf House* and the Dawes Point land, to the Australian Steam Navigation Company (ASN Co) for £100,000.⁵

2.3 Addition of the Third Level, c.1885

A 1882 Dove plan shows the Stores, known at this time as the Metcalfe Bond, as 11 bays and two storeys (Figure 2.10 with the notation of '2' on each bay). With an 1887 revision to the plans, the Stores had an additional level, which was constructed of brick (Figures 2.9 & 3.8).⁶

By May 1887, the Australasian Steam Navigation Co Ltd, then in liquidation, had offered Campbell's Wharf to the government for £300,000. The ASN Co had sold all of its ships and most of the wharf was described as leased. By October of that year, the government had purchased the whole site and facilities for the sale price of £275,000.⁷

⁵ McCallum, H., 'Campbell, John (1802 – 1886)', *Australian Dictionary of Biography*, Vol. 1, Melbourne University Press, 1966, p. 199.

⁶ GML, 2014, p19

⁷ GML CMP pp15-16,28, 29



Figure 2.9
Second level wall indicating a main beam for the third level floor that was added c.1885, and its raised position relative to the position of the original roof beam 'pocket' with brick infill, to its lower left. Note the change from stonework to brickwork with the third level addition

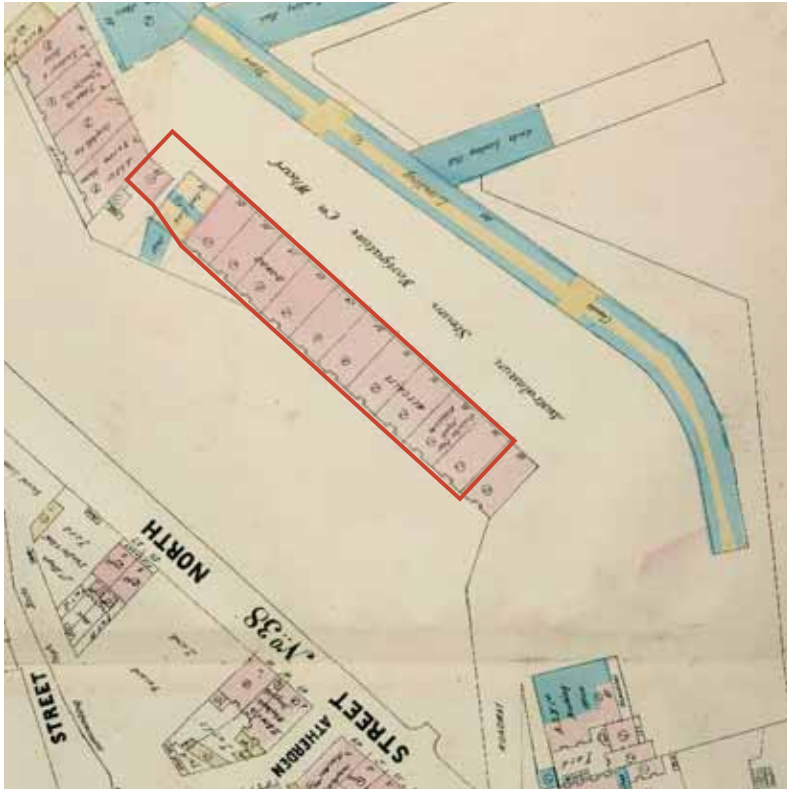


Figure 2.10
c.1880 Percy Dove plan showing Campbell's Stores, known as 'Metacalfe Bonds' as eleven bays and a timber shed 'sail loft' at the northern end (Bay 12). The subject site is outlined in red, and note the first stage of the ASN Co building at bottom right

Source: City of Sydney Historical Atlas

2.4 Addition of Bay 11, c.1895

Following the government's purchase from the ASN Co in 1887, other than the Campbell's Stores, most of the adjacent buildings were demolished to make way for the construction of a Navigation Board slipway. An 1895 survey showed the Government Printing Office, constructed in brick and currently known as Bay 11, abutting the northern-most Bay 10 of the Stores (Figure 2.12).

In 1901 the precinct and infrastructure was taken over by the Sydney Harbour Trust, which proceeded to demolish large parts of Millers Point and The Rocks for redevelopment of the wharf areas, and included the re-alignment of roadways (Figures 2.12 & 2.16).

Hickson Road was re-built in 1914 at a higher level, taking into account the 'benching' topography, and appears to have included the 'back-filling' of the road against the rear wall of the Campbell's Stores. Photographs in the CMP indicate a substantial retaining wall approximately 300mm from the rear wall of the Stores (Figure 2.29).⁸ The result

⁸ GML CMP, 2014

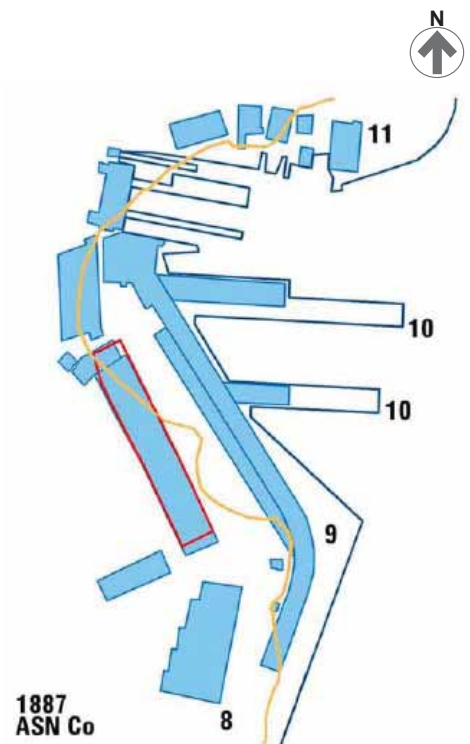
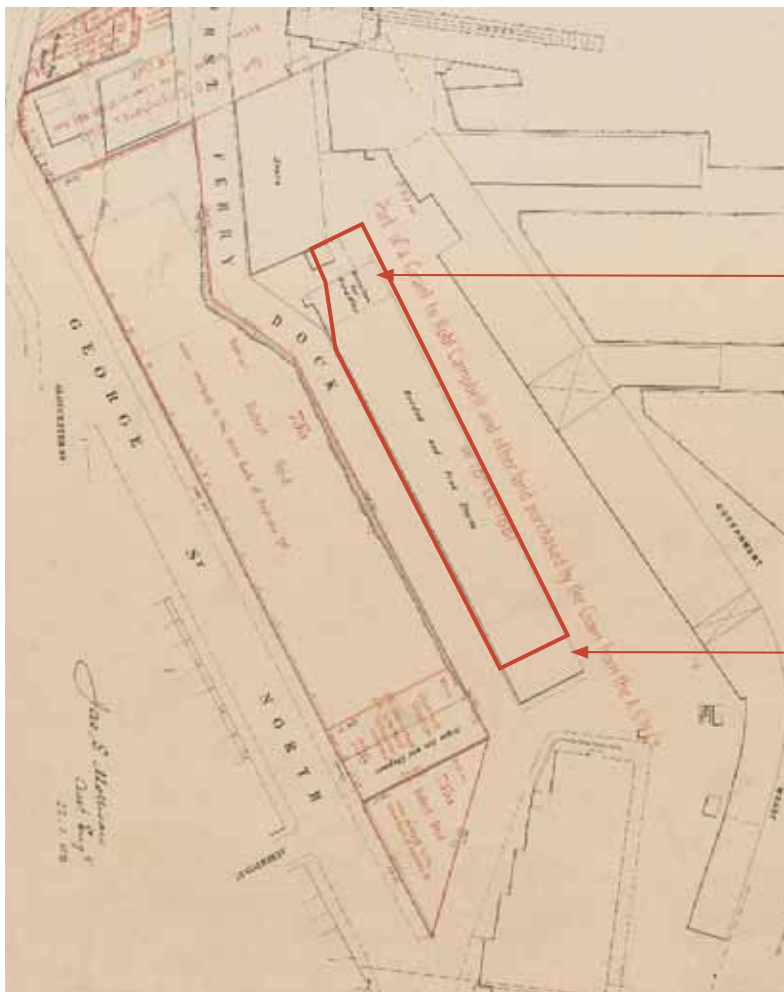


Figure 2.11
Stage 3: CMP (2004) figure with the numbers on the sketch representing: No. 8, ASN Co building; No. 9, ASN Co Stores; No. 10, ASN Co wharves; No. 11, Mercantile Rowing Club.

Source: Built form sketches from Godden Mackay Logan's *Conservation Management Plan 2014*, Figure 3.2, page 97.

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Bay 11

Bay X

Figure 2.12

1902 The Rocks Resumption plan showing the Bay 11 addition (Government Printing Office), and before the re-building of Hickson Road. The site is outlined in red

Note the excavated site of the Metcalfe Bond Stores and the retaining wall against George Street

Source: City of Sydney Historical Atlas

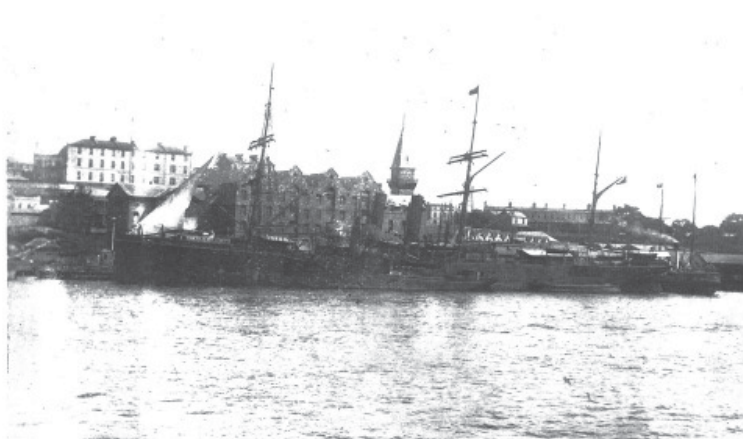


Figure 2.13

c.1890s photograph showing the newly constructed ASN Co Ltd building on the site of *Wharf House*, with the gable ridge lines of Campbell's Stores just visible beyond the docked ships

Source: (image provenance unknown)

was that the three storey building was transformed into the current two storey structure fronting Hickson Road (Figure 2.21).

At this time Hickson Road was realigned at the northern end of the Stores, 'shaving' the corner off Bays 10 and 11, the result of which is the oblique angle of the west wall (Figure 2.16). The hipped roof of Bay 11 was also altered to a Dutch gable, and it may have been at the same time as part of accommodating the alteration to the western elevation (Figure 2.18).

In 1932 a fire in this section of the building (Bay 11) resulted in re-building and, possibly, the construction of the substantial roof vents that appear in later photographs (Figure 2.19).



Figure 2.14

Pre-WWI photograph of the east and north facades of the existing Bay 11, showing the configuration of the northern wall prior to the changes introduced during the 1970s conservation and adaptive re-use program

Source: SLNSW



Figure 2.15

Pre-WWI photograph of the northern and western facades of the existing Bay 11, showing the configuration of the western wall and parapet. Note the hipped roof appears to be damaged, and the Hickson Road facade has yet to be altered for the roadway realignment

Source: unknown

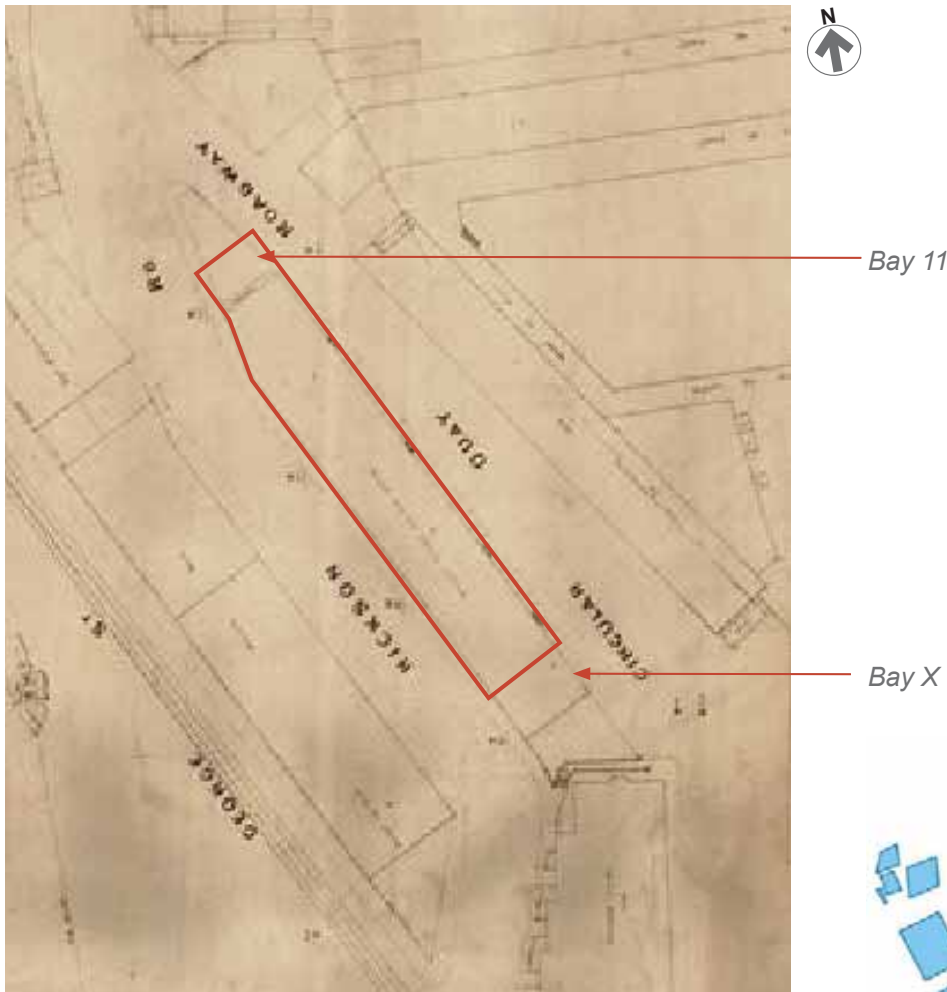


Figure 2.16
1920 plan after the Hickson Road re-alignment and the alteration to Bays 10 & 11, with the current site outlined in red

Source: Austral Archaeology report, 2015, p37

2.5 Demolition of Bay 1, 1958

The post-WWII period saw the western side of Sydney Cove begin to alter from commercial activity to one of tourism, as evidenced with the construction of the Overseas Passenger Terminal. This involved the demolition of the southern Bay X in 1958 to erect the elevated roadway connecting Hickson Road with the passenger terminal (Figure 2.20). The resulting exposed stone wall was covered with a render, although it is not clear as to whether this was a direct application or applied to an installed brick wall in front of the stonework (Figure 2.27).

Campbell's Stores subsequently came into the control of the Sydney Cove Redevelopment Authority (SCRA) after being handed over by the Maritime Services Board (successor to the Sydney Harbour Trust) in the 1970s.

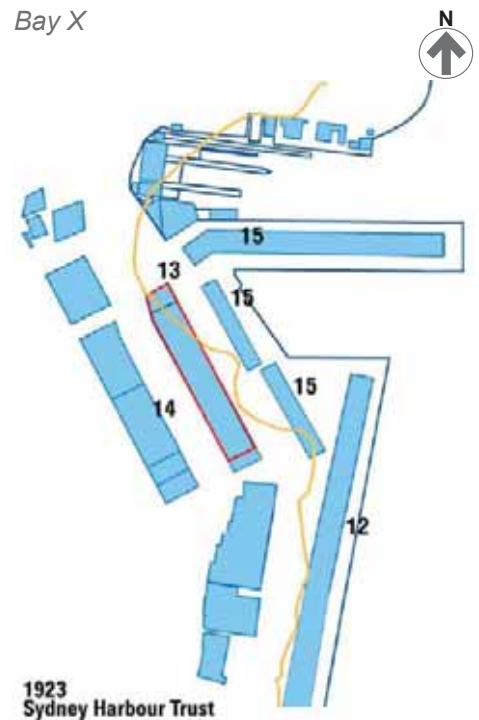


Figure 2.17
Stage 4: CMP figure with the numbers on the sketch representing: No 12 represents the Norddeutscher Lloyd Wharf and Stores; No 13, the Government Printing Office added to the Stores; No 14, the Metcalf Stores, and No 15, the Sydney Harbour Trust Stores and wharves.

Source: Built form sketches from Godden Mackay Logan's *Conservation Management Plan 2014*, Figure 3.2, page 97.



Figure 2.18

Circa 1940s photograph, showing the northern section of the Campbell's Stores (including Bay 11 prior to the construction of the roof vents) but after the re-building of the roof to a Dutch gable

Source: Historic Houses Trust



Figure 2.19

c.1950s photograph before the construction of the Overseas Passenger Terminal, and before the southern bay of the Campbell's Stores was demolished. Note the large masonry roof vent of Bay 11

Source: National Library of Australia



Figure 2.20

c.1961 photograph showing the northern section of the Campbell's Stores, including Bay 11 and after the construction of the bridge to the Overseas Passenger Terminal on the left. Note the location of the stairs and the prominence of the chimney duct

Source: SHFA Archive



Figure 2.21

c.1961 photograph of Campbell's Stores from Hickson Road after demolition of Bay X, and the south wall finish on the right before the later brick duct enclosure.

Note the footpath level above the window sill and before the levels were adjusted, as well as the original ground floor opening configuration before the installation of doorways in the 1980s, although the first window and door interventions are evident to Bays 4 and 5

Source: SHFA Archives

2.6 Conservation Works 1970s

During the 1970s, SCRA undertook major conservation works at the Campbell's Stores. The extent of these works is illustrated in the following c.1970s photographs, held by the Sydney Harbour Foreshore Authority's (SHFA) archives. The works included the excavation of the Hickson Road footpath against the rear wall, to install a services tunnel along the length of the building (Figures 2.29-2.31). A skin of brickwork was built against the original stone wall with a gap of 100mm (Figure 3.14).



Figure 2.22

c.1970s Campbell's Stores with the former Circular Quay West roadway, and before the stairway, south of Bay 1 on the left

Source: SHFA



Figure 2.23
c.1970s Campbell's Stores at the time of re-roofing with slate. Note the masonry chimney/vent between Bays 10 and 11 to the upper left

Source: SHFA



Figure 2.24
c.1970s Eastern facade of Campbell's Stores with areas of render to the ground floor level before remediation. Note the original location of the stairs to the first floor level, reflecting the original tenancy separation of each floor

Source: SHFA



Figure 2.25
c.1970s Campbell's Stores after the removal of the weathered sandstone and before replacement, to the ground floor level of the eastern facade

Source: SHFA



Figure 2.26

c.1970s Campbell's Stores after wall remediation and replacement sandstone to the ground floor level of the eastern facade. Note the timber 'bridges' across the stone paving

Source: SHFA



Figure 2.27

c.1970s South wall of Bay 1 of Campbell's Stores before the construction of the brick duct and the stairway. Note the render finish to the wall after the demolition of the original south bay for the construction of the bridge to the Overseas Passenger Terminal, on the left

Source: SHFA



Figure 2.28

c.1970s South facade of Campbell's Stores, Bay 1 with construction of the brickwork duct over the previously rendered wall

Source: SHFA



Figure 2.29

c.1970s Hickson Road footpath excavation during restoration works with Campbell's Stores on the left. Note the retaining wall next to the Stores, likely constructed c.1915 at the time of re-building Hickson Road

Source: GML CMP 2014, p91, from SHFA Archives, CCM 172



Figure 2.30

c.1970s Excavation to the Hickson Road side of Campbell's Stores during construction of the sub-floor services tunnel, with the window opening to what was the original ground floor, reflecting the back-filling required for the onstruction of Hickson Road. Note the break through to the loading dock area below the bridge to the Overseas Passenger Terminal

Source: SHFA

The CMP notes that by 1978 new window openings had been inserted into Bays 4 and 5 of Hickson Road (Figure 2.21), and a number of the original Level 2 windows (ground floor to Hickson Road) had been enlarged into doorways.

The SHFA archives also have a series of architectural working drawings by a number of architects, in particular by Devine Erby and Mazlin Pty Ltd (DEM). For example, there are drawings for the restoration of the windows, dated 1973 (possibly Stage 1), and drawings dated May to December 1976 for 'Stage 2' and 'Stage 3', that reflect the works shown in Figures 2.22-2.30. These drawing details and notes include:

- The break-up of an existing slab to the ground floor level and excavation for a new raft concrete slab-on-ground
- Works to the elevations including the removal of weathered sandstone and replacement to match (Figures 2.24-2.26)

- The construction of the brick duct to the south wall of Bay 1 with a detail indicating a previous render finish to the stonework (Figure 2.28)
- A new lowered footpath to Hickson Road
- New bathroom and stair details.

A 1978 DEM drawing details the demolition of the high masonry chimney to Bay 11, down to roof level, and a 1979 drawing details the work to the stairway adjacent to Bay 1.

2.7 Development Works 1980-90s

After the conservation of the Stores, SCRA redeveloped the site to enable their current use as a restaurant venue. The drawing details include:

- *The Waterfront Restaurant* (Bays 1-3), detailed in drawings by John Nicholas Architect, dated April 1979, which include the enlarged double door opening to Hickson Road. A 1999 drawing by Misho + Gordon details the installation of timber louvres for air intakes to the Hickson Road gables of Bays 1-3, similar to the shape of the current openings to Bays 7 and 8. However these were only partially built as drawn.
- *The Imperial Peking Restaurant* (Bays 7-8), detailed by Lai & Wjtulewicz Architects, dated August 1981, which include the alteration of windows to doorways to Hickson Road
- *The Italian Village* (Bays 9-11), detailed by Ercole Palazzatti Architecture, dated May 1985. A 1996 drawing by the same architects details the proposed alterations to the windows and doors to the north elevation of Bay 11 (Figure 2.32).

In the mid-1980s, substantial works were undertaken in the vicinity of Campbell's Stores as part of the bicentenary celebrations that included the removal of some of the wharfage to Campbell's Cove. This work included the landscaping of the Circular Quay foreshore including the promenade in front of the Stores.

With the construction of the Hyatt Hotel in 1990 to the north of the Stores (Figure 2.35), the promenade boardwalk was incorporated into the development, making a significant connection with Dawes Point. Interpretation of the boardwalk included visibility of a stonework slipway below a section of the hotel restaurant.

A small glazed pavilion was built to the north of Bay 11, c.1990 (Bay 12), as a function centre and is used as a part of the *Italian Village* (Figure 2.34).

In 1998, the Sydney Harbour Foreshore Authority (SHFA) assumed control of the area, including Campbell's Stores.⁹



Figure 2.31

Service tunnel below the Hickson Road footpath that provides goods delivery and waste removal from the back-of-house. In addition to A/C ducting and services' reticulation, the tunnel houses coolrooms and storage



Figure 2.32

North elevation of Bay 11 of Campbell's Stores showing the alteration and enlargement of the windows that were lowered relative to the windows facing Hickson Road on the right (compare with Figure 2.15)

⁹ GML CMP (2004) p18

Further conservation works for SHFA, primarily to the rainwater goods and box gutters between the bays, were detailed by Design 5 Architects, dated March 2008. Details include the installation of louvres to the Hickson Road gables of Bays 7 and 8, however this was not carried out as the openings are completely unsealed, with the interior roof structure evident from the street (Figure 2.33).

This brings into question as to whether the drawn detailed works by Design 5 for the roof were actually carried out, as the CMP notes that this area of the roof is in poor condition.¹⁰



Figure 2.33 left
Hickson Road elevation detail of Bay 7 of Campbell's Stores showing the gable vent without the installation of a louvre (refer Figure 5.7)



Figure 2.34
c.1990 glazed pavilion to the north of the Stores Bay 11 in the background, with pedestrian access from Hickson Road from the right, down to the Campbells Cove promenade to the left. The shadow of the Hyatt Hotel is in the foreground. Note the condition of the tree between the pavilion and Bay 11

¹⁰ CMP, 2014, p143

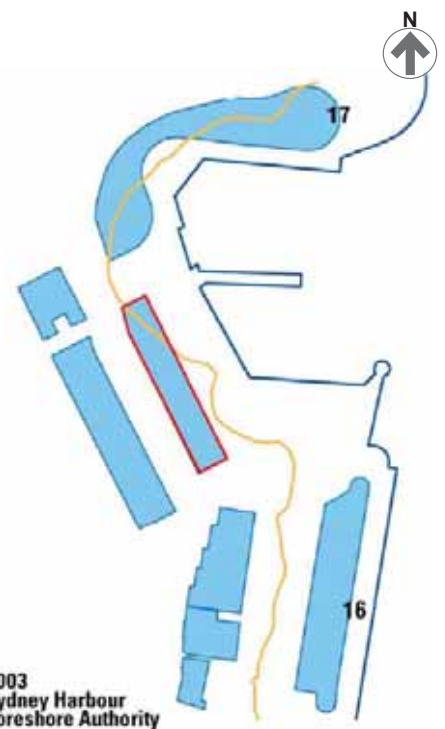


Figure 2.35
Stage 5: CMP figure with the numbers on the sketch representing: No 16 the Overseas Passenger Terminal, and No 17, the Park Hyatt Hotel

Source: Built form sketches from Godden Mackay Logan's *Conservation Management Plan* 2014, Figure 3.2, page 97.

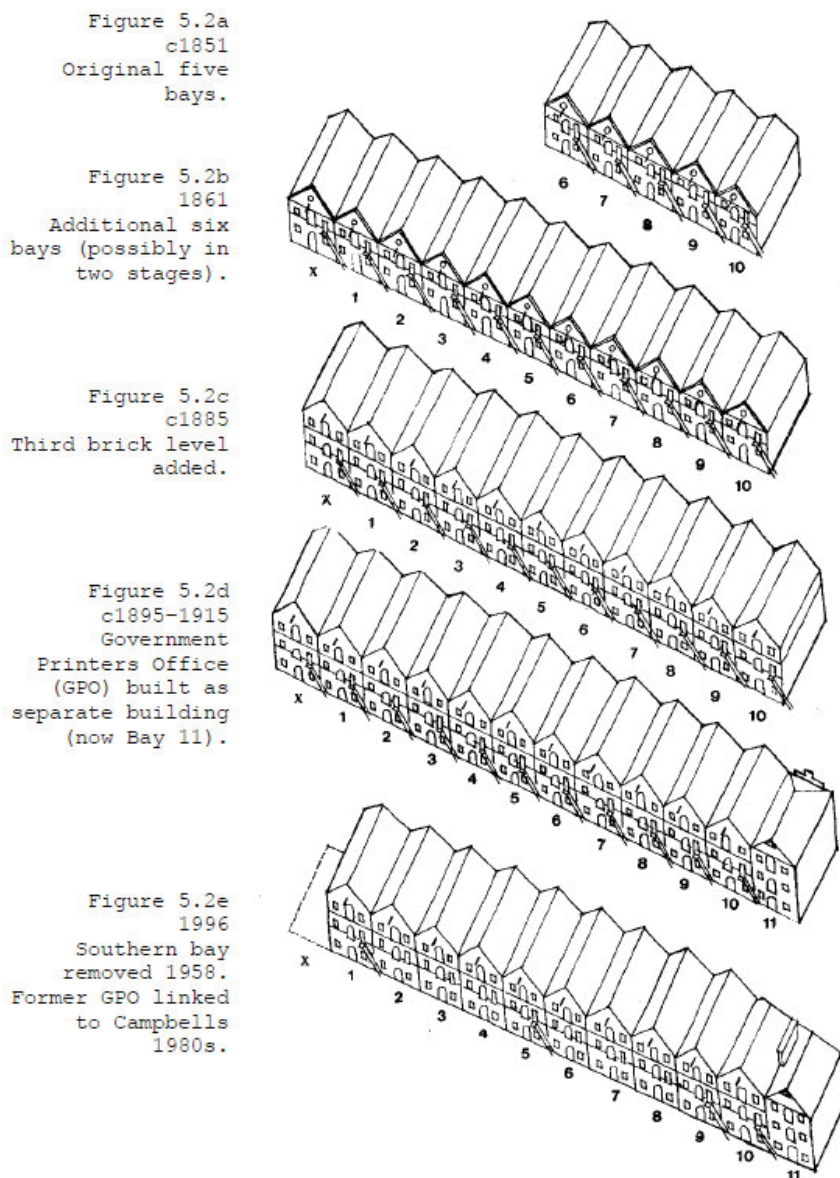


Figure 2.36
Campbell's Stores and summary of the phases of development

Source: Godden Mackay Logan *Conservation Management Plan* 1996, Figure 5.1

Site Description

3.0

3.1 Urban Context

Campbell's Stores are located on the waterfront of Campbell's Cove, opposite the Sydney Opera House (Figures 3.1 & 3.2). The Stores provide a significant component to the harbours' edge and form a backdrop to the pedestrian promenade that extends from Circular Quay to Dawes Point, as well as maintaining forefront scale to The Rocks precinct behind (Figures 3.3 & 3.4).

In the urban context and historically, the Stores provide an important component in the timeline of development as one of the earliest structures in the precinct (1851-61). Within the cultural amphitheatre of Campbells Cove, the Stores are adjacent to (Figures 1.2 & 3.2):

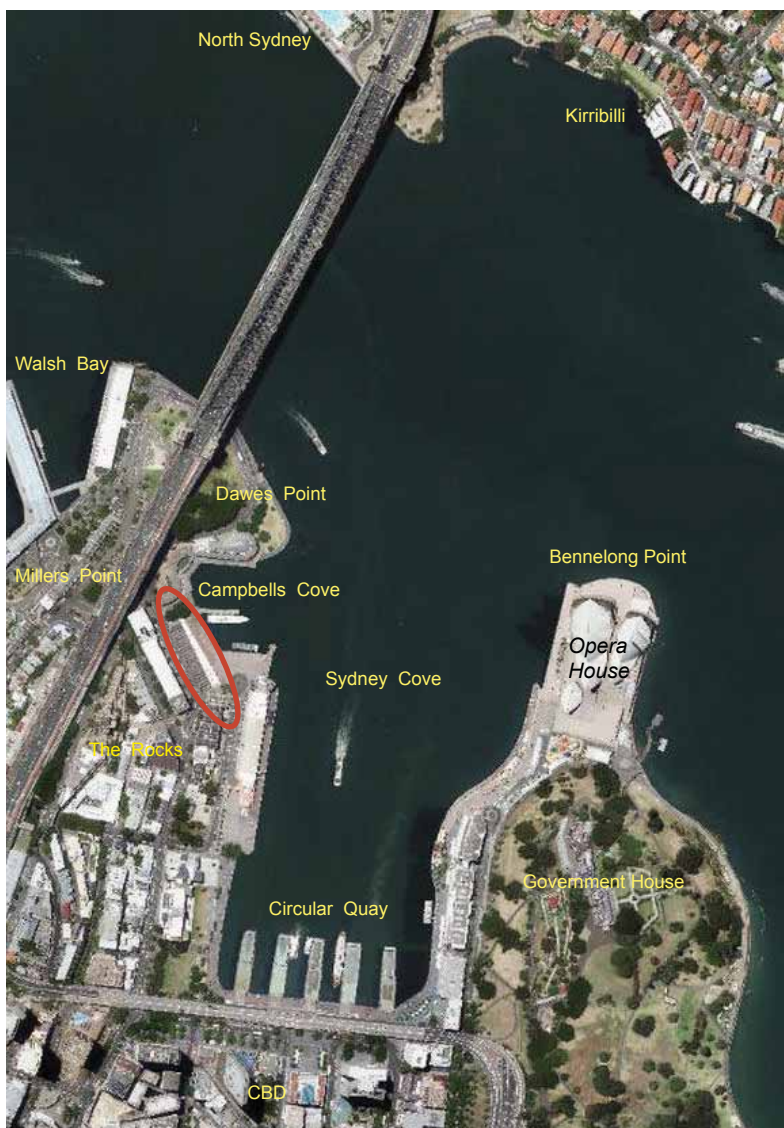


Figure 3.1

The context of Campbell's Stores, circled in red, in relation to Sydney Cove, the Opera House and the broader harbour



Figure 3.2

The context of Campbells Cove, from the Overseas Passenger Terminal, the tower of the ASN Co building on the left, the Metcalfe Bond stores behind Campbell's Stores, and the Hyatt Hotel in front of the Harbour Bridge

Source: JPW presentation report 2015



Figure 3.3

The Hickson Road elevation of the Stores from the south, with the stairway and elevated roadway to the Overseas Passenger Terminal on the right

Source: JPW presentation report 2015



Figure 3.4

Hickson Road from the north, with the Hyatt Hotel on the left, Bay 11 of Campbell's Stores in the middle distance and the Metcalfe Bond stores on the right

Source: JPW presentation report 2015

- Metcalfe Bond Stores (1912-1916) to the west, across Hickson Road, are five storey brick buildings
- Bushells Warehouse (1886-1912) to the south-west and next to the Metcalfe Bond Stores, is a seven storey brick clad building
- ASN Co Building (1884-85) to the south, across the elevated roadway to the Overseas Passenger Terminal, is a four storey brick building with a landmark tower
- Hyatt Hotel (1989), to the north, is a four storey pre-cast concrete clad building.

3.2 Views to and from the Site

The gable fronted facade of Campbell's Stores provides a distinctive form that can be viewed from around Sydney Cove, and particularly from the Opera House and harbour (Figure 3.6). Although a low rise building of three storeys, its length of 100 metres, and its repetitive facade elements provides a distinctive frontage to the foreshore promenade and a landmark foreground to The Rocks precinct.

Given its location fronting Campbells Cove, the Stores are evident from the Dawes Point reserve to the north, and from the vicinity of the Overseas Passenger Terminal to the east (Figure 3.2). From the Campbell's Stores itself, there are commanding views to the Opera House and beyond (Figure 3.5), with an almost uninterrupted 180 degree vista of the harbour, including the Harbour Bridge.



Figure 3.5
The vista from the Campbell's Stores to the Opera House and beyond, with the shade canopies in the foreground

Figure 3.6
Location of the subject site in Campbell's Cove, from the harbour with Metcalfe Bond Stores behind, the Hyatt Hotel to the right, and the Overseas Passenger Terminal on the left, in front of the tower of the ASN CO building. This is a similar view from the Opera House.

Note the distinctive gable roof outline, the uneven line of the current canopies and how they obscure the lower section of the Stores



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3.2 Description of the Building Exterior

The three storey structure as it faces east, reflects the phases of development, with the first two levels built of sandstone and the later third level constructed of brickwork (Figure 3.8). The current 10 bay configuration is emphasised by the slate gable roofs to each individual bay, whereas the later Bay 11, at the north end of the row, has a differing window fenestration and a Dutch gable (Figure 3.7). There is a c.1990 glazed pavilion structure to the north of the Stores, which is used in conjunction with the adjacent restaurant (Figure 2.34).

The western, two storey facade facing Hickson Road reflects the falling topography from one street, down to the lower former street level of the current forecourt (Figure 5.4). Originally the Stores facing Hickson Road were built on grade where the topography was cut to provide a level platform upon which the structure was built (Figure 2.30). The re-building in 1915 of the roadway 'infilled' the back of the Stores, and later in the 1970s the services' tunnel was installed (Figure 2.29-2.31).

For a full description of the exterior and interior of the building refer to the GML CMP of 2014.



Figure 3.7
Bay 11 of the east elevation with the sunken courtyard underneath the canopies. Note the Dutch gable roof in contrast to the gables of the other bays

Figure 3.8
East elevation with the sunken courtyard underneath the canopies, and the ship masts to the left. Note the change in material from sandstone to brickwork at the top floor level



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Figure 3.9

The two storey structure of the west elevation with a sandstone lower level and brickwork to the upper level. Note the simple openings of this side in contrast to the harbour elevation, and the splayed corner to the end of the building at the left. The door and window openings of the two bays on the right indicate later alterations from the original

3.2.1 Exterior Intrusive Items

There are a number of items that have been identified by the CMP as *Intrusive*.¹

- Sail ship masts in the eastern forecourt to Bays 1-3
- Plastic shade canopies to the eastern forecourt for the length of the building (Figure 3.10)
- Staircase (Bay X) linking Hickson Road to the harbourside promenade (Figures 3.17 & 5.2)
- Glass pavilion to the north of Bay 11 (Bay 12)

Other items considered to be intrusive are:

- The sunken court along the eastern front of the Stores
- Bay 1 south wall brickwork duct (Figure 3.3)
- The row of flag poles located along the edge of the sunken court (Figure 3.8).

3.3 Description of the Building Interior

The interior structure is essentially a series of adjoining bays separated by sandstone party walls on the lower levels and brick walls on the top floor (Figures 3.11 & 3.16-3.18). The floor structure



Figure 3.10

Forecourt side of the Stores within the shading canopy, with the eastern wall on the left and the promenade on the right. Note the restricted height and lack of view to the 'outside'

¹ CMP 2014, p133

is based on hardwood posts and beams running in a north-south direction, with floor joists running from front to back and notched into the main beams (Figure 3.12).

The building has been significantly altered since it became a series of four restaurants dating from the 1970s and 1980s. With the inclusion of stairways, numerous amenities and service areas, including interior design that reflects the theme of each of the restaurants (Figure 3.13). The internal spaces as a consequence are quite complex and disparate.

Numerous openings have been inserted into the fabric of the building, including various configured voids through the floors, and new doorways through the common walls (Figure 3.15). The interior design has also included the introduction of brickwork, adjacent to and in contrast with the original sandstone walls (Figure 3.13).

The basic post and beam structure remains however, with new stairways and voids inserted by the removal of floor joists, and generally between the main north-south beams.

In order to service the restaurant 'back-of-house' at ground floor, a service tunnel runs the length of the building below the Hickson Road footpath with doorways through the former window openings (Figure 3.16). At the southern end, the tunnel leads to the loading dock located between the Stores and the ASN Co building, and at the northern end the tunnel exits between Bay 11 and the glass pavilion.



Figure 3.11

An example of an opening in a sandstone party wall to allow for expanded restaurant space. Note the brickwork of the third level addition, upper right

3.3.1 Restaurant Fitouts

The current use of restaurants has divided the building into four tenancies that date from the 1980s. The tenancies at Ground Level (eastern forecourt) are:

- Bays 1-3 *The Waterfront Restaurant*
- Bays 4-6 *Wolfie's Restaurant*
- Bays 6-8 *The Imperial Peking Restaurant*
- Bays 9-11 *The Italian Village*

The tenancies vary on Levels 1 and 2 as detailed in the CMP:²

- The *Waterfront* occupies Bays 1-3, Level 1 and Bays 1-4, Level 2
- *Wolfie's* occupies Bays 4-5, Level 1 and Bay 5, Level 2
- The *Imperial Peking*, occupies Bays 6-8, Levels 1 and 2
- The *Italian Village* occupies Bays 9-11, Levels 1 and 2

The degree of fabric intervention varies but is most evident in the *Italian Village* restaurant, which has an elaborate stairway, combined with void spaces and double height, arches lined with sandstock brickwork. This is also the most 'themed' restaurant with references to Mediterranean village roofs and walls (Figure 3.13).

² GML CMP, 2014, p65



Figure 3.12
A contemporary stair opening showing the main beam structure with floor joists to one side and the notched recesses where the joists have been removed (Bay 2)

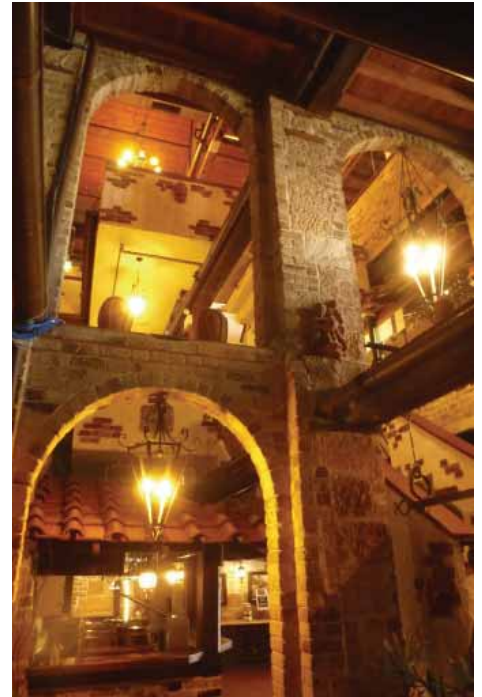


Figure 3.13
Substantial alterations to a party wall with the lining of the arch with sandstock brickwork (Bay 9 of the Italian Village)



Figure 3.14
Services tunnel below the Hickson Road footpath, with a doorway to the building on the left, the original sandstone wall between the door frame and the brickwork skin next to it on the right



Figure 3.15
An example of a contemporary stair inserted between the column grid (Bay 5). Note the brickwork to the top of the sandstone and the infill below the beams where the former roof structure was bearing

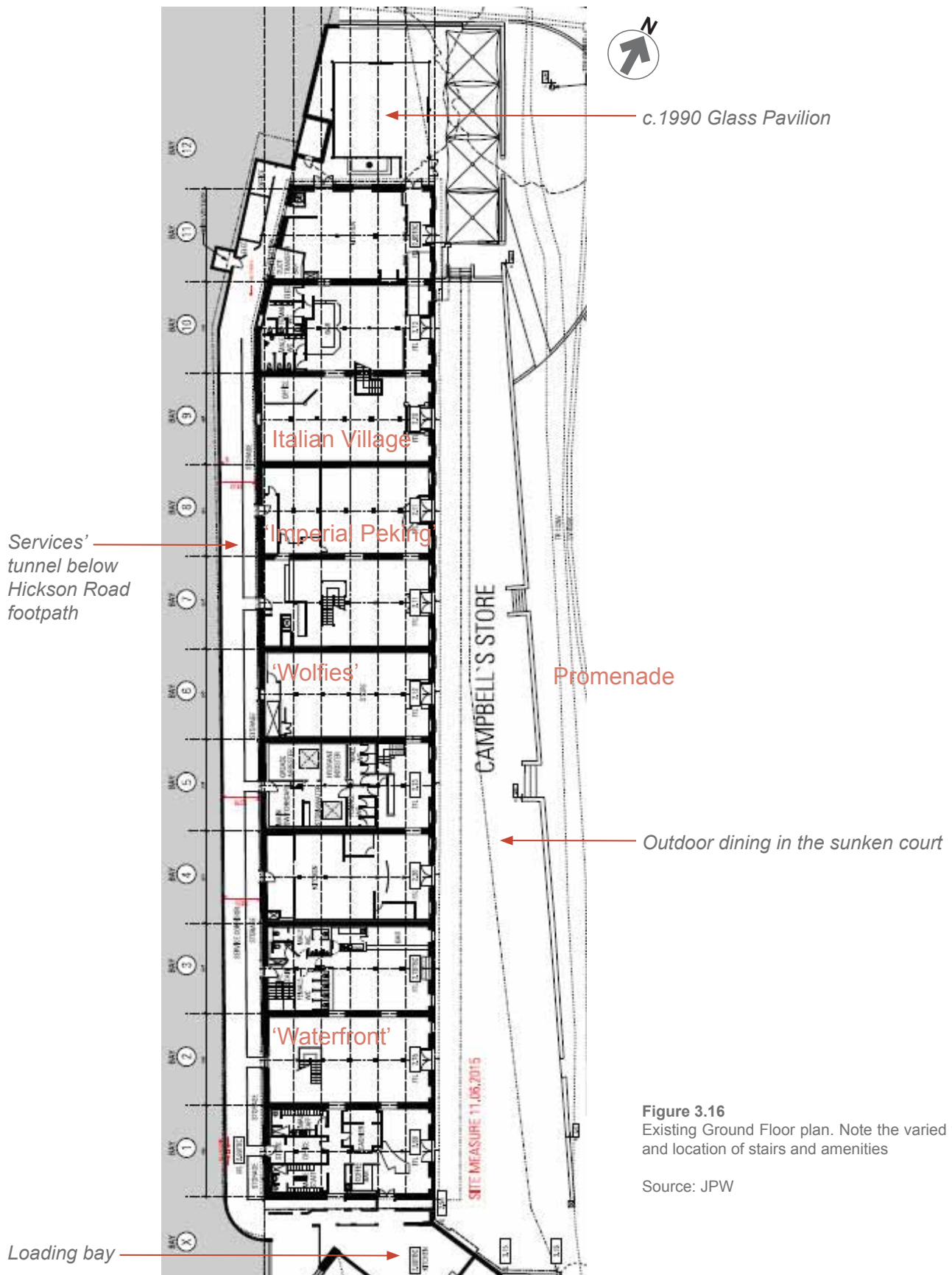


Figure 3.16
Existing Ground Floor plan. Note the varied type and location of stairs and amenities

Source: JPW

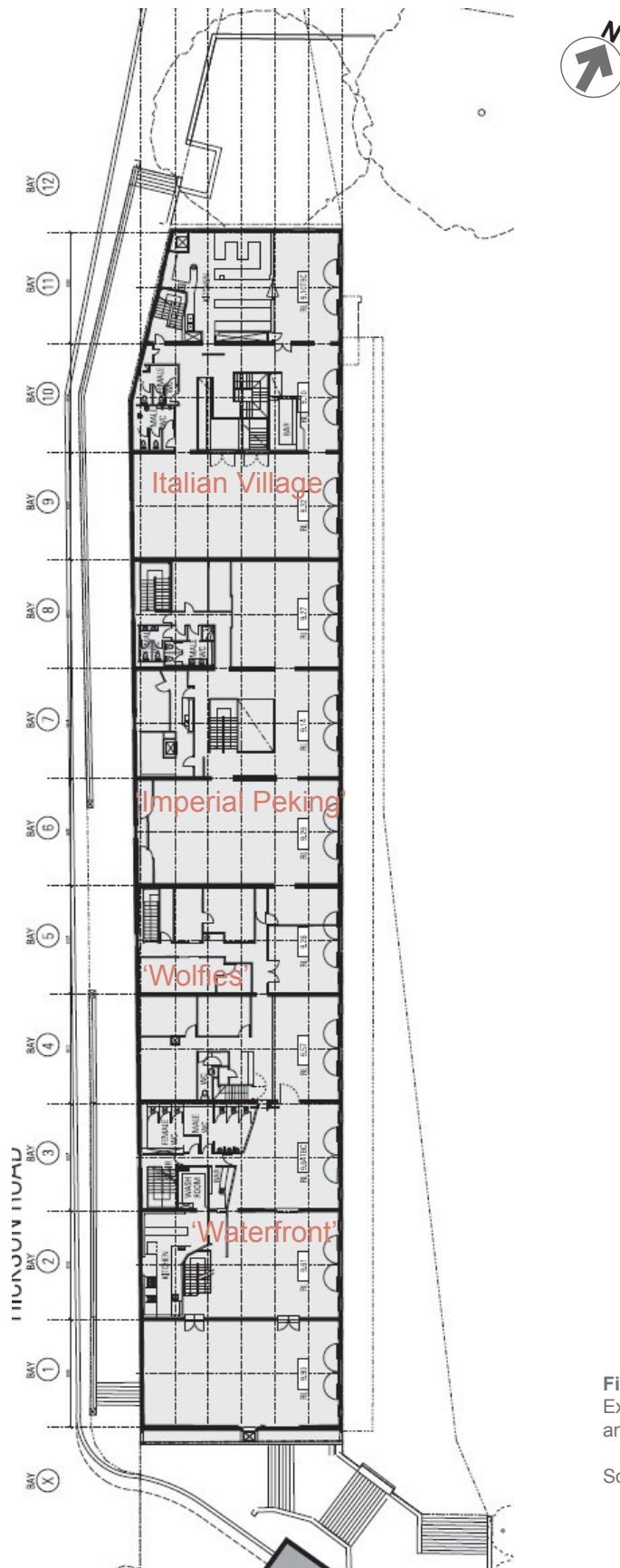


Figure 3.18
Existing Level 2 Floor plan. Note the varied type
and location of stairs and amenities

Source: JPW

The *Imperial Peking* has a central stairway that also includes voids through the three levels, and includes a hanging sculpture. Each of the tenancies have numerous kitchens and amenities towards the western side of the building, and as a result the original spaces can be difficult to appreciate, particularly on the ground floor.

3.4 Industrial Heritage

There are extant examples of industrial equipment throughout the Campbell's Stores' including attachment to the eastern facade, which provide evidence of materials handling technology (Figure 3.19). The examples reflect the stage of technology at the time, from mid-19th century basic handling, through to the introduction of motor machinery. The CMP, which provides a detailed history and description, has identified the following phases as:³

- Manual Handling
- Hydraulic Hoisting Equipment
- Gas Engine-driven Winch
- Motor-driven Winch

In addition to the extant industrial items in the building, SHFA has additional items in storage. There is an opportunity to review the stored items for reinstatement and interpretation.

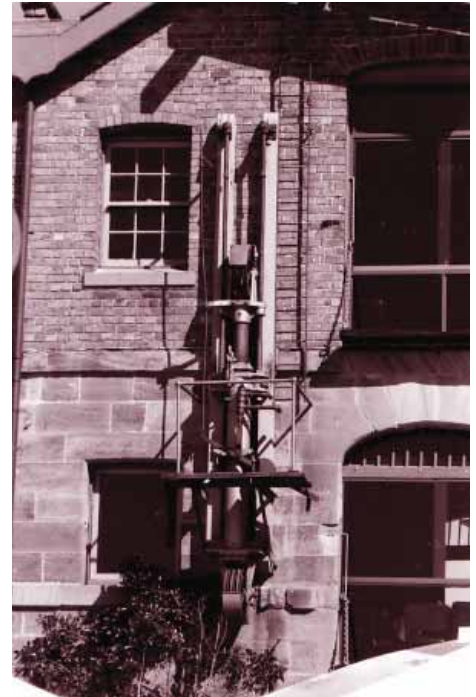


Figure 3.19
Example of materials handling technology on the face of the eastern facade

Source: SHFA archives, undated

3.5 Condition and Integrity

3.5.1 Condition

The condition of the Stores is generally good, given its age and a history of alteration and reuse, however there are ongoing issues of rising damp in the east external walls and internal cross walls that need to be addressed.

In addition to the visible deterioration of the eaves and bargeboards, as well as the loading dock timber apron boards, the CMP has identified the projecting masonry parapets between the bay roofs as not being capped,⁴ in spite of the Design 5 (2005) drawings detailing a capping.

The condition of the building has also been compromised with restaurant fitouts that have obscured original fabric and altered the internal spaces. In addition there are inappropriate and haphazard services' reticulation, including the poor placement of light fittings to the harbourfront facade, and a satellite dish to the roof of Bay 1.

There is also an issue with the constant hosing down of the outdoor restaurant areas adjacent to the eastern facade, which contributes to the moisture content of the paved area and the base of the

³ CMP, 2014, p78

⁴ *ibid*, p143

external wall. To address this problem requires management review and constant monitoring.

The ability to inspect parts of the building has left unresolved issues with regard to understanding the extent and condition of original fabric, and include:

- The extent of toilets, kitchen storage and cold rooms, as well as floor to ceiling wall tiles to the numerous facilities, limits inspection and assessment
- Photographic evidence indicates a layer of render to the Bay 1 south wall that exists behind the extant brick duct. Although a DEM drawing detail indicates a layer of render only, a c.1970 photograph appears to reflect brickwork coursing under the render (Figure 2.27)
- The Hickson Road service tunnel is lined with a brick wall against the original west elevation stonework, and various amenities located against the inside of the original wall, makes it difficult to ascertain original openings and condition of fabric (Figure 3.14)
- A current inspection of the roof has not been carried out.

There have been a number of detailed reports carried out in order to identify issues of deterioration and recommended remediation solutions:

- 1) Design 5 Architects, *Condition Survey and Project Plan For Repairs to Building*, November 2005

This report provided an overall survey of all the elements of the building, including the issue of sub-floor ventilation which currently is not possible due to the concrete slab-on-ground construction of the 1970s. The report discussed the relative merits of replacement of the slab with a suspended structure, either of concrete or timber, in order to achieve a flow of air to the exterior.

- 2) Design 5 Architects, *Investigations of Potential Causes of Ground Floor Rising Damp*, June 2008
- 3) Design 5 Architects, *Stormwater Drainage Report Campbell's Storehouse*, December 2008
- 4) Jasper Swann, *Ground Level Stonework Conservation Report*, July 2011

The condition of the stonework at the time of the Swann report is summarised as:⁵

⁵ Jasper Swann, 2011, p2

In summary, the ground floor stonework is generally in fair to poor condition owing to the significant salt damp problems related to the harbourside location of the premises and the inadequate drainage of the site. Much sandstone in the lower four courses has suffered significant deterioration through salt contamination and subsequent disaggregation.

An original slate damp proof course exists in parts, but this has deteriorated and has little, if any, effectiveness in performing its intended function.

The Swann report, also identified drainage as a key issue for the site, and offered the following approach to attempt to ameliorate further deterioration of stonework:

- Attend to resolving site drainage before any other remediation works is carried
- Desalination of the stonework, including sample testing before, during and after treatment to ascertain levels of contamination
- Introduction of a Damp Proof Course, offering a number of different methods
- Use of Air Drains against the internal and possibly the eastern external wall

- 5) David West, International Conservation Services,
Campbell's Stores - Rising Damp Review, July 2014

This report reiterated the findings of the Swann report but emphasised the main problem of rising damp was likely from the proximity of the water table below the building.

- 6) Taylor Thompson Whitting, *Civil Engineering Schematic Design and Integrated Water Management Report*, August 2015

This report, as a part of the current design phase, has noted the problem of 'overland flow paths and flooding':

Campbell's Store and Campbell's Promenade stormwater drainage system was identified as a problem with respect to deterioration of the sandstone structure, varying levels of rising damp and flowing water in the dining area in larger storm events.

Some existing stormwater services are blocked and the connectivity unclear.

The promenade has been built up during development changes around 1988 which had the effect of creating a trapped low point in front of the stores building.

3.5.2 Integrity

Campbell's Stores currently exhibit 150 years of evolution with changes of:

- scale
- use
- materials' handling
- tenancy layouts
- service areas and plant rooms
- civil works to all sides of the building
- location in a marine environment with resulting deterioration.

Whilst some of these elements, such as the 1980s approach to reuse should be reversed, the primary aspects of the building will be carried forward in its evolved state.

The integrity of the structure is generally good, however this has been compromised over time with the restaurant fitouts that have altered some of the window materials and position of openings, particularly along the Hickson Road elevation. A problem with the current A/C is that some of the Hickson Road windows are used directly for ventilation that has an adverse appearance on the building (Figure 3.20)

The integrity of the interior has also been compromised by new openings to both the walls and floors with stairways and voids. These alterations can however be reversed with re-construction of the missing floor joists and floorboards, and the appropriate infilling of redundant wall openings.

The restaurant service areas have also been carried out in such a manner that often the original spatial understanding of the separate bay configuration has been obscured for the observer.



Figure 3.20
Hickson Road window with an A/C unit using the open window for ventilation

Established Heritage Significance

4.0

4.1 Established Significance of the Subject Site

The following Statement of Significance for the Campbell's Stores has been sourced from the *Conservation Management Plan* 2014.¹

Statement of Significance:

Campbell's Stores is a superb example of mid-nineteenth century warehouse buildings, now rare in Sydney. It is the only warehouse of its type remaining on the foreshore of Sydney Cove, the hub of commerce and international shipping transport until the late nineteenth century. As a memorable landmark in The Rocks, visible from a wide area of Sydney Harbour, it is a symbol of mid-nineteenth century Sydney.

Campbell's Stores has historic significance for its association with the Campbell family, one of the most influential families in early Colonial Australia. It is the surviving element of a complex of wharves and stores that began in 1801 with the construction by Robert Campbell of the first privately-owned wharf in Australia. Later significant associations include the Australasian Steam Navigation Company, one of the most important commercial shipping and transport companies in Australia, and the Sydney Harbour Trust, established by the Government following the bubonic plague scare of 1900.

Campbell's Stores is significant for its association with commercial Bond and Free store usage for over one hundred and twenty years, with each successive owner (including the Sydney Harbour Trust and Maritime Services Board) leasing sections of the Stores to a variety of merchant companies.

The changes made to Campbell's Stores provide evidence of the changing commercial fortunes of maritime Sydney. The construction of the first five bays demonstrates economic growth following the 1840s depression and the additional six bays demonstrate further economic growth following the 1850s gold rushes. The construction of the third level by the ASN Co in the mid-1880s demonstrates a further period of economic growth and also of a change in the functional operation of the Stores, as evidenced by the inter-connection of the top floor spaces. The adaptive reuse of the building in the 1970s represents an early approach to the conservation of historic buildings. The continued subsequent use of the building for a series of restaurants demonstrates the changing uses of Sydney Cove from industrial purposes to largely tourist-related purposes.

The design, form and materials of Campbell's Stores contribute to its aesthetic significance as a complex of buildings of high

¹ Godden Mackay Logan, Campbell's Stores, The Rocks, 2014, p123

visual and sensory appeal. Their design elements reflect their original function in a simple but dignified manner. Their form is a coherent whole, made up of repetitive gabled bays combined with an undulating rhythm of door and window openings. The consistent use of sandstone, brick and slate materials reinforces this visual coherence and provides an appearance of solidity and quality.

Campbell's Stores represents a surviving example of mid-nineteenth-century style warehouses; a building type once common around Sydney Cove, but now rare. The gabled bay form, cathead beams, hoists, goods aprons and doors are evidence of an older warehouse style. The form, bars on openings and lack of internal connections between bays evidence the security required for bond store use.

Campbell's Stores has technical/research significance because of its potential to contribute further to our understanding of the early maritime activity around Sydney Cove and, in particular, within the Campbell's Wharf complex. It also has the ability to contribute further to our understanding of the use and operations of mid-nineteenth-century warehouse buildings, particularly in relation to goods handling and the changes in technology that occurred over time.

The remnant hoisting equipment of the Campbell's Stores building provides evidence of the changes of technology in goods handling and haulage that occurred during the nineteenth and twentieth century. The hydraulic hoisting equipment and the winches in particular are evocative of the industrial nature of the site and the hydraulic hoists are prominent examples within the Sydney area. They demonstrate the scale and efficiency of the industrial processes undertaken at Campbell's Stores during its use as dockside goods storage.

The CMP prepared by GML in 2014 provides the following summary grading of significance that identifies the extent of integrity of the subject item:²

Exceptional Significance

- Original fabric from the first three phases of construction (up to c1885).

High Significance

- Bay 11 (Branch Stores Government Printer) c1895
- Bay 10 alterations made to facilitate construction of Hickson Road, c1915

² GML CMP, 2014, p131

- New door openings made on Level 2 as a result of the construction of Hickson Road in c1915.

Moderate Significance

- Restoration and reconstruction of fabric as part of 1970s restoration program (eg sandstone restoration of eastern ground floor facade).

Little Significance

- Ongoing restaurant fitouts that are reversible and allow for an appreciation of original spaces and fabric
- Fabric introduced to provide for new uses in 1970s works (eg service tunnel at rear, original restaurant adaptation work, etc).

Intrusive Fabric

- Fabric introduced primarily for decorative reasons (eg sailboat masts in front of the Waterfront restaurant, 'themed' fit-out works, etc, that obscure original spaces and fabric, or cause visual impacts on an appreciation of original spaces and fabric
- Fabric which obscures and contributes to the deterioration of fabric requiring maintenance and conservation works. Such fabric includes linings added to walls, cool rooms and so on.

4.2 Established Significance of The Rocks Conservation Area

The subject site, 7-27 Circular Quay West, is located within the boundaries of the *The Rocks Conservation Area*, which is listed as an item of heritage significance under the Heritage Act, s.170 of a NSW State agency heritage register of the Sydney Harbour Foreshore Authority (Figure 4.1).

The NSW Heritage Inventory contains the following information for the s.170 register listing for *The Rocks Conservation Area*.³

Statement of Significance (short form):

The Rocks, with its complex layering of significant fabric, uses and associations, is a precinct of national cultural significance. The Rocks is valued as a place of major social history, reflecting more than two centuries of significant activity; including European invasion, early contact between Aboriginal people and European settlers, and colonial settlement. The drama of cross-cultural encounters reflects The Rocks' focal location as

³ www.environment.nsw.gov.au/heritage

a place linking continental, colonial, city and maritime histories. The Rocks was saved through fierce battles for its conservation, and by government ownership. Despite ongoing incremental change in The Rocks, continuity and authenticity remain major themes, manifest in increasingly rare and fragile relics of original topography and built fabric, remnants of history and a living community.

Description: (s.170 extract)

The Rocks is sited on a rocky promontory projecting into the Harbour on the western side of Sydney Cove, with the southern approach of the Sydney Harbour Bridge along the ridge marking the western boundary of the area, and is some 21 hectares in area. The ground falls steeply to the east, in a series of sandstone escarpments, giving the important harbour views characteristic of the area. The topography gave rise to an erratic street pattern with many cuts into the rock to provide building materials and enable streets and stepped pedestrian ways to traverse the area. The conservation of The Rocks from the 1970s has reinforced these diverse streetscapes, laneways and pedestrian links.

There is a mixture of individually important buildings by significant architects and more humble shops, cottages and terraces from different eras. Within this diversity the area has a coherent and consistent character of streetscapes and urban spaces in a very strong topographical setting. It has a strong maritime character, with warehouses and bond stores, and philanthropic buildings for seamen who also were abundantly catered for in the provision of public houses. A large amount of public open space is included in the area, including Dawes Point Park, with its early fortifications and archaeological remains, Foundation Park, West Circular Quay, First Fleet Park, the public domain around the Museum of Contemporary Art, Overseas Passenger Terminal, Campbells Cove, Park Hyatt and the Hickson Road Reserve.

Discussion:

The Campbell's Stores is a significant element of the *The Rocks Conservation Area* as it provides evidence of the commercial architecture of the early 19th century and its importance to the development of the colony. Visually the Stores provide an important comparison with the later architecture of the surrounding structures, such as the Metcalfe Bond Stores (1912-16) and the ASN Company building (1887), not only in terms of scale but detailing (Figure 3.2).

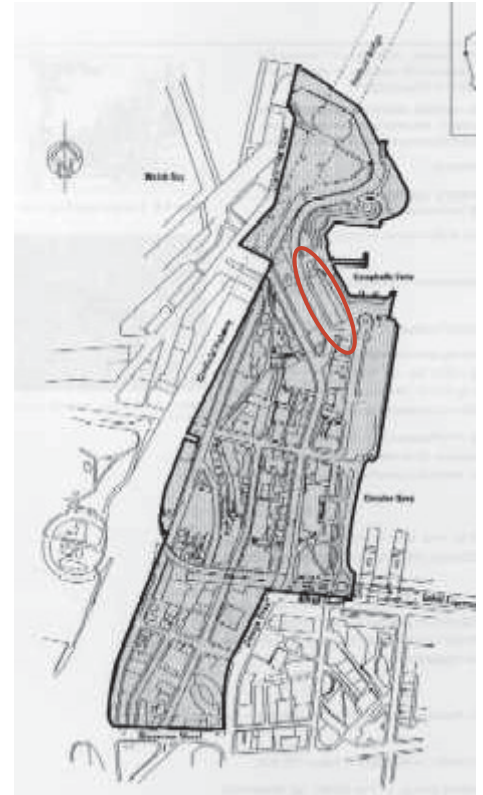


Figure 4.1
Map of SHR listing for *The Rocks Conservation Area*, with the subject site circled in red

Source: SHFA, *The Rocks Heritage Management Plan*, Volume 1, 2010

4.3 Established Significance of Heritage Items in the Vicinity of the Subject Site

4.3.1 The Rocks Vicinity

There are a number of individually listed SHR heritage items in the vicinity of the Campbell's Stores, that are listed as items of significance on the State Heritage Register (Figure 4.2). The items also appear on the SHFA Heritage and Conservation (s.170) Register as follows:

- ASN Co Building at 1-5 Hickson Road/35-45 Circular Quay West
- Bushells Warehouse and Bushells Place at 86-88 George Street
- Metcalfe Bond Stores at 68-84 George Street
- Mining Museum (former) at 36-64 George Street.

The ASN Co Building to the south and the other three buildings across Hickson Road to the west, collectively known as the Metcalf Bond Stores, are an important ensemble of structures both historically and visually. They provide historical context and a 'background' to the precinct in which the Campbell's Stores are situated at the forefront to the harbour. The three storey height of the Stores provides scale to the higher structures that surround it.

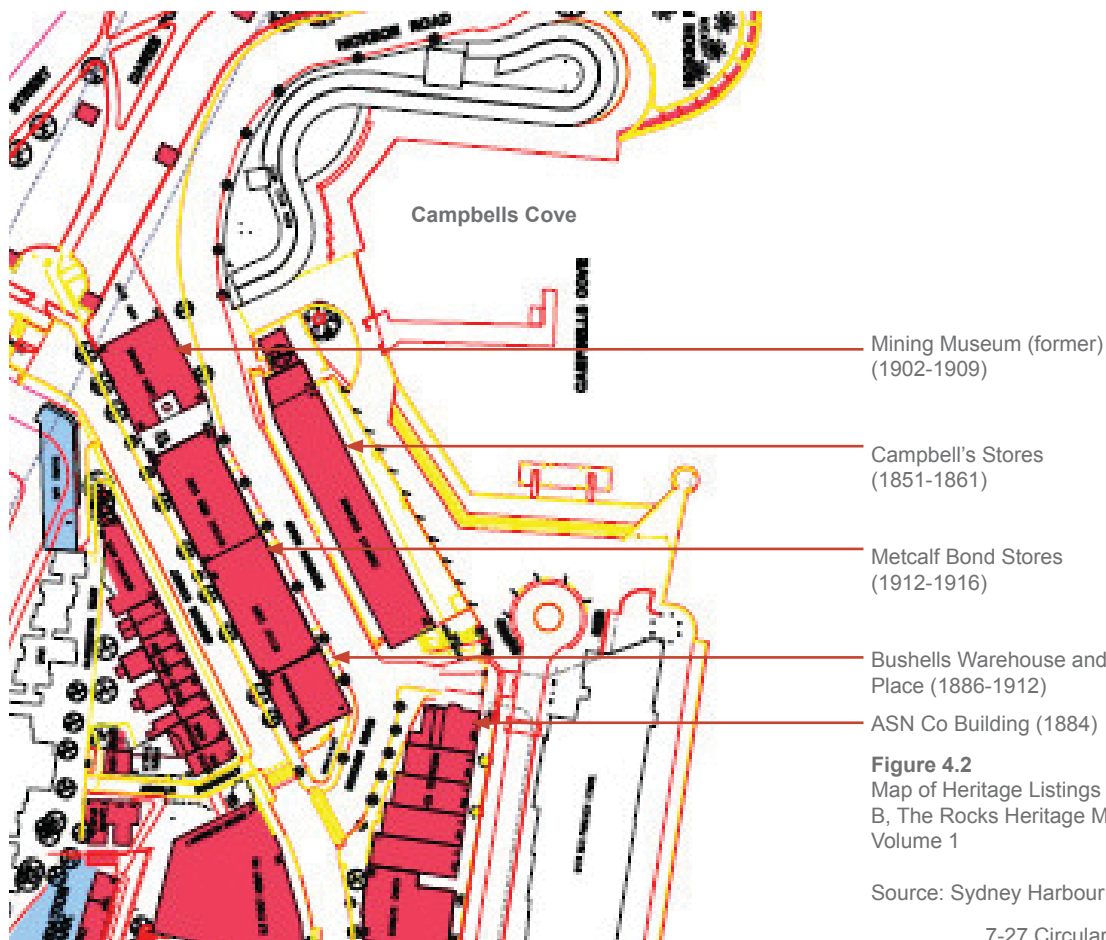


Figure 4.2
Map of Heritage Listings in the Rocks, Appendix B, The Rocks Heritage Management Plan: Volume 1

Source: Sydney Harbour Foreshore Authority

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4.3.2 Sydney Opera House World Heritage Listing

The placement of the Sydney Opera House on the UNESCO World Heritage (2005) register included a nominated Buffer Zone, which includes West Circular Quay and The Rocks (Figure 4.3). The nomination document includes the following description of the Buffer Zone:⁴

The buffer zone centres on the inner waters of Sydney Harbour. It includes places around Sydney Harbour within a radius of 2.5 kilometres that have been identified as offering critical views to and from the Sydney Opera House that contribute to its World Heritage significance.

The following are excerpts from the description included in the nomination:

Inaugurated in 1973, the Sydney Opera House is a great architectural work of the 20th century that brings together multiple strands of creativity and innovation in both architectural form and structural design. A great urban sculpture set in a remarkable waterscape, at the tip of a peninsula projecting into Sydney Harbour, the building has had an enduring influence on architecture. The Sydney Opera House comprises three groups of interlocking vaulted 'shells' which roof two main performance halls and a restaurant. These shell-structures are set upon a vast platform and are surrounded by terrace areas that function as pedestrian concourses. In 1957, when the project of the Sydney Opera House was awarded by an international jury to Danish architect Jørn Utzon, it marked a radically new approach to construction.

The Sydney Opera House constitutes a masterpiece of 20th century architecture. Its significance is based on its unparalleled design and construction; its exceptional engineering achievements and technological innovation and its position as a world-famous icon of architecture. It is a daring and visionary experiment that has had an enduring influence on the emergent architecture of the late 20th century. Utzon's original design concept and his unique approach to building gave impetus to a collective creativity of architects, engineers and builders. Ove Arup's engineering achievements helped make Utzon's vision a reality. The design represents an extraordinary interpretation and response to the setting in Sydney Harbour. The Sydney Opera House is also of outstanding universal value for its achievements in structural engineering and building technology. The building is a great artistic monument and an icon, accessible to society at large.

All elements necessary to express the values of the Sydney Opera House are included within the boundaries of the nominated area and buffer zone. This ensures the complete representation of its significance as an architectural object of great beauty in its waterscape setting.

⁴ Sydney Opera House Nomination Document,

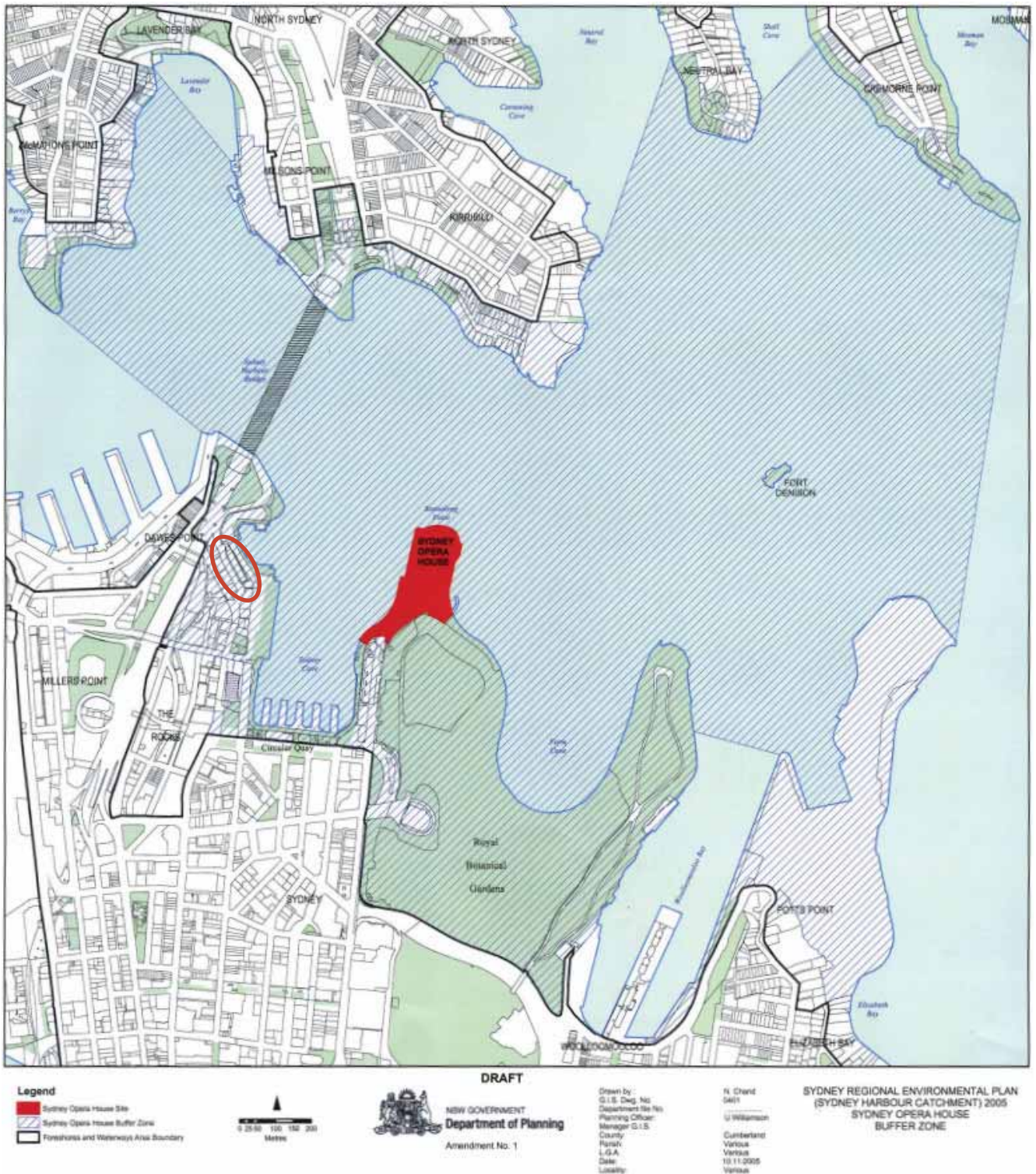


Figure 4.3
Plan of the Sydney Opera House Buffer Zone, cross hatched, with the subject site circled in red

Source: UNESCO World Heritage website

4.4 Curtilage Analysis

The NSW Heritage Office (now the Heritage Division of the NSW Office of Environment and Heritage) publication *Heritage Curtilages*⁵ defines 'heritage curtilage' as the area of land surrounding an item or area of heritage significance which is essential for retaining and interpreting its heritage significance. Heritage curtilage can be classified as one of four types:

- **Lot Boundary Heritage Curtilage:** for places where the legal boundary of the allotment is defined as the heritage curtilage. The allotment should, in general, contain all significant related features, for example outbuildings and gardens, within its boundaries.
- **Reduced Heritage Curtilage:** for places where an area less than the total allotment is defined as the heritage curtilage. Applicable where not all parts of a property contain places associated with its significance.
- **Expanded Heritage Curtilage:** for places where the heritage curtilage is larger than the allotment. Particularly relevant where views to and/or from a place are of significance.
- **Composite Heritage Curtilage:** for larger areas that include a number of separate related places, such as heritage conservation areas based on a block, precinct or whole village.

The *Lot Boundary Curtilage* is largely the footprint of the Stores building, however an *Expanded Heritage Curtilage* is the more appropriate curtilage given its location on the harbour foreshore at Campbells Cove, and the relationship to Sydney Cove and beyond.

The 2014 CMP has identified an expanded heritage curtilage that 'protects' the visual context of the Stores on the eastern side forecourt, between the Overseas Passenger Terminal and the Hyatt Hotel (Figure 4.4). The western side curtilage is protected by the Hickson Road reserve and Metcalfe Bond Stores.

The proposed curtilage that extends to the line of the foreshore, recognises the significant relationship between the Stores and the harbour promenade, the visual sensitivity of the harbour front with views from the direction of the Opera House and Dawes Point, and the harbour in general. It also recognises the urban importance of the Campbell's Cove 'amphitheatre' as a gathering point for events and pedestrian perambulation.

By virtue of proximity to other SHR listed buildings as well as being within the *The Rocks Conservation Area*, the subject site can also be regarded as having a *Composite Heritage Curtilage*. The curtilage of the ASN Co building for example overlaps, or is mutually beneficial with the curtilage of the Stores.

5 Warwick Mayne-Wilson, *Heritage Curtilages*, NSW Heritage Office and the Department of Urban Affairs and Planning, NSW, 1996

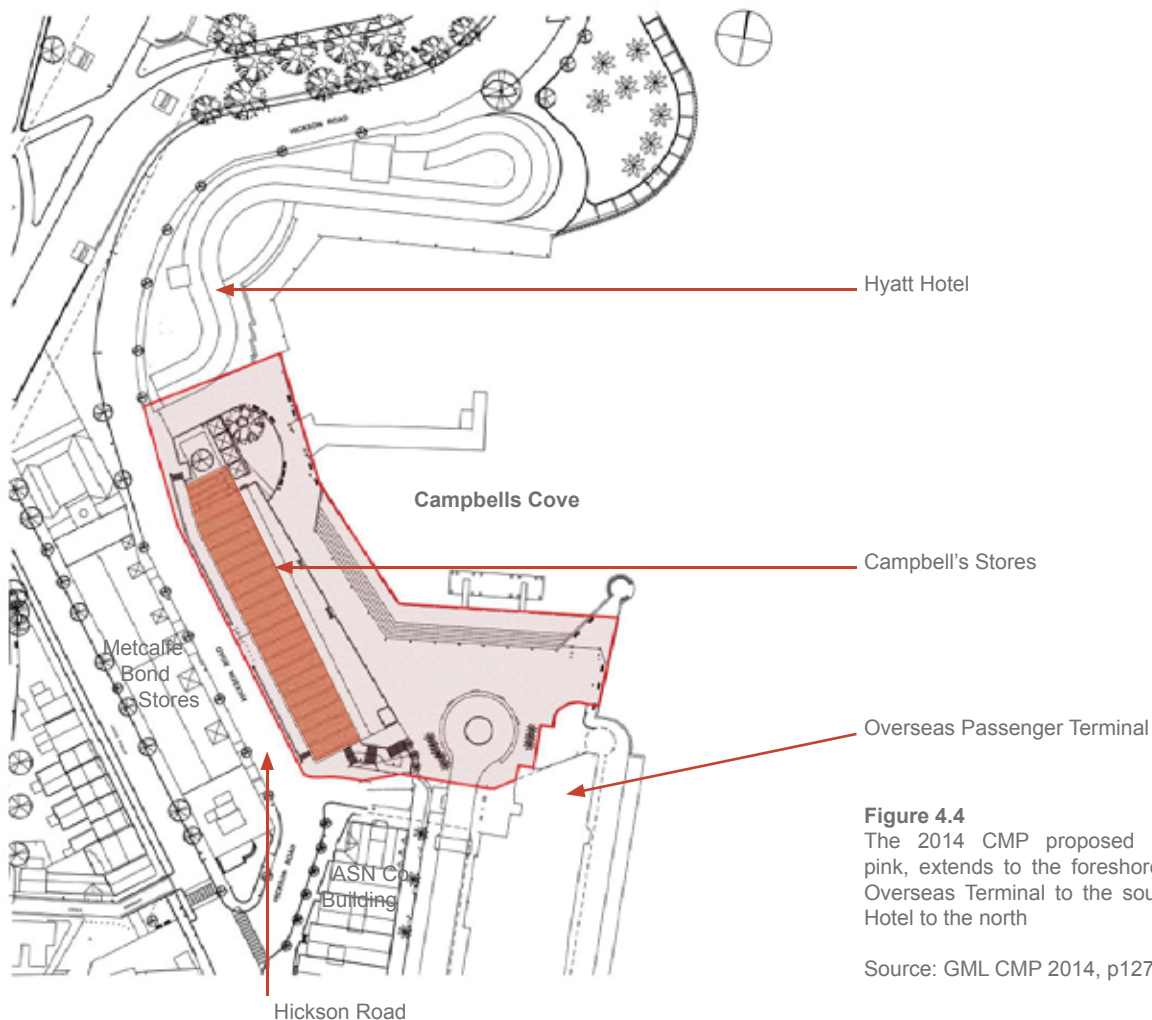


Figure 4.4
The 2014 CMP proposed curtilage, shaded pink, extends to the foreshore to the east, the Overseas Terminal to the south, and the Hyatt Hotel to the north

Source: GML CMP 2014, p127

4.5 Archaeological Potential

The 2014 CMP has referred to the potential for archaeological sensitivity, primarily in three areas that are applicable to the proposal:

- Generally sub-floor areas from Bays 1-11
- The area to the north of Bay 11 and the existing pavilion
- The forecourt area east of the subject building.

A recommendation by the CMP is for an Archaeological Assessment to be carried out before any approvals so as to determine the requirements for further investigations.

As a part of this proposal an archaeological assessment has been carried out by Austral Archaeology Pty Ltd, *Aboriginal and Historical Archaeological Assessment, Statement of Heritage Impact & Research Design*, August 2015 (draft).

In addition a number of excavated test pits have been carried out to ascertain the nature and potential of sub-floor stratigraphy. The *Austral Archaeology* report notes the following areas of archaeological potential (Figure 4.5):⁶

The assessment has identified that there is moderate potential for archaeological remains of early European land formation processes, such as reclamation, and perhaps indications of early occupation in deposits or artefacts.

There is moderate to high potential for evidence from the earliest stores built on the site, constructed by Robert Campbell, as well as associated structures, such as walls and indicators of smaller surrounding buildings, which are perhaps unmapped. Additionally, potential evidence may relate to the surrounding landscape and occupation.

While the 1850 stores are currently extant, there may be evidence which relates to their construction, modification, and over a century of occupation. Particularly in the area immediately to the north of the stores, there is high potential for remains of various structures constructed during the many decades the stores functioned as maritime warehouses.

This potential is lessened by various disturbances. Analysis of such disturbance combined with results from recent geotechnical investigations indicates that there is still moderate potential within the footprints of buildings, both demolished and extant. The area to the north is of particular interest, as it has been the site of numerous structures and their exact position, function and construction method is not entirely certain.



Figure 4.5
Areas of archaeological potential from the 2015 Austral Archaeology report. Note the areas of pink are 'High' potential, yellow are 'Moderate' and green are 'Low'

Source: *Austral Archaeology Pty Ltd*, 2015, piii

⁶ Austral Archaeology, 2015, piii

Description of the Proposal

5.0

5.1 Proposed Design

5.1.1 Introduction

The proposed development, designed, by Johnson Pilton Walker (JPW), is detailed in the following DA drawings:

- EA-A-0001-A Context Plan Existing
- EA-A-0002-A Site Plan Proposed
- EA-A-1001-A Ground Floor
- EA-A-1002-A Level 01 Plan
- EA-A-1003-A Level 02 Plan
- EA-A-1004-A Level 03 Plan
- EA-A-1005-A Roof Plan
- EA-A-1006-A Bay 12 Roof Plan
- EA-A-2000-A Elevations
- EA-A-3002-A Section - Bay 2
- EA-A-3003-A Section - Bay 3
- EA-A-3006-A Section - Bay 6
- EA-A-3009-A Section - Bay 9
- EA-A-3010-A Section - Bay 10
- EA-A-3011-A Section - Bay 11
- EA-A-3012-A Section - Bay 12
- EA-A-3013-A Section - Bay 13

GBA have also reviewed the engineering services' sketch design drawings prepared by Northrop Consulting Engineers. The proposed works for DA consent and the subject of this report, are limited to the area defined by the *Agreement for Lease*.

The aim of the proposal is to revitalise Campbell's Stores, to remove intrusive elements, carry out conservation and remedial works, upgrade facilities, and to provide a simple weather protective canopy structure to the external dining area fronting the foreshore promenade (Figure 5.1). The proposed works to the Stores ensures on-going viability while retaining the important significant features of the building.

The proposal also includes for the integration of the Stores with the surrounding precinct with a pedestrian through link from Hickson Road to the harbourside promenade via Bay 6. Although not a part of this DA, there is an additional Public Domain proposal for lowering the Promenade between the Stores eastern forecourt and Campbells Cove, the reconfiguration of the public stairway (Bay X) to the south of the Stores (Figure 5.2), as well as footpath widening of Hickson Road for increased activation, thus ensuring accessibility and the on-going viability of the area.

The proposal is summarised in the following sections, and are illustrated by indicative design drawings. Refer to the submitted DA drawings.



Figure 5.1

The proposed development with the lowered promenade and new canopy structure. Note the proposed reconfigured stairway for Bay X, adjacent to Bay 1 on the left

Source: Johnson Pilter Walker Concept Design, 2015

5.1.2 Removal of Intrusive Exterior Elements

- The demolition of the shade canopies and sail ship masts from the external dining area to the east of the Stores, Bay 1-3
- The removal of the southern brick wall duct to Bay 1
- The removal of the northern glazed pavilion that adjoins the Stores building, known as Bay 12.

5.1.3 Removal of Internal Fitouts

- Removal of numerous amenity areas including kitchens, cool rooms, toilets, and goods lifts throughout the Stores
- Removal of numerous stairways
- Removal of all services including the redundant air-conditioning and exhaust systems.

5.1.4 Conservation and Reconstruction Works

The CMP refers to the conservation works carried out in the 1970s with some ongoing maintenance as noted in the previous sections of this report. The CMP and condition reports have further identified areas of the building that require conservation and remediation. These identified areas are also evident from a number of site inspections.



Figure 5.2

The southern stairs (Bay X) with Bay 1 of the Stores on the right and the elevated roadway to the Overseas Passenger Terminal on the left

Source: Johnson Pilter Walker, 2015

The proposed works represent the next cycle of required conservation and remediation works to overcome deferred maintenance and fabric deterioration issues, and include:

- Renew the stormwater system draining the eastern side of the Stores (refer to TTW report recommendations¹)
- Provide additional sub-surface drainage, and a tide valve to limit the ingress of sea water to the building footings²
- Lower the forecourt promenade (subject of a separate DA), and ensure pavement levels fall away from the building
- Remediate the sandstone walls affected by salt and the associated problem of sub-surface moisture, with improved sub-floor ventilation
- Investigate alternative conservation options for deteriorated fabric, particularly joinery including, windows, bargeboards and loading dock aprons attached to the east elevation
- Replace the slate roof and provide maintenance to the rainwater goods
- The reconstruction of redundant internal wall and floor openings
- Reinstatement of industrial heritage.

The proposal aims to provide conservation benefit by a net reduction in the large number of openings and voids. The above works are subject to the removal of the internal fitouts and a further reassessment of the areas not currently visible.

5.1.5 New Works

The design approach of the proposal has been regarded as an opportunity for a comprehensive review of the historic configuration of the whole building, and has included a holistic design methodology for its reuse.

External:

- Improved reconfiguration of the adjacent eastern forecourt to provide a level, drained area (Figure 5.4)
- The installation of a new steel free-standing glazed canopy with a column structure located three metres from the sandstone facade, which provides weather protection for the external dining area
- The construction of a new glass block clad retail building to the north of the Stores, referred to as Bay 12 (Figures 5.8 - 5.10)
- The installation of a glazed section of roofing to the east end of the Bay 6 roof, as an element of the proposed through link from Hickson Road to the Promenade (Figure 5.4)
- The installation of new entries from Hickson Road to Bays 3, 6 and 9, including wider doorways and steel sheet canopies (Figures 5.3 & 5.5)
- The installation of gable vents to the Hickson Road elevation (Figures 5.3, 5.7 & 5.7).

¹ TTW, 2015, p10

² *ibid*

Internal Fitout:

- The installation of new amenities and kitchen infrastructure that reduces the overall number of facilities within the Stores, and includes raised floors for services' reticulation
- The installation of new stairways and voids that rationalises the current sizes and types, and allows for egress requirements
- The installation of three lifts that allows for Access requirements
- The reconstruction of existing openings in the floors and walls
- The incorporation of a passive ventilation strategy that minimises the use of a new air-conditioning system
- The incorporation of an exhaust system for the kitchens and toilets.

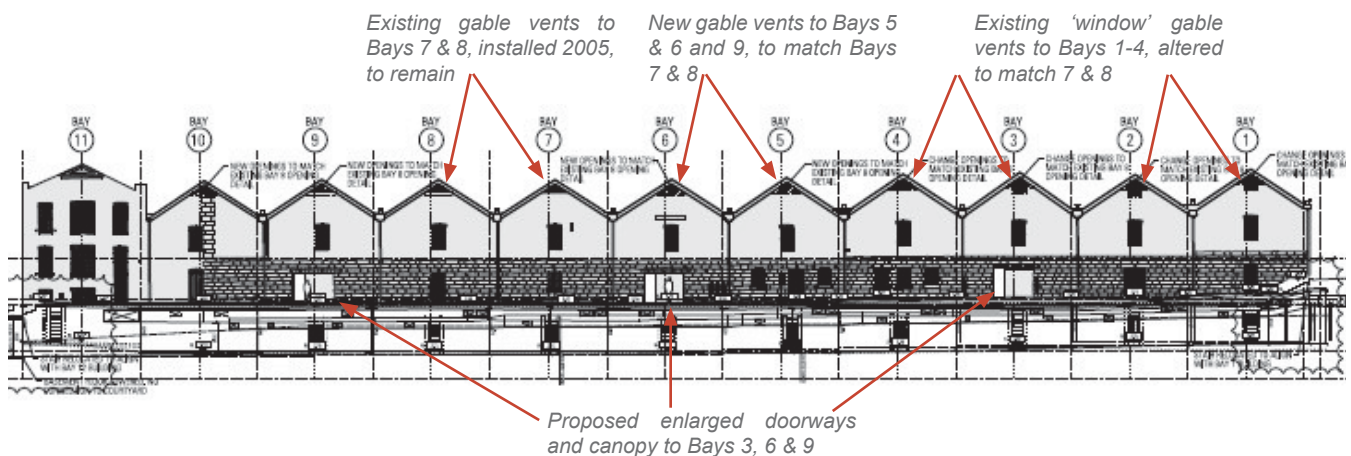


Figure 5.3

Hickson Road elevation (west) with the proposed alterations for gable vents and enlarged doorway openings

Source: Johnson Piltner Walker, part drawing SK-A-2000 (C), 2015

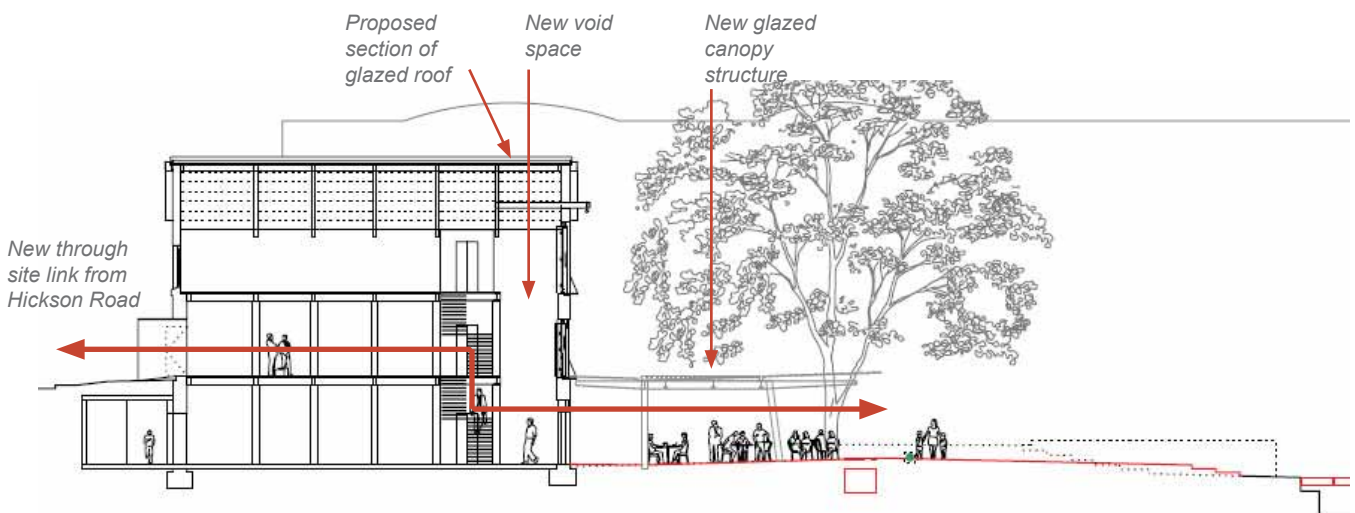


Figure 5.4

Cross section of the proposed development through Bay 6, the proposed through site link, with the lowered forecourt indicated dotted and subject of a separate DA, and the new canopy structure positioned away from the face of the heritage facade

Source: Johnson Piltner Walker Concept Design, 2015

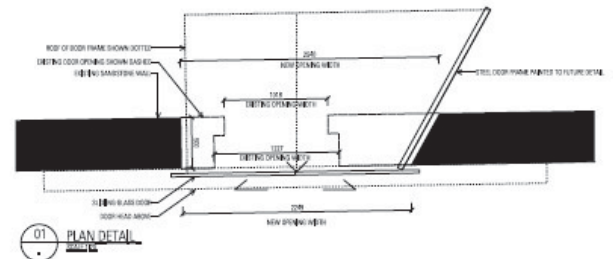
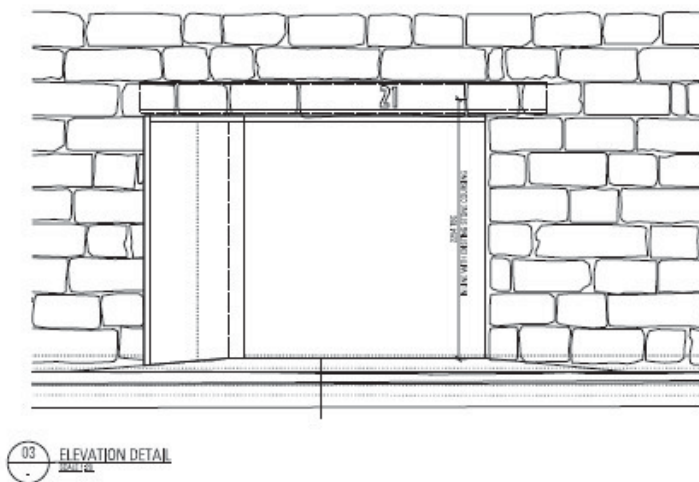


Figure 5.5 above
Proposed Hickson Road details for doorway openings for Bays 3, 6 & 9. Note the retention of the original sandstone blocks to the door head

Source: JPW part drawing SK-A-4101 (draft)



Figure 5.6 left
Hickson Road elevation (west) of the subject building with Bays 1 and 2. Note the vent openings to the gable apex, proposed to be altered

Note the two doorway openings that were later alterations of windows



Figure 5.7
Hickson Road elevation (west) of the subject building with Bays 7 and 8 and the 2005 vent openings to the gable apex, but without any louvres. These vent openings are proposed for all the gable ends to achieve ventilation requirements.

Note the later doorway opening on the left and the original window on the right at ground level

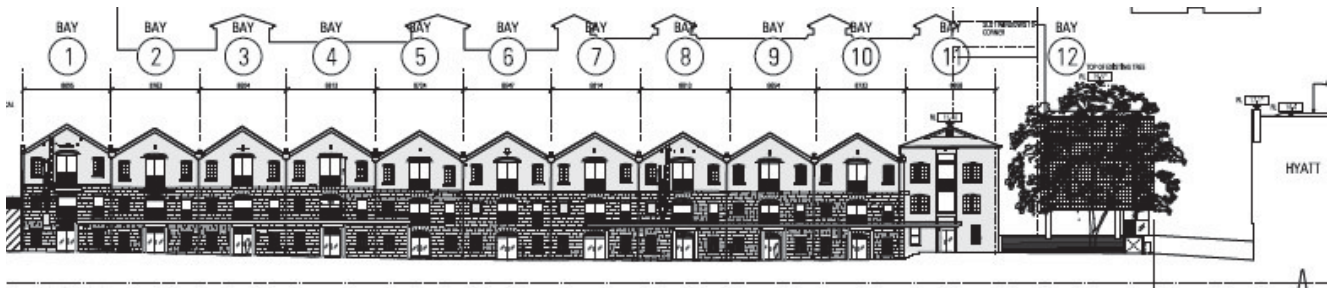


Figure 5.8
East elevation of the proposed glass block clad building (Bay 12) to the right of the Stores Bay 11, obscured by the tree, and its relationship with the Hyatt Hotel on the right

Source: Johnson Piliter Walker Drawing EA-A-2000-A, 16/10/2015

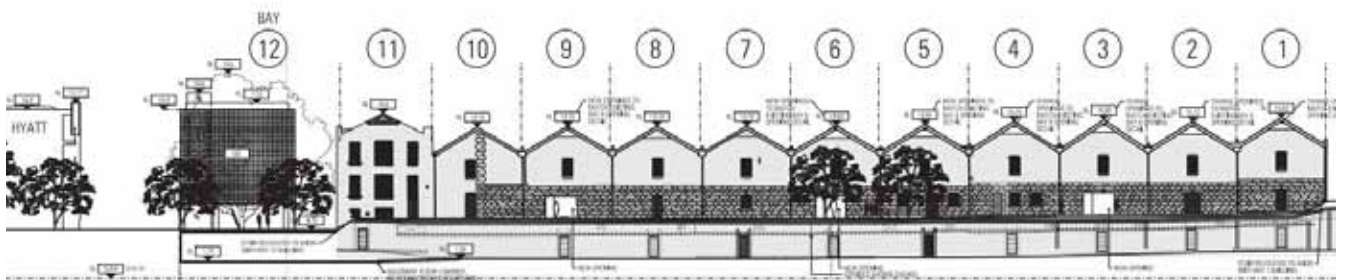


Figure 5.9
Hickson Road elevation (west) of the proposed glass block clad building (Bay 12) to the left of the Stores Bay 11. Note the height of the proposed building is in line with the Hyatt Hotel on the left and the subject building to the right

Source: Johnson Piliter Walker Drawing EA-A-2000-A, 16/10/2015

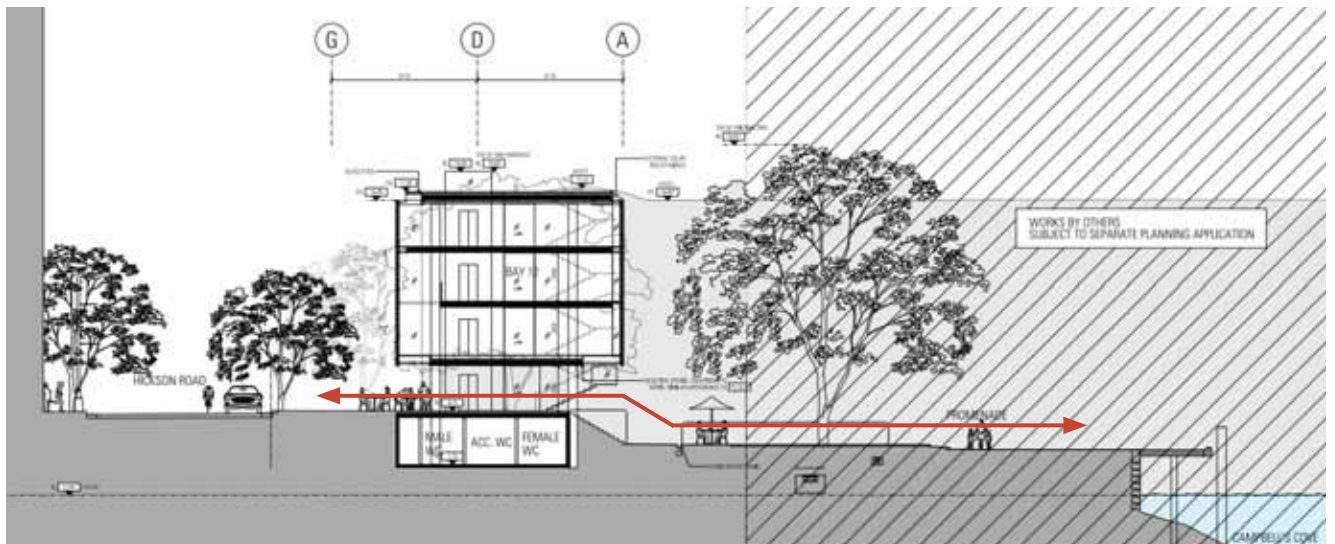


Figure 5.10
Cross section through the proposed Bay 12 building indicating the fall from Hickson Road to the Campbells Cove shoreline. Note the vista created by the undercroft of the proposed building that replaces the extant glazed pavilion

Source: Johnson Piliter Walker Drawing EA-A-3012-A, 16/10/2015

Assessment of Heritage Impact

6.0

6.1 Introduction

This assessment has been prepared in relation to the following impact assessment criteria, and the New South Wales Heritage Office (now the Heritage Division of the NSW Office of Environment and Heritage) guidelines, *Altering Heritage Assets* and *Statements of Heritage Impact*. This section of the report provides a detailed analysis of the statutory controls applying to this site, with regard to heritage, as well as reference to the policies of the CMP 2014.

6.2 Overview of the Potential Heritage Impacts

The primary heritage impacts and overall benefit of the proposed works are summarised into broad areas of approach as follows:

| Proposed Works | Impact |
|--|---|
| Removal of Intrusive Elements | |
| Sail ship masts, shade canopies, Bay 1 brick duct, Bay 12 glazed pavilion | Positive, with resultant improvement in visibility of building presentation |
| Removal of Internal Fitouts | |
| Remove various and varied facilities including all kitchens and toilets, stairs and services | Positive, as allows for the exposure of original fabric and facilitates required conservation |
| Conservation/Reconstruction | |
| Stonework conservation including the removal of salts and increased ventilation to the sub-floor | Positive |
| Civil works to address sub-surface and surface drainage | Positive |
| Conservation of deteriorated fabric, subject to further analysis | Positive |
| Reinstatement of internal and wall openings | Positive |
| Replacement of slate roof | Positive |
| Exposure of the original Bay 1 wall | Positive (subject to investigation) |
| Interpretive use of Industrial Heritage | Positive |
| New Works for Reuse Concept | |
| New consolidated and coordinated approach to restaurant reuse | Positive, for ongoing viable use and maximum public access |
| Consolidated circulation and self contained amenity pods into Bays 3, 6 & 9 | Positive use of space, minimum physical impact, and delineation of original and new fabric |
| Consolidated rationalisation of existing wall and floor openings for either retention, infill and new openings | Positive, with net reduction of wall and floor openings and a net area increase in fabric restoration |

| | |
|--|--|
| Provide a holistic design for renewed facilities with a clear delineation of contemporary stand alone structures and original fabric, which includes: <ul style="list-style-type: none"> Tenancy Fitout Guidelines Tenancy Signage Guidelines Building Signage Guidelines | Positive, with a more legible understanding of each of the original bays by the occupants |
| Services Upgrade | |
| Renewal and rationalisation of electrical and hydraulic services | Positive, with planned reticulation to minimise fabric intervention |
| Mechanical exhaust/air conditioning combined with natural ventilation utilising existing openings. Includes new metal balustrades to the loading dock doorways that references the original bars to the windows and loading dock heads | Generally positive with some adverse impact to the Hickson Road gables for reticulation and openings to Bay 11 roof |
| Through Site Link (from Hickson Road to Promenade foreshore) Responds to SHFA policy for the precinct | |
| Utilise Bays 3, 6 and 9 with increased opening width to respective doorways and associated steel canopies | Positive, with improved access but with some adverse impact on Hickson Road stonework openings |
| Incorporate a glazed roof and a void space, with selective removal of flooring and joists, to the eastern end of Bay 6 | Some adverse impact with non-traditional roof material. Mitigation by reversibility |
| Combined with proposed amenities and circulation of Bays 3, 6 & 9, for efficiency of space | Positive |
| Eastern Forecourt Upgrade for external dining | |
| Removal of the clutter of the shade canopies and sail ship masts | Positive, for the appreciation of the Stores facade as well as for the vista |
| Proposed glazed canopy with a simple structure | Positive for allowing greater exposure of the Stores facade |
| Bay 12 | |
| Construction of a new free-standing glass block clad building, elevated above the ground plane | Positive, with increased activation, through link and view lines from Hickson Road. Retains exposure of Bay 11 north elevation. Signifies urban renewal for the precinct |

| | |
|--|---|
| Separate Public Domain Works | |
| Hickson Road Upgrade | |
| Re-configuration of the footpath to accommodate al fresco dining | Positive, to provide activation and through connection with Metcalfe Bond Stores |
| Bay X Stairway | |
| Re-configuration of the stairway to the south side of Bay 1, and which includes al fresco dining | Positive, with activation and through link from Hickson Road to the foreshore promenade |
| Campbells Cove Promenade | |
| Lower the promenade height in front of the current sunken dining area to form a level promenade | Positive, for the precinct setting and appreciation of the building |

6.3 Evaluation Against the Guidelines of the NSW Heritage Division

The NSW Heritage Office (now the Heritage Division of the NSW Office of Environment and Heritage) has published the following series of criteria for the assessment of heritage impact of new development in the vicinity of a heritage item. These are answered below in relation to the impact on the property at 7-27 Circular Quay West, The Rocks Conservation Area and the heritage items in its vicinity at 1-5 Hickson Road and 36-88 George Street

The following aspects of the proposal respect or enhance the heritage significance of the item or conservation area for the following reasons:

- Consolidated capacity to refresh and renew the whole site and precinct to reverse the current fragmented arrangements
- Enhancement of the Campbell's Stores' elevation and presentation to the Campbells Cove Promenade and beyond
- Retention of the building's street presentation to Hickson Road
- Enhancement of the eastern forecourt with improved accessibility and amenity for patrons
- Retention of original internal features, such as the timber columns, main beams and roof structure, sandstone walls and timber floor boards
- The relatively understated and low visibility of the proposed canopy addition along the eastern elevation that replaces a series of intrusive canopy structures and sail ship masts
- The construction of a glass block clad building to the north of Bay 11 that will enhance and activate the area, and which replaces an intrusive structure
- Through site link in accordance with SHFA policy.

Minor Partial Demolition

The following minor partial demolition is proposed as part of this development:

- Openings in the lower ground concrete floor for lift pits as well as areas of slab removal for new services' installation
- Openings between the floors, for stairways and lift shafts
- Openings in the internal common walls between the Bays
- Enlarge three external doorway openings along the Hickson Road elevation
- Removal of some brickwork to the gable apex of three bays on

the Hickson Road elevation and the alteration of four existing louvred vents on another four bays

- Removal of the brick duct to the south wall of Bay 1, and new doorway openings in the south elevation
- Removal of some aluminium windows to the ground floor of the eastern elevation
- Form new openings to the Bay 11 roof for A/C ventilation
- Minor openings in walls and floors for services' penetrations
- Demolition of the extant Bay 12 glazed pavilion.

The relevant 'questions to be answered' in the *NSW Heritage Manual* 'Statements of Heritage Impact' guidelines relating to minor partial demolition are:

- *Is the demolition essential for the heritage item to function?*
- *Are important features of the item affected by the demolition (e.g. fireplaces in buildings)?*
- *Is the resolution to partially demolish sympathetic to the heritage significance of the item?*
- *If the partial demolition is a result of the condition of the fabric, is it certain that the fabric cannot be repaired?*

Comments for Wall and Floor openings and demolition: The proposed openings in the floors are essential for the functioning of the current restaurant use as tenancies will require access over each of the three floors. The openings in floors are confined to the dismantling of joists and not main beams, and is reversible.

Similarly, each tenancy requires access across a number of Bays to allow for unrestricted functioning and egress requirements. This horizontal circulation reduces the need for vertical circulation through the floor fabric by access to amenities on the same level as well as back-of-house ease of use.

In the case of the proposed minor external wall demolition for enlarged doorways, although not essential for the subject building to function as restaurants, the current leasor wants to increase the accessibility of the building from Hickson Road and the Metcalfe Bond Stores. This will also benefit The Rocks on a broader level and the policies of SHFA for visitor accessibility and amenity.

The removal of some brickwork for the gable vents includes a rationalisation of the existing vents to Bays 1-4 that differ from the openings to Bays 7-8. This proposal to satisfy air conditioning is a preferred option to that of intervention through the slate roof, and is an improvement on the current circumstance where A/C units are placed directly in front of open windows and are visible from the footpath.

The removal of the south wall of Bay 1 as well as the Bay 12 glazed pavilion is acceptable as they have been identified as *Intrusive* in the CMP 2014.

No important features of the building are to be removed as part of the proposed demolition. The existing sandstone walls, timber beams and columns, and industrial heritage items are to be retained.

The proposed minor demolition has to be viewed in terms of the proposed reinstatement of a large number of existing openings and voids, some of which are considered to be intrusive. There will be a net benefit to the conservation of the building in terms of reducing the random nature and size of the voids to a more legible arrangement, thus providing a sympathetic contribution to the significance of the item.

Comments for Window removal: The proposed removal of fixed pane, aluminum framed windows to the ground floor east elevation is acceptable as they have been identified as *Intrusive* in the CMP (2014).

Comments for Bay 11 roof openings: Although this roof is of significance it has also been altered and re-roofed, as well as showing signs of rust that will require replacement. The proposal is to vent through this roof in preference to the slate roofs. Although this will have minor heritage impact it is a preferred outcome in heritage terms.

The proposed minor demolition is not related to the condition of the fabric.

Conservation Works

The following conservation works are a critical component of the proposal:

- Sandstone remediation including addressing rising damp
- Joinery including windows, bargeboards and loading dock aprons
- Redundant openings
- Slate re-roofing
- Bay 1 duct wall removal
- Industrial heritage interpretation

Comment

The conservation works are necessary to retain heritage significance for the building, and are regarded as deferred maintenance.

The proposed re-roofing of the Stores with slate is replacing the extant slate with the same material. The replacement slate is to be supplied by SHFA from stored material specifically for the purpose.

Consolidated Adaptive Reuse

The following minor alterations and additions are proposed as part of this development:

- The installation of stairs and lifts to Bays 3, 6 and 9
- The provision of free-standing toilet pods within the space of Bays 3, 6 and 9

- The installation of sections of raised floors to service areas
- The installation of glazed double doors to the eastern elevation, where the timber doors are to be left in an open position
- The installation of glazed sashes to replace the aluminium fixed windows on the ground floor, for ventilation
- The installation of 'folded plate' steel canopies to the Hickson Road enlarged doorways to Bays 3, 6 and 9 for through site links
- The installation/alteration of vents to the gable apex of Bays 1-6 and 9
- The installation of new services including electrical, hydraulics and mechanical
- Fire system upgrading
- New landscaping to the eastern forecourt.

The relevant 'questions to be answered' in the *NSW Heritage Manual*, 'Statements of Heritage Impact' guidelines relating to minor additions are:

- *How is the impact of the addition on the heritage significance of the item to be minimised?*
- *Can the additional area be located within an existing structure? If no, why not?*
- *Will the additions tend to visually dominate the heritage item?*
- *Is the addition sited on any known, or potentially significant archaeological deposits? If so, have alternative positions for the additions been considered?*
- *Are the additions sympathetic to the heritage item? In what way (eg form, proportions, design)?*

Comments for Lifts: The proposed lifts are the hydraulic type to avoid a lift overrun through the roof, and instead to have a lift pit through the concrete floor, to minimise fabric intervention and adverse visibility.

Comments for Stairs: The stairs are proposed to be installed between the column grid to avoid the removal of the main structure and minimise fabric removal to floor joists and floorboards only.

Comments for New Toilet Pods: The proposal is to provide free-standing pod structures that clearly delineate 'new' and 'old'.

Comments for Raised floors: Generally the timber floorboards are to be left exposed except in service areas where they will be used for services' reticulation, thus minimising fabric penetrations and the requirement for dropped ceilings. A raised floor will also provide floorboard protection in 'abrasive' service areas.

Comments for New Glazed Doors and Windows: The existing doors do not comply with egress requirements and the ground floor fixed windows prevent ventilation. Significant timber doors and shutters will be retained in place in an open position and frameless glass installed to provide enclosure/ventilation. In the case of level 1 and 2 doors, a metal balustrade that has a similar configuration as the window bars and the metal framing to the doorway heads, is to be used for safety compliance.

Comments for Hickson Road Steel Canopies: The inverted 'L' shaped folded plate canopies to Bays 3, 6 and 9 are a sculptural intervention to 'funnel' people through the Stores building. Although this proposal will have some impact on stonework, it is in the context of a ground floor elevation that has had numerous alterations to window openings.

The proposal will involve retaining the head stones over the doorway, albeit cut for reinstatement in the supporting steel beam. This is considered an acceptable intervention relative to the precinct thoroughfare benefit for The Rocks and the Stores.

Comments for Gable Vents to Hickson Road: The proposed installation of vents to the apex of the gables avoids penetrations through the roof, and rationalises the existing two types of vents, thus providing a consistency to the elevation configuration.

Comments for New Services

The proposal incorporates new services in a manner that are consistent with heritage practice by ensuring that piping and conduits are surface mounted, not chased and therefore reversible.

Ongoing heritage advice has been provided in terms of reticulation of mechanical, electrical and hydraulic services and minimising impact of fabric intervention. This includes the location of mechanical plant in the least sensitive areas below Bay X, or existing plant located in the roof space of Bay 11. The rear services tunnel has been used for horizontal mechanical routing.

There will be a net reduction in the number of services by the rationalisation of amenities, and the use of a raised floor under the amenity footprint to avoid fabric penetrations.

There are no existing services identified as significant.

The proposed installation of three lift pits to the ground floor is the subject of archaeological assessment as referred to under section 7.2 *Recommendations*.

Comments for Fire Upgrading

The proposed exits are:¹

- Ground floor - egress direct to forecourt open space
- Level 1 - egress direct to Hickson Road open space
- Level 2 - egress via stairs to either Level 1 or ground floor.

The issue of stair egress has been fire engineered to preserve the original beam and column structure, rather than adhere strictly to minimum stair width requirements, which would have required a greater width between the timber beams and columns, thus an adverse impact on the fabric of the building.

¹ BCA Logic, 2015, p11

Comments for New Landscaping

The area of the eastern forecourt was previously the roadway of Circular Quay West before it was pedestrianised in the 1980's. Therefore it is not expected that there are former significant landscape works in the area. This is also the location of a building that may have partly occupied the site before the present structure.

As noted above, an archaeological assessment has been carried out by *Austral Archaeology* to provide advice on investigations and monitoring in areas of identified potential sensitivity.

The proposed landscape works relate to the lowering of levels of the promenade, to remove the sunken dining court immediately in front of the subject building. The proposal is a positive solution to the intrusive sunken court and will also reduce the current stormwater problem that has an adverse impact on the fabric of the building.

The lowering of the promenade (subject of a separate DA) will also have a positive impact by removing an impediment to views, to and from the subject building.

Major Additions

The following major additions are proposed as part of this development:

- A free-standing canopy is proposed to be installed along the eastern elevation to provide weather protection and amenity for the restaurant patrons.
- Bay 12 as a separate glass block clad building located north of Bay 11, four storeys high including an undercroft.

The relevant 'questions to be answered' in the *NSW Heritage Manual* 'Statements of Heritage Impact' guidelines relating to major additions are:

- *How is the impact of the addition on the heritage significance of the item to be minimised?*
- *Can the additional area be located within an existing structure? If no, why not?*
- *Will the additions tend to visually dominate the heritage item?*
- *Are the additions sited on any known, or potentially significant archaeological deposits? If so, have alternative positions for the additions been considered?*
- *Are the additions sympathetic to the heritage item? In what way (eg form, proportions, design)?*

Comments on the Canopy: The proposed canopy is a simple steel structure with a 'flat' glazed roof. The structure is single storey with a minimum height below the Level 1 floor.

The canopy line of columns are set three metres away from the facade of the item, with a projecting structure that is not attached to the sandstone wall. The glazed roof only projects across to

the building in a limited number of places for weather protection to the existing doorways, and thus a clear delineation of forms is maintained between the new structure and the heritage item.

The proposed canopy structure replaces a series of intrusive canopies. The use of an external space for al fresco functioning of the restaurants is an essential requirement of the place that activates Campbells Cove. It is therefore important that the existing canopies are replaced but with a structure that is more sympathetic to the heritage item.

The proposed canopy has been designed to minimise its impact on the heritage elevation with a rectilinear structure and column grid that relates to the gable structure of the item and its main doorway openings.

To maximise exposure of the heritage elevation the canopy is broken into two elements and stops short of the building at each end. The central break between the two sections of canopy maximises exposure of the middle Bay. Thus the proposed low profile canopy structure is subservient to the item and allows the gable elevation to be visually dominant. By its clear and simple delineation of structure, the proposal is a sympathetic addition to the item that does not detract from its significance.

Comments on the Bay 12 Building: The design approach to the Bay 12 building is to create a 'jewel box' that stands alone from the 19th century architecture of the Stores to the south, and the 20th century architecture of the Hyatt Hotel to the north. The proposed design does not 'mimic' other structures in its vicinity but provides for a 'pure' box that will be relatively neutral in its context.

The form of the proposed Bay 12 structure in plan, relates to the lines of the subject heritage building as well as the adjacent Hyatt Hotel. The proposed building height is similar to that of the Hyatt Hotel and the Stores. The proposal provides a semi-transparent neutral link between the two disparate building forms on either side in terms of materials and form.

The proposed Bay 12 structure has been separated from the heritage item by a four metre space. It is an elevated structure above the ground plane that allows for maximum visual separation from the subject item as well as sight lines to the harbour. This also allows for greater exposure of the subject item and views to it from around the immediate precinct.

The proposal allows for increased visual curtilage of the heritage item at its northern end, and is an improvement on the current intrusive pavilion.

Refer to the archaeological assessment prepared by *Austral Archaeology* as a part of the EIS submission.

New Development in the Vicinity of a Heritage Item

The subject site is in the vicinity of the ASN Co Building, the Bushells Building, the former Mining Museum and the Metcalfe Bond Stores, which are within The Rocks precinct.

The relevant 'questions to be answered' in the *NSW Heritage Manual* 'Statements of Heritage Impact' guidelines relating to major additions are:

- *How is the impact of the new development on the heritage significance of the item or area to be minimised?*
- *Why is the new development required to be adjacent to a heritage item?*
- *How does the curtilage allowed around the heritage item contribute to the retention of its heritage significance?*
- *How does the new development affect views to, and from, the heritage item? What has been done to minimise negative effects?*
- *Is the development sited on any known, or potentially significant archaeological deposits? If so, have alternative sites been considered? Why were they rejected?*
- *Is the new development sympathetic to the heritage item? In what way (e.g. form, siting, proportions, design)?*
- *Will the additions visually dominate the heritage item? How has this been minimised?*
- *Will the public, and users of the item, still be able to view and appreciate its significance?*

Comments on Items in The Rocks: The above comments with regard to Bay 12 impact on the subject item are also applicable to the adjacent items of the former Mining Museum and Metcalfe Bond Stores, except that there is greater separation from across Hickson Road (Figure 4.4).

The proposal for the Campbell's Stores will generally have a minor impact on adjacent buildings by virtue of the relatively minor nature of the new works.

The removal of the shade canopies and ship masts will have a positive impact on the visual curtilage of the ASN Co Building by eliminating visual 'clutter' in an area of visual sensitivity.

The proposed curtilage to the Stores as identified by the CMP (Figure 4.4) will benefit the adjacent buildings, including the ASN Co Building, as it is similar to a Composite Heritage Curtilage that will maintain views to and from each of the buildings around the Stores.

The proposed through link from Hickson Road to the Promenade, the widening of Hickson Road, the undercroft of Bay 12 and the proposed reconfigured stairway south of Bay 1, will have a positive overall benefit for the precinct in terms of public activation.

Impact on the Sydney Opera House World Heritage Buffer Zone

The subject site is within the Sydney Opera House Buffer Zone of the UNESCO World Heritage listing. The subject site is located on the opposite shoreline of Sydney Cove to that of the Opera House, and the main considerations, given the intervening distance, are:

- The proposed glazed canopy to the Stores' eastern elevation
- The Proposed Bay 12 glass building.

The proposed canopy, with a minimal skeletal structure and as viewed from the east, will increase exposure of the Stores from what is currently the case with the intrusive shade canopies. Together with the proposed lowering of the promenade, the outcome will have a positive impact as a vista from the Opera House.

The proposed Bay 12 glass block clad building will be located to the west, behind a large tree that will screen it from views in the direction from the Opera House. However in the future event in which the tree is removed, the proposed building, being a similar height as both extant buildings either side, will be an appropriate 'infill' addition to the line of structures facing onto, and providing a backdrop to the amphitheatre of Campbells Cove.

6.4 Evaluation Against The Rocks Heritage Management Plan: Volume 1, SHFA

The SHFA document for the management of the heritage significance of the precinct is *The Rocks Heritage Management Plan: Volume 1* (2010). Comment is provided on the following applicable extracts:

Clause 4.1.5 *Minimise Heritage Impacts*:

Principle: Activities which have an adverse impact on the heritage value of The Rocks should be actively managed to avoid or minimise such impacts

Comment

The design process for the proposal has been carefully managed by the applicant to ensure, not only conservation of the Stores building, but reconstruction of elements to increase significance. The proposal will not impact on the heritage values of *The Rocks Conservation Area*, nor the listed SHR items in the vicinity, as they do not impact on their visual sensitivity or their curtilage.

Clause 5.4.1 Retention of Significance

Heritage conservation includes all processes for looking after a place so as to retain cultural significance, including maintenance, preservation, restoration, reconstruction and adaptation.

Comment

The proposal encompasses all aspects of conservation management, from maintenance and conservation of the fabric, removal of intrusive elements, reconstruction of openings in walls and floors, and the adaptation of the building to ongoing sustainable use.

Clause 5.4.3 Conservation: Tangible and Intangible

Above and below-ground archaeological resources should be actively conserved and managed.

Comment

An archaeological assessment has been carried out and the recommendations will guide the management of any discovery of potential archaeological resources.

Clause 5.4.4 Protection:

New development should be required to comply with 'infill' principles to respect the form, scale, character and texture of The Rocks.

Comment

The proposed Bay 12 building has been designed to be a similar height as the Hyatt Hotel to its north and the Stores to the south, thus providing an 'infill' element to the shorefront line of buildings. The proposed building is relatively small in scale and the use of glass blocks provides for a 'mottled', small building unit appearance, suitable for the fine grain textures of The Rocks.

6.5 Evaluation Against the 2014 CMP Conservation Policies

The proposed development is generally consistent with the Conservation Policies of the CMP that relate to the development of the Campbell's Stores and within the Conservation precinct of The Rocks. The following is a list of applicable policies from the CMP, which relate directly to the proposed development, and our comments in response.²

² GML, 2014, p157

Policy 1

The future use of Campbell's Stores should be consistent with its outstanding cultural significance, should not impact on significant fabric and spaces, and should provide for public access to the building.

Comment

The proposal retains its current use, which the CMP has noted as acceptable. The proposal does not impact on its fabric, other than in a minor way to three of the doorways in the western elevation, and vent openings to the apex of the gables.

The minor impact to the enlarged doorways is mitigated by an increase in public access through the building, from Hickson Road down to the ground level forecourt, which will benefit precinct activation.

The design methodology proposes the reinstatement and appreciation of the spaces of the bays to a more original configuration.

Policy 8

Significant fabric should be conserved using conservation processes appropriate to the assessed level of significance. Restoration and reconstruction should aim to recover or reveal significance.

Comment

The main elements of significance, the sandstone walls and timber post and beam structure, are to be retained with a level of reconstruction of internal walls and floors that will provide a benefit in terms of reinstating heritage significance.

Policy 9

Fabric identified as Intrusive should be removed where this would recover significance, or replaced in more appropriate materials and/or finishes.

Comment

Elements that have been identified as intrusive are to be demolished, in particular the sail ship masts and shade canopies along the eastern front, the south brick wall duct, and the Bay 12 glazed pavilion.

Policy 11

External alterations or additions should be discouraged; however, if required to meet approved interpretation, re-use or cultural tourism requirements, these should be of a minor nature, and subservient to the primary architectural features and composition of the existing structure. New works should not obscure significance.

Comment

The major addition to the site is the replacement of the canopies with a steel and glass structure to provide amenity and weather protection for restaurant patrons. This is a significant benefit for The Rocks in terms of cultural tourism. The proposed canopy is a vast improvement on the existing shade structures, as it will not obscure the heritage item nor reduce the vistas to the harbour, as is currently the case.

The attractiveness of other parts of The Rocks, such as the Nurses' Walk and the Argyle complex, stems from the ability of the public to walk through laneways and explore buildings. The proposed enlargement of a number of doorways to the Hickson Road elevation, if carried out carefully, will have some impact on original fabric. However there is a considerable benefit to this part of The Rocks as it will allow increased 'permeability' of pedestrian flow from the Metcalfe Bond building and Hickson Road, through the Stores building to the Campbells Cove promenade.

The proposed alterations and additions are subservient to the significant forms of the building.

Policy 12

The spatial qualities of Campbell's Store contribute to its significance and interpretation and therefore should be conserved, as part of the ongoing use, ongoing management and any future development strategy.

Comment

The external spatial quality of the eastern forecourt will be enhanced by the removal of the intrusive canopies and sail ship masts, as well as removal of the sunken courtyard.

Internally there will be removal of a varied number and type of kitchens and bathrooms, simplified to a reduced number that will reinstate the spacial simplicity of the bays. There will be reinstatement of wall and floor openings, however in order for the building to function as a series of restaurants, it is necessary for some openings to remain, or to provide new openings for efficient use and egress requirements.

The net result is a reduction in the size and number of openings. There will also be a benefit in the visual legibility of each of the bays.

Policy 13

An appropriate physical and visual setting should be maintained for Campbell's Stores by allowing no development within the setting that would adversely impact on the place or on views to and from the place.

Comment

This policy also provides specific guidelines for a new canopy and the proposal follows these principles (Figure 6.1):



Figure 6.1

CMP Policy 13, Campbell's Stores forecourt canopy design principles

Source: GML CMP 2014, Figure 7.1B, p170

- There is a generous distance between the canopy structure and the face of the building to allow ease of access along its front
- The canopy will not be attached to the building and only sections of the canopy glass will project over to the entries to the building, thus maximising a visual separation between the two structures
- The canopy is divided into two sections with a large space between, centred on Bay 6 as the point of the east-west link through the site. The north and south ends of the canopy are also set back from the ends of the building
- The canopy structure is a single storey height, has a 'flat' roof and its column structure relates to the main doorways
- The proposed canopy is designed to be minimal in its structure and therefore have minimal impact on the setting of Campbell's Stores and the Promenade.

The removal of the raised forecourt will improve views from the external seating area out to Campbells Cove. Together with the removal of the current intrusive shade canopy, the views towards the heritage item will also be improved, allowing for a clearer 'reading' of the main elevation as seen from the Opera House.

The proposed Bay 12 building is located at the north end of the Stores and is not within sensitive view lines of the heritage item. The proposed setbacks and elevated podium design of Bay 12 will increase the curtilage of the Stores and allow for vistas from Hickson Road towards the direction of the Opera House.

Policy 14

As a necessary minimum, the ongoing maintenance should include works that will ensure that each element retains its current level of significance and not allow the loss of significance due to the deterioration of fabric.

Comment

The proposed works for the Stores include for conservation, reconstruction and remedial maintenance, as a consequence of deferred maintenance. Refer to section 7.2 *Recommendations*.

Policy 15

Features and fixtures associated with the site's historic use as a commercial warehouse should be retained and preserved. Those that have been removed should be reinstated or reconstructed and interpreted.

Comment

The proposal includes the retention and reinstatement of industrial heritage for interpretation. Refer to section 7.2 *Recommendations*.

Policy 16

The heritage significance of Campbell's Stores should be interpreted on site by appropriate methods.

Comment

Refer to section 7.2 *Recommendations* regarding interpretation.

Policy 17

No external alterations or additions should occur to the Campbell's Stores building including minor additions for services

Comment

The only alterations for services are the installation or alteration of existing openings to the Hickson Road gables and openings in the Bay 11 corrugated iron roof, for ventilation. The location of these openings have been chosen in preference to intervention into the slate roofing and roof forms of Bays 1 to 10.

The proposed openings to the gables will provide consistency to the existing varied configuration of vents in the facade. The proposed openings to the Bay 11 roof are in the roof sheeting, and do not require the installation of any ducts above the roof plane.

Policy 18

Any new development must respect the cultural significance of the property and its setting and not destroy or obscure historical associations. The introduction of new fabric should be undertaken in such a manner that it does not result in a lessening of the cultural significance of the place. New work should be identifiable as such and should, wherever possible, be capable of being removed without damage to significant fabric or spaces.

Comment

The proposed Bay 12 glass block clad building is a separate, discrete element that will not only be physically removed from the Campbell's Stores but visually distinct. There will be no doubt to the observer that the proposed design will represent a structure of its time, and provide contrast to that of the subject building.

The proposed canopy to the east of the Stores is also a separate item and its minimal design is appropriate for the location in maximising exposure to the heritage item, but providing amenity to patrons.

By virtue of their separation the new structures will be readily reversible without impact on the original heritage item.

Policy 19

The extension or alteration of existing services in Campbell's Store is acceptable in the context of re-use, but should not have a detrimental impact to the significance of the building components as a whole.

Comment

The proposed replacement of the amenities to fewer in number within the building will involve the rationalisation of services and their careful reticulation to minimise adverse impact on heritage fabric.

Policy 20

Plant and equipment should be discreetly located and is not to impact on the significant roofscape of the Stores. Ideally plant and equipment should be contained within existing ancillary structures and not impede on the original building envelope.

Comment

The air-conditioning plant has been located in the non-sensitive areas of the loading dock at the south end of the complex, and to where there is existing plant in the roof space of Bay 11. This design strategy has been specifically developed to avoid any plant on the roofs, nor any penetrations to the slate roofing of Bays 1 to 10. Necessary roof vents have been confined to the Bay 11 corrugated roof, which will need to be replaced due to its current rusted condition

Policy 21

Approaches to compliance with building ordinances for the conservation and upgrading and re-use programs of the various building components of Campbell's Store should focus on responding to the spirit and intent of the ordinances if strict compliance would adversely affect the significance. Uses which require an unacceptable degree of intervention for upgrading to ordinance compliance should be avoided.

Comment

The design approach to the incorporation of BCA requirements has used fire engineering to avoid adverse impact or alteration to significant fabric. With the addition of stairways, minimum egress widths have been fire engineered to maintain the extant column and beam structure

Policy 22

Signage and external lighting should have no adverse impact on significant heritage fabric and the overall character of the place.

Comment

Refer to section 7.0 *Recommendations* regarding signage and lighting, which is also applicable to Policy 23.

Policy 24

Any proposed work to Campbell's Stores will fully comply with all the relevant policies of the Authority applicable to development in The Rocks.

Comment

Refer to Section 6.5 of this report. The design proposal has been presented to SHFA for their comment, and will be submitted for their evaluation.

Policy 25

Any potentially encountered Aboriginal archaeological resources should be conserved in accordance with the requirements of the NSW National Parks and Wildlife Act 1979 and their potential for interpretation considered.

Comment

Refer to the *Austral Archaeology* report that addresses Aboriginal Archaeology.

Policy 26

Any potential archaeological resources on the property should be conserved in accordance with the requirements of the NSW Heritage Act 1977 and their potential for interpretation considered.

Comment

Any archaeological resources are to be protected in accordance with the relevant NSW NPWS Act 1979 and the NSW Heritage Act 1977, and as per the *Austral Archaeology* recommendations.

Policy 27

Where the site is identified as having archaeological potential all excavation should be carried out under supervision by a qualified archaeologist. Appropriate permits should be sought from the NSW Heritage Council prior to the commencement of works.

Comment

Refer to the *Austral Archaeology* report that addresses excavation and appropriate permits.

Policy 28

All applications for building modifications and fit-outs are to demonstrate measures which work toward the objectives of the Authorities Sustainability Policy.

Comment

The mechanical air-conditioning system has been designed with a strategy to maximise passive cross-flow ventilation by ensuring that existing windows and doors are able to be kept in an open position. This is to reduce reliance on an A/C system and to also reduce the adverse impact on fabric by minimising openings in walls and the roof.

Policy 29

Full and mixed mode mechanical ventilation systems (air-conditioning) in heritage buildings will not be considered by Sydney Harbour Foreshore Authority unless it can be demonstrated that passive ventilation solutions and the inherent properties of the building envelope cannot provide comfortable thermal environments throughout the majority of the seasonal calendar.

Comment

The proposed mechanical air-conditioning system is to replace an existing system and has been designed as a mixed mode system with a strategy of cross-flow ventilation as described for Policy 28.

Conclusions and Recommendations

7.0

7.1 Conclusions

- The Campbell's Stores are listed as an item of heritage significance on the NSW State Heritage Register.
- The Stores are also located within the *The Rocks Conservation Area* and is adjacent to the listed heritage items ASN Co Building (1-5 Hickson Road), Bushells Warehouse (36-88 George Street), Metcalfe Bond Stores and the former Mining Museum.
- The Stores are located within the Sydney Opera House UNESCO World Heritage Buffer Zone.
- The significance of the gable fronted building, and its ability to contribute to the streetscape and the conservation area, will be retained as well as enhanced.
- The proposed alterations and additions to the fabric of Campbell's Stores are limited to the building interiors and the rear of the property.
- The proposed adaptive reuse includes conservation and reconstruction works that will increase significance of the Stores building
- New structures are proposed with a new canopy along the eastern side of the Stores, and a new glass block clad building to Bay 12, both of which are acceptable in heritage terms as contributing to the amenity and use of the precinct.
- The proposal includes the removal of intrusive elements, including the shade canopies, the sail ship masts, the Bay 12 glazed pavilion and the Bay 1 brick wall duct.
- The removal of some original fabric is considered acceptable given that original features such as the form and fabric of the front and rear facades, original internal walls, timber beam, column and roof structure, and floorboards are to be retained.
- Where possible existing openings in internal walls have been reused. The impact of new openings in internal walls is to be mitigated by the reconstruction of a large number of existing openings with a net reduction in wall and floor openings.
- The proposal includes the incorporation of a through site link from Hickson Road to the Campbells Cove promenade

- The proposed alterations and additions to 7-27 West Circular Quay will have no adverse impact on the heritage significance of the the adjacent properties at 1-5 Hickson Road, and 36-88 George Street, or *The Rocks Conservation Area*, or the Sydney Opera House World Heritage Buffer Zone.
- Should any unexpected relics be disturbed during excavation of the site they must be managed under the Archaeological provisions of the *NSW Heritage Act*.

7.2 Recommendations

Subject to the following recommendations, Graham Brooks & Associates has no hesitation, from a heritage perspective, in recommending this application to the Minister of Planning for approval.

- Given the nature of the current building, all of the tenancy fixtures and fittings should be removed before finalising the tender documents for construction, for conservation review and detailed design.
- Ongoing design development, fabric conservation, detailed construction drawings, and construction stage works should include liaison and review by a heritage consultant
- Further investigative excavation to Pit 9 is carried out to establish the nature of the sub-floor wall thickening and depth of bearing
- An investigative inspection is carried out to Bay 1 south wall to establish the nature of the render cover and ease of removal
- An inspection of the roof and rainwater goods, including the preparation of a report, with the results incorporated into construction documentation
- An inspection of the roof structure and the areas of the deteriorated sandstone walls is to be carried out by a heritage structural engineer
- A detailed Schedule of Conservation Works is carried out by a heritage consultant and a stone conservation specialist to ensure best practice conservation of the building
- An interpretation Strategy is to be prepared for the building and the site
- An external lighting strategy is to be prepared for the building

- An external signage strategy is to be prepared for the building
- A maintenance plan is to be prepared for the building
- Tenancy fitout guidelines are to be prepared for the building
- A Tenancy Signage Strategy is to be prepared for the building
- All reports and strategies including coordination with the Public Domain of Hickson Road, Bay X and the Promenade, is to include oversight by a heritage consultant.

The recommendations of the *Austral Archaeology* report are summarised as (refer to the final report dated 2/9/2015):

- An Excavation Permit is required under Section 60 of the NSW Heritage Act 1977, for testing and salvage. This assessment report (which includes a research design) should be appended to the permit application
- Works to this site should avoid areas of high archaeological potential or significance
- If such works cannot be avoided, the area of high archaeological potential located to the north of Bay 11 should be archaeologically tested and if necessary, a salvage archaeological excavation undertaken
- Any ground works in areas of moderate potential, such as construction of elevator pits in Bays 3, 6 and 9, and column supports for the glazed canopy, should be subject to archaeological monitoring
- The limited impact of the column supports for the glazed canopy in areas of high archaeological potential should be subject to archaeological monitoring
- Should any below ground works be planned in the area of high potential located to the south-east of the store buildings, it should be archaeologically tested and if necessary, a salvage archaeological excavation undertaken
- In the event that historical archaeological relics not assessed or anticipated by this report are found during the works, all works in the immediate vicinity are to cease immediately and SHFA be notified in accordance with Section 146 of the NSW Heritage Act 1977. A qualified archaeologist should be contacted to assess the situation and consult with SHFA regarding the most appropriate course of action
- In the event that Aboriginal archaeological material or deposits are encountered during earthworks, all works affecting that material or deposits must cease immediately to allow an

archaeologist to make an assessment of the find. Section 89A of the NPW Act 1974 requires that the OEH must be notified of any Aboriginal objects discovered within a reasonable time.

- Should the proposed development be altered from the proposed concept design, then a reassessment of the heritage/ archaeological impact may be required. This includes any impacts not explicitly stated in Section 10, and includes the installation of any subsurface services such as stormwater, electrical or communications.
- Consideration should be given to interpreting any archaeological evidence that results from any archaeological investigation of the site. Interpretation should be in accordance with Policy 16 of the CMP (GML 2014:172)..

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