

SICEEP Public Private Partnership DA SSD 13_5752

Submission to NSW Department of Planning and Infrastructure
22 May 2013



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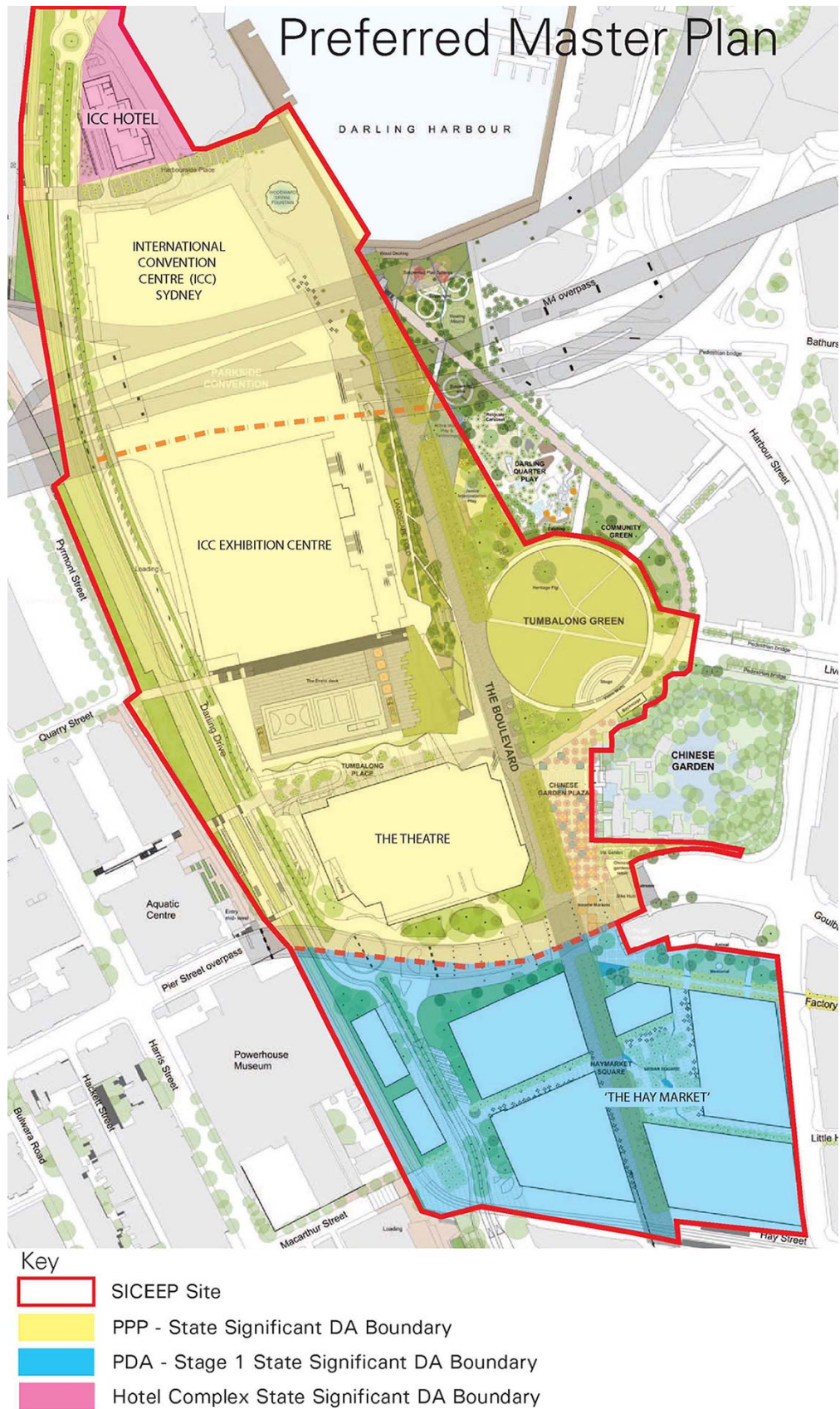


Figure 1. Project Stages
Source: Proponent SSD Application

1.0 Executive Summary

This submission responds to the development application for the Public Private Partnership (PPP) component (core facilities) submitted to the Department of Planning and Infrastructure (DoPI) by the *Darling Harbour Live* Consortium (Proponent). This follows an INSW tender process and project development agreement(s) executed for the construction and management of the Sydney International Convention, Exhibition and Entertainment Precinct (SICEEP).

It is acknowledged that the current facilities have a high usage rate, are a significant drawcard for business tourism and play a major role in supporting hotels, restaurants, shops and key workers in Chinatown, Ultimo/Pymont and the western CBD. At the same time, there is broad industry and NSW Government agreement behind the scenes that complete renewal with a changed operational format is preferable to refurbishment. Renewal of the existing exhibition, convention and entertainment centres is evidently a high priority for the NSW Government aiming to restore Sydney's competitive edge with international event markets. While the choice to renew in the existing location comes with significant temporary works, dislocation and lost revenue costs, the Darling Harbour location does provide close proximity to Chinatown, the Haymarket, the City centre, and the harbour waterfront – key location attributes for a growing Asian market offering larger high-end inbound convention and exhibition events.

With some strong qualifications, there is general support for the broad urban design principles in plan arrangement, including the major north-south pedestrian boulevard extending Quay Street, and the retention of existing open space. Exhibition buildings are often dismantled and relocated, and it is essential that the existing Cox/Arup structure is repurposed with a budget to serve unmet needs in the State (like the Sydney Fish Markets). The architecture of the proposed Convention venue requires an improved design – relying on transparent imagery when reflection is more likely.

It is imperative that Government infrastructure investment, particularly for meeting transport, childcare and future schooling needs is provided for and the City's support for the project is conditional on these needs being met.

The City acknowledges that a number of informative workshops hosted by the City were attended by Infrastructure NSW, DoPI staff, City of Sydney staff and key consortium representations in January and April 2013. This submission discusses many of the matters identified by the City at the workshops and outlines issues which have not, or could not, be resolved throughout the workshops.

The submission **Overview** is found in **Section 2** of this report.

The submission **Recommendations** are found in **Section 3** of this report.



Figure 2. Photomontage of proposed Exhibition Centre from Tumbalong Green
The architecture of the Exhibition Centre is the most individual of the three venues but the green embankment is the least resolved edge

Figure 3. Photomontage of proposed Entertainment Centre (The Theatre) from Tumbalong Green
High levels of internal illumination during the day making the glass transparent are unlikely
Architecture by HASSELL and Populous
Source: Proponent SSD Application

A. Executive Summary Outline

The key issues are as follows:

1. Reuse of the existing Exhibition Centre structure needs to be prioritised, budgeted and achieved
2. All facilities along The Boulevard must have additional active events/frontages
3. The exhibition loading dock over Darling Drive requires further detailed design resolution
4. The Event Deck must provide usable open space and the green edging should be reconsidered or refined to function without balustrades
5. The underside of Pier Street needs to be transformed
6. A number of connections within, to and from the development require more seamless integration
7. The architecture and built form of the PPP facilities require improvement
8. Active recreation spaces for locals and visitors should be included
9. The PPP sustainability infrastructure should be shared with other projects/precincts and the sustainability targets should be consistent with *Sustainable Sydney 2030*.

B. Executive Summary Expansion

The City has reviewed the PPP DA and recommends the following:

1. Reuse of the existing Exhibition building needs to be prioritised and achieved

It is well recognised that the 1988 Sulman Award-winning Cox/ARUP design for the Exhibition Centre has endured the test of time and remains a modern, unique and practical structure which is modular in nature. All possible opportunities for the dismantling and re-erection of the building, or several bays of the building, must be explored exhaustively. Examples of the kinds of re-use opportunities for the primary and secondary structure include:

- Sydney Fish Markets at Blackwattle Bay
- A country music venue at Tamworth
- An equestrian centre in the Hunter Valley
- A sports facility for a university or Defence
- Film studios

In addition to the Government's internal enquiries, the City recommends that INSW contact MPs and both Mayors and Directors of Planning at each regional council across NSW to seek projects suitable for relocating all or some bays of this modular building.

2. All facilities along the Boulevard must have additional active events/frontages

It is recommended that active retail or other land uses should be incorporated at the Boulevard level for at least 50% of the eastern frontage of the Theatre, Exhibition Centre and Convention Centre. The Exhibition Centre frontage in particular provides important opportunities for flexible retail, display or other active spaces. It is understood that the Proponent is in favour of retail activation along the Boulevard.

The Convention Centre is an obvious location for more active uses at the ground floor, having regard to the loading facilities at the rear of the building. The indicative cantilevered design causes the convention activities to be separate to ground activities and weakens the relationship of the Convention Centre with the dozens of restaurants and bars at Harbourside shopping centre.

3. The Exhibition loading dock over Darling Drive requires further detailed design resolution

The extent of the overhang of the upper Exhibition Centre loading dock over Darling Drive as a continuous 150m long concrete deck from the Western Distributor underpass is not supported in its current form. The industrial structure will make this section of Darling Drive even more service-like than it is currently, and have the feel of an unsafe and unsurveyed environment. It may also necessitate substantial structural columns within the street.

In briefings with Infrastructure NSW and the Proponent, Darling Drive was viewed as an extension of The Goods Line/Ultimo Pedestrian Network, at least to the Convention Centre light rail station. However the pedestrian and visual amenity of an extended pedestrian corridor past the Exhibition Centre loading dock would be compromised by the current design of the loading dock structure unless it is transformed into a more sculptural and well finished outcome. The loading dock should be rationalised as much as possible and moved east as much as possible to open Darling Drive to the sky. Should a significant cover over Darling Drive remain, the underside of the loading dock would benefit from compelling design attributes and lighting strategy.

4. The Event Deck must provide useable open and the green edging should be reconsidered or refined to function without balustrades

The Event Deck may prove to have limited public value and access because of physical inaccessibility due to its elevation above the public domain and managed inaccessibility due to its likely purpose as a bookable external exhibition space or delegate breakout space.

When events are underway on the Event Deck, the concept of direct access to Quarry Street may be lost due to temporary marquees, tents, canopies and restricted access. The Event Deck should remain an open deck in perpetuity

rather than covered by any permanent or even temporary awnings or structures. It is essential that greater priority and design emphasis is given to a ground level pedestrian connection from Pyrmont and Ultimo to the site crossing the light rail at a station location.

The design intention to have a green landscaped edge along the Event Deck will not be readily legible from the public domain to the canopy's elevation. The green edge may be costly, difficult to maintain and add little value to the proposal.

5. The underside of Pier Street needs to be transformed

The existing appearance of the Pier Street pedestrian underpass will be potentially substandard compared to the new entertainment, exhibition and conference facilities and the southern precinct buildings. The underpass requires a radical transformation befitting of the role of The Boulevard as the critical north-south corridor of the development. It is recommended that a design competition by domestic and international artists and architects, with the scope to include completely skinning the underside of the bridge. The existing appearance of the Pier Street/Darling Drive vehicular underpass also requires upgrading if it is to compliment the contemporary mixed use neighbourhood.

6. A number of connections within, to and from the Project require more seamless integration

The City believes that the success of the *Darling Harbour Live* proposal will be heavily dependent on how well it integrates with adjoining land and transport corridors. A commitment to public domain integration outside the Project boundaries is essential to the success of the Project. There needs to be a clear resolution of who is responsible and what processes will be put in place to fund and deliver essential infrastructure. The connections need a working group or steering committee to discuss funding, approvals strategies and implementation.

The Quarry Street link in particular (as noted in Issue 5) requires analysis, solutions, forward planning and funding. More broadly, wayfinding and seamless integration from the CBD and Central Station must be highly managed. With regard to critical connections within, to and from the Project, the following recommendations are made:

- The junction of the Exhibition Centre and the Entertainment Centre forecourt needs to allow for an influx of thousands of people at one time
- The east-west connection from Quarry Street, through the site and onto Liverpool Street, needs an improved ground level route rather than the bridge link traversing a 9m vertical rise, potentially finding that diversions may be in place during events on the Events Deck, then dropping about 15m down to The Boulevard
- Pedestrian desire lines to and from the Sydney CBD need to be examined from several viewpoints, such as the mindset of a

businessperson attending a conference, a concert goer or an international tourist, each making different decisions on how they get to their destination, what they do during a lull in attendance and what they do afterwards

- Pedestrians are not catered for along Darling Drive at all, or where there is some allowance for footpaths, these footpaths appear to terminate
- Cycling infrastructure throughout the site needs to be logically linked to existing routes and set out in a hierarchy for commuters and tourists
- There needs to be modelling of the future impacts of the Sydney Light Rail Strategy, including possible connections into the site.

7. The architecture and built form of the PPP facilities require further resolution

The height of the PPP facilities, in particular the Convention Centre, should be reduced where possible to minimise reduce impact and view loss, overshadowing and sense of bulk. Materials and finishes on large expanses need to have a greater degree of variation and richness. More waterside forecourt area for street theatre and public gathering is preferable which would entail moving the waterside façade further back from the water's edge.

8. Active recreation spaces for locals and visitors should be included

Darling Harbour is also the primary recreational green open space for many existing residents and the proposed future residents of the southern precinct project. Tumbalong Green must function as a genuine green parkland space for local families, children and youth (the nearest major parks are Wentworth Park and Belmore Park, some distance away). Lend Lease's southern precinct is providing a hard paved square only, and the additional residential population will be relying on the central precinct to meet their green space and local recreational needs. To achieve this, it is recommended that Tumbalong Green remain a public park and not become a frequent event space or an extension to the commercial exhibition offering, alienating the public (as has become the case in parts of the Domain); that two outdoor basketball courts be provided on ground; that the Event Deck have clearly delineated community usage times and range of uses; and an all-weather meeting space be provided for children.

9. The Project sustainability infrastructure should be shared with other projects/precinct and the sustainability targets should reflect Sustainable Sydney 2030

The PPP offers opportunities for integration of water, energy and waste infrastructure and services with other precincts, community and other proposed developments (particularly Barangaroo). For example, opportunities should be sought to integrate water, energy and waste infrastructure services with the Barangaroo development, and having the same proponent develop both these sites offers significant opportunities for innovation.

The City of Sydney strongly encourages the proponent to consider and meet the objectives and targets set out in *Sustainable Sydney 2030*.



Figure 4. Photomontage of proposed Convention Centre north of Pier Street
There is concern about the height of this building (winter overshadowing of the harbourside forecourt in the afternoon) and the siting in relation to the waterfront and Woodward fountain
Architecture by HASSELL and Populous
Source: Proponent SSD Application



Figures 5. and 6. Aerial views of Darling Harbour Bicentennial Projects, 1987
Buildings simultaneously under construction include the National Maritime Museum, Sydney Exhibition Centre, Powerhouse Museum (courtyard), Sydney Aquatic Centre and Harbourside Shopping Centre. The Exhibition and Convention Centre when opened in 1988 cost around \$287 million.
(Aerial Photography: Graham Jahn).

2.0 Background

2.1 Introduction

The existing Sydney Convention and Exhibition Centre at Darling Harbour completed in 1988 (Figures 5 & 6) and the Sydney Showgrounds at Olympic Park in Homebush (2000) are the primary and secondary venues for large exhibition and convention events in Sydney. Industry and professional advisers broadly concluded in 2009 that both venues had limitations and did not meet industry standards for exhibition space and flexibility, including hosting dual events.

Between 1997 and 2008, International Congress and Convention Association (ICCA) member meetings globally doubled from 3,713 to 7,475 events. In 1997, Sydney was ranked 4th in the world with 53 ICCA events, and by 2008 it was ranked 24th in the world with 64 ICCA events¹. According to Deloitte's 2009 report, future bookings for Melbourne (50) had exceeded future bookings for Sydney (43).

Background Issues

Due to increasing competition for international events principally from the Melbourne Convention and Exhibition Centre (MCEC)² and Suntec Singapore Convention Centre, concerted industry lobbying, operator advocacy and consultant reports were undertaken to persuade the NSW Government and Opposition to consider dedicated and co-ordinated event support and to commit funds³ towards expanding and upgrading facilities in Darling Harbour.

The existing Darling Harbour facilities include 27,500 m² of exhibition space in five halls of 5,000 m² each and one hall of 2,200 m². The replacement cost of the 1988 exhibition and convention facilities was estimated in the PwC report to be around \$630 million in 2010 dollars.

In 2009, Deloitte⁴ summarised the shortcomings of the venues as follows:

- Exhibition halls are too small for many events and inefficient in their operation
- Convention space lacks an adequate mix of divisibility of breakout space
- Ballroom is poorly configured and the plenary hall lacks divisibility and staging capacities

¹ Deloitte: Research Report: Sydney Convention and Exhibition Space needs, Nov 2009.

² The MCEC is owned by the Victorian Government and managed by the Melbourne Convention and Exhibition Trust. It is a 6-star green star venue, with 52 meeting rooms, grand banquet room, 5550 person subdividable plenary space and a 30,000 sqm exhibition centre with a 460 person auditorium. www.mcec.com.au

³ Deloitte ibid p26: 17 events were lost/withdrawn from Sydney due to lack of Government support.

⁴ Deloitte ibid p28

Upgrades to the existing buildings to cover roof, air conditioning, kitchen, IT and loading docks were estimated in 2010 at around \$320 million. In addition, the theatre capacity of the Entertainment Centre was considered to be part of the offering for large events, and integrated co-location was desirable. The high number of annual and bi-annual local consumer show bookings in the peak months of March and October were seen by some to disadvantage the booking opportunities for higher economic impact overseas-sourced trade shows and conventions which brought inbound business, delegates and organisers to Sydney.

Background Reports

Key reports undertaken to cement support for a high level of investment in convention facilities include:

- Report by HVS International: Revitalising the Convention and Exhibition Industry in Sydney (commissioned by TTF Australia and Property Council of Australia), February 2007
- Report by URS: Economic Impact of Expanded Convention and Exhibition Facilities in Sydney, February 2007
- Report by John O'Neill: Review into Sydney Convention and Exhibition Space, February 2008
- Report by Decisive Consulting Pty Ltd: Assessing Sydney's competitiveness in business events and the role of the Sydney Convention and Exhibition Centre, October 2009
- Report by Deloitte: NSW Ministerial Taskforce into Tourism, Planning and Investment – Sydney Convention and Exhibition Space Needs (commissioned by Tourism NSW), November 2009
- Report by PwC: A world class Convention and Exhibition Centre for Sydney: Prefeasibility Study (commissioned by Infrastructure Partnerships Australia), September 2010
- Report by Infrastructure Partnerships Australia: Towards a New Convention and Exhibition Centre Project for New South Wales, January 2011

O'Neill Report (2007-08)

In 2008, the final O'Neill Report⁵ concluded that international and national conventions should be hosted in upgraded facilities at Darling Harbour and that lower value domestic exhibition and national conventions should be hosted in upgraded facilities at Olympic Park in Homebush. This conclusion was challenged by the Exhibition Organiser's Coalition⁶ who did not want to be relegated to Olympic Park for domestic trade shows and conferences.

⁵ John O'Neill AO: Review into Sydney Convention and Exhibition Space, Feb 2008.

⁶ Exhibition Organiser's Coalition response, Feb 2008.

<http://www.specialevents.com.au/magazine/pages08/news2/208102.html>

Sustainable Sydney 2030 (2008)

At around the same time, the City of Sydney Council publicly identified an alternative location worth investigating for convention facilities in *Sustainable Sydney 2030 Vision*. The suggested location was the airspace over the rail lines at Central railway adjoining Prince Alfred Park⁷. Council's 2030 Vision briefly noted in relation to Central Station rail line airspace:

“Through development of the airspace over the rail lines the suburbs surrounding Central Station would be reconnected through new development, with walking links following the traditional street pattern. Access by rail means that future development could include major public facilities, including a Convention Centre”

p.143 Sustainable Sydney 2030

The City requested Government include consideration of the airspace over Central rail lines as a development option (refer to 2010 PwC Report).

Government Response (2008-10)

From around 2008, the lemma /Rees/Keneally Governments requested the Sydney Harbour Foreshore Authority (SHFA) in their capacity as land owner and manager to develop design proposals to rework and upgrade the existing facilities including the entertainment centre. Master plan proposals including those of COX architects, the original exhibition centre designers, were developed. However, this approach appears to not have been bipartisan and by March 2010, the Opposition Leader Barry O'Farrell announced⁸ that a fresh approach would be taken and a new fast-tracked and expanded convention and exhibition facility would be launched through a public private tender process by the Coalition while still in Opposition.

Price Waterhouse Coopers Report (2010)

In September 2010, a detailed report by Price Waterhouse Coopers (PwC) commissioned by Infrastructure Partnerships Australia updated and readdressed the rationale for convention/exhibition facilities, reviewed current facilities, considered location options for new facilities, as well as delivery and funding models and risk allocation. Five potential locations for new or redeveloped facilities were the subject of a high-level desk-top commentary and included:

- Darling Harbour (including Entertainment Centre)
- Sydney Showground, Homebush
- Airspace over rail lines at Central Station
- Glebe Island and White Bay
- Barangaroo and Millers Point

⁷ Sustainable Sydney 2030. Idea 2, page 143. www.sydney2030.com.au

⁸ <http://www.smh.com.au/nsw/convention-centre-backed-20100302-pgf0.html>

The advantages and disadvantages for the Central rail airspace in the PwC report was noted as follows:

Central Rail Airspace	
Advantages	Disadvantages
Close proximity to CBD and transport options	Costs of building over railway
Ability to continue business as usual at current SCEC site in Darling Harbour	Future use of SCEC site Darling Harbour
In line with Sustainable Sydney 2030 plan	Site not likely to be iconic from delegates point of view (not waterside)

Source: PwC Report: "A world class Convention and Exhibition Centre for Sydney: Pre-feasibility Study, September 2010

The advantages and disadvantages for an alternative Glebe Island and White Bay location in the PwC report was noted as follows:

Glebe Island and White Bay	
Advantages	Disadvantages
Waterfront site with large amount of available space	Lack of business tourism infrastructure in the surrounding neighbourhood
Ability to continue business as usual at current SCEC site in Darling Harbour	Need to create a precinct master plan to underpin development; also inability to leverage off existing infrastructure
	Other than vehicle access via Anzac Bridge there is limited public transport options to move delegates – ferry, light rail and existing bus routes are absent
	Distance from CBD
	Lack of nearby accommodation

Source: PwC Report: "A world class Convention and Exhibition Centre for Sydney: Pre-feasibility Study, September 2010

The PwC report concluded that Darling Harbour was the best location for either new or expanded (reworked) facilities due to the ability to leverage higher levels of established foot traffic⁹ for ancillary use revenues and ability to integrate with existing facilities surrounding Darling Harbour including the CBD, major hotels and transport

⁹ Darling Harbour attracts 27 million visitors per annum; p19 PwC Report: A World Class Convention and Exhibition Centre for Sydney: Prefeasibility Study

networks¹⁰. The number of existing hotel rooms (and their dependency on the existing facility being nearby) and the proximity of the Darling Harbour site to a number of stations on the existing and soon to be expanded light rail network were significant advantages for delegates and workers at the Darling Harbour location without precipitating duplicate costs.

Government Response (2011)

Although elected in March 2011, the NSW Coalition had formulated their current approach while in Opposition and adopted early actions largely consistent with the recommendations contained in the January 2011 report by Infrastructure Partnerships Australia (IPA)¹¹. These were adopted prior to taking power.

The key recommendations in the 2011 IPA report were:

- A new convention centre should be brought to the market within 6 months of the election and commissioned by 2015
- The facility should be world-class
- The site should be Darling Harbour and involve the largest possible site area (including the Sydney Entertainment Centre and car park)
- A new entertainment facility should be included for up to 10,000 people
- The project should be managed by a new agency being Infrastructure NSW with appropriate powers
- The planning risk should not be transferred to the private sector
- A period of six months should be used for project definition and EOI document
- The project should be delivered under a Public Private Partnership model like the Melbourne equivalent
- The Government should retain control over bookings
- The PPP should be based around an availability payment – with appropriate upside from related development (i.e., southern precinct)
- The project should encourage additional revenue streams to lower costs including parking, at least one premium hotel and retail
- The project should be brought forward to co-ordinate with other projects (Barangaroo, UTS, CBD etc.,)

¹⁰ p.18 PwC Report ibid

¹¹ The Infrastructure Partnerships Australia Report: Towards a New Convention and Exhibition Centre Project for NSW was endorsed/supported by Australian Tourism Export Council, Property Council of Australia, Sydney Business Chamber, Tourism and Transport Forum and assembled by PwC, Minter Ellison and Crone Studios.
www.infrastructure.org.au/DisplayFile.aspx?FileID=583

Council's Outline Submission (2012)

In May 2012, Council resolved that the City of Sydney be closely involved in the development process for the convention/exhibition project and its interface with the surrounding streets and urban conditions. This involvement would allow the City to provide input into the 'preferred project' plans by sharing expertise regarding the planning, design and access issues that affect the precinct.

In July 2012, an outline submission was provided by Council to INSW that set out a number of core design, access and built-form considerations as well as principles that should guide a high quality and integrated development on the proposed site(s) including the Entertainment Centre site south of Pier Street.

2.2 Project Chronology

The chronological order to the Public Private Partnership (PPP) State Significant Development DA includes the initial announcement of the Project, the call for development proposals and the preferred proponent selection process. A chronology associated with the Project is provided at **Appendix A**.

2.3 Workshops

In January 2013, representatives of Infrastructure NSW and the Proponents contributed to a number of workshops hosted by City staff to present the Preferred Master Plan and provide rationale with regard to the ideas and commercial imperatives behind the design solution. The workshops were generally grouped having regard to the following key issues:

- City Farm
- Stormwater and Flooding
- Architecture and Built Form
- Social and Cultural Issues
- Sustainability, Transport and Traffic.

City officers found the workshops beneficial and could serve as a model for similar consultation practices for future state significant Government projects. The workshops allowed stakeholders to understand and discount some issues, while refining other issues for the purposes of this submission. The workshops did not result in any significant alterations to the concept plan submission.

3.0 Scope of this Submission

3.1 Introduction

The City's submission responds to the development application for the Public Private Partnership (PPP) component submitted to the Department of Planning and Infrastructure (DoPI) by the *Darling Harbour Live* consortium. This follows an INSW bid process and development agreement with the Darling Harbour Live consortium for the construction and management of the Sydney International Convention, Exhibition and Entertainment Precinct (SICEEP).



Figures 7. The Public Private Partnership Component outlined in red
Source: Proponent SSD Application

The City of Sydney has previously suggested that the existing venues should continue to function in their current location up until a time when new venues are built in alternative locations. This was estimated by the City to save up to half a billion dollars in dislocation costs and lost revenue, although it would have generated site preparation costs elsewhere. This pre-tender proposal was not supported by the new Government for a range of reasons. Given that a public private development agreement is in place, the City does not propose to revisit the broader proposition that the facilities should be built elsewhere, but rather inform government on how to improve the proposal which in a modified form will no doubt proceed.

3.2 Development Application Scope

The PPP application covering the central precinct located between the southern precinct commercial development proposal (referred to as 'The Haymarket' by the consortium) and the future 900 room Hotel project to the north of the site adjoining the Harbourside shopping centre. The site is identified as State Significant development, making the Minister for Planning and Infrastructure (or delegate) the consent authority. It is likely that the project will be delegated to the Planning Assessment Commission (PAC) for determination given the high probability of objections. If not the PAC, the Minister will be the consent authority.

The application is not a staged DA (Concept Plan). It is a detailed DA for approval to proceed and demolish, construct and operate. The development application seeks approval for the following:

- Construction of and use of the Convention Centre
- Construction and use of the Exhibition Centre
- Construction and use of the Theatre (Entertainment function)
- Construction and use of retail, car parking and ancillary uses
- Demolition of existing structures
- Site remediation and preparation
- Landscaping works including expansion of Tumbalong Green
- Central pedestrian spine known as The Boulevard
- Pedestrian bridge over Darling Drive to Quarry Street
- Retention of cascade water feature in front of convention centre
- New square in front of Chinese Gardens
- Realignment of Darling Drive
- Retail kiosks
- Utilities augmentation
- Building signage

- Upgrade and amendment of the Sydney Metro Light Rail office/maintenance facilities and stations

3.3 Development Metrics

According to the Environmental Impact Statement (EIS), the application provides for the following:

- 14.84 hectares central precinct PPP site area
- 35,000 m² dedicated internal exhibition space (including 2,500 m² in convention centre) [existing 27,200 m²]
- 5,000 m² of addition exhibition capacity in multipurpose spaces (such as Banquet and Formal Meeting spaces)
- 8,000 m² meeting rooms [existing 4,336 m²]
- 4,400 seat convention (2,500 and 1,000 dedicated plenary spaces plus additional flat floor spaces) [existing 4,500]
- 2,000 seat banquet [existing 1,000]
- 8,000-9,000 seat theatre (6,000 delegates in convention mode) [existing 10,000 – 12,000]
- 5,000 m² event deck

3.4 Consortium

The development consortium is presently named *Darling Harbour Live* which consists of the following consortium members:

- Lend Lease
- Capella Capital
- AEG Ogden
- Spotless

Architecture

The architectural team for all three precincts includes the following:

- Office for Metropolitan Architecture (OMA)
- Populous
- HASSELL
- Denton Corker Marshall (DCM)
- Allen Jack + Cottier (AJ + C)

The lead architects for this precinct are HASSELL and Populous.

Existing Convention Centre and surrounds



Figure 8. Convention Centre by John Andrews International and built by Balderstone
Construction value \$150 million (1985-88) and opened in 1988

Figure 9. Convention Centre Forecourt without performance event
Source: City of Sydney

Existing paved public spaces



Figure 10. Convention Centre forecourt where amateur performers draws large crowds in the open air

Figure 11. Central pedestrian walk with linear water feature known as the 'Urban Stream'

This feature was designed by McConnel Smith & Johnson the overall master planners for Darling Harbour

Source: City of Sydney

Existing green public spaces



Figure 12. Green space referred to as Tumbalong Green

Figure 13. Green embankment concealing Exhibition Centre car park

This embankment provides on grade access to the exhibition halls. The shade is cast by the freeway overpasses constructed in the 1980s
Source: City of Sydney

Existing Darling Drive (originally termed 'Western Boulevard' in 1988)



Figure 14. Darling Drive and the Darling Harbour venues are separated by a service road at the rear

Figure 15. Darling Drive with public car park to the left and monorail to be removed

This corridor includes light rail alignment, four lane heavy vehicle route and elevated monorail from 1987 (which will be dismantled in July 2013). The rear of the 1988 Harbourside Shopping Centre designed by RTKL Associates and Clarke Perry Blackmore (Architects Oceania) is situated on the right

Source: City of Sydney

Existing Exhibition Centre and surrounds

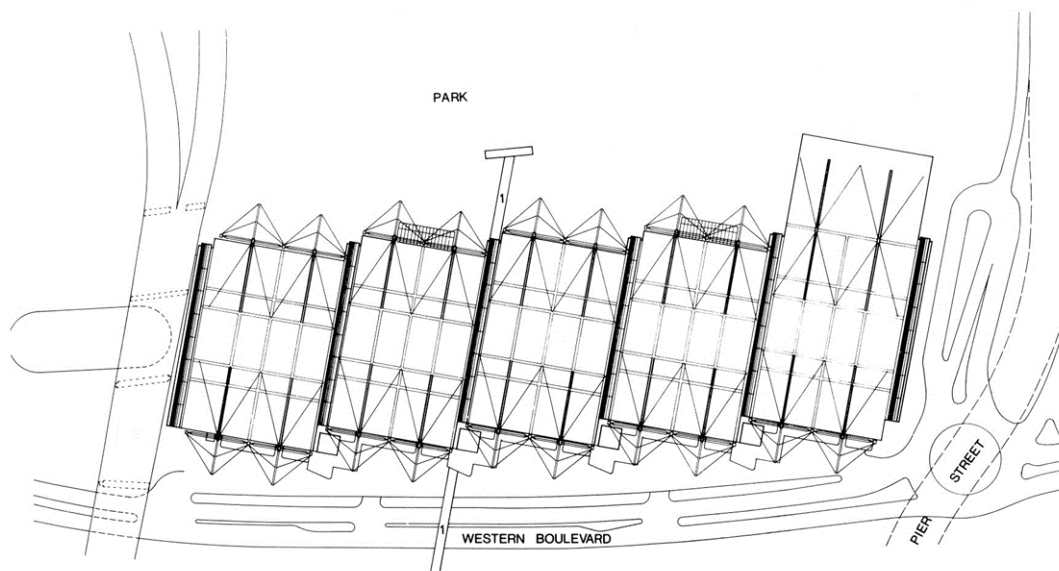
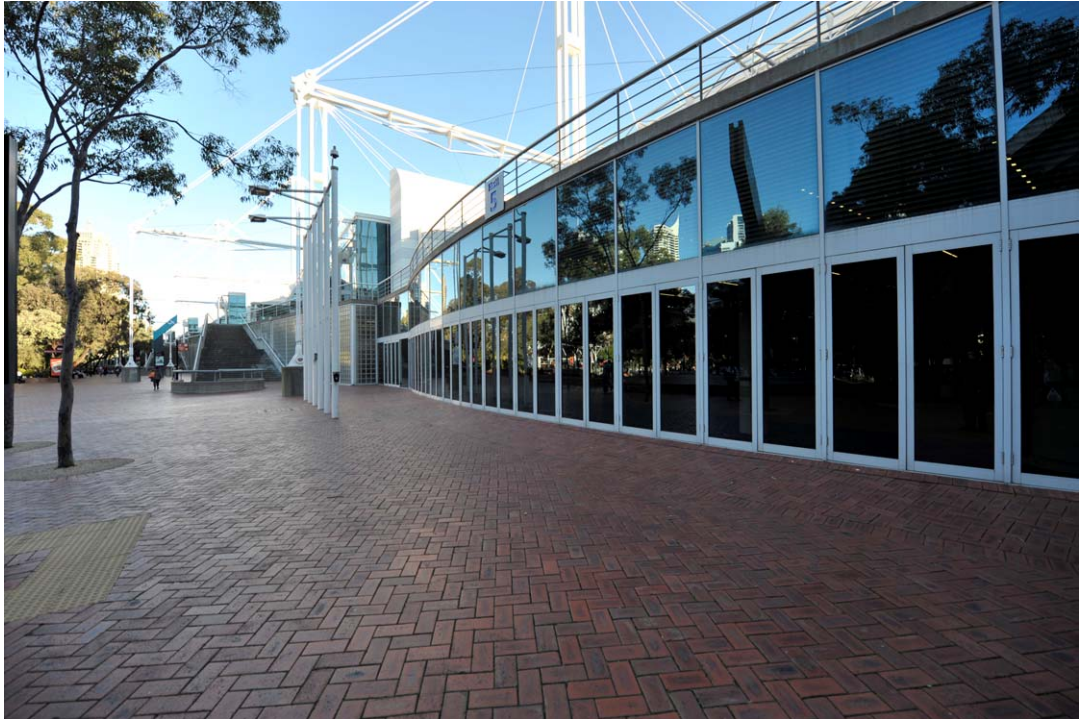


Figure 16. Parkside elevation of the 1988 Exhibition Centre by architects Philip Cox, Richardson, Taylor & Partners
Source: City of Sydney

Figure 17. Exhibition Centre presentation plan by Philip Cox, Richardson Taylor & Partners
Source: p.45 Graham Jahn, Contemporary Australian Architecture, 1994

4.0 Discussion of Issues

4.1 The making of Darling Harbour – 1984-88

The impetus for the redevelopment of what was a coal-dusted bay of nineteenth century rail sidings and finger wharves began with the dramatic urban intervention of the Sydney Entertainment Centre. At the time, this almost alien structure, built on the corner of Hay and Harbour Streets between 1979 and 1983, was surrounded by a brick and timber landscape of old warehouses, markets and rail corridors.

Designed by Edwards Madigan Torzillo & Briggs¹², the austere concrete monolith (since reclad in aluminium panels) established the seminal theme of what Darling Harbour has since become – Sydney's centre for visitor attractions.

The ferment of ideas for Darling Harbour was clearly based on international exemplars of waterfront urban renewal. The key architectural and business model references at the time included Harbour Place in Baltimore¹³, Granville Island in Vancouver and Faneuil Hall Marketplace in Boston renowned for its shopping, dining and street theatre.

Four years out from the 1988 Bicentennial celebrations, which marked the 200th year since British settlement in Sydney, Labor Premier Neville Wran¹⁴ announced in May 1984 that Sydney's Darling Harbour would be redeveloped into a 'world class' waterfront location with attractors such as museum, aquarium, retail, conferences, exhibitions, promenades, parks, gardens, parking, hotel/casino (and monorail).

In contrast to the current proposal, Premier Wran's vision would be publicly funded and a new *Darling Harbour Authority Act* was immediately established and operational within four months. The Act unfortunately removed planning and development control from the City of Sydney, and broad-scale demolition and construction proceeded at a break-neck speed, guided by a sketch master plan. All projects were fast-tracked and contractor driven, with some foundations laid (including the Exhibition Centre) before the buildings were fully designed.

Although not entirely completed on time, Darling Harbour was opened in spectacular fashion by HRH Queen Elizabeth in May 1988 with a State Banquet. The 'high technology' family entertainment venue 'Discovery Village' (and the later 1997 Sega World) failed to survive, while the 540 seat IMAX Theatre was later added to the mix.

¹² Edwards Madigan Torzillo & Briggs designed the High Court of Australia and the National Gallery of Australia in Canberra and specialised in concrete structures and off-form finishes.

¹³ Even the names given to the Darling Harbour projects reflected the overseas precedents – Harbourside Festival Marketplace was based on Baltimore's Harbour Place.

¹⁴ The NSW Labor Government introduced sweeping reforms to planning legislation in 1979 through the Environmental Planning and Assessment Act following the lengthy four-term Premiership of Robert Askin. These reforms were part of a package including the 1977 Heritage Act and the 1979 NSW Land and Environment Court Act.

4.2 The remaking of Darling Harbour – 25 years on

The core business-related facilities of Wran's waterfront showcase, once dubbed *possibly the most controversial urban project in our short history*¹⁵ are the essential elements of the proposed renewal of Darling Harbour by the O'Farrell Government.

The key feasibility move behind the current proposal is the demolition and relocation of the 1983 Entertainment Centre (renamed The Theatre) north of Pier Street, so that the large site is made available for market-led property development by the consortium developer partner Lend Lease. This move generates equity and revenue which in turn lowers the cost to Government for obtaining use of the venues.

The Environmental Impact Statement (EIS) shows that the proposed renewal does not radically change the number of seats for events or the number of square metres for exhibitions. A comparison between what is there now and what is proposed in numeric terms reveals that some capacities increase (indoor exhibition space), others remain relatively neutral (conference seats), others significantly increase (separate meeting rooms) and others decrease (theatre seats).

Evidently the case for renewal is not about a sheer increase in metres or seats but the transformative benefits of technology, format and interoperability. The additional \$700 million or more to renew the operations over and above the likely \$350 million or so to refurbish and update the existing facilities, must be a compelling case. But the case is not clearly made in either the documentation or more broadly for the public to understand. The functional benefit of many more meeting rooms over increased plenary seats (so that professional conferences can conduct their business as well as meet) and the advantages of hosting dual but separate exhibitions (one above the other) attached to dual but separate conferences (that literally don't meet in the hallway) has not been well articulated to date and needs explanation.

Yet, there must be a strong belief by the NSW Government and the Proponent that a greater share of the international convention market will be drawn to Sydney if reformatting changes are made. It is agreed that the visitor numbers (delegates, presenters, advertisers and organisers), if indeed drawn, will add significantly to Sydney's increasingly service-based economy, which in turn will be increasingly dependent on geographical tourism over a fading secondary export trade.

The City is not in a position to comment on the business case or the value-for-money proposition associated with the remaking of Darling Harbour. However, the City of Sydney generally supports concerted Government focus on positioning Sydney as a viable economy in future-based industries like business tourism and international education, both areas of which the City's economic research suggests, will be income earners for future global Sydney.

¹⁵ p.56 Roger Pegrum: "From Yulara to Darling Harbour", *Australian Architects: Philip Cox, Richardson, Taylor and Partners*

4.3 Re-use of Exhibition Centre

The project application proposes that the most notable architectural work in Darling Harbour, the Sydney Exhibition Centre, is to be sacrificed by the development. However, it is universally recognised that the Sulman award-winning Philip Cox/Arup designed five bay modular building has endured the test of time and remains a modern, practical structure with an authentic structural logic.

Where required, exhibition buildings, like those for world expos and world fairs are often moved and reassembled. The City believes that the Government has a moral and financial responsibility to reuse this structure profitably for a worthy cause, and that not enough focus or budget allowance has been brought to this task.

All possible opportunities for the dismantling and reconstruction of the building, or several bays of the building, must be exhausted before demolition starts. Potential prominent re-use opportunities include:

- Sydney Fish Markets
- An equestrian centre in the Hunter Valley
- A gymnastic sports facility for a University
- Music venue in a regional or country city
- Large column-free film studio
- Museum for large objects (aircraft or weaponry)

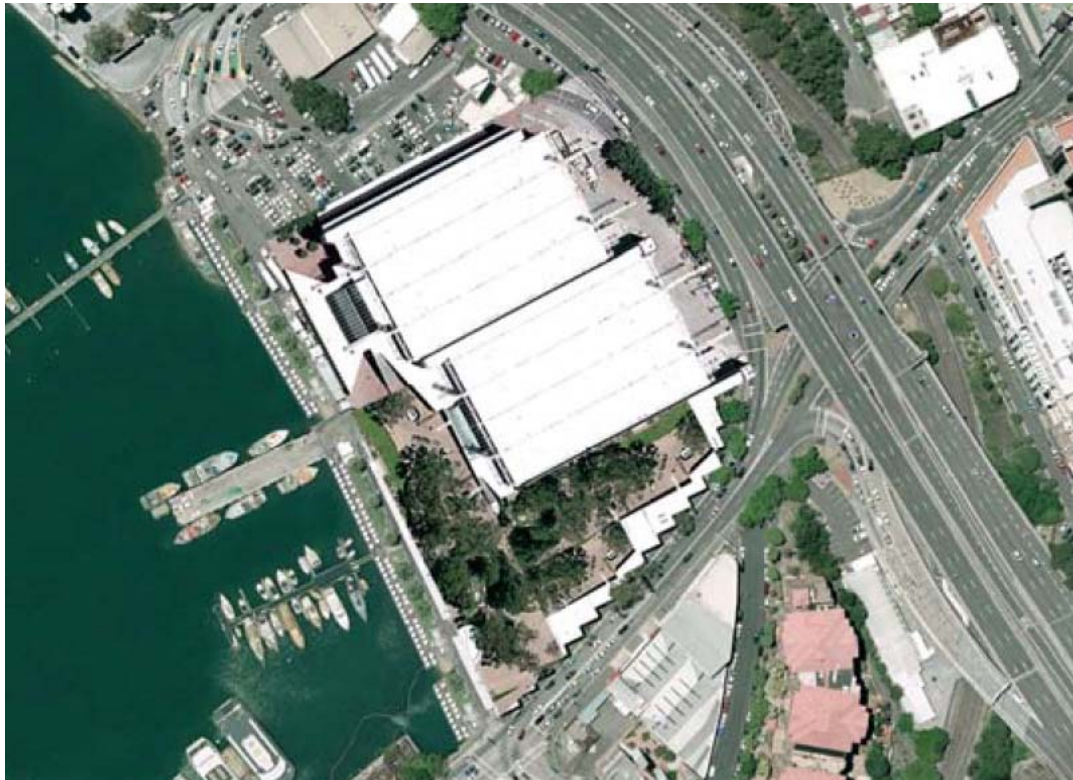


Figure 18. Photomontage of Exhibition Halls potentially reused to house the Sydney Fish Markets
Source: City of Sydney

Recommendation 1:

All opportunities to re-use the existing Exhibition Centre need to be exhausted before demolition is commenced.

4.4 Active frontage for Exhibition, Convention and Entertainment Centres

The Boulevard

It is recommended that active retail or other active land uses be incorporated along The Boulevard for at least 50% of the eastern edge of The Theatre, Exhibition Centre and Convention Centre frontages. These could be 'pop up' (designed well) or integrated into the landscape edge. The DA indicates that The Theatre and 'Tumbalong Place' are to have restaurants or cafes for activation. Presumably The Theatre frontage to The Boulevard may only be active when major events are taking place. Nevertheless, the provision would be in place for extended trading. Tacky and clutter dependent uses must be avoided at all cost.

The Exhibition Centre frontage is shown somewhat confusingly on the Public Domain Master Plan as having 'RETAIL'. It is understood that the Proponent is open to retail frontage to The Boulevard. The City believes that the proposed landscape terracing/folds have limited utility due to the steepness and landscaping, and that at least 50% of the frontage, should have an active edge.

If the frontage cannot be taken up by retail or similar, the City encourages that small exhibition/display spaces be dedicated to local innovation and promotion, such as for NSW Trade and Investment to market to visitors of the PPP facilities.

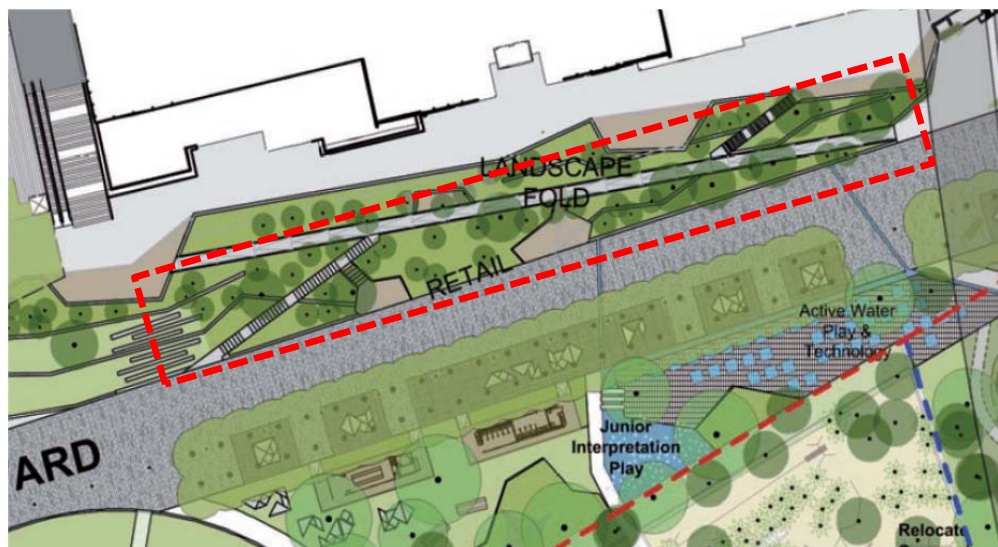


Figure 19. Active frontage required along The Boulevard
Source: Proponent SSD Application

Recommendation 2:

Active uses should be incorporated for at least 50% of the frontage of the Exhibition, Convention and Entertainment Centre to The Boulevard.

Tumbalong Place

The northern side of Tumbalong Place indicates a proposed blank wall along the southern elevation of the new Exhibition Centre. In this location there should be a continuation of the quality of the southern side of the plaza with its retail and outdoor seating character. Consideration should be given to providing a 4-6 metre wide retail linear tenancy edge (real coffee with character for locals and commuters, wine bar at night with event catering) to permanently activate the northern side of the plaza.

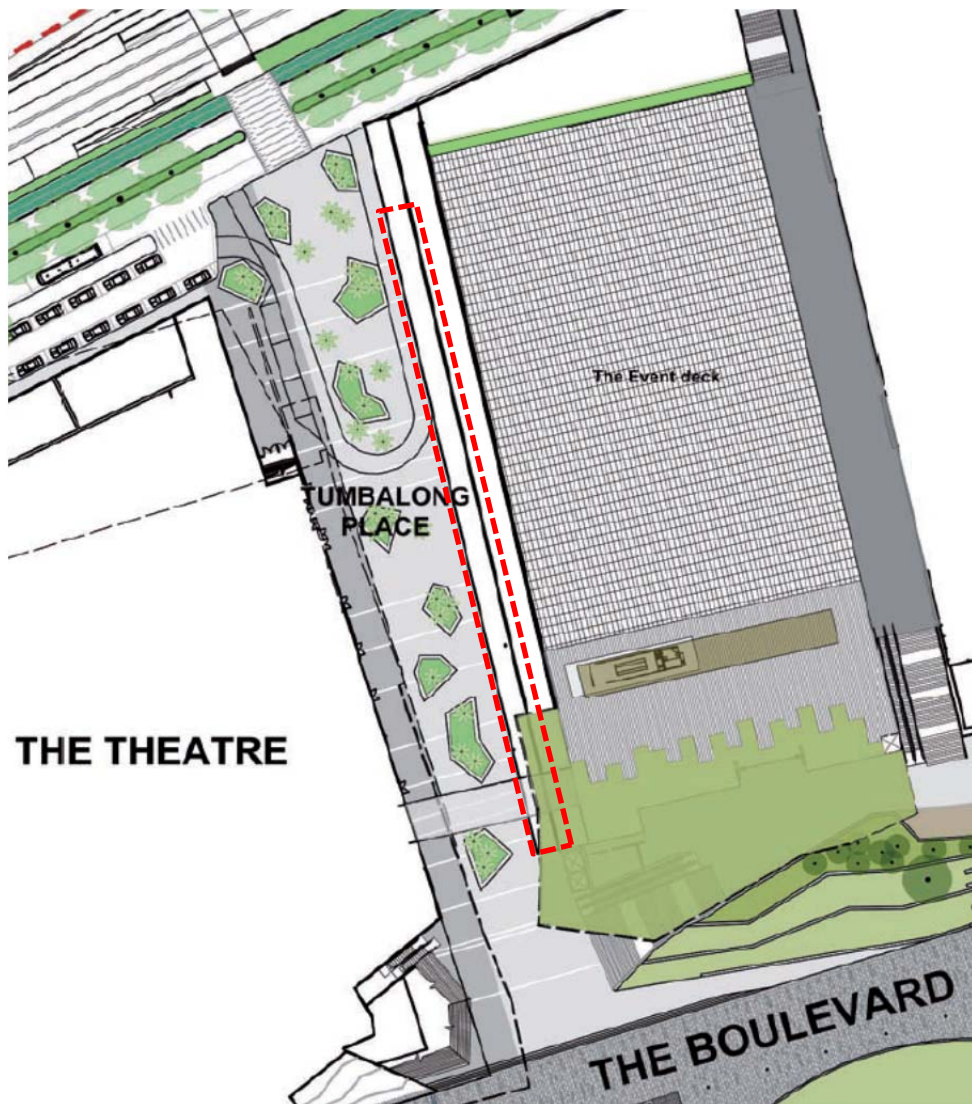


Figure 20. Opportunities for small retail tenancies along northern edge of Tumbalong Place
Source: Proponent SSD Application

Recommendation 3:

A linear edge of retail tenancies about 4-6 metres wide should be incorporated on the northern side of Tumbalong Place even if the pedestrian space is slightly narrowed.

Convention Centre

The Convention Centre is a logical location for active uses at the ground floor. However, the design proposes an elevated terrace all around the north-eastern corner. This separates ground activities (and the relationship of the Convention Centre) with the dozens of restaurants and bars at Harbourside shopping centre. It is recommended that the internal levels should be lowered to match the north-eastern corner of the building at the forecourt level. Figure 21 shows the desired extent, with this area being chosen carefully to include lift shafts and stairs/ramps for access between two levels. There are no obvious flood-level of climate change impediments in this location to reducing the floor level.

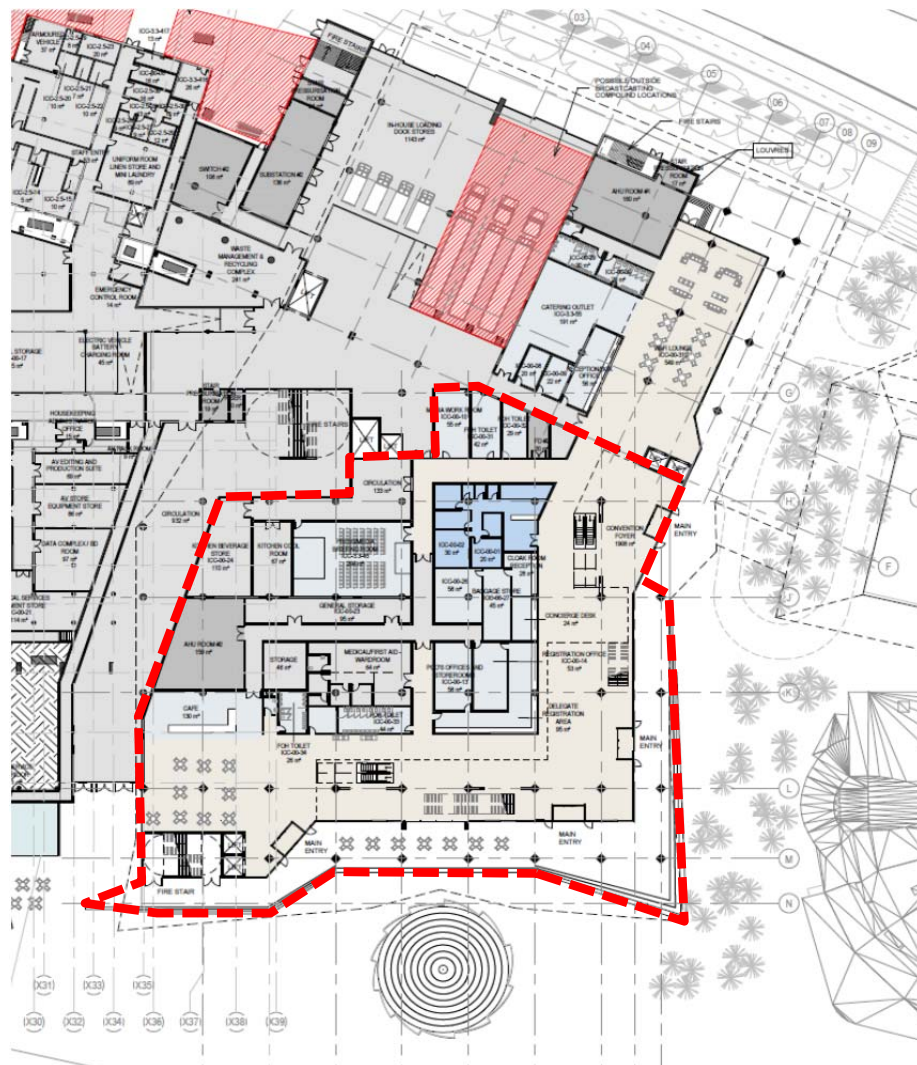


Figure 21. Extent of Convention Centre to be lowered to forecourt level
Source: Proponent SSD Application

Recommendation 4:

The internal floor levels of the Convention Centre should be adjusted so to ensure that an active frontage is provided at the forecourt level.

The Theatre

The depth of retail tenancies along a large portion of Tumbalong Place is limited by the car parking layout located behind (Figure 22). There are only 8 car parking spaces behind the retail tenancies, but a disproportionate amount of circulation space to access them. It is recommended that the car parking spaces be deleted and the depth of the retail tenancies be proportionately increased.

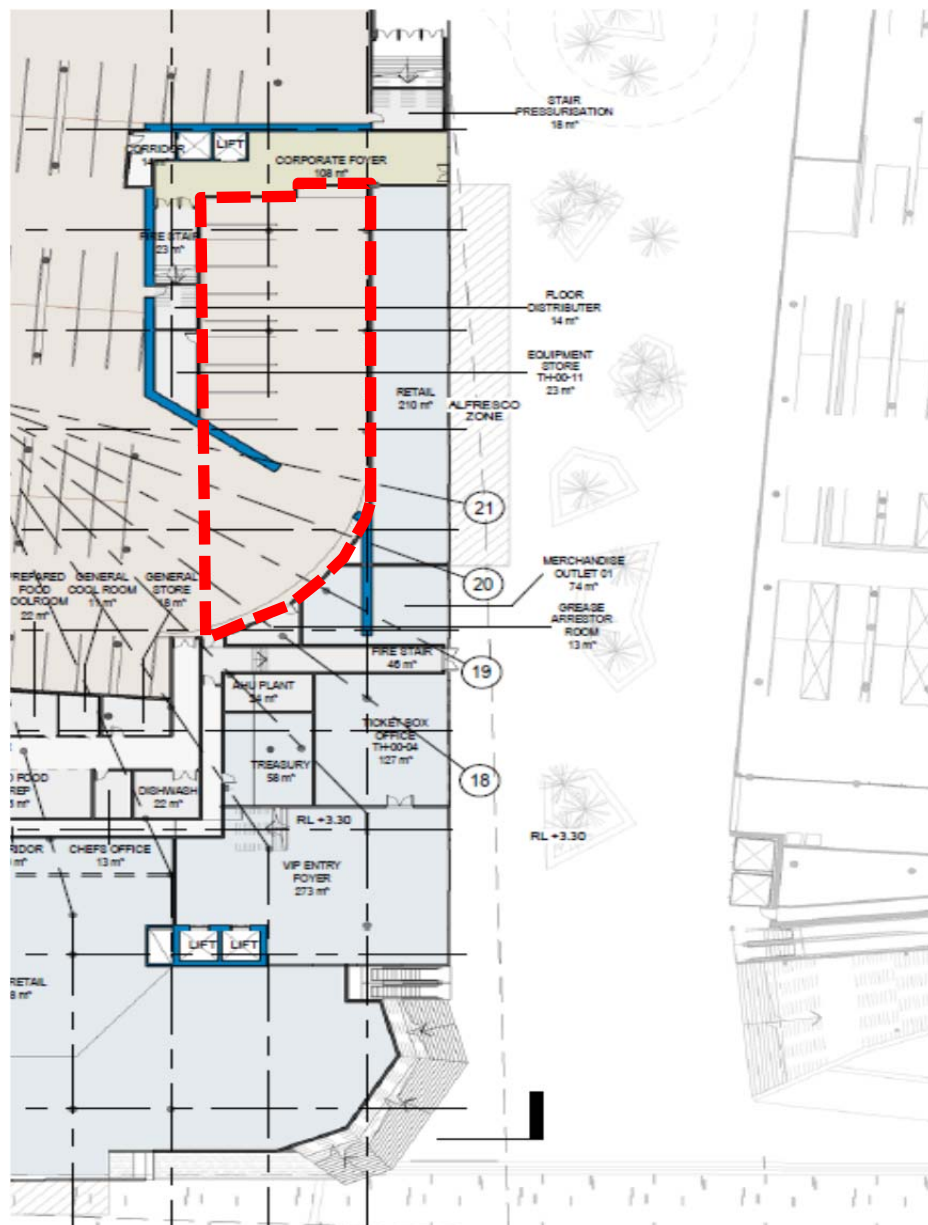


Figure 22. Car parking which should be removed to increase depth of retail tenancies
Source: Proponent SSD Application

The north-western corner of The Theatre has a fire stair which discharges into Tumbalong Place well before Tumbalong Place reaches Darling Drive (Figure 23). The stair results in a significant loss of active frontage along Tumbalong Place. It is recommended that the fire corridor be extended to allow greater retail frontage.

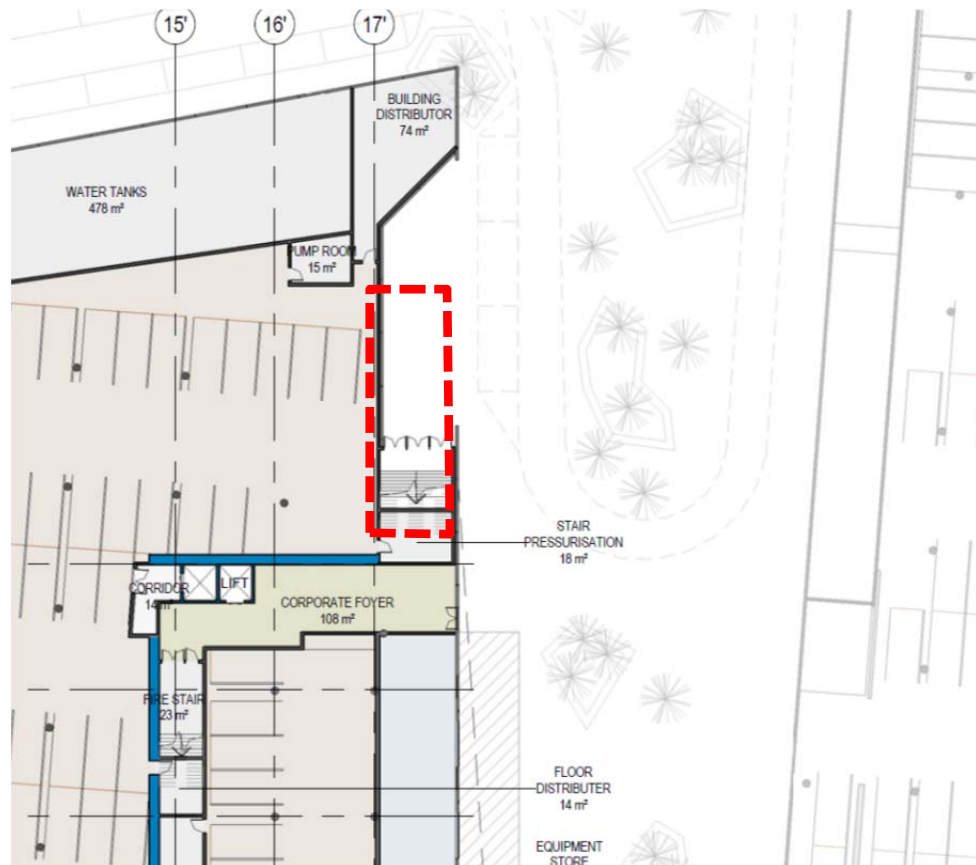


Figure 23. Fire stairs should be extended/relocated to provide more active frontage
Source: Proponent SSD Application

Recommendation 5:

More active frontage can be provided along the northern side of The Theatre towards Tumbalong Place with minor revisions to fire stair configuration.

4.5 Exhibition Centre Loading Dock

The extent of the overhang of the upper Exhibition Centre loading dock over Darling Drive as a continuous 150m long concrete deck from the Western Distributor underpass is not supported without a truly sculptural outcome. The structure will make this section of Darling Drive even more service like, have the feel of an unsafe and unsurveyed environment and may necessitate substantial structural columns within the street. In briefings with Infrastructure NSW, the Proponent (and in recent discussions with the Light Rail team from Transport for NSW), Darling Drive is being viewed as an extension of The Goods Line/Ultimo Pedestrian Network at least as far

as the Convention Centre light rail station. The pedestrian amenity of the extended Goods Line past the Exhibition Centre loading dock could be significantly compromised by the loading dock.

The loading dock should be rationalised as much as possible and moved east as much as possible. Should significant built-cover over Darling Drive remain, the underside of a more sculptural loading dock could also have a decorative feature/art treatment and an interesting lighting strategy. Any columns also need to be shaped or tapered to produce sculptural effect.

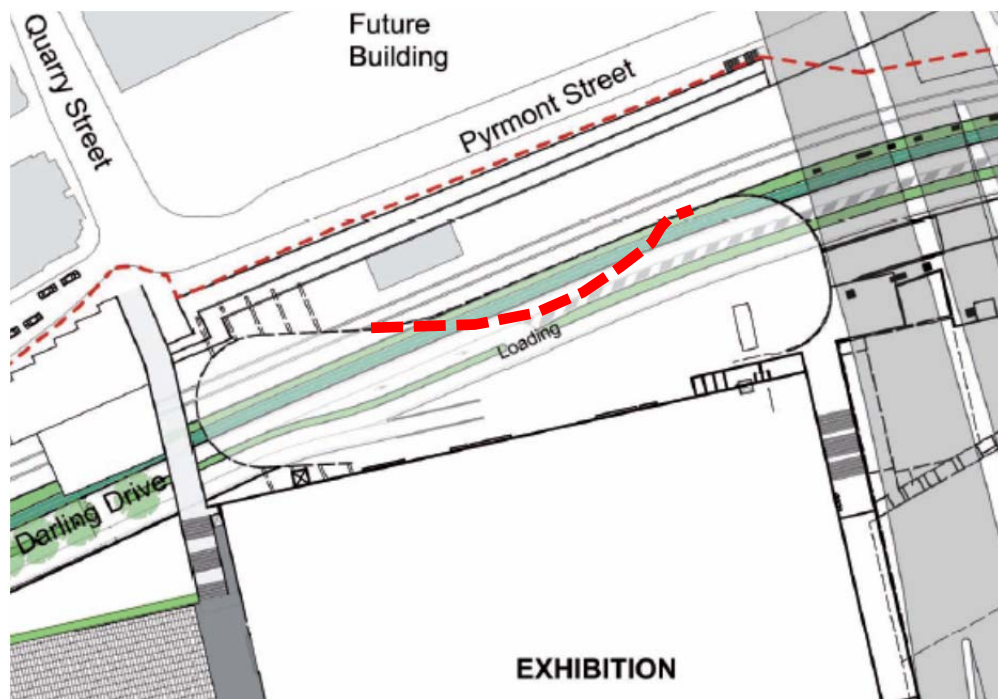


Figure 24. Recommended scaling back of Exhibition Centre loading dock
Source: Proponent SSD Application

Recommendation 6:

The extent of the Exhibition Centre loading dock above Darling Drive should be reduced as much as possible and the underside should be decoratively treated.

4.6 Event Deck

The Event Deck could be a source of future conflict. It is likely to be a bookable outdoor exhibition or break out space with limited fulltime public value because of its inaccessibility. This is due to its elevation above the public domain. When the space is being commercially used, the concept of direct and legible access to and from Quarry Street would be practically lost due to security, temporary marquees, tents, canopies and other restricted access. This necessitates a permanent alternative high-quality ground level route to Quarry Street crossing Darling Drive and the light rail line.

The Event Deck should remain an open air deck in perpetuity rather than (immediately or in time) being covered by permanent marquees, awnings or structures and should have a management program that provides for scheduled community use including active recreation. The impact on nearby residents at night will need to be sensitively managed down to avoid undue disturbance.

The City looks forward to the Proponent's design development phase for the Event Deck in order to review the nature of any ephemeral structures used on the space. An Operational Management Plan/Plan of Management is also foreshadowed within the DA and this will demonstrate whether the Event Deck can have a range of alternative uses when not in 'event mode' or being used for breakout.

The design intention to have a green landscaped edge to the Event Deck may not be readily legible and may end up with extensive balustrades. The green edge will be difficult to maintain and may add little value to the proposal unless a clear commitment is made for its management and survival.

Recommendation 7:

The Event Deck should remain an open deck in perpetuity with scheduled community use including active recreation such as basketball. Neighbour disturbance must be avoided. The green edge should be reconsidered including the necessary commitments to maintain and manage.

4.7 Pier Street Underpass

The existing appearance of the Pier Street underpass will be substandard compared to the new entertainment, exhibition and conference facilities and the southern precinct buildings. The pedestrian experience of the Pier Street walkway needs to be enhanced at ground level and the connections to/from the overpass need to transition from unfinished concrete to visually interesting finishes which blend with the underside of the bridge. The underpass requires a radical transformation befitting of the role of The Boulevard as the critical north-south corridor of the development. It also requires an upgrade around Darling Drive to enhance motorist and cyclist experiences. The City would recommend a design competition for domestic and international artists or architects, with the scope to include completely skinning the underside of the bridge.

Recommendation 8:

The Pier Street underpass must be transformed. A design competition should be held in relation to comprehensive refurbishment of the Pier Street pedestrian and vehicle underpass.

4.8 Connections to and from and within the development

Pedestrian desire lines to and from the CBD, Chinatown/Haymarket and Pyrmont need to be examined from several viewpoints, such as the mindset of a businessperson attending a conference, a concert goer, an international tourist, or local, each making different decisions on how they get to their destination.

A number of recommendations are made below and in the remainder of this submission regarding the public domain which are designed to assist in pedestrian and cyclist movements between the PPP facilities, the CBD and Pyrmont.

For example, the Bathurst Street link to Town Hall Station is a critical connection that generally needs increased attention. A commitment to public domain integration outside the development boundaries is essential to the success of the Project. There needs to be clear resolution of who is responsible and what processes will be put in place to fund and deliver essential infrastructure to support the development integration.

Quarry Street Link

As previously noted, the proposed pedestrian connection from Quarry Street to the CBD traverses significant changes in level across the Event Deck roof and access is not guaranteed. The proposed Quarry Street access represents an indirect route linking via a series of level changes from ground floor (RL 3), up 18m across the Event Deck (to RL 21) and back down 9m to Pyrmont (RL12). It does not represent a clear and highly legible route and the presence of exhibitions on the Event Deck may preclude access altogether or confine access to an even more circuitous route.

The concept of an east-west connection from Liverpool Street to Quarry Street is supported. However, it is recommended that the proposed design of the connection be improved. At the very least:

- pedestrians should be able to walk directly to/from the Event Deck to The Boulevard without having to deviate excessively
- the Quarry Street side of the link should be redesigned, and in part if necessary be an accessible ramped bridge rather than stairs.

In light of the constraints, it appears more beneficial to strengthen the Tumbalong Place/Pyrmont Street/Quarry Street on-grade link for pedestrians and cyclists by providing coherent pathways and ramps. Compared to the proposed development, this route should be widened and should include a direct (i.e. straight) ramp from the eastern end of Quarry Street to the light rail crossing.

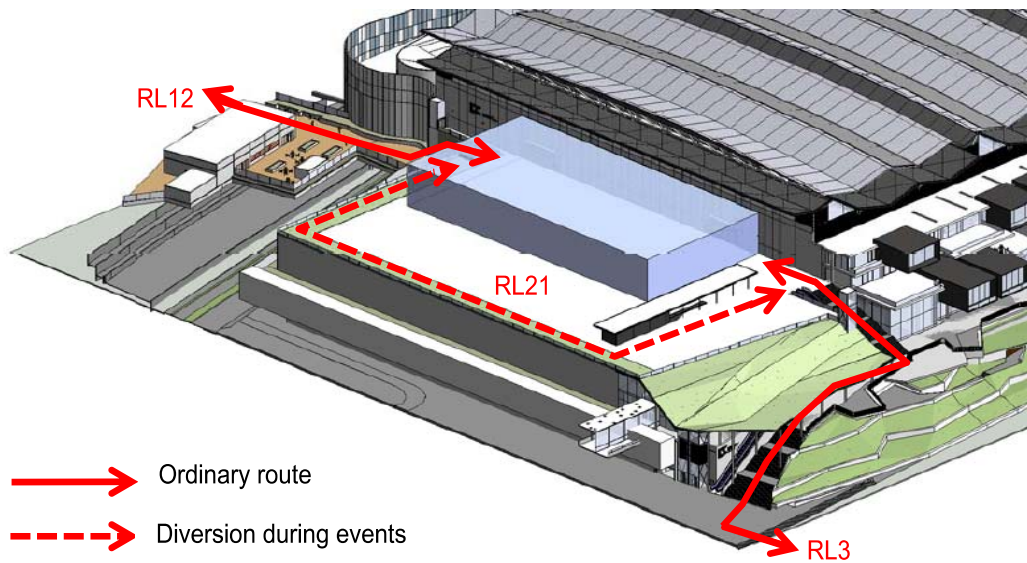


Figure 25. Proposed Quarry Street link over Event Deck
Source: Proponent SSD Application

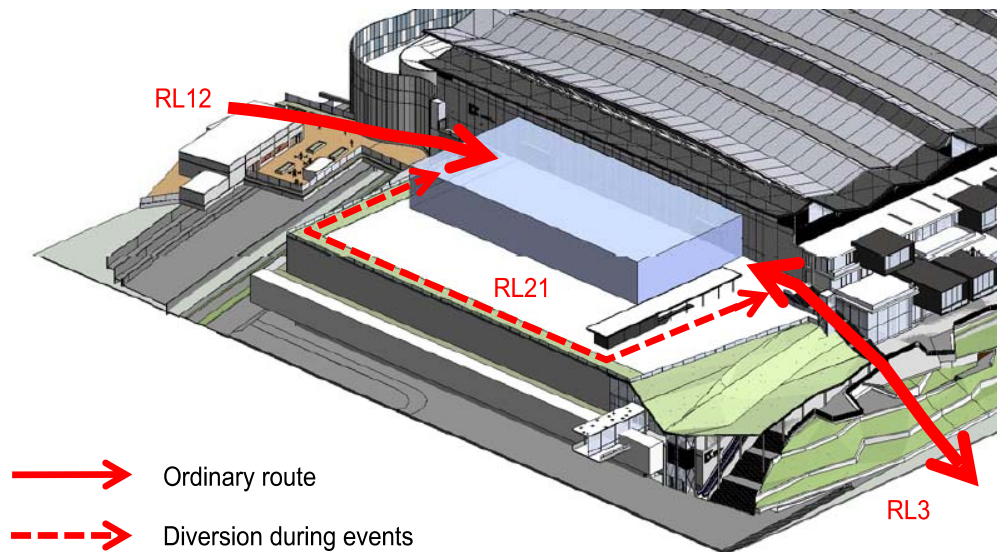


Figure 26. City's preference for direct route to/from the Event Deck
Source: Proponent SSD Application

Recommendation 9:

The secondary connection from Quarry Street should be simplified by a ramped bridge to The Event Deck and a direct staircase (and lift) to The Boulevard.

Tumalong Green Footpath Capacity

The size and scale of the pedestrian paths across and around Tumalong Green must cater for the projected volume of people to and from the City.

A balance must also be struck between Tumbalong Green as a connection and Tumbalong Green as a local park for open space and recreation let alone occasional event. The following image displays a series of recommendations for improved access to, from and across Tumbalong Green. Pathways must be sensitively done.

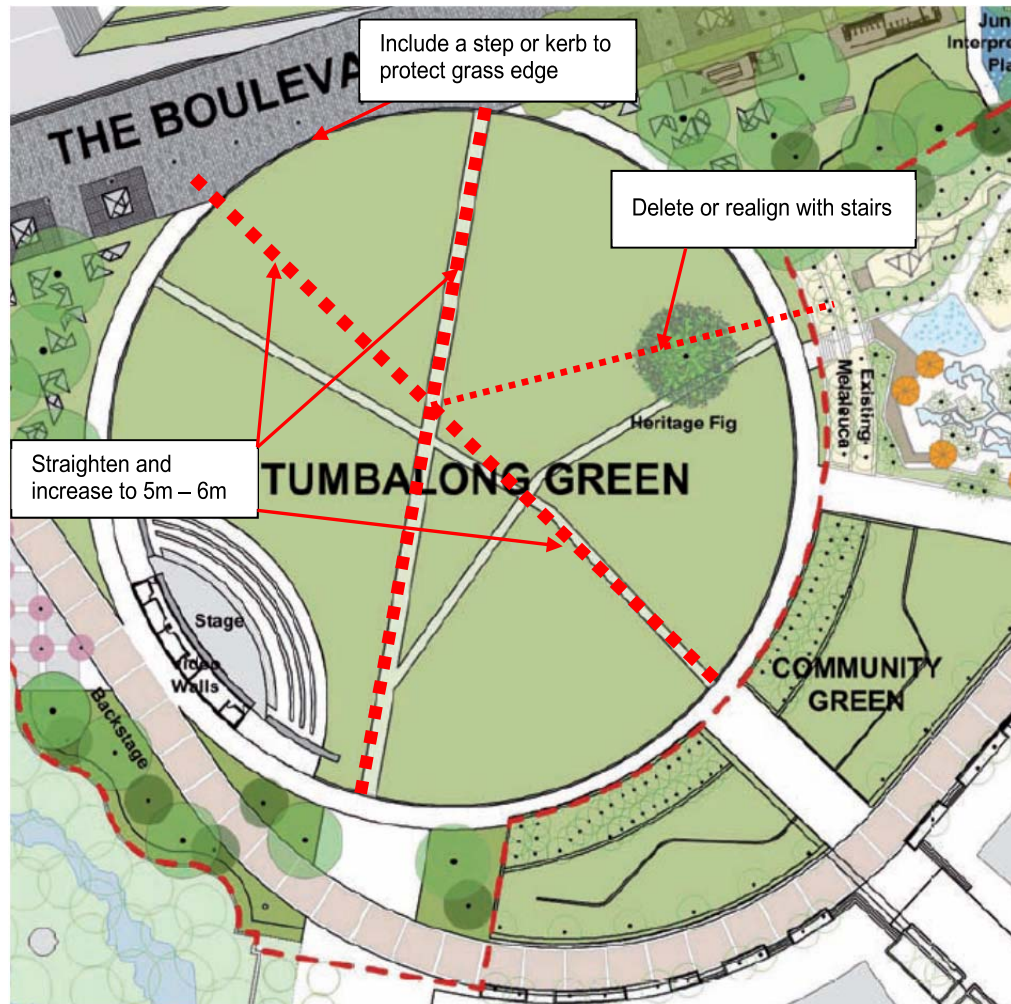


Figure 27. Tumbalong Green recommendations
Source: Proponent SSD Application

Recommendation 10:

The size and scale of the pedestrian paths across Tumbalong Green must cater for the projected volume of people to and from Town Hall Station. Footpaths should be made wider and must line up with surrounding connections.

Tumbalong Green Surrounds

The current design of the proposed footpath on the southern side of Tumbalong Green creates awkward geometry, landscaping and intersects the backstage area. It may also not cater for an influx of people from the Entertainment and Exhibition Centres at any one time.

It is recommended that the eastern pathway terminate and a new pathway around the southern edge be installed with its own geometry which widens at The Boulevard. Depending on pedestrian modelling, all tree planting, turf and seats may need to be removed from the junction of The Boulevard with the southern pathway. Conscious of the need to provide truck access and backstage marques for the stage, a small plaza area may be created behind the stage, but should remain separated from the pathway.

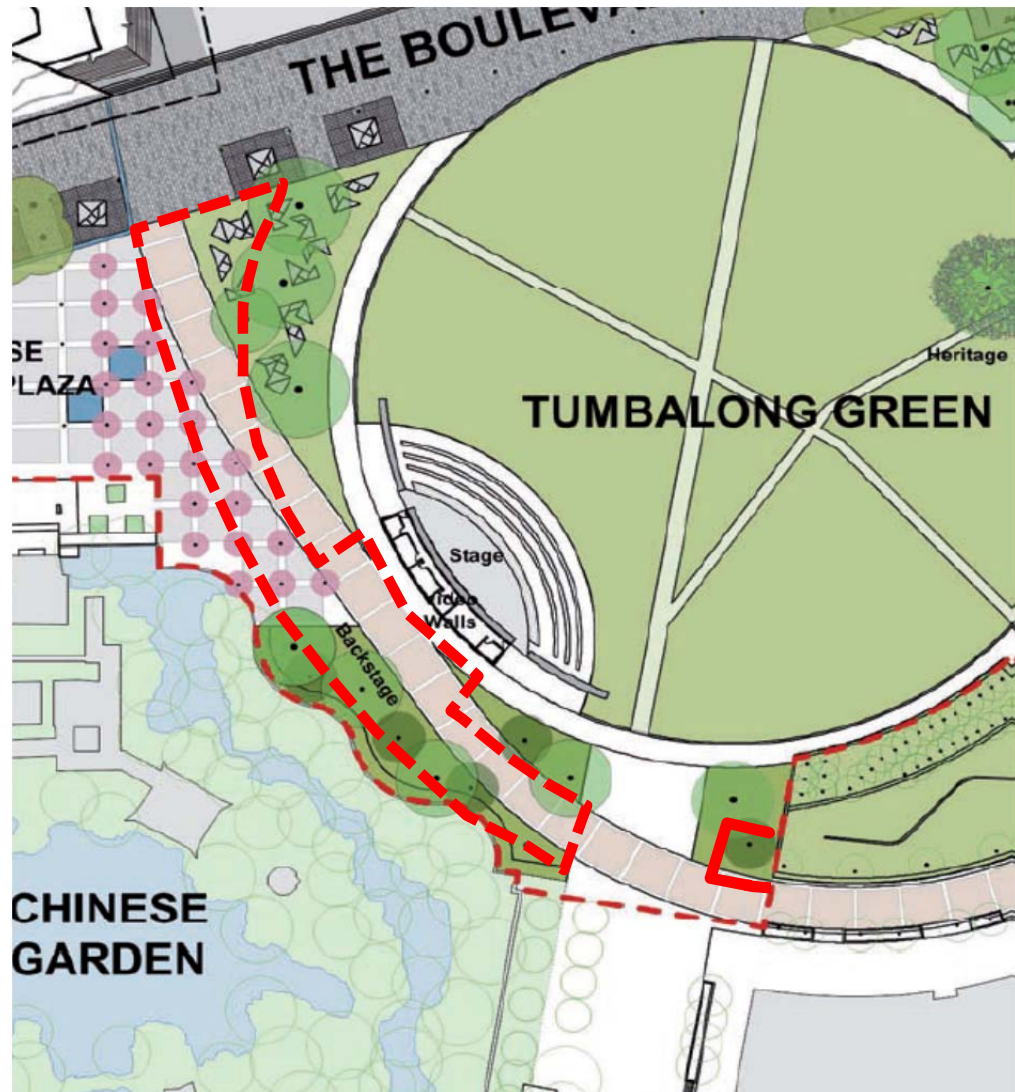


Figure 28. Tumbalong Green path capacity enhancements
Source: Proponent SSD Application

Recommendation 11:

The southern pathway around Tumbalong Green should have its own geometry and widen at The Boulevard to cope with likely pedestrian demands.

Exhibition Centre and Entertainment Centre forecourt

The junction of the Exhibition Centre and the Entertainment Centre forecourt needs to allow for an influx of thousands of people at one time. Stairs are not considered conducive to efficient and equitable access and a ramp is preferable.

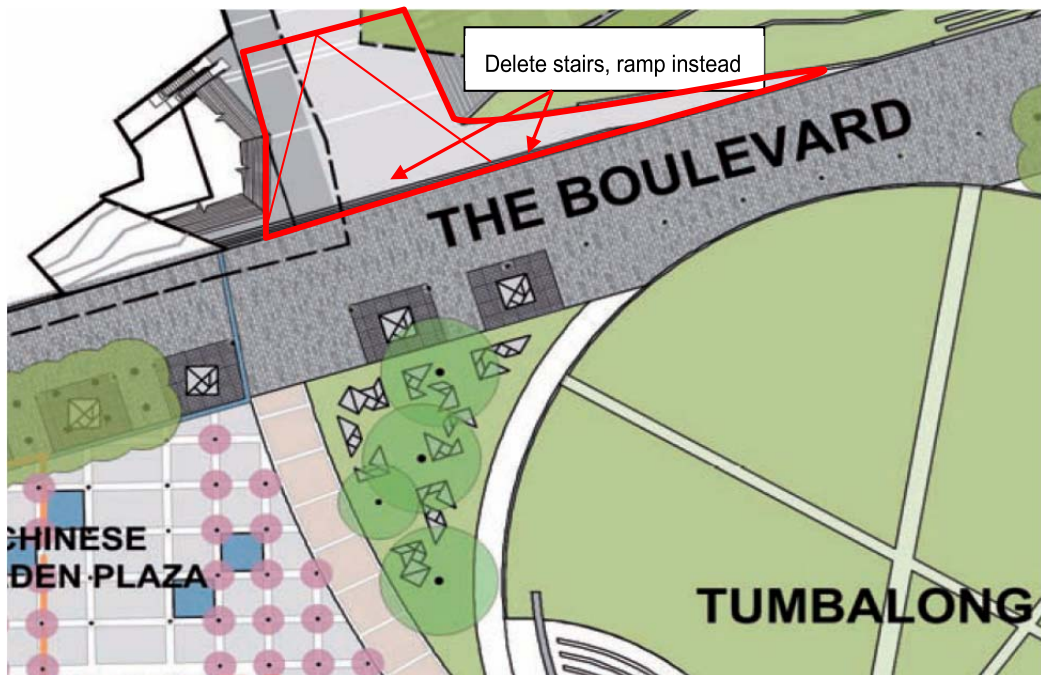


Figure 29. Exhibition / Entertainment Forecourt
Source: Proponent SSD Application

Recommendation 12:

The Exhibition Centre and Entertainment Centre forecourt should provide at-grade surfaces including ramps if necessary.

Materials and Wayfinding

Materials should echo and compliment the high quality design of new buildings. Where the proposal indicates the use of the city's palette of materials, public domain areas should be constructed to the same standards as the Sydney Streets Design Code. It has been indicated that approximately 70-80% of public domain materials are to be drawn from the City's palette and this needs to be further detailed.

Wayfinding signage should be complimentary with existing public transport and cultural facilities signage to best direct people into and around the area. Where possible, it should be integrated with City wide wayfinding signage under development.

Recommendation 13:

- (a) Materials and finishes are to be discussed with the City in further workshops.
- (b) Wayfinding signage must be integrated with transport and other city signage.

Termination of The Boulevard

It suggested that the northern end of The Boulevard needs termination where it reaches Cockle Bay. Otherwise it blends with the surrounds and is unlikely to have the desired effect of marking the entry and exit to and from the PPP facilities.

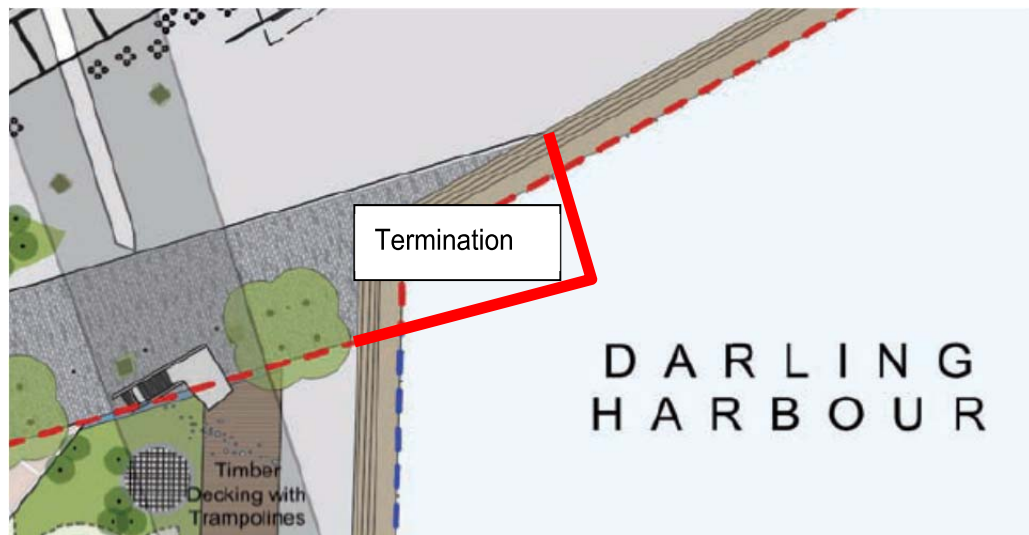


Figure 30. Recommended termination of The Boulevard
Source: Proponent SSD Application

Convention Centre Drop Off

For the limited number of times that visitors of the Convention Centre warrant being dropped off alongside the building, the demand for a turning facility should not take precedence over pedestrian primacy and desire lines. Pedestrians coming from the light rail or Novotel should be given priority over vehicles.

The design of the turning facility may also result in:

- vehicles dropping off or picking up from the port cochere of the proposed Hotels using the turn facility before getting onto Darling Drive
- vehicles turning right onto Darling Drive across the pedestrian crossing
- excessive driveways over the cycleway

Figure 31 shows recommendations adjustments to the vehicle drop off.

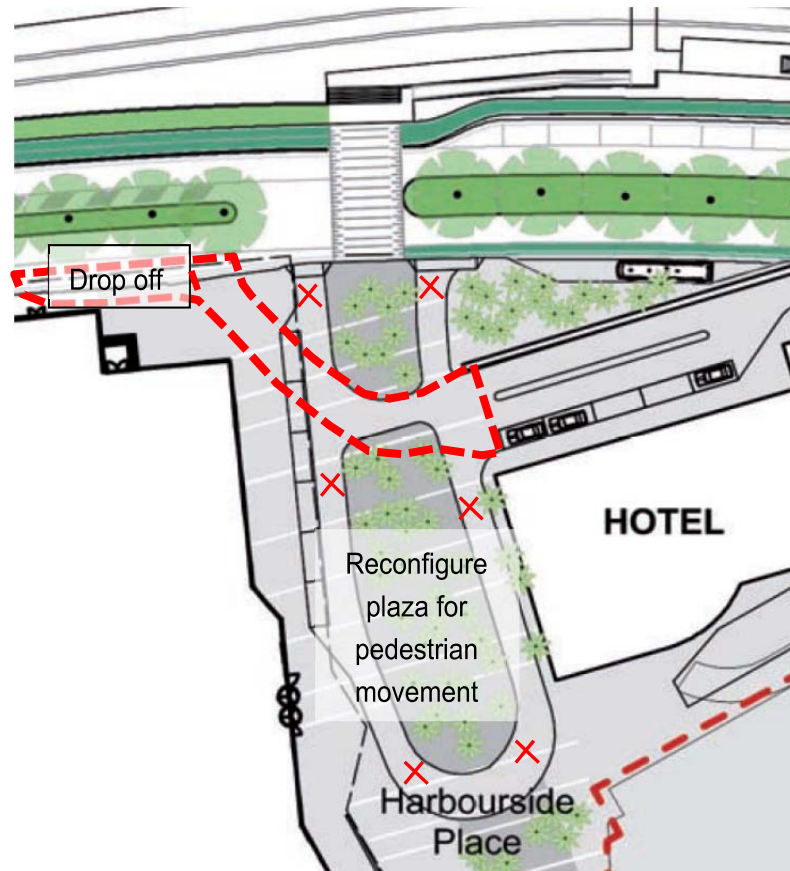


Figure 31. Recommended changes to vehicle drop off
Source: Proponent SSD Application

Recommendation 14:

The Convention Centre drop off should be reconfigured to provide pedestrian primacy.

4.9 Architecture and Built-Form of the PPP Facilities

Building Scale

The City recommended that the scale of the proposed facilities be reduced in height towards RL 32 consistent with the City of Sydney submission to INSW July 2012. The proposed scale of the Convention Centre at RL 50 is overpowering at the waterfront and is a cause for view loss concerns for a number of residents of the apartments behind.

The City believes that the Proponents should review the height of the Convention Centre and determine what changes, if any, could be made to

reduce view loss impacts. These may include reviewing:

- all floor to ceiling heights
- roof top plant
- roof form, including parapet and ceiling space

Recommendation 15:

The height of the Convention Centre should be reduced where possible.

Materials and Finishes

Very large surfaces have been treated in a monolithic manner particularly on the western facades. These surfaces need a degree of 'richness' and should be designed with a combination of large and fine detail to the surface build up. Areas of aluminium sandwich panels could increase richness by:

- introducing a super pattern
- using varied colour
- varying gloss levels
- introducing depth by overlapping or staggering

Areas of precast concrete could introduce richness through:

- introducing a super pattern
- using fine grain rich surface casting (e.g. recent Westfield project)
- introducing colour variation
- using shadow play

It is suggested that some back of house areas do not need to be screened. Depending on the circumstance, it maybe more interesting for the public to have views into loading areas either through loggias, screens or windows.

It is suggested that the roof form of the entertainment building should have a single roof expression. It should not switch to low cost materials on the west faces compared to the waterfront faces. It is important to consider that the Convention Centre will be viewed from above by occupants of a 900 room hotel as well as existing building around the site and needs to look like a unified object within Darling Harbour. The large pattern on the aluminium panels on the western face of the

exhibition building is too large and simple to be effective and should be redesigned considering the above notes.

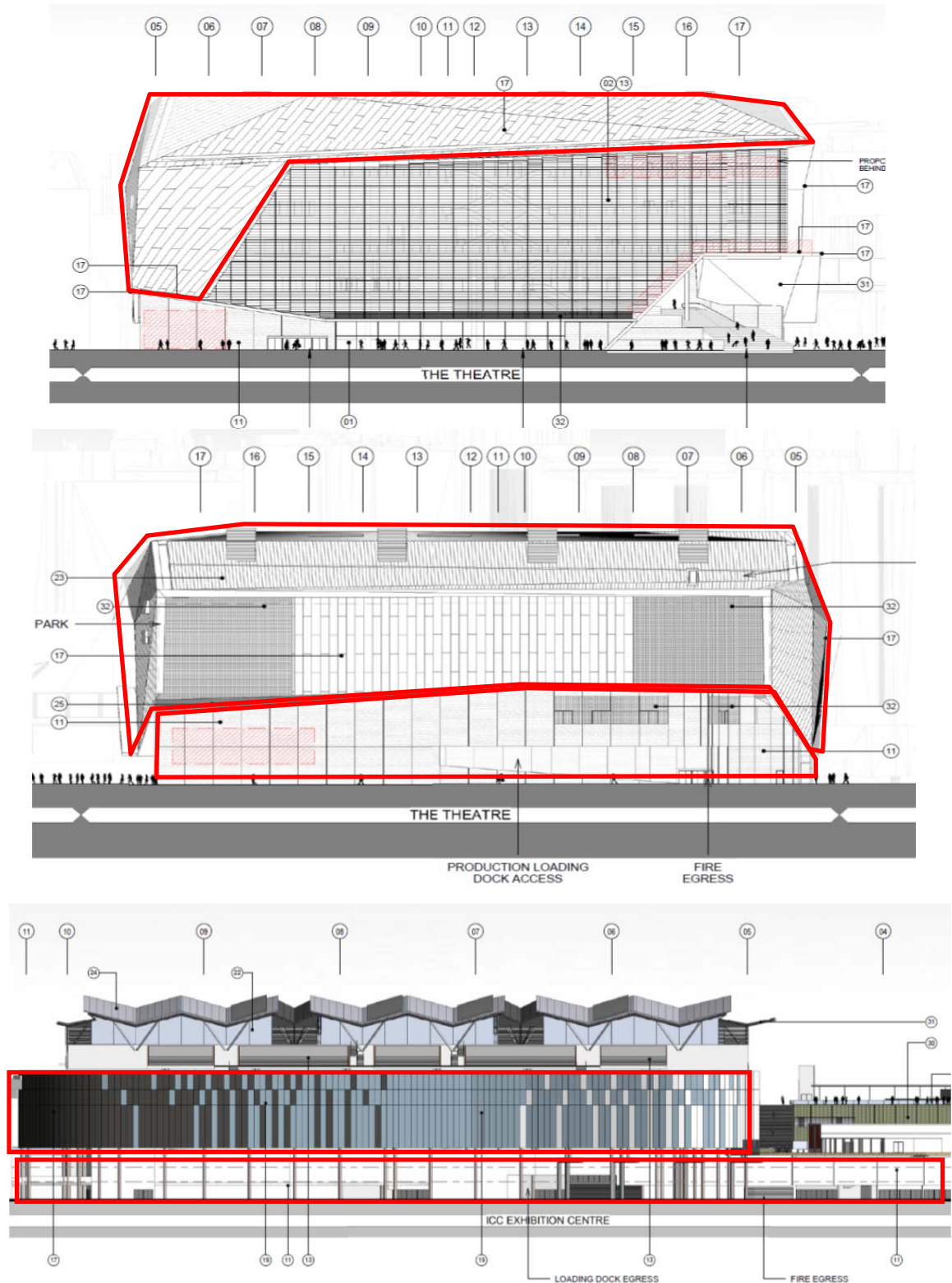


Figure 32. Area of roof and façade requiring richer features
Source: Proponent SSD Application

Recommendation 16:

Large metal and precast concrete surfaces, particularly on the western side of the facilities, require richer and more diverse materials and finishes.

4.10 Traffic and Transport Issues

Car Parking Provision

Although it is acknowledged that the total amount of parking is declining on site (from what currently exists), the amount of car parking provided may still be excessive for the road capacity. The TMAP provides little discussion regarding alternative transport modes which could be promoted through appropriate design and encouragement strategies. Public transport, walking and cycling connections are excellent and the location of the development relative to complimentary land uses and the CBD lend themselves to more aggressive modal shift targets. The demand scenario approach to calculating the parking requirement for the site is generally accepted as justification for the parking rate. However, an additional reduction based on achieving a lower car use for the site would be the preferred approach of the City. This would go further to addressing the sustainable transport requirements of the Director-General's Requirements.

Electronic car parking detection systems should be considered to allow for a better efficiency of the car parking across the site.

Recommendation 17:

- (a) The TMAP should include further discussion regarding the available alternative transport modes and enable an understanding of the baseline, reasonable and stretch targets for sustainable transport.
- (b) Electronic parking systems should be used for car parking efficiency.

Darling Drive Cycleway

There appears to be discrepancy between the documents about the proposed cycling infrastructure along Darling Drive with references to separated cycleways and shared paths used in the documentation.

The Darling Drive route must be well designed to ensure seamless connection to the surrounding routes at each end and at intermediate connecting points and destinations. If it is well designed, it will attract a greater number of commuters.

The connections at each end of the cycleway are critical to ensure cyclists are transitioned from one facility to another rather than being dumped into mixed traffic at the end of the boundary.

Consideration needs to be given to connections between the proposed cycleway and existing/proposed facilities at either end of the development. In particular, this includes:

- Transition between the one-way pairs on Darling Drive to the north of ICC Bayside and the proposed shared path
- Transition between the proposed shared path and the subject site at the Darling Drive/Harbour Street roundabout
- Transition between the proposed shared path and existing/proposed facilities on Hay Street.

The CoS Cycleway Strategy shows the proposed cycleway on the western side of Darling Drive continuing to the northern limit of the development site. It is unclear why the proposed cycleway does not start at this location, but instead approximately 50m further south.

Recommendation 18:

- (a) That the Proponent liaises further with the City's Cycling Team to ensure all cycle connections are designed and built to match the existing and planned infrastructure.
- (b) The inconsistencies with the Darling Drive cycle way design must be addressed and the City given the chance to comment on the design.

Cycling Infrastructure

Cycling is constantly referred to throughout the traffic and transport documentation; however there is no detailed information regarding cycle parking or associated end of trip facilities at the site (lockers, showers etc.). In order to meet the targets and requirements set out by the TMAP and the DGRs, a quantity of visible and good quality cycle facilities is required. The plans submitted generally show bicycle parking areas, however these are often just areas in the left over spaces of car parks.

Bicycle parking should also be provided in the open public domain areas and should be plentiful, well located (i.e. close to destinations), designed to provide a welcoming environment and designed to reduce casual inappropriate car parking.

There must be good bicycle permeability and connectivity in both the north-to-south and east-to-west directions for commuters and maximum permeability for walking and cycling must be maintained on completion and during construction. The key issue is to ensure cycle connectivity to and through the precinct, and interface with the

existing networks. It is important to design and manage cycling so that it can coexist with pedestrians in the precinct.

The DGRs require the provision of cycleways is to be consistent with the locations in the City of Sydney Cycle Strategy and Action Plan 2007-2017. The 'key' cycle facilities in the vicinity of SICEEP listed in the report do not accurately reflect the above Strategy, because the wrong maps have been referenced. Those in the vicinity of SICEEP also include:

- Hay Street to Liverpool Street through Darling Harbour
- Mary Ann/Tomas and Jones Streets.

More detail is required as to how the proposed cycle network will interface with the existing and proposed network in the vicinity of SICEEP bearing in mind the key planning and design principles for developing the effective cycle network - consistency, convenience, safety, directness, etc.

The application is of such a scale that it should provide an impetus for implementation or upgrade of cycle facilities on the routes connecting the site to other trip attractors in the vicinity.

Recommendation 19:

- (a) Bicycle parking and end trip facilities must be provided in line with the requirements set out for a convention centre in the NSW Planning Guidelines for Walking and Cycling. The customers/visitors should be set against the capacity of the venues rather than seating capacity. This is in line with the DGRs.
- (b) The bicycle parking and end trip facilities require more detail.
- (c) The bicycle parking spaces and end trip facilities must be visible and inviting, not just put in left over areas of car parks.
- (d) Cycle parking should be accessible from both The Boulevard and Darling Drive.
- (e) The Boulevard must be designed to accommodate cyclists and pedestrians over its entire length.
- (f) All pedestrian connections through the site must be designed to be suitable for cyclists.
- (g) The cycle planning around the site must be reviewed to ensure that the DGR to provide a consistency with the City of Sydney Cycle Strategy and Action Plan 2007 – 2017 is suitably addressed, as at present it does not.

Road Design

The new Darling Drive roundabout provides poor pedestrian crossing facilities. It is recommended the design is improved to provide pedestrian refuges on all approaches to the roundabout.

Recommendation 20:

The Darling Drive roundabout should be improved to provide pedestrian refuges on all approaches to the roundabout.

Taxi Facilities

There are concerns that the number of taxis which are able to wait at the proposed taxi areas is insufficient. If sufficient space is not provided for taxis to park/hold within the development area then they may attempt to congregate, and therefore block, Darling Drive. The size of the three uses in the PPP is likely to result in large crowds occasionally exiting at the same time and requiring taxi services.

Two taxi ranks are proposed within the PPP site – one on the western side of the Theatre and one on the northern side of the ICC Bayside. It is unclear whether or not there are adequate pedestrian connections between ICC Exhibition and the taxi ranks.

Recommendation 21:

- (a) The size of taxi storage needs to be reviewed.
- (b) The design of the footways around the taxi areas needs to be reviewed to ensure there are sufficient patron storage areas, while maintaining an adequate pedestrian thoroughfare.
- (c) Connections between the Exhibition Centre and the taxi ranks are also a concern. Sufficient way finding will need to be used for this and all the sites.

Pedestrian Access along Darling Drive

Pedestrians are not catered for along Darling Drive at all, or where there is some allowance for footpaths, these footpaths terminate.

Where pedestrian access has been provided – behind the Theatre – it is for access to a taxi slip lane. However, the footpath is shown to continue south beyond the taxi lane and terminate at the loading dock driveway (with no further connection). The portion of the footpath which has no destination should be removed and converted to landscaping.

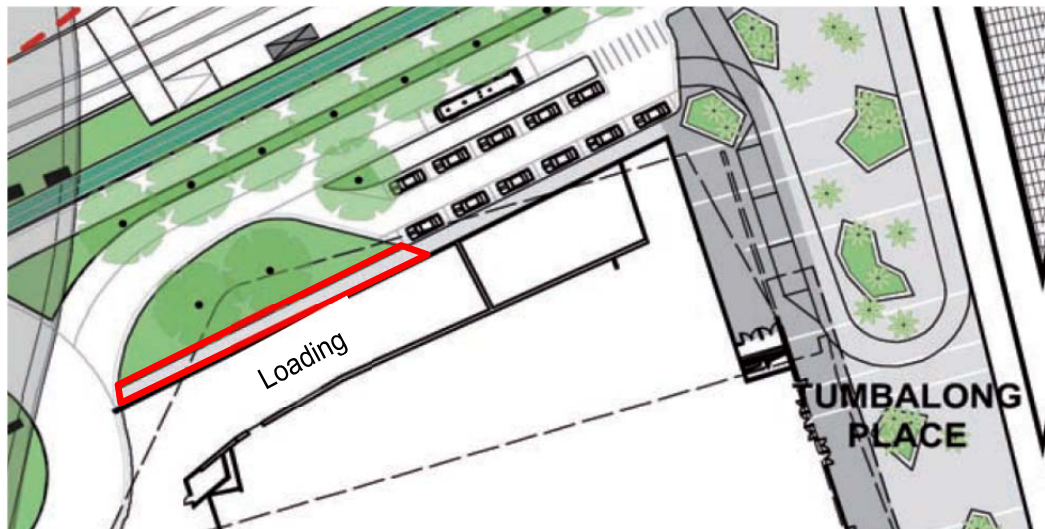


Figure 33. Footpath behind The Theatre which terminates
Source: Proponent SSD Application

Recommendation 22:

- (a) Pedestrian access should be incorporated along the eastern side of Darling Drive.
- (b) A footpath shown behind The Theatre should be deleted and converted to landscaping.



Figure 34. Darling Drive surrounds and existing entry to Convention Centre
Source: City of Sydney

Pedestrian legibility from Convention Light Rail Station

Pedestrian connection from the Convention Light Rail Station can easily be improved with minor amendment. In this regard, the proposed stairs from the station down to Darling Drive are unnecessary and the connection should be made via a full pedestrian ramp.

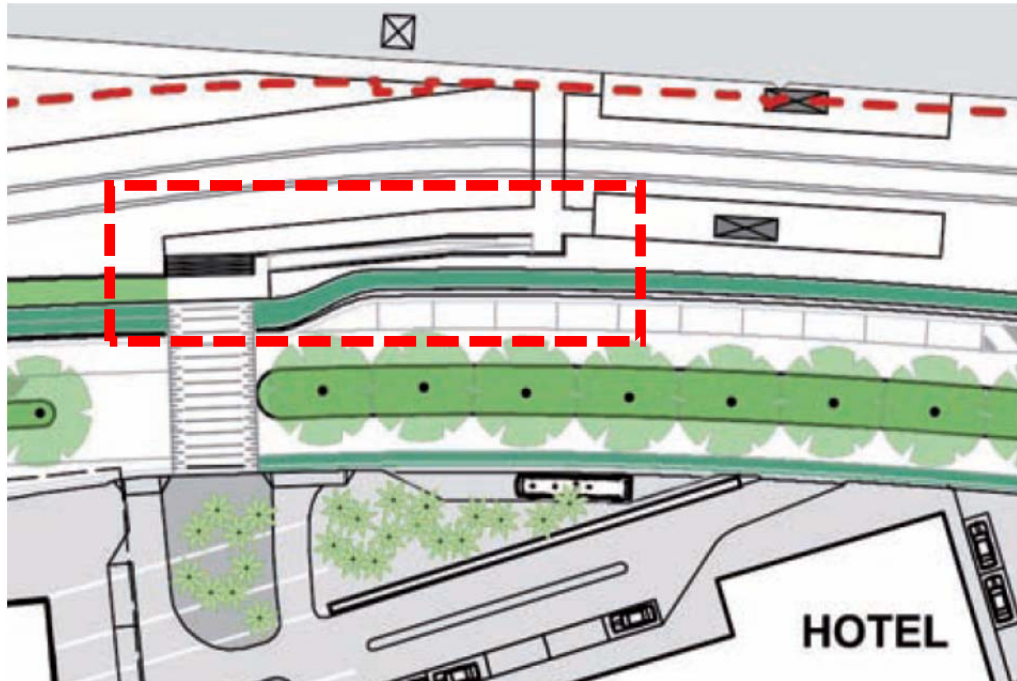


Figure 35. Pedestrian stair from Convention Light Rail
Source: City of Sydney

Recommendation 23:

Stairs from the Convention Centre Light Rail Station should be converted to a ramp.

4.11 Sustainability

The vision of *Darling Harbour Live* to integrate environmental and social infrastructure into the PPP component is fully supported, including the key initiatives proposed. The following comments are provided on the relevant environmental initiatives:

- **Solar energy** – Maximum use of roof top space and building façade to incorporate Solar PV opportunities is encouraged. Also the proposed partnership with the local community is supported.
- **Rainwater harvesting** – This initiative is supported. The proponent is also encouraged to incorporate other water harvesting initiatives including stormwater/sewer mining and incorporation of dual water reticulation system throughout the development. This is a premier development for Sydney City, and maximum self-sufficiency for the facility to meet its potable water demand is encouraged.

- **Reuse and Recycling** – The targets of 90% of construction waste to be re-used or recycled and 70% of event waste diverted from landfill are supported.
- **Gold LEED rating** – It is recommended that the development meets the US Green Building Council's Leadership in Energy and Environmental Design (LEED) rating tool's **Platinum** rating.
- **Green Star rating** – At least a 6 **star** rating under Green Building Council of Australia's Green Star Performance rating should be obtained.
- **Gold Earth Check rating** – *Darling Harbour Live* should aspire for a **Platinum** rating. Projects recently have obtained platinum rating in the travel and tourism industry – e.g. Queenstown Top 10 Holiday Park Creeksyde, Amsterdam manor.

The PPP offers opportunities for integration of water, energy and waste infrastructure and services with other precincts, community and other proposed developments (particularly Barangaroo). For example, opportunities should be sought to integrate water, energy and waste infrastructure services with the Barangaroo development, and having the same proponent develop both these sites offers significant opportunities for innovation.

The City of Sydney strongly encourages the proponent to consider and meet the objectives and targets set out in *Sustainable Sydney 2030*.

Recommendation 24:

Achieve highest sustainability rating for proposed initiatives:

- Maximum utilisation of roof space and building façade for solar energy, aiming for 30% of energy use from local renewables generation.
- Meet at least 10% of potable water demand by stormwater harvesting and grey water recycling, including provision of dual reticulation for all buildings.
- Platinum LEED rating, 6 star Green Star rating (As Design and As Built) and Platinum Gold Earth Check rating.

Energy

A major objective of *Sustainable Sydney 2030* is to position Sydney as one of the world's leading green cities to counter climate change. To achieve this, the City has committed to reducing greenhouse gas emissions by 70% by 2030 from 2006 levels. In addition, the City has also set a target of 30% renewable electricity by 2030.

In terms of the proposed thermal plant, the opportunity identified to expand the thermal plant into the PDA components of the precinct and potentially beyond the precinct to interface with precinct scale tri-generation is strongly supported. The proponent is encouraged to work closely with the City to explore this opportunity further.

Recommendation 25:

Due consideration should be given to meeting the Sustainable Sydney 2030 greenhouse emissions and renewable energy targets by:

- Integrating thermal needs of the development with nearby developments – e.g. Barangaroo.
- optimum generation of energy using tri-generation .
- committing to energy efficient plant and equipment, and low energy lighting such as LEDs in the buildings, public domain and communal areas.
- co-locating thermal pipes, recycled water networks and possibly automated waste collection systems.

Water

City of Sydney's Decentralised Water Master Plan sets water objectives to deliver 10% water efficiency of existing buildings, 30% of water supply by local water capture and reduce stormwater pollutants by 50%.

The City of Sydney fully support the initiative proposed of bringing recycled water produced at the nearby Barangaroo South Treatment Plant to these facilities and PDA buildings. Also the proposed water sensitive urban design approach is fully supported.

The proponent is encouraged to actively meet the *Sustainable Sydney 2030* targets by:

- achieving a 5 Star NABERS Water or equivalent
- demonstrating how recycled water is maximised on site with the aim of achieving at least 30% of water use on site from non-mains sources
- reducing 50% of sediments, suspended solids and nutrient loads discharged into Cockle Bay from run-off generated within the proposed site.

Recommendation 26:

The proponent is strongly encouraged to reduce water demand, increase water efficiency and improve stormwater quality by:

- optimum holistic consideration of the water cycle including stormwater generation, potable and non-potable demands
- Achieve 5 Star NABERS Water or equivalent,
- demonstrating how recycled water is maximised on site with the aim of achieving at least 30% of water use on site from non-mains sources.
- implementing WSUD to treat stormwater runoff, provision of rain water capture, rain gardens and other measures.

Materials and Waste

The City fully supports the initiatives proposed for reuse of demolition materials and the proposed recycling commitments, including:

- Reuse of existing structure for the new facilities and recycling of demolition materials
- 90% of construction waste diverted from landfill
- Operational waste management commitments to recycle 75% of waste and 90% of organic waste diverted from landfill

The City encourages the centralising of waste and recycling storage for the development as it should provide benefits from reduction in vehicle movements and storage space requirements, while also being compatible with the strategy of advanced collection and treatment of waste.

Recommendation 27:

The Proponent is encouraged to consider the following:

- Full reuse and recycling of demolition materials first, before utilising it to replace fill material requirements;
- Maximise centralised waste and recycling storage for the development to provide benefits from reduction in vehicle movements for waste collection
- Ensuring waste storage areas are future-proofed to be compatible with advanced waste collection and treatment

ends



Appendix A – Project Chronology

Chronology of Events – Darling Harbour Live

February 2007	Report by HVS International: Revitalising the Convention and Exhibition Industry in Sydney.
February 2007	Report by URS: Economic Impact of Expanded Convention and Exhibition Facilities in Sydney.
February 2008	Final Report by John O'Neill: Review into Sydney Convention and Exhibition Space.
November 2006	City of Sydney Sustainable Sydney 2030 released and included a vision for exhibition and convention space integrated with Central Station.
February 2011	Replacement of existing facilities at Darling Harbour declared to be a 'Major Project' under the former Part 3A of the Environmental Planning and Assessment Act.
September 2011	Expressions of interest from developers and facilities managers were invited for the project.
October 2011	Site declared to be a State Significant Site within State Environmental Planning Policy (State and Regional Development) 2011.
December 2011	Proponents shortlisted and NSW Department of Planning and Infrastructure issued Director-General's Requirements for the project (as a whole) to Infrastructure NSW.
April 2012	NSW Premier announced that the size of the project was increased by two-thirds, newly including Tumbalong Park and the Monorail corridor.
August 2012	Two proponents lodged bids for the project.
September 2012	NSW Government announced the location for an interim exhibition facility at Glebe Island during construction of the proposed facilities.

December 2012	<p>NSW Premier announced the selection of the preferred proponent (a consortia containing Lend Lease, Capella, AEG Ogden and Spotless) and revealed the Preferred Master Plan.</p> <p>City of Sydney provided input to the amended Director-General's Requirements for the project.</p>
January 2013	NSW Department of Planning and Infrastructure provided the preferred proponent with revised Director-General's Requirements for the planning applications.
February 2013	Preferred proponent and Infrastructure NSW consulted the City of Sydney, Powerhouse Museum, transport authorities, environmental authorities, service authorities and nearby residents.
March 2013	First DAs lodged.



Appendix B – Selected References

- **Premier's Media Release**
- **Preferred Master Plan, December 2012**
- **Destination Sydney Media Release**



Barry O'Farrell MP

Premier of NSW

Minister for Western Sydney

MEDIA RELEASE

Tuesday 11 December 2012

PLAN REVEALED TO TRANSFORM SYDNEY'S CONVENTION, EXHIBITION AND ENTERTAINMENT PRECINCT

Australia's largest convention and exhibition facilities, Sydney's largest red carpet entertainment venue, a hotel complex of up to 900 rooms plus a new urban neighbourhood in Haymarket will be part of one of the most exciting urban renewal projects the city has ever seen, the NSW Government announced today.

Premier Barry O'Farrell said the transformation of a 20-hectare precinct at Darling Harbour, stretching from Cockle Bay to Haymarket and Ultimo, would generate about \$5 billion in economic benefit for NSW.

Mr O'Farrell said Destination Sydney had been selected as the preferred bidder to partner with the NSW Government for the development, which has the working title "Darling Harbour Live". These facilities and the upgrade of the public domain at Darling Harbour will cost about \$1 billion.

Destination Sydney is a consortium comprising AEG Ogden, Lend Lease, Capella Capital and Spotless.

"Along with the development of Barangaroo, this project will transform the western fringe of Sydney's Central Business District and is the biggest and most exciting change to Darling Harbour in 25 years," Mr O'Farrell said.

"This plan fulfils a key election commitment and will create jobs for 1,600 people during the three year construction, which starts at the end of 2013, and provide ongoing employment for 4,000 people across the precinct."

Opening in late 2016, the new world-class exhibition, convention and entertainment facilities are aimed at ensuring Sydney remains *the* choice in Australia and Asia Pacific for the competitive – and lucrative – business events industry.

Deputy Premier and Minister for Trade and Investment Andrew Stoner said the convention and exhibition facilities will generate \$200 million per year in economic benefit for NSW, or \$5 billion, over the period of the 25 year concession.

"International business delegates spend up to six times more than other tourists, and this development will ensure Sydney has world-class facilities in a prime CBD location so we continue to compete on the global stage," Mr Stoner said.

"Underscoring the need for new facilities, the Sydney Harbour Foreshore Authority (SHFA) advises that over the past five years the existing facilities have been unable to

accommodate about 169 conventions and 12 exhibitions. These events could have created \$150 million in economic benefit for NSW.”

The new convention, exhibition and entertainment facilities will be built north of Pier St, while the new urban neighbourhood will be created south of Pier St on the site of the existing Sydney Entertainment Centre and car park.

“The redevelopment goes far beyond improving facilities – it’s also about re-shaping the city. Darling Harbour already attracts 25 million people a year and this development will create a more vibrant place on Sydney Harbour,” Mr O’Farrell said.

“This development creates a cosmopolitan neighbourhood in the heart of the city at The Haymarket that is humming with life seven days a week. The Haymarket will also be known as a thriving technology precinct with IT businesses and an ‘IQ Hub’ that is aligned with the University of Technology Sydney and focused on helping start-ups.”

The existing Sydney Convention & Exhibition Centre will close December 2013. The new facilities and the improved public spaces at Darling Harbour will be opened in December 2016. Construction will commence on The Haymarket in 2014, allowing the existing Sydney Entertainment Centre to remain open until December 2015.

Mr O’Farrell said the existing Sydney Convention Exhibition Centre, Sydney Entertainment Centre and its staff had been instrumental in ensuring Sydney’s success in the major events industry, and was now well-placed for further growth.

During the three year construction period, Darling Harbour will remain open for business. SHFA will work with Destination Sydney and retailers at Darling Harbour, Cockle Bay and King St Wharf to keep the precinct active.

Infrastructure NSW will now enter into detailed negotiations to finalise the contract in the first half of 2013. The contract is for Destination Sydney to build the convention, exhibition and entertainment facilities and then maintain and operate for 25 years as well as the development rights for The Haymarket and hotel complex.

Destination Sydney will work with Infrastructure NSW to enter into Project Development Agreements to develop the hotel complex and The Haymarket site, which is adjacent to the convention and exhibition site, with an anticipated end development value of about \$1.5 billion.

****A fact sheet of the preferred plan is attached****

**MEDIA: Brad Burden or Cameron Hamilton (Premier) 9228 5239
Sandy Olsen (Infrastructure NSW) 0411 495 812**

HIGHLIGHTS OF THE PREFERRED PLAN:

- The largest exhibition space in Australia at 40,000 sqm - the equivalent of five football fields. It includes 35,000 sqm of dedicated space and a further 5,000 sqm of flexible space.
- The biggest total meeting room space in Australia at 8,000 sqm across 40 rooms, linked to both convention and exhibition areas.
- The biggest Australian convention capacity - known as plenary space - able to accommodate more than 12,000 people over four different areas, allowing multiple events to take place at the same time.
- Sydney's largest ballroom, for at least 2,000 people - almost double the current capacity.
- A red carpet, premium entertainment facility with a capacity of at least 8,000 people, suitable for both entertainment events and 'mega' conferences.
- State-of-the-art technology throughout, such as free wireless connectivity across all facilities and 10 free Wi-Fi hot spots in the public open space.
- Up to 900 hotel rooms in a hotel complex at the northern end of the precinct.
- The new convention, exhibition and entertainment facilities will be built north of Pier St.
- Renewed and upgraded public domain that has been increased by a hectare, including an outdoor event space for up to 25,000 people at an expanded Tumbalong Park.
- A new neighbourhood at the southern end of the precinct, called The Haymarket, home to high-tech businesses, apartments, student accommodation, shops, cafes and restaurants.
- The new urban neighbourhood will be created south of Pier St on the site of the existing Sydney Entertainment Centre and car park.
- Improved pedestrian connections drawing people between Central, Chinatown and Cockle Bay Wharf as well as east-west between Ultimo/Pymont and the city.

Preferred Master Plan



Media Release

Destination Sydney set to revitalise Sydney's exhibition and entertainment hub

11 December 2012

Destination Sydney, a consortium comprising AEG Ogden, Lend Lease, Capella Capital and Spotless, today announced that it has been selected as the preferred proponent to deliver Sydney's new convention, exhibition and entertainment precinct (SICEEP).

Chief Executive of Destination Sydney, Malcolm Macintyre, said the consortium was thrilled to be chosen to deliver the landmark redevelopment which will not only revitalise the much loved area of Darling Harbour but also generate significant long term economic benefits for NSW through a sustained increase in national and international visitation.

"Destination Sydney is committed to ensuring that Darling Harbour becomes a first choice destination for conventions and exhibitions, entertainment events and tourism, and of course a place that all of Sydney will feel is theirs," Mr Macintyre said.

"This project will redefine Sydney as a global city and create one of the world's great meeting and entertainment destinations. Not only will it become a beacon for international visitors for conventions and events but will also build on the appeal of the Darling Harbour area for Sydneysiders creating an entertainment hub that promises to reconnect and re-energise the city."

"We congratulate the Premier and NSW Government on its vision for this area. We too share in this vision to finally create real linkages between the CBD, Haymarket, Chinatown, the education precincts and the media and creative hub that is Pyrmont/Ultimo. We also applaud Infrastructure NSW for progressing this vital infrastructure that will deliver profound economic dividends for the people of NSW."

Under the consortium arrangement Lend Lease's infrastructure development business Capella Capital will act as PPP development manager and financial advisor. The PPP capital commitments are fully underwritten, and Lend Lease will invest 50 per cent of the equity alongside the national Industry super fund for tourism and hospitality, HOSTPLUS, with debt being provided by a consortium of local and overseas banks.

Lend Lease's project management and construction business will design and construct the project while AEG Ogden will be the operator of the circa A\$1 billion facility to be known as the International Convention Centre Sydney (ICC Sydney). Spotless will be responsible for delivering the Facility Management services for the PPP.

Lend Lease Chief Executive Officer of Construction and Infrastructure, Mark Menhinnitt said, "Together with the NSW Government we are building a smarter, more sustainable Sydney. At the completion of this project, Haymarket will become Sydney's innovative district, anchored in knowledge, innovation, media and technology. Buildings will be smarter and reliably connected to a wide range of technology services," he said.

In addition, Lend Lease will work directly with Infrastructure NSW to develop a mixed use site of five hectares adjacent to the SICEEP site with an end development value in excess of A\$1.5 billion. The mixed use site includes a landmark hotel located immediately adjacent the International Convention Centre Sydney, residential apartments, student accommodation, retail and commercial space, public car parking, and community space and public realm upgrades.

HSTPLUS Chief Executive Officer, David Elia, said this development was fitting for HSTPLUS as it gives the Fund the ability to invest back into the Hospitality, Tourism and Recreation sectors and provide appropriate returns for its members.

"This project significantly strengthens the position of HOSTPLUS as a major and active participant in the Australian tourism, hospitality and recreation sector. This development is a great example of our Fund's ability to leverage the consortium partners' financial and sector expertise to create the capacity for greater returns to HOSTPLUS members and lead the way in developing tourism and hospitality infrastructure that fosters greater benefits within the Australian economy more broadly."

Welcoming today's announcement, AEG Ogden Chairman and CEO Harvey Lister commended the NSW Government's vision to create a world-class, integrated facility that will ensure Sydney is a leading player in the business and events industries well into the 21st century.

"AEG Ogden is privileged and excited by the opportunity to work with Government and industry in ensuring the venue is a key contributor to economic growth in Sydney.

"ICC Sydney is destined to be a showcase international venue in one of the world's leading cities and is a welcome addition to the AEG Ogden family of award winning venues in Australia, Asia and the Middle East," Mr Lister said.

Bruce Dixon, Spotless Group CEO said he was proud to be a partner in the Destination Sydney consortium.

"As the leader in Facility Management solutions across Australia, the SCIEEP project represents the 13th successful PPP that Spotless has been awarded, including 5 in NSW. This is an enviable record and confirms the value proposition that Spotless brings to the project. Spotless have a long history of working with our Consortium Partners on other projects and will be closely involved with the design development aspects over the coming years as well as working in an integrated fashion with AEG Ogden during the operational phase," he said.

Works on the convention, exhibition and entertainment facilities are expected to be completed by late 2016.

ENDS

For further information, please contact:

Corporate Affairs:

Christian Sealey
Communications Director
Destination Sydney
Tel: 0467 767 592

About the consortium partners:

AEG Ogden

AEG Ogden is a specialist operator of international convention, exhibition and entertainment venues. AEG Ogden manages venues throughout the Asia Pacific region, including the Brisbane, Cairns, Darwin, Kuala Lumpur and Qatar and Oman (under construction) Convention Centres, Suncorp Stadium in Brisbane and live theatres including Perth's His Majesty's Theatre and the Perth Concert Hall and entertainment arenas including Sydney's Allphones Arena, the Brisbane Entertainment Centre, the Newcastle Entertainment Centre, and the Perth Arena. In Western Australia, AEG Ogden also manages the State Theatre Centre, Albany Entertainment Centre, and Subiaco Arts Centre. Across the globe, venues operated by AEG Ogden present over 10,000 events annually.

Lend Lease

Lend Lease is a leading international property and infrastructure group. Listed on the Australian Securities Exchange and with circa 18,000 employees worldwide, Lend Lease's capabilities span the property and infrastructure spectrum. In Australia we offer

development management; investment management; project management & construction and asset & property management. Our expertise covers multiple sectors including commercial, residential, retail, retirement and infrastructure.

HOSTPLUS

HOSTPLUS is the national superannuation fund for the hospitality, tourism, recreation and sport industries in Australia. The Australian Hotels Association (AHA) and United Voice (formerly the Liquor Hospitality and Miscellaneous Union) jointly established the Fund in 1987. It is one of the largest super funds in Australia with 1 million members, over 80,000 employers and \$11 billion in funds under management.

Capella Capital

Capella Capital is the infrastructure financing and development arm of Lend Lease which possesses a substantial track record in the infrastructure sector, particularly in the delivery of PPP transactions. Capella Capital provides a fully integrated infrastructure solution – from project inception and financial advisory, through to development and project financing and into long-term ownership and equity management on behalf of Lend Lease.

Spotless

Spotless is a leader in the delivery of Facilities Management solutions across Australia. The SCIEEP project represents the 13th successful PPP that Spotless has been awarded, including 5 in NSW providing recent local experience. Spotless brings the necessary expertise to deliver efficient, non-disruptive facilities management services.