



KEMPSEY DISTRICT HOSPITAL REDEVELOPMENT (SSD5363)

RESPONSE TO SUBMISSIONS REPORT



<div><div></div><div><div>LFA (PACIFIC) PTY LIMITED</div><div>MASTERPLANNING URBAN DESIGN ARCHITECTURE LANDSCAPE ARCHITECTURE INTERIORS</div></div></div> <div><div><div>SUITE 4, EDGECLIFF COURT, 2 NEW McLEAN ST, EDGECLIFF NSW 2027</div><div>M PO BOX 259, EDGECLIFF NSW 2027 T 02 9327 6822 F 02 9327 5554</div><div>E lfai@lfa.com.au W www.lfa.com.au ABN 92 830 134 905 QM ISO 9001-2008</div></div><div><div>NOMINATED ARCHITECTS</div><div>ALF LESTER NO.2128 STEPHEN ANDERS NO.5764</div></div><div>LFA (Pacific) Pty Ltd. All rights reserved. This work is covered by copyright and cannot be reproduced or copied in any form or by any means without written permission of LFA (Pacific) Pty Ltd.</div></div>	Report	Response to Submissions	Rev	Amendments	Date	Prepared In Conjunction with:
	Project	Kempsey District Hospital	01	Revised as per HI comments	21.10.2013	Health Infrastructure NSW Enstruct GTA Consultants Rehbein Airport Consulting SKM S2F
	Project Status	Post-Exhibition	02	Revised as per HI comments	23.10.2013	
	RtS Director	AL				
	Checked by	AL				
	Date Issued	23 October 2013				
	Issue Type	For Minister's Determination				
	Proponent	<div><div></div><div>Health Infrastructure</div></div>				

CONTENTS

1.0	INTRODUCTION.....	5
1.1	Background.....	5
1.2	Purpose of the Report.....	5
1.3	Structure of the Report.....	5
2.0	SCHEDULE OF SUBMISSIONS – ISSUES AND RESPONSES.....	7
2.1	Civil Aviation Safety Authority (CASA)	8
2.2	Kempsey Shire Council (KSC).....	9
2.3	NSW Rural Fire Services	14
2.4	NSW Heritage Council	16
2.5	Office of Environment and Heritage (OEH).....	17
2.6	Transport for NSW (TfNSW) / Roads and Maritime Services (RMS) ..	18
3.0	DP&I MATTERS	19
3.1	Department of Planning & Infrastructure.....	19
4.0	CHANGES TO THE PROPOSED DEVELOPMENT	23
4.1	NEW HOSPITAL BUILDING	23
4.2	REFURBISHMENT WORKS.....	25
4.3	General Landscape.....	26
5.0	CONCLUSION	27
6.0	SCHEDULE OF APPENDICES.....	28

This page has been left intentionally blank

1.0 INTRODUCTION

1.1 BACKGROUND

In late July 2013, NSW Health Infrastructure (the Proponent) lodged a State Significant Development Application (SSDA) and Environmental Impact Statement (EIS) for the Kempsey District Hospital Redevelopment (SSD5363) under State Environmental Planning Policy (State and Regional Development) 2011 (SRD SEPP).

The SSDA and EIS seek consent for:

- Construction of a new five storey hospital building
- Refurbishment of existing hospital buildings
- Demolition of existing Blood Bank building
- Modified Main Entrance
- Modified car parking for staff, patients and visitors
- Landscape works
- Associated infrastructure works

The SSDA and EIS was formally lodged on 30 July 2013 and was publicly exhibited by the Department of Planning & Infrastructure (DP&I) from 7 August 2013 to 5 September 2013.

Following public exhibition, a letter from the Department of Planning & Infrastructure (DP&I) dated 13 September advised the Proponent of the submissions lodged by State government agencies and Kempsey Shire Council, with a request that the Proponent respond to such issues. There were no submissions received from the general public.

In addition, DP&I requested the following matters to be addressed:

- Traffic impacts and the need for potential intersection upgrades or works;
- Further details regarding pedestrian and vehicle movements across and around the site, including a pedestrian and vehicle circulation plan, and any further measures proposed to ensure pedestrian safety;
- Potential relocation of bus stops to improve vehicular and pedestrian safety; and

- Clarification regarding the extent of works forming part of the application (i.e. whether the proposed site works form part of the application) and, if so, incorporate details of the proposed works on the staging plan, relevant floor plan and landscaping plans.

1.2 PURPOSE OF THE REPORT

This report has been prepared on behalf of the Proponent as part of the SSDA and EIS under SRD SEPP 2011.

The information contained in the report has been prepared in response to the issues raised in the submissions lodged during Public Exhibition as well as the matters raised by DP&I in their letter dated 13 September 2013.

1.3 STRUCTURE OF THE REPORT

This report is structured as follows:

PART	DESCRIPTION
Introduction	Provides an overview of the project and relevant background information.
Schedule of Submissions - Issues and Responses	For each submission, a schedule of issues and responses is set out.
DP&I Matters	Provides response to matters raised by DP&I
Changes to the Proposed Development	Provides additional information with regard to changes to the proposed building design and landscaping.
Conclusion	Provides a recommendation to approve the proposed development outlined in the Project Application and this Preferred Project Report.
Schedule of Appendices	Documentation in support of the Proponent's responses.

TABLE 01: Structure of the Report

Accordingly, the Proponent looks forward to the Minister's favourable consideration of the SSDA, EIS and this supporting report.

This page has been left intentionally blank

2.0 SCHEDULE OF SUBMISSIONS – ISSUES AND RESPONSES

Following public exhibition of the Project Application and Environmental Assessment, submissions were received from:

- Civil Aviation Safety Authority (CASA)
- Kempsey Shire Council (KSC)
- NSW Rural Fire Service (RFS)
- Office of Environment and Heritage (OEH)
- NSW Heritage Council
- Transport for NSW (TfNSW) and Roads and Maritime Services (RMS).

In response to DP&I's request for the Proponent to address the issues raised from the above mentioned parties, this section provides a schedule of the key issues and responses for each submission.

Copies of the submissions are provided in Appendix A.

2.1 CIVIL AVIATION SAFETY AUTHORITY (CASA)

ISSUES / COMMENTS	RESPONSE
<p><i>CASA recommends the following consultation process to assess the potential impact of the proposed development on aviation:</i></p> <ul style="list-style-type: none"> <i>Consult with Kempsey Shire Council, operator of Kempsey Aerodrome, to determine any impact on their OLS. Once Kempsey Shire Council makes an assessment as to whether or not there will be an impact on the OLS, CASA will be in a position to act on such advice and make subsequent recommendations. Part of the process is consultation with Airservices Australia to assess any potential impact on PANS-OPS and airways facilities; and</i> <i>Consult with the operators of the helicopter landing pad in the vicinity to determine any operational impact on their facility; The same consultation process should be applied to construction cranes. The crane information should be reviewed at the planning stage of the development.</i> 	<p>Kempsey Shire Council (KSC) has been consulted with respect to the impact of the proposed development on the operation of the Kempsey Aerodrome. KSC confirmed that the proposed permanent structures will not infringe the height restrictions associated with the Obstacle Limitation Surfaces (OLS). Any cranes which may be required during construction will need to be assessed against the map provided by KSC to determine if they will infringe the OLS. Any infringements of the OLS will be subject to assessment and approval by KSC, CASA and Airservices Australia and if accepted may require marking and lighting to ensure any hazard to aircraft operations is acceptable.</p> <p>As the proposed development does not penetrate to Kempsey Aerodrome OLS it should not affect the currently published instrument procedures associated with the aerodrome. If penetration of the OLS during construction is identified, this will need to be subject to more detailed assessment by Airservices Australia at that time.</p> <p>Consultation with the Ambulance Service of New South Wales (ASNSW) with respect to air ambulance operations at the adjacent Helicopter Landing Site (HLS) has been undertaken in relation to the upgrade of the HLS which forms part of the redevelopment project.</p>

2.2 KEMPSEY SHIRE COUNCIL (KSC)

ISSUES / COMMENTS	RESPONSE
<p>KSC-01 2.8 and 4.14 Flooding</p> <p><i>The flood information contained in this section is incorrect. The following data needs to be incorporated into the EIS: -</i></p> <p><i>The 1% AEP flood level ranges from RL 12.24m AHD in the Macleay River opposite Polwood Street to RL 11.48m AHD opposite Broughton Street.</i></p> <p><i>The Probable Maximum Flood level ranges from RL 20.81m AHD in the Macleay River opposite Polwood Street to RL 20.1m AHD opposite Broughton Street.</i></p> <p><i>During the PMF flood event all road access to the hospital will be cut preventing any sort of vehicle movements to or from the Kempsey Hospital. The preferred road access North Street to River Street will be cut during this event by flood waters at depth of 3.3m.</i></p> <p><i>The EIS needs to make reference to the NSW State Emergency Services document Kempsey Shire Local Flood Plan and the Kempsey Local Disaster Plan.</i></p> <p><i>Restrictions on critical emergency response and recovery facilities and infrastructure such as this hospital has not been addressed for flooding above the 1% AEP flood event.</i></p>	<p>Flood mapping data provided by the State Emergency Service (SES) and Kempsey Shire Council (KSC) for the 1 in 100 year flood demonstrates that the hospital site, which is situated at a high point within Kempsey, remains entirely above the maximum extent of flooding. The site is also fully accessible by road via River Street and North Street to the north throughout the storm event, permitting safe access and egress for emergency vehicles. The EIS discusses this in detail and the attached SES information pamphlet illustrates the extent of flooding.</p> <p>During more extreme rainfall events (longer return period) up to the probable maximum flood (PMF), water levels within Kempsey will exceed those of the 1 in 100 year event. The Kempsey Shire Local Flood Plan document – Annex A: The Nature of Flooding in the Kempsey Shire (refer to Appendix B) indicates that during the PMF event much of Kempsey town centre will experience widespread and severe flooding. However, both the existing and proposed buildings within the hospital campus, as well as the access roads into the site from River Street and Polwood Street remain above the PMF flood levels provided by KSC. Design of building floor levels has been carried out in accordance with the NSW Floodplain Development Manual 2005.</p> <p>Based on flooding levels provided by KSC, during the PMF event it is likely that vehicular access to the site by road will be impractical by virtue of the extent of floodwater. In this event the strategy for site access and/or egress will be via waterborne craft and wading or by air to the helicopter compound which is also above the PMF flood levels. In the event of extreme flooding, the best strategy for users and patients in the hospital would be to wait for floodwaters to recede until access/egress by roads becomes navigable. During this time, the hospital has flooding resilience in the form of a backup generator for power and similarly LPG and medical gases are available in on site storage tanks. The time needed for floodwaters to recede and site access by road to become available would typically be in the order of 1-3 days and Council may have more information on this timeframe based on hydraulic modelling.</p> <p>The proponent considers this approach to be a satisfactory planning, design and operational response for the hospital campus reacting to the PMF event flooding owing to the extreme severity of this scenario and the widespread extent of flooding within Kempsey. For lesser events than the PMF flood which exceed the 1 in 100 year event, access and egress to/from the site may be possible by road depending on the depth of floodwater and the speed of flows. Emergency services requiring access to the hospital during such events will need to assess the safety implications of driving vehicles through streets flooded with standing water before proceeding.</p>

ISSUES / COMMENTS	RESPONSE
	<p>To remedy these access issues associated with the PMF event would involve either relocating the hospital campus or implementing significant town-wide infrastructure upgrades to surrounding streets, neither of which are viable or feasible for the scale of proposed works. It is further noted that the project is primarily a redevelopment of existing facilities and as such will not exacerbate the existing condition. Nevertheless, the project maintains demonstrable access routes and evacuation paths in the 1 in 100 year flood event which has not been exceeded in the history of flood recordings from 1838 (Kempsey Local flood plan Annexure A in Appendix B).</p>
<p>KSC-02 2.12.4 Pedestrian</p> <p><i>Council has in place a Pedestrian Access and Mobility Plan for Kempsey. The figure at Appendix A shows the Pedestrian Route Network for Kempsey. This plan should be referred to.</i></p>	<p>The Pedestrian Access and Mobility Plan (PAMP) was prepared in 2003 and details the long, medium and short term priorities for Kempsey. The plan proposes:</p> <ul style="list-style-type: none"> • Footpath along the south side of Polwood Street and a pedestrian refuge at the intersection of Polwood Street and River Street. • Pedestrian refuge and blisters along River Street opposite the hospital. (similar to the proposed) • Footpath on the east side of Tozer Street. <p>These facilities were classified as medium term works, however they have not yet been funded/ implemented by Council. Implementation of the PAMP works would benefit hospital users but are not required as part of the redevelopment.</p>
<p>KSC-03 <i>It is proposed to link River Street Pedestrian path with the existing internal pedestrian paths to the new Polwood Street entrance.</i></p> <p><i>The existing pedestrian path is in River Street and there are is no pedestrian path from the intersection of Polwood and River Streets along Polwood Street road reserve or any other interconnections at Tozer Street. The closest other pedestrian paths is located on the eastern corner of Tozer and Broughton Streets.</i></p>	<p>Noted.</p> <p>There is no specific requirement for the project to modify the current pathway system on Council's verge.</p>

ISSUES / COMMENTS	RESPONSE
<p>KSC-04 1. Section 94 (Ministers Discretion)</p> <p><i>If Council is to seek to collect contribution for the Local Traffic Plan then an argument needs to be put to the Minister.</i></p> <p><i>The following preliminary comment is provided: -</i></p> <ul style="list-style-type: none"> <i>Development claiming exemption for local roads plan on the basis “Given that Health Infrastructure NSW will carry out local road improvements”.</i> <i>There is no indication in the EIS or associated documents that there will be improvements to Council roads undertaken as part of the proposed redevelopment. At recent meetings there was a clear intention not to upgrade any part of Polwood Street.</i> <i>Section 4.6.4 of the EIS states the redevelopment will generate an additional 50 vehicles per hour at the peak and a total of 250-300 vehicles per day.</i> <i>The additional traffic generated by the redevelopment will not require by itself intersection upgrading, cycle paths, or the like on the associated Council roads.</i> <i>However they will add traffic and have a cumulative impact which will generate the need for upgrades. Therefore a contribution needs to be paid now to go toward future upgrading of the surrounding public roads.</i> 	<p>The findings of the exhibited Traffic Report indicated that the existing road network has sufficient capacity to accommodate the estimated traffic generation associated with Kempsey District Hospital Redevelopment, without the need for any road network upgrades. It would appear that KSC has accepted the above findings.</p> <p>However, Council has suggested that the traffic generation in combination with an increase in local traffic along this road (associated with other developments etc.) would result in future road upgrades.</p> <p>The redevelopment is a significant investment in social infrastructure on the Kempsey and therefore in accordance with current Government Policy would be exempt from contributions levied under section 94.</p> <p>It is noted that the majority of forecast growth in demand is a function of the existing facility rather than attributable to the redevelopment itself.</p> <p>Based on the exhibited Traffic Report there is no nexus between the redevelopment and the infrastructure requested by Council.</p>

ISSUES / COMMENTS	RESPONSE
<p>KSC-05 2. S64 Water and Sewer</p> <ul style="list-style-type: none"> <i>The only indication of the type and size of the redevelopment is contained in the various supplementary reports appended to the EIS.</i> <i>The Water Directorate provides standard ET figures for hospitals at the current rate of for water 0.9et/bed and sewer 1.4et/bed.</i> <i>There will be an additional 13 beds but more importantly a total additional gross floor area of 3700 m2.</i> <i>In any negotiations with Council and certainly in this part of the EIS the Crown needs to identify the use to which the additional gross floor area is to be put in the context of either replacing and or expansion of existing services.</i> 	<p>It is noted that the proposal is for the redevelopment, rather than the expansion, of hospital facilities. The increase in floor area is predominantly due to implementation of the latest Health Facility Guidelines which have dramatically increased in the clinical area required to provide contemporary and more efficient service delivery in comparison to the old (approximately 60 years and over) facilities. For example, theatres are now 55m2 compared to 33m2. It is further noted that a significant proportion of floor space is required to link into existing facilities and encapsulate travel and engineering.</p> <p>The Hospital's existing 150mm sewer main has been discharging into an authority's sewer manhole located at the intersection of Tozer and Tabrett Streets with 450mm outlet. Discussions with Council to date confirm that this existing authority sewer main has sufficient capacity for discharge from Hospital.</p> <p>A review of the existing and proposed water demand and sewer discharge load associated with the redevelopment of the hospital has been undertaken and it is confirmed that there will be no specific increase of water supply and sewer discharge associated with the site as a consequence of the Hospital redevelopment. In addition, decommissioning and demolition of the existing commercial laundry and sterilisation services reduces the existing pre-redevelopment usage /discharge by an estimated min 1440L/day (based on standard hospital laundry operational parameters of 24l/Day/bed).</p> <p>In relation to water supply, it is confirmed that the new works will be incorporating on-site Fire Services water storage provisions of 230,000L and a 10,000L for domestic water supply. These provisions have been incorporated to confirm site redundancy and reducing the water supply suction demand / load on the existing Council infrastructure.</p>

ISSUES / COMMENTS	RESPONSE
<p>KSC-06 4.15.2 Stormwater Management Strategy</p> <ul style="list-style-type: none"> • <i>There is no reference to the Enstruct's Stormwater Management Plan (SMP) at Appendix K2;</i> • <i>There is no mention in either the SMP or the EIS on water quality targets and means of achieving them, only that a gross pollutant capture system will be installed prior to discharge to the Macleay River which does not address water quality discharge from the site as a whole;</i> • <i>There is no discussion anywhere in the EIS or supporting documentation on the location of the proposed new piped stormwater drain outlet at the Macleay River and the method to be deployed to reduce the outlet velocity in the pipe to prevent scour at the river bank;</i> • <i>There are no details on the effect on the existing environment (flora and fauna habitat) at the proposed pipe discharge point at the Macleay River;</i> 	<p>The new stormwater drain outlet has been approved under the provisions of ISEPP via Review of Environmental Factors (REF). Details of the water quality treatment works being undertaken as part of the approved REF are touched on in the stormwater management report and illustrated on design drawings included in the Appendices.</p> <p>In brief, the works involve the installation of two vortex separator devices which primarily aim to remove suspended sediment and gross pollutants as well as free oils from surface water run off. Further information is available from the manufacturer's website.</p> <p>These devices are intended to deliver good quality clean stormwater discharge to the River Macleay. The design of these devices considers the water quality impacts of the hospital development and the associated stormwater drainage servicing the building perimeter approved under the EIS.</p>

2.3 NSW RURAL FIRE SERVICES

RECOMMENDED CONDITIONS	RESPONSE
<p>(1) <i>The development proposal is to comply with the layout identified on the drawing titled Site Plan - Proposed, prepared by BVN Donovan Hill, reference Drawing No. 5027-BVN-AR-DWG-1 DA-A01 .</i></p>	<p>It is noted that the relevant drawing has been revised. Please refer to Section 4.0 of this report and Appendix C.</p>
<p>Asset Protection Zones</p> <p><i>The intent of measures is to provide sufficient space for fire fighters and other emergency services personnel, ensuring radiant heat levels permit operations under critical conditions of radiant heat, smoke and embers, while supporting or evacuating occupants. To achieve this, the following conditions shall apply:</i></p> <p>(2) <i>At the commencement of building works and in perpetuity the entire site of the proposed Hospital redevelopment shall be managed as an inner protection area (IPA) as outlined within section 4.1.3 and Appendix 5 of ‘Planning for Bush Fire Protection 2006’ and the NSW Rural Fire Service’s document ‘Standards for asset protection zones’.</i></p> <p>(3) <i>Landscaping of the site shall comply with the principles of Appendix 5 of ‘Planning for Bush Fire Protection 2006’.</i></p>	<p>Agreed.</p>
<p>Design and Construction</p> <p><i>The intent of measures is that buildings are designed and constructed to withstand the potential impacts of bush fire attack. To achieve this, the following conditions shall apply:</i></p> <p>(4) <i>The existing buildings located less than 100m from bushfire prone land on the western side of River Street (as shown in Figure 12 of the Bushfire Protection Assessment prepared by Australian Bushfire Protection Planners Pty Ltd, dated 20 April 2013) shall be upgraded for ember protection. This is to be achieved by enclosing all openings (excluding roof tile spaces) or covering openings with a non-corrosive metal screen mesh with a maximum aperture of 2mm. Where applicable, this includes any sub floor areas, openable windows, vents, weepholes and eaves. External doors are to be fitted with draft excluders.</i></p> <p>(5) <i>Roofing of the mental health and maternity buildings shall be gutterless or guttering and valleys are to be screened to prevent the build up of flammable material. Any materials used shall be non-combustible.</i></p>	<p>Agreed.</p>

RECOMMENDED CONDITIONS	RESPONSE
<p>Access</p> <p><i>The intent of measures for internal roads is to provide safe operational access for emergency services personnel in suppressing a bush fire, while residents are accessing or egressing an area. To achieve this, the following conditions shall apply:</i></p> <p>(6) <i>Internal roads shall comply with section 4.2.7 of 'Planning for Bush Fire Protection 2006'.</i></p>	Agreed.
<p>Water and Utilities</p> <p><i>The intent of measures is to provide adequate services of water for the protection of buildings during and after the passage of a bush fire, and to locate gas and electricity so as not to contribute to the risk of fire to a building. To achieve this, the following conditions shall apply:</i></p> <p>(7) <i>Water, electricity and gas are to comply with section 4.2.7 of 'Planning for Bush Fire Protection 2006'.</i></p>	Agreed.
<p>Evacuation and Emergency Management</p> <p><i>The intent of measures is to provide suitable emergency and evacuation (and relocation) arrangements for occupants of special fire protection purpose developments. To achieve this, the following conditions shall apply:</i></p> <p>(8) <i>An emergency and evacuation plan addressing Section 4.2. 7 of 'Planning for Bush Fire Protection 2006' shall be prepared for the hospital redevelopment, or the existing plan is to be amended to include considerations pertaining to the proposed redevelopment.</i></p> <p><i>A copy of the plan shall be provided to the consent authority prior to the issuing of an occupation certificate.</i></p>	Agreed.

2.4 NSW HERITAGE COUNCIL

ISSUES / COMMENTS	RESPONSE
<p><i>It is noted that the development site is listed in Schedule 1 of the Kempsey LEP 1987 and the Department of Health S170 register. The site is NOT listed on the State Register.</i></p> <p><i>The Heritage Division raises no objection and supports the recommendations outlined in Section 7.2 of the Heritage Impact Statement by Graham Brooks and Associates (in relation to archaeological relics, archival recording, and interpretation). The Heritage Division recommends that the following condition be included:</i></p> <ul style="list-style-type: none"> <i>Where substantial intact archaeological relics of State or local significance are discovered during excavation, work must cease in the affected area and the Heritage Council must be notified in writing in accordance with section 146 of the Heritage Act 1977. Depending on the nature of the discovery, additional assessment may be required prior to the recommencement of excavation in the affected area.</i> 	<p>The proposed condition is a standard condition. The Proponent accepts the recommended condition.</p>

2.5 OFFICE OF ENVIRONMENT AND HERITAGE (OEH)

ISSUES / COMMENTS	RESPONSE
<p><i>Threatened species, endangered populations and ecological communities</i></p> <ul style="list-style-type: none"> <i>OEH confirms that no endangered populations or ecological communities are present. Accordingly, OEH supports the conclusion in the EIS that the proposal is unlikely to adversely affect threatened species and their habitat, endangered populations or ecological communities and/or any wildlife corridors. No mitigation measures or conditions of consent are recommended by OEH.</i> <p><i>Flooding, coastal floodplains and estuaries</i></p> <ul style="list-style-type: none"> <i>OEH is of the view that the proposal is unlikely to have an adverse impact upon flooding, coastal floodplains or estuaries. No additional mitigation measures or conditions of consent are recommended by OEH</i> <p><i>Aboriginal Cultural Heritage</i></p> <ul style="list-style-type: none"> <i>OEH supports the management strategies to be implemented in the event that any Aboriginal objects are subsequently discovered.</i> <i>If Aboriginal objects are uncovered during the proposed development, the objects must be recorded and managed in accordance with the National Parks and Wildlife Act 1974 (NPW Act) and avoided if possible. A suitably qualified cultural heritage specialist and representatives of the local Aboriginal community must be contacted to determine the nature, extent and significance of any finds. The site is to be registered in the OEH Aboriginal Heritage Information Management System (AHIMS) and the management outcome for the site included in the information provided to AHIMS. The proponent must consult with representatives of the local Aboriginal community, and a cultural specialist to develop and implement and appropriate management strategies for all objects/sites.</i> <i>Any management strategy must comply with appropriate legislative provisions. OEH advises that the requirements of the NPW Act have been amended recently and recommends familiarisation by the proponent with these amendments prior to work commencing. Provided the proposed management strategies listed in the EIS and OEH requirements above are included in appropriate consent conditions, OEH has no additional issues to raise with the Aboriginal cultural heritage assessment.</i> 	<p>Noted and agreed.</p>

2.6 TRANSPORT FOR NSW (TFNSW) / ROADS AND MARITIME SERVICES (RMS)

ISSUES / COMMENTS		RESPONSE
RMS-01	<i>The key interests for TfNSW/RMS are the safety and efficiency of the road network, traffic management, the integrity of infrastructure assets and the integration of land use and transport.</i>	Noted.
RMS-02	<i>River Street (MR75) forms part of the classified road network. Kempsey Shire Council is the 'Roads Authority' for this road and all other roads in the area. RMS concurrence is required under S138 of the Roads Act 1993 for works within the road reserve of MR75. Consultation with the Council, as the roads authority, should be undertaken by the consent authority.</i>	Noted.
RMS-03	<p><i>The 'Transport & Accessibility Study' as prepared by GTA Consultants has 'generally' addressed the direct access and parking impacts of the proposal. However, TfNSW/RMS notes that the report has not adequately addressed the following issues:</i></p> <ul style="list-style-type: none"> <i>The impacts of additional traffic generation on adjacent intersections - In particular, the identified increase in traffic movements at the River Street / Polwood Street intersection should be considered given that the existing intersection layout has no protection for vehicles turning right into Polwood Street from River Street. The proposed redevelopment will result in increased demand for this right-turn movement given the location of on-site parking and Ambulance access. TfNSW/RMS recommends that an appropriate intersection treatment be provided to safely accommodate the traffic movements generated by the development. RMS and Council need to be consulted by the proponent for technical input and comment on any option proposed.</i> 	<p>The Traffic Consultant has assessed the warrants for implementing a right turn treatment at the intersection of River Street and Polwood Street based on Austroads Guidelines.</p> <p>A channelised right turn treatment could be justified on the basis of both existing and predicted future traffic volumes. However, such a treatment is not justified on intersection performance grounds or crash history. On-site observations indicate that the right turn movement operates effectively and that through vehicles can overtake safely using the wide lane (approx 7m). Given that there would be a small increase in turning movements (approx. 10 vehicles per peak hour) it would be more suitable to monitor the impacts on turning movements before implementing costly intersection improvements.</p>

3.0 DP&I MATTERS

This section addresses the matters raised by DP&I in its letter dated 13 September 2013:

3.1 DEPARTMENT OF PLANNING & INFRASTRUCTURE

ISSUES / COMMENTS	RESPONSE
<ul style="list-style-type: none"> <i>Traffic impacts and the need for potential intersection upgrades or works;</i> 	<p>Traffic impacts have been addressed in the GTA report. No intersection upgrades are justified on the basis of the anticipated traffic volume increases (refer to response to RMS-03 on page 17).</p>
<ul style="list-style-type: none"> <i>Further details regarding pedestrian and vehicle movements across and around the site, including a pedestrian and vehicle circulation plan, and any further measures proposed to ensure pedestrian safety;</i> 	<p>Refer to Figure 1.</p> <p>It is noted that an island refuge crossing on River Street to facilitate safe crossing from the approved car park to the west of River Street to the main Hospital site.</p>
<ul style="list-style-type: none"> <i>Potential relocation of bus stops to improve vehicular and pedestrian safety; and</i> 	<p>Bus stop facilities have been addressed in the above response to TfNSW comments (refer to response to RMS-03 on page 18).</p>
<ul style="list-style-type: none"> <i>Clarification regarding the extent of works forming part of the application (i.e. whether the proposed site works form part of the application) and, if so, incorporate details of the proposed works on the staging plan, relevant floor plan and landscaping plans.</i> 	<p>The extent of works forming part of the application is shown in Figure 2 and is included in the Appendix C.</p> <p>The staging plans provided in Appendix A of the Exhibited EIS indicate the various works to be undertaken on site and the approval strategy for each component. Refer to exhibited drawings:</p> <ul style="list-style-type: none"> 5027-BVN-AR-DWG-1DA-A11 5027-BVN-AR-DWG-1DA-A12 5027-BVN-AR-DWG-1DA-A13 5027-BVN-AR-DWG-1DA-A14 (It is note that this drawing is now superseded by Figure 2 / Drawing 5027-SKM-AR-DWGN1-B101 contained in Response to Submissions report).

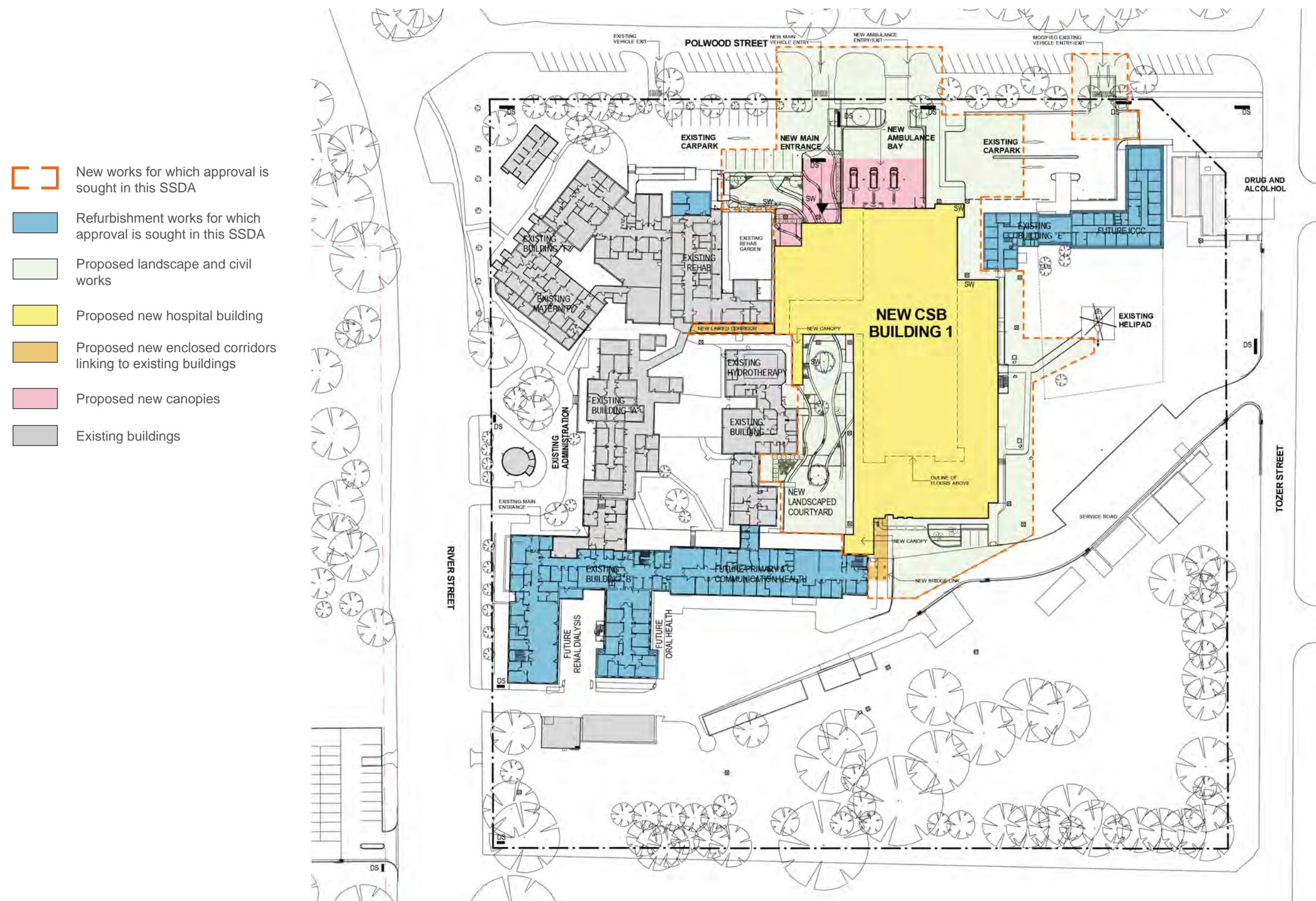


FIGURE 02: Site Plan - Proposed Works

Source: SKM / S2F

This page has been left intentionally blank

4.0 CHANGES TO THE PROPOSED DEVELOPMENT

4.1 NEW HOSPITAL BUILDING

As part of the ongoing review and resolution of the proposed redevelopment, the built form of the new hospital has been subject to minor changes.

The key outcomes from the design review process were:

- Simplified building shape and form
- More efficient access circulation systems
- Easier access
- Functional layouts with improved clarity and efficiency
- Architecture that is enhanced and simplified to suit scale of the regional hospital.

Revised architectural and landscape drawings are provided in Appendix C and D respectively. The following describes the key changes that have been made to the exhibited design.

Revised 3D visualisations of the revised development are shown in Figures 2, 3 and 4.

General Layout

The revised design creates a simplified building footprint with better adherence to Health Infrastructure's systemised modular design, optimising floor area efficiency. The revised design realigns many corridors to simplify and improve wayfinding and circulation.

Ambulance Bay

Ambulance access and manoeuvring space has been improved by:

- Realigning the ambulance entry driveway
- Changing the shape and size of the manoeuvring area
- Preventing unwanted pedestrian access to the ambulance bay
- Adding a fourth ambulance bay

Main Entry Forecourt

- The main entrance of the hospital has been brought out closer to the drop off point to minimise the travel distance needed to get inside the building. This represents a minor expansion of the ground floor building footprint at the north-western corner of the building.
- Closer proximity of the building to Polwood St increases the visibility and profile of the main entrance.
- The revised landscape design frames and compliments the new building, creating a welcoming arrival point and an access way from the carpark.
- There is a covered walkway between the carpark and building, seating elements, and screening planting to the western edge adjoining the existing Rehab building. A continuous paving treatment connects the entry and internal areas reinforcing circulation and indoor / outdoor flow.

The previous design proposed two separate entries for the emergency department and the main hospital entrance. The revised design combines these at a single entrance location, so that:

- The entrance lobby is more prominent and articulated more effectively.
- One point of entry for the hospital creates a clearer public identity.

Main Entry Foyer

The revised scheme relocates the Cultural Room to the Polwood St entry of the hospital, allowing greater accessibility from the public domain and after hours.

The revised design scheme rationalises access to the upper floors by improving the alignment of the stairs and lifts so that users arrive at the same location on all the floors.

Link corridor to Maternity

The previous scheme had an enclosed link corridor from the new build to the maternity department, located in the middle of a landscaped area between the rehab building and hydrotherapy. The revised design locates the corridor against the Rehab building leaving a landscaped space sufficiently wide for daylight to enter.

Simplified connection to ICCC

The previous design scheme had a service connection to the ICCC building that required access through the emergency department and down a dog leg ramp to the existing entrance door into the future ICCC. The revised scheme uses a route that avoids the need to enter any department and instead allows direct access east through the new building at ground level to an external 1:20 ramp running north to the the ICCC building through new entry doors. This is a more direct and efficient path with less turns.

Simplified Roof and Plant Enclosure

The revised design has a simplified roof geometry and a rationalised specification of materials using smaller plant enclosures with colorbond steel roofs.

As a consequence, the maximum height of the building is reduced from RL 48.574 as shown in the exhibited drawings to RL 47.100 in the roof plan included in Appendix C.

Open fire stairs

Since the fire stairs are used for escape only and aren't required for circulation around the building, the revised scheme has open fire stairs which avoid the need for stair pressurisation. A fire engineered approach has also allowed a reduction in the number of fire compartments on ground level and better coordination of compartments with the HVAC air supply from risers, eliminating the need many fire dampers.



FIGURE 03: Revised main entrance from Polwood Street



FIGURE 04: View from Tozer Street

Entrance vista to landscaped courtyard

The previous scheme proposed a foyer with a hindered vista through to the internal courtyard from the Northern entry point. This vista faced directly towards the canopy entrance of the hydrotherapy building rather than out into the landscape courtyard. Also the entry stair obscured the view out to the landscape.

In the revised scheme the foyer is relocated to the east so that the vista through the lobby continues outwards to the landscaped courtyard instead of facing towards the canopy of the hydrotherapy building.

4.2 REFURBISHMENT WORKS

Approval for refurbishment works associated with the following is no longer sought as part of this SSDA:

- Drug & Alcohol
- Hotel Services
- Aboriginal Health
- HETI
- Health Promotion

The above refurbishment works are very minor (additional furniture and painting) and will not involve structural works. Accordingly, the works are to be carried out as Exempt Development.

4.3 GENERAL LANDSCAPE

The landscape design adopts healing principles, centred on the interaction of patients with the landscape. This is carried through with the use of traditional medicinal and bush tucker plants in communal green spaces. Circulation and sight lines across the hospital are reinforced and enhanced. Detailed landscape drawings are provided in Appendix D.

Central Healing Garden (Cleared of Obstructions)

The central healing garden links the new building to existing facilities and forms an extension of the main entry foyer. The garden will be viewed from all sides and has pockets of seating and turf gathering areas. Planting is mostly beds of mass planting around the edges of the space with a few trees. The curved line motif carries through into this area from the entry forecourt.

The revised design clears the courtyard of various obstructions notably the cultural room and associated toilets. These are now located at the front of the building and can be directly accessed off the main entry lobby. This enhances the landscape design and amenity of the courtyard.



FIGURE 05: View of internal courtyard

5.0 CONCLUSION

The responses set out in this report address the issues raised in the submissions lodged by State government agencies and Kempsey Shire Council together with clarification on issues raised relating to the information provided in the EIS (including Appendices) and the Proponent's agreement to suggested conditions of consent proposed by State Government agencies.

The report also provides additional information and revised drawings regarding changes to the proposed building design.

This report and accompanying Appendices A and B, together with the SSDA and EIS and Appendices A- R lodged with the Department of Planning and Infrastructure in July 2013, form the complete application for the Kempsey District Hospital Redevelopment (SSD5363).

Accordingly, Health Infrastructure looks forward to the Minister's favourable consideration of the Project Application and this supporting report.

6.0 SCHEDULE OF APPENDICES

A. Submissions
Received by the Department of Planning and Infrastructure
B. Kempsey Shire Council Flood Information
C. Revised Architectural Drawings and Shedule of Materials and Finishes
Prepared by SKM S2F
D. Revised Landscape Drawings
Prepared by Site Image

APPENDIX **A**

Submissions



Australian Government
Civil Aviation Safety Authority

AIRSPACE AND AERODROME REGULATION

File Ref: G113/987

Your Ref: SSD 5363

28 August 2013

Ms Megan Fu
NSW Government
Planning & Infrastructure
GPO Box 39
SYDNEY NSW 2795



Dear Ms Fu,

Re: Kempsey District Hospital Redevelopment (SSD 5363)
Notice of Exhibition

The airspace above the Kempsey Hospital area is affected by the Kempsey Aerodrome Obstacle Limitation Surfaces (OLS) and OLS related to the hospital helicopter landing area.


CASA recommends the following consultation process to assess the potential impact of the proposed development on aviation:

- Consult with Kempsey Shire Council, operator of Kempsey Aerodrome, to determine any impact on their OLS. Once Kempsey Shire Council makes an assessment as to whether or not there will be an impact on the OLS, CASA will be in a position to act on such advice and make subsequent recommendations. Part of the process is consultation with Airservices Australia to assess any potential impact on PANS-OPS and airways facilities; and
- Consult with the operators of the helicopter landing pad in the vicinity to determine any operational impact on their facility;


The same consultation process should be applied to construction cranes. The crane information should be reviewed at the planning stage of the development.

If you require more information or clarification please do not hesitate to contact me (02) 8651 3110 or by email slavica.despotovic@casa.gov.au

Yours sincerely,


Slavica Despotovic
Aerodrome Inspector
Airways and Aerodromes

GPO Box 2005 Canberra ACT 2601 Telephone 131 757
Canberra, Brisbane, Darwin, Cairns, Townsville, Tamworth, Sydney, Melbourne, Adelaide, Perth



Kempsey

Shire Council

www.kempsey.nsw.gov.au
ksc@kempsey.nsw.gov.au
ABN: 70 705 618 663

Your council Our community

Ref: D13/25180, T6-13-248, LA 2765
KJA:JH

10 September 2013

Ms Heather Warton
Director, Industry, Social Projects and Key Sites
GPO Box 39
SYDNEY NSW 2001

Dear Ms Warton

DEVELOPMENT APPLICATION T6-13-248
LOT 2 SEC 20A DP759080, 119 RIVER STREET WEST KEMPSEY
KEMPSEY HOSPITAL REDEVELOPMENT STAGE 1

Thank you for the opportunity to comment of the redevelopment of the Kempsey Hospital.
Please find below Council's comments on the EIS:


SUBJECT	COMMENTS
<p>2.8 and 4.14 FLOODING</p>	<p>The flood information contained in this section is incorrect. The following data needs to be incorporated into the EIS: -</p> <p><i>The 1% AEP flood level ranges from RL 12.24m AHD in the Macleay River opposite Polwood Street to RL 11.48m AHD opposite Broughton Street.</i></p> <p><i>The Probable Maximum Flood level ranges from RL 20.81m AHD in the Macleay River opposite Polwood Street to RL 20.1m AHD opposite Broughton Street.</i></p> <p><i>During the PMF flood event all road access to the hospital will be cut preventing any sort of vehicle movements to or from the Kempsey Hospital. The preferred road access North Street to River Street will be cut during this event by flood waters at depth of 3.3m.</i></p> <p><i>The EIS needs to make reference to the NSW State Emergency Services document <u>Kempsey Shire Local Flood Plan</u> and the <u>Kempsey Local Disaster Plan</u>.</i></p> <p>Restrictions on critical emergency response</p>

22 Tozer Street
West Kempsey

PO Box 3078
West Kempsey NSW 2440

Customer Service Tel: 02 6566 3200
Fax: 02 6566 3205

Library Tel: 02 6566 3210
Fax: 02 6566 3215



Kempsey

Shire Council

www.kempsey.nsw.gov.au
ksc@kempsey.nsw.gov.au
ABN: 70 705 618 663

Your council Our community


SUBJECT	COMMENTS
	<p>and recovery facilities and Infrastructure such as this hospital has not been addressed for flooding above the 1% AEP flood event.</p>
<p>2.12.4 Pedestrian</p>	<p>Council has in place a <i>Pedestrian Access and Mobility Plan</i> for Kempsey. The figure at Appendix A shows the Pedestrian Route Network for Kempsey. This plan should be referred to.</p>
<p>3.7 Access</p>	<p>It is proposed to link River Street Pedestrian path with the existing internal pedestrian paths to the new Polwood Street entrance.</p> <p>The existing <i>pedestrian</i> path is in River Street and there are is no pedestrian path from the intersection of Polwood and River Streets along Polwood Street road reserve or any other interconnections at Tozer Street. The closet other pedestrian paths is located on the eastern corner of Tozer and Broughton Streets.</p>
<p>4.13 Contributions</p>	<p>1. <i>Section 94(Ministers Discretion)</i></p> <p>If Council is to seek to collect contribution for the Local Traffic Plan then an argument needs to be put to the Minister.</p> <p>The following preliminary comment is provided: -</p> <ul style="list-style-type: none"> Development claiming exemption for local roads plan on the basis "Given that <i>Health Infrastructure NSW</i> will carry out local road improvements". There is no indication in the EIS or associated documents that there will be improvements to Council roads undertaken as part of the proposed redevelopment. At recent meetings there was a clear intention not to upgrade any part of Polwood Street. Section 4.6.4 of the EIS states the redevelopment will generate an

22 Tozer Street
West Kempsey

PO Box 3078
West Kempsey NSW 2440

Customer Service Tel: 02 6566 3200
Fax: 02 6566 3205

Library Tel: 02 6566 3210
Fax: 02 6566 3215



Kempsey

Shire Council

Your council Our community

www.kempsey.nsw.gov.au
ksc@kempsey.nsw.gov.au
ABN: 70 705 618 663


SUBJECT	COMMENTS
	<p>additional 50 vehicles per hour at the peak and a total of 250-300 vehicles per day.</p> <ul style="list-style-type: none"> The additional traffic generated by the redevelopment will not require by itself intersection upgrading, cycle paths, or the like on the associated Council roads. However they will add traffic and have a cumulative impact which will generate the need for upgrades. Therefore a contribution needs to be paid now to go toward future upgrading of the surrounding public roads.
	<p>2. <u>S64 Water and Sewer</u></p> <ul style="list-style-type: none"> The only indication of the type and size of the redevelopment is contained in the various supplementary reports appended to the EIS. The Water Directorate provides standard ET figures for hospitals at the current rate of for water 0.9et/bed and sewer 1.4et/bed. There will be an additional 13 beds but more importantly a total additional gross floor area of 3700 m². In any negotiations with Council and certainly in this part of the EIS the Crown needs to identify the use to which the additional gross floor area is to be put in the context of either replacing and or expansion of existing services.
4.15.2 Stormwater Management Strategy	<ul style="list-style-type: none"> There is no reference to the Enstruct's <i>Stormwater Management Plan (SMP)</i> at appendix K2; There is no mention in either the SMP or the EIS on water quality targets and means of achieving them, only that a gross pollutant capture system will be installed prior to discharge to the Macleay River which does not address water quality discharge from the site as a whole; There is no discussion anywhere in the EIS

22 Tozer Street
West Kempsey

PO Box 3078
West Kempsey NSW 2440

Customer Service Tel: 02 6566 3200
Fax: 02 6566 3205

Library Tel: 02 6566 3210
Fax: 02 6566 3215



Kempsey

Shire Council

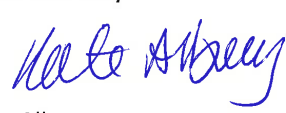
Your council Our community

www.kempsey.nsw.gov.au
ksc@kempsey.nsw.gov.au
ABN: 70 705 618 663

SUBJECT	COMMENTS
	<p>or supporting documentation on the location of the proposed new piped stormwater drain outlet at the Macleay River and the method to be deployed to reduce the outlet velocity in the pipe to prevent scour at the river bank;</p> <ul style="list-style-type: none"> There are no details on the effect on the existing environment (flora and fauna habitat) at the proposed pipe discharge point at the Macleay River;

If you have any questions please do not hesitate to contact me of 65663200.

Yours faithfully



Kate Alberry
MANAGER DEVELOPMENT ASSESSMENT
SUSTAINABLE ENVIRONMENT

22 Tozer Street
West Kempsey

PO Box 3078
West Kempsey NSW 2440

Customer Service Tel: 02 6566 3200
Fax: 02 6566 3205

Library Tel: 02 6566 3210
Fax: 02 6566 3215

All communications to be addressed to:

Headquarters
NSW Rural Fire Service
Locked Mail Bag 17
GRANVILLE NSW 2142

Telephone: (02) 8741 5175
e-mail: csc@rfs.nsw.gov.au

Headquarters
NSW Rural Fire Service
15 Carter Street
LIDCOMBE NSW 2127

Facsimile: (02) 8741 5433



Director General
NSW Planning & Infrastructure
GPO Box 39
SYDNEY NSW 2001

Your Ref: SSD 5363
Our Ref: S12/0014
DA13080688478 PC

Attention: Ms Megan Fu

29 August 2013

Dear Ms Fu,

**State Significant Development – SEPP (State and Regional Development) 2011
Kempsey District Hospital Redevelopment, River Street, West Kempsey
Lots 1 - 8 Section 20A DP 759080, Lots 1 & 14 Section 23A DP 759080 and Lot 20 DP
112084**

I refer to your letter dated 31 July 2013 seeking comments from the NSW Rural Fire Service regarding the above development proposal.

The NSW Rural Fire Service has reviewed the submitted environmental impact statement and accompanying documents and provides the following recommended conditions regarding bush fire protection measures for any approval granted to the proposed development:

1. The development proposal is to comply with the layout identified on the drawing titled Site Plan – Proposed, prepared by BVN Donovan Hill, reference Drawing No. 5027-BVN-AR-DWG-1DA-A01.

Asset Protection Zones

The intent of measures is to provide sufficient space for fire fighters and other emergency services personnel, ensuring radiant heat levels permit operations under critical conditions of radiant heat, smoke and embers, while supporting or evacuating occupants. To achieve this, the following conditions shall apply:

2. At the commencement of building works and in perpetuity the entire site of the proposed Hospital redevelopment shall be managed as an inner protection area (IPA) as outlined within section 4.1.3 and Appendix 5 of 'Planning for Bush Fire Protection 2006' and the NSW Rural Fire Service's document 'Standards for asset protection zones'.
3. Landscaping of the site shall comply with the principles of Appendix 5 of 'Planning for Bush Fire Protection 2006'.

Design and Construction

The intent of measures is that buildings are designed and constructed to withstand the potential impacts of bush fire attack. To achieve this, the following conditions shall apply:

4. The existing buildings located less than 100m from bushfire prone land on the western side of River Street (as shown in Figure 12 of the Bushfire Protection Assessment prepared by Australian Bushfire Protection Planners Pty Ltd, dated 20 April 2013) shall be upgraded for ember protection. This is to be achieved by enclosing all openings (excluding roof tile spaces) or covering openings with a non-corrosive metal screen mesh with a maximum aperture of 2mm. Where applicable, this includes any sub floor areas, openable windows, vents, weepholes and eaves. External doors are to be fitted with draft excluders.
5. Roofing of the mental health and maternity buildings shall be gutterless or guttering and valleys are to be screened to prevent the build up of flammable material. Any materials used shall be non-combustible.

Access

The intent of measures for internal roads is to provide safe operational access for emergency services personnel in suppressing a bush fire, while residents are accessing or egressing an area. To achieve this, the following conditions shall apply:

6. Internal roads shall comply with section 4.2.7 of 'Planning for Bush Fire Protection 2006'.

Water and Utilities

The intent of measures is to provide adequate services of water for the protection of buildings during and after the passage of a bush fire, and to locate gas and electricity so as not to contribute to the risk of fire to a building. To achieve this, the following conditions shall apply:

7. Water, electricity and gas are to comply with section 4.2.7 of 'Planning for Bush Fire Protection 2006'.

Evacuation and Emergency Management

The intent of measures is to provide suitable emergency and evacuation (and relocation) arrangements for occupants of special fire protection purpose developments. To achieve this, the following conditions shall apply:

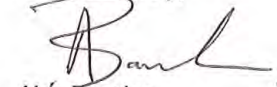
8. An emergency and evacuation plan addressing Section 4.2.7 of 'Planning for Bush Fire Protection 2006' shall be prepared for the hospital redevelopment, or the existing plan is to be amended to include considerations pertaining to the proposed redevelopment. A copy of the plan shall be provided to the consent authority prior to the issuing of an occupation certificate.

General Advice – consent authority to note

The NSW Rural Fire Service notes that certain site works (including the relocation of buildings) is proposed to be carried out under provisions of SEPP (Infrastructure) 2007. The advice provided in this letter does not relate to these proposed works. No advice or comment has been sought from, or provided by, the RFS with respect to the works proposed under SEPP (Infrastructure) 2007.

For any enquiries regarding this correspondence please contact Paul Creenaune on 1300 NSW RFS.

Yours sincerely,



Alan Bawden
Team Leader – Development Assessment and Planning

For information on *Planning for Bush Fire Protection 2006* visit the RFS web page www.rfs.nsw.gov.au



3 Marist Place
Parramatta NSW 2150

Locked Bag 5020
Parramatta NSW 2124
DX 8225 PARRAMATTA

Telephone: 61 2 9873 8500
Facsimile: 61 2 9873 8599

heritage@heritage.nsw.gov.au
www.heritage.nsw.gov.au

Contact: Lily Chu
Phone: (02) 9873 8595
Email: lily.chu@heritage.nsw.gov.au
Job no. A1653380
File no. 13/15029

Ms Megan Fu
Environmental Planning Officer
Major Development Assessment
NSW Department of Planning & Infrastructure
GPO Box 39
SYDNEY NSW 2001

Dear Megan

Exhibition of EIS for Kempsey District Hospital Redevelopment (SSD 5363)

Reference is made to your letter dated 31 July 2013 to the Office of Environment and Heritage inviting comments and advice on recommended conditions of consent on the above mentioned proposal that involves the demolition of the existing 1919 Blood Bank Building and construction of a new five storey hospital building. The Heritage Division of the Office of Environment and Heritage provides the following advice on behalf of the NSW Heritage Council.

It is noted that the development site is listed in Schedule 1 of the *Kempsey LEP 1987* and the Department of Health S170 register. The site is NOT listed on the State Register.

The Heritage Division raises no objection and supports the recommendations outlined in Section 7.2 of the Heritage Impact Statement by Graham Brooks and Associates (in relation to archaeological relics, archival recording, and interpretation). The Heritage Division recommends that the following condition be included:

- Where substantial intact archaeological relics of State or local significance are discovered during excavation, work must cease in the affected area and the Heritage Council must be notified in writing in accordance with section 146 of the *Heritage Act 1977*. Depending on the nature of the discovery, additional assessment may be required prior to the recommencement of excavation in the affected area.

Thank you for the opportunity to comment. Please contact Lily Chu at the Heritage Division should you wish to discuss any of the matters raised.

Yours faithfully



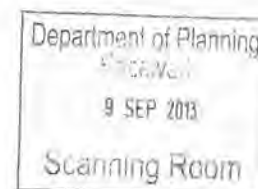
06/09/2013
Vincent Sicari
Conservation Manager, Heritage Division
Office of Environment & Heritage
AS DELEGATE OF THE NSW HERITAGE COUNCIL

[Helping the community conserve our heritage](http://www.heritage.nsw.gov.au)



Your reference: SSD 5363
Our reference: DOC13/40896
Contact: John Martindale 02 6659 8222

Ms Heather Warton
Director, Industry, Social Projects and Key Sites
Department of Planning and Infrastructure
GPO Box 39
SYDNEY NSW 2001



Att: Megan Fu, Development Assessments

Dear Ms Warton

Re: Proposed Kempsey District Hospital Redevelopment

I refer to your letter dated 31 July 2013 advising that an Environmental Impact Statement (EIS) relating to the above proposal has been placed on public exhibition as a major project and requesting comment and advice on recommended conditions of consent from the Office of Environment and Heritage (OEH). I appreciate the opportunity to provide input.

OEH has statutory responsibilities relating to threatened species, endangered populations and ecological communities, flooding, coastal floodplains and estuaries, Aboriginal cultural heritage and Historic heritage. OEH has reviewed the EIS and provides the following comments on these matters.

Threatened species, endangered populations and ecological communities

OEH notes that the proposed development is located within the existing hospital precinct and largely within areas covered by existing buildings. The precinct is surrounded by roads and existing residential development to the east and north, by a cemetery to the south and the Macleay River to the west. The only potential fauna habitat on site appears to comprise mainly exotic and planted species. OEH confirms that no endangered populations or ecological communities are present. Accordingly, OEH supports the conclusion in the EIS that the proposal is unlikely to adversely affect threatened species and their habitat, endangered populations or ecological communities and/or any wildlife corridors. No mitigation measures or conditions of consent are recommended by OEH.

Flooding, coastal floodplains and estuaries

OEH notes that the proposed location lies above Kempsey Shire Council's 1:100 year flood maps and some 9m above the probable maximum flood level identified in Council's Flood Risk Management Policy. The adjacent Macleay River is subject to negligible tides at this distance upstream and the proposed stormwater strategy and pollution controls, developed in accordance with water sensitive urban design criteria as per the Director General's Requirements, have the support of the Council. Consequently, OEH is of the view that the proposal is unlikely to have an adverse impact upon flooding, coastal floodplains or estuaries. No additional mitigation measures or conditions of consent are recommended by OEH.

Locked Bag 914, Coffs Harbour NSW 2450
Federation House Level 7, 24 Moonee Street,
Coffs Harbour NSW 2450
Tel: (02) 6651 5946 Fax: (02) 6651 6187
ABN 30 841 387 271
www.environment.nsw.gov.au

Page 2

Aboriginal Cultural Heritage

OEH acknowledges the significance of the local environment to the local Aboriginal community and the existence of numerous registered Aboriginal sites in the immediate locality. However, the site is highly disturbed and the field assessment undertaken on 17 January 2013 did not identify any Aboriginal cultural heritage values. Nonetheless, OEH supports the management strategies to be implemented in the event that any Aboriginal objects are subsequently discovered.

If Aboriginal objects are uncovered during the proposed development, the objects must be recorded and managed in accordance with the *National Parks and Wildlife Act 1974* (NPW Act) and avoided if possible. A suitably qualified cultural heritage specialist and representatives of the local Aboriginal community must be contacted to determine the nature, extent and significance of any finds. The site is to be registered in the OEH Aboriginal Heritage Information Management System (AHIMS) and the management outcome for the site included in the information provided to AHIMS. The proponent must consult with representatives of the local Aboriginal community, and a cultural specialist to develop and implement appropriate management strategies for all objects/sites.

Any management strategy must comply with appropriate legislative provisions. OEH advises that the requirements of the NPW Act have been amended recently and recommends familiarisation by the proponent with these amendments prior to work commencing. Provided the proposed management strategies listed in the EIS and OEH requirements above are included in appropriate consent conditions, OEH has no additional issues to raise with the Aboriginal cultural heritage assessment.

Historic Heritage

The application has been referred to the OEH Heritage Division for the provision of separate comment to your department in relation to historic heritage.

If you require further information or clarification please contact Mr John Martindale, Conservation Planning Officer, on telephone (02) 6659 8222.

Yours sincerely

Dimitri Young 5 September 2013

DIMITRI YOUNG
Senior Team Leader Planning - North East
Regional Operations



Heather Warton
Director, Industry, Social Projects and Key Sites
Department of Planning and Infrastructure
23-33 Bridge Street
Sydney NSW 2000

Attention: Megan Fu

Response to Kempsey District Hospital Redevelopment (SSD 5363)

Dear Ms Fu

Thank you for your email dated 31 July 2013 requesting for Transport for NSW (TfNSW) comments on the redevelopment proposal for Kempsey District Hospital.

TfNSW has reviewed the document *Transport and Accessibility Study, Kempsey District Hospital Redevelopment, dated 5 April 2013* prepared as part of the Final Schematic Design Report by GTA Consultants.

Consolidated comments from TfNSW and Roads and Maritime Services (RMS) are provided below:


- The key interests for TfNSW/RMS are the safety and efficiency of the road network, traffic management, the integrity of infrastructure assets and the integration of land use and transport.
- River Street (MR75) forms part of the classified road network. Kempsey Shire Council is the 'Roads Authority' for this road and all other roads in the area. RMS concurrence is required under S138 of the Roads Act 1993 for works within the road reserve of MR75. Consultation with the Council, as the roads authority, should be undertaken by the consent authority.
- The 'Transport & Accessibility Study' as prepared by GTA Consultants has 'generally' addressed the direct access and parking impacts of the proposal. However, TfNSW/RMS notes that the report has not adequately addressed the following issues:
 - The impacts of additional traffic generation on adjacent intersections - In particular, the identified increase in traffic movements at the River Street / Polwood Street intersection should be considered given that the existing intersection layout has no protection for vehicles turning right into Polwood Street from River Street. The proposed redevelopment will result in increased demand for this right-turn movement given the location of on-site parking and Ambulance access. TfNSW/RMS recommends that an appropriate intersection treatment be provided to safely accommodate the traffic movements generated by the development. RMS and Council need to be consulted by the proponent for technical input and comment on any option proposed.

18 Lee Street Chippendale NSW 2008
PO Box K659 Haymarket NSW 1240
T 8202 2200 F 8202 2209
www.transport.nsw.gov.au
ABN 18 804 239 602

- Pedestrian safety issues at the River Street/Polwood Street intersection - No suitable pedestrian facilities are provided to cross River Street and access to the hospital from the bus stop located on the western side of MR75, adjacent to the River Street/Polwood Street intersection. It is recommended that the applicant relocate the bus stop to the south of the newly proposed car park and pedestrian facility on River Street. This would improve pedestrian connectivity to public transport and pedestrian safety.
- Vehicle safety issues at the River Street/Polwood Street intersection - The existing bus stop arrangement on the western side of MR75, adjacent to the River Street/Polwood Street intersection, requires buses to stop adjacent to right turning traffic and blocking through traffic movements at this location. The recommended bus stop relocation to the south of the newly proposed car park and pedestrian facility on River Street would provide sufficient space to accommodate a suitable protected right-turn treatment at the River Street/Polwood Street intersection.
- Additionally, TfNSW/RMS advises that the use of pedestrian crossings internally to the site be carefully considered in relation to public liability against the hospital management. Official pedestrian facilities in road-related areas can be discussed with the Local Traffic Committee and must meet the applicable guidelines.
- Where external connections to the bicycle network are not available, consideration should be given to how the hospital may be better connected to this alternate transport mode.
- TfNSW/RMS recommends that a detailed Construction Traffic Management Plan be prepared to address traffic and safety related impacts arising from the construction process. This should address access points/adjacent intersections operation, parking, service vehicle access, bus movements, pedestrians and cyclist access.

Should you have any questions regarding this review, please contact Mark Ozinga on 8202 2198 or Mark.Ozinga@transport.nsw.gov.au

Yours sincerely,


Mark Ozinga
Manager, Land Use and Transport Planning
Planning and Programs

28/8/13

CD13/15149



Contact: Megan Fu
Phone: 02 9228 6531
Fax: 02 9228 6540
Email: megan.fu@planning.nsw.gov.au
Our ref: SSD 5363

Mr Sam Sangster
Chief Executive
Health Infrastructure
PO Box 1060
NORTH SYDNEY NSW 2059

Dear Mr Sangster

**Kempsey District Hospital Redevelopment (SSD 5363)
Response to Submissions**

The exhibition of the Environmental Impact Statement (EIS) for the above proposal ended on Thursday 5 September 2013. All submissions received by the department during the exhibition of the proposal are available on the department's website at:

http://majorprojects.planning.nsw.gov.au/index.pl?action=view_job&job_id=5363.

In accordance with clause 85A of the Environmental Planning and Assessment Regulation 2000, the Director-General requires Health Infrastructure to respond to the issues raised in these submissions. Particular consideration should be given to the issues raised in the combined Transport for NSW and RMS submission and Kempsey Shire Council submission. Please provide your response to submissions to the department as soon as possible.

The department has also undertaken a preliminary review of the EIS, in light of the submissions, and requests that the following matters be addressed:

- traffic impacts and the need for potential intersection upgrades or works;
- further details regarding pedestrian and vehicle movements across and around the site, including a pedestrian and vehicle circulation plan, and any further measures proposed to ensure pedestrian safety;
- potential relocation of bus stops to improve vehicular and pedestrian safety; and
- clarification regarding the extent of works forming part of the application (i.e. whether the proposed site works form part of the application) and, if so, incorporate details of the proposed works on the staging plan, relevant floor plan and landscaping plans.

Note that under clause 113(7) of the Environmental Planning and Assessment Regulation 2000, the days occurring between the date of this letter and the date on which you provide your response to submissions to the department are not included in the deemed refusal period.

Your contact officer, Megan Fu, can be contacted on 9228 6531 or via email at megan.fu@planning.nsw.gov.au. Please mark all correspondence to the attention of the contact officer.

Yours sincerely



Heather Warton
Director, Industry, Social Projects and Key Sites
as delegate for the Director General

13/9/13
Development Assessment Systems and Approvals 23-33 Bridge St Sydney NSW 2000
GPO Box 39 Sydney NSW 2001 Phone 02 9228 6111 Fax 02 9228 6455 Website planning.nsw.gov.au

APPENDIX B

Kempsey Shire Council Flood Information

Extract from “Annex A to the Kempsey Shire Local Flood Plan – The Nature Of Flooding In The Kempsey Shire”

The table highlights the irregularity of serious flooding on the lower Macleay River. Several bad floods may occur in a short period of time, as was the case in the periods 1863-75 and 1890-93, and 1949 and 1950 saw Kempsey’s worst two floods ever within eight months of each other. Equally, there may be long periods in which few if any serious floods are experienced (for example, between 1921 and 1949 and since 1967). The same irregularity applies for floods of lesser significance.

Since 1989, the **minor** flood level at Kempsey has been exceeded on only three occasions. These were in May 1996, February 1997 and July 1999. None of these floods reached the moderate flood level.

PEAK FLOOD HEIGHTS (IN METRES) AT GEORGES CREEK, BELLBROOK AND KEMPSEY, 1838-1999					
DATE	GEORGES CREEK	BELLBROOK	KEMPSEY (ROAD BRIDGE)	AEP (%) AT KEMPSEY (approx)	ARI (years)AT KEMPSEY (approx)
1838			6.7		
1841			6.7		
Aug 1848			5.8		
Feb 1863			6.1		
Feb 1864			7.4		
July 1864			6.3		
Aug 1864			7.3		
July 1866			6.3		
April 1867			6.7		
March 1870			6.3		
March 1875			7.6	2.5%	40
June 1879			6.1		
March 1890			6.1		
April 1892		8.8	6.1		
March 1893			6.7		
June 1893		17.1	7.5	3%	30
July 1921		16.16	7.32		
Feb 1928			5.56		
Feb 1929		12.20	6.25		
March 1946		12.73	5.99		
Aug 1949	14.10	17.22	7.92	1%	90
June 1950		18.06	7.77	2%	60
Aug 1952		13.03	6.02		
Feb 1954		11.23	5.79	20%	5
Nov 1959		9.75	5.59		
April 1962		8.15	5.54		
May 1963	13.50	15.54	7.14	7%	15
June 1967		10.24	6.02	16%	6
Jan 1968		8.84	5.77	20%	5
Jan 1974	8.63	7.70	5.56	25%	4
March 1974	6.78	7.11	5.69	22%	4.5
Feb 1976	8.64	7.56	5.54	25%	4
May 1977	7.60	6.75	5.56	25%	4
May 1980	7.00	7.14	5.73	20%	5
April 1989	6.72	6.34	5.57	25%	4

The lack of serious flooding in recent times, with no floods reaching even the 6.0 metre level since 1967, should not be taken as implying that the flood

Kempspey Flood Pamphlet

What should we do if we have to evacuate?

- Place moveable belongings on tables, beds and benches, with electrical items on top
- Gather up medicines, financial documents and personal items (including mementoes and photo albums) and take them with you
- If possible, check to see if your neighbours need help
- Listen to a local radio station for information
- Take three days' supply of clothes for each person
- Take your pets with you. AgricultureNSW will arrange for their care if necessary
- Turn off the electricity and gas
- Make your way to the evacuation centre: the SES will advise where this is
- Contact the SES on 132 500 if you need transport or other help
- When evacuating, avoid entering flood waters if possible. Driving and walking into floods are major causes of death during times of flooding



Keep this guide handy and refer to it when you hear a flood warning for the Macleay River

Where can I get more information?

The Kempsey Shire Local Flood Plan is available in all council libraries. It describes the arrangements that guide responses to flood emergencies in Kempsey.

The SES and Kempsey Shire Council can provide information on how to protect yourself and your property from floods.

FOR EMERGENCY HELP IN FLOODS
AND STORMS, CALL THE SES ON

132 500

Kempsey SES	6562 2228
Kempsey Shire Council (office hours)	6562 6077
Police	000
Ambulance	000

Better FloodSafe than Sorry!

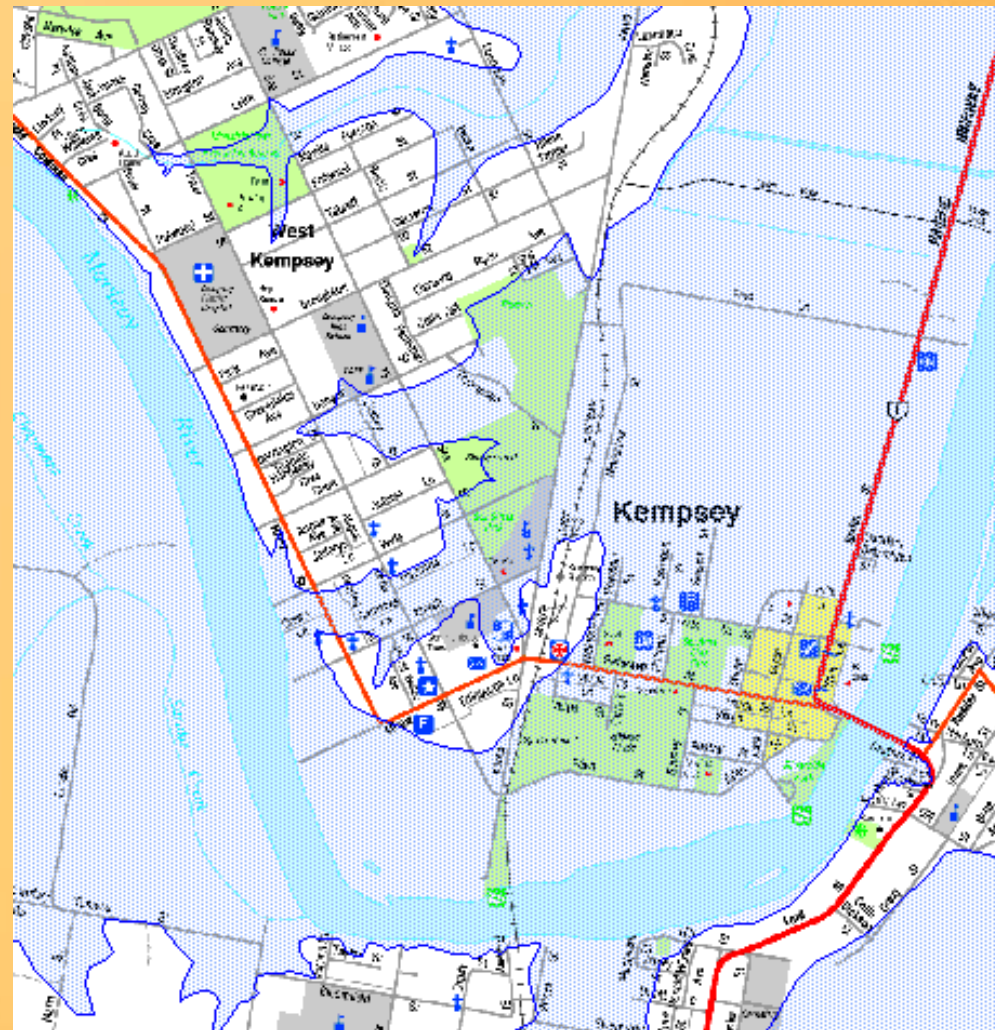
This brochure has been prepared for your protection by the State Emergency Service and Kempsey Shire Council.

FloodSafe in Central and West Kempsey



What is the flood problem here?

- Unfortunately, much of Kempsey is built on a floodplain and severe flooding is possible despite the levees
- A flood of 6.15 metres at the Kempsey Traffic Bridge gauge will cause water to flow over the Eden St levee and cause the CBD to fill rapidly
- Properties south of Eden St are threatened before water flows over the Eden St levee
- The 1949 flood, which reached 7.92 metres at the Traffic Bridge gauge, flooded all of central Kempsey and the much smaller 2001 flood almost did the same thing
- Low-lying parts of West Kempsey begin to be flooded at about 7.0 metres, which is about the peak height reached in March 2001
- More severe floods than the 1949 one are possible



Map showing the estimated extent of the 1% flood (similar to the 1949 flood)

What can we do to be ready?

- Prepare a family or business flood plan. This plan should indicate which items need to be raised or relocated and what you will need to do if you have to evacuate. The SES can provide information to help you prepare your plan
- Put your plan where you can be sure to find it when a flood is approaching

How will we know when a flood is coming?

- Bureau of Meteorology flood warnings will be broadcast over ABC Mid North Coast Radio, 2MC/ROX FM and TANK FM
- Listen to one of these stations for information. It is wise to have a battery powered radio and fresh batteries in case the power fails
- Flood warnings will give actual current heights at various upstream gauges and predicted heights at the Kempsey Traffic Bridge gauge. The SES will indicate the likely consequences at the predicted heights and advise appropriate actions for residents and business people

The key to being ready is recognising the danger and knowing what you can do to manage it.

This includes realising that evacuation may be necessary in the more severe floods

APPENDIX C

Revised Architectural Drawings and Schedule of Materials and Finishes

This page has been left intentionally blank

[illegible]

NOTE: THIS DRAWING SHALL BE
PRINTED AND READ IN COLOUR

CLIENT



PROJECT MANAGER



PROJECT
KEMPSEY DISTRICT HOSPITAL
REDEVELOPMENT - STAGE 1

SKM-S2E PROJECT NUMBER

NR11593

DRAWING KEY



TRUE NORTH

PROJECT NORTH

GRAPHIC SCALE (mm)

GRAPHIC SCALE (mm)

0 500 2000 4000

1:100mm

SCALE

1:500 @ A1 DO NOT SCALE

STATUS

FOR APPROVAL

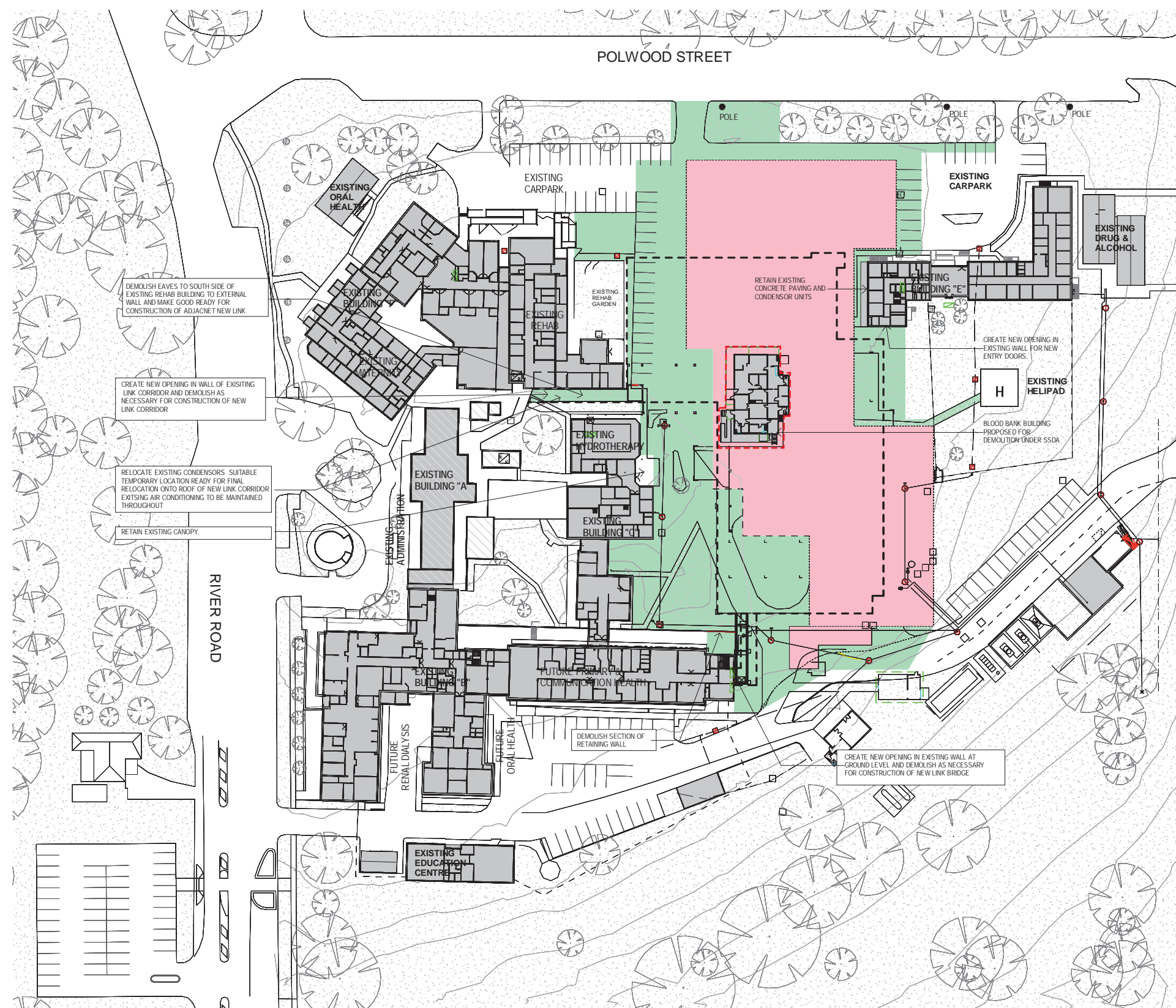
DRAWING
SITE PLAN - EXISTING &
DEMOLITION

5027-SKM-AR-DWG-
N1-B102

	IS
--	----

3

16/10/2013 3:21:12 PM



EXISTING BUILDING
(NOT IN SCOPE OF WORKS)

APPROVED DEMOLITION CARRIED OUT UNDER ISEPP

PROPOSED DEMOLITION OF EXISTING PAVEMENTS
AND SURFACES TO BE CARRIED OUT UNDER SSDA

NEW HOSPITAL
BUILDING OUTLINE

NOTE:
THIS PLAN SHOWS THE GENERAL EXTENT OF WORKS. THE
FULL SCOPE OF WORKS IS SHOWN ON THE RELEVANT
DOCUMENTATION FOR EACH DISCIPLINE

 PROPOSED DEMOLITION OF BLOOD BANK BUILDING TO BE CARRIED OUT UNDER SSDA

FOR PLANNING APPROVAL:
INTERNAL LAYOUT IS INDICATIVE AND
SUBJECT TO CHANGE

[illegible]

NOTE: THIS DRAWING SHALL BE
PRINTED AND READ IN COLOUR

CLIENT

 **NSW**
GOVERNMENT

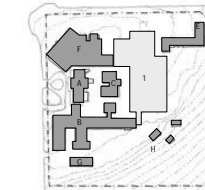
Health
infrastructure

PROJECT MANAGER

JOHNSTAFF
PROJECT
KEMPSEY DISTRICT HOSPITAL
REDEVELOPMENT - STAGE 1

SKM-S2F PROJECT NUMBER
NR11593

DRAWING KEY



TRUE NORTH PROJECT NORTH

GRAPHIC SCALE (mm)

0 2500 10000 20000

1:500mm

SCALE

1:500 © A1 DO NOT SCALE

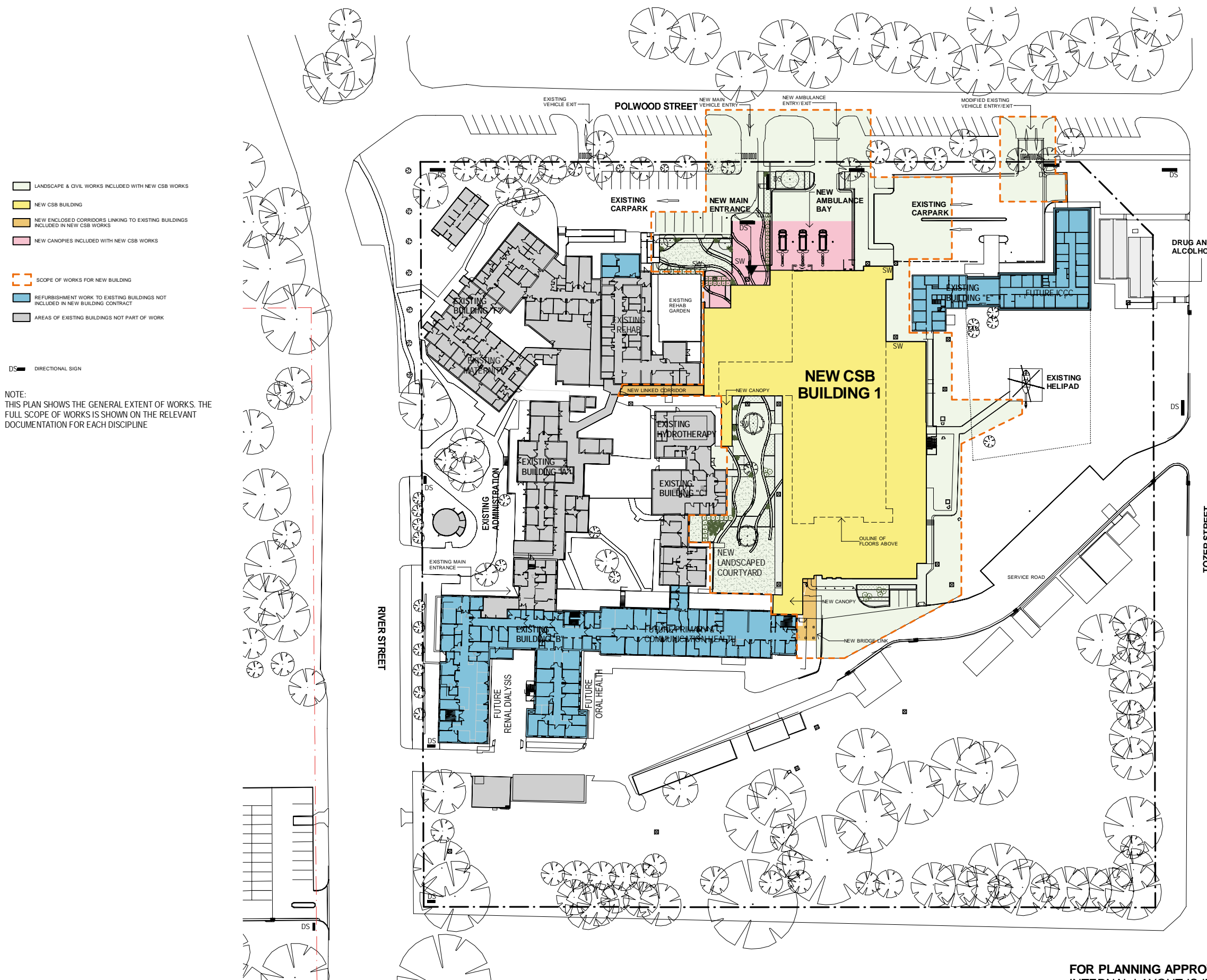
STATUS

FOR APPROVAL

DRAWING
SITE PLAN - PROPOSED

5027-SKM-AR-DWG- N1-B101	IS 6
-----------------------------	---------

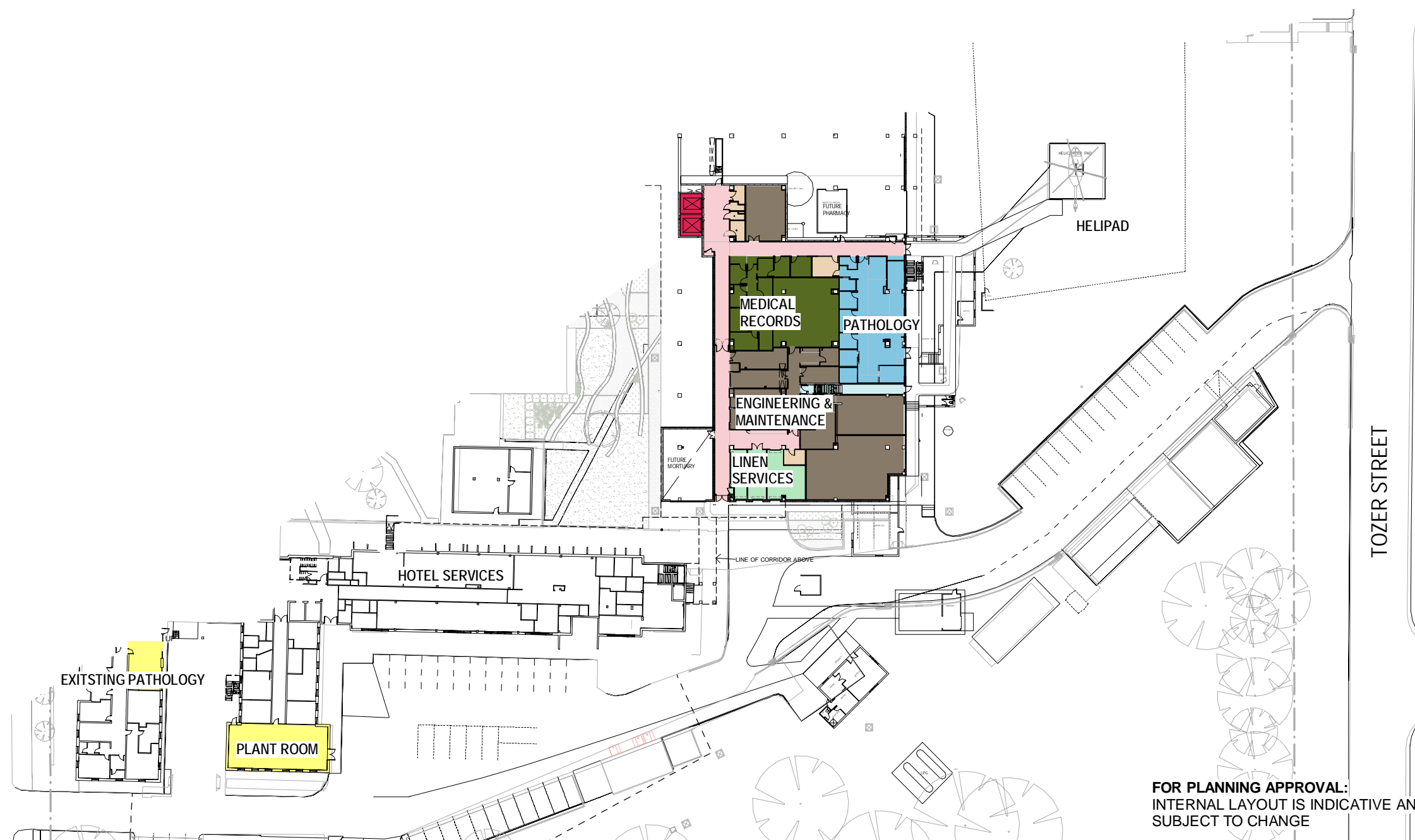
16/10/2013 1:47:40 PM



FOR PLANNING APPROVAL:
INTERNAL LAYOUT IS INDICATIVE AND
SUBJECT TO CHANGE

[illegible]

NOTE: THIS DRAWING SHALL BE
PRINTED AND READ IN COLOUR



FOR PLANNING APPROVAL:
INTERNAL LAYOUT IS INDICATIVE AND
SUBJECT TO CHANGE

CLIENT

 **NSW**
GOVERNMENT

 **Health**
Infrastructure

PROJECT MANAGER

JOHNSTAFF
PROJECT
KEMPSEY DISTRICT HOSPITAL
REDEVELOPMENT - STAGE 1

SKM-S2F PROJECT NUMBER
NB11593

DRAWING KEY



TRUE NORTH PROJECT NORTH

GRAPHIC SCALE (mm)

0 1750 7000 14000

1:250mm

SCALE

1:350	@ A1	DO NOT SCALE
STATUS		

FOR APPROVAL

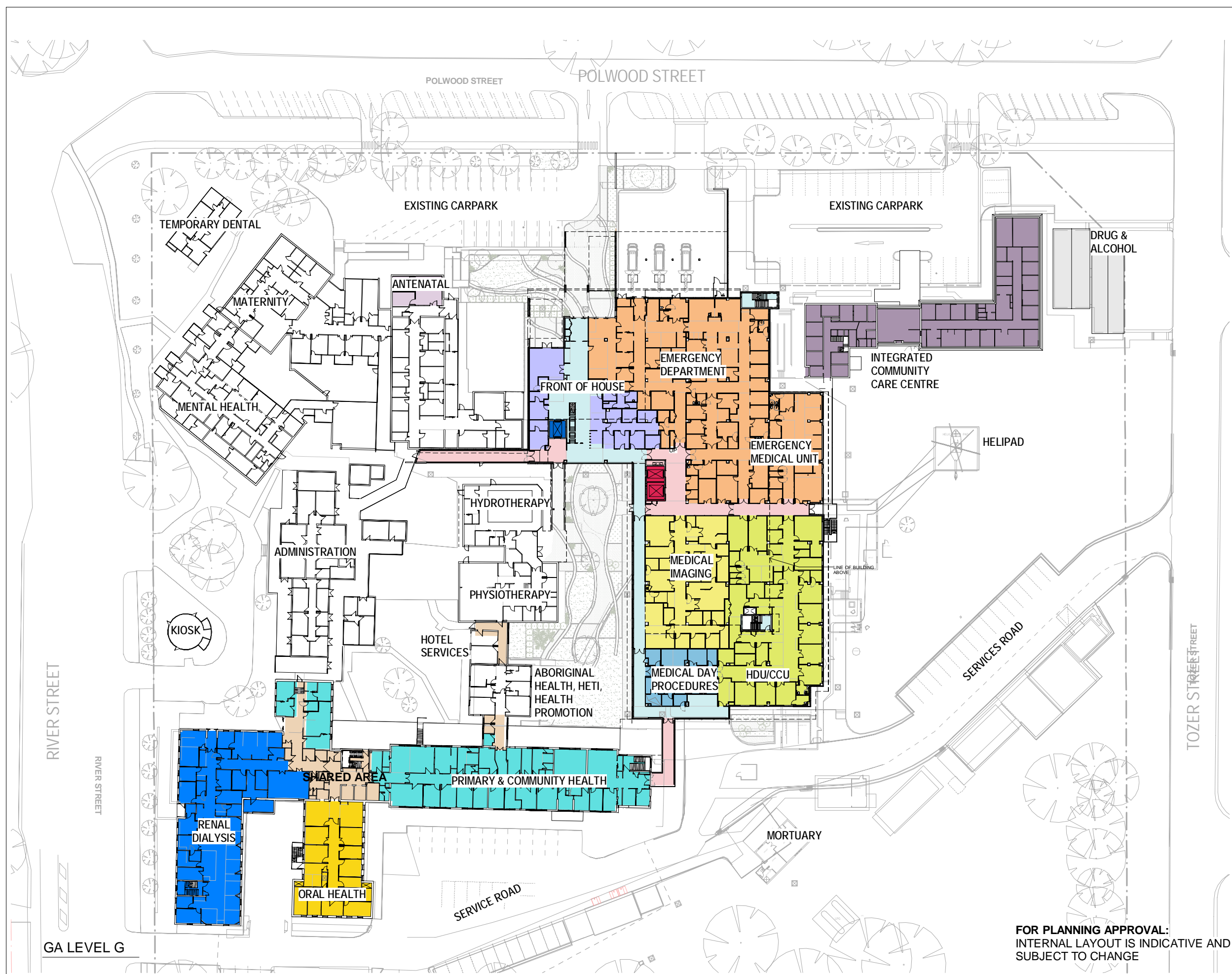
DRAWING

DEPARTMENTAL PLAN -
LOWER GROUND FLOOR

5027-SKM-AR-DWG- N1-Y201	IS 2
-----------------------------	---------

[illegible]

NOTE: THIS DRAWING SHALL BE
PRINTED AND READ IN COLOUR



[illegible]

NOTE: THIS DRAWING SHALL BE
PRINTED AND READ IN COLOUR

CLIENT

 **Health Infrastructure**

PROJECT MANAGER

JOHNSTAFF
PROJECT
KEMPSEY DISTRICT HOSPITAL
REDEVELOPMENT - STAGE 1

SKM-S2F PROJECT NUMBER

NB11593

DRAWING KEY



TRUE NORTH PROJECT NORTH

GRAPHIC SCALE (mm)

0 1750 7000 14000

1:350mm

SCALE

1:350 @ A1 DO NOT SCALE

STATUS

FOR APPROVAL

DRAWING
DEPARTMENTAL PLAN -
LEVEL 01

5027-SKM-AR-DWG- N1-Y203	ISSN 2
-----------------------------	-----------

16/10/2013 1:48:48 PM

FOR PLANNING APPROVAL:
INTERNAL LAYOUT IS INDICATIVE AND
SUBJECT TO CHANGE

[illegible]

NOTE: THIS DRAWING SHALL BE
PRINTED AND READ IN COLOUR

CLIENT

 **NSW** GOVERNMENT | **Health Infrastructure**

PROJECT MANAGER

JOHNSTAFF
PROJECT
KEMPSEY DISTRICT HOSPITAL
REDEVELOPMENT - STAGE 1

SKM-S2F PROJECT NUMBER
NB11593

DRAWING KEY



TRUE NORTH PROJECT NORTH

GRAPHIC SCALE (mm)

0 1750 7000 14000

1:350mm

SCALE

1:350 @ A1 DO NOT SCALE

STATUS

FOR APPROVAL

DRAWING
DEPARTMENTAL PLAN -
LEVEL 02

5027-SKM-AR-DWG- N1-Y204	IS 2
-----------------------------	---------

16/10/2013 1:48:51 PM

FOR PLANNING APPROVAL:
INTERNAL LAYOUT IS INDICATIVE AND
SUBJECT TO CHANGE

[illegible]



COPYRIGHT
The concepts and information contained in this document are the intellectual property of Sinclair Knight Merz. Use or copying of the document in whole or in part without the written permission of Sinclair Knight Merz constitutes an infringement of copyright.

NOTES:

1. DO NOT SCALE DRAWINGS. WRITTEN DIMENSIONS GOVERN.
2. ALL DIMENSIONS ARE IN MILLIMETERS UNLESS NOTED OTHERWISE.
3. THIS DRAWING MUST BE VERIFIED ON SITE BEFORE PROCEEDING WITH THE WORK. SSM SHALL BE NOTIFIED IN WRITING OF ANY DISCREPANCIES.
4. THIS DRAWING MUST BE READ IN CONJUNCTION WITH ALL RELATED CONTRACTS, SPECIFICATIONS, REPORTS AND DRAWINGS.

THIS DRAWING IS AN UNCONTROLLED COPY UNLESS NOTED OTHERWISE.

[illegible]

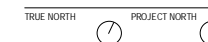
COMMENT:
REFER TO FINISHES AND MATERIALS SCHEDULE
[5027-SKM-AR-SCH-N1-V401]
FOR ELEVATION TAGS LEGEND



JOHNSTAFF
PROJECT
KEMPSEY DISTRICT HOSPITAL
REDEVELOPMENT - STAGE 1

SKM-S2F PROJECT NUMBER
NB11593

DRAWING KEY



GRAPHIC SCALE (mm)

0 500 2000 4000

1:100mm

SCALE

1:100 @ A1 DO NOT SCALE

FOR APPROVAL

DRAWING
NORTH ELEVATION

5027-SKM-AR-DWG- N1-E301	ISS 7
-----------------------------	----------

16/10/2013 1:47:56 PM

FOR PLANNING APPROVAL:
INTERNAL LAYOUT IS INDICATIVE AND
SUBJECT TO CHANGE

[illegible]

COMMENT:
REFER TO FINISHES AND MATERIALS SCHEDULE
[5027-SKM-AR-SCH-N1-V401]
FOR ELEVATION TAGS / LEGEND

CLIENT

 **NSW**
GOVERNMENT

 **Health**
Infrastructure

PROJECT MANAGER

JOHNSTAFF
PROJECT
KEMPSEY DISTRICT HOSPITAL
REDEVELOPMENT - STAGE 1

SKM-S2F PROJECT NUMBER
NB11593

TRUE NORTH PROJECT NORTH

GRAPHIC SCALE (mm)

A horizontal line with vertical tick marks at 0, 500, 2000, and 4000. The numbers are placed below the line.

SCALE

1:100 @ A1 DO NOT SCALE

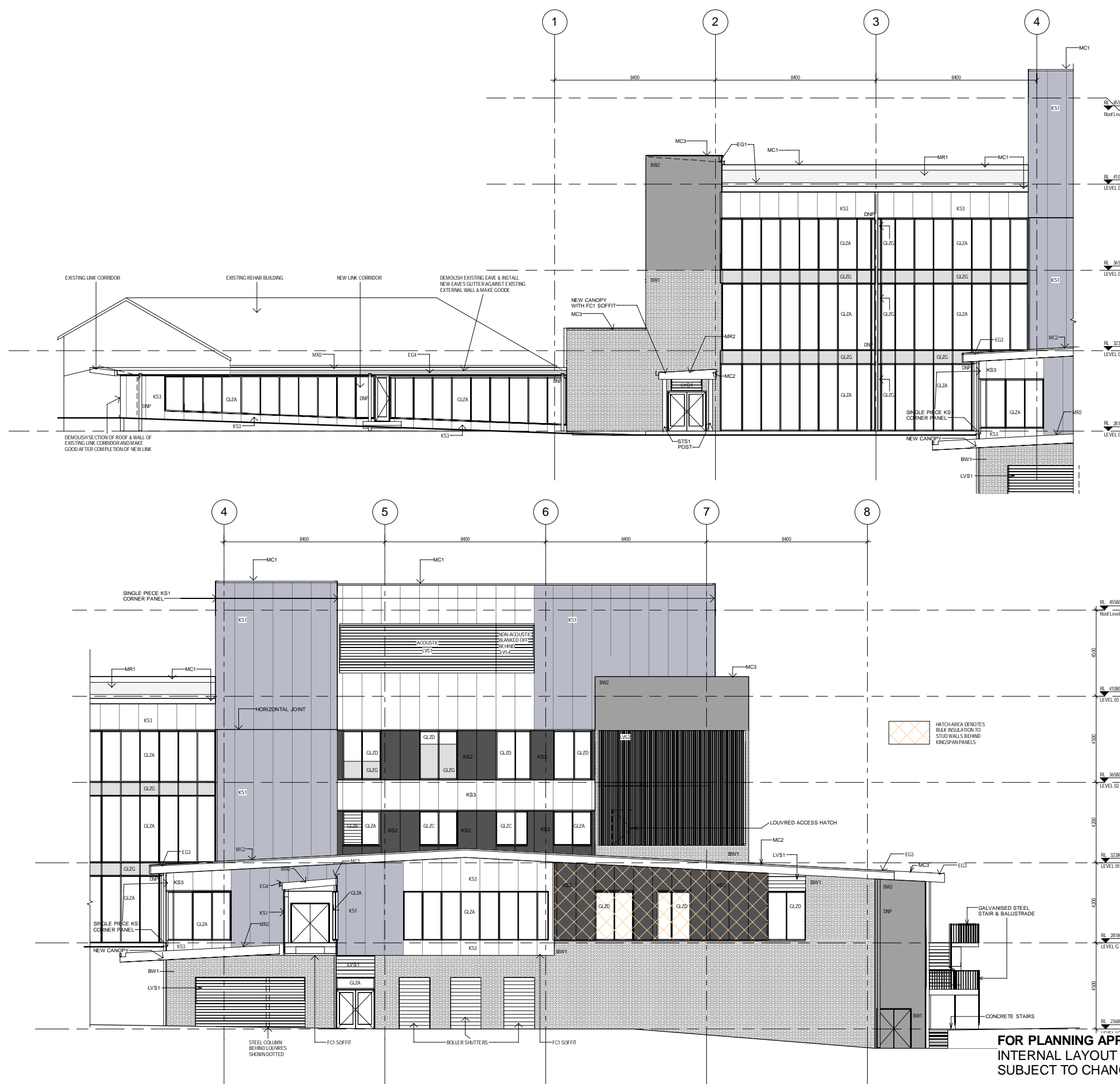
FOR APPROVAL

DRAWING
SOUTH ELEVATION

5027-SKM-AR-DWG- N1-F302	IS 7
-----------------------------	---------

14/10/2012 1:49:00 PM

PART 2



[illegible]

COMMENT:
REFER TO FINISHES AND MATERIALS SCHEDULE
[5027-SKM-AR-SCH-N1-V401]
FOR ELEVATION TAGS / LEGEND

CLIENT

 **NSW**
GOVERNMENT

 **Health**
Infrastructure

PROJECT MANAGER

JOHNSTAFF
PROJECT
KEMPSEY DISTRICT HOSPITAL
REDEVELOPMENT - STAGE 1

SKM-S2F PROJECT NUMBER
NB11593

DRAWING KEY

DRAWING KEY



TRUE NORTH PROJECT NORTH

GRAPHIC SCALE (mm)

0 500 2000 4000

1:100mm

SCALE

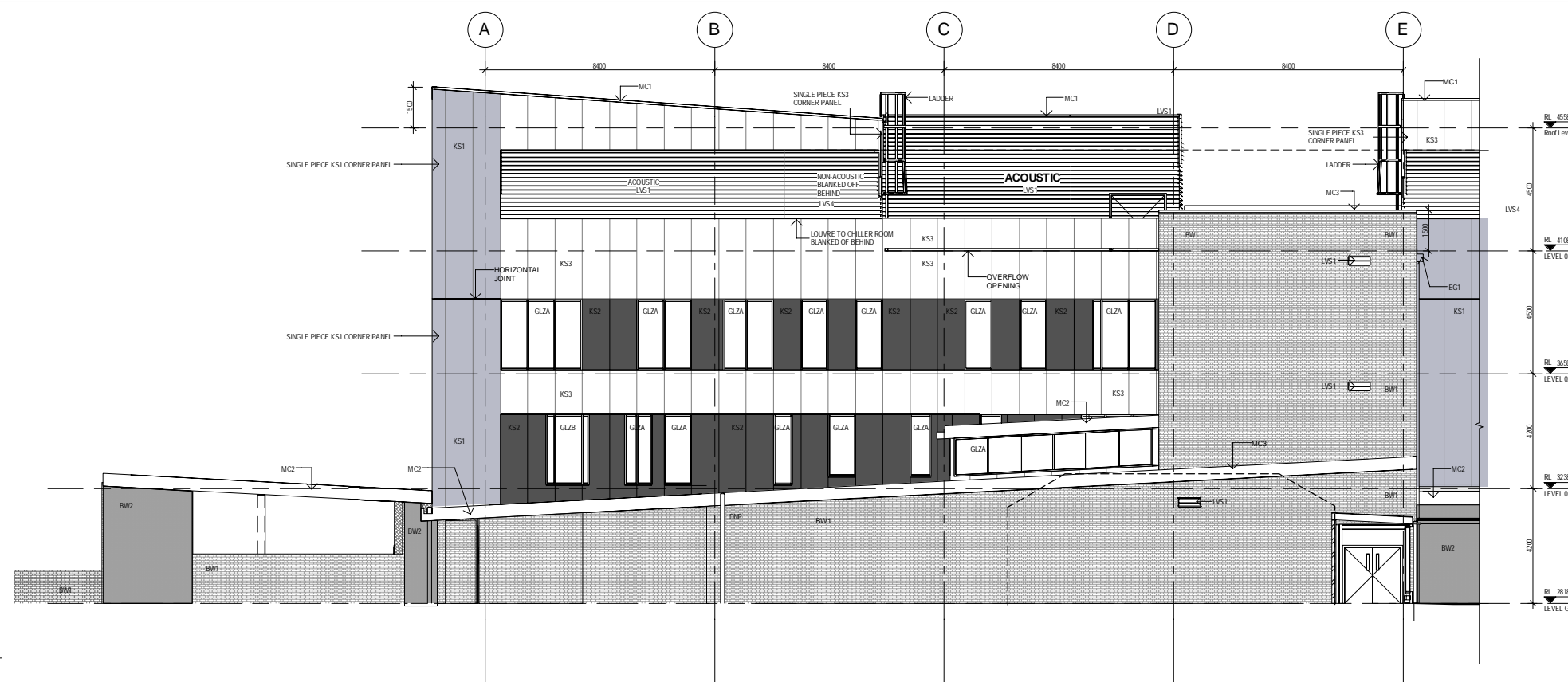
1:100 @ A1 DO NOT SCALE

FOR APPROVAL

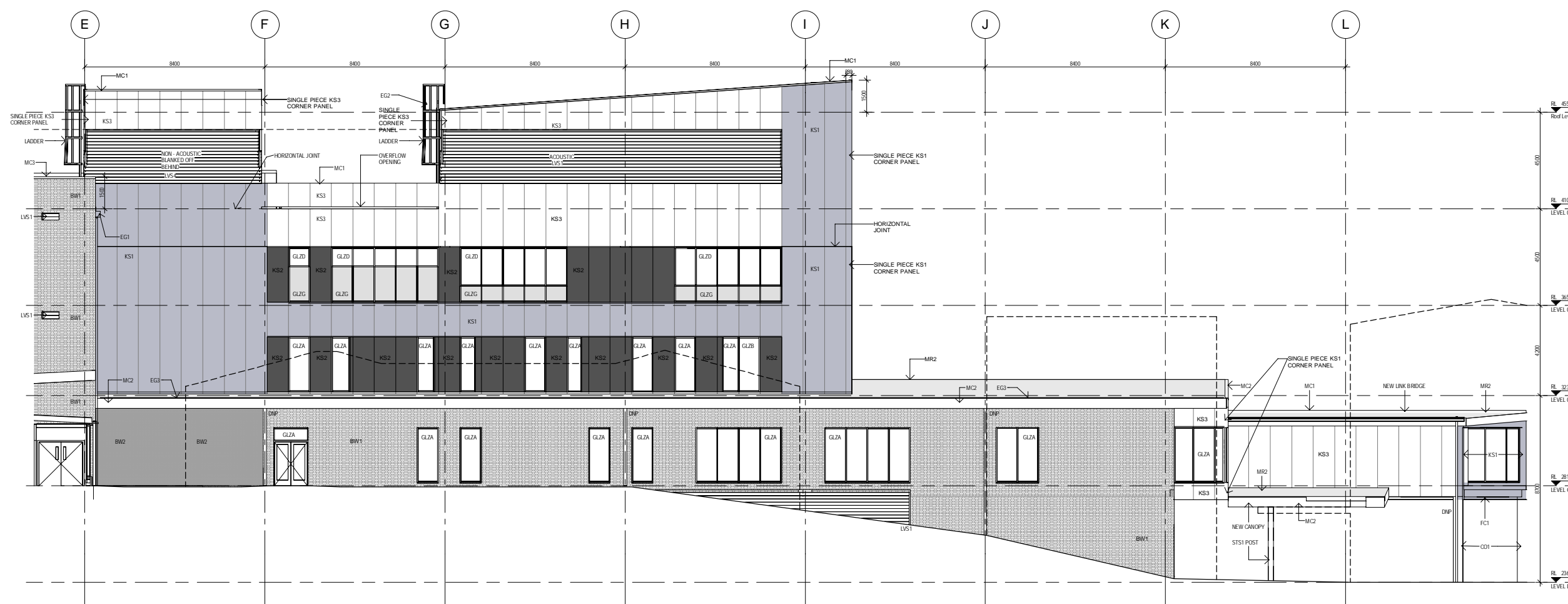
DRAWING
WEST ELEVATION

5027-SKM-AR-DWG-
N1-E304

14/10/2013 1:48:12 PM

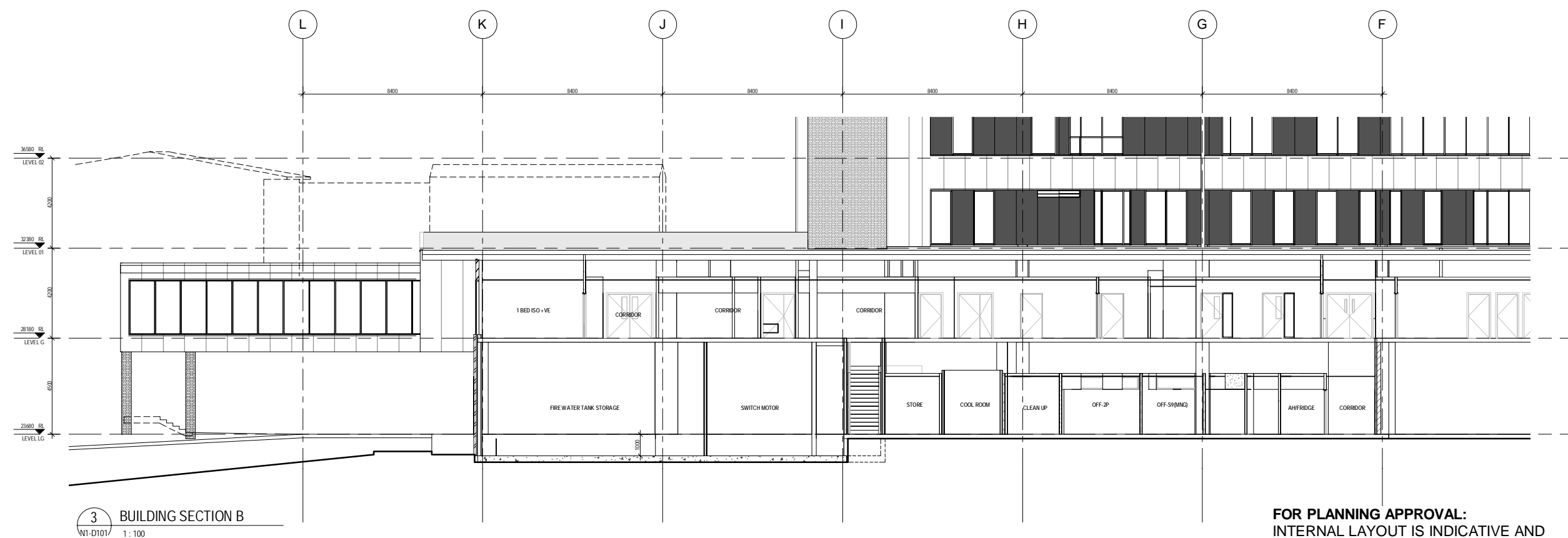
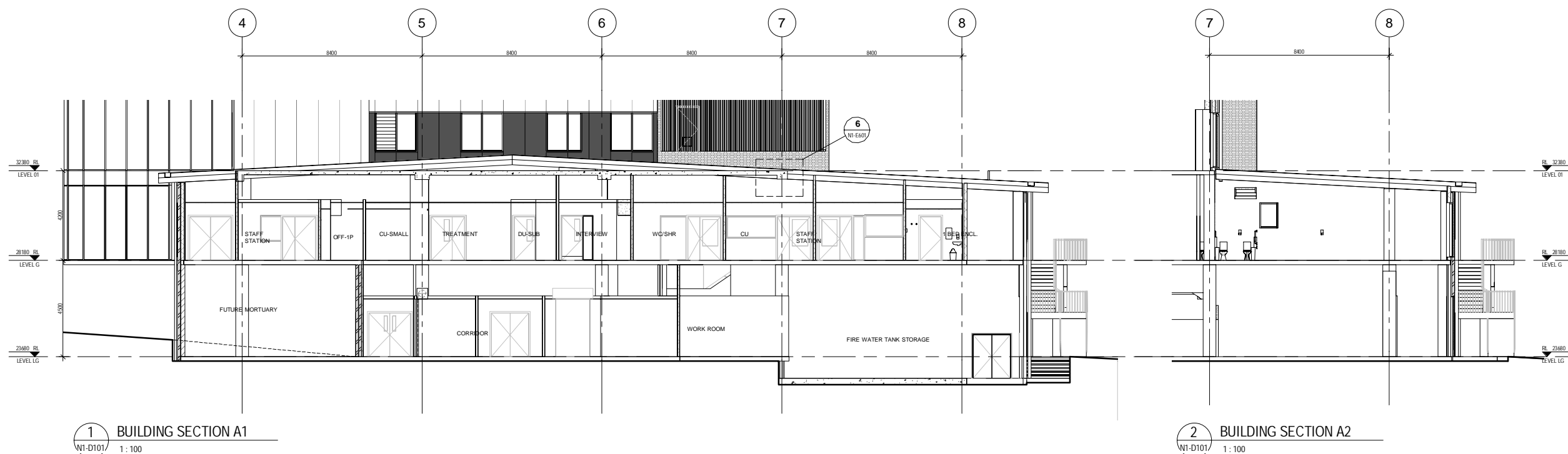


PART 1



FOR PLANNING APPROVAL:
INTERNAL LAYOUT IS INDICATIVE AND
SUBJECT TO CHANGE

PART 2

[illegible]

FOR PLANNING APPROVAL:
INTERNAL LAYOUT IS INDICATIVE AND
SUBJECT TO CHANGE



COPYRIGHT
The concepts and information contained in this document are the intellectual property of Sinclair Knight Merz. Use or copying of the document in whole or in part without the written permission of Sinclair Knight Merz constitutes an infringement of copyright.

NOTES:

1. DO NOT SCALE DRAWINGS. WRITTEN DIMENSIONS GOVERN.
2. ALL DIMENSIONS ARE IN MILLIMETRES UNLESS NOTED OTHERWISE.
3. ALL DIMENSIONS SHALL BE TYPED ON THE DRAWING PROCEDURE WITH THE WORK. SHM SHALL BE NOTIFIED IN WRITING OF ANY DISCREPANCIES.
4. THIS DRAWING MUST BE READ IN CONJUNCTION WITH ALL RELEVANT CONTRACT & SPECIFICATIONS, REPORTS & DRAWINGS.

THIS DRAWING IS AN UNCONTROLLED COPY. UNLESS NOTED OTHERWISE, IT IS NOT VALID FOR CONSTRUCTION.

[illegible]

CLIENT

 **NSW** GOVERNMENT | **Health Infrastructure**

PROJECT MANAGER

JOHNSTAFF
PROJECT
KEMPSEY DISTRICT HOSPITAL
REDEVELOPMENT - STAGE 1

SKM-SZF PROJECT NUMBER
NB11593

DRAWING KEY



TRUE NORTH PROJECT NORTH

GRAPHIC SCALE (mm)

0 500 2000 4000

1:100mm

SCALE

1:100 @ A1 DO NOT SCALE

STATUS

FOR APPROVAL

DRAWING SECTIONS

5027-SKM-AR-DWG- N1-E402	ISS 4
-----------------------------	----------

16/10/2013 1:48:26 PM

FOR PLANNING APPROVAL:
INTERNAL LAYOUT IS INDICATIVE AND
SUBJECT TO CHANGE

Schedule of External Materials and Finishes

CODE	ITEM	PRODUCT	COLOUR	FINISH
EXTERIOR				
ROOFING				
AP130	Anchor Point - Roof.	Refer to Performance and Technical Specifications.	-	To Manufacturers specification.
DC1	Drainage Cell.	Refer to Performance and Technical Specifications.	-	-
DP1	Downpipe - 150 ø.	Colorbond Ultra.	'Citi Pearl'.	Colorbond Ultra.
DP2	Downpipe - 100 ø.	Colorbond Ultra.	'Citi Pearl'.	Colorbond Ultra.
DP3	PVC Downpipe - 100ø.	PVC.	-	PVC.
EG1	Eaves Gutter 1 - Level 3 (Periop) roof.	Colorbond Ultra.	'Citi Pearl'.	Colorbond Ultra.
EG2	Eaves gutter 2 - Level 4 (plant room) roofs.	Colorbond Ultra.	'Citi Pearl'.	Colorbond Ultra.
EG3	Eaves gutter 3 - Level 1 (ED) roofs.	Colorbond Ultra.	Fascia: 'Whitehaven' Internal: 'Dune'.	Colorbond Ultra.
EG4	Eaves gutter 4 - Link corridor roofs.	Colorbond Ultra.	'Citi Pearl'.	Colorbond Ultra.
FC1	Painted set fibre cement soffit lining.	Refer to Performance and Technical Specifications.	Dulux 'Natural White' PN1E1.	Matt.
GW1	Roof walkway.	Walkway system.	Hot dipped galvanised.	To Manufacturers specification.
INS1	Roof insulation.	Glasswool Blanket. R3.2	Glasswool.	-
INS2	Roof insulation.	Glasswool Blanket. R3.7	Glasswool.	-
INS3	Roof insulation.	Glasswool Blanket. R1.8	Glasswool.	-
INS4	Concrete soffit insulation.	80mm R3.0. K10 FM soffit board.	Glasswool.	-
INS5	Concrete soffit insulation.	R3.7 soffit board.	Thermoset Insulation.	-

CODE	ITEM	PRODUCT	COLOUR	FINISH
INS6	Concrete soffit insulation.	40mm R1.9. K10 FM soffit board.	Thermoset Insulation.	-
LD01	Step ladder.	Custom.	Hot dipped galvanised.	Galvanised.
LD400	Roof access ladder.	Modular Ladder.	Hot dipped galvanised.	To Manufacturers specification.
LD402	Roof access ladder Bracket.	Ladder support bracket.	Hot dipped galvanised.	To Manufacturers specification.
MC1	Parapet Cappings and Flashings.	Colorbond Ultra.	'Citi Pearl'	Colorbond Ultra.
MC2	Parapet Cappings and Flashings.	Colorbond Ultra.	'Whitehaven'	Colorbond Ultra.
MC3	Parapet Cappings and Flashings.	Colorbond Ultra.	'Dune'	Colorbond Ultra.
MR1	Sheet Metal Roofing.	Kliplock 406.	'Whitehaven'	Colorbond Ultra.
MR2	Sheet Metal Roofing.	Kliplock 406.	'Dune'	Colorbond Ultra.
OF1	Overflow Slot.	Custom.	To match cladding.	Colorbond Ultra.
RWH1	Rain Water Head 1.	Colorbond Ultra.	'Citi Pearl'	Colorbond Ultra.
SL1	Roof Safety Line.	Static Line.	-	To Manufacturers specification.
SP1	Spreader from Downpipe.	Powdercoated metal.	'Citi Pearl'	Colorbond Ultra.
WMR1	Waterproof Membrane Roof.	Refer to Performance and Technical Specifications.	-	-

CODE	ITEM	PRODUCT	COLOUR	FINISH
WALLS & FENESTRATION				
BW1	Face Brickwork: Flemish Bond.	Boral 'Escura' extruded brick.	'Volcanic Velour'.	Flemish Bond.
BW2	Architectural Patterned Brickwork: Flemish Bond with projecting perpend.	Boral 'Escura' extruded brick with Boral 'Escura' drypressed projecting perpend.	'Volcanic Velour' (extruded), with 'Silver Shadow' (drypressed) projecting perpend.	Flemish Bond with projecting perpend.
EXD	Exterior Door Frames & Leaves.	Painted steel frame as specified.	Dulux 'Namadji' PG1F8/PN2E9.	Semi Gloss.
GLZA	Typical Window Frame with fixed Double-Glazed IGU.	IGU: 6mm performance glass, 12mm air gap, 10mm toughened clear glass Frame: 150 deep powdercoated extruded aluminium frame.	Glass: Clear with minimum grey tint required to achieve required thermal performance Frame: Dulux 'Eternity Charcoal Pearl' Satin 900-88394.	Frame: Satin
GLZB	Typical Window Frame with Openable Glass Louvres.	Glass: 10mm clear toughened glass Frame: 150 deep powdercoated extruded aluminium frame.	Glass: Colour to match GLZA Frame: Dulux 'Eternity Charcoal Pearl' Satin 900-88394.	Frame: Satin
GLZC	Typical Window Frame with fixed Double-Glazed IGU, with laminated inner pane.	IGU: 6mm performance glass, 12mm air gap, 10.38mm laminated clear glass. Frame: 150 deep powdercoated extruded aluminium frame.	Glass: Clear with minimum grey tint required to achieve required thermal performance Frame: Dulux 'Eternity Charcoal Pearl' Satin 900-88394.	Frame: Satin
GLZD	Typical Window Frame with fixed double-glazed IGU, plus additional single glazed Jockey Sash to interior with interstitial venetian blind.	IGU: 6mm performance glass, 12mm air gap, 10mm clear glass Jockey Sash: 6mm toughened clear glass Frame: 150 deep powdercoated extruded aluminium frame.	Glass: Clear with minimum grey tint required to achieve required thermal performance Frame: Dulux 'Eternity Charcoal Pearl' Satin 900-88394.	Frame: Satin

CODE	ITEM	PRODUCT	COLOUR	FINISH
GLZE	Deep Void Window Frame with fixed double-glazed IGU, plus single glazed Jockey Sash to interior with interstitial venetian blind.	IGU: 6mm performance glass, 12mm air gap, 10.38mm laminated clear glass. Jockey Sash: 6mm toughened clear glass Frame: powdercoated extruded aluminium frame. Minimum 100mm deep void.	Glass: Clear with minimum grey tint required to achieve required thermal performance Frame: Dulux 'Eternity Charcoal Pearl' Satin 900-88394.	Frame: Satin
GLZF	Frameless Glazing.	Glass: 10mm toughened glass, all exposed edges arressed and polished. Frame: Fully recessed powdercoated channel.	Glass: Clear Fully recessed channel: Dulux 'Eternity Charcoal Pearl' Satin 900-88394.	-
GLZG	Typical Window Frame with colourback Opaque Spandrel Glass.	Glass: 6mm colourback glass Frame: 150 deep powdercoated extruded aluminium frame.	Glass: Charcoal Frame: Dulux 'Eternity Charcoal Pearl' Satin 900-88394.	Frame: Satin
GLZM	Clear Laminated Frameless Glazing with artwork interlayer.	Glass: 10mm toughened glass, all exposed edges arressed and polished. Frame: Fully recessed powdercoated channel.	Glass: Clear with full colour artwork interlayer. Fully recessed channel: Dulux 'Eternity Charcoal Pearl' Satin 900-88394.	-
KS1	Kingspan Architectural Wall Panelling.	Kingspan KS1000 AWP - FM Wall Panel System - 100mm thick panels.	'Silver Coin'	Profile: Mini-micro
KS2	Kingspan Architectural Wall Panelling.	Kingspan KS1000 AWP - FM Wall Panel System - 80mm thick panels.	'Ebony'	Profile: Mini-micro
KS3	Kingspan Architectural Wall Panelling.	Kingspan KS1000 AWP - FM Wall Panel System - 80mm thick panels.	'Silver Coin'	Profile: Mini-micro
INS7	Wall Insulation.	75mm R2.0 Gold wall batts or equal (18kg).	Glasswool.	-

CODE	ITEM	PRODUCT	COLOUR	FINISH
INS8	Concrete Wall Insulation.	50mm . R2.5. K10 FM soffit board.	Thermoset Insulation.	-
LVS1	Horizontal Acoustic Louvres.	Double stage louvres - Deep section.	Dulux 'Eternity Charcoal Pearl' Satin 900-88394.	Dulux Duratec X15 powdercoat.
LVS2	Vertical Louvres.	200 x 50mm powdercoated aluminium box sections.	Dulux 'Eternity Citi Silver Pearl' Matt 9007024Q.	Dulux Duratec X15 powdercoat.
LVS3	Horizontal Non-acoustic Louvres.	Single stage louvre - Deep section.	Dulux 'Eternity Citi Silver Pearl' Matt 9007024Q.	Dulux Duratec X15 powdercoat.
LVS4	Horizontal Louvres blanked-off behind.	Single stage louvre - Deep section.	Dulux 'Eternity Citi Silver Pearl' Matt 9007024Q.	Dulux Duratec X15 powdercoat.
STS1	Exterior Exposed Structural Steel.	Dulux.	Dulux 'Namadji' PG1F8 / PN2E9.	Semi Gloss.
SCF	Steel columns - Interior Fire Rated, Exposed.	International Paints Intumescent paint 212, Interthane 870 top coat.	Dulux 'Namadji' PG1F8 / PN2E9.	Interthane 870 top coat.
SCF2	Steel columns - Interior Fire Rated, Hidden.	Intumescent paint.	-	-
WPM1	Water Proof Membrane 1.	Refer to Performance and Technical Specifications.	-	-
WPM2	Water Proof Membrane 2.	Refer to Performance and Technical Specifications.	-	-
WPM3	Water Proof Membrane 3.	Refer to Performance and Technical Specifications.	-	-

APPENDIX D

Revised Landscape Drawings

1:150



PLANT SCHEDULE							
Symbol	Botanical Name	Common Name	Mature Height (m.)	Mature Spread (m.)	Quantity	Spacings	Pot Size
	Trees						
Ba	<i>Brachychiton acerifolium</i>	Illawarra Flame Tree	25	5	2	As Shown	200L
Cc	<i>Cyathea cooperi</i>	Australian Tree Fern	5	2	3	As Shown	75L
Jm	<i>Jacaranda mimosifolia</i>	Jacaranda	10	6	2	As Shown	200L
MLG	<i>Magnolia grandiflora</i> 'Little Gem'	Little Gem Magnolia	5	3	3	As Shown	75L
TL	<i>Tristanopsis laurina</i> 'Luscious'	Water Gum	9	8	1	As Shown	75L
	Shrubs						
CLJ	Callistemon 'Little John'	Little John Callistemon	1.5	1	3	As Shown	300mm
SAB	Syzygium 'Aussie Boomer'	Aussie Boomer Lilly Pilly	1.5	1.5	28	As Shown	300mm
Vo	<i>Viburnum odoratissimum</i>	Sweet Viburnum	5	3.5	5	As Shown	300mm
	Accents						
Aa	<i>Asplenium australasicum</i>	Birds Nest Fern	1.5	1.5	44	As Shown	300mm
Ac	<i>Alpinia caerulea</i>	Native Ginger	2	1	67	As Shown	300mm
De	<i>Doryanthus excelsa</i>	Gymea Lily	2	2	25	As Shown	300mm
	Groundcovers / Grasses						
Dc	<i>Dianella caerulea</i> 'Breeze'	Blue Flax Lily	0.4	0.4	96	5.00/m2	150mm
Cg	<i>Carpobrotus glaucescens</i>	Pigface	0.1	1	182	5.00/m2	150mm
Hs	<i>Hibbertia scandens</i>	Glimming Guinea Flower	0.15	1	323	5.00/m2	150mm
LT	<i>Lomandra longifolia</i> 'Tanika'	Matt Rush Grass	0.7	0.6	811	5.00/m2	150mm
Tj	<i>Poa labillardieri</i>	Tussock Grass	0.8	0.5	242	5.00/m2	150mm
PI	<i>Trachelospermum jasminoides</i>	Star Jasmine	0.2	0.5	318	5.00/m2	150mm
Vh	<i>Viola hederacea</i>	Native Violet	0.1	0.3	38	5.00/m2	150mm

[illegible]

ISSUE	DATE	FOR
1	05/07/2013	50% Design Development
2	23/08/2013	Tender
3	06/09/2013	Final Tender

[illegible]

CLIENT



PROJECT MANAGER

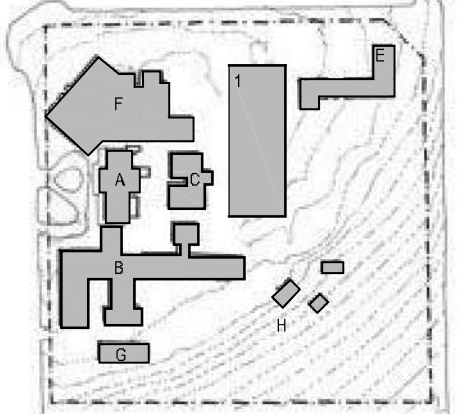


JOHNSTAFF
PROJECT
KEMPSEY DISTRICT HOSPITAL
REDEVELOPMENT - STAGE 1

SITE IMAGE PROJECT NUMBER

SS13-2613

DRAWING KEY



TRUE NORTH

SUBJECT NORTH

GRAPHIC SCALE (mm)

0 1000 3000

1:150mm

SCALE

1:150 @ A1 DO NOT SCALE

STATUS

TENDER

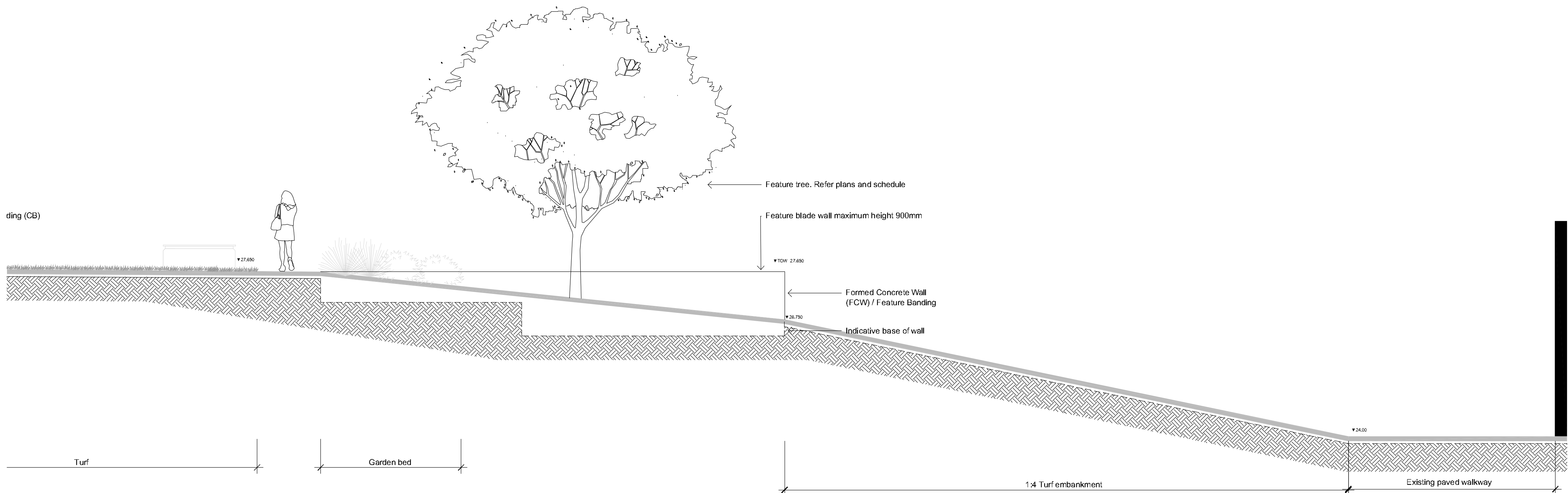
DRAWING

Landscape Plan -
Entry Area

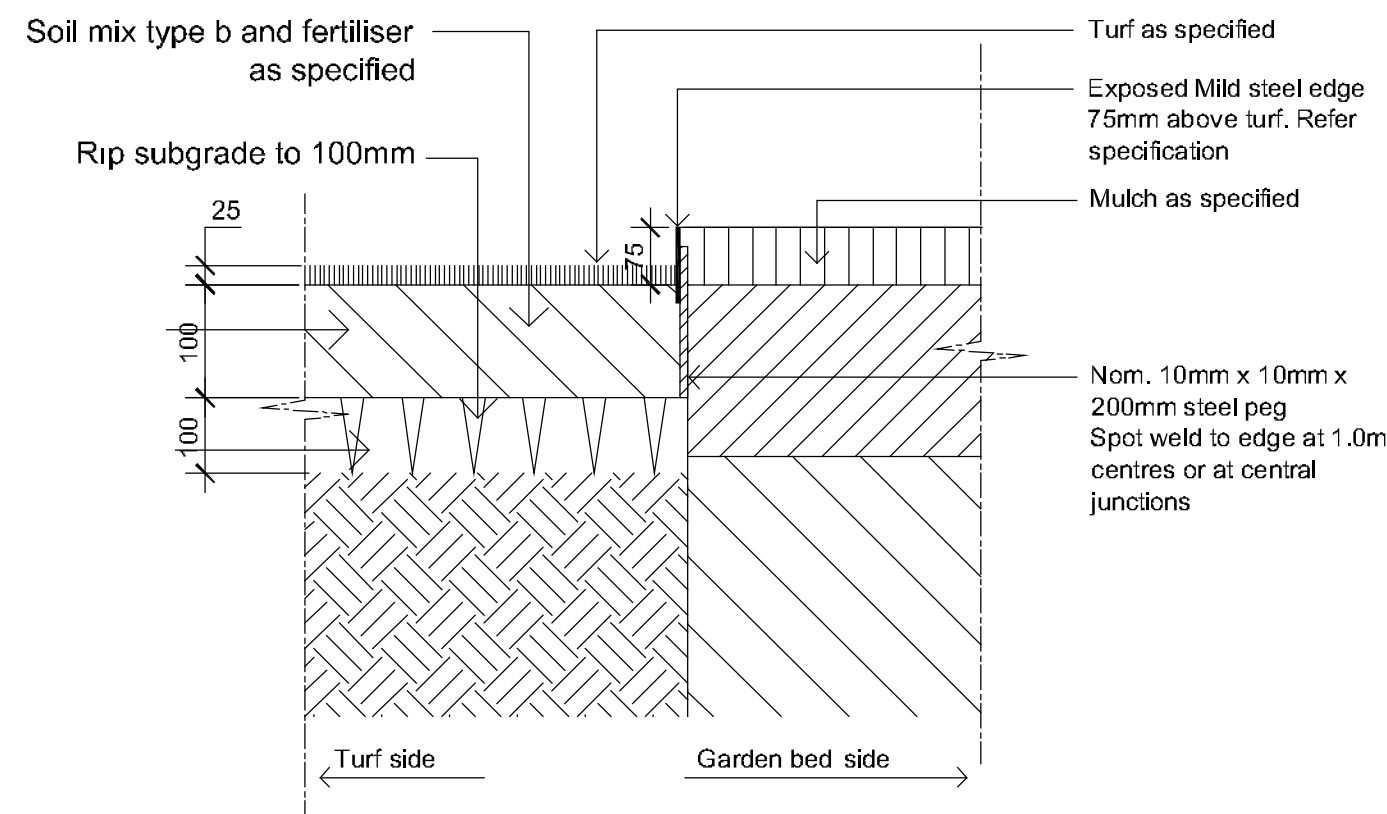
DRAWING NUMBER	ISSUE
5027-SI-LA-DWG- N-N-D1-01	3

NOTES:
1. DO NOT SCALE DIMENSIONS, REFER TO DIMENSIONED COORDINATES.
2. ALL DIMENSIONS ARE IN MILLIMETRES UNLESS OTHERWISE SPECIFIED.
3. ALL DIMENSIONS SHALL BE VERIFIED BY THE CONTRACTOR PRIOR TO CONSTRUCTION.
4. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS.
5. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS.
6. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS.
7. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS.

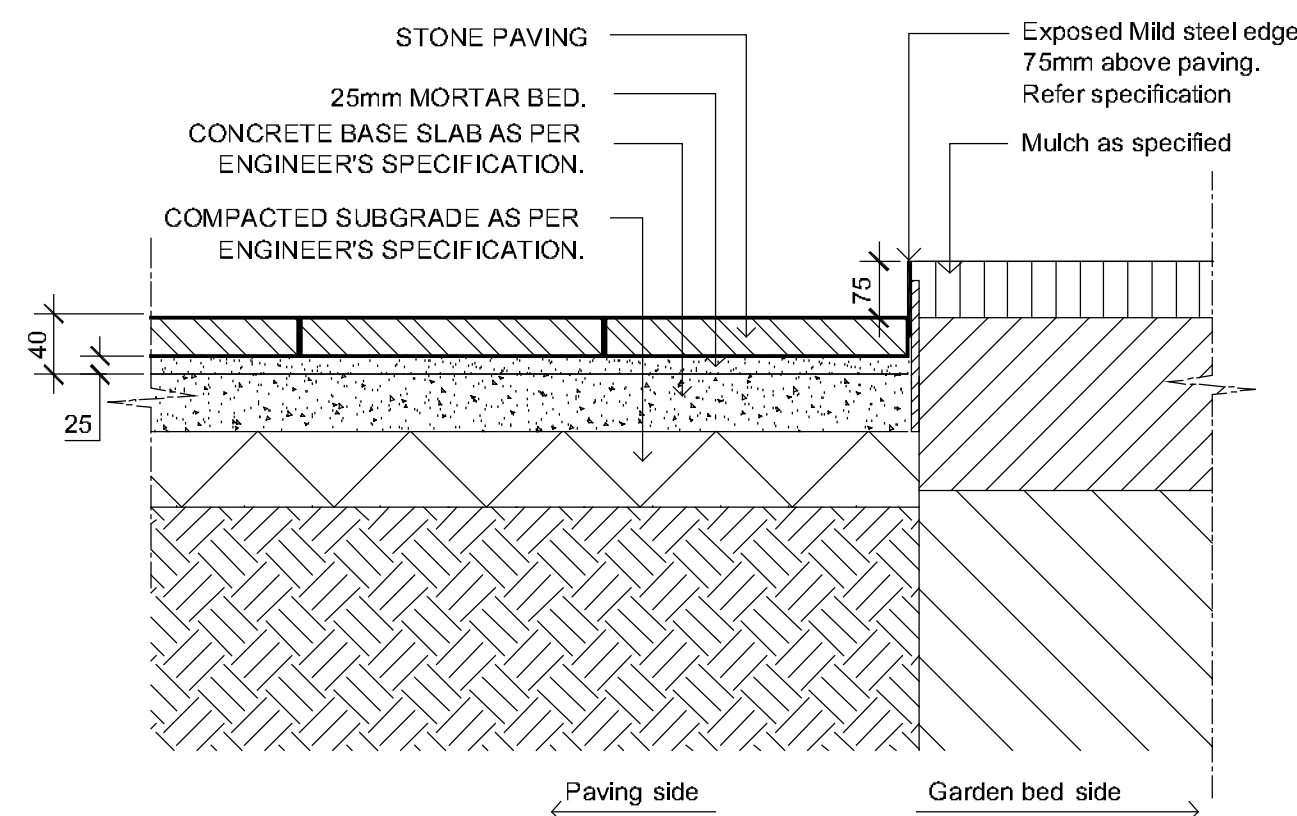
ISSUE	DATE	FOR
1	05/07/2013	50% Design Development
2	23/09/2013	Tender
3	06/09/2013	Final Tender



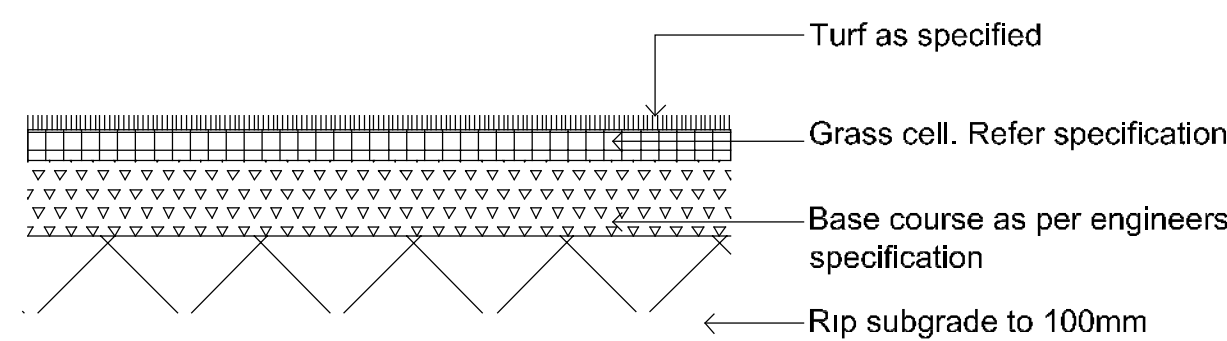
01 Section B - Central Courtyard
501 SCALE 1:50



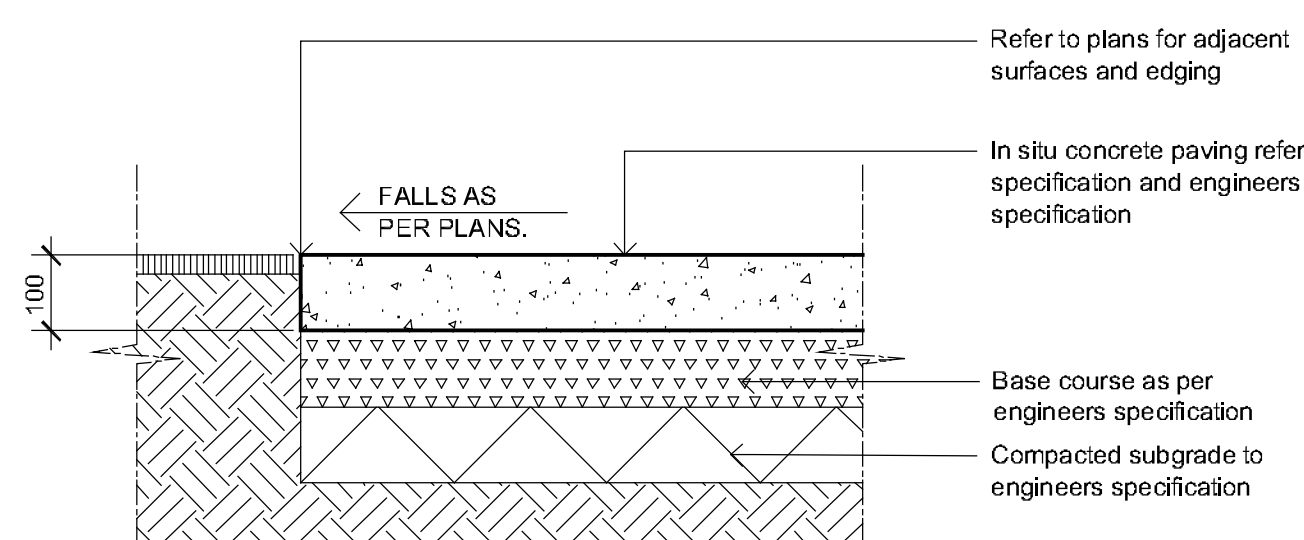
02 TYPICAL MILD STEEL EDGE & TURF
501 SCALE 1:10



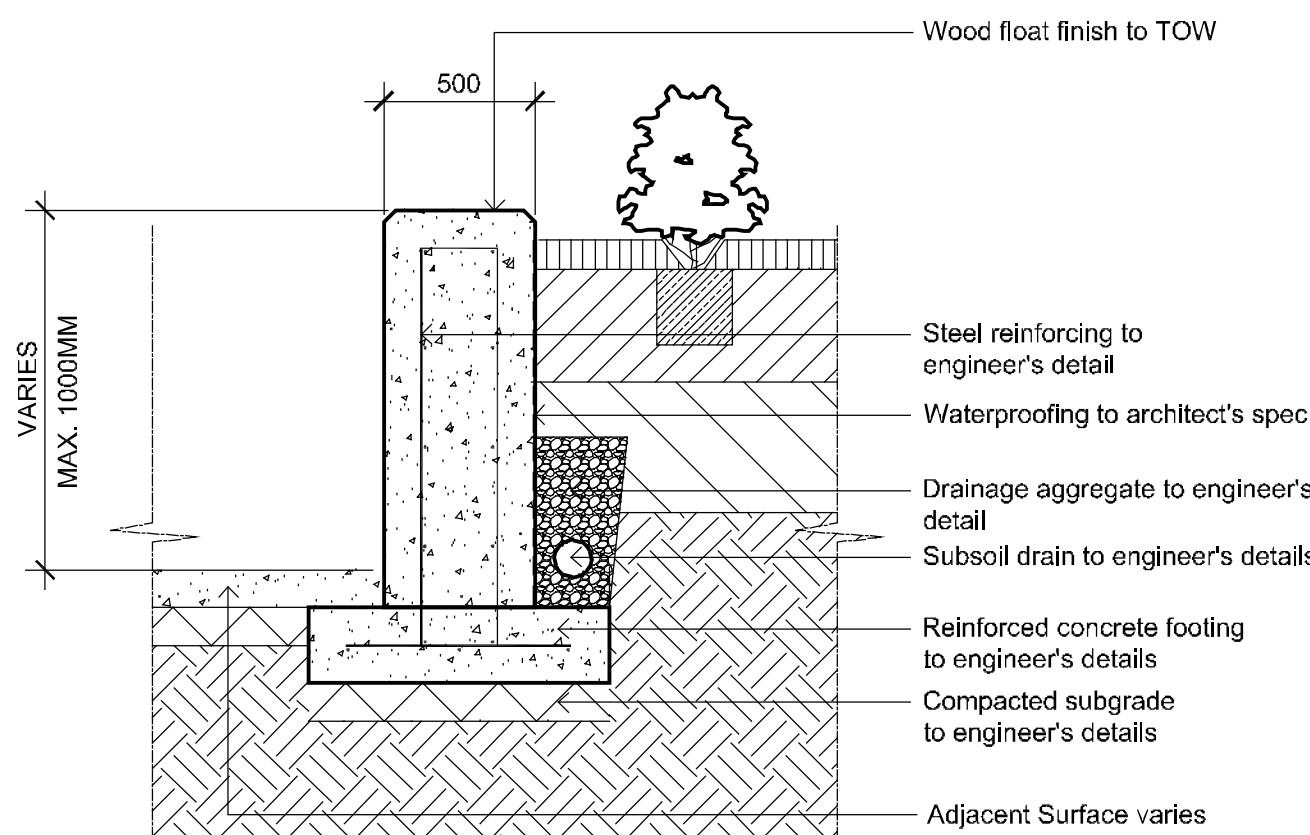
05 TYPICAL MILD STEEL EDGE & STONE PAVERS
501 SCALE 1:10



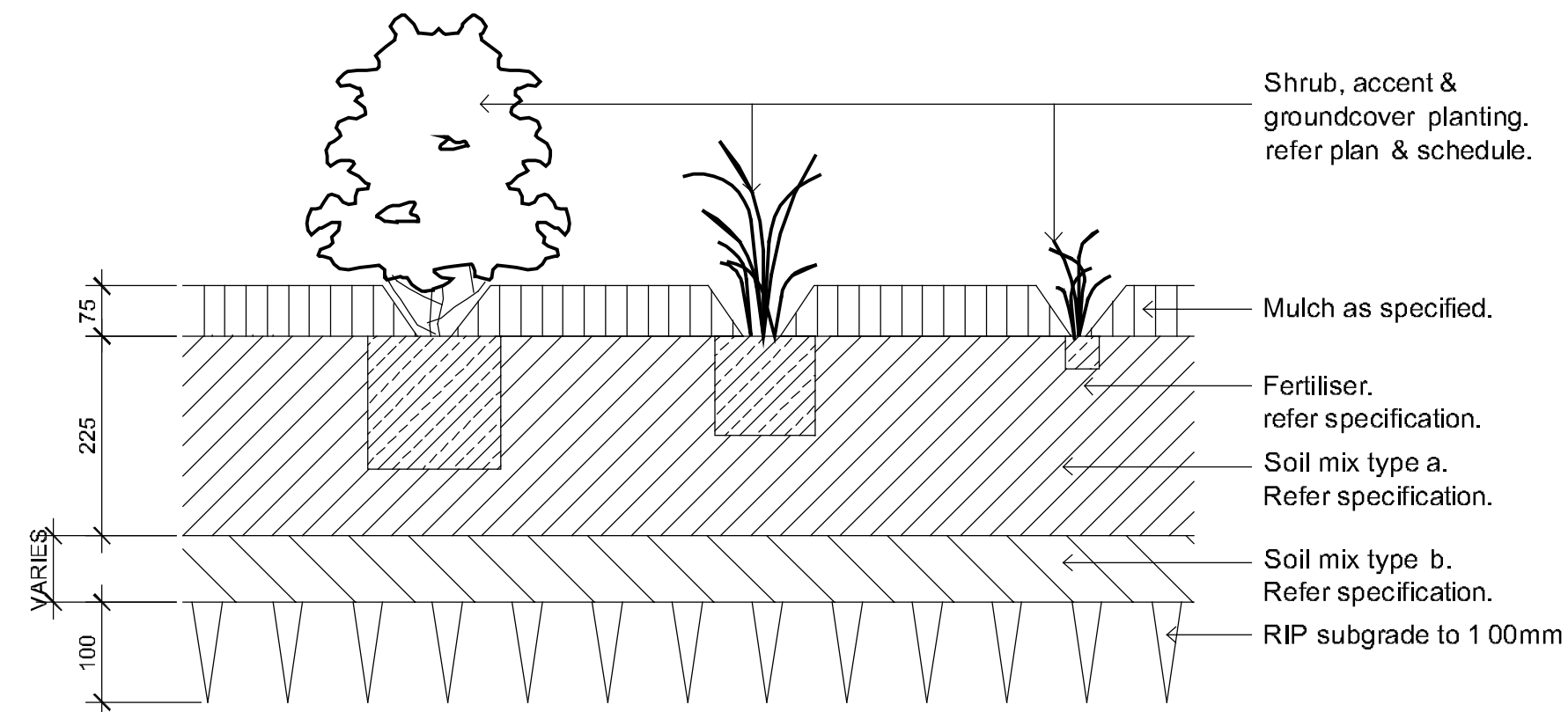
03 REINFORCED TURF
501 SCALE 1:10



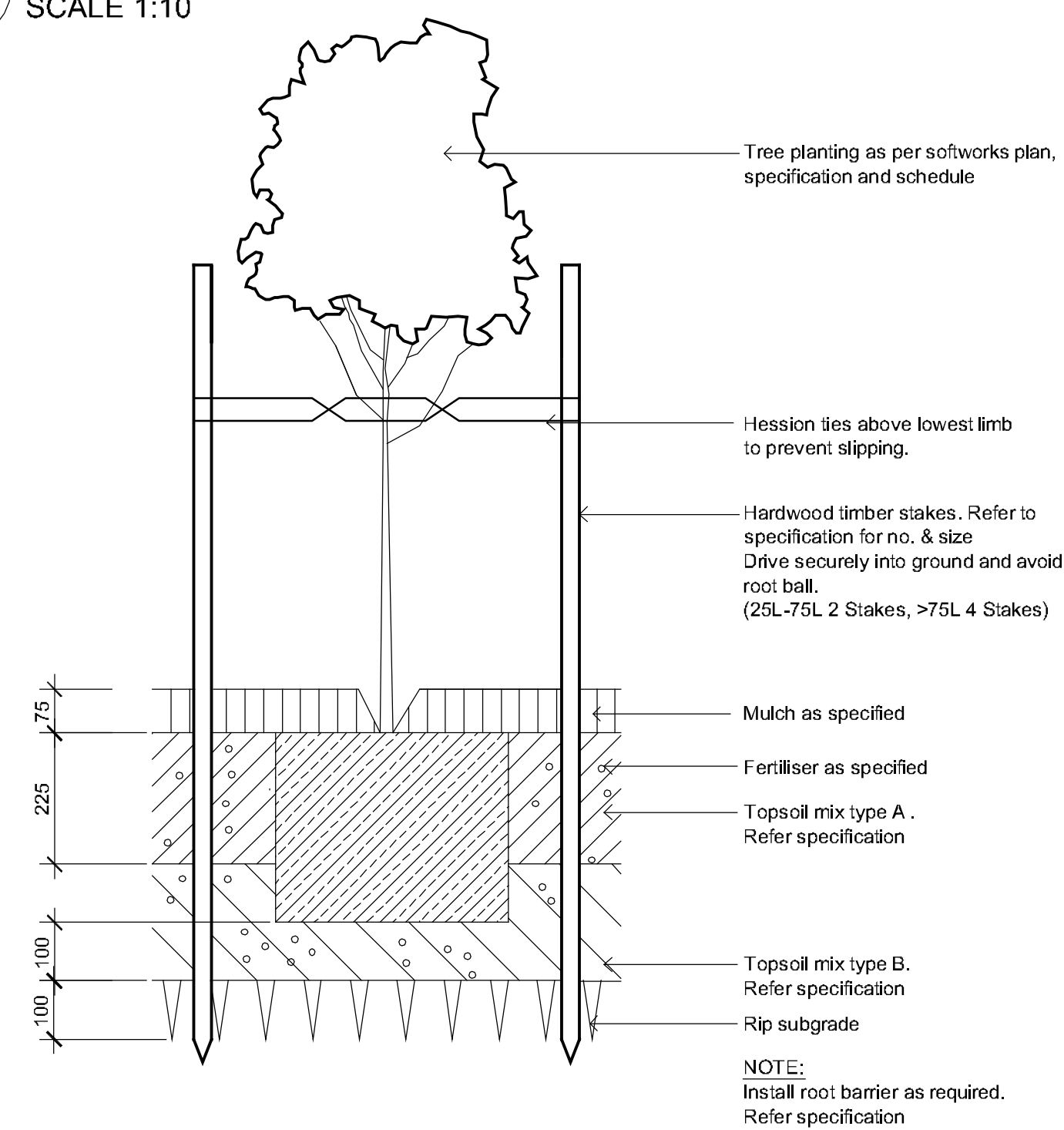
04 Insitu Concrete
501 SCALE 1:10



06 FORMED INSITU CONCRETE RETAINING WALL
501 SCALE 1:20



07 SHRUB, ACCENT & GROUND COVER PLANTING ON GRADE
501 SCALE 1:10



08 25L-75L TREE PLANTING ON GRADE
501 SCALE 1:10

