

Traffic Management Plan



Developed by Men AT Work For:

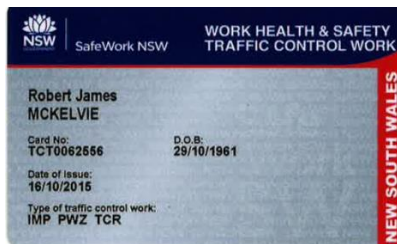
LIPMAN Pty Ltd.

23-31 Gordon St Coffs Harbour

Prepared By:

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DATE:	23rd March 2021
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SIGNATURE



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1. Introduction

1.1. Project Overview

Men AT Work (MAW) has been engaged by Lipman Pty Ltd (Lipman) to prepare a Construction Traffic Management Plan (CTMP) for the construction of a multi-purpose development (the Proposal) at 23-31 Gordon Street, Coffs Harbour (the Site), which states:

“In relation to construction traffic addressing the following:

- Assessment of cumulative impacts associated with other construction activities in the local area;
- Details of the construction program detailing the anticipated construction duration and highlighting significant and milestone stages and events during the construction process;
- Details of anticipated peak hour and daily construction vehicle movements to and from the site;
- Details of access arrangements of construction vehicles, construction workers to and from the site, emergency vehicles and service vehicles
- Details of temporary cycling and pedestrian access during construction.
- Details of proposed construction vehicle access arrangements at all stages of construction
- Traffic and transport impacts during construction, including cumulative impacts associated with other construction activities, and how these impacts will be mitigated for any associated traffic, pedestrian, cyclists, parking and public transport.
- Relevant Policies and Guidelines:
 - Guide to Traffic Generating Developments (Roads and Maritime Services)
 - Austroads Guide to Traffic Management Part 6
 - Austroads Guide to Road Design Part 4A”

The purpose of this report is to detail a traffic plan for construction that would minimise traffic impacts on the surrounding road network, ensure the safety and efficiency of all workers, pedestrians and road users, and provide information regarding the construction vehicle access routes and any changed road conditions (if applicable).

It is expected that this plan would be updated should any necessary changes to the currently proposed arrangements arise in the future. Any changes to this plan shall be done in consultation with Council.

Any special events would be subject to a separate request for a specific permit not covered by this report (if required).

Please note, MAW is responsible for the preparation of this Plan only and not for its implementation, which is the responsibility of Lipman.

1.2. CTMP Compliance with Condition of Consent

A summary of the relevant requirements of the conditions of consent and this CTMP's compliance with each is provided below for clarity.

CONSTRUCTION PEDESTRIAN & TRAFFIC MANAGEMENT SUB PLAN

Prior to the commencement of works, a Construction Pedestrian and Traffic Management Sub Plan prepared by a suitably qualified person. The plan shall address, but not limited to the following matters:

C13	Consent Condition	Reference in document
a	Location of the proposed work zone(s)	Sect 1.3
b	Location of any cranes(s)	Sect 2.9
c	Haulage routes	Sect 2.4 and Figure 4
d	Construction vehicle access arrangements.	Sect.s 2.3 & 2.4
e	Proposed construction hours	Sect 2.2
f	Estimated number of construction vehicle movements and details of vehicle types, noting vehicle movements are to be minimised during peak periods.	Sect.s 3.1.1, 3.1.2 & 3.1.3
g	Details of construction activities and timing of these activities.	Sect 2.2
h	Consultation strategy for liaison with surrounding stakeholders.	Sect 5.2
i	Any potential impacts to general traffic, cyclists, pedestrians and bus services within the vicinity of the site from construction vehicles during the construction works.	VMP
j	Cumulative construction impacts of projects within or around the development site, to ensure coordination of work activities are managed to minimise impacts on the road network.	Refer Section 3.1.4
k	Should any impacts be identified, the duration of the impacts and measures proposed to mitigate any associated traffic, public transport, pedestrian and cyclist impacts must be clearly identified and included in the plan.	Sect 5.1 Monitoring

1.3. Site Location

The subject Site has a street address of 23-31 Gordon Street, Coffs Harbour and is legally known as Lot 20, Section 6 of DP 758258, Lot B of DP 346105 and Lot 123 of DP 749233. It is bound by Gordon Street to the east and Riding Lane to the west. The Site shares a frontage with a single storey church to the south and a low-rise office development to the north. Within the broader locale, surrounding developments comprise predominantly commercial, business and retail uses. The Location and Road Hierarchy Plan presented as Figure 1 provides an appreciation of the Site and its location.

1.4. Road Hierarchy

The road hierarchy in the vicinity of the Site is shown in Figure 2, with the following roads considered noteworthy:

- **Pacific Highway** – a State (arterial) road that runs in a north-south direction to the west of the Site. The road provides four travel lanes and two parking in both directions and provides a link between Korora and Boambee. Pacific Highway has a posted speed limit of 60 km/h in the vicinity of the Site.
- **Gordon Street** – a local road which runs in the north-south direction along the eastern frontage of the Site. This bidirectional road provides two trafficable lanes and two parking lanes with 2P parking restrictions. Gordon Street is restricted to a speed limit of 40 km/h in the vicinity of the Site as Coffs Harbour CBD is classified as a High Pedestrian Activity Area (HPAA).
- **Coff Street** – a local road that runs in the east-west direction and is located to the north of the Site. The road is bidirectional and generally provides four travel lanes plus a turning lane at the Grafton St intersection with a posted speed limit of 40km/h.
- **Vernon Street** – a local road that runs in an east-west direction and is located to the south of the Site. The road is bidirectional and generally provides two travel lanes and two parking lanes with 1P parking restrictions and has a posted speed limit of 40km/h except for a small section of Shared Zone (10km/h speed limit) at the Coffs Central pedestrian entrance.
- **Riding Lane** – a one-way lane that runs along the western frontage of the Site and is subject to a speed limit 40 km/h. It provides one travel lane in the southbound direction and provides vehicular to properties along Gordon Street and the existing Council offices. Furthermore, Riding Lane provides two exit points from the Castle Street Car Park complex.

The Site is conveniently located with primary access to the arterial and local road network serving the region (Pacific Highway to the north). It is therefore able to effectively distribute traffic onto the wider road network, minimising traffic impacts on local roads.

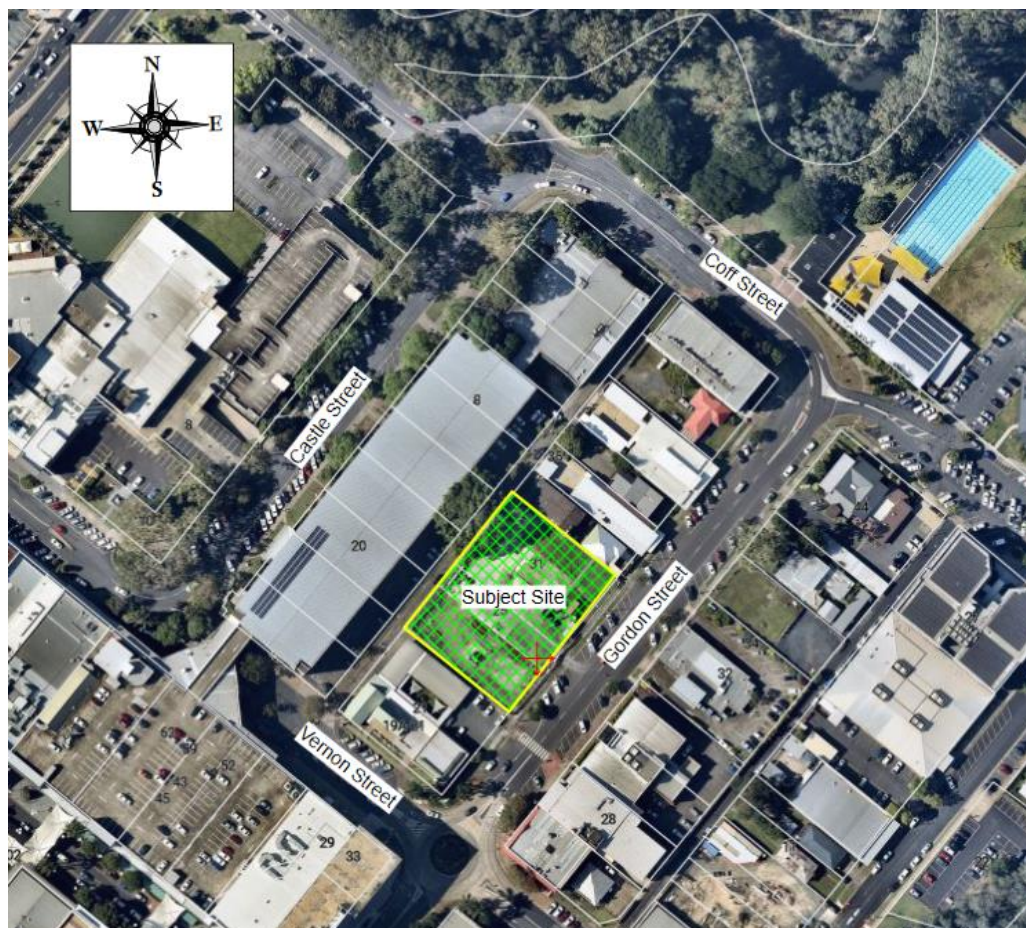


Figure 1: Location Plan

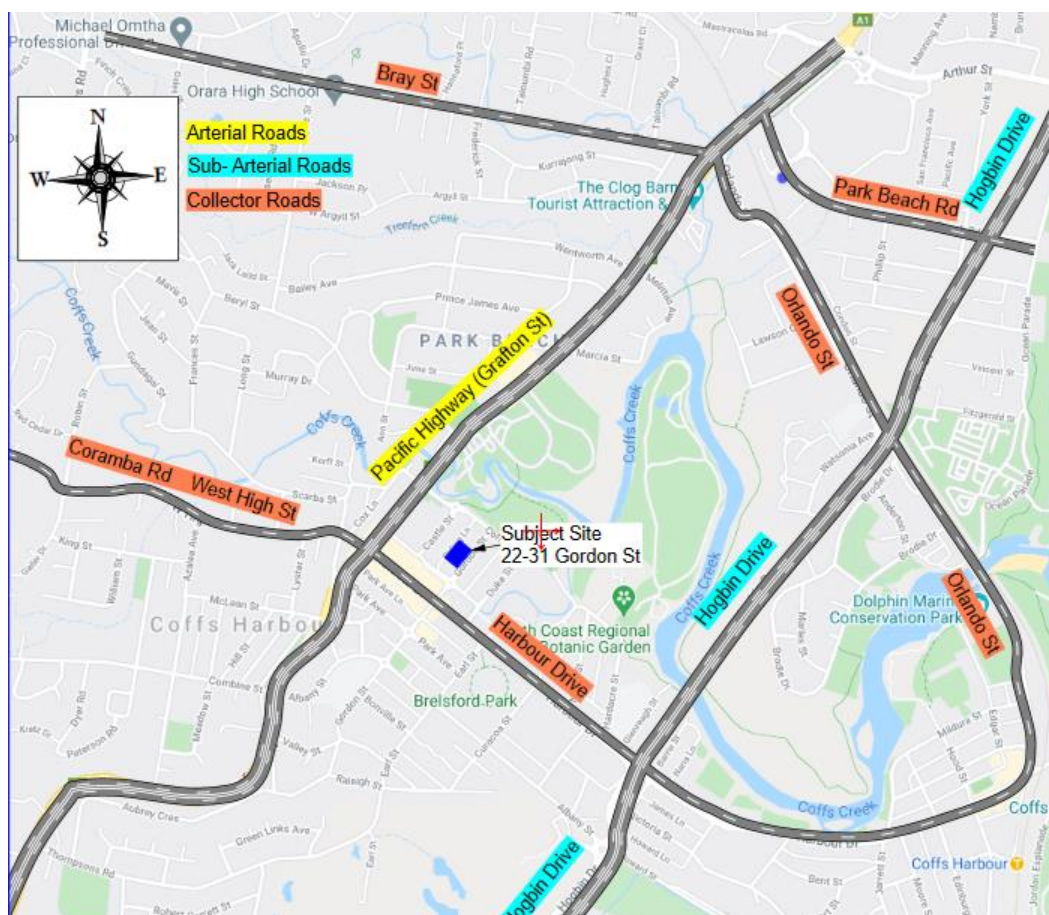


Figure 2: Road Hierarchy

1.6 Non- Car Access

1.6.1 Existing Public Transport

The Site's proximity to public transport is shown in Figure 3, which highlights the locations and distances to bus services surrounding the Site.

Existing Bus Services

The Integrated Public Transport Service Planning Guidelines, Sydney Metropolitan Area (Transport for NSW (TfNSW), December 2013) states that bus services influence the travel mode choices of areas within 400 metres walk (approximately 5 minutes) of a bus stop. In this regard, the bus services within walking distance to the Site are as follows:

Bus route 360

- Coffs Harbour Base Hospital to Park Beach Plaza – operates with one service during the morning \peak period and approximately every 30 minutes during the evening peak period.
- Park Beach Plaza to Coffs Harbour Base Hospital – operates with one service during the morning peak period and approximately every 30 minutes during the evening peak period.

Bus route 361

- Bellingen to Coffs Harbour – operates with one service during the morning peak period and does not operate during the evening peak period.
- Coffs Harbour to Bellingen – does not operate during the morning peak period and operates with one service during the evening peak period.

Bus route 365

- Park Ave to Park Beach Plaza via The Jetty – operates approximately every 60 minutes and 30-60 minutes during the morning and evening peak periods, respectively.
- Park Beach Plaza to Park Ave via The Jetty – operates approximately every 60 minutes and 30- 60 minutes during the morning and evening peak periods, respectively.

Pedestrian Connectivity

The Site provides a high level of pedestrian connectivity. The key pedestrian desire lines within the vicinity of the Site primarily relate to connections to the town centre and existing public transport infrastructure (bus stops close by). In this regard, footpaths are provided along both sides of all roads surrounding the site, except for directly along the front of the site. Pedestrians will be directed across Gordon St via 2 pedestrian crossings (see below).

The footpaths provided are of a high quality, with generous widths and dropped kerbs provided at points of crossing. The footpaths vary in width but most within the vicinity of the site are at least 1.2m. There are signalised pedestrian crossings located at the intersection of Gordon Street and Harbour Drive. These signalised crossings provide connectivity to the retail and restaurant precincts surrounding the site.

Further to the signalised crossing mentioned above, there is an existing Pedestrian Crossing on Gordon St just to the south of the site and a new (temporary) Pedestrian Crossing to be installed on Gordon St North of the site. These are noted on the VMP and will have appropriate signage installed.

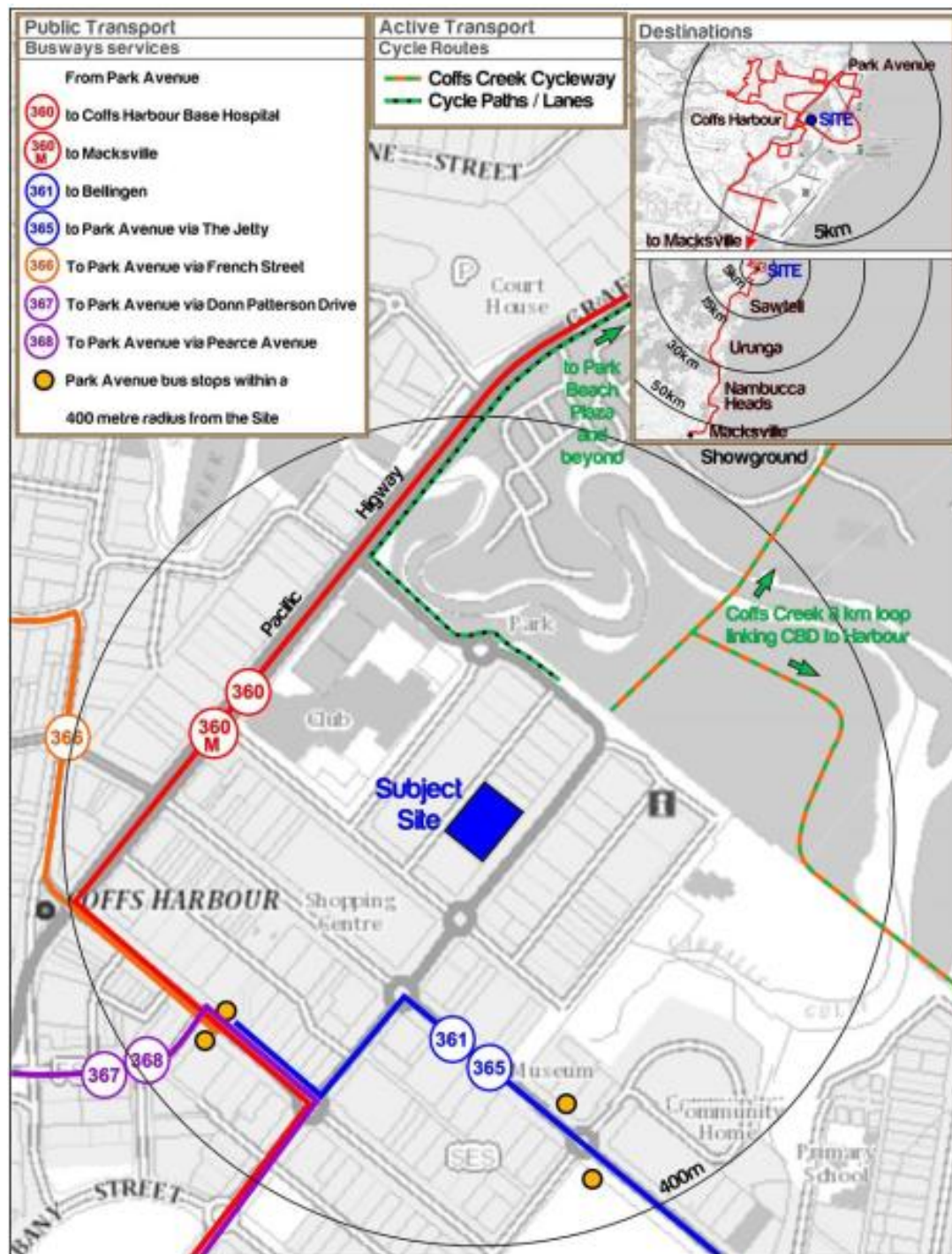


Figure 3: Existing Public Transport Map

2. Overview of Works

2.1. Staging and Duration of Works

Recognising the purpose of this CTMP, it is estimated that the total duration of the construction works will be approximately 18 months from the commencement date. The following summarises key aspects of the construction stages:

- Shoring and Excavation is estimated to begin in April 2021 and to continue for 3 months.
- General Construction and Concrete Pours are estimated to begin in July 2021 and

to continue for approximately 8 Months. Peak construction activities are expected to occur during this stage of works – especially during Concrete Pours.

- External Finishes are expected to begin in November 2021 and continue for approximately 7 months.
- Completion of works are expected to commence in June 2022 and continue for 2 months.

During this time, it is expected that occupancy certificates and other 'practical completion' works are to be undertaken.

Staging of VMP

1. Erect signage
2. Site Fence and pedestrian redirection
3. Removal and reinstatement of roadways
4. Barriers and gates installations
5. Construction works.

2.2. Hours of Operation

The type of work being undertaken will vary depending on the phase of construction and associated activities. This includes both construction and design personnel. Notwithstanding, all works will be undertaken within the following timeframes which are as follows:

- Monday to Friday (other than Public Holidays): 7:00am – 5.30pm.
- Saturday: 8:00am – 1:00pm
- Sunday & Public Holidays: No works to be undertaken.

2.3. Proposed Site Access

Construction vehicles will enter and exit the Site via a temporary entrance to the south of the Site on Gordon Street. The largest vehicle accessing the Site could be a 16m Truck and Dog, although at this stage it is expected that 12.5m Heavy Rigid Vehicles (HRV's) would be the largest vehicles to access to site. Figure 4 illustrates how heavy vehicles will access the site from the north without crossing the road by way of a U Turn at the Vernon St roundabout.

During construction pedestrians will be directed by signage to use the existing pedestrian crossing on the south side and the new / temporary pedestrian crossing to the north. Site personnel will also be able to access the Site by foot via a secure access gate along Riding Lane.

Emergency vehicle access to and from the Site will be available at all times while the Site is occupied by construction workers. This process would be implemented through emergency protocols on the site which will be developed by the Contractor.

2.4. Construction Vehicles Access Routes

All construction vehicles would enter and exit the Site via the routes shown in Figure 4. The routes shown are to be utilised by all construction vehicles travelling to and from the site and represents the shortest route available - hence minimising the

impacts of the construction process. A copy of the approved routes will be distributed by the Contractor to all drivers before their arrival to Site.

All construction vehicles associated with the construction project will enter and exit the Site (internal and Work Zone) in a forward direction.

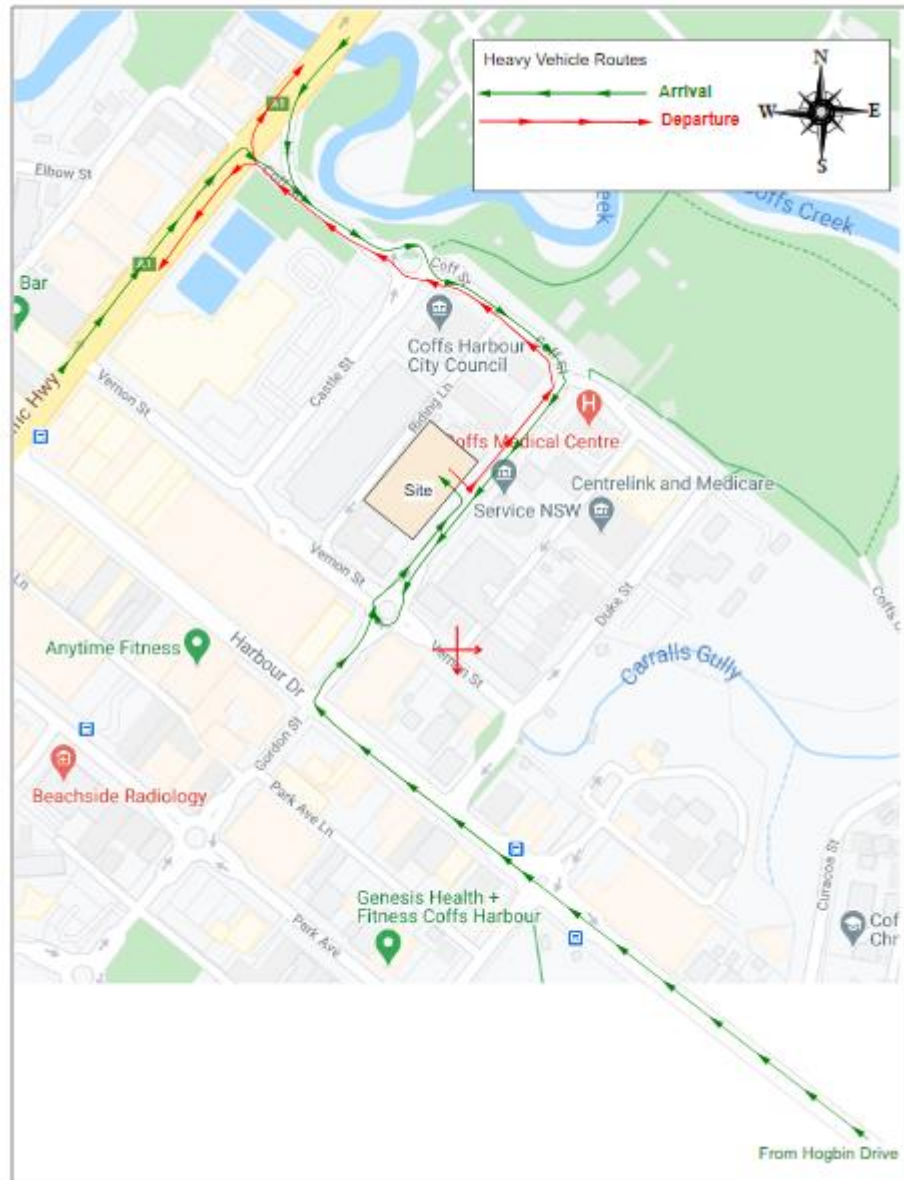


Figure 4: Construction Vehicle Route

2.5. Fencing Requirements

Security fencing will be erected along the entire boundary of the site and will be maintained for the duration of the construction program. The fencing is to ensure unauthorised persons are kept out of the Site. Site vehicle access gates will be provided along Gordon Street and will be closed at all times outside of the permitted construction hours.

A class hoarding will be installed along Gordon Street throughout the construction of the development. The proposed hoarding types and timelines are outlined within

Table 2. Hoarding layout and timings may change throughout the development, however prior approval shall be sought from Council.

Table 2: Proposed Hoarding

Street Frontage	Estimated timeline	Hoarding Type
Riding Lane	Entire Project	Site fence
Gordon St	Entire Project	A Class

2.6. Work Zone

Site constraints restrict the ability for all vehicles to be contained wholly within the Site. A Truck and Dog is suitably the largest vehicle able to access the Site. A work zone is to be proposed to assist with the excavation and construction activities.

The work zone shall be located on Gordon Street, directly adjacent to the Site to the south. The work zone is to run the length of the site frontage and can provide parking for up to 3 Truck & Dogs at any one time.

During times of vehicle movements the gate will be manned to ensure only authorised vehicles have access to the site.

2.7. Materials Handling

It is proposed that all material loading will occur within the construction site boundary. Equipment, materials and waste will be kept within the construction site boundary. Should materials handling be required from the public roadway then prior approval shall be sought and obtained from Council. Refer VMP and Materials Handling Plan.

2.8. Site Management

Site management will notify adjacent properties of any temporary traffic restrictions and measures being implemented in advance.

Some works may be required within the roadway during the external finishes stage. These works would most likely be undertaken at night or during off peak periods to limit any interaction with peak traffic conditions along Gordon Street.

Any Traffic Control measures necessary for these works will be submitted to Council for approval and notice would be provided to adjoining property owners as required. Pedestrian amenities and footpaths will be kept to serviceable conditions during the construction periods.

2.9. Site Plan

Figure 5 provides the layout for the Site during the construction period and illustrates the main Site accesses to be used and the location of the internal roads and offices.

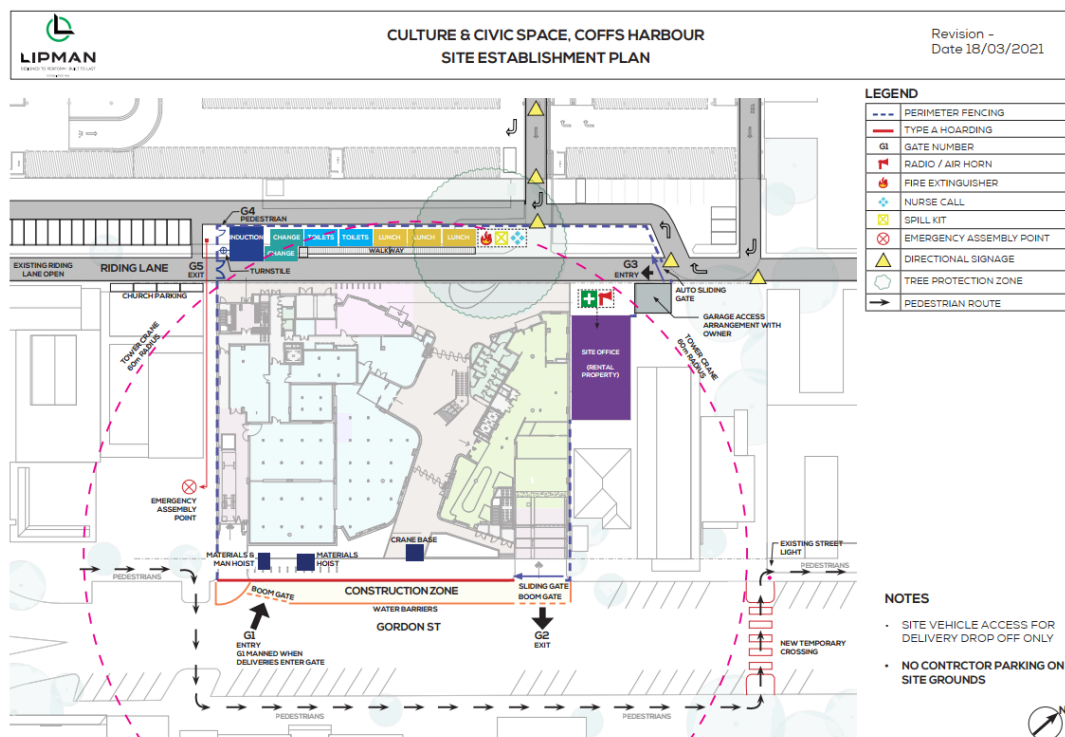


Figure 5: Site Plan (General Operation)

2.10. TMP – Monitoring & Review Process

The CTMP has been based on the existing site conditions and information provided by Lipman and Men AT Work. Consultation with Council will continue to be undertaken to ensure that the cumulative traffic impacts of construction within the area does not adversely impact the road network. The CTMP will be reviewed and monitored frequently to confirm that the construction traffic methodologies reflect the current traffic situation in the Site's locality.

In summary the CTMP has provided the following targeted management measures:

- No On-Site contractor parking.
- Traffic Controllers to manage pedestrian / cyclist traffic along site frontage.
- Traffic re-direction in Riding Lane.
- Additional temporary pedestrian crossing located on Gordon St under existing street lighting.

3. Assessment of Traffic & Transport Impacts

3.1. Construction Vehicle Traffic Generation

3.1.1 Truck Movements

Information provided by Council indicates the following breakdown of truck movements.

The scheduling of deliveries and the arrival of other vehicles will be done in such a way as to avoid peak periods on Gordon Street.

Furthermore truck movement along Harbour Drive will be ceased during the 3pm to 3.15pm during school pick up times.

An estimated 33 truck movements a day can be converted to an average of 3 trucks movements an hour (3 in and 2 out) across a 10.5-hour day. It is understood that peak volumes would be associated with Excavation, General Construction and Concrete Pours. During these peak periods, trucks are expected to arrive and depart the Site between the hours of 7:00am – 5.30pm.

There may be occasions when some increase in volumes is required for concrete pours, however it is not expected to exceed the averages across a daily period.

3.1.2 Light Vehicle Movements

In relation to light vehicle movements, it is anticipated that a maximum of 98 workers would be on-site at any one time. No parking spaces for employees / contractors are proposed on-site. Refer to section 3.3 for further parking requirements.

3.1.3 Traffic Impact

Importantly, the peak hour construction traffic volumes (33 heavy vehicle arrivals / departures in each peak) are expected to be lower than the approved operational volumes associated with the development, which is forecast to generate a site peak of 109 veh/hr.

Site constructions vehicles will share Coffs Street and Gordon Street with vehicles generated by the Coffs Harbour CBD, the most common of which are standard Vehicles. As referred to in section 2.3 above and illustrated in Figure 4, no construction vehicles entering or exiting the site shall cross over the road.

Regarding construction worker vehicles, as previously stated the workforce arrival and departure peaks sit outside the commuter peak hours; as such, these trips are not expected to impact on the operation of the broader local road network, given that background traffic flows are significantly lower than during peak periods.

3.1.4 Cumulative Traffic Impacts

Consultation has been undertaken with Council with regard to other construction projects within the immediate vicinity of the site.

There is a single project approximately 60m to the west of the site. This project has recently been completed and consists of a mixed use, 4 storey development which includes 2 levels of retail space, 2 levels of office space and provision for 28 spaces within an underground car park. The development has a subsequent stage which includes 6 levels of hotel space (80 rooms and a hotel dining room) which is indefinitely suspended.

For the purpose of this report there will be no cumulative impacts on the road network as a result of construction works at 21-31 Gordon Street, and is therefore considered acceptable.

3.2 Vehicle Management

3.2.1 Principles

In accordance with Road and Maritime Services (RMS) requirements, all vehicles transporting loose materials would have the entire load covered and/or secured to prevent any large items, excess dust or dirt particles depositing onto the roadway during travel to and from the site. All drivers are to be familiar with the Driver Code of Conduct and the CTMP before attending the Site.

Further to covering/securing the load to prevent deposits onto the roadway, a device is proposed at the point of vehicle egress to minimise the risk of dirt tracking out onto Gordon Street. The responsibility of the driver to ensure that the device is driven over would be included as part of the Driver Code of conduct.

All subcontractors must be inducted by the Contractor to ensure that the procedures are met for all vehicles entering and exiting the construction site including the Drivers Code of Conduct and the CTMP.

Vehicle movements to, from and within the Site shall do so in a manner, which does not create unreasonable or unnecessary noise or vibration. No tracked vehicles will be permitted or required on any paved roads. Public roads and access points will not be obstructed by any materials, vehicles, refuse skips or the like, under any circumstances.

3.2.2 Queuing

It is expected that a schedule for deliveries of materials and goods will be established prior to that day, with Traffic Controllers always maintaining radio contact with construction vehicles. Thus, at no stage shall queueing occur on the public road network. Refer to notes 1 & 2 below.

NOTE 1 – Vehicle Layover areas to be used to prevent overcrowding in the immediate vicinity of the site. All vehicles regardless of whether they arrive from the south or the north shall use the lay over area designated on Hogbin Drive.

NOTE 2 – all truck drivers are to be tool boxed in relation to the contents and requirements of this CTMP and the Drivers Code of Conduct.

In order to ensure no queuing in Gordon St, all waiting trucks will be directed to the Hogbin Drive layover area with a maximum of 3 trucks on site at any time. The release of trucks from the layover area will be facilitated by an onsite representative (with a UHF Radio) as one or more vehicles exit the construction zone.

3.3 Contractor parking

As previously mentioned, there will be no parking provided on-site. Contractors would be encouraged to utilise the available public transport services within the area. If Contractors still wish to use private vehicles travelling to and from site, then there are several public car parks which can be utilised within the immediate vicinity of the Site.

Parking on Gordon Street is restricted and is therefore unsuitable for contractor parking. It is subject to 2-hour parking from 8:30 AM to 6:00 PM Monday to Friday and 8:30 AM to 12:30 PM Saturday, and generally apply to the roads surrounding the subject Site.

Contractor parking is therefore to be on the top floor of both the:

1. Coffs Central Car Park and
2. The Palm Centre.

3.4 Pedestrian and Cyclist Access

Pedestrians and other footpath users will be directed with signage to an alternative path. The VMP and the Traffic Guidance Scheme (TGS) for Stop / Slow illustrate the new travel path for pedestrians via the 2 Pedestrian Crossings.

4 Traffic Control

4.1 Traffic control

The TfNSW guide “Traffic Control at Worksites” (TCAW) Version 6 manual contains standard traffic control plans (TCPs) for a range of work activities. The manual’s objective is to maximise safety by ensuring traffic control at worksites complies with best practice. The TfNSW TCAW Version 6 outlines the requirements for a Vehicle Movement Plan (VMP).

In line with this the following has been included in Appendix B

- A site specific VMP for the construction site and surrounds based on TGS D 4.7
- A site specific Traffic Guidance Scheme (TGS) for Stop / Slow if traffic requires stopping in Gordon St at any time.

If at any time other TGS’s are required they will be supplied by an authorised and qualified representative of Men At Work and presented to council for approval.

4.2 Authorised Traffic Controller

Men At Work have been engaged to supply Traffic Control Crew(s) with all members holding the appropriate qualification for the task being undertaken, as required by Lipman.

It is also anticipated that at least one ongoing site worker will be a qualified Traffic Controller in order to manage any unforeseen or ad hoc requirements.

5 Monitoring and Communication Strategies

5.1 Monitoring

This CTMP shall be subject to ongoing review and will be updated accordingly. Regular reviews will be undertaken by the on-site coordinator. As a minimum, review of the CTMP shall occur monthly.

All and any reviews undertaken should be documented, however key considerations regarding the review of the CTMP shall be:

- Tracking deliveries against the estimated volumes.
- To identify any shortfalls and develop an updated action plan to address issues that may arise during construction (Parking and access issues)
- To ensure TCP's are updated (if necessary) by "Prepare a Work Zone Traffic Management Plan" card holders to ensure they remain consistent with the set-up on-site. Men At Work have been engaged for this requirement.
- Regular checks undertaken to ensure all loads are leaving site covered as outlined within this CTMP.

5.2 Communications Strategy

The communications strategy will include the most effective communication methods to ensure adequate information within the community and assist the project team to deliver the traffic changes with minimal disruption to the road network.

To facilitate this, Lipman will be meeting with all stakeholders on the 24th March 2021 to discuss any and all impacts and agree a process for ongoing effective and efficient communication.

All adjacent businesses / property owners and occupiers shall be notified of any work that is deemed disruptive to the surrounding network prior to commencement. Ongoing communication is also proposed so that all stakeholders are kept up to date of works and potential impacts.

Nearby property owners that may be affected by the construction works shall be included within the communications strategy.

6 Summary

This CTMP has been prepared to ensure appropriate pedestrian, cyclist and traffic management is undertaken during construction of 23-31 Gordon Street, Coffs Harbour. This CTMP report has regard for the principles outlined in the RMS Traffic

Control at Worksites Manual Version 6 and AS1742.3, Guide to Traffic Generating Developments (Roads and Maritime Services), Austroads Guide to Road Design Part 4A, and Austroads Guide to Traffic Management Part 6 and is recommended for adoption. Any minor variation to these standards is considered acceptable having regard to the constraints inherent by the Site and proposed development. The following measures should be undertaken to minimise the impacts across each construction phase:

- Traffic control would be required to manage and regulate construction vehicle traffic movements into and out of the site during construction.
- All vehicles transporting loose materials will have the load covered and/or secured to prevent any items depositing onto the roadway during travel to and from the Site.
- All vehicles to enter and exit the site in a forward direction with reverse movements to occur only within the property boundary as necessary, prior approval and subject to supervision.

In summary, the CTMP has provided the following targeted management measures:

- No On-Site Contractor Parking.
- Traffic Controllers to Manage Pedestrian / Cyclist traffic along the Site frontage.
- Traffic re-direction in Riding Lane
- Additional temporary pedestrian crossing located on Gordon St underneath existing street lighting

In summary, the detailed CTMP report is proposed in accordance with the RMS TCAW.

Appendix A - Driver Code of Conduct

Safe Driving Policy for 23-31 Gordon Street, Coffs Harbour.

Objectives of the Drivers Code of conduct

- To minimise the impact of earthworks and construction on the local and regional road network;
- Minimise conflict with other road users;
- Minimise road traffic noise; and
- Ensure truck drivers use specified routes.

Code of Conduct

All vehicle operators accessing the site must:

- Take reasonable care for his or her own personal health and safety.
- Not adversely, by way of actions or otherwise, impact on the health and safety of other persons.
- Notify their employer if they are not fit for duty prior to commencing their shift.
- Obey all applicable road rules and laws at all times.
- In the event an emergency vehicle behind your vehicle, pull over and allow the emergency vehicle to pass immediately.
- Obey the applicable driving hours in accordance with legislation and take all reasonable steps to manage their fatigue and not drive with high levels of drowsiness.
- Obey all on-site signposted speed limits and comply with directions of traffic control supervisors in relation to movements in and around temporary or fixed work areas.
- Ensure all loads are safely restrained, as necessary.
- Drive over devices – located at the Site's access – to vibrate off any loose material attached to construction vehicles.
- Operate their vehicles in a safe and professional manner, with consideration for all other road users.
- Hold a current Australian State or Territory issued driver's licence applicable to the class of vehicle being driven.
- Notify their employer or operator immediately should the status or conditions of their driver's license change in any way.
- Comply with other applicable workplace policies, including a zero tolerance of driving while under the influence of alcohol and/or illicit drugs.
- Not use mobile phones when driving a vehicle or operating equipment. If the use of a mobile device is required, the driver shall pull over in a safe and legal location prior to the use of any mobile device.
- Advise management of any situations in which you know, or think may, present a threat to workplace health and safety.
- Drive according to prevailing conditions (such as during inclement weather) and reduce speed, if necessary.
- Have necessary identification documentation at hand and ready to present to security staff on entry and departure from the site, as necessary, to avoid unnecessary delays to other vehicles.

Crash or incident Procedure

- Stop your vehicle as close to it as possible to the scene, making sure you are not hindering traffic.

Ensure your own safety first, then help any injured people and seek assistance immediately if required.

- Ensure the following information is noted:
 - Details of the other vehicles and registration numbers
 - Names and addresses of the other vehicle drivers
 - Names and addresses of witnesses
 - Insurers details
- Give the following information to the involved parties:
 - Name, address and company details
 - If the damaged vehicle is not occupied, provide a note with your contact details for the owner to contact the company.
- Ensure that the police are contacted should the following circumstances occur:
 - If there is a disagreement over the cause of the crash.
 - If there are injuries.
 - If you damage property other than your own.
- As soon as reasonably practical, report all details gathered to your manager.

Appendix B – Traffic Guidance Schemes (Attached to submission)

Vehicle Management Plan (VMP)

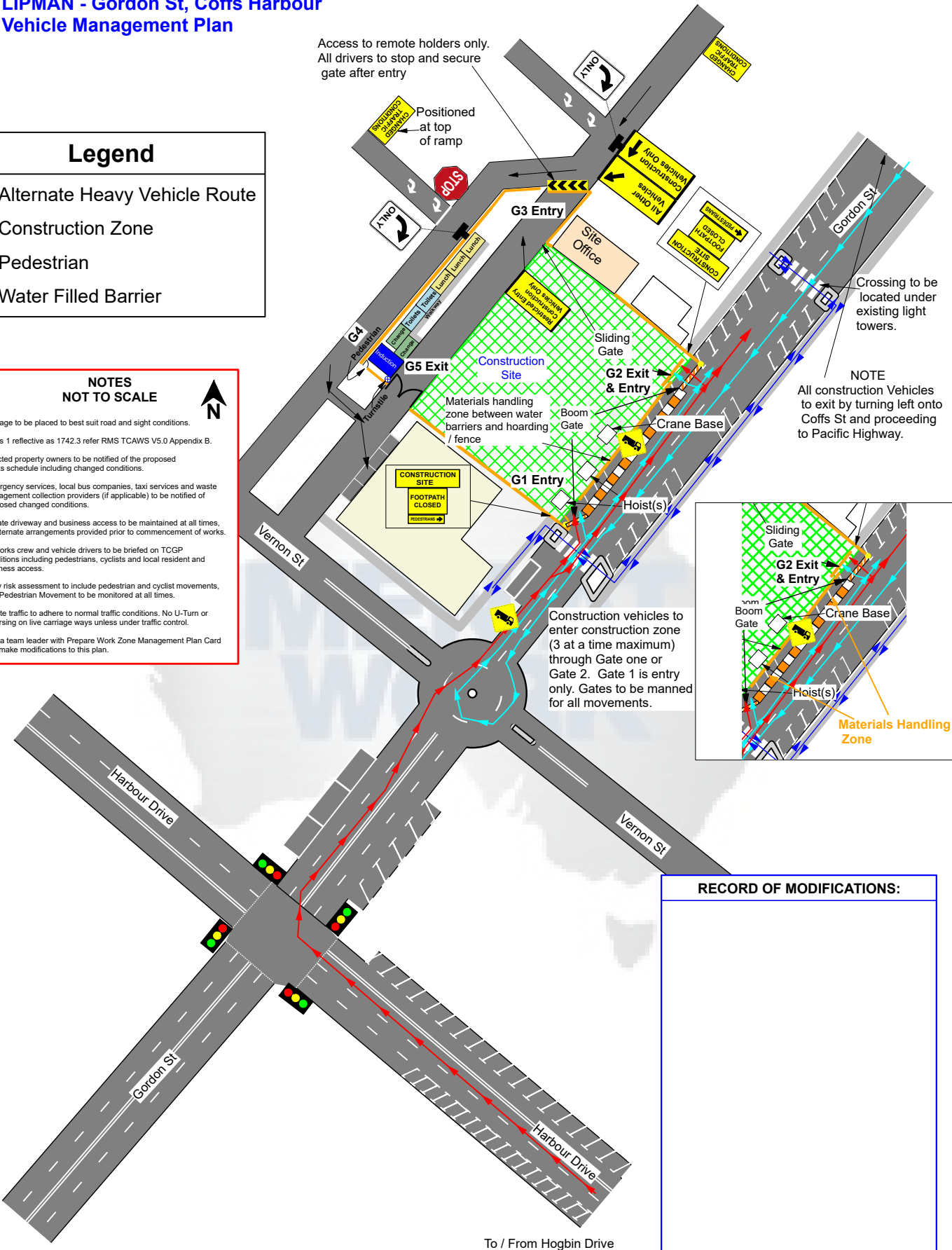
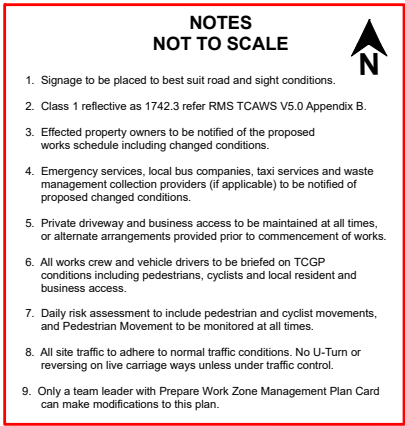
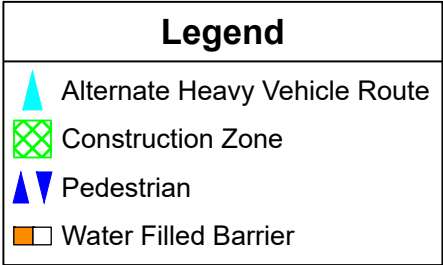
Traffic Guidance Scheme (TGS) – for Stop/ Slow of traffic on Gordon St

Lipman Traffic Management Plan

Lipman Materials Handling Plan

Lipman Site Establishment plan

LIPMAN - Gordon St, Coffs Harbour Vehicle Management Plan



LIPMAN - Gordon St, Coffs Harbour
Stop / Slow Traffic control

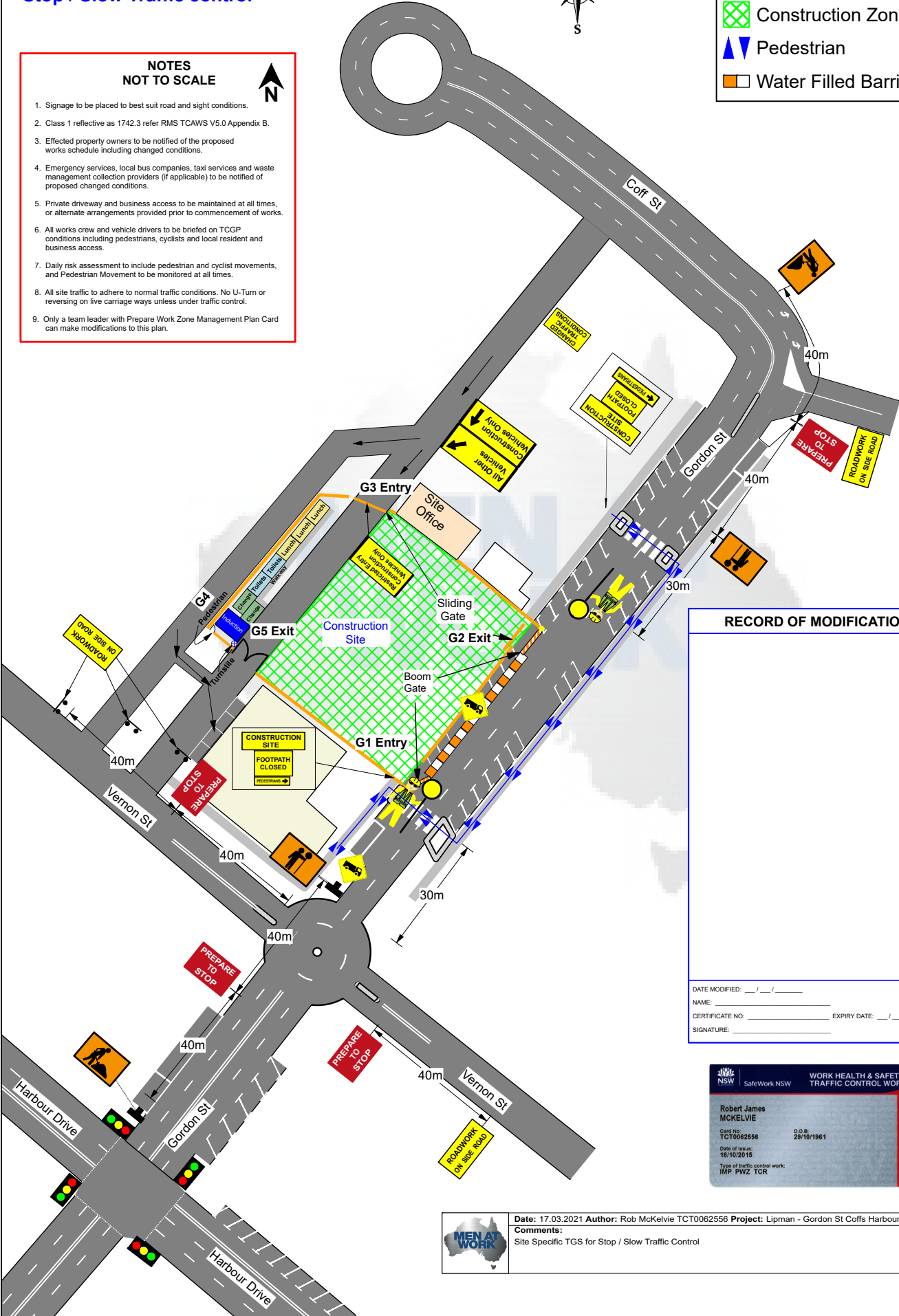
NOTES
NOT TO SCALE

- 1. Signage to be placed to best suit road and sight conditions.
- 2. Class 1 reflective as 1742.3 refer RMS TCAWS V5.0 Appendix B.
- 3. Effected property owners to be notified of the proposed works schedule including changed conditions.
- 4. Emergency services, local bus companies, taxi services and waste management collection providers (if applicable) to be notified of proposed changed conditions.
- 5. Private driveway and business access to be maintained at all times, or alternate arrangements provided prior to commencement of works.
- 6. All works crew and vehicle drivers to be briefed on TCGP conditions including pedestrians, cyclists and local resident and business access.
- 7. Daily risk assessment to include pedestrian and cyclist movements, and Pedestrian Movement to be monitored at all times.
- 8. All site traffic to adhere to normal traffic conditions. No U-Turn or reversing on live carriage ways unless under traffic control.
- 9. Only a team leader with Prepare Work Zone Management Plan Card can make modifications to this plan.




Legend

- Construction Zone
- Pedestrian
- Water Filled Barrier



RECORD OF MODIFICATIONS:

DATE MODIFIED: ____ / ____ / ____
NAME: _____
CERTIFICATE NO: _____ EXPIRY DATE: ____ / ____ / ____
SIGNATURE: _____



SafeWork NSW

WORK HEALTH & SAFETY
TRAFFIC CONTROL WORK

Robert James
MCKELVIE

Card No:
TCT0062556

Date of Issue:
16/10/2015

Type of traffic control work:
IMP PWZ TCR

29/10/1961

NEW SOUTH WALES

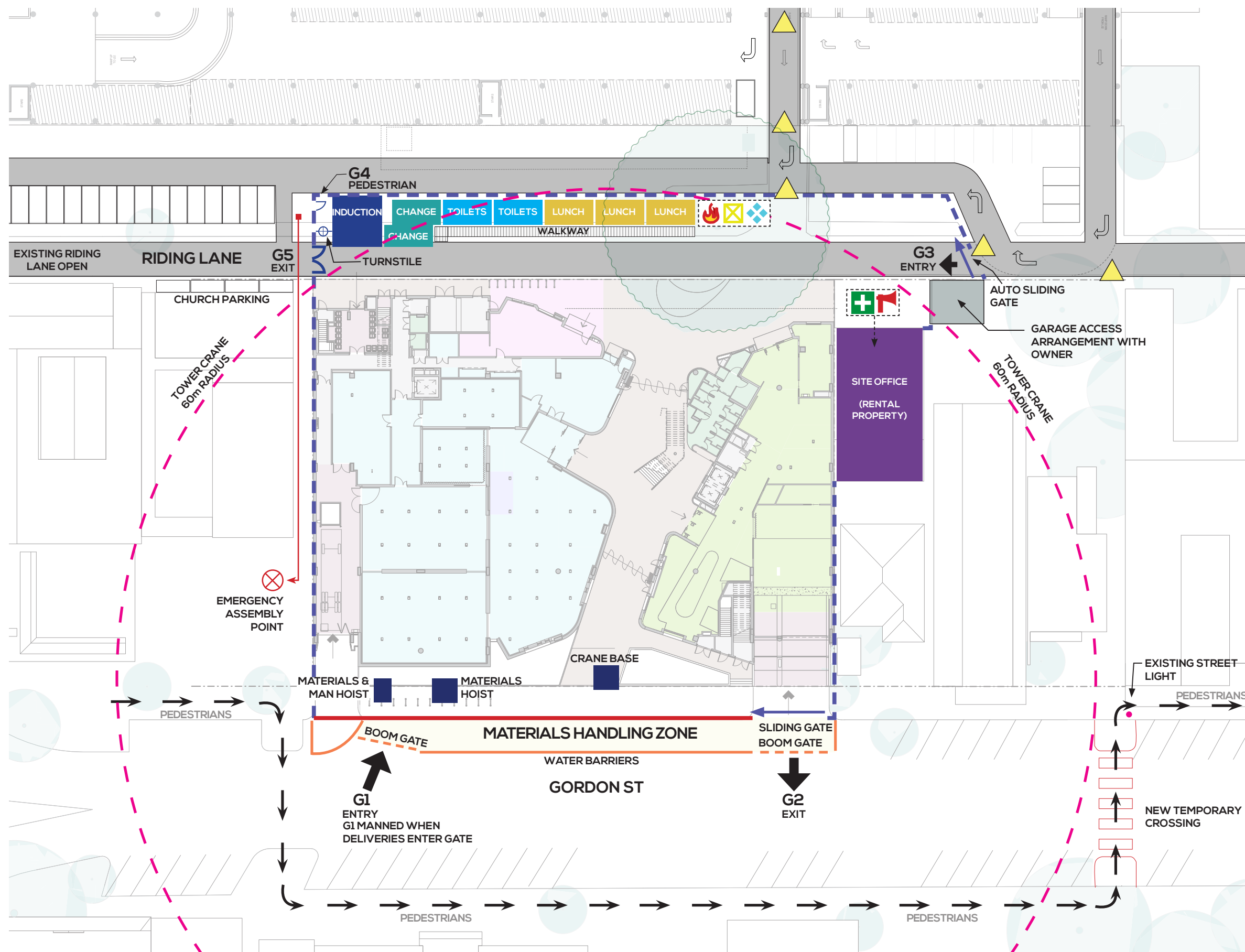
Date: 17.03.2021 Author: Rob McKelvie TCT0062556 Project: Lipman - Gordon St Coffs Harbour
Comments:
Site Specific TGS for Stop / Slow Traffic Control



CULTURE & CIVIC SPACE, COFFS HARBOUR

MATERIALS HANDLING PLAN

Revision -
Date 18/03/2021



LEGEND

---	PERIMETER FENCING
---	TYPE A HOARDING
G1	GATE NUMBER
📡	RADIO / AIR HORN
🔥	FIRE EXTINGUISHER
🏥	NURSE CALL
🧴	SPILL KIT
⊗	EMERGENCY ASSEMBLY POINT
🚦	DIRECTIONAL SIGNAGE
🌳	TREE PROTECTION ZONE
➡	PEDESTRIAN ROUTE

NOTES

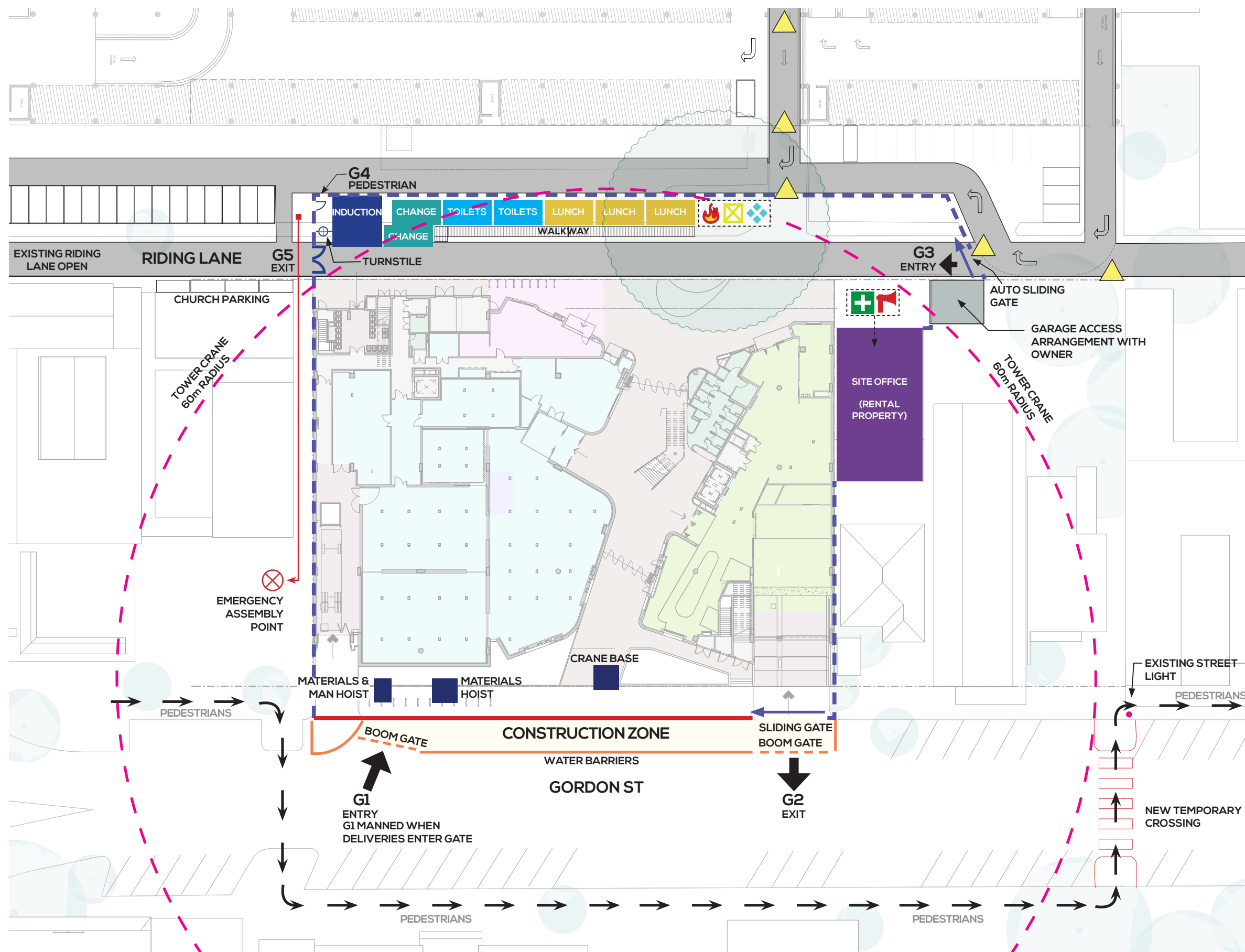
- ALL MATERIALS HANDLING THROUGH GORDON STREET ACCESS ONLY
- ALL DELIVERIES BY ARRANGED TIMES ONLY
- ALL CRANE BOOKINGS TO BE MADE 48 HOURS IN ADVANCE
- NO CONTRCTOR PARKING ON SITE GROUNDS



CULTURE & CIVIC SPACE, COFFS HARBOUR

SITE ESTABLISHMENT PLAN

Revision -
Date 18/03/2021



LEGEND

	PERIMETER FENCING
	TYPE A HOARDING
	GATE NUMBER
	RADIO / AIR HORN
	FIRE EXTINGUISHER
	NURSE CALL
	SPILL KIT
	EMERGENCY ASSEMBLY POINT
	DIRECTIONAL SIGNAGE
	TREE PROTECTION ZONE
	PEDESTRIAN ROUTE

NOTES

- SITE VEHICLE ACCESS FOR DELIVERY DROP OFF ONLY
- NO CONTRCTOR PARKING ON SITE GROUNDS

