



24 December 2021

File No: NTH20/00312/04

Your Ref: SSD-11920082

The Director
Major Projects
Department of Planning Industry & Environment
Locked Bag 5022
PARRAMATTA NSW 2124

Attention: James McDonough – Team Leader

Dear Sir / Madam,

RE: Response to Submissions – Hansons Tweed Sand Plant Expansion (SSD-10398)

I refer to the Response to Submissions (RTS) for the abovementioned State Significant Development Application referred to Transport for NSW (TfNSW) via the NSW Planning Portal on 1 November 2021.

Transport for NSW Response

TfNSW response to EIS dated 27 May 2021 identified the roles and responsibilities for road infrastructure, addressed relevant facts about the development and TfNSW interest in the development application.

TfNSW attended a meeting with the Applicant's Consultant on 2 June 2021 and provided further advices dated 7 July 2021 and 9 September 2021 in response to an amended access proposal and to inform the revised Traffic Impact Assessment (TIA). The TIA now identifies an acceptable operational solution for access to the proposed development. TfNSW appreciates the Applicant for engaging to identify a suitable access arrangement that will enable existing and proposed development to safely access the Freeway interchange.

TfNSW has reviewed the Response to Submissions (RTS) and supporting documentation, including the revised Traffic Impact Assessment (TIA), and provides the following comments to assist the Department in finalising this assessment:

1. The revised TIA proposes a new eastern roundabout, which will result in a lower speed environment and improve safety for heavy vehicle accessing the proposed development and the adjoining site occupied by Australian Bay Lobster Producers. Additionally, works are proposed to adjust the existing western roundabout to accommodate the proposed increase in heavy vehicle movements.

TfNSW recommends that the identified works be a requirement of any project approval and that relevant conditions be imposed to require all necessary agreements, approvals, and completion of works prior to commencement of the extraction. It is requested that TfNSW and Tweed Shire Council be provided the opportunity to endorse any draft conditions of consent relating to road works prior to the Consent Authority's determination.

2. The proposed new roundabout will require the dedication of land outside of the existing road corridor as public road. The proposed roundabout footprint will occupy Lot 51 DP 1056966, which TfNSW-owned land that is currently subject to a licence agreement with Australian Bay Lobster Producers (ALBP) for vehicular access to approved development on Lot 1 DP 1192506.

It is understood that Council has submitted an objection to the development application. TfNSW has discussed the proposed roundabout with Council and the potential benefits of existing and proposed heavy vehicle being directed to the Pacific Motorway, noting forecast growth along Tweed Coast Road and Kingscliff locality. Should Council be amenable to approval of the development, then TfNSW can dedicate land as public road to support the roundabout, provided suitable access is provided to the adjoining property as shown in the submitted concept design.

TfNSW recommends that the Department obtain a property adjustment plan and agreement in-principle from Tweed Shire Council to proposed access solution. The dedication of TfNSW land as public road will be subject to agreement of all parties and approval of the SSD application. Any land dedicated as public road and then transferred to Council will then be adjusted to align with the final road boundaries.

3. TfNSW notes that the Economic Analysis included in the updated TIA may potentially underestimate the cost of the new roundabout and ongoing maintenance costs to the relevant Roads Authorities over the life of the development. Experience suggests that a concrete roundabout is likely to incur a higher capital cost. The increase in heavy vehicle movements will also reduce the existing pavement life on the Tweed Valley Way and on the freeway ramps, bringing forward public expenditure on rehabilitation and ongoing maintenance.

The proposed new roundabout will form part of Tweed Valley Way, a classified (Regional) road, which is managed by Tweed Shire Council. The connection of the southbound freeway ramps to the new roundabout will extend infrastructure managed by TfNSW.

The purpose of the new roundabout is to provide access to development and is necessary to mitigate development impacts on the safety, efficiency and ongoing operation of the classified road. Consideration should be given to future uses of the subject site and adjoining properties with respect to any adjustments to be required on project completion.

TfNSW recommends that the Developer be required to provide a contribution to the additional maintenance burden on Roads Authorities generated over the life of the development. This will need to be identified in agreements with Council and TfNSW prior to granting of the required approvals under the *Roads Act 1993*.

4. The proposed buffer between extraction areas and the Pacific Motorway is noted as 50m, with reduced setback of 10m proposed to the interchange off-ramp and new intersection. Following the construction of any new intersection and adjustment of the ramps, further geotechnical assessment should be provided to justify any proposed extraction less than 20m from the road reserve boundary.

TfNSW highlights that in determining the application under the *Environmental Planning and Assessment Act 1979*, it is the Consent Authority's responsibility to consider the environmental impacts of any roadworks which are ancillary to the development. This includes any works which form part of the proposal and/or any works which are deemed necessary to include as requirements in the conditions of project approval.

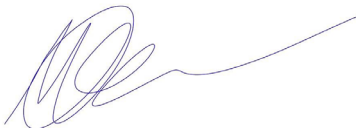
Any roadwork on the classified road network are to be designed and constructed in accordance with the current Austroads Guidelines, Australian Standards and TfNSW Supplements.

The Developer will be required to enter into a Works Authorisation Deed (WAD) or other suitable agreement as required by TfNSW prior to any road works on the Pacific Highway interchange ramps. The developer will be responsible for all costs associated with the roadwork and administration for the WAD. It is recommended that developers familiarise themselves with the requirements of the WAD process. Further information can be obtained from the TfNSW [website](#).

Any works required on the Tweed Valley Way will require approval of Tweed Shire Council with concurrence from TfNSW in accordance with Section 138(2) of the *Roads Act 1993*.

TfNSW is willing to review any General Terms of Agreement or draft conditions of consent prior to determination where the department deems appropriate. If you have any further enquiries regarding the above comments please do not hesitate to contact the undersigned on (02) 6640 1362 or via email at: development.northern@transport.nsw.gov.au

Yours faithfully,

A handwritten signature in blue ink, appearing to read 'MA', followed by a long horizontal flourish.

Matt Adams
Acting Manager, Development Services
Community and Place | Region North
Regional & Outer Metropolitan
Transport for NSW