

single, wider, set. This plan also depicts the concrete seawall south of Lyons Boatshed, constructed as part of the reclamation process in the 1930s, as well as a “waiting room” and public toilet facilities in The Spit Reserve. The timber jetty located to the north of Lyons Boatshed remains unchanged.

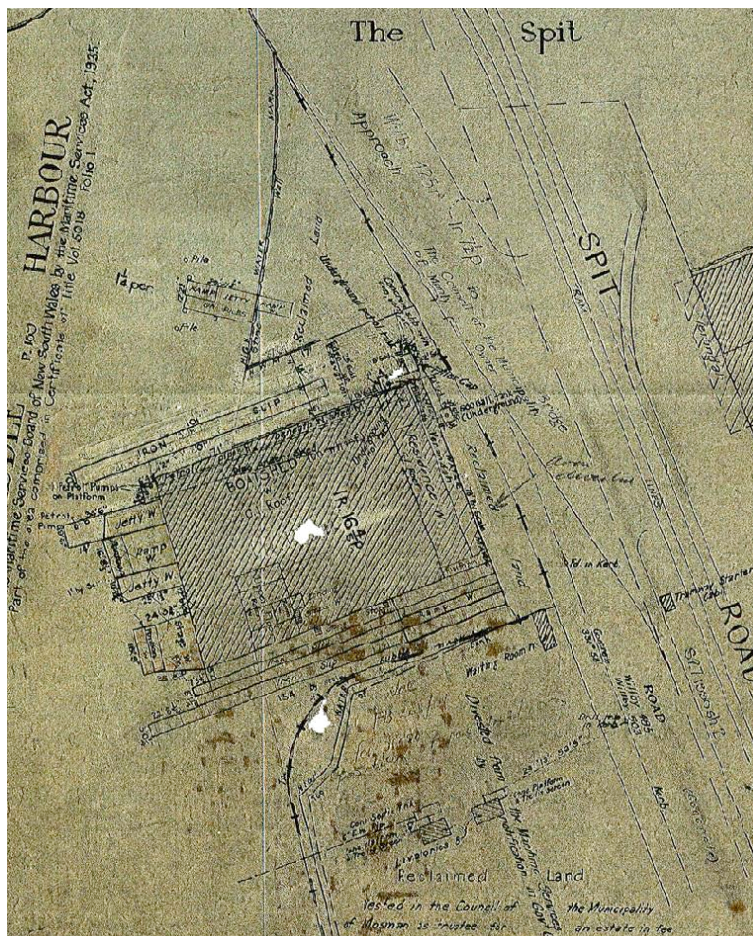


Figure 3.37: Chief Surveyor & Land Valuer (1924 – with amendments to 1963) *Plan of The Spit, Middle Harbour, Municipality of Mosman, Parish of Willoughby, County of Cumberland.*⁹⁷

In the early to mid 1960s, Lyons Boatshed was sold. d'Albora Marinas purchased the property and by the early to mid 1970s, the structures of Lyons Boatshed had been demolished.⁹⁸ A new building was built on site and a marina consisting of a main jetty extending seaward with three marina arms, was constructed extending from the new boatshed into the waters of Pearl Bay – refer to Figures 3.38 to 3.40.



Figure 3.38: “Pearl Bay, The Marina and The Spit Bridge, open, 1974”⁹⁹

⁹⁷ NSW Maritime Authority, Willby 232.

⁹⁸ Andrews, G (2002)

Figure 3.39: Byron Campbell
“Aerial photograph of Spit looking
towards Sydney, 1977.”¹⁰⁰

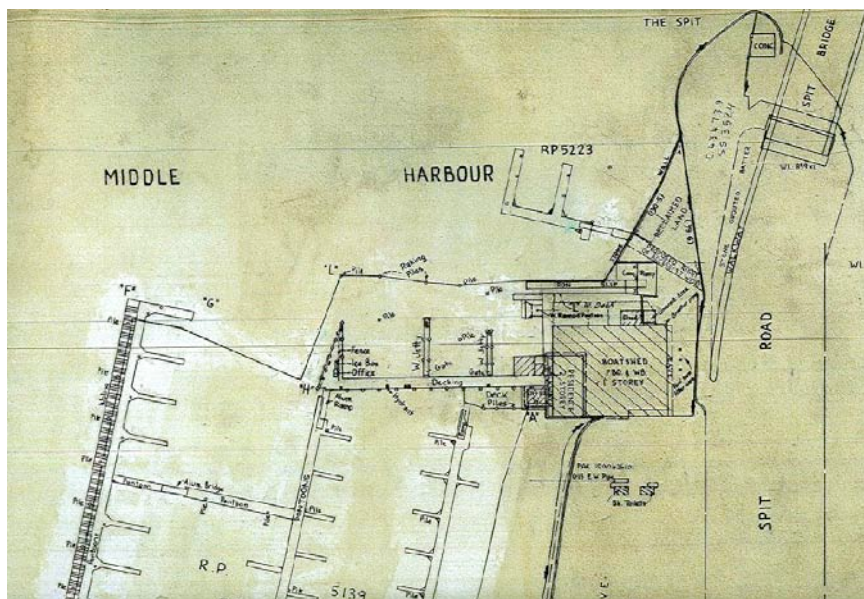


Figure 3.40: Chief
Surveyor & Land
Valuer (1973 – with
amendments to
1999) *Plan of The
Spit, Middle
Harbour,
Municipality of
Mosman, Parish of
Willoughby, County
of Cumberland.*¹⁰¹

⁹⁹ Manly Council Library, Image No. 006/006144

¹⁰⁰ Australian Photographic Agency Image No. 16835

¹⁰¹ NSW Maritime Authority, WL 232(A) XL

3.2 Site Formation & Development within the Study Area – Historic Overlays

The following section provides a series of historic overlays using plans of the study area from the early to mid 20th century. These images serve to present a visual depiction of site formation and development within the study area from the earliest known development since the 1910s to the 1990s. A detailed image of all identified European cultural structures overlying a plan of the proposed development at d'Albora Marina – thus showing their relation to the proposed works – is present in Section 6.0.

Early 20th century – 1920s construction of first Spit Bridge

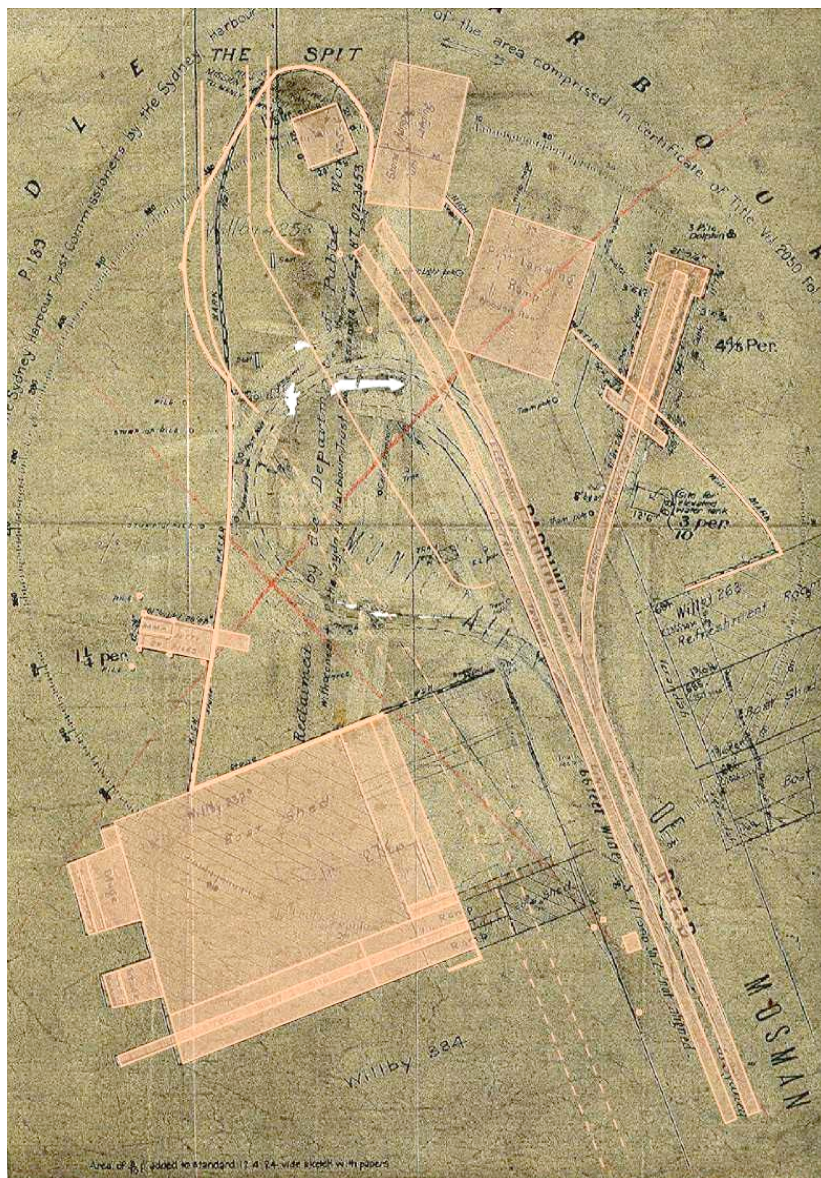


Figure 3.41 illustrates the development of The Spit from 1910 to 1929; including the resumption of a strip of land running along the western side of the original roadway to construct the new Spit Road and southern approach to the Spit Bridge. The overlay shows how this resulted in demolition of the two storey building at Lyons Boatshed, as well as the landward extent of the southern slip, and the subsequent construction of a much narrower "residence" building fronting the new Spit Road. The realignment of the northern curve of the western seawall is clearly demonstrated, as is the removal of the tram balloon loop and realignment of the northern extent of the tram lines.

Figure 3.41: Features on the 1929 surveyors plan of The Spit, overlain onto the 1910-1921 surveyors plan.

1920s – 1950s construction of second Spit Bridge

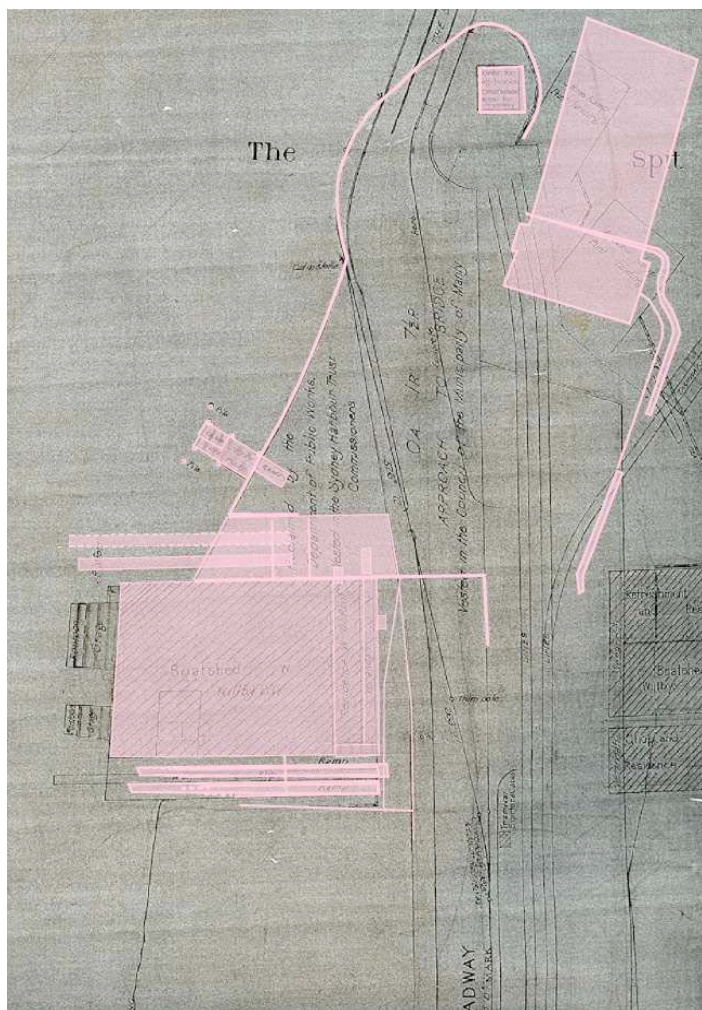


Figure 3.42 illustrates the development of The Spit from 1920s – 1950s; including the construction of the second Spit Bridge, decommissioning of the first Spit Bridge and removal of the tram service. Also clearly shown is the construction of the northern slip at Lyons Boatshed, the associated realignment of the southern extent of the western stone seawall, and the duplication of the southern slip rails. The construction of a landward timber ramp attached to the timber jetty situated to the north of Lyons Boatshed can also be seen in the overlay. It should be noted that the details of the western extent of the boatshed on the 1914-1959 plan (overlay) are not complete – the absence of the boat stages on the overlay does not indicate that they were removed.

Figure 3.42: Features on the 1914-1959 surveyors plan of The Spit, overlain onto 1929 surveyors plan.

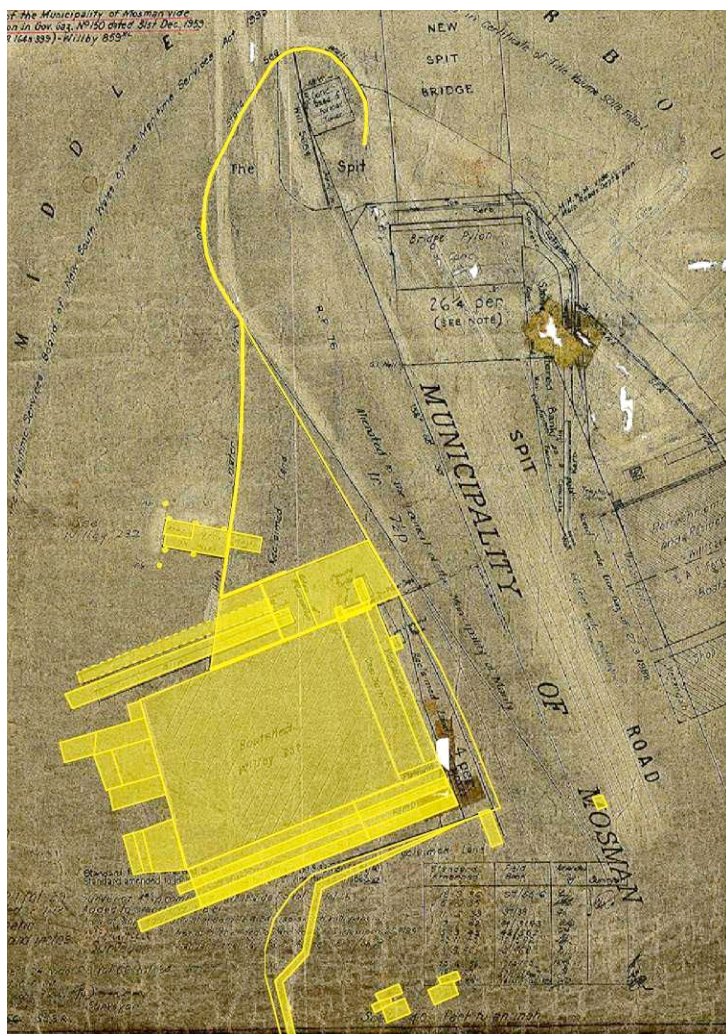


Figure 3.43, representing features from a 1924-1963 surveyors plan overlain on the earlier 1914-1953 plan illustrates the complete set of maritime structures associated with Lyons Boatshed by the early 1960s.

Figure 3.43: Features on the 1924-1963 surveyors plan of The Spit, overlain onto the 1914-1959 surveyors plan.

1960s – 1990s - demolition of Lyons Boatshed & construction of d'Albora Marina

Figure 3.44 shows the features of a surveyors plan dating from 1973 – 1999 overlain on the earlier surveyors plan dating to 1924-1963. This image clearly illustrates the relation of the d'Albora Marina structure to the earlier Lyons Boatshed. The new marina building overlies the footprint of Lyons Boatshed, however the d'Albora building is wider – extending further to the south and overshadowing the former southern slips, and slightly narrower. The fixed timber jetties and main marina arm of d'Albora marina overlie the former boat stages of Lyons Boatshed. The slips originally constructed to the north of Lyons Boatshed remain, however, the timber jetty north of these slips has been removed and a new fixed timber jetty, forming the northernmost arm of the marina, has been constructed.



Figure 3.44: Features on the 1924-1963 surveyors plan of The Spit, overlain onto the 1914-1959 surveyors plan.

3.3 Current Condition of d'Albora Marina, The Spit, December 2007

The d'Albora Marina at The Spit comprises a commercial and recreational marina with a floating marina with four berthing arms extending from the deck of the marina building complex, with a concrete slip and a single fixed jetty offering additional berths to the north. The marina building houses numerous businesses including restaurants and kiosks, boat sales and various boat repair, detailing and servicing businesses.

The landscape within, and surrounding, the Marina forms reclaimed land and substantial development exists up to, and including, the waterline.

Figures 3.45-3.60 below depict the condition of the structures and areas of the existing d'Albora Marina site relevant to the current archaeological assessment.



Figure 3.45: Concrete and iron slip (1930s) on the northern side of the boatshed – image facing NW. The slip remains in good condition.

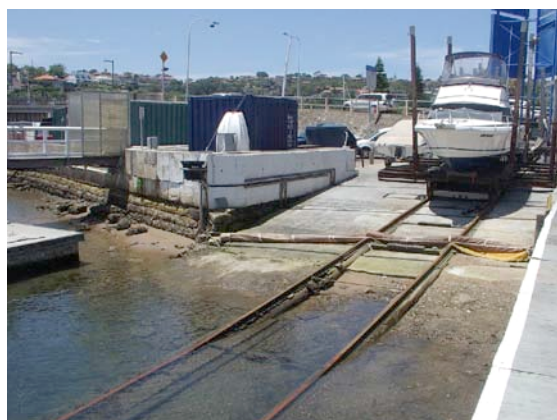


Figure 3.46: Concrete and iron slip (1930s) on the northern side of the boatshed – image facing NNE



Figure 3.47: Western stone seawall (originally constructed in 1900s-1910s),– image facing SSE. The stone seawall survives in good condition, despite the addition of the modern upper courses and the loss of some isolated stone blocks.



Figure 3.48: Western stone seawall (originally constructed in 1900s-1910s),– image facing NNE



Figure 3.49: Western stone seawall (originally constructed in 1900s-1910s), showing detail of original sandstone courses and modern massed concrete upper courses – image facing NNW



Figure 3.50: Detail of sandstone block in upper course of western stone seawall, showing peck marks



Figure 3.51: Site of former timber jetty and ramp (1900s-1910s) – largely underneath current fixed jetty - image facing NNE



Figure 3.52: Close up of the western section of the site of former timber jetty and ramp (1900s-1910s)– image facing NW



Figure 3.53: Site of former pontoon jetty (1930s-1950s) – foreground, and site of former timber jetty and ramp (1900s-1910s) – background (largely underneath current fixed jetty) – image facing SSE



Figure 3.54: View towards southern aspect of marina boatshed from Spit Reserve, showing concrete seawall, former location of public toilets and waiting room (1930s) and southern slip (adjacent to boatshed) – image facing N



Figure 3.55: View of relation between concrete seawall and current boatshed – the site of the former southern slip is located underneath the southern end of the current boatshed – image facing E



Figure 3.56: View towards southern aspect of marina boatshed, showing concrete seawall and site of former boatsheds (within footprint of current building) – image facing N



Figure 3.57: View underneath southern aspect of marina boatshed showing various phases of timber piles (some with concrete jackets) – image facing NNW



Figure 3.58: View towards northern aspect of marina building, showing northern slips in foreground and site of earlier boatsheds (within footprint of current building) – image facing SE



Figure 3.59: View underneath northern aspect of marina boatshed, incorporating part of the site of earlier boatsheds (within footprint of current building) showing various phases of timber piles –image facing SE

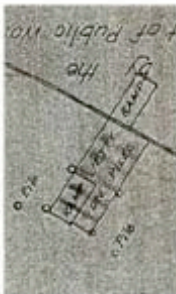



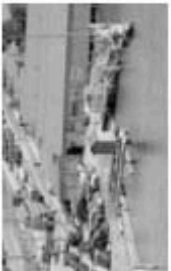





Figure 3.60: View across carpark and garbage disposal facilities on northern side of marina building – representing former site of tram lines / balloon loop (1910s) – image facing SW

3.4 Summary of Known & Potential European Historic Cultural Resources within the Study Area

The following section deals with European cultural features that are known to be present within the current study area as shown by archival plans, maps and photographs.

HISTORIC STRUCTURE, FEATURE OR DEPOSIT	DESCRIPTION	CURRENT CONDITION OF ITEM / LOCATION	POTENTIAL OF ARCHAEOLOGICAL REMAINS
<p>Tram lines / balloon loop 1910s</p>  	<p>Electric tram lines connecting the tram service from Sydney to the tramcar transfer jetty at The Spit. The specific section of tracks within the study area represents a tram turning circle or balloon loop.</p> <p>Descriptions of the tramways servicing The Spit state that the line was substantially railed with 60 lb and 80 lb T sectioned metals, 100 lb girder rails and crossing loops of 10 ft tracks. Associated features including tram poles and a tram office existed around the loop.</p>	<p>The tram balloon loop was decommissioned in the mid 1920s to facilitate the construction of the first Spit Bridge. Much of this area on the north-eastern side of The Spit was redeveloped and built up to enable construction of the approach to the new bridge. Further alteration occurred in the 1950s with the removal of the first bridge and the construction of the second Spit Bridge.</p> <p>The Spit Road, a bituminised car parking area, garbage disposal area and underground fuel storage facility associated with d'Albora Marina, now occupy the former location of the tram balloon loop.</p> 	<p>It is likely that the tram rails and associated tram poles were removed when the balloon loop was decommissioned. The type and condition of the foundations of the tram line – i.e. timber v. concrete – is unknown. If they were timber, it is likely that they were also removed. However, if they were concrete, it is possible that they still remain under the layers of later fill deposited on the site. Modern alteration to the locale, particularly the excavation of an existing underground fuel storage tank, would have disturbed and removed any remains of the balloon loop in certain locations, however, there is a potential for some remains of the loop to exist underneath fill in adjacent areas. There is also a potential for other artefact deposits associated with the operation and use of the tram service to exist within the fill in area.</p>
<p>Western stone seawall Original construction 1900-1910s</p>  	<p>Stone seawall encompassing the north-western extent of The Spit, extending south to d'Albora. The seawall is formed of rectangular sandstone blocks.</p>	<p>The alignment of part of northern curved section of the stone seawall was altered in the 1920s to facilitate the construction of the first Spit Bridge. Part of southern alignment also appears to have been removed / altered through modern foreshore development. Two courses of modern massed concrete blocks have also been laid along much of the length of the seawall, on top of the sandstone construction. This signifies the raising of the ground level in recent years.</p> 	<p>The stone seawall survives as standing feature – albeit with a slightly altered northern curve and alterations in the form of additional modern courses.</p>

HISTORIC STRUCTURE, FEATURE OR DEPOSIT	DESCRIPTION	CURRENT CONDITION OF ITEM / LOCATION	POTENTIAL OF ARCHAEOLOGICAL REMAINS
Timber jetty & ramp 1900-1910s  	<p>Originally constructed as a timber jetty on piles. In the early 1920s an attached timber ramp was constructed landward of the jetty. Images throughout the 1930s-1950s show some form of davit or derrick functioned on the jetty for a period of time.</p>	<p>The timber jetty was originally situated to the north of Lyons Boatshed and later d'Albora Marina boatshed. The structure apparently remained as a functioning jetty up to the 1970s but was removed sometime in the 1980s-early 1990s, presumably when the fixed timber jetty / D arm of d'Albora Marina was constructed.</p> 	<p>The timber superstructure of the jetty and ramp has obviously been demolished and a new timber jetty / marina arm constructed in its place. However, it is unlikely that all remains of the foundations / timber piles of the jetty were removed during the demolition of the structure or the construction of the new jetty. Stumps of the timber piles are likely to remain in the seabed; having been cut, sawn or snapped near the base rather than being pulled out. Some form of foundation, likely also to be timber pile stumps, of the timber ramp may also remain below ground level on the landward side of the stone sea wall. There is also the potential for artefact deposits associated with the use and operation of the jetty to be present on or within the seabed.</p>
Pontoon jetty 1930s-1950s  	<p>A pontoon jetty, that appears to have been formed of a line of barrels parallel to the shoreline and held in place by three timber gangways leading from the shore and braced by dolphins. The dolphins appear to have been in place since the 1910s-1920s. Images show that a timber shed was situated atop the pontoon, near the seaward end, for a few years during the late 1930s-1940s.</p>	<p>By the early 1950s, the pontoon jetty and associated shed, was removed. While the removal occurred around the same period as the second Spit Bridge was beginning to be constructed, no associated development occurred within the location of the pontoon jetty. It is likely that the structure simply ceased to serve its purpose. No modern redevelopment appears to have occurred within the seaward location of the jetty, however, the landward extent appears to have been raised and filled slightly.</p> 	<p>The superstructure of the pontoon jetty has obviously been removed, however, the jetty was braced by timber piles driven into the seabed. It is likely that these piles were cut, sawn or snapped near the level of the seabed rather than pulled out. As such, it is likely that the stumps remain. There is also the potential for artefact deposits associated with the use and operation of the jetty to be present on or within the seabed.</p>

HISTORIC STRUCTURE, FEATURE OR DEPOSIT	DESCRIPTION	CURRENT CONDITION OF ITEM / LOCATION	POTENTIAL OF ARCHAEOLOGICAL REMAINS
<p>Northern Slips 1930s</p>  	<p>The boat slips situated on the northern side of the boatshed originally (1930s) appear to have comprised two sets of iron rails laid on a concrete slab. Sometime during the 1940s-1950s, the two sets were removed and replaced with a single, wider iron slip.</p>	<p>The northern slip remains as a single set of iron rails on a concrete slab.</p>  <p>Image taken facing W Dec 2007</p>	<p>The northern boatshed remains as an existing structure in its original location and original broad configuration. Apart from the removal of the original two sets of rails and replacement with a single wider set, the only alteration to the slip appears to be low level maintenance works. Elements of the original two slips are unlikely to remain. However, artefact deposits associated with the use of the slips may be present on or within the seabed, beyond the toe of the slip.</p>
<p>Southern Slips 1910s – 1960s</p>  	<p>Iron slip on timber ramp boatshed at landward end. The slip originally (1910s-1920s) appears to have comprised a single set of rails, which was later removed and replaced with two sets sometime in the 1930s-1950s.</p>	<p>The southern slips were decommissioned sometime after the late 1970s when the d'Albora Marina boatshed was extended to the south.</p>  	<p>The original slip ramp was of timber construction and it is not clear in the historic record whether it was later replaced with concrete. Remains of both the timber and / or possible later concrete slabs are likely to be present under the southern extent of the existing boatshed.</p>

Additional Cultural Deposits – formed by accidental or deliberate discarding from jetties, boat ramps and recreational vessels

Various items – including personal items, food and drink containers – are often discarded or lost overboard from jetty structures and vessels. Such deposits are likely to have been somewhat disturbed through later development and use of the area. However, they are unlikely to have been removed or destroyed altogether.

Historic collections of such items, particularly heavier items, may have formed on the seabed in the vicinity of the d'Albora marina. Such a discard process would have been concentrated close to the location of historic jetties, ramps and stages, as well as a 'boats width' away from the structures – artefacts discarded from the seaward side of a vessel tied up to the wharf.

Shipwrecks –

A search of the NSW Maritime Heritage online database did not produce any references of shipwrecks in The Spit area. However, historic information indicates that this area was used mainly for recreational boating, with these vessels being generally small and privately owned and operated. Consequently, the documenting of boating accidents, scuttling, and wrecking would tend not to be entirely consistent or thorough and there is a possibility of wrecks, or remnants thereof, to be present within the region of The Spit.

Nonetheless, based on the landscape of the area, the probability of wrecks occurring is considered extremely low. It is quite possible that some vessels moored at The Spit over the last one hundred years may have foundered, but it is considered highly likely that such vessels would have been raised and recovered or removed due to the potential hazards to navigation.

Ultimately, the likelihood of shipwrecks being present in the current study area is considered to be very low.

4.0 ASSESSMENT OF SIGNIFICANCE

4.1 Significance Assessment

An assessment of cultural significance or heritage significance seeks to understand and establish the importance or value that a place, site or item may have to select communities and the general community at large. The Australian ICOMOS *Charter for the Conservation of Places of Cultural Significance* (the *Burra Charter* 1979, most recently revised in 1999), the standard adopted by most heritage practitioners in Australia when assessing significance, defines cultural significance as;

“Aesthetic, historic, scientific or social value for past, present or future generations”¹⁰²

This value may be contained in the fabric of the item, its setting and relationship to other items, the response that the item stimulates in those who value it now, or the meaning of that item to contemporary society.

Accurate assessment of the cultural significance of sites, places and items is an essential component of the NSW heritage assessment and planning process. A clear determination of a site's significance allows informed planning decisions to be made for place, in addition to ensuring that their heritage values are maintained, enhanced, or at least minimally affected by development.

Assessments of significance are made by applying standard evaluation criteria:

Historic Cultural Heritage Significance Criteria (NSW Heritage Branch Guidelines)

- a. *An item is important in the course or pattern of NSW's cultural or natural **history** (or the cultural or natural history of the local area);*
- b. *An item has strong or special **associations with the life or works of a person, or group of persons, of importance in NSW' cultural or natural history** (or the cultural or natural history of the local area);*
- c. *An item is important in demonstrating **aesthetic characteristics** and/or a high degree of **creative or technical achievement** in NSW (or the local area);*
- d. *An item has strong or special **associations with a particular community or cultural group** in NSW (or the local area) **for social, cultural or spiritual reasons;***
- e. *An item has **potential to yield information** that will contribute to an understanding of NSW's cultural or natural history (or the cultural or natural history of the local area);*
- f. *An item possesses **uncommon, rare or endangered** aspects of NSW's cultural or natural history (or the cultural or natural history of the local area);*
- g. *An item is important in **demonstrating the principal characteristics of a class of NSW's cultural or natural places;** or cultural and natural environments.¹⁰³*

¹⁰² Australia ICOMOS Inc. (1999) *The Burra Charter: The Australia ICOMOS Charter for Places of Cultural Significance 1999* – Article 1.2.

¹⁰³ NSW Heritage Office, July (2001) *Assessing Heritage Significance*.

4.2 Evaluation

The following section provides an evaluation of the cultural significance of the known and predicted archaeological sites, features and deposits within the current study area. The significance of certain components of the identified historic cultural resource at The Spit has previously been addressed as part of a Mosman Council heritage study¹⁰⁴ – a report that was referred to during the initial preparation of heritage listings in the *Mosman Local Environmental Plan 1998*. The significance statements contained within the heritage study are incorporated in the following evaluation.

4.2.1 Aboriginal Cultural Heritage Significance

No Aboriginal sites, items or places are known or predicted to occur within the boundaries of the current proposed development footprint, and no information regarding any social or cultural significance issues was provided by the Aboriginal stakeholders, Metropolitan Local Aboriginal Land Council (MLALC). Consequently, no assessment of Aboriginal cultural heritage significance has been conducted for this project.

4.2.2 Historic Cultural Heritage Significance

TRAM LINES / BALLOON LOOP 1910s –

- a. An item is important in the course or pattern of NSW's cultural or natural history (or the cultural or natural history of the local area);**

The construction of the electric tramway along The Spit in 1900 represents the first public transport service established to connect travelers to the punt across Middle Harbour. The balloon loop, built in the early 1910s, coincided with the construction of the Manly-Spit tram service and the tram transfer ferry across the harbour. As such, the tram loop reflects the demands placed on infrastructure to enable travel across Middle Harbour due to the rapid increase in occupants and visitors to the northern suburbs. The tram service and transfer ferry greatly improved transport between the south and north shores of the harbour and thus form an important component of the historical development of the local area.

By the standards of this criterion the tramlines / balloon loop can be considered to be of *Local* significance.

- b. An item has strong or special associations with the life or works of a person, or group of persons, of importance in NSW's cultural or natural history (or the cultural or natural history of the local area);**

Research into the construction of the tram lines / balloon loop has not revealed that the structure had any strong or special associations with the life or works of a person, or group of persons, of importance in the cultural history of NSW or the local area.

By the standards of this criterion the tramlines / balloon loop can be considered to be of *Minimal* significance.

¹⁰⁴ Godden Mackay Logan Pty Ltd (1996) *Mosman Heritage Study*. Unpublished report prepared for Mosman Municipal Council, NSW.

c. An item is important in demonstrating *aesthetic characteristics* and/or a high degree of *creative or technical achievement* in NSW (or the local area);

Such tram lines and loops were commonplace throughout Sydney during the early to mid 20th century. No technical or creative merits for this line come to light.

By the standards of this criterion the tramlines / balloon loop can be considered to be of *Minimal* significance.

d. An item has strong or special *associations with a particular community or cultural group* in NSW (or the local area) for *social, cultural or spiritual reasons*;

Research into the construction of the tram lines / balloon loop has not revealed that the structure had any strong or special associations with a particular community or cultural group in NSW or the local area.

By the standards of this criterion the tramlines / balloon loop can be considered to be of *Minimal* significance.

e. An item has *potential to yield information* that will contribute to an understanding of NSW's cultural or natural history (or the cultural or natural history of the local area);

Any surviving remains of the tram loop / balloon loop have the potential to provide information on the form and construction of electric tramways during the early 20th century – including the form and fabric of the tram line foundations. While the location and alignment of the balloon loop is relatively well documented in historical plans, our understanding of this structure would be enhanced through the archaeological recording of any physical remains. The examination of any associated artefacts that may have been deposited in the surrounding fill can also add to our understanding of early 20th century activities and transport facilities at The Spit.

As no examples of this balloon loop on either side of The Spit / Middle Harbour are visible or have been properly documented, the manner of the construction of the foundations and associated artefacts would have some archaeological value.

By the standards of this criterion the tramlines / balloon loop can be considered to be of *Local* significance.

f. An item possesses *uncommon, rare or endangered* aspects of NSW's cultural or natural history (or the cultural or natural history of the local area);

Segments of buried tram lines have previously been documented throughout Sydney, however, none have previously been recorded for the local region.

By the standards of this criterion the tramlines / balloon loop can be considered to be of *Local* significance.

g. An item is important in *demonstrating the principal characteristics of a class of NSW's cultural or natural places*; or cultural and natural environments.

Based on the prediction that very little of the tram balloon loop survives, the archaeological remains are unlikely to be representative of tram transport systems in NSW or the local area.

By the standards of this criterion the tramlines / balloon loop can be considered to be of *Minimal* significance.

WESTERN STONE SEAWALL 1900s – 1910s

a. An item is important in the course or pattern of NSW's cultural or natural history (or the cultural or natural history of the local area);

The western stone seawall was constructed in the 1900s-1910s, in conjunction with the first significant land reclamation and on the western side of The Spit. Essentially, the construction of this seawall enabled the first deposition and retention of large volumes of fill, thus significantly expanding the area of land at The Spit that could be developed. Construction of Lyons Boatshed and fast development of boating facilities at The Spit commenced shortly after the erection of this wall.

In the early 1920s, the alignment of the northern curve of the seawall was extended to facilitate the construction of the first Spit Bridge and southern approach. Since that time, the stone seawall has remained as a functioning structure in its largely original form and alignment.

By the standards of this criterion the western stone seawall can be considered to be of *Local* significance.

b. An item has strong or special *associations with the life or works of a person, or group of persons, of importance in NSW' cultural or natural history* (or the cultural or natural history of the local area);

Research into the construction of the western stone seawall has not revealed that the structure had any strong or special associations with the life or works of a person, or group of persons, of importance in the cultural history of NSW or the local area.

By the standards of this criterion the western stone seawall can be considered to be of *Minimal* significance.

c. An item is important in demonstrating *aesthetic characteristics* and/or a high degree of *creative or technical achievement* in NSW (or the local area);

The western stone seawall is not considered to possess a high degree of technical or creative achievement. However, the wall demonstrates early 20th century land reclamation and development of The Spit and forms one of the earliest standing structures associated with the early history of The Spit. As such, the wall possesses aesthetic values.

By the standards of this criterion the western stone seawall can be considered to be of *Local* significance.

d. An item has strong or special *associations with a particular community or cultural group* in NSW (or the local area) *for social, cultural or spiritual reasons*;

Research into the construction of the western stone seawall has not revealed that the structure had any strong or special associations with a particular community or cultural group in NSW or the local area.

By the standards of this criterion the western stone seawall can be considered to be of *Minimal* significance.

e. An item has *potential to yield information* that will contribute to an understanding of NSW's cultural or natural history (or the cultural or natural history of the local area);

The construction of the western stone seawall would be similar to numerous structural walls in the greater Sydney Harbour context and thus is unlikely to yield new information.

By the standards of this criterion the western stone seawall can be considered to be of *Minimal* significance.

f. An item possesses *uncommon, rare or endangered* aspects of NSW's cultural or natural history (or the cultural or natural history of the local area);

The western stone seawall is a relatively rare structure as it represents one of the earliest seawalls in the local area and demonstrates one of the earliest land reclamation processes in the middle reaches of Middle Harbour.

By the standards of this criterion the western stone seawall can be considered to be of *Local* significance.

g. An item is important in *demonstrating the principal characteristics of a class of NSW's cultural or natural places*; or cultural and natural environments.

The western stone seawall is representative of the early 20th century structures and phases of development – including both land reclamation and construction of maritime structures and facilities – at The Spit.

By the standards of this criterion the western stone seawall can be considered to be of *Local* significance.

TIMBER JETTY AND RAMP 1900-1910s

a. An item is important in the course or pattern of NSW's cultural or natural history (or the cultural or natural history of the local area);

The timber jetty was originally constructed in the 1900s-1910s; in c. early 1920s, an attached timber ramp was constructed landward of the jetty. Images throughout the 1930s-1950s show some form of davit or derrick function on the jetty for a period of time. The structure appears to have remained as a functioning jetty up to the 1970s but was removed and replaced with the fixed timber jetty / D arm of the current marina. As a timber jetty associated with a commercial and recreational boatshed, this structure would not normally be important in the course or pattern of the cultural history of the area. Such a jetty is representative of activities that were, and still are, commonplace in Middle Harbour by the early 20th

century – namely low intensity boat maintenance and water-based recreational pursuits. However, the construction of this jetty and ramp reflects the increasing use and development of boating facilities and recreational resources at The Spit and the local region.

There is also a possibility that this timber jetty and ramp served a more specialised function. Historical sources indicate that a “wharf” operated by the NSW Explosives Department throughout the early to mid 20th century existed at The Spit. This “wharf” was apparently as support infrastructure for explosives shipments and activities at the Bantry Bay Magazine, in operation from 1915-1974. The “wharf” at The Spit served as a mooring and loading structure for explosive launches, tugs and vessels transporting Explosives Department workers.

The specific location of The Spit explosives “wharf,” the dates of its operation and the nature of its construction – i.e. whether it was specifically constructed by the Explosives Department or was a pre-existing wharf commissioned for such use – are unknown. However, a heritage study of the Mosman local government area conducted in 1996,¹⁰⁵ suggests that the timber jetty and ramp previously located on the northern side of Lyons Boatshed actually represents the “explosives” wharf. This study was used as a reference report during the initial compilation of the heritage listings contained within the *Mosman Local Environmental Plan 1998* and the site of the former timber ramp and jetty has been listed on the LEP as the site of the former explosives “wharf.”

If the site does indeed represent the remains of the NSW Explosives Department “wharf” at The Spit, it does form a structure that is important in the course or pattern of the cultural history of the local region and forms a component of a greater complex of sites associated with the Bantry Bay magazine, which is important in the history and development of the greater Sydney region.

By the standards of this criterion the timber jetty and ramp can be considered to be of *Local* significance.

b. An item has strong or special associations with the life or works of a person, or group of persons, of importance in NSW’ cultural or natural history (or the cultural or natural history of the local area);

Research into the construction of the timber jetty and ramp has not revealed that the structure had any strong or special associations with the life or works of a person, or group of persons, of importance in the cultural history of NSW or the local area.

By the standards of this criterion the timber jetty and ramp can be considered to be of *Minimal* significance.

c. An item is important in demonstrating aesthetic characteristics and/or a high degree of creative or technical achievement in NSW (or the local area);

Research into the development of the timber jetty and ramp has not indicated that the structure was demonstrative of particular aesthetic characteristics, creative or technical achievement within the local area or in the broader Sydney region.

By the standards of this criterion the timber jetty and ramp can be considered

¹⁰⁵ Godden Mackay Logan Pty Ltd (1996)

to be of **Minimal** significance.

d. An item has strong or special *associations with a particular community or cultural group* in NSW (or the local area) for social, cultural or spiritual reasons;

If the timber jetty and ramp and The Spit explosives “wharf” are one and the same, the remains of the jetty can be said to have associations with the NSW Explosives Department workers employed at Bantry Bay and associated complex.

By the standards of this criterion the timber jetty and ramp can be considered to be of **Local significance.**

e. An item has *potential to yield information* that will contribute to an understanding of NSW’s cultural or natural history (or the cultural or natural history of the local area);

Any surviving remains of the timber jetty and ramp have the potential to provide information on at least two aspects of activities at The Spit during the early to mid 20th century: jetty construction and waterfront activities. While the development of the timber jetty and ramp is relatively well documented in historical plans, our understanding of this structure would be enhanced through the archaeological recording of any physical remains. The examination of any artefacts that may have been deposited on the seabed in the immediate vicinity of the southern slip – either accidentally or deliberately – can also add to our understanding of life and waterfront activities conducted at The Spit, and may also assist in a determination of whether this structure actually was the explosives “wharf.”

By the standards of this criterion the timber jetty and ramp can be considered to be of **Local significance.**

f. An item possesses *uncommon, rare or endangered* aspects of NSW’s cultural or natural history (or the cultural or natural history of the local area);

If the timber jetty and ramp does in fact represent the explosives “wharf”, it possesses rare aspects. These aspects, however, are unlikely to be of a physical nature, rather the structure would be considered to be historically uncommon.

By the standards of this criterion the timber jetty and ramp can be considered to be of **Local significance.**

g. An item is important in *demonstrating the principal characteristics of a class of NSW’s cultural or natural places*; or cultural and natural environments.

Based on the prediction that very little of the timber jetty and ramp survives, the archaeological remains are unlikely to be representative of jetties – even those possibly used by the NSW Department of Explosives - in NSW or the local area.

By the standards of this criterion the timber jetty and ramp can be considered to be of **Minimal significance.**

TIMBER PONTOON JETTY 1930s-1950s

a. An item is important in the course or pattern of NSW's cultural or natural history (or the cultural or natural history of the local area);

The timber pontoon jetty, situated to the north of Lyons Boatshed, appears to have existed only for a period of approximately twenty years. The form and construction of the jetty further indicates that the structure was of an ephemeral nature. Historical research conducted for this assessment has not revealed the specific function of this structure and it does not appear to have been built during any particular period of works at The Spit – i.e. reclamation or construction of the second Spit Bridge. Nonetheless, it is assumed that the jetty served as a simple supplementary structure, possibly associated with activities at Lyons Boatshed. However, there is also a possibility that this structure represented the explosives “wharf” at The Spit, rather than the timber jetty and ramp.

If this pontoon jetty was not employed for use as an explosive “wharf”, it would be representative of activities that were, and still are, commonplace in Middle Harbour by the early – mid 20th century – namely, low intensity boat maintenance and water-based recreational pursuits. However, the construction of the “temporary” jetty reflects the increasing use and development of boating facilities and recreational resources at The Spit and the local region.

If the pontoon jetty does represent the former NSW Explosives Department “wharf” at The Spit, it does form a structure that is important in the course or pattern of the cultural history of the local region and forms a component of a greater complex of sites associated with the Bantry Bay magazine, which is important in the history and development of the greater Sydney region.

By the standards of this criterion the timber pontoon jetty can be considered to be of *Local* significance.

b. An item has strong or special *associations with the life or works of a person, or group of persons, of importance in NSW' cultural or natural history* (or the cultural or natural history of the local area);

Research into the construction of the timber pontoon jetty has not revealed that the structure had any strong or special associations with the life or works of a person, or group of persons, of importance in the cultural history of NSW or the local area.

By the standards of this criterion the timber pontoon jetty can be considered to be of *Minimal* significance.

c. An item is important in demonstrating *aesthetic characteristics* and/or a high degree of *creative or technical achievement* in NSW (or the local area);

The pontoon jetty does not appear to display a high degree of creative or technical achievement.

By the standards of this criterion the timber pontoon jetty can be considered to be of *Minimal* significance.

d. An item has strong or special *associations with a particular community or cultural group* in NSW (or the local area) *for social, cultural or spiritual reasons*;

Research into the construction of the timber pontoon jetty has not revealed that the structure had any strong or special associations with a particular community or cultural group in NSW or the local area.

By the standards of this criterion the timber pontoon jetty can be considered to be of *Minimal* significance.

e. An item has *potential to yield information* that will contribute to an understanding of NSW's cultural or natural history (or the cultural or natural history of the local area);

Any surviving remains of the timber pontoon jetty has the potential to provide information on the form and function of this structure. This structure has not been documented on any identified historic plans and its specific form, function or purpose is currently unknown. An examination of any artefacts that may have been deposited on the seabed in the immediate vicinity of the "temporary" jetty – either accidentally or deliberately – could contribute to our understanding of the activities that may have been associated with this structure.

By the standards of this criterion the timber pontoon jetty can be considered to be of *Local* significance.

f. An item possesses *uncommon, rare or endangered* aspects of NSW's cultural or natural history (or the cultural or natural history of the local area);

The timber pontoon jetty represents a very common structure type throughout Sydney.

By the standards of this criterion the timber pontoon jetty can be considered to be of *Minimal* significance.

g. An item is important in *demonstrating the principal characteristics of a class of NSW's cultural or natural places*; or cultural and natural environments.

Based on the prediction that very little of the timber pontoon jetty survives, the archaeological remains are unlikely to be representative of jetties – even those possibly used by the NSW Department of Explosives - in NSW or the local area.

By the standards of this criterion the timber pontoon jetty can be considered to be of *Minimal* significance.

NORTHERN SLIPS 1930s

a. An item is important in the course or pattern of NSW's cultural or natural *history* (or the cultural or natural history of the local area);

The construction and development of the northern slips is representative of activities that were, and still are, commonplace across Middle Harbour; namely low intensity boat maintenance and water-based recreational activities. However, the construction and development of the northern slips demonstrates the

increasing use of boating facilities at The Spit and development of infrastructure to service increasing requirements at The Spit and local region.

By the standards of this criterion the northern slips can be considered to be of *Local* significance.

*b. An item has strong or special **associations with the life or works of a person, or group of persons, of importance in NSW' cultural or natural history** (or the cultural or natural history of the local area);*

Research into the construction of the northern slips has not revealed that the structure had any strong or special associations with the life or works of a person, or group of persons, of importance in the cultural history of NSW or the local area.

By the standards of this criterion the northern slips can be considered to be of *Minimal* significance.

*c. An item is important in demonstrating **aesthetic characteristics** and/or a high degree of **creative or technical achievement** in NSW (or the local area);*

The northern slips would not be considered to possess a high degree of creative or technical achievement, or aesthetic characteristics.

By the standards of this criterion the northern slips can be considered to be of *Minimal* significance.

*d. An item has strong or special **associations with a particular community or cultural group** in NSW (or the local area) **for social, cultural or spiritual reasons**;*

Research into the construction of the northern slips has not revealed that the structure had any strong or special associations with a particular community or cultural group in NSW or the local area.

By the standards of this criterion the northern slips can be considered to be of *Minimal* significance.

*e. An item has **potential to yield information** that will contribute to an understanding of NSW's cultural or natural history (or the cultural or natural history of the local area);*

The existing structure of the northern slips has the potential to provide information on at least two aspects of activities at The Spit during the early to mid 20th century: slip construction and waterfront activities. While the development of the southern slips is relatively well documented in historical plans, our understanding of this structure would be enhanced through the archaeological recording of any physical remains. The examination of any artefacts that may have been deposited on the seabed in the immediate vicinity of the northern slip – either accidentally or deliberately – can also add to our understanding of life and waterfront activities conducted at The Spit.

By the standards of this criterion the northern slips can be considered to be of *Local* significance.

f. *An item possesses **uncommon, rare or endangered** aspects of NSW's cultural or natural history (or the cultural or natural history of the local area);*

The northern slip represents a common type of structure throughout Sydney.

By the standards of this criterion the northern slips can be considered to be of *Minimal* significance.

g. *An item is important in **demonstrating the principal characteristics of a class of NSW's cultural or natural places**; or cultural and natural environments.*

The fact that the northern slips survives in a good condition means that the structure can be considered a well preserved example of early to mid 20th slips in the Middle Harbour region.

By the standards of this criterion the northern slips can be considered to be of *Local* significance.

SOUTHERN SLIPS 1910s-1930s

a. *An item is important in the course or pattern of NSW's cultural or natural **history** (or the cultural or natural history of the local area);*

The construction and development of the southern slips is representative of activities that were, and still are, commonplace across Middle Harbour; namely low intensity boat maintenance and water-based recreational activities. However, the construction and development of the southern slips demonstrates the increasing use of boating facilities at The Spit and development of infrastructure to service increasing requirements at The Spit and local region.

By the standards of this criterion the southern slips can be considered to be of *Local* significance.

b. *An item has strong or special **associations with the life or works of a person, or group of persons, of importance in NSW' cultural or natural history** (or the cultural or natural history of the local area);*

Research into the construction of the southern slips has not revealed that the structure had any strong or special associations with the life or works of a person, or group of persons, of importance in the cultural history of NSW or the local area.

By the standards of this criterion the southern slips can be considered to be of *Minimal* significance.

c. *An item is important in demonstrating **aesthetic characteristics** and/or a high degree of **creative or technical achievement** in NSW (or the local area);*

The southern slips do not appear to demonstrate a high degree of creative or technical achievement.

By the standards of this criterion the southern slips can be considered to be of *Minimal* significance.

d. An item has strong or special *associations with a particular community or cultural group* in NSW (or the local area) *for social, cultural or spiritual reasons*;

Research into the construction of the southern slips has not revealed that the structure had any strong or special associations with a particular community or cultural group in NSW or the local area.

By the standards of this criterion the southern slips can be considered to be of *Minimal* significance.

e. An item has *potential to yield information* that will contribute to an understanding of NSW's cultural or natural history (or the cultural or natural history of the local area);

The physical remains of the southern slips has the potential to provide information on at least two aspects of activities at The Spit during the early to mid 20th century: slip construction and waterfront activities. While the development of the southern slips is relatively well documented in historical plans, our understanding of this structure would be enhanced through the archaeological recording of any physical remains. Of particular interest would be to note the construction fabric and technique of the foundations of the southern slip – particularly as the structure is primarily of timber construction (i.e. as opposed to the more modern method of the construction of a concrete slab base). The examination of any artefacts that may have been deposited on the seabed in the immediate vicinity of the southern slip – either accidentally or deliberately – can also add to our understanding of life and waterfront activities conducted at The Spit.

By the standards of this criterion the southern slips can be considered to be of *Local* significance.

f. An item possesses *uncommon, rare or endangered* aspects of NSW's cultural or natural history (or the cultural or natural history of the local area);

The southern slips represent a common structure type throughout Sydney, however, the possibility of the remains of early 20th century slip structures means the site could be considered relatively uncommon.

By the standards of this criterion the southern slips can be considered to be of *Local* significance.

g. An item is important in *demonstrating the principal characteristics of a class of NSW's cultural or natural places*; or cultural and natural environments.

Based on the prediction that relatively little of the southern slips survive, the archaeological remains are unlikely to be representative of slips within NSW or the local area.

By the standards of this criterion the southern slips can be considered to be of *Minimal* significance.

EARLIER BOATSHEDS 1890s – LYONS BOATSHED 1900s-1910s

a. An item is important in the course or pattern of NSW's cultural or natural history (or the cultural or natural history of the local area);

The construction and development of the earlier boatsheds is representative of activities that were, and still are, commonplace across Middle Harbour; namely low intensity boat maintenance and water-based recreational activities. However, alteration and redevelopment of these buildings reflects the increasing popularity and use of boating facilities at The Spit and the local area. Furthermore, several of the alterations of the earlier boatsheds were the result of changes in the surrounding landscape – i.e. land reclamation and construction of the Spit bridges. Consequently, the different phases of boatsheds on the site reflect the broader changes and developments at The Spit, representative of increasing land and transport demands due to pressures of residential growth in the broader region.

By the standards of this criterion the remains of the earlier boatsheds can be considered to be of *Local* significance.

b. An item has strong or special associations with the life or works of a person, or group of persons, of importance in NSW' cultural or natural history (or the cultural or natural history of the local area);

The Lyons Boatshed c. 1910s-1970s, has obvious associations with the Lyons family who are known to have had an investment in The Spit since the 1890s, if not earlier. The name of Lyons became strongly associated with boating services and facilities at The Spit throughout the early to mid 20th century.

By the standards of this criterion the remains of the earlier boatsheds can be considered to be of *Local* significance.

c. An item is important in demonstrating aesthetic characteristics and/or a high degree of creative or technical achievement in NSW (or the local area);

Any remains of the earlier boatshed are unlikely to possess a high degree of creative or technical achievement.

By the standards of this criterion the remains of the earlier boatsheds can be considered to be of *Minimal* significance.

d. An item has strong or special associations with a particular community or cultural group in NSW (or the local area) for social, cultural or spiritual reasons;

Research into the construction of the earlier boatsheds has not revealed that the structure had any strong or special associations with a particular community or cultural group in NSW or the local area.

By the standards of this criterion the remains of the earlier boatsheds can be considered to be of *Minimal* significance.

e. An item has *potential to yield information* that will contribute to an understanding of NSW's cultural or natural history (or the cultural or natural history of the local area);

The physical remains of earlier phases of boatsheds on the site of d'Albora Marina have the potential to provide information on at least two aspects of activities at The Spit during the late 19th to mid 20th century: boatshed construction and waterfront activities. While the broad development of the 20th century boatsheds is relatively well documented in historic plans, our understanding of these structures would be enhanced through the archaeological recording of any physical remains. Of particular interest would be to note the construction fabric and technique of the foundations of the boatsheds. The configuration of the foundations may also provide additional evidence on the shape and design of the former boatsheds – a level of detail that is not recorded on historic plans. The examination of any artefacts that may have been deposited on the seabed and in land reclamation in the immediate vicinity of the former boatshed phases – either accidentally or deliberately – can also add to our understanding of life and waterfront activities conducted at The Spit and local region.

By the standards of this criterion the remains of the earlier boatsheds can be considered to be of *Local* significance.

f. An item possesses *uncommon, rare or endangered* aspects of NSW's cultural or natural history (or the cultural or natural history of the local area);

Any remains of the earlier boatsheds would represent a common feature throughout Sydney.

By the standards of this criterion the remains of the earlier boatsheds can be considered to be of *Minimal* significance.

g. An item is important in *demonstrating the principal characteristics of a class of NSW's cultural or natural places*; or cultural and natural environments.

Based on the prediction that relatively little of the earlier boatsheds survive, the archaeological remains are unlikely to be representative of maritime facilities within NSW or the local area.

By the standards of this criterion the remains of the earlier boatsheds can be considered to be of *Minimal* significance.

WAITING ROOM, PUBLIC TOILETS & CONCRETE SEAWALL 1930s

a. An item is important in the course or pattern of NSW's cultural or natural *history* (or the cultural or natural history of the local area);

Research into the construction of the waiting room, public toilets and concrete seawall has not revealed that any of these structures were important in the course or pattern of the cultural history of NSW or the local area.

By the standards of this criterion the waiting room, public toilets and concrete seawall can be considered to be of *Minimal* significance.

b. An item has strong or special *associations with the life or works of a person, or group of persons, of importance in NSW' cultural or natural history* (or the cultural or natural history of the local area);

Research into the construction of the waiting room, public toilets and concrete seawall has not revealed that any of these structures had any strong or special associations with the life or works of a person, or group of persons, of importance in the cultural history of NSW or the local area.

By the standards of this criterion the waiting room, public toilets and concrete seawall can be considered to be of *Minimal* significance.

c. An item is important in demonstrating *aesthetic characteristics* and/or a high degree of *creative or technical achievement* in NSW (or the local area);

Any remains of the waiting room, public toilets or concrete seawall are unlikely to possess a high degree of creative or technical achievement.

By the standards of this criterion the waiting room, public toilets and concrete seawall can be considered to be of *Minimal* significance.

d. An item has strong or special *associations with a particular community or cultural group* in NSW (or the local area) *for social, cultural or spiritual reasons*;

Research into the construction of the waiting room, public toilets and concrete seawall has not revealed that the any of these structures had any strong or special associations with a particular community or cultural group in NSW or the local area.

By the standards of this criterion the waiting room, public toilets and concrete seawall can be considered to be of *Minimal* significance.

e. An item has *potential to yield information* that will contribute to an understanding of NSW's cultural or natural history (or the cultural or natural history of the local area);

Any remains of the waiting room, public toilets or concrete seawall are unlikely to provide any information that will contribute to an understanding of the history of the area.

By the standards of this criterion the waiting room, public toilets and concrete seawall can be considered to be of *Minimal* significance.

f. An item possesses *uncommon, rare or endangered* aspects of NSW's cultural or natural history (or the cultural or natural history of the local area);

The waiting room, public toilets and concrete seawall all represent common structures throughout Sydney.

By the standards of this criterion the waiting room, public toilets and concrete seawall can be considered to be of *Minimal* significance.

g. An item is important in *demonstrating the principal characteristics of a class of NSW's cultural or natural places*; or cultural and natural environments.

Based on the prediction that relatively little of the waiting room, public toilets and concrete seawall survives, the archaeological remains are unlikely to be representative within NSW or the local area.

By the standards of this criterion the waiting room, public toilets and concrete seawall can be considered to be of *Minimal* significance.

4.3 Statement of cultural significance

The collection of archaeological sites features and deposits identified within the current study area provides evidence of the variety of maritime structures and numerous phases of land reclamation and construction at The Spit since the early 20th century. Their cultural significance lies largely in their role in the historical development of The Spit – and thus also in the residential growth of the broader region of Middle Harbour and increasing popularity of water-based activities – and in their potential to provide information regarding the waterfront activities conducted at the Spit over the last century. This significance is accentuated by the fact that The Spit is a Sydney landmark, well known as a popular location for recreational boating across New South Wales.

5.0 IMPACT OF THE PROPOSED DEVELOPMENT

5.1 Introduction

The following section identifies the impact of the proposed works on the cultural heritage significance of the known and potential archaeological resources within the current study area. Refer to Section 1.2 for a detailed description of the proposed works. Based on the NSW Heritage Office and Department of Urban Affairs & Planning publication (2002) *Statements of Heritage Impact*, a Statement of Heritage Impact must address a number of questions relevant to the proposed works with regards to items of cultural significance. These questions help to ascertain whether all options have been explored prior to the proposed works or actions taking place, and to determine how the heritage value of an item can be conserved, or preferably enhanced, by the proposed development.

The impacts of the proposed development on each identified cultural item and / or deposit within the study area are dealt with separately in the following sections and the relevant questions addressed where appropriate.

5.2 Impact Assessment

5.2.1 Aboriginal Cultural Heritage Resource

No previously identified Aboriginal cultural heritage sites, items or places occur within the study area, and the Aboriginal archaeological assessment presented in Section 2 has determined that the likelihood of unidentified Aboriginal archaeological sites or items to occur within the boundaries of the proposed development at d'Albora Marina is extremely low. Consequently, it may be stated that the proposed development will not have a physical impact on the significance of the Aboriginal cultural resource of The Spit and surrounding locale.

5.2.2 Historic Cultural Heritage Resource

Figure 5.1 below presents the traced outlines of the phases of identified European structures and features previously present at The Spit (as taken from early-mid 20th century surveyors plans), overlying the current proposed development. This image shows the physical relation of the proposed works to the identified European historic cultural resource.



Figure 5.1: outline of the phases of early to mid 20th century European structures and features depicted on surveyors plans, overlain on current proposed development plan.

▪ TRAM LINES / BALLOON LOOP 1910S

Any remains of the tram lines / balloon loop that may survive would be situated under the early-mid 20th century fill deposited on the site. The only component of the

proposed development that may have a physical impact on these remains includes the excavation for an additional underground fuel storage. This excavation would serve to disturb and remove any archaeological remains and deposits associated with the tram lines within the proposed location of the fuel storage. However, the extent of the land disturbance would cause impact to only a small proportion of the potential remains of the tram lines / balloon loop.

Based on the assessed significance of the item in conjunction with the small and targeted nature of the impacts (i.e. within the footprint of the proposed fuel tank only), it is determined that the proposed development will have a **minor impact** on the cultural value of the tram lines / balloon loop.

▪ **WESTERN STONE SEAWALL 1900s-1910s**

The western stone seawall survives as a standing feature and continues to perform its original function. The northern curve of the wall was altered and expanded slightly in the early 1920s to facilitate the construction of the first Spit Bridge, and part of the southern extent of the wall appears to have been removed / altered through modern foreshore – marina development. However, the remainder of the seawall appears to survive in its original form, fabric and alignment. Two courses of modern massed concrete blocks have been laid along the top of much of the seawall, however, this addition is reversible and does not appear to have effected the physical construction of the historic seawall.

The proposed development will not have any direct physical impact on the western stone seawall. The proposed construction of a hardstand on the northern side of the existing boatshed building, however, represents new development in the vicinity of the seawall. The construction of the hardstand has been designed not to effect the physical integrity of the historic seawall, consequently it is determined that the proposed development **will not** impact on the archaeological value of the wall.

▪ **TIMBER JETTY AND RAMP 1900s-1910s**

The timber jetty, constructed in the 1900s-1910s and associated ramp, built in the 1920s apparently remained as a functioning jetty up to the 1970s but was removed sometime in 1980s-early 1990s, presumably when the fixed timber jetty / D arm of d'Albora Marina was constructed. While the superstructure has been entirely removed, it is possible that the timber pile foundations of the structure were cut, sawn or snapped off near seabed level rather than being pulled out. Consequently, it is anticipated that the remains of some of these pile stumps survive within the seabed. It has also been determined that there is a potential for artefact deposits associated with the use and operation of this jetty to be present on the seabed in the immediate vicinity.

The location of the proposed concrete hardstand to the north of the existing boatshed building overshadows the location of the former timber jetty and ramp and any surviving remains thereof. As such, any impacts delivered onto the seabed through the construction of this hardstand have a potential to impact the cultural remains of this timber jetty. However, due to the proposed style of construction of the hardstand, impacts to the seabed are of a dispersed and relatively minor nature. At present, it is proposed that the hardstand will be supported over c. 500mm diameter concrete piles laid over a grid of c. 6m x 6m. The likelihood of the new piles directly impacting on any physical remains of the timber jetty and associated deposits is small. Even if one or a

few of the new piles do cause impact to the former site of the jetty, it is determined that such impact will be low – i.e. effecting <10% of the archaeological site and deposit.

Based on the assessed significance of the item in conjunction with the small and targeted nature of the impacts (i.e. within the immediate footprint of spaced piles), it is determined that the proposed development will have a **minor impact** on the cultural value of the former timber jetty and ramp.

▪ **TIMBER PONTOON JETTY 1930S**

The timber pontoon jetty, constructed c. 1930s, was removed sometime during the 1950s. As with the timber jetty and ramp discussed above, while the superstructure of the “temporary” jetty has been entirely removed, it is possible that the timber pile foundations of the structure were cut or sawn near seabed level, rather than being pulled out. It is thus anticipated that the remains of these pile stumps survive within the seabed, as well as possible artefact deposits associated with the use and operation of the jetty.

The location of the proposed concrete hardstand to the north of the existing boatshed building overshadows the former location of the “temporary” timber jetty and any surviving remains thereof. As such, any impacts delivered onto the seabed through the construction of this hardstand have a potential to impact the cultural remains of this timber jetty. However, due to the proposed style of construction of the hardstand, impacts to the seabed are of a dispersed and relatively minor nature. At present, it is proposed that the hardstand will be supported over c. 500mm diameter concrete piles laid over a grid of c. 6m x 6m. The likelihood of the new piles directly impacting any physical remains of the timber jetty and associated deposits is small. Even if one or a few of the new piles does cause impact to the former site of the “temporary” jetty, it is determined that such impact will be low – i.e. effecting <10% of the archaeological site and deposit.

Based on the assessed significance of the item in conjunction with the small and targeted nature of the impacts (i.e. within the immediate footprint of spaced piles), it is determined that the proposed development will have a **minor impact** on the cultural value of the former “temporary” timber jetty.

▪ **NORTHERN SLIPS 1930S**

The boat slip situated on the northern side of the boatshed building remains as an existing structure in its original location and original broad configuration. Partial alterations to this structure, originally built in the 1930s, have included replacement of the original two sets of iron rails with a single, wider iron slip sometime during the 1940s-1950s, and small scale maintenance works. This slip has been assessed to be of some local cultural significance based on its role in the history of development at The Spit and its potential to yield some levels of archaeological information.

The proposed redevelopment of d'Albora Marina involves the demolition and removal of this structure (which is no longer in use) to facilitate the construction of the proposed concrete hardstand.

When the demolition of a building or structure of heritage value is proposed, a number of specific issues must be addressed;

- *Have all options for retention and adaptive re-use been explored?*

- *Can all of the significant elements of the heritage item be kept and any new development be located elsewhere on the site?*
- *Is demolition essential at this time or can it be postponed in case future circumstances make its retention and conservation more feasible?*¹⁰⁶

Based on the proposed style of construction of the hardstand, it would appear that the removal of the northern slip is preferable rather than essential. Thus options for retention and adaptive re-use do exist.

However, it is suggested that the assessed level of significance of this structure does not warrant its retention or re-use – particularly in light of the fact that if it were retained it would be adversely impacted and obscured by the proposed hardstand.

The significant elements of the northern slips include its historical role, physical information on its construction and any associated archaeological deposits. A series of mitigation measures, including archaeological documentation and recording prior to removal, are proposed to reduce the impact of the slips removal on the significance of the broader cultural resource of The Spit (refer to Section 7.0).

Based on the assessed significance of the item in conjunction with the implementation of a series of mitigation measures, it is determined that the removal of the northern slips as part of the proposed development will have a **minor impact** on the significance of the cultural heritage resources of The Spit.

▪ **SOUTHERN SLIPS 1910S**

The boat slip situated on the southern side of the boatshed building, originally constructed c. 1910s, comprised an iron slip on timber ramp covered with a small timber boatshed at the landward end. In the 1930s-1950s, the structure was altered with two sets of iron slips being laid. The southern slip remained in operation until sometime after the late 1970s, when it was largely removed to facilitate the extension of the d'Albora Marina boatshed to the south. It is unlikely, however, that the foundations and / or timber piles of the structure were entirely removed during its demolition. As such, it is expected that remains of these foundations and / or timber piles survive within the seabed.

The redevelopment of the boatshed building at d'Albora Marina overshadows the location of the former southern slips and any surviving remains or associated deposits thereof. As such, any impacts delivered onto the seabed through the redevelopment of the boatshed have a potential to impact the cultural remains of the southern slips.

However, due to the proposed style of demolition of the existing boatshed and construction of the new boatshed, impacts to the seabed are of a dispersed and relatively minor nature. At present, with regards to the demolition of the existing boatshed, it is proposed that all the existing piles that fall within the footprint of the proposed new boatshed will be cut or sawn off at their bases, close to the seabed, rather than removed. This serves to minimise disturbance to the seabed. The new boatshed will be supported over spaced piles. There is a small likelihood that the new piles will directly impact physical remains of the southern slips and associated deposits, however, the level of impact is predicted to be low.

Notwithstanding, the remains of the southern slips have been assessed to be of some local cultural significance based on their role in the historical development of The Spit

¹⁰⁶ NSW Heritage Office and Department of Urban Affairs & Planning publication (2002)

and their potential to yield archaeological information regarding their construction and associated activities.

Aspects of this significance could be salvaged through a series of mitigation measures – including above water archaeological documentation underneath the existing boatshed in the former location of the southern slips (refer to Section 7.0).

Based on the assessed significance of the item in conjunction with the implementation of a series of mitigation measures, it is determined that the proposed development will have a **minor impact** on the cultural value of the former southern slips.

▪ **EARLIER BOATSHEDS 1890s – LYONS BOATSHED**

A first boatshed on the NE side of The Spit was constructed in 1890. In the 1910s, this structure was demolished and following reclamation of the NE section of The Spit and construction of the stone seawall, a new boatshed was constructed – Lyons Boatshed. A series of alterations, expansions and redevelopments were carried out at Lyons Boatshed throughout the 20th century until the 1970s when the buildings were demolished and the current d'Albora Marina boatshed constructed. It is considered unlikely that various phases of construction and redevelopment of the boatsheds and associated facilities (boat stages etc) at the site resulted in the complete removal of foundations / timber piles – rather they are more likely to have been sawn or cut at their bases. As such, it is expected that some of the remains of these foundations and / or timber piles stumps survive within the seabed and land reclamation. There is also a potential for artefact deposits associated with the use and operation of the boatsheds to survive on the seabed and land reclamation in the immediate vicinity.

The redevelopment of the boatshed building at d'Albora Marina overshadows the location of the former boatshed constructions and any surviving remains or associated deposits thereof. Consequently, any impacts delivered onto the seabed through the redevelopment of the boatshed have a potential to impact the cultural remains of these earlier boatsheds.

However, due to the proposed style of demolition of the existing boatshed and construction of the new boatshed, impacts to the seabed are of a dispersed and relatively minor nature. At present, with regards to the demolition of the existing boatshed, it is proposed that all the existing piles that fall within the footprint of the proposed new boatshed will be cut or sawn off at their bases, close to the seabed, rather than removed. This serves to minimise disturbance to the seabed. The new boatshed will be supported over spaced piles. There is a small likelihood that the new piles will directly impact physical remains of the earlier boatsheds and any associated deposits, however, the level of impact is predicted to be low.

Notwithstanding, the remains of the earlier boatsheds have been assessed to be of some local cultural significance based on their role in the historical development of The Spit and their potential to yield archaeological information regarding their construction and associated activities.

Aspects of this significance could be salvaged through a series of mitigation measures – including above water archaeological documentation underneath the existing boatshed in the location of the earlier boatsheds (refer to Section 7.0).

Based on the assessed significance of the item in conjunction with the implementation of a series of mitigation measures, it is determined that the proposed development will have a **minor impact** on the cultural value of the former southern slips.

6.0 HERITAGE LISTINGS & STATUTORY REQUIREMENTS

6.1 Heritage Listings within the Subject Site

In NSW there are various types of statutory listings for cultural heritage sites, objects and places:

- Listing on the heritage schedule of a council's environmental planning instrument, e.g. *Local Environmental Plan* (LEP);
- Listing on a State agency's Heritage & Conservation Register;
- Inclusion on the DECCW AHIMS (Aboriginal sites only);
- State listing on the NSW Heritage Branch State Heritage Register, and;
- National listing on the Australian Government National Heritage List or Commonwealth Heritage List.

Statutory registers provide legal protection for heritage items; in NSW this protection generally comes from the NSW *Heritage Act 1977*, the NSW *National Parks & Wildlife Act 1974*, the NSW *Environmental Planning & Assessment Act 1979*, and the Federal *Environmental Protection & Biodiversity Conservation Act 1999*.

Cultural heritage may also be listed on one of two non-statutory registers – including the Register of the National Estate and the Register of the National Trust of Australia. These registers carry no legal force, however, they are public recognitions of the value of heritage sites or items to the Australian community.

SUMMARY OF HERITAGE LISTINGS WITHIN THE SUBJECT SITE – d'Albora Marina

HERITAGE REGISTER	ASSOCIATED STATUTORY PROTECTION	LISTINGS WITHIN THE SUBJECT SITE
Australian Heritage Database	<i>Environment Protection and Biodiversity Conservation Act 1999</i>	No listed sites or items
NSW DECCW AHIMS	<i>NSW National Parks & Wildlife Act 1974</i> .	No listed sites or items. * Metropolitan Local Aboriginal Land Council did not provide any additional information following consultation.
State Heritage Register	<i>NSW Heritage Act 1977</i>	No listed sites or items on the SHR. However, archaeological remains of structures and / or any associated deposits are identified as a "relics" under the Act.
<i>Sydney Regional Environmental Plan (REP) (Sydney Harbour Catchment) 2005</i>	<i>Sydney Regional Environmental Plan (REP) (Sydney Harbour Catchment) 2005, made under the Environmental Planning & Assessment Act 1979</i>	No listed sites or items on the REP. However, the REP affords protection to sites that, in the opinion of the consent authority, has the potential to be an archaeological site, even if it is not so specified.
<i>Mosman Local Environmental Plan (LEP) 1998</i>	<i>Mosman Local Environmental Plan (LEP) 1998, made under the Environmental Planning & Assessment Act 1979</i>	<ul style="list-style-type: none"> ▪ "Site of Former Explosives Wharf" Spit Road (Schedule 3) ▪ "Remains of Former Bridge & Seawall" Spit Road (Schedule 3)
Register of the National Estate	Non-statutory register	No listed sites or items
National Trust (NSW) Register	Non-statutory register	No listed sites or items

As shown in the table above, two sites listed on the Mosman LEP (made under the NSW *Environment Planning & Assessment Act 1979*) and archaeological remains and / or associated deposits identified as “relics” under the NSW *Heritage Act 1977* and covered in the Sydney REP 2005, occur within the current study area. As such, protection is afforded to these sites, remains and deposits under the NSW *Heritage Act 1977*, the Sydney REP 2005 and the Mosman LEP 1998.

The following section provides a brief summary of the relevant legislation regarding the current study area.

6.2 Statutory Protection

6.2.1 NSW Heritage Act 1977 (amended 1999)

The NSW *Heritage Act 1977* is the primary piece of State legislation affording protection to all items of environmental heritage (natural and cultural) in New South Wales. Under the Act, “items of environmental heritage” include *places, buildings, works, relics, moveable objects* and *precincts* identified as significant based on *historical, scientific, cultural, social, archaeological, architectural, natural or aesthetic* values.

Although no sites, items or deposits listed on the State Heritage Register are contained within the subject site, archaeological remains and deposits that would be defined as relics under the Act are either known or predicted to occur.

Relics provision & protection

In addition to buildings and items listed on the State Heritage Register, various cultural heritage sites, items and archaeological features and deposits are afforded automatic statutory protection by the relic's provisions of the NSW *Heritage Act 1977*. The Act defines a ‘relic’ as any deposit, artefact, object or material evidence that:

- a) *relates to the settlement of the area that comprises New South Wales, not being Aboriginal settlement, and;*
- b) *is of State or local heritage significance.*¹⁰⁷

Sections 139 to 145 of the Act prevent the disturbance or excavation of any land if there is a reasonable cause to suspect that a relic will be discovered, exposed, moved, damaged or destroyed, unless prior permission has been issued by the Heritage Council of NSW. The type of approval that is required generally depends on the formally assessed and registered level of cultural significance of the relic(s):

- If a proposed development has the potential to affect relics listed on the State Heritage Register or covered by an Interim Heritage Order, approval under Section 60 of the Act must be obtained (unless the proposed works are deemed to be minor in nature and fall under the schedule of gazetted Standard Exemptions to Section 60¹⁰⁸);
- If a proposed development has the potential to affect relics not listed on the State Heritage Register or covered by an Interim Heritage Order, approval under Section 140 of the Act must be obtained (unless the proposed works are deemed to be minor in nature and fall under the schedule of gazetted General Exceptions¹⁰⁹ or Additional Exceptions to Section 140¹¹⁰);

¹⁰⁷ NSW *Heritage Act, 1977*, Part 4 – Definitions

¹⁰⁸ http://www.heritage.nsw.gov.au/docs/info_standardexemptions2006.pdf

¹⁰⁹ <http://www.heritage.nsw.gov.au/docs/generalexceptions.pdf>

¹¹⁰ http://www.heritage.nsw.gov.au/docs/info_additionalexceptions.pdf

However, when a proposed development is determined to be a Major Project under Part 3A the *Environment Planning & Assessment Act 1979*, a different process of approval is required (see the following section). Essentially, the standard permits (Section 60 or Section 140) are not required. However, if heritage issues or constraints that would normally trigger such approval requirements arise as part of the Environmental Assessment process under Part 3A, the NSW Heritage Branch must be consulted for advice by the Director-General and such advice must be taken into consideration as part of the Part 3A approval.

6.2.2 *Environmental Planning & Assessment Act 1979*

The *Environmental Planning & Assessment Act 1979* establishes the framework for cultural heritage values to be formally assessed in the land use planning and development consent process. The Act requires that environmental impacts are considered prior to land development; this includes impacts on cultural heritage items and places and archaeological sites and deposits.

Part 3A of the Act concerns the approvals process for major infrastructure development and other projects that classify as 'Major Projects' under the *State Environmental Planning Policy (Major Projects) 2005* regulations.

As the current development proposal has been listed as a Major Project under Part 3A of the Act, the proponent will not be required to apply for heritage approvals or permits under the *Heritage Act 1977*, the *National Parks & Wildlife Act 1974* or at Local Council level with regard to LEPs or REPs. However, the NSW Department of Planning is still required to fully assess the heritage impacts of any proposal under Part 3A in accordance with established State legislation and guidelines. To this end, under Section 75F of the Act, an Environmental Assessment – including heritage issues – must be prepared based on requirements prepared by the Director-General in consultation with the relevant public authorities. Once the Assessment has been conducted, the relevant public authorities are provided an opportunity to review the Assessment and advise the Director-General on an key issues with regard to appropriate conditions of approval.

The *Environment Planning & Assessment Act* also requires that Local Governments prepare planning instruments (such as Local Environmental Plans, Development Control Plans) in accordance with the Act to provide guidance on the level of environmental assessment required.

The current study area falls within the boundaries of the *Sydney Regional Environmental Plan (REP) – Sydney Harbour Catchment 2005* and the *Mosman Local Environmental Plan 1998*.

Sydney Regional Environmental Plan (REP)– Sydney Harbour Catchment 2005

Part 5 of the *Sydney Regional Environmental Plan (REP) – Sydney Harbour Catchment 2005* contains provisions for the protection of cultural heritage sites, items and values and archaeological and potential archaeological sites and deposits– both Aboriginal and non-Aboriginal.

No specific heritage sites, archaeological or potential archaeological sites listed on the REP occur within the subject site, however, the REP also provides protection to sites

that, in the opinion of the consent authority has the potential to be an archaeological site, even if it is not so specified in the heritage schedule of the REP.¹¹⁴

In general circumstances, Regulations 55, 57 and 58 of the REP state that before development consent is granted to any proposed works that have the potential to affect archaeological sites or potential archaeological sites – Aboriginal or non-Aboriginal – a heritage impact assessment must be conducted and approved by the relevant consent authority. Unless the proposed works represent integrated development by virtue of consent required under the *National Parks & Wildlife Act 1974* or the *Heritage Act 1977*, the consent authority under the REP is generally the relevant local council (in this case Mosman Council).

As stated above, however, in the case of Major Projects under Part 3A of the *Environment Planning & Assessment Act* the standard approvals process is varied. Notwithstanding, the regulations within the REP effectively establish the Mosman Council as a relevant public authority with regards to heritage and the Part 3A consultation and approval process.

Mosman Local Environmental Plan (LEP) 1998

Part 9 of the *Mosman Local Environmental Plan (LEP) 1998* contains provisions for the protection and conservation of cultural heritage sites, items and values and archaeological sites or potential archaeological sites – both Aboriginal and non-Aboriginal.

As mentioned above, two archaeological sites contained within the current subject site are listed in Schedule 3 of the Mosman LEP:

- “Site of Former Explosives Wharf” Spit Road, and;
- “Remains of Former Bridge & Seawall” Spit Road.

Regulations 33, 35 and 37 of the *Mosman LEP 1998* state that any proposed development affecting or having the potential to affect a heritage item or a known or potential archaeological site can only be carried out with the consent of Mosman Council. In processing such development applications, Council must consider the likely effect of the development on the physical aspects and / or cultural significance of the heritage or archaeological site.

Nonetheless, in the case of Major Projects under Part 3A of the *Environment Planning & Assessment Act 1979*, the standard approvals process is varied. As with the REP, the regulations within the *Mosman LEP 1998* effectively establish the Mosman Council as a relevant public authority with regards to heritage and the Part 3A consultation and approval process.

¹¹⁴ *Sydney Regional Environmental Plan (Sydney Harbour Catchment) 2005 – Schedule 99.*

7.0 MEASURES TO BE TAKEN TO MINIMISE IMPACT

7.1 Summary of key findings

This Aboriginal, Historic & Maritime Archaeological Assessment has found that:

1. No Aboriginal sites, items or places – submerged or on land – are known or predicted to occur within the boundaries and scope of the current proposed development footprint;
2. Two European historic cultural sites listed as items of local heritage significance on the *Mosman Local Environmental Plan 1998*, are situated within the proposed development footprint;
 - “Remains of Former Bridge and Seawall” (NB: only the seawall is located in the development footprint)
 - “Site of Former Explosives Wharf”
3. The archaeological remains of late 19th century to mid 20th century maritime structures and Spit transport infrastructure – including several phases of earlier boatshed, jetty and slip construction and an early 20th century tramway – are likely to survive within the boundaries of the proposed development footprint;
4. These archaeological remains dating to the late 19th – mid 20th century are considered “relics” under the NSW *Heritage Act 1977* and are thus afforded protection within the Act;
5. An early 20th century slip, assessed to be of local cultural significance, also is present within the study area and will be removed as part of the current proposed development;
6. Archaeological deposits formed from objects discarded – intentionally or unintentionally – from vessels, jetties, slips and boatsheds, and artefacts contained within early – mid 20th century land reclamation / fill deposits, are also likely to be present in the boundaries of the current proposed development;
7. The cultural heritage resource identified within the study area is assessed to range from **Minimal** to **Local** Significance;
8. The proposed works will deliver varied impacts to the identified cultural heritage and archaeological resource within the boundaries of the development footprint. It has been assessed that these impacts will be of a **minor** nature to the cultural significance of the resource – conditional, in some instances, upon the implementation of mitigation measures.

7.2 Recommendations

Based on the findings of this Aboriginal, Historical and Maritime Archaeological Assessment, best heritage practices and relevant statutory provisions, the following management recommendations are provided in order to mitigate the impacts of the proposed development on the identified cultural / archaeological resources within the boundaries of the proposed development footprint. It is advised that these mitigation measures form part of the Statement of Commitments for the proposed development.

Recommendation 1 – Archival recording prior to commencement of development

A program of above water archival recording should be conducted regarding the following structures and sites, prior to the commencement of the proposed development:

- The northern slip (1930s);
- The site of the former southern slips (1910s-1960s), underneath the existing d'Albora Marina boatshed; and
- The site of the earlier boatsheds (1890s-Lyons Boatshed), underneath the existing d'Albora Marina boatshed.

The preparation of the archival record should include:

- archival photography in adherence to the specific requirements detailed by the NSW Heritage Office publication *Heritage Information Series – Photographic Recording of Heritage Items using Film or Digital Capture* (2006);
- detailed physical examination (i.e. non-disturbance survey), documentation and interpretation of identified archaeological features and deposits (no archaeological excavation is required).

Copies of the completed archival record should be provided to and kept by the client, Mosman Municipal Council and Mosman Council Library.

Recommendation 2 – Archaeological monitoring and documentation during certain components of the proposed development

Archaeological monitoring and documentation by a qualified archaeologist should be conducted during the following components of the proposed development:

- Subsurface excavation associated with the installation of the new underground fuel storage in order to ascertain whether any remains of the former tram lines / balloon loop (1910s) survives and to document any archaeological features and / or deposits that may be encountered;
- Demolition and removal of the northern slip in order to examine and record any remains of earlier slip constructions or associated artefact deposits that may be present;
- Any land-based subsurface excavation that may be associated with the proposed redevelopment of the boatshed building in order to ascertain whether any remains of the earlier boatsheds survives and to document any archaeological features and / or deposits that may be encountered.

Such archaeological monitoring would be of a minor scale, involving the presence of an archaeologist on site during the subsurface excavation in these areas. Recording would be conducted by the archaeologist as the earthworks are carried out by relevant construction contractors. It is anticipated that such monitoring would require an archaeologist on site for a few days only.

Recommendation 3 – Notification of previously unidentified or unanticipated archaeological finds

Should any archaeological sites, items or deposits – Aboriginal and non-Aboriginal – be encountered during works associated with the proposed development, a qualified archaeologist should be notified and requested to inspect the finds in order to determine the appropriate course of action.

Recommendation 4 – Development activities conducted with due care

All activities involved in the proposed development should be conducted with due diligence and care in order to ensure that no undue damage to the identified cultural resource of the subject site occur. Specifically, impacts to the seabed should be minimised where possible and direct physical impacts to the western stone seawall should be strictly avoided.

Recommendation 5 – Review of archaeological impact assessment

If the development proposal is significantly altered to include impacts not identified in this report, a review of this impact assessment by a qualified cultural heritage practitioner should be conducted.

Consultation Requirements –

Recommendation 6 – Consultation with relevant statutory authorities in lieu of permitting requirements

In order to remain consistent with NSW statutory objectives and requirements under the relevant heritage regulations, including sections of the *Environmental Planning & Assessment Act 1979*, the *National Parks & Wildlife Act 1974* and the *Heritage Act 1977*, the following government authorities should be consulted with:

- Mosman Municipal Council – regarding impacts to two items of local heritage significance listed on the *Mosman Local Environmental Plan 1998*; and
- NSW Heritage Branch – regarding impacts to archaeological features and deposits defined as “relics” under the *NSW Heritage Act 1977*.

It is assumed that this consultation will take place as part of the review of the Environment Assessment by the relevant agencies.

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Internet Resources / Databases

- NSW Heritage Office – the State Heritage Inventory
http://www.heritage.nsw.gov.au/07_subnav_01.cfm
- Australian Heritage Council – Australian Heritage Database; the Register of the National Estate <http://www.environment.gov.au/cgi-bin/ahdb/search.pl>
- NSW Heritage Office – *Shipwreck Database*
http://maritime.heritage.nsw.gov.au/public/search_ship.cfm
- Department of Environment & Heritage – *National Shipwreck Database*
<http://eied.deh.gov.au/nsd/public/welcome.cfm>
- Encyclopedia of Australian Shipwrecks* – compilation edited by Peter Stone
<http://oceans1.customer.netSPACE.net.au/easw.html>

APPENDIX A

NSW Heritage Office Heritage Inventory Listing – Site of Former Explosives Wharf, Spit Road, Mosman, NSW



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Site of former Explosives Wharf

Item

Name of Item: Site of former Explosives Wharf

Type of Item: Archaeological-Maritime

Group/Collection: Maritime Industry

Category: Other - Maritime Industry

Primary Address: Spit Road, Mosman, NSW 2088

Local Govt. Area: Mosman

Property Description:

Lot/Volume Code	Lot/Volume Number	Section Number	Plan/Folio Code	Plan/Folio Number
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Boundary: The Wharf Structure and its Abutments

All Addresses

Street Address	Suburb/Town	LGA	Parish	County	Type
Spit Road	Mosman	Mosman	Willoughby	Cumberland	Primary

Statement of Significance

A typical small modern jetty which doesn't show its association with the Bantry Bay Explosives Magazine. Indicative of the developmental history of the Spit as well as the operation of the Magazine. The wharf site remains in use.

Date Significance Updated: 15 Aug 01

Note: There are incomplete details for a number of items listed in NSW. The Heritage Office intends to develop or upgrade statements of significance and other information for these items as resources become available.

Description

Designer: N/A

Builder: N/A

Physical Description: A modern pontoon and concrete pile pump-out jetty projecting from the Seawall into Pearl Bay. The pump-out jetty is connected to the former abutment of the Explosives Wharf immediately north of the Marina.

Modifications and Dates: Sympathetic Major Alteration.

History

Historical Notes: Established c1915 in association with the Explosives Magazine at Bantry Bay. It operated as the primary loading and commuting wharf for communication with the Magazine and for daily staff movements. The magazine closed in the 1970s but the wharf has continued to be used for general access to the west side of the Spit. It has

been replaced by the present structure.

Assessment of Significance

SHR Criteria f) This item is historically rare regionally. This item is socially rare regionally.
[Rarity]

SHR Criteria g) This item is aesthetically representative locally. This item is historically representative regionally. This item is socially representative locally.
[Representitiveness]

Assessment Criteria Items are assessed against the  **State Heritage Register (SHR) Criteria** to determine the level of significance. Refer to the Listings below for the level of statutory protection.

Listings

Heritage Listing	Listing Title	Listing Number	Gazette Date	Gazette Number	Gazette Page
<i>Regional Environmental Plan</i>	The Explosives Wharf	SREP 23			
<i>Local Environmental Plan</i>	Site of former Explosives Wharf		23 Feb 01	41	
<i>Heritage study</i>					

Study Details

Title	Year	Number	Author	Inspected by	Guidelines Used
Mosman Heritage Study	1996	467	Godden MacKay Logan Pty Ltd	TB 5/96	Yes

References, Internet links & Images

None

Note: Internet links may be to web pages, documents or images.



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Data Source

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Name: Local Government
Database Number: 2060467
File Number: 467

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APPENDIX B

NSW Heritage Office Heritage Inventory Listing – Remains of Former Bridge and Seawall, Spit Road, Mosman, NSW



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Remains of Former Bridge and Seawall

Item

Name of Item: Remains of Former Bridge and Seawall

Other Name/s: Bridge and Seawall

Type of Item: Archaeological-Maritime

Group/Collection: Transport - Water

Category: Sea-wall

Primary Address: Spit Road, Mosman, NSW 2088

Local Govt. Area: Mosman

Property Description:

Lot/Volume Code	Lot/Volume Number	Section Number	Plan/Folio Code	Plan/Folio Number
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Boundary: Physical Boundaries of the Structure

All Addresses

Street Address	Suburb/Town	LGA	Parish	County	Type
Spit Road	Mosman	Mosman	Willoughby	Cumberland	Primary

Statement of Significance

Provides evidence of earlier structures and of the layered development of crossing arrangements at the Spit. One of the few remnants of prior phases of activity in the vicinity. Forms part of a complex of land/water interface structures along the foreshores of the Spit.

Note: There are incomplete details for a number of items listed in NSW. The Heritage Office intends to develop or upgrade statements of significance and other information for these items as resources become available.

Description

Designer: Not known

Builder: Not known

Physical Description: A short stretch of bitumen roadway terminating at the foreshore on a substantial concrete abutment which contains the imprint of former timber beam locations. Beyond the abutment, a platform of sand is retained by a semi-circular stretch of weathered sandstone seawall.

History

Historical Notes: The original or first Spit Bridge was erected in 1924, replacing the vehicular ferry service which operated on the eastern side of the isthmus. Construction of this bridge did not appear to affect the existing seawall, which appears to be associated


with construction works for the tramway to the Spit, and was probably erected between 1900 and 1916. A new Spit Bridge was opened on the east side of the first in 1958, after which the earlier bridge was removed.

Assessment of Significance

SHR Criteria f) This item is aesthetically rare locally.
[Rarity]

SHR Criteria g) This item is historically representative locally. This item is socially representative locally.
[Representativeness]

Integrity/Intactness: Removed (site only)

Assessment Criteria Items are assessed against the  **State Heritage Register (SHR) Criteria** to determine the level of significance. Refer to the Listings below for the level of statutory protection.

Listings

Heritage Listing	Listing Title	Listing Number	Gazette Date	Gazette Number	Gazette Page
Local Environmental Plan	Remains of Former Bridge and Seawall		23 Feb 01	41	
Heritage study					

Study Details

Title	Year	Number	Author	Inspected by	Guidelines Used
Mosman Heritage Study	1996	468	Godden MacKay Logan Pty Ltd	TB	Yes

References, Internet links & Images

None

Note: Internet links may be to web pages, documents or images.



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