

THE PROPOSAL

Project approval is sought for the subdivision of Lot 2 DP 581117 into 46 lots, which will occur in three stages.

Stage 1

Creation of three lots being:

- Lot 1 (8 057 m²) containing the existing dwelling (8 057 m³);
- Lot 2, a development lot; and
- Lot 3 containing the existing sewer pump station to be dedicated to Kempsey Shire Council.

Stage 2

- Subdivision of Lot 2 to create 20 residential allotments (Lots 101 - 120) and one residue (Lot 121).

Stage 3

- Subdivision of residue lot 121 to create 24 residential allotments (Lots 121 – 144); and
- The land containing the proposed detention basin will be dedicated at no cost to Kempsey Shire Council.

A new access road from Cooper Street will be required as part of Stage 2 and extended as part of Stage 3. All proposed streets will be two-way. There will be no direct access from residential lots to Gregory Street or Arakoon Road.

The project incorporates a 20 metres wide asset protection zone along the east boundary of the site (rear of proposed Lots 134, 133 and part of Lot 132) and a 18 to 25 metres wide asset protection zone along the south boundary of the site (rear of Lots 121 to part of Lot 132).

Refer to:

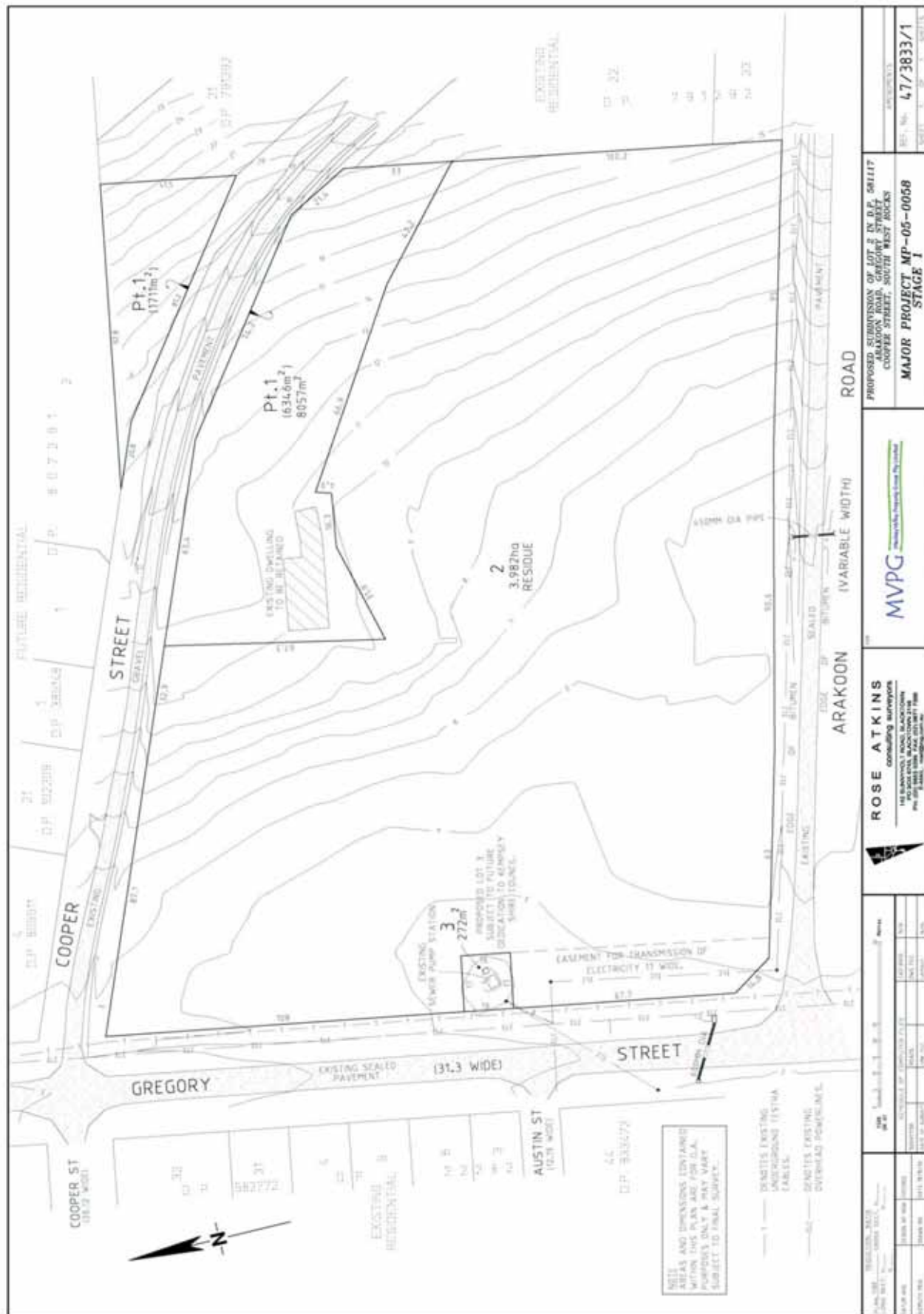
- Proposed Subdivision of Lot 2 in DP 581117 Plans prepared by Rose Atkins, reference number 47/3833/1, 47/3833/2, 47/3833/3 and 47/3833/5 (*Figures 3.1, 3.2, 3.3 and 3.4*);
- HDS Plans File Reference Number 51162-C (two sheets) (*Figures 3.5 and 3.6*).

In formulating the proposed subdivision, Macleay Valley Property Group and its consultants explored a number of design alternatives relating to road layout and lot orientation.

An alternate arrangement investigated included a layout with vehicle access to Arakoon Road. Arakoon Road links Gregory Street/South West Rocks Road with the residential and rural residential areas of Arakoon to the east. It also caters for tourist vehicle movements to Lighthouse Road, Hat Head National Park and the Arakoon State Recreation Area.

Due to the road alignment and the high speed environment direct access to Arakoon Road was not considered an appropriate design outcome. Arakoon Road has an 80 kilometres per hour speed limit adjacent to the site and safe sight distance requirement in an 80km per hour zone is approximately 160 metres. Any access from the site would therefore require an offset of at least this distance to the bend in Arakoon Road, east of the site. For traffic safety reasons, a minimum clearance of approximately 40 metres would be required from Gregory Street/Arakoon Road intersection. The combination of these traffic safety measures restricts access opportunities to Arakoon Road from the site.

In addition to traffic safety, the requirement for asset protection zones and advice in the Director General's requirements that "*connection to Arakoon Road is considered unfavourable*" meant that direct access to that road was not appropriate.



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Project:	Lot 2 DP 581117 South West Rocks
Drawing No:	0045027_04
Date:	20/12/06
Drawn by:	SP
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3.3 SUBDIVISION LAYOUT, DESIRED FUTURE CHARACTER AND SUSTAINABILITY

3.3.1 Subdivision Layout and Sustainability

The subdivision design is based on comprehensive analysis of the opportunities and constraints of the site and its surrounds. The analysis considered the surrounding land uses, potential bushfire risk, existing vegetation, road network and transportation links, views, orientation, aboriginal heritage and stormwater management. A Detail Survey (*Figure 2.2*) and an Opportunities and Constraints Plan (*Figure 3.7*) were prepared to provide an understanding of the features and topography of the site.

The subdivision incorporates a mix of lot sizes to create an attractive opportunity for future residential development. These are detailed in *Table 3.1*.

Table 3.1 Proposed Lot Sizes

Lot Size (square metres)	Number of Lots
500 - 650	20
651 - 800	14
801 - 950	5
951-1200	2
1201+	3

The subdivision layout creates regular shaped lots with a north-south or east-west orientation, providing opportunities for good solar access, in accordance with ESD principles. The layout also provides each lot with good development potential and access from an internal road. There are some more conventional sized lots, however more than half (25 lots) are greater than 650 square metres, providing opportunities for tree retention, asset protection zones and landscaping. The range in lot sizes provides a diversity of choice for future owners.

It incorporates best practice stormwater management by providing a detention basin / sediment pond, limited filling within flood prone areas, the installation of a separate non-potable water supply pipe network and stormwater controls. The detention basin / sediment pond will be located in the existing transmission easement in the south west corner of the site and will be landscaped with banks mounded to a height of 3.6 metres (*refer to Figures 3.5 and 3.6*). The installation of a separate non-potable water supply pipe network is proposed to be ready to connect to Council's proposed recycled water reticulation main when it comes online.

The implementation of Water Sensitive Urban Design (WUSD) principles aim to ensure a reduction in the runoff rate and improve the quality of stormwater runoff to waterways. For the proposed development, rainwater tanks will be installed on each dwelling and water efficient fittings will be prescribed in dwellings.

The proposed road network includes:

- *Internal Road Numbers 1 and 2* that have a road reserve of 16 metres which includes a nine metres wide and seven metres wide pavement, respectively; and
- *Internal Road No.3* which serves only six allotments. It will have a road reserve of 15 metres and 7 metres wide sealed pavement.

The proposed street layout provides a subdivision pattern that responds to the topography and features of the site. The wider verge (4.5 metres wide) created in road Number 2 provides an opportunity to plant larger trees without impacting on the utility services.

The intersection of Gregory and Cooper Streets will remain a give way T-intersection and no new access is proposed to Gregory Street.

The site is within close proximity to a number of foreshore open space areas such as parks and beachside areas at Front Beach and Horseshoe Cove. The design provides for pedestrian, cycle and vehicle links to Gregory Street which provides access to the existing open spaces.



Figure 3.7
Opportunities and Constraints

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Gregory Street, South West Rocks
OPPORTUNITIES AND CONSTRAINTS

DWG No : 1648-LP-01 Revision : A



The area of the proposed subdivision is within the coastal zone set out in State Planning Policy 71. The design of the subdivision is based on the principles in the Coastal Design Guidelines, which supplements State Environmental Planning Policy 71, in particular it has considered:

- the relationship to the surrounding environment;
- visual sensitivity;
- streetscape;
- the design of future houses.

Relationship to the Surrounding Environment

Consideration of the guidelines ensures that the proposal provides low density housing opportunities in South West Rocks, in a manner that respects the environmental aspects of the site. Technical investigations of aboriginal cultural places, ecological links, existing vegetation, stormwater management and soil types have been completed to create a subdivision that responds to the surrounding environment. This is further discussed in section 6 of this report.

The existing residential area of South West Rocks is west of the site, on the west side of Gregory Street. The existing residential lots in proximity to the site, which includes the area south of Lindsay Noonan Drive, are a mix of conventional and larger sized residential allotments. The internal lots are arranged to front a loop road (Wilfred Partridge Street, which is accessed from Lindsay Noonan Drive) or a cul de sac (Aston Close which is accessed from Wilfred Partridge Street). The remaining lots front Cooper Street, Gregory Street or Lindsay Noonan Drive.

The dwellings immediately opposite the site are characterised by a mix of brick and weatherboard detached single storey dwellings with colorbond or tile roofs on lots that are larger than a conventional residential lot. The dwellings front Gregory Street with some shrub and low level planting within the front setback and no front fences.

The lots along Cooper Street (east of Gregory Street) are generally rural residential allotments, accommodating detached single and two storey dwellings. There is also a large residential allotment opposite the subject site. The dwellings along Cooper Street are constructed of brick or weatherboard and set within a treed / vegetated landscape. Land on the north side Coopers Street is also the subject of a residential subdivision, to create approximately 17 lots.

The proposed subdivision includes lots a mix of lot sizes, which is generally consistent with the existing residential pattern in proximity to the site. Where possible, trees are to be retained and additional trees and vegetation planted to provide an attractive setting for future dwellings. A single dwelling covenant will be placed on each lot maintain the existing land use pattern. Future dwellings on lots 101 to 104 will be required to front Gregory Street, consistent with the residences on the west side of Gregory Street.

Visual Sensitivity and Streetscapes

Given distance of the site from the foreshore, the proposal will have no impact on views from the coastal foreshore area. The subdivision design however, has taken into consideration the aesthetics of the site as it is at the entrance to South West Rocks. The proposal endeavours to retain as many trees as possible within the development to contribute to attractive streetscapes along Gregory Street, Arakoon Road and internal streets. The ability to retain trees has involved arboricultural, ecological and bushfire management input. In addition, to tree retention landscaping of the public spaces will create a desirable internal streetscape and an attractive interface with Gregory Street and Arakoon Road (*Refer to Figures 3.8, 3.9 and 3.10*).

The subdivision has been designed to avoid the development 'turning its back' on Gregory Street by prohibiting high back fences along Gregory Street and constructing a 1.2 metres high timber post and railing fence along the site frontage to Gregory Street. The permeable fencing will sit on top of a retaining wall, which will be approximately 800mm high. Dwellings will be required to front Gregory Street whilst all vehicular access will be from the internal road system. Photographs on the next page illustrate example streetscapes with a similar arrangement as proposed along Gregory Street.

Fencing along Arakoon Road is proposed to be a height of 1.5 metres and be constructed of material that is suitable for locating in asset protection zones. Landscaping, along the fence line will soften the appearance of the fence along Arakoon Road. Fencing detail along Gregory Street will be permeable to continue an interface between the site and the street (*Refer to Figure 3.10*).

The retained vegetation is to be protected in accordance with the *South West Rocks Rosarii Ecological Assessment* and the Statement of Commitments. A suitably qualified person would be engaged to inspect and maintain the 19 habitat boxes if required for a period of 12 months after the installation and provision of a security bond of \$10,000.

The statement of commitment includes a condition pertaining to the management of the landscaping. The condition stipulates that upon the release of the Subdivision Certificate, the landscaped areas will be maintained for a period of 6 months with a landscape bond to the amount of \$20,000.



Figure 3.8
Proposed Civil Works

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Gregory Street, South West Rocks

EXISTING TREE PROPOSALS

DWG No : 1648-LP-02

Revision : C

Scale 1:500 @ A1



Figure 3.9
Landscape Concept Plan

Client:	Mackay Valley Property Group
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ENVIRONMENTAL RESOURCES MANAGEMENT

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Gregory Street, South West Rocks
LANDSCAPE CONCEPT PLAN

DWG No : 1648-LP-03 Revision : C Scale 1:500 @ A1

Client: **MVPG** Mackay Valley Property Group

sma SCOTT MURRAY & ASSOCIATES





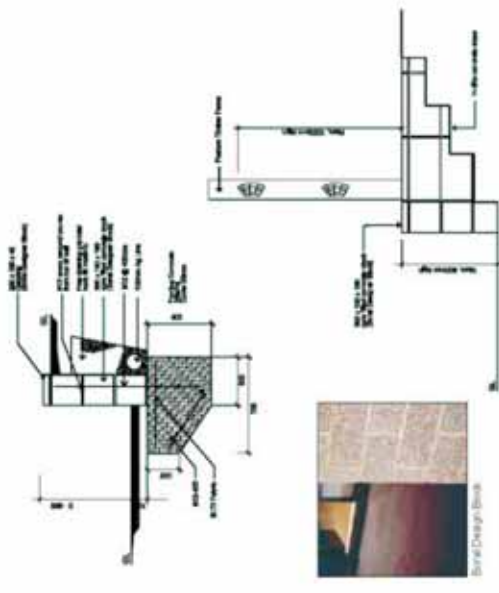
DETAIL 1 - Feature Timber Post and Rail Fence

- fence detail occurs locally and ties the development into the surrounding rural/rural

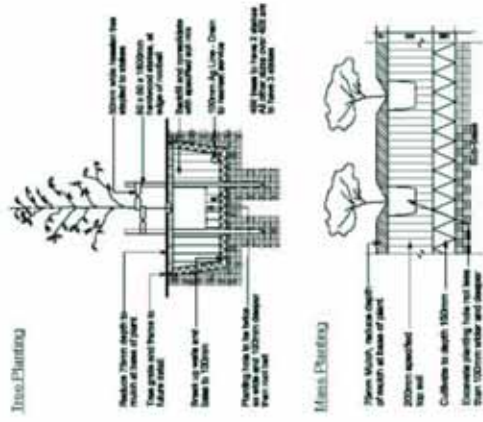


DETAIL 3 - Proposed Timber / Metal (1.5m / 1.8m high)

- side fences to be metal with timber surrounds, therefore more suitable for locating in APZ areas. Metal to be Lysaght Menscram or similar. Provides greater interest and detail than standard Colorbond fence.



DETAIL 2 - Concrete Block Retaining Wall to Gregory Street



DETAIL 4 - Planting Details



Proposed Tree Planting
All trees planting to be indigenous tree species, eg. Eucalyptus tereticornis and Eucalyptus microcarpa



Proposed Shrub Planting
The shrub planting palette is to contain a high proportion of locally indigenous species, supported by a number of non indigenous native species, and a small amount of non natives for highlight colour

Figure 3.10
Details Plan

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Gregory Street, South West Rocks DETAILS

DWG No : 1648-LP-04 Revision : B



Photograph 9

Example of a streetscape with vehicle access denied from the street.



Photograph 10

Example of a streetscape with vehicle access denied from the street.



Photograph 11

Example of access denied frontages with retaining walls.

A single dwelling covenant will be placed on each lot in order to continue the landuse pattern of the surrounding areas. In addition, to create high quality development and streetscapes, houses design guidelines will be prepared that include development controls for height, site coverage, set-backs, architectural details, landscaping, adoption of ESD measures and car parking. This is included in the Statement of Commitments set out in *Section 7* of this report.

3.3.3 *Staging*

The subdivision will be constructed in three stages. The indicative staging is nominated on *Figures 3.1, 3.2 and 3.3*. Infrastructure works including the water management measures outlined in this report will be implemented progressively.

Stage 1 is to excise the existing dwelling and sewer pump station on the property.

Stage 2 creates 20 residential allotments (lots 101-120) and one residue lots (Lot 121). The second stage will the following infrastructure works.

- The reconstruction of half the width of Cooper Street fronting proposed lots 105 to 107 and 120.
- The full construction and dedication to the public of the 16 metres wide proposed road as shown on Figure 3.2.
- The full construction and dedication to the public of the proposed new access-way at 10 metres wide fronting lot 114 as shown on Figure 3.2.
- The construction of a temporary turning circle and a water quality/detention pond to service the proposed development as shown on Figure 3.2.
- The construction of an underground electrical reticulation system to Country Energy standard to service lots 101 to 120, including street lighting to the upgraded section of Cooper Street and proposed new road.
- The construction of a reticulated potable and recycled water supply to Macleay Water standard to service lots 101 – 120, including lead out provisions to lot 121 to service the future development of that lot.
- The construction of reticulated waste water system to Macleay Water standard to service lots 101 -120, including lead out provisions to lot 121 to service the future development of Lot 121.
- The landscape embellishment of the verges fronting lots 101 to 120 for the existing (Gregory and Cooper Street) and the proposed road 1 as shown on Figure 3.2, including entry statements, street trees, gardens, turf, concrete cycle-ways and footpaths.

Stage 3 creates 24 residential lots. The third stage will involve the following infrastructure works.

- Full construction and dedication to the public of the 16 metres wide proposed road No 2 as shown on figure 3.3.
- Full construction and dedication to the public of the 13.5 metres wide proposed road No. 3.
- The demolition of the temporary turning circle constructed in Stage 2.
- The construction of an underground electrical reticulation system to Country Energy standard to service lots 121 to 144, including street lighting for the proposed new roads.
- The construction of reticulated potable and recycled water supply to Macleay Water standard to service lots 121 to 144.
- The construction of reticulated waste water system to Macleay Water standard to service proposed lots 121 to 144 and the proposed lot in Stage 1 to service the existing dwelling.
- The landscape embellishment of the proposed verges fronting lots 121 to 144 for the proposed new roads, including street trees, gardens, turf and concrete footpaths. It also includes the selected fencing of the Arakoon Road boundary.

