



Response to Submission Heritage Impact Statement

Harbourside
Redevelopment

12 February 2020



Document Information

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3	28/01/20	Final Draft	For client review	Tatiana Barreto	Natalie Vinton
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1. Introduction

1.1. The Purpose of This Report

Curio Projects Pty Ltd have been engaged by Mirvac Projects Pty Ltd to prepare a revised Heritage Impact Statement (HIS) report that addresses the Response to Submissions (RtS) (as relevant to heritage), and the assessment of the proposed amended Concept Proposal in relation to State Significant Development (SSD) Development Application (DA) for the redevelopment of the Harbourside Shopping Centre (Harbourside) (SSD 7874).

The SSD DA was publicly exhibited for a period of 62 days from 15 December 2016 to 14 February 2017. During this time, ten (10) submissions were received from government agencies and City of Sydney Council and over 140 submissions were received from the general public.

This report should be read in conjunction with the original *Heritage Impact Statement—Harbourside Redevelopment*, prepared by Curio Projects, dated September 2016, as well as Appendix 2 and 3 to the original HIS: Curio Projects 2016, Redevelopment of Harbourside Shopping Centre—Aboriginal Heritage Due Diligence Assessment Report; and Curio Projects 2016, Harbourside Shopping Centre, Darling Harbour—Historical Archaeological Assessment, to support the Harbourside Concept Proposal Square (SSD 7874).

Curio Projects has prepared this HIS with reference to the following architectural package as provided by the client:

- FJMT, *Harbourside Architectural Drawing Set*, 24 January 2020
- Virtual Ideas, *Harbourside Hero Renders*, 23 January 2020

This report also addresses all heritage-related submissions received by both government agencies and the public in response to the exhibition of the development EIS, as per the Department of Planning and Environment letter dated 9 March 2018.

1.2. Proposed Amended Development

Since the exhibition of the proposal and given the nature and range of submissions made from agencies and the public, Mirvac has been reviewing the overall approach and elements of the Concept Proposal. This has accordingly led to developing an Amended Concept Proposal. The final Concept Proposal, therefore, includes substantial amendments made by Mirvac pursuant to Clause 55 of the *Environmental Planning & Assessment Regulation*, in the main to address matters raised in the submissions and deliver an overall significantly improved outcome on the site and for the broader Darling Harbour precinct.

The final Concept Proposal seeks approval for the following key components and development parameters:

- Demolition of existing site improvements, including the Harbourside Shopping Centre, pedestrian bridge link across Darling Drive, obsolete monorail infrastructure, and associated tree removal;

- A network of open space areas and links generally as shown within the Public Domain Concept Proposal, to facilitate re-integration of the site into the wider urban context;
- Building envelopes;
- Land uses across the site, non-residential and residential uses;
- A maximum total Gross Floor Area (GFA) across the Harbourside site of 87,000sqm for mixed-use development (49,000sqm non-residential and 38,000sqm residential development);
- Basement car parking;
- Car parking rates to be utilised in subsequent detailed (Stage 2) Development Applications);
- Urban Design and Public Realm Guidelines to guide future development and the public domain; and
- Strategies for utilities and services provision, drainage and flooding, and ecologically sustainable development.

The following key amendments have been made to the proposal:

Relocation of the Tower

The tower element of the Concept Proposal has been relocated from the north of the site to the centre of the site (the widest part of the site) to allow for an increased setback from the heritage listed Pyrmont Bridge, improved relationship to the waterfront and ICC Hotel, to minimise view impacts from 50 Murray Street, together with reducing overshadowing impacts on the public domain and improved solar amenity to the northern end of the retail centre.

Reduction in Height of the Tower

The height of the tower has also been reduced from RL 166.35 to RL 153.75. The reduction in the height will minimise overshadowing impacts to the public domain as well better relate to the height of the ICC Hotel.

Reduction in Height of the Podium

A portion of the podium height at its northern extent has been partly reduced from 30.5 RL to RL 25. The reduction in height provides for improved view sharing from 50 Murray Street.

Removal of Tower 'Tail' element

As part of the relocation of the tower and refinement of the podium, the stepped form of the lower tower element has now been removed. This design move has been made in order to again improve views from adjacent buildings from the west.

Building Footprint of the Tower

The building footprint of the tower has increased in width, to accommodate the floor space from the reduction in height of the tower and removal of the 'tail'.

Gross Floor Area / Land Use Mix

The amended proposal retains the same overall 87,000sqm of GFA, however, there is a minor adjustment in the split between non-residential and residential:

- Non-residential uses floor space – 49,000sqm; and
- Residential uses floor space – 38,000sqm

In response to market demand and the focus of local and regional strategic planning policies, it is proposed for the podium to include both retail and commercial land uses. Indicatively, comprising ~23,000,000sqm lettable area of commercial and ~21,000sqm gross lettable area of retail.

The podium enables large campus sized commercial floor plates that are favoured by large multinational tech, finance and professional services companies.

Apartment Numbers

As a result of a review of the mix and sizing of apartments, there is a minor reduction in the indicative number of apartments, from 364 to 357. Note, this yield is on the 'Indicative Design' only and will be subject to future design development and a Stage 2 DA. This Stage 1 DA only seeks approval for land uses and the building envelope comprising a total of 87,000sqm GFA

Car Parking Spaces

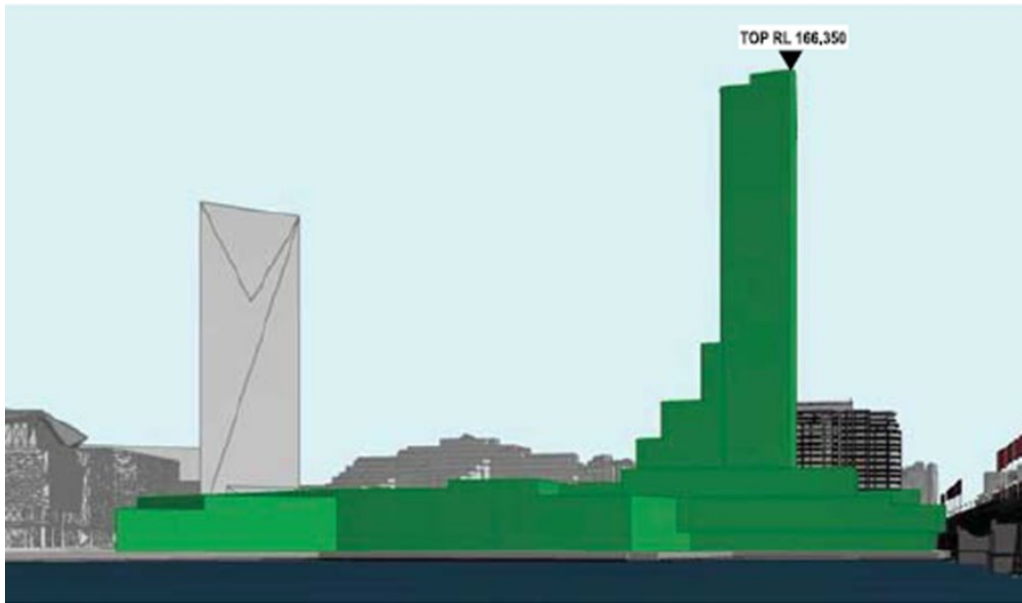
The extent of the basement will remain the same, but there has been a minor increase of 11 car parking spaces from 295 spaces to 306 spaces. As above, this is based on the 'Indicative Design' only.

Landscaped Open Space and Public Domain

All of the key concepts and public benefits as originally proposed are retained under the amended Concept Proposal, with the addition of further landscaping opportunities on the northern rooftop extent of the retail podium, further enhancing views and outlook from 50 Murray Street.

A more detailed and comprehensive description of the amended proposal is contained in the Response to Submissions and Amended Concept Proposal prepared by Ethos Urban.

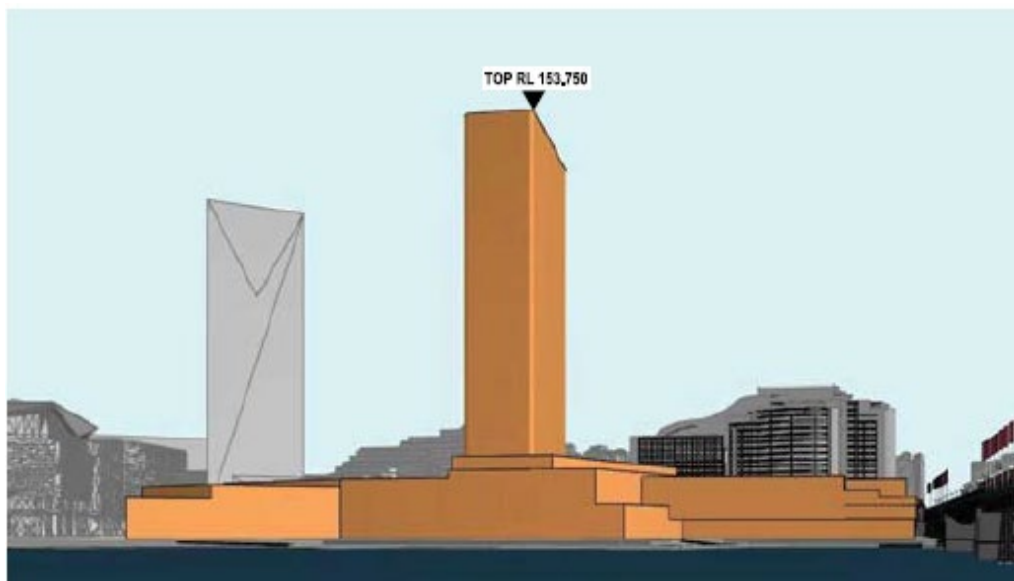
PREVIOUS DA



Total 87,000m² GFA, including:

- 52,000m² podium (including 28,000m² GLAR retail)
- 35,000m² tower (including 28,000m² NSA residential)
- 305 cars (2 x level basement)

NEW DA



Total 87,000m² GFA, including:

- 49,000m² podium (including 23,000m² NLA commercial, 16,000m² GLAR retail)
- (Note, reduction of retail, and introduction of commercial in the podium)
- 38,000m² (including 29,500m² NSA residential)
- 301 cars (3 x level basement)
- Public domain concepts remain the same. However, the observation deck has been removed.

Figure 1.1: Original and Amended submitted Concept Proposal (Source: Ethos Urban)

1.3. Limitations

The report includes an assessment of the potential for the site to impact on Aboriginal archaeological objects and/or places but does not include an assessment of the potential Aboriginal Cultural Heritage significance (intangible values) of the site. This report addresses potential heritage impacts only.

1.4. Authorship

This report has been prepared by Sam Cooling, Senior Archaeologist and Heritage Specialist, and Tatiana Barreto, Architectural Consultant, with specialist input and review by Natalie Vinton, Director, of Curio Projects Pty Ltd.

2. Assessment of Heritage Impacts | Amended Proposal

2.1. Description of Amended Proposal

The major amendment to the Harbourside Redevelopment proposal involves the relocation of the residential tower from the northern end of the site (as per the original proposal and DA), to the centre of the site. The retail envelope and extent of the basement car parking proposals remain similar to previous, with a slight reduction of the height of the retail podium.

The main elements of the amended development proposal, particularly as relevant to the assessment of heritage impact, are summarized as follows:

- Demolition of existing site features including Harbourside Shopping Centre, pedestrian bridge link across Darling Drive, monorail infrastructure, and associated tree removal;
- Building envelope for the new Harbourside Shopping Complex and residential tower;
- Basement car parking; and
- Public domain works concept plan.

2.2. Physical Impacts

The proposed physical impacts to the site have been assessed in terms of their potential impacts at this concept stage of the development, as per the amended concept proposal.

2.2.1. Built Heritage Impacts

While the original proposal included the provision to create direct pedestrian access between the new tower and Pyrmont Bridge, the relocation of the tower envelope further towards the south of the site has removed this requirement. Thus, the original proposed pedestrian bridge linking directly to Pyrmont Bridge has been deleted (Figure 2.1). The amended proposal still proposes the conservation of the Pyrmont approaches and surfaces of the Pyrmont Bridge, in accordance with the Pyrmont Bridge CMP policies. Conservation of the Bridge approach surfaces will be achieved by replacement of the existing surface with paving that is more commensurate with the heritage fabric and form of Pyrmont Bridge. This proposed conservation of the surface of the bridge at its western end will allow this location to become readable as fabric associated with the heritage bridge. The design details of the conservation of the surfacing of the Pyrmont Bridge will be finalised through the Stage 2 DA.

Therefore, the amended proposal does not propose a physical impact to any heritage item or significant heritage fabric.

The new pedestrian bridge from Bunn Street across the Light Rail, creating a pedestrian thoroughfare from Bunn Street, Pyrmont, to the Darling Harbour foreshore via the new retail complex, as proposed through the original concept plan, will be retained through the amended concept plan (although this overpass presents no physical impact to any heritage items or fabric). The current Murray Street pedestrian bridge that provides direct access from the Murray

Street apartment complex will also be retained as part of the amended concept plan development.



Figure 2.1: Amended proposal northern setback and interface between retail podium envelope and Pyrmont Bridge. No additional pedestrian bridge proposed through the amended proposal. Conservation of surfacing of bridge and approaches. N.B. indicative design only. Future design development in stage 2 DA (Source: Virtual Ideas, 2020)

2.2.2. Aboriginal Archaeological Impacts

The Curio Projects 2016 report *Redevelopment of Harbourside Shopping Centre—Aboriginal Heritage Due Diligence Assessment Report*, stated with regards to potential Aboriginal archaeology within the study area that:

Aboriginal archaeological deposits, should they be present within the vicinity of the study area, would most likely be either stone artefact sites, shell middens, or a combination of both. In order for these archaeological deposits to be present in situ, they would require the retention of natural soil profiles in the area that would be extant from 1788. The historical development at the study area including the large area of land reclamation within the study area itself, as well as the use of the area and surrounds for railway lines and the Darling Harbour Railway Goods Yards, indicates

that there is low to nil potential for natural intact soil profiles to be retained in this area. The soil landscape mapping of the region is consistent with this assessment, as the study area is mapped to lie within 'Disturbed Terrain'. It should be noted that the western study area boundary is immediately adjacent to a mapped area of the 'Deep Creek' soil profile, however it is also highly unlikely that any natural soil profiles are retained in this location due to the nature of the industrial development along the western shoreline of Darling Harbour from the 1850s to the 1970s.

Based on the assessment of environmental and archaeological context, as well as in consideration of the historical background for the site, previous land use and disturbance, and in the current statutory context (SSD development application), the following conclusions are made with respect to potential Aboriginal archaeology within the curtilage of the Harbourside redevelopment:

- *The study area does not contain any previously registered sites.*
- *The study area is located predominantly on reclaimed land along the western shoreline of Darling Harbour/Cockle Bay.*
- *The study area and surrounds were historically an integral part of the industrial use of Darling Harbour from the 1850s through to the 1970s, and were part of/adjacent to the Darling Harbour Railway Goods Yard and Railway tracks.*
- *The study area is located wholly across the soil profile of 'Disturbed Terrain', associated with the extensive land reclamation of the area in the 1870s and 1910s, and no natural soil profiles are likely to have been retained in this area.*
- *There is low to no potential for in situ Aboriginal archaeological deposits to be present within the study area.*
- *There is a low potential for Aboriginal archaeological deposits such as stone artefacts or shell middens to be located at the study area in a disturbed context.*

The extent of the proposed basement is the same as the amended proposal. Therefore, as the basement excavation extent has not increased, the assessment of potential Aboriginal archaeological impact remains as above.

Should any unexpected Aboriginal objects be discovered through the course of development works, these would be managed in accordance with an Unexpected Finds Protocol for Aboriginal Objects, to be developed through the Stage 2 SSDA process.

2.2.3. Historical Archaeological Impacts

While the amended proposal includes the relocation of the tower and other design modifications, particularly along the northern interface between the retail's development and Pyrmont Bridge, the proposed excavation impacts (i.e. development works that would have the potential to impact upon the identified potential archaeological resources), has not altered greatly from the original proposal.

As this report relates to the amended concept design (Stage 1 of the SSDA process), information regarding detailed design of the development and the scope of site works, including construction methodologies (such as exact details of the depth and extent of proposed subsurface impacts), were not available for consideration as part of this impact assessment and will be addressed in detail through Stage 2 of the development application process.

However, relevant to the Amended Concept Plan, the following activities are considered to have the potential to impact on remnant archaeological resources, should they be present within the study area:

- Demolition of the existing Harbourside Shopping Centre;
- Construction of the proposed new building, including the construction of new subsurface footings; and
- Bulk excavation below the existing ground surface for the construction of a three-level basement subsurface carpark (Figure 2.8 and Figure 2.13 to Figure 2.15).

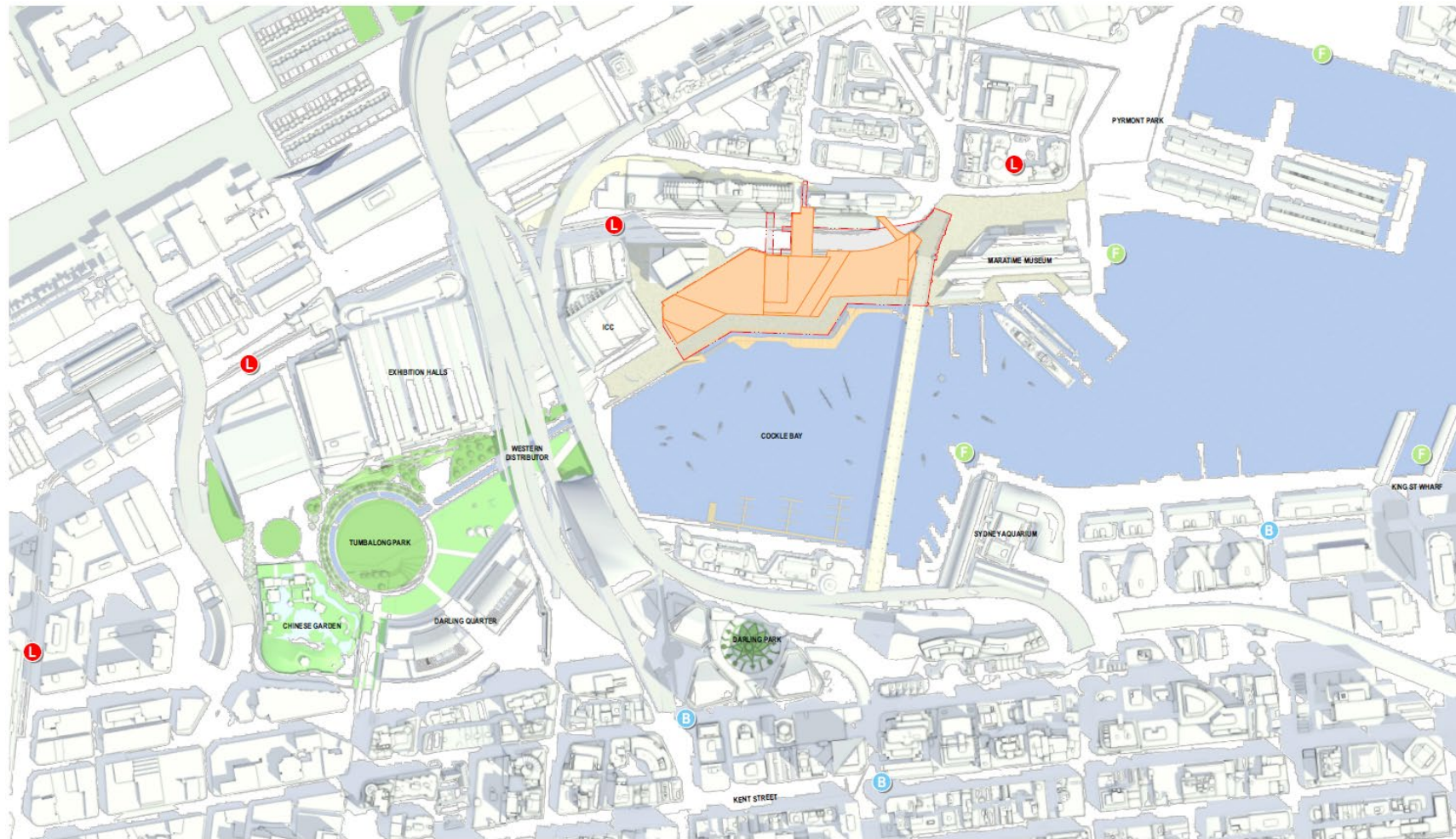
These activities are likely to result in a range of large scale and localised impacts to the identified historical archaeological resource. Bulk excavation for the proposed subsurface carpark would be likely to entirely remove the surviving historical archaeological resource within the basement footprint.

Other activities, such as the demolition of the existing Harbourside Shopping Centre and the construction of new subsurface foundations for the proposed redevelopment, are likely to result in localised impacts to archaeological remains. Historical archaeological remains in the vicinity of elements of the existing structure, particularly the structure's foundations, would be likely to be disturbed by their demolition and removal. The construction of the proposed new structure, particularly the installation of subsurface footings, would also result in disturbances to nearby surviving historical archaeological remains.

While the specific locations of such ground disturbance are not yet completely available, the amended concept design indicates that the existing structure and the proposed new development extend across much of the subject site. As a result, activities associated with demolition and construction are likely to disturb and/or remove much of the historical archaeological resource across the site, except in the southern portion of the site where it is proposed to minimise any disturbance or impact to the Water-Cooling System and Manifold.

The proposed development will thus likely result in the removal of or disturbance to large areas of the subject site's potential historical archaeological resource through both large-scale impacts (e.g. bulk excavation for the car park) and localised impacts (removal/construction of footings).

This statement of archaeological impact will be refined and updated for the Stage 2 development as more information about existing site conditions (including detailed geotechnical data, service plans) and the proposed development (e.g. detailed design and actual sub-surface impacts) become available.



BOUNDARY LEGEND
 --- PROPOSED LOCAL GOV. BOUNDARY
 --- WATERFRONT'S PRESENT AND FUTURE
 --- DEVELOPER EXISTENT

Figure 2.2: Location Plan (Source: FJMT, 2020)

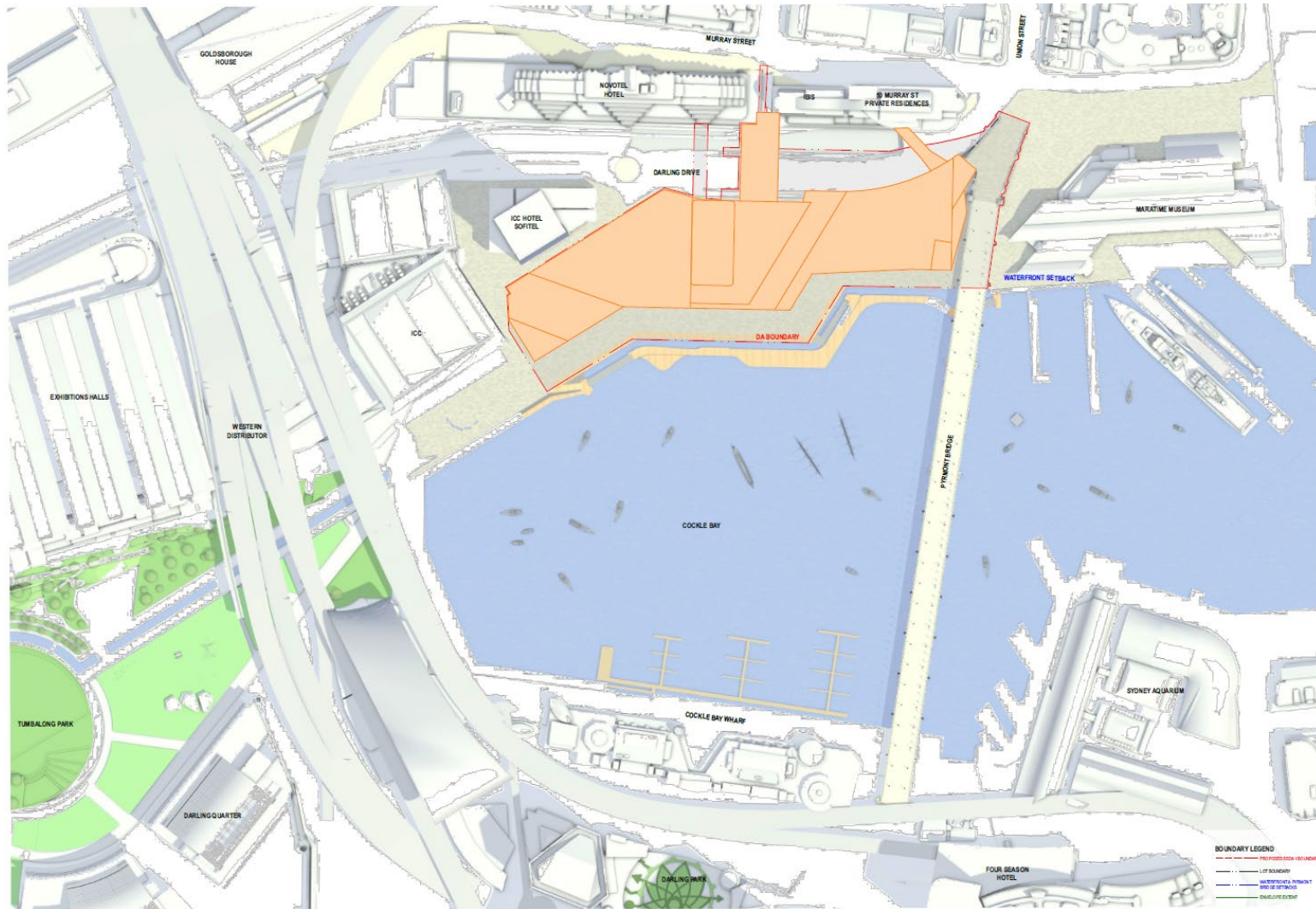


Figure 2.3: Context Plan (Source: FJMT, 2020)

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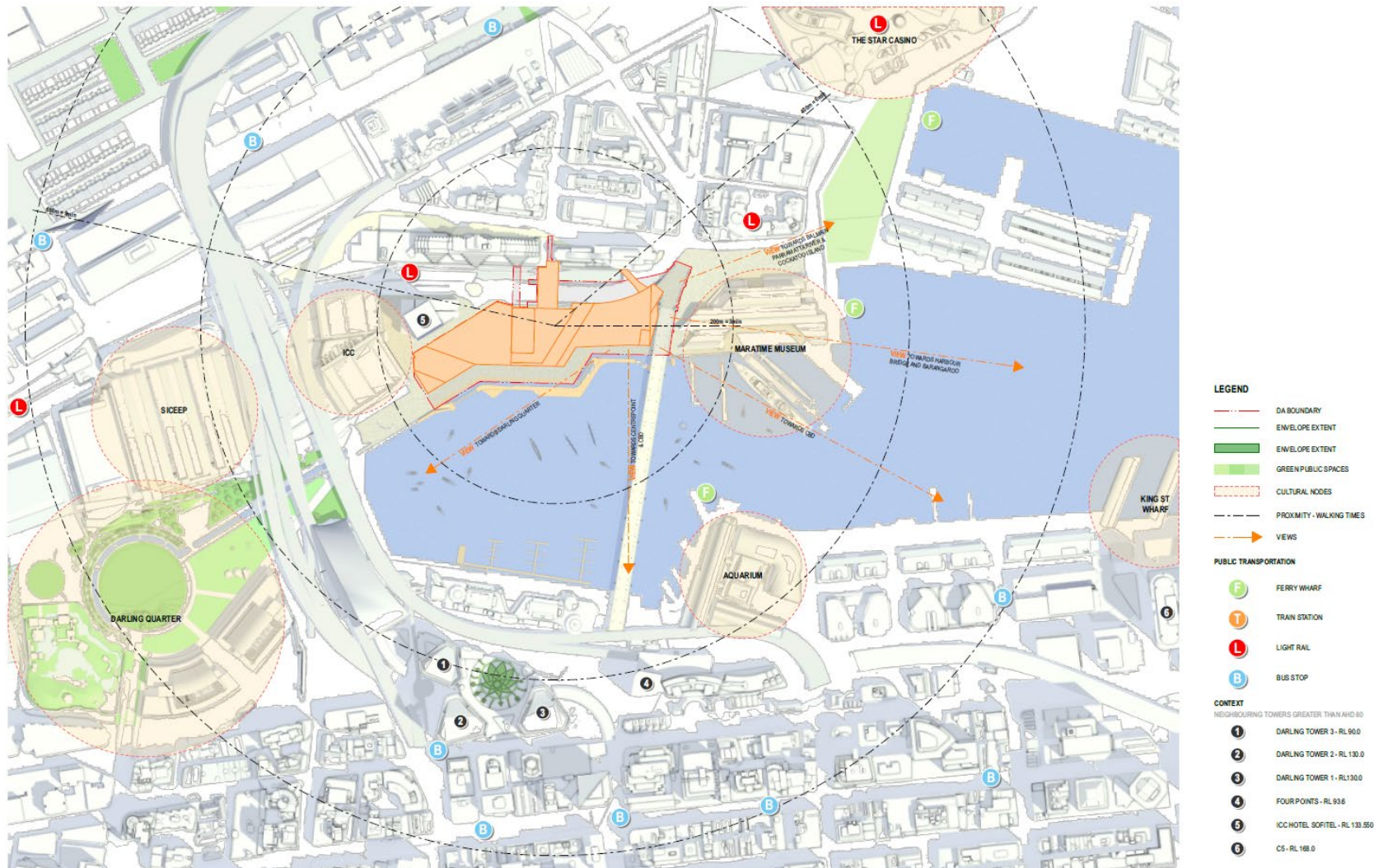


Figure 2.4: Site Analysis (Source: FJMT, 2020)

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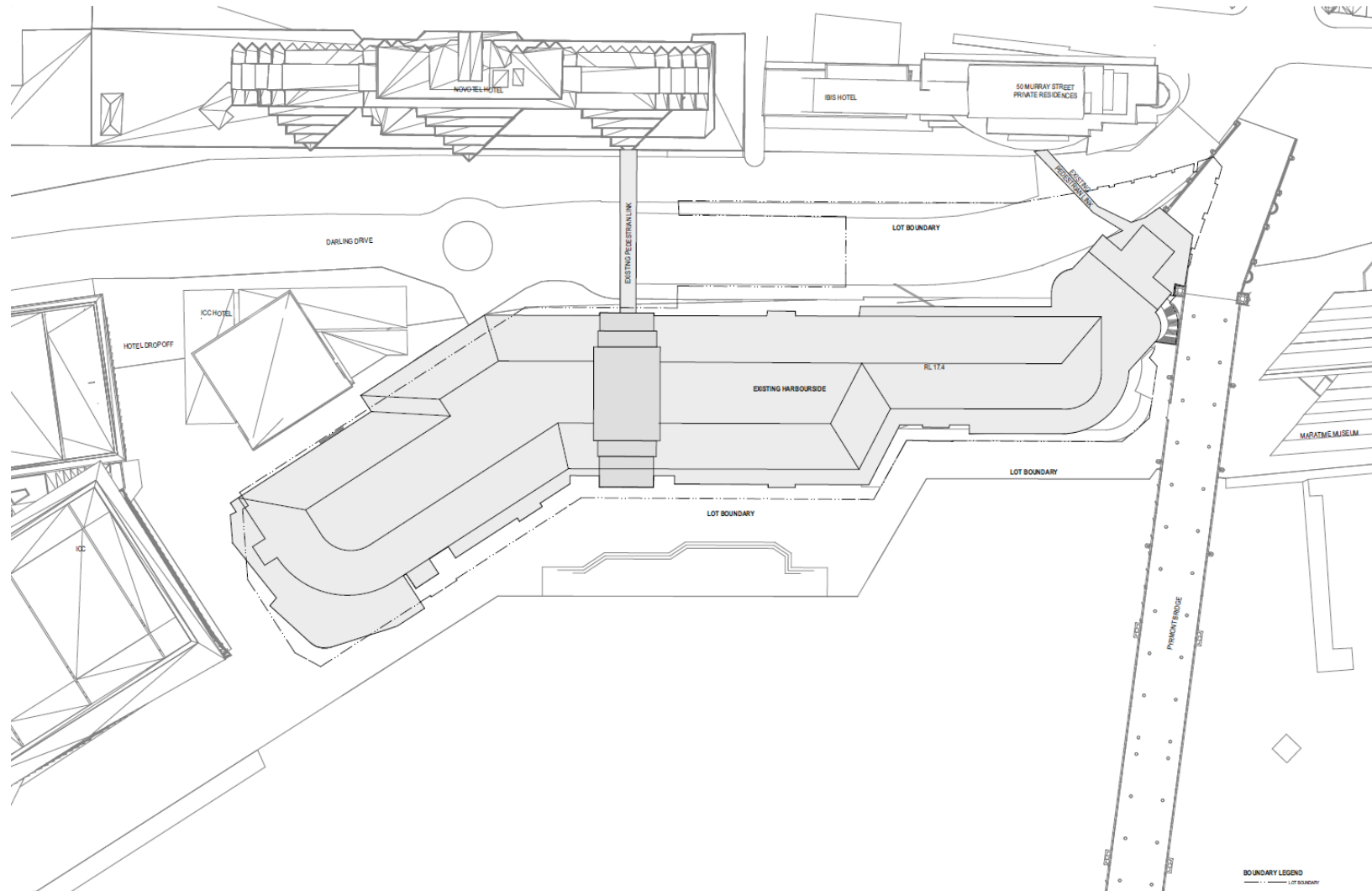


Figure 2.5: Existing Plan (Source: FJMT, 2020)

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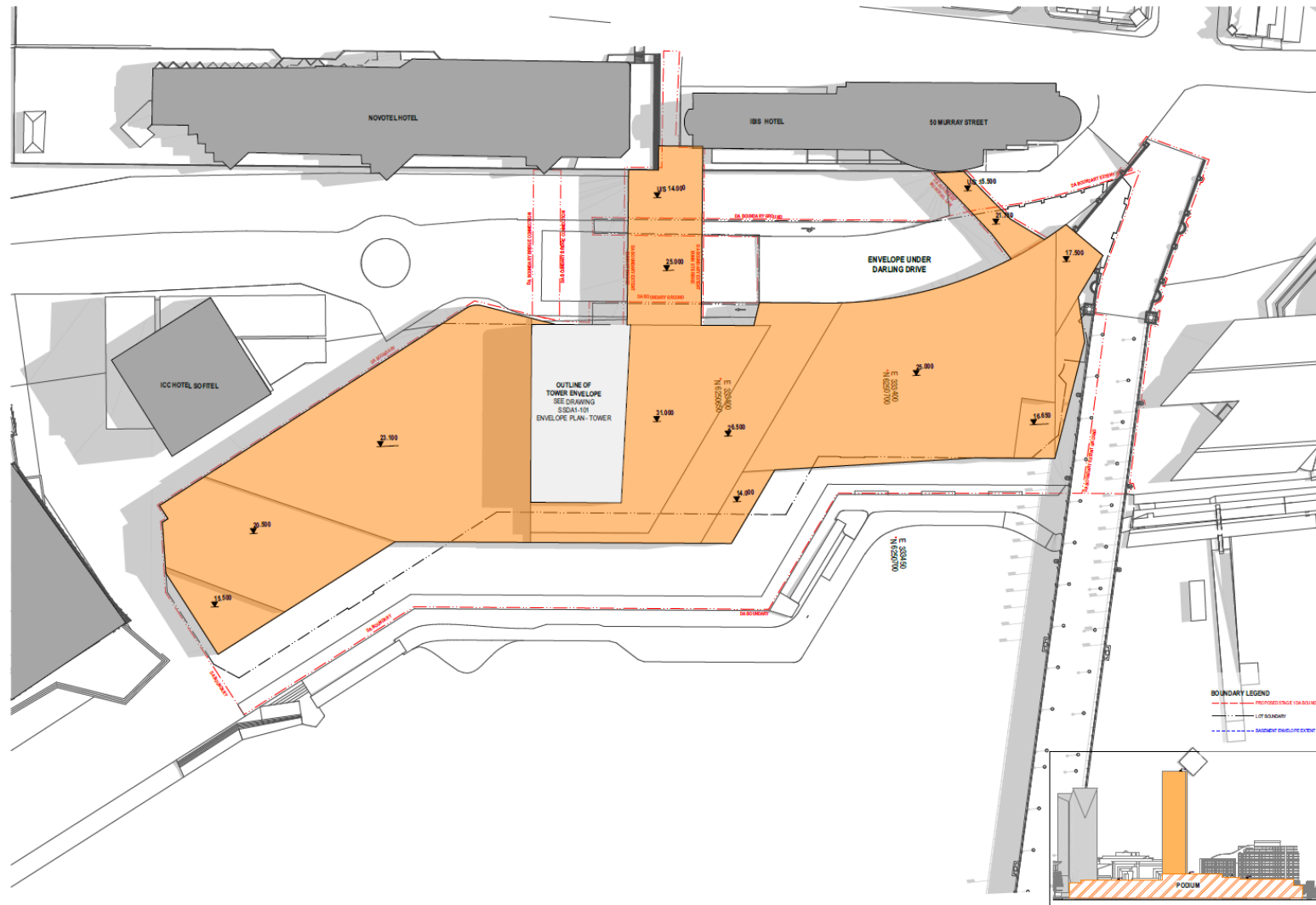


Figure 2.6: Envelope Plan – Podium (Source: FJMT, 2020)

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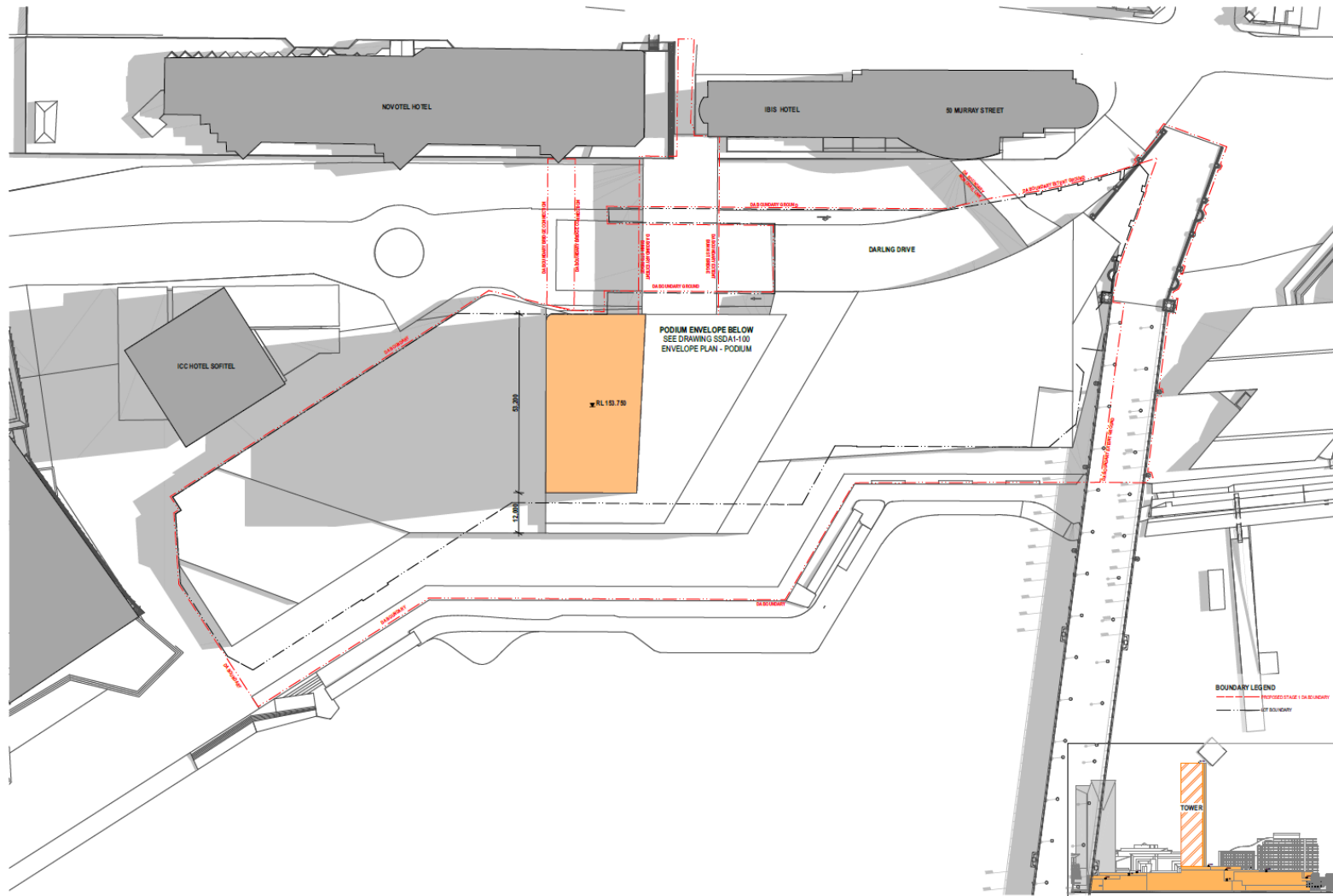


Figure 2.7: Envelope Plan – Tower (Source: FJMT, 2020)



Figure 2.8: Envelope Plan – Basement (Source: FJMT, 2020)

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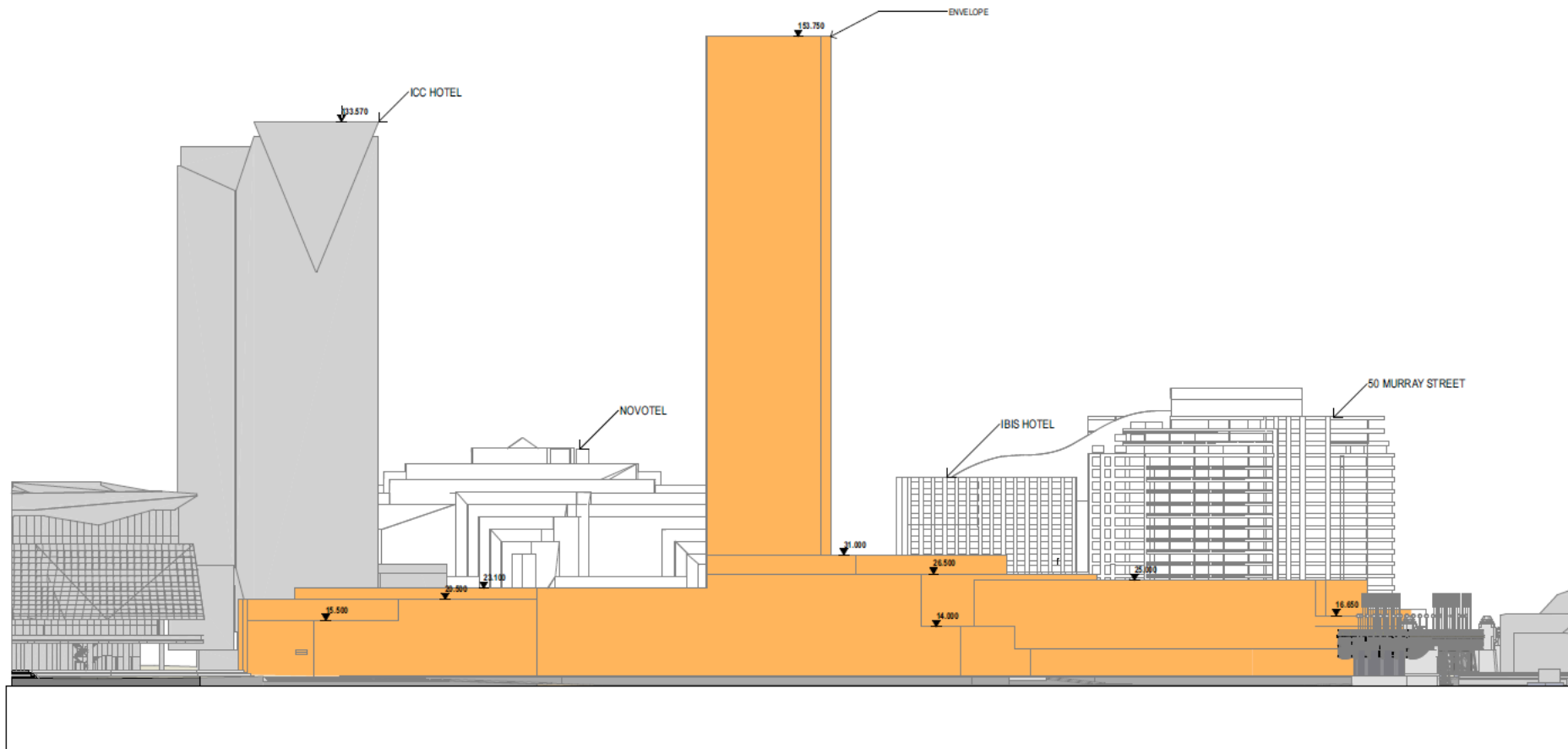


Figure 2.9: Envelope East Elevation (Source: FJMT, 2020)

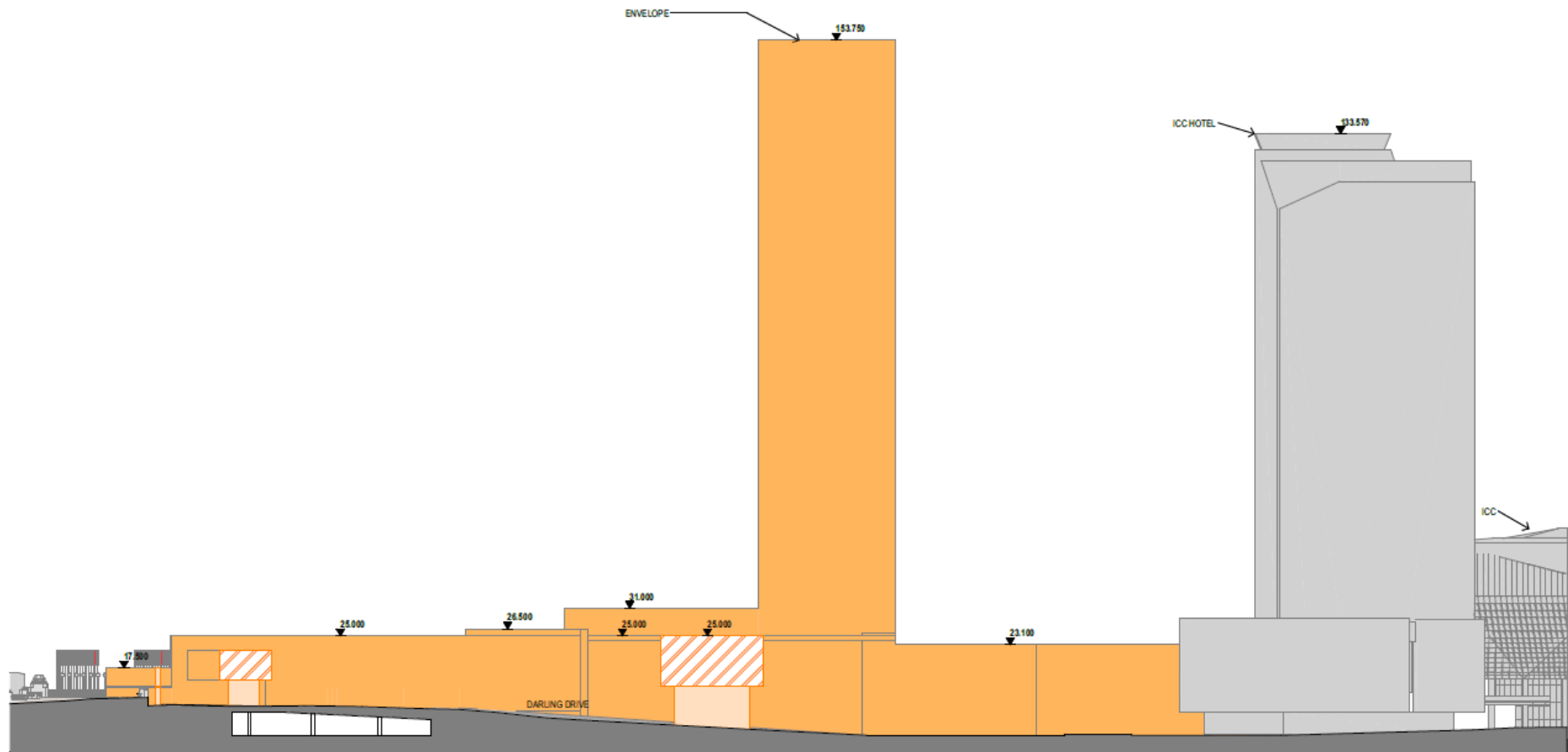


Figure 2.10: Envelope West Elevation (Source: FJMT, 2020)

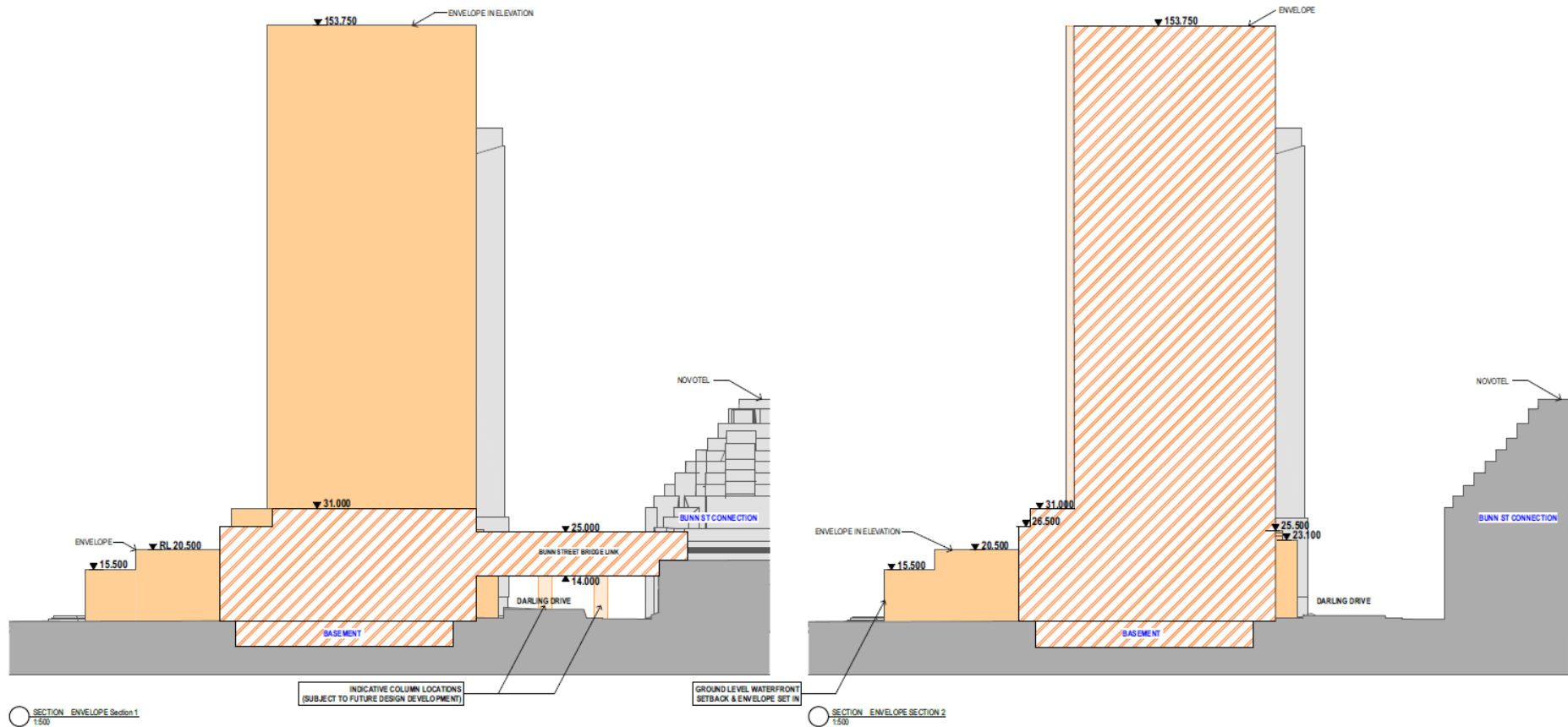


Figure 2.11: Envelope Cross Sections (Source: FJMT, 2020)

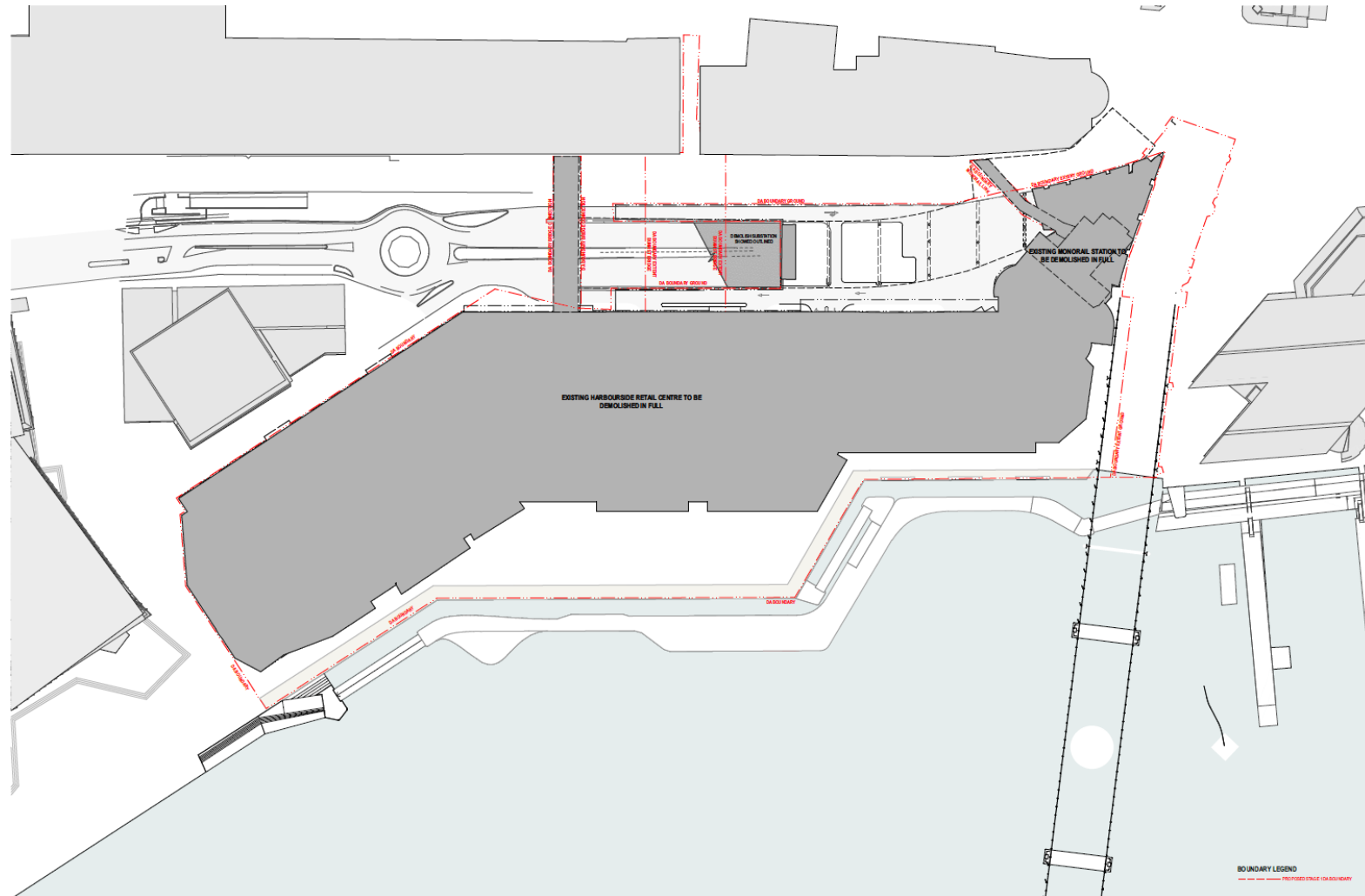


Figure 2.12: Demolition Plan (Source: FJMT, 2020)

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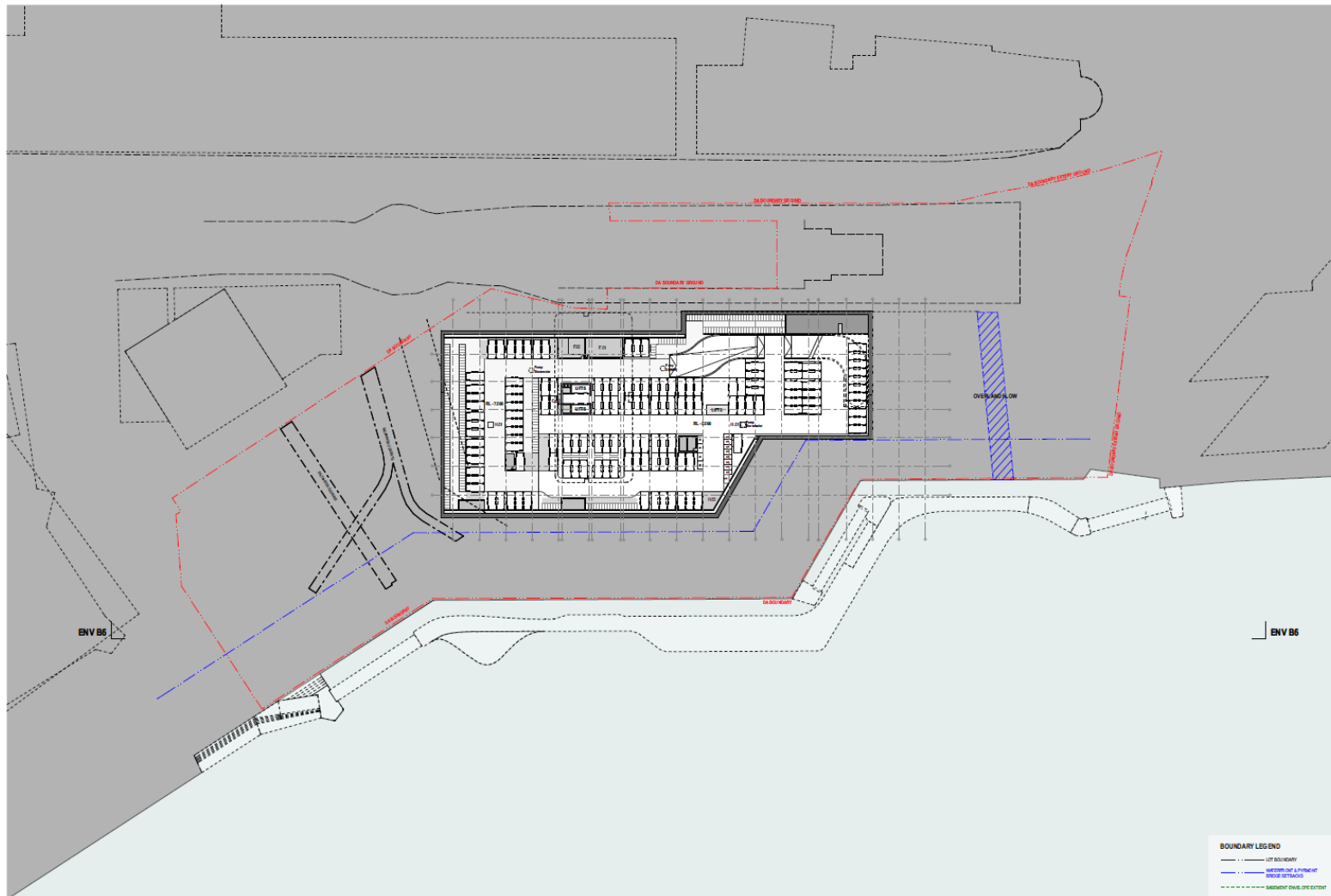


Figure 2.13: Basement 3 – Indicative Plan Only (Source: FJMT, 2020)

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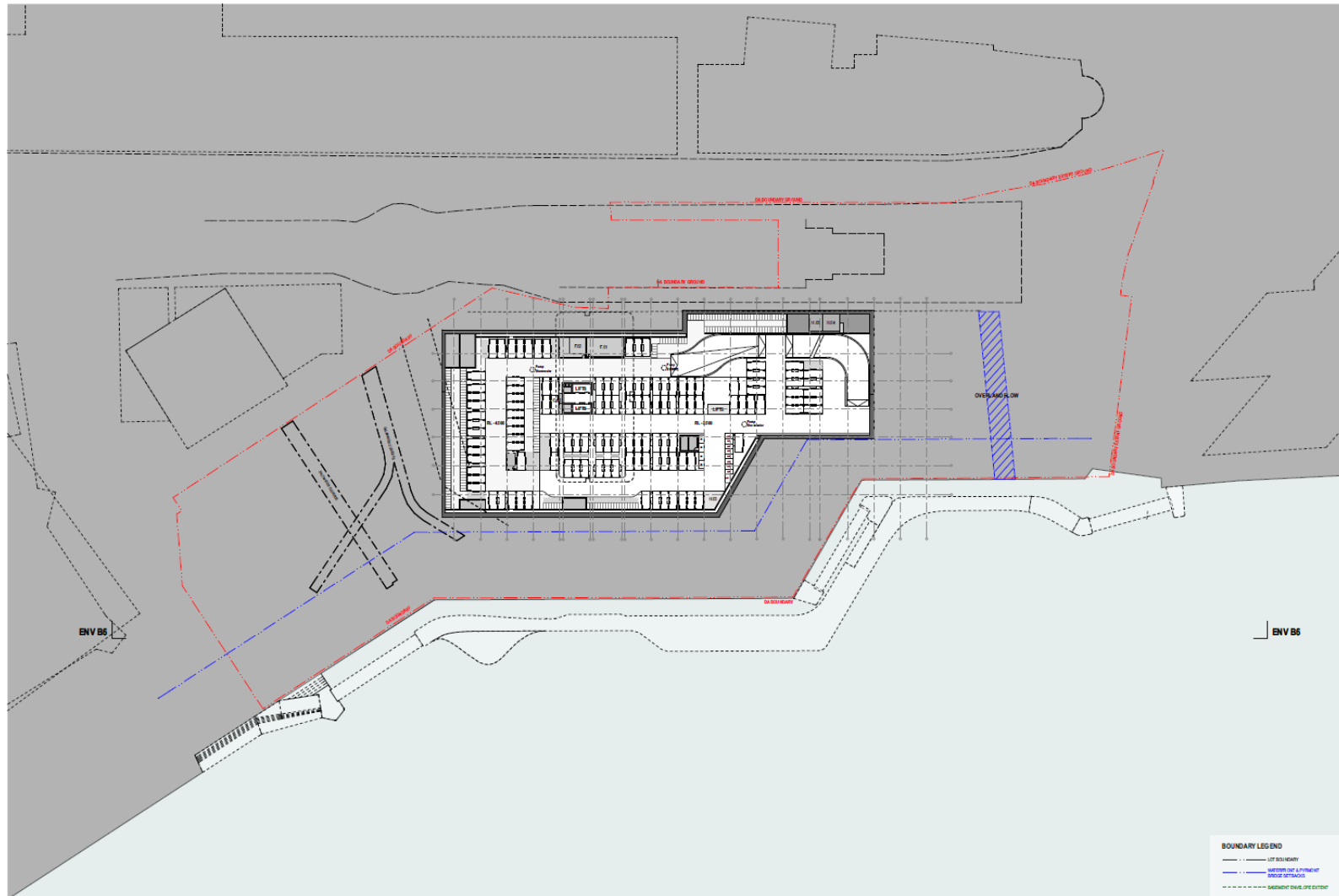


Figure 2.14: Basement 2 – Indicative Plan Only (Source: FJMT, 2020)

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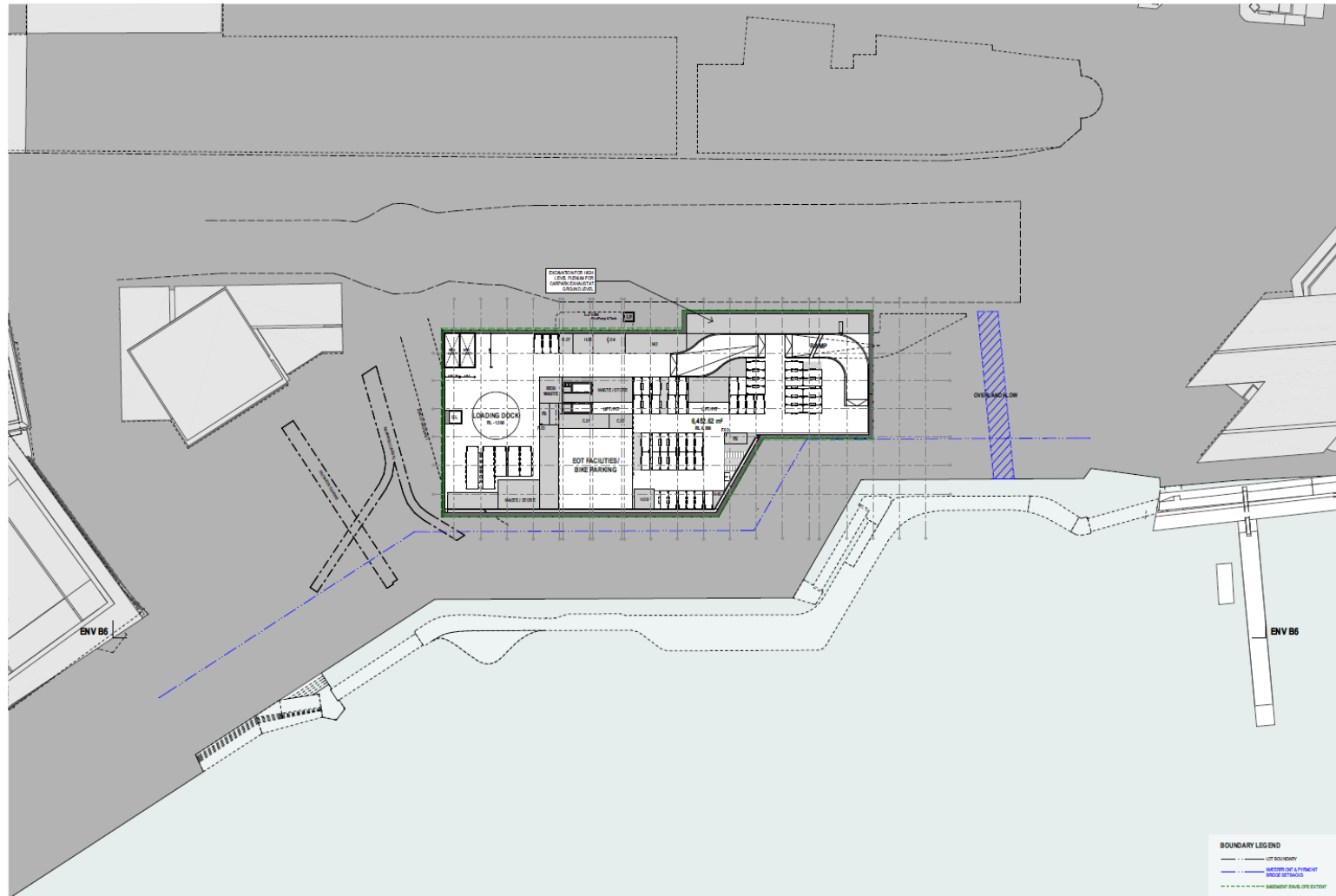


Figure 2.15: Basement 1 – Indicative Plan Only (Source: FJMT, 2020)

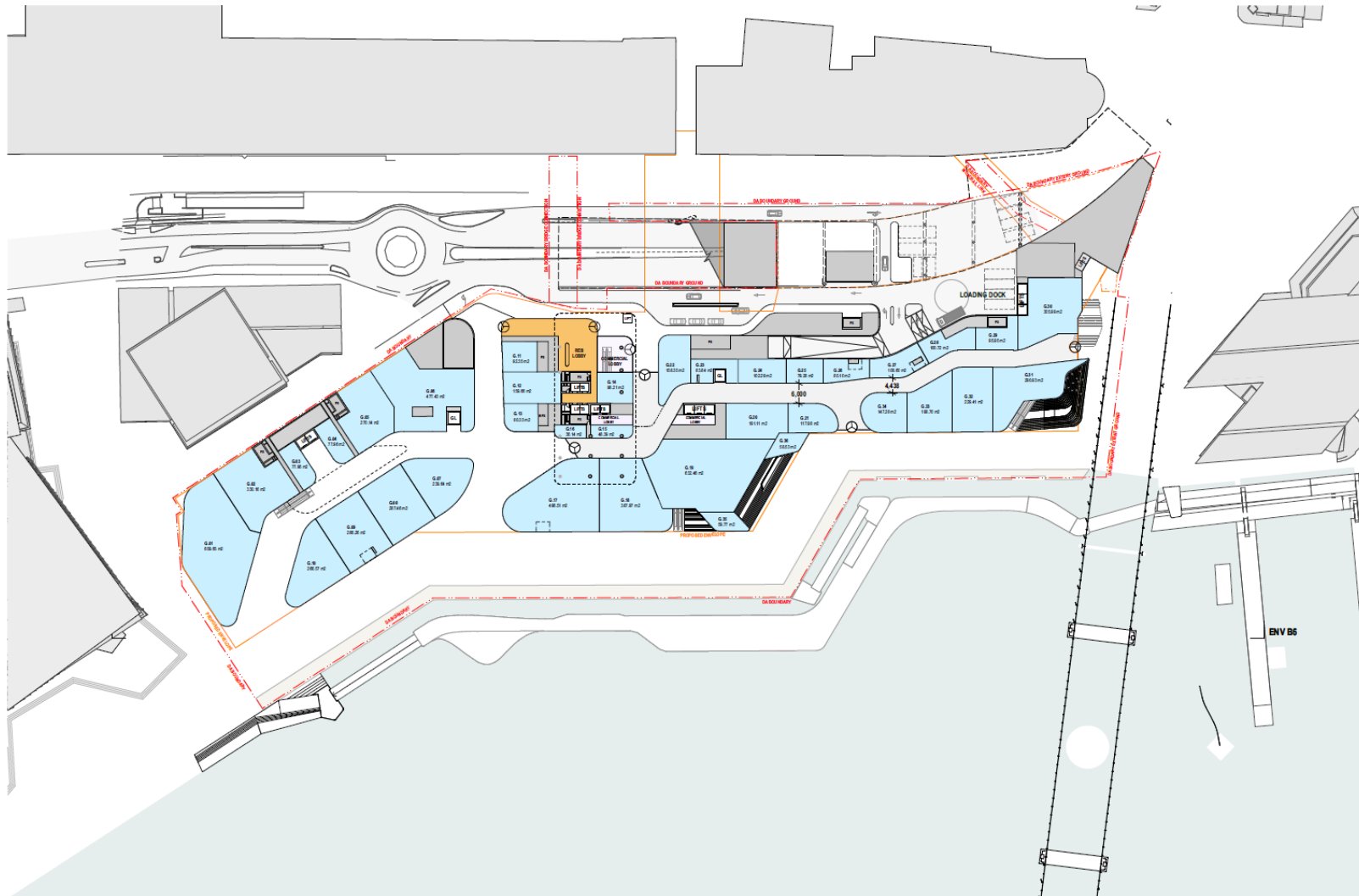


Figure 2.16: Ground Floor Retail – Indicative Plan Only (Source: FJMT, 2020)

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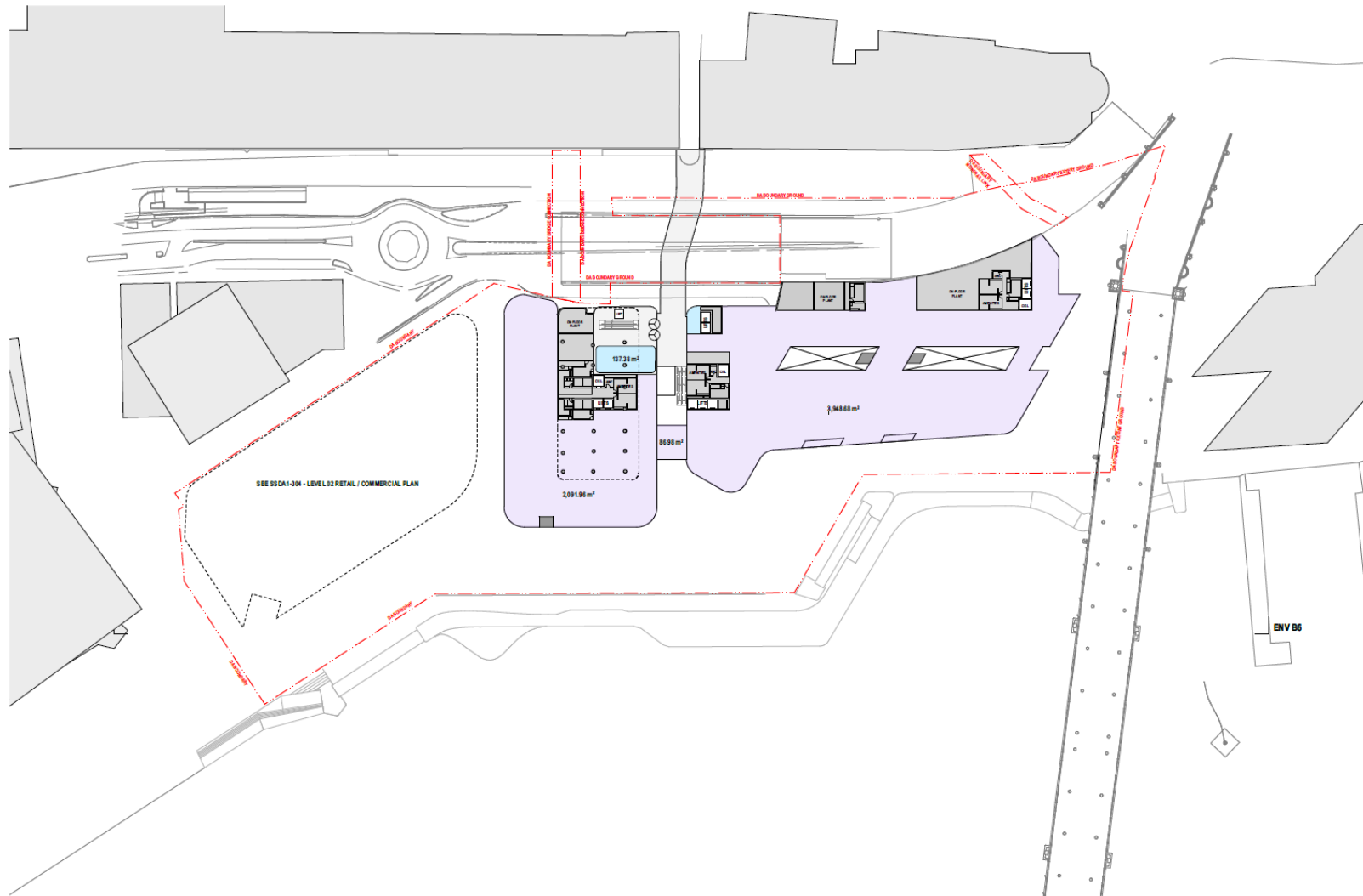


Figure 2.19: Level 3 Commercial – Indicative Plan Only (Source: FJMT, 2020)

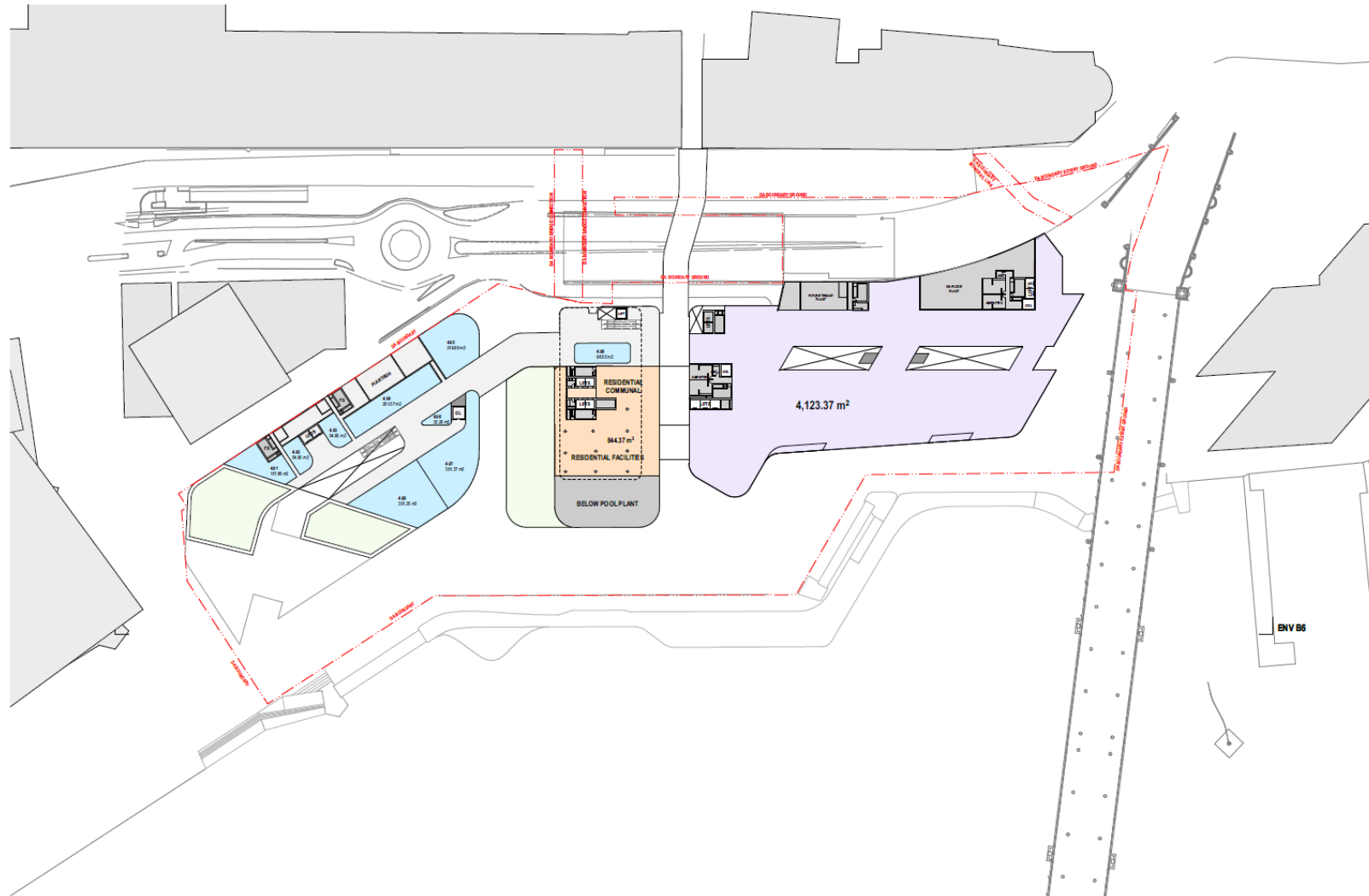


Figure 2.20: Level 4 Retail / Commercial / Residential – Indicative Plan Only (Source: FJMT, 2020)

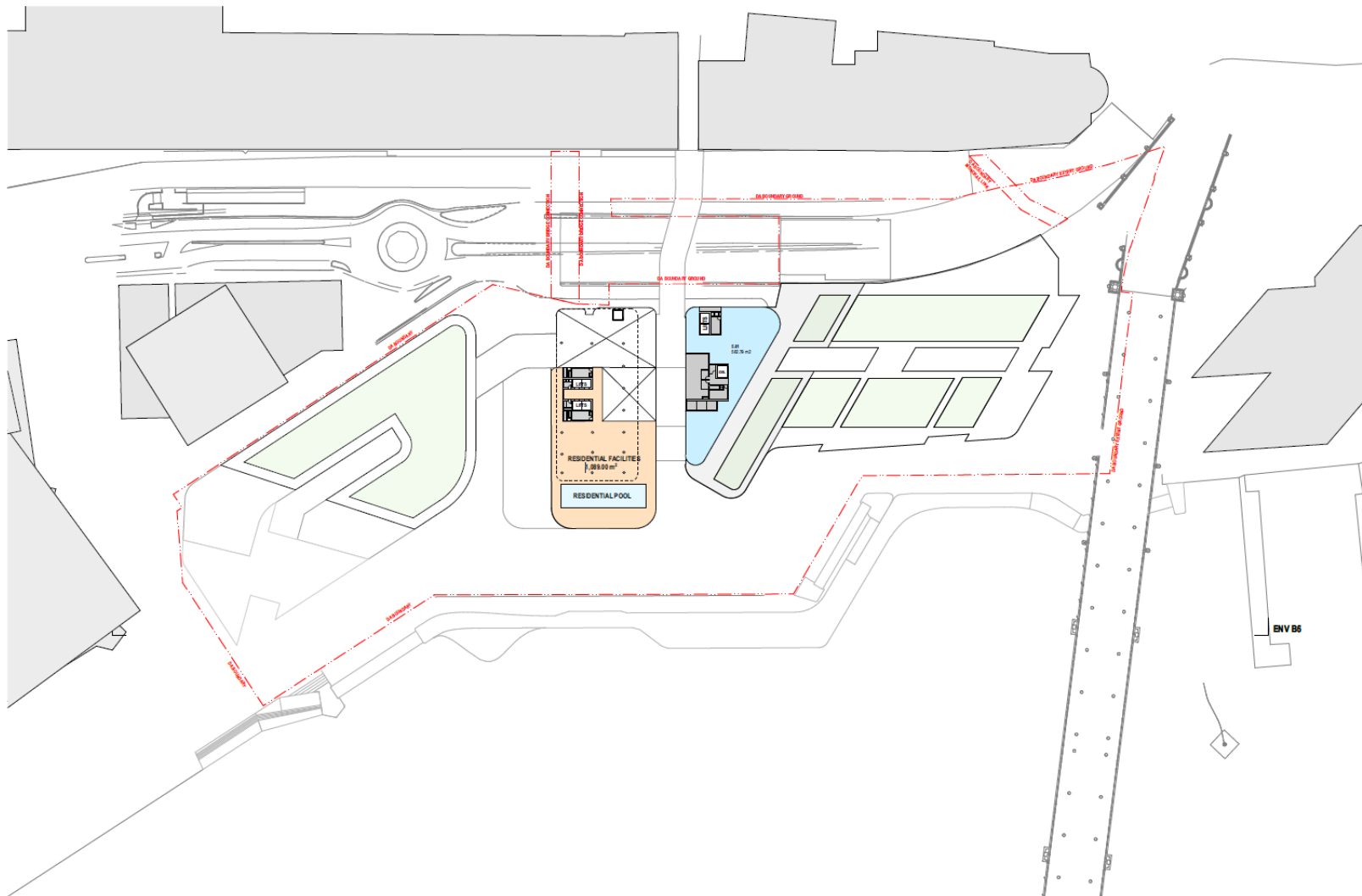


Figure 2.21: Level 5 Retail / Residential & Podium Roof Plan – Indicative Plan Only (Source: FJMT, 2020)

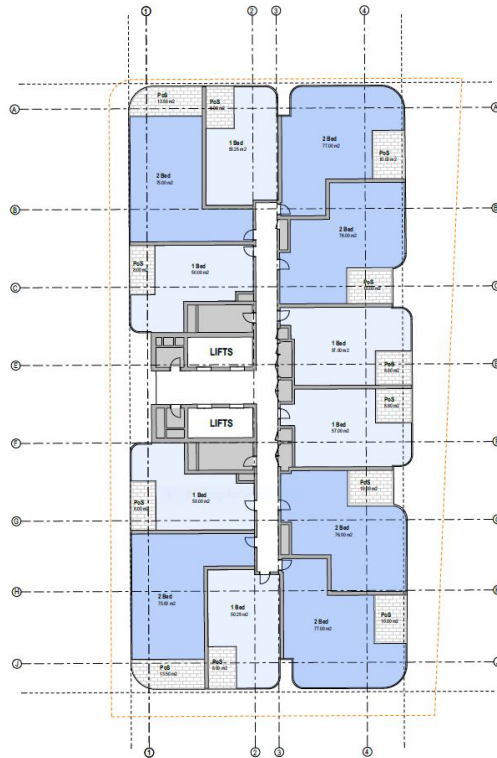


Figure 2.22: Typical 1B – Indicative Plan Only
(Source: FJMT, 2020)

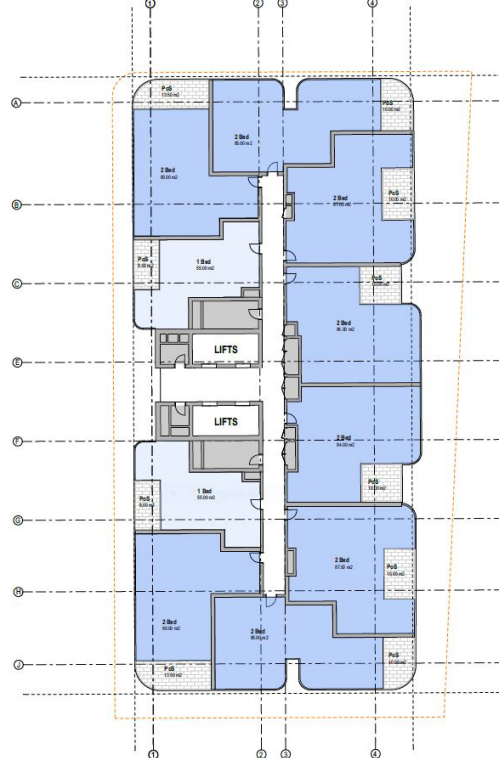


Figure 2.23: Typical 2B – Indicative Plan Only
(Source: FJMT, 2020)

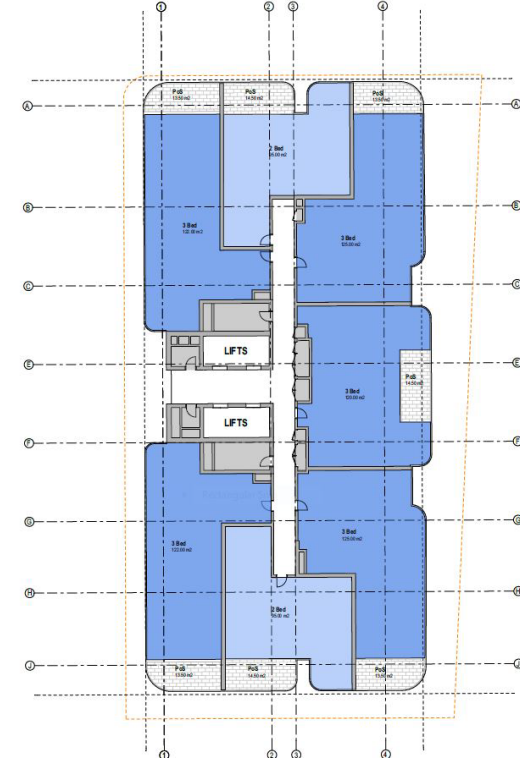


Figure 2.24: Typical 2B – Indicative Plan Only
(Source: FJMT, 2020)

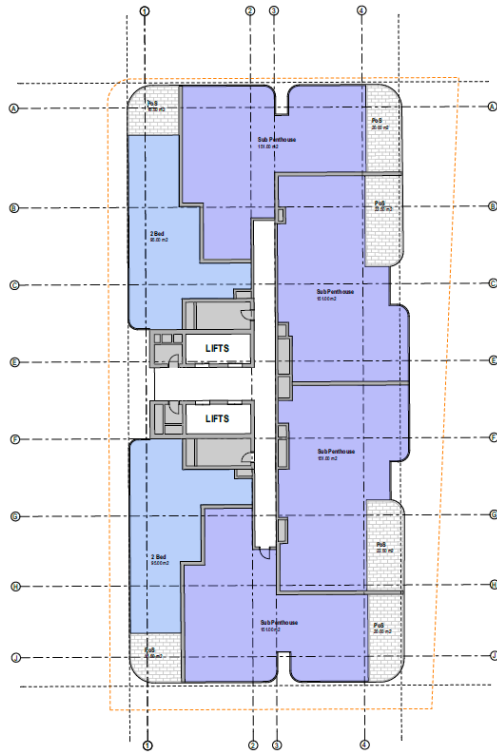


Figure 2.25: Sub Penthouse – Indicative Plan Only
(Source: FJMT, 2020)

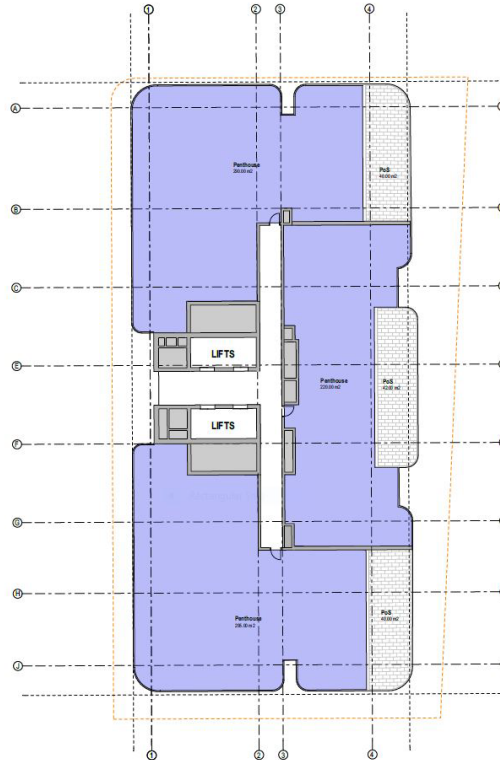


Figure 2.26: Penthouse – Indicative Plan Only
(Source: FJMT, 2020)

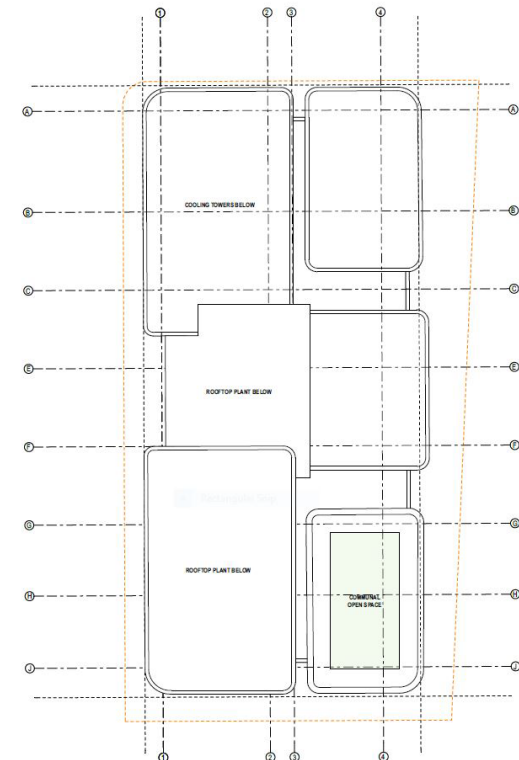


Figure 2.27: Roof – Indicative Plan Only
(Source: FJMT, 2020)

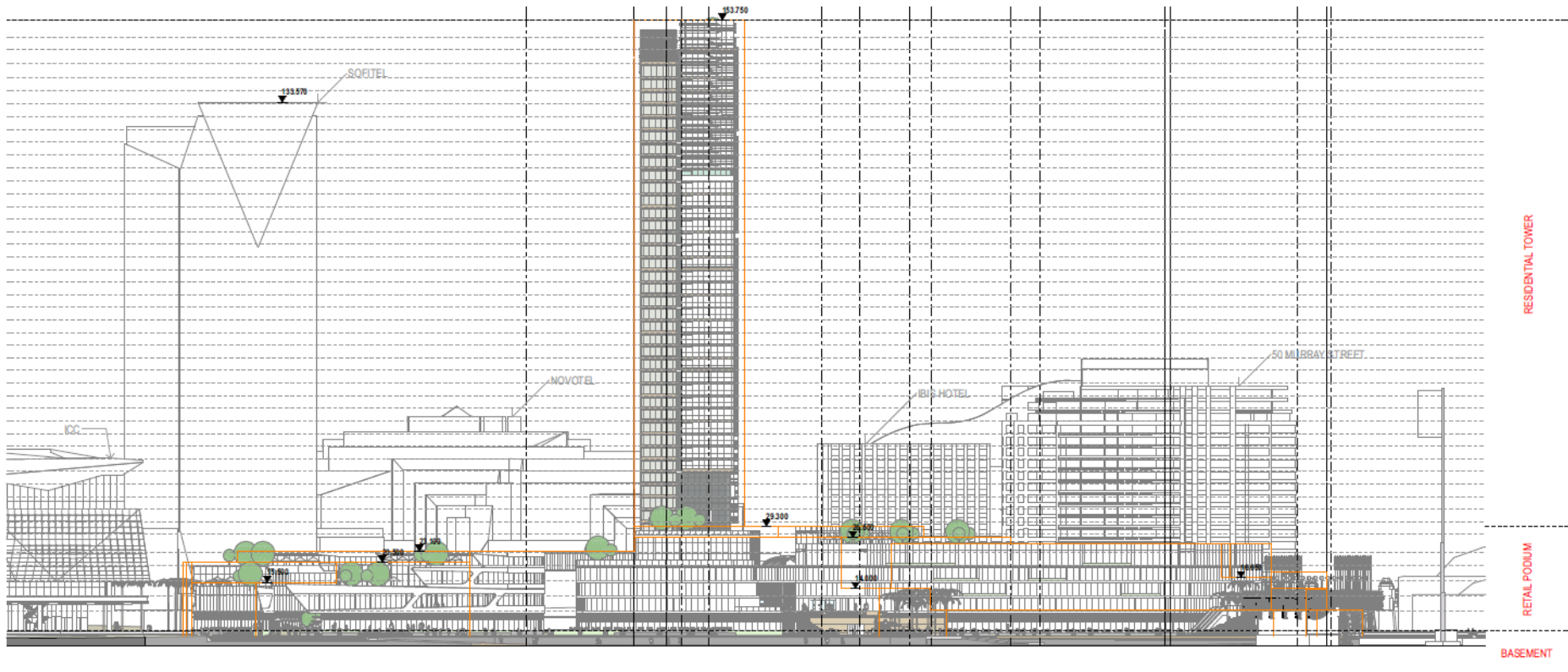


Figure 2.28: East Elevation – Indicative Elevation Only (Source: FJMT, 2020)

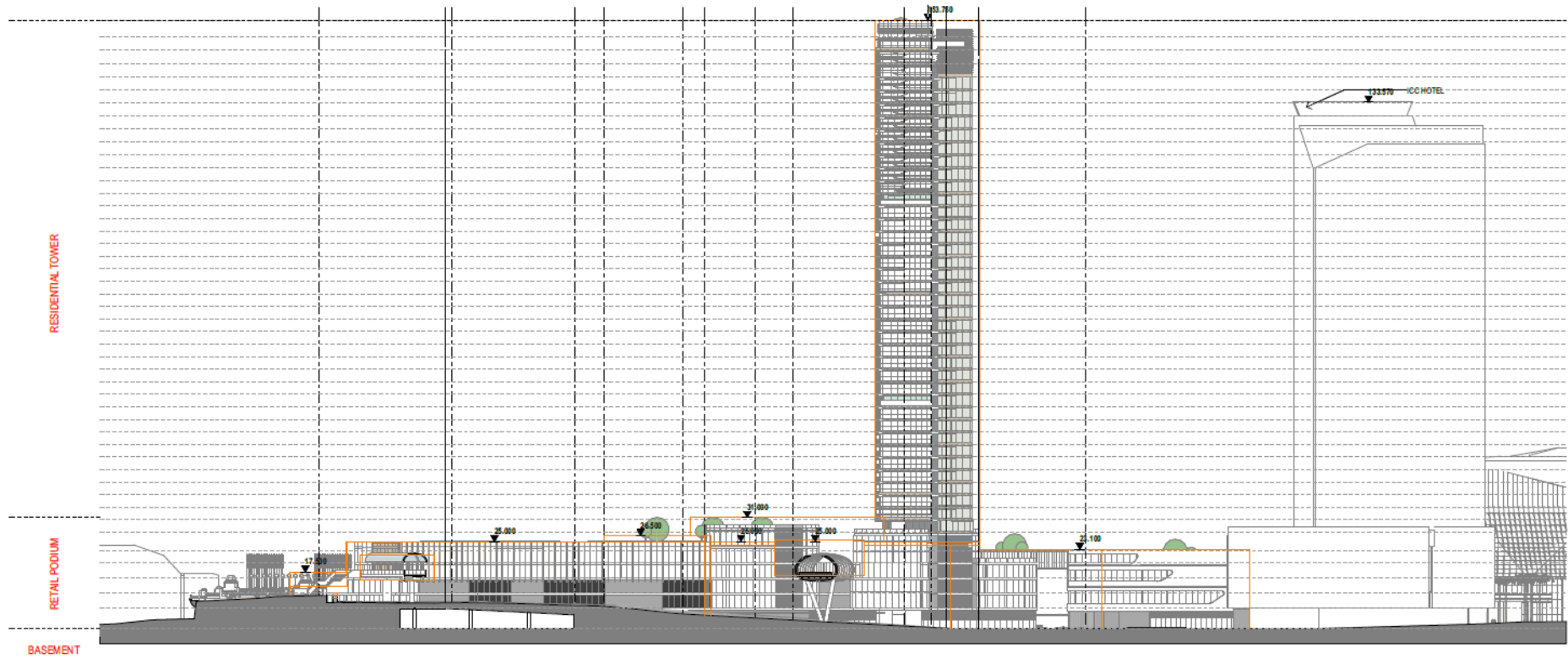


Figure 2.29: West Elevation – Indicative Elevation Only (Source: FJMT, 2020)

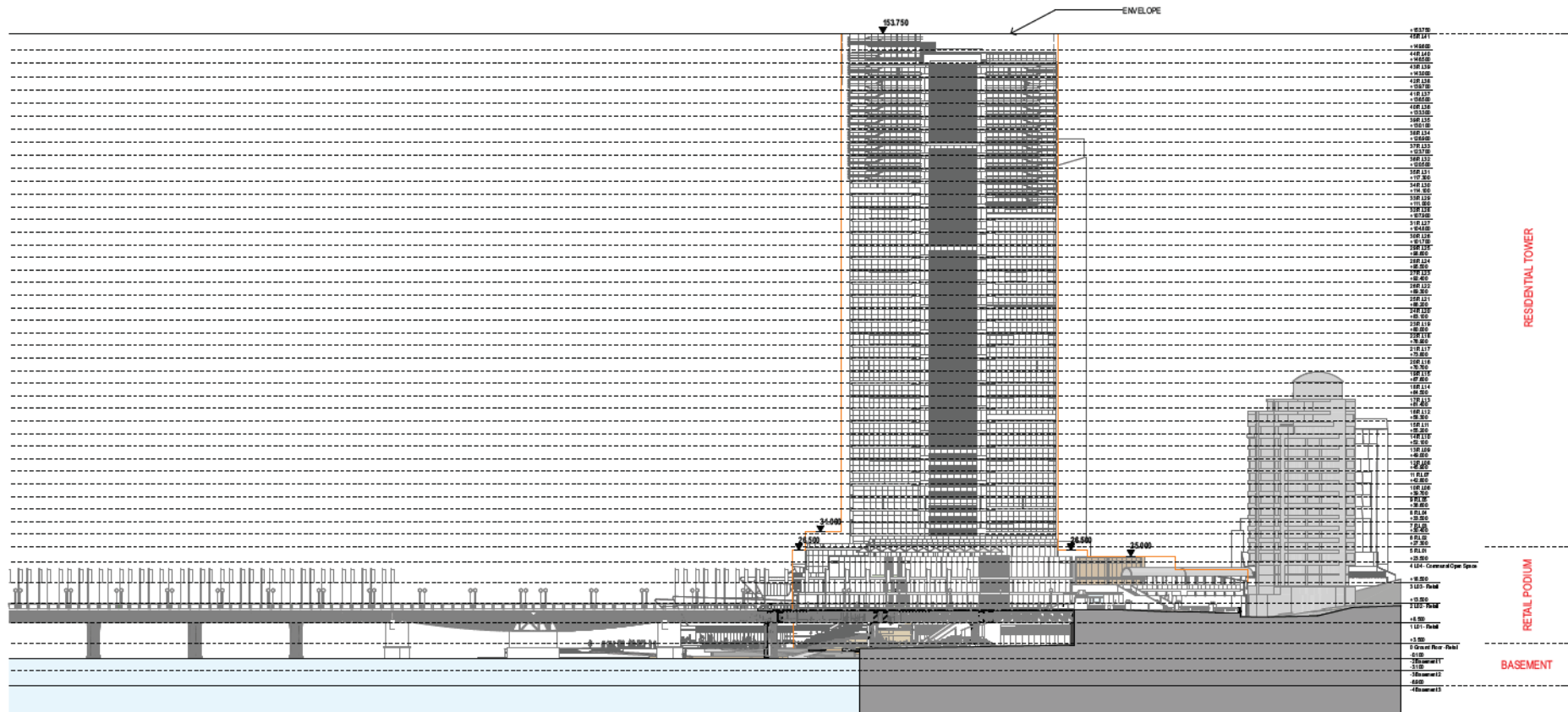


Figure 2.30: North Elevation – Indicative Elevation Only (Source: FJMT, 2020)

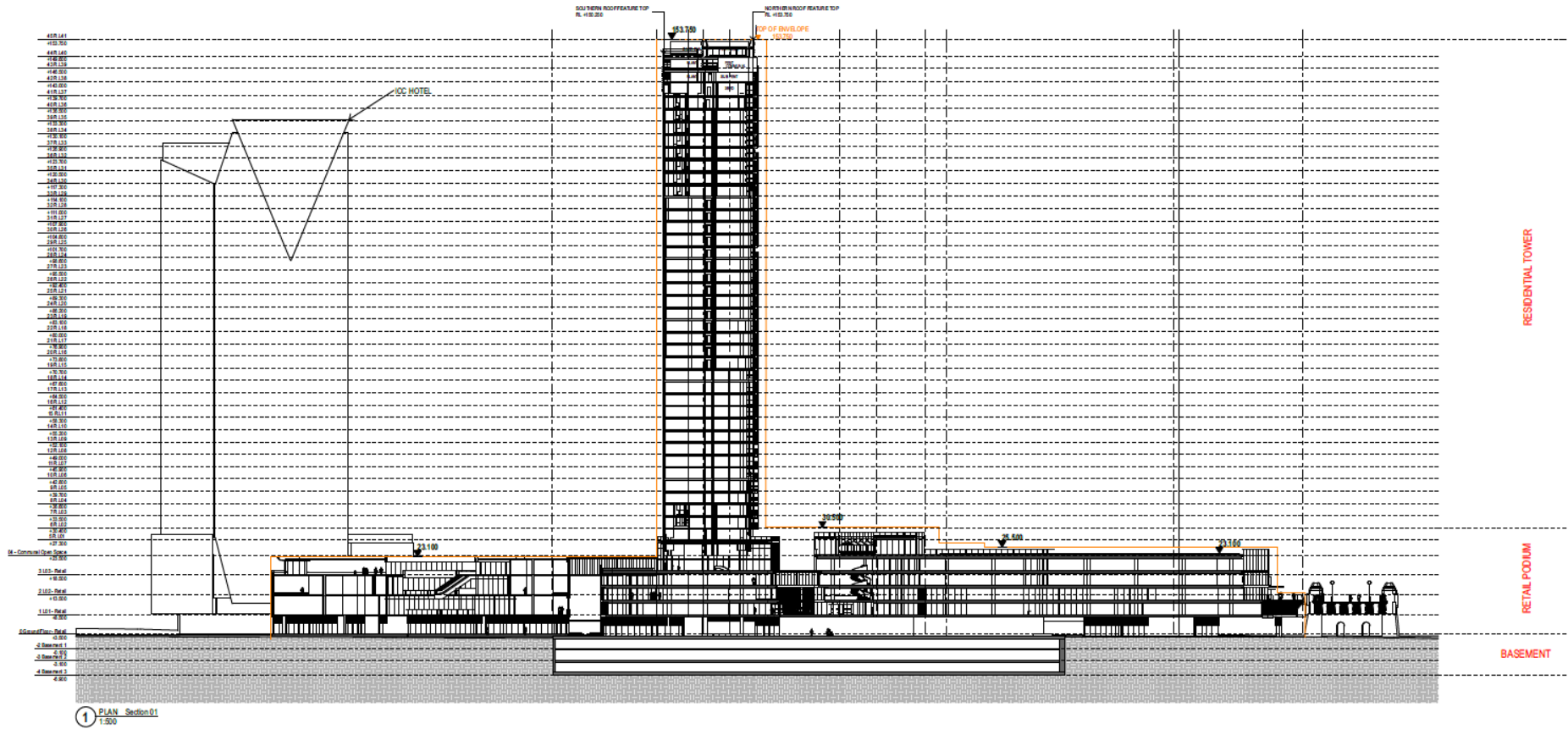


Figure 2.31: Section (23.1) – Indicative Only (Source: FJMT, 2020)

2.3. Visual Impacts

2.3.1. Tower Envelope Relocation and Amendments

The amended development proposal includes the relocation of the tower envelope from the north of the site, to approximately in the centre of the development site. The re-positioning of the tower envelope in the centre of the site, provides a generous distance of c.135m between the tower envelope and Pyrmont Bridge, effectively removing any visual relationship (or potential negative visual impact) between the new tower and the heritage item.

In addition, the reduced distance between the new tower envelope and the new ICC Hotel tower serves to further remove any relationship between Pyrmont Bridge and the new Harbourside tower, by suggesting visual connectivity between the new ICC complex and tower in the south of Darling Harbour, as opposed to association north to Pyrmont Bridge.

The placement of the new proposed tower envelope in the centre of the Harbourside development site places the tower out of the immediately viewline along the west-facing approach across Pyrmont Bridge, and therefore will not alter the aesthetic of the existing modern western backdrop to Pyrmont Bridge, nor detract from the reading of the bridge in its harbour setting when viewed from key public spaces around the harbour.

The reduction in height of the tower envelope by 12.6m (making it closer in height to the new ICC Hotel) will also serve to minimize overshadowing impacts to the public domain and relate better to the height of the ICC Hotel, serving to soften the appearance of the new tower along the western Darling Harbour skyline, making it more consistent in appearance with the existing new development context of the western side of Darling Harbour.

The slight increase in width of the tower envelope footprint, in order to accommodate the required floor space from the reduction of tower height and removal of the 'tail', will have no visual heritage impact, due to the relocation of the tower further from Pyrmont Bridge.

Overall, the amended location and form of the new tower envelope is considered to have a neutral visual impact to the heritage listed Pyrmont Bridge, within the context of the Darling Harbour foreshore.

The impact of the relocated proposed tower envelope on the Pyrmont Heritage Conservation Area (HCA) and the twelve nearby heritage items identified as having local significance has been assessed to be only minor in nature. The relocation of the tower envelope will not further impact the views from the HCA and heritage items towards the Darling Harbour Precinct. The form of the Darling Harbour Precinct already provides a backdrop and an access point between Pyrmont and city will only be slightly altered by the addition of a second tower within the Darling Harbour Precinct.

Generally, the views and vistas between the HCA and heritage items, and Pyrmont Bridge, that were obstructed by the construction of the Novotel Hotel, Hotel Ibis and Residential Apartments will remain unchanged by the proposed development.



Figure 2.32: Visualisation of the tower located in the centre of the development, relocated further south from Pyrmont Bridge. N.B. Indicative design only. Future design development in Stage 2 DA (Source: Virtual Ideas, 2020)



Figure 2.33: Visualisation west across Pyrmont Bridge and Darling Harbour. Tower now distinct element separate from Pyrmont bridge. N.B. Indicative design only. Future design development in Stage 2 Da (Source: Virtual Ideas, 2020)



Figure 2.34: Visualisation of the tower located in the centre of the development, further south from Pyrmont Bridge, removing any visual connectivity to Pyrmont Bridge. N.B. Indicative design only. Future design development in Stage 2 DA (Source: Virtual Ideas, 2020)

2.3.2. Harbourside Shopping Centre

The setback of the northern side of the retail podium envelope as proposed in the original concept plan has been retained through the amended plan. That is, a proposed buffer setback of 10 metres between the retail podium and Pyrmont Bridge, which is an increase from the existing arrangement.

As discussed in the original concept plan HIS, the existing Harbourside Shopping Centre currently abuts the western end of Pyrmont Bridge and does not provide the opportunity for the bridge and its approaches to be adequately viewed and appreciated. The highly negative visual impact of adhoc development too close to the bridge is most evident at the bridge approaches where there are little to no setbacks between the existing built structures associated with the

shopping centre, the remnant fabric of the monorail and the fabric of the bridge itself (Figure 2.35 and Figure 2.36).

The amended proposal provides for the increase of the setback between the existing Harbourside shopping complex from c.4.7m, to a new 10m setback from the Bridge, including the creation of an open, terraced public domain space around the bridge and its approaches (Figure 2.37). This allows for the removal of the key intrusive elements and provides an opportunity for the bridge approaches, undercarriage, trusses and sandstone elements to be better visually appreciated and interpreted.

The reduction of height of podium envelope at the northern end (i.e. adjacent to Pyrmont Bridge) by c.4.5m (from 30.5 RL to RL 25), as proposed in the amended proposal, will serve to further improve the visual relationship between the complex and Pyrmont Bridge, by improving height relationship between the new development and the heritage item.

Therefore, the demolition of the existing Harbourside shopping complex, and replacement with that as per the amended concept design, would, in fact, have a positive visual impact on the relationship between the retail complex, and the heritage listed Pyrmont Bridge. The improvement of this relationship will also serve to provide new opportunities for site users to appreciate visually, and interact with, the heritage form and industrial architecture of the Pyrmont Bridge, which is currently greatly obscured and undervalued.

The addition of landscaping opportunities as proposed through the amended concept plan (including a public observation deck, public event steps, widened public boulevard, and glazing of parts of the retail precinct, allowing views to the harbour from the interior), will further facilitate access and appreciation of numerous views and vistas across Darling Harbour and Pyrmont Bridge, not currently available from the existing Harbourside complex.



Figure 2.35: Current lack of separation between existing buildings and significant heritage fabric of the bridge.



Figure 2.36: Building columns and poorly located shops currently have a major impact on the visibility of the bridge.



Figure 2.37: Improved interface between bridge and northern retail podium, increase in the setback. N.B. Indicative design only. Future design development in Stage 2 DA (source: FJMT, 2020)

2.4. Landscaping and Public Domain Upgrade Works

The key concepts and benefits of the landscaped open space and public domain as proposed through the original development design are retained under the amended Concept Proposal, with the addition of further landscaping opportunities on the northern rooftop extent of the retail podium. Therefore, the assessment of the impact of the landscaping and public domain upgrade works are considered to be as per the original concept proposal.

The landscaping and public domain upgrade works, as described in the draft concept plan prepared by Aspect Studios provides an overall concept plan that will enhance the connectivity between the heritage significance of the former industrial site and the site's redevelopment as a significant destination for the proposed new commercial tenants, retailers, visitors and the local community.

The landscaping and public domain works will encourage site users to spend more time within the public spaces of the site, appreciating the Pyrmont Bridge and harbour as a backdrop to their daily activities, and will allow site users to discover and explore the new heritage interpretive elements proposed -features such as integrated heritage and public art installations, innovative heritage interpretation.

The proposed works contain carefully considered key design features that will create positive heritage outcomes, in terms of providing a much improved, informative and creative interpretation of the significant former uses of the site within public domain spaces, and a greater appreciation of the historic Pyrmont Bridge, the significance of the Darling Harbour Water Feature, the Darling Harbour Rail Corridor, the former Goods Yards and the Water Cooling System and Manifold.



Previously, in removing the industrial fabric of the earlier occupants of the site to create a place for the people, the history and significance of the precinct was overshadowed and removed from the landscape. Whilst it was a product of its time, now such redevelopment would likely be done more sensitively to ensure that the legacy that items such as the Pyrmont Bridge represent, can be appreciated, enjoyed and understood – through a variety of different medias, including in situ retention and reuse of the former buildings. The proposed redevelopment of the Harbourside Shopping Centre and immediate public domain areas allows for the significance of the place to be reinterpreted and placed firmly back into the landscape of Darling Harbour.

3. Response to Submissions—Heritage

The following Response to Submissions (RtS) has been prepared in order to address heritage issues raised as part of the SSDA assessment process for the Harbourside Redevelopment project. The following subsections of the report respond to the specific issues raised. A summary of all submissions with respect to heritage has been provided in Table 1, noting how the amended development addresses or responds to each submission, and where the relevant information has been provided within this report (if relevant).

The main comments are addressed in the sections below, with reference to the amended concept plan, and with any additional information provided, if required.

3.1. Pyrmont Bridge

The primary topic of heritage-related submissions was in relation to concerns about potential physical and visual impacts to Pyrmont Bridge in its setting of the Darling Harbour foreshore, in relation to the proposed development.

The amended concept plan addresses and responds to the majority of these concerns, by relocating the new tower envelope further towards the centre of the site (c.135m setback from Pyrmont Bridge), and the deletion of the originally proposed pedestrian connection to Pyrmont Bridge. The relocation of the tower will mean that there is no visual connectivity between the new tower and Pyrmont Bridge, retaining the existing setting of Pyrmont Bridge without visual impact, from both the western and eastern approaches. The deletion of the proposed pedestrian connection to Pyrmont Bridge means the development will pose no physical impact to the Pyrmont Bridge heritage item, nor any heritage fabric. In addition, the conservation of the surfacing (i.e. proposed new paving commensurate with the heritage item) along the western approach to the Bridge will be a positive outcome for the heritage values of the item.

In addition, the improvement of visual connectivity between the northern section of the new retail podium, as well as the increase of the existing setback of the retail in this location to 10m (from the existing 4.7m setback between the existing Harbourside shopping complex to Pyrmont Bridge), will improve the visual setting of Pyrmont Bridge, as viewed from both the western and eastern sides of the Darling Harbour foreshore.

Overall, it is considered that the widening of the waterfront boulevard (resulting in a total of 352m² being given back to the public domain, compared with the existing arrangement), increased setback of northern retail podium ('ribbon stairs') and addition of the northern observation deck, will significantly improve existing sightlines to Pyrmont Bridge, both from eastern approach and from within the new development.

The amended concept proposal will ensure that the new tower does not compete visually with, nor detract from, the aesthetics and significance of Pyrmont Bridge.

3.2. Other Views and Vistas

The City of Sydney Council noted concern regarding potential impacts upon views to Pyrmont Bridge and the former Goldsbrough Mort Woolstores. The potential impacts to views to Pyrmont Bridge have been addressed and resolved by the relocation of the tower and deletion of the direct pedestrian connection to the Bridge. With regards to the Goldsbrough Mort Woolstores, while it is noted that this is not a heritage listed item, it is also noted that views from the Woolstores have already been impacted by the construction of the ICC Hotel Tower and ICC Complex. It is considered that the proposed Harbourside redevelopment will not further impact these already impaired viewlines.

3.3. Aboriginal Archaeology

An Aboriginal Heritage Due Diligence Assessment process has been followed for the project in accordance with OEH Statutory Guidelines, as presented in Curio Projects 2016, Redevelopment of Harbourside Shopping Centre—Aboriginal Heritage Due Diligence Assessment Report. While the due diligence assessment identified that there is low to nil potential for Aboriginal archaeology to be present within the development site, an Unexpected Finds Protocol for Aboriginal Objects will be developed through Stage 2 SSDA for the site (as recommended in Section 3.2 of the 2016 Curio Projects Aboriginal Heritage Due Diligence report).

3.4. Historical (and Maritime) Archaeology

Submissions to the Harbourside redevelopment concept plan relating to historical archaeology included:

- Requirement for nomination of appropriately qualified archaeological consultant for future archaeological works, and Heritage Induction for on-site contractors and personnel.
- Implementation of a Research Design and Archaeological Excavation Methodology in accordance with Heritage Council Guidelines, preparation of a final excavation report following archaeological works including identification of artefacts and proposed long term storage.
- Requirement for analysis of additional site information including geotechnical data, when available, and service plans should be undertaken to refine the understanding of the site's archaeological potential and level of significance.
- Unexpected Finds Protocol for Historical archaeology.

As stated in the introduction to this report, the Harbourside Redevelopment project is being undertaken as a staged State Significant Development, and as such, this report (and the original HIS, Historical Archaeological Assessment and Aboriginal Due Diligence Heritage Assessment reports, to which this revised Return to Submissions HIS refers), have been prepared with reference to the Stage 1 Concept Plan only.

As such, the exact development ground impacts have not yet been confirmed, as the project is still awaiting detailed design, which will be undertaken as part of the Stage 2 DA process.

Therefore, this assessment of potential archaeological impacts as presented in the Curio Projects 2016 Historical Archaeological Assessment Report for the subject site, is preliminary only (as it relates only to the Concept Plan) and will require revision once specific development impacts are known through the Stage 2 detailed design for the project.

Therefore, it is considered that the majority of the above submission comments regarding historical archaeology will be fulfilled through the Stage 2 DA process. The Stage 2 DA process will include:

- Reassessment of the historical archaeological potential and potential historical archaeological impacts, with regards to the specific development impacts (once known);
- Analysis of additional site information including geotechnical data, when available, and service plans in order to refine the understanding of the archaeological potential of the site;
- Preparation of a Historical Archaeological Research Design and Excavation Methodology to guide archaeological investigation within the subject site;
- Preparation of an Unexpected Finds Protocol for historical archaeology; and
- Preparation of Heritage Induction document for all on-site contractors and personnel.

All future archaeological works at the site would be undertaken by/supervised by an appropriately qualified archaeological team under the guidance of a qualified historical archaeological Excavation Director.

3.5. Heritage Interpretation

Several submissions concerned the need for the preparation of a Heritage Interpretation Plan for the Harbourside Redevelopment project, specific to the built heritage and heritage items, as well as Aboriginal and historical archaeological values of the site.

This was recommended in the original Heritage Impact Statement for the original concept plan (Curio Projects 2016: Section 9.2.4) and is still proposed for the amended concept plan. The Heritage Interpretation Plan for the site will be prepared in accordance with the relevant heritage guidelines issued by the NSW Heritage Division and is proposed to be undertaken as part of the Stage 2 DA, once the specific design and impacts of the development have been confirmed.

Table 3.1: Summary of Heritage Response to submissions

TOPIC	COMMENT	CURIO RESPONSE	RELEVANT SECTION OF REPORT
City of Sydney Council (10 Feb 2017)			
Setting and Views of Pyrmont Bridge	<p>The proposed setbacks, bulk and scale will have adverse impacts on the setting and views of the Pyrmont Bridge. The View Analysis indicates that the development will have a negative impact on the aesthetic values of the Bridge, due to the loss of views and the setback. The proposed 10-metre setback of the podium and the 50m setback of the tower from the Pyrmont Bridge is insufficient to retain the heritage significance.</p> <p>The CMP Policy 10.0 advises: <i>Ensure that new works do not</i></p> <ul style="list-style-type: none"> ▪ <i>Detract visually from the bridge or its harbour setting;</i> <p>Policy 5.3 Views and Vistas advises that: <i>Ensure that an appropriate visual setting is maintained for the Pyrmont Bridge, and that the bridge itself contributes to the character of Darling Harbour as a whole.</i></p> <p>The proposed scheme is not consistent with the above policies.</p>	<p>The relocation of the tower to c.135m setback from Pyrmont Bridge, as well as the deletion of the proposed new pedestrian link between the shopping centre and bridge, will remove any negative visual impact presented by the development to the heritage listed Pyrmont Bridge.</p> <p>In fact, the existing Harbourside complex provides virtually no setback from the bridge and is unsympathetic to the bridge in its current iteration. Therefore the replacement of the existing interface with a c.10m setback of the retail podium from the bridge will be a positive visual impact to the form and presentation of the heritage item in its setting and context along the western side of Darling Harbour.</p> <p>New works are assessed to provide no visual detracting from the Pyrmont Bridge in its harbour setting.</p>	Section 2.3.1 and 2.3.2

TOPIC	COMMENT	CURIO RESPONSE	RELEVANT SECTION OF REPORT
Setback	The setback of the podium from Pymont Bridge should be defined by a 30-degree angle of separation. This would provide a setback of 30m. The setback of the proposed stair from Pymont Bridge should be increased to a minimum of 5m.	Tower has been relocated to 135m distance from Pymont Bridge. Podium setback of c.10m will significantly improve the existing setback of 4.7m, along with the introduction of more sympathetic visual integration between the northern extent of the shopping complex and Pymont Bridge.	Section 2.3.1
Proposed Access Bridge	The access to the Bridge from the proposed tower will result in extreme adverse heritage impacts.	The proposed pedestrian access from the tower to the Bridge has been deleted from the amended proposal. Therefore, there is no longer any need to remove any part of the balustrade.	Section 2.2.1
Adverse impact upon views to the Pymont Bridge and the former Goldsborough Mort Woolstores	The View Analysis demonstrates that the proposal will impact views of the Pymont Bridge and the historic buildings including the Goldsborough Mort Woolstores. This will impact the visual connectivity between the Bridge and the Darling Harbour wharves and railway goods line.	Relocation of tower has removed visual connectivity with Pymont Bridge. Widening of waterfront boulevard and increased setback of northern retail podium ('ribbon stairs') and addition of northern observation deck, have improved sightlines to Pymont Bridge, both from eastern approach and from within the new development.	Section 2.3.1 and 2.3.2

TOPIC	COMMENT	CURIO RESPONSE	RELEVANT SECTION OF REPORT
		<p>Views to Goldsbrough Mort Woolstores have already been impacted by the ICC Hotel Tower and ICC Complex. The proposed development will not impact this further. Sightlines between Goldsbrough Mort and Pymont Bridge are maintained.</p> <p>It is also noted that the heavily modified Goldsbrough Mort Woolstores is not listed as an individual heritage item, and it not identified as requiring unobstructed view lines to Pymont Bridge.</p> <p>Potential visual impacts to views to Pymont Bridge has been addressed and resolved by the relocation of the tower and deletion of the direct pedestrian connection to the Bridge.</p>	
Conservation works to Pymont Bridge	The proposal to 'make good' the Pymont Bridge should be replaced with the term 'conservation'. The important landmark structure should be conserved to a suitable standard of excellence.	Reference to 'make good' has been replaced with 'conservation', where conservation reflects the use of new paving that will be more commensurate with the heritage of the Bridge.	Section 2.2.1

TOPIC	COMMENT	CURIO RESPONSE	RELEVANT SECTION OF REPORT
Woodward Fountain	The proposed tower will overshadow the Woodward Fountain in the middle hours of the day during the winter months. Such overshadowing would have an adverse impact upon the heritage significance.	Tower has been relocated in amended concept proposal. Additionally, the residential tower is slender, therefore associated shadow will be fast-moving, not increasing the overshadowing of the foundation any more than existing surrounding development.	N/A
Appropriation of public-owned land	The sale of prime public land along a substantive section of the foreshore of Darling Harbour for private ownership is not supportable from a heritage perspective.	The NSW Government will remain as the landowner of the site. Mirvac will only be a long-term leaseholder.	N/A
Heritage Interpretation	A heritage interpretation strategy should be developed in close consultation with the City of Sydney and should include not only the interpretation of Pyrmont Bridge but also of this section of the historical development of Darling Harbour and Pyrmont.	Heritage Interpretation Strategy to be developed through Stage 2 DA in consultation with NSW OEH and CoS.	Section 4.2
Aboriginal Heritage	Although the requirements for Aboriginal heritage management in accordance with the NPW Act do not apply to SSD development it should be managed according to best practice as described by the Act, in consultation with NSW Heritage.	Aboriginal Heritage Due Diligence Assessment process has been followed (in accordance with OEH Statutory Guidelines) for the project. Unexpected Finds Protocol for Aboriginal Objects will be developed through Stage 2 SSDA for the site.	Section 2.2.2

TOPIC	COMMENT	CURIO RESPONSE	RELEVANT SECTION OF REPORT
NSW Heritage Council (14 Feb 2016)			
Historical Archaeology	An archaeological consultant should be nominated for the works. All contractors and personnel are to be inducted and informed by the archaeological consultant prior to work commencing. All state significant archaeological items must be retained.	<p>Agreed. All archaeological works would be undertaken by/supervised by an appropriately qualified archaeologist (who meets Excavation Director requirements).</p> <p>A Heritage induction should be prepared and undertaken prior to construction works for all site contractors and personnel.</p>	Section 3.4
	A Research Design and Archaeological Excavation Methodology must be prepared in accordance with Heritage Council Guidelines. The final excavation report should identify artefacts and long-term storage. The Interpretation Plan should be prepared in accordance with the Guidelines issued by the Heritage Council NSW.	Agreed. As recommended in Section 6.2 of the Archaeological Assessment	<i>Historical Archaeological Assessment</i> (Curio Projects 2016), Section 6.2
	Unexpected finds protocol	Agreed. An Unexpected Finds Protocol for Historical archaeology will be developed through the Stage 2 DA process, in association with the ARD.	Section 3.4

TOPIC	COMMENT	CURIO RESPONSE	RELEVANT SECTION OF REPORT
	Specific assessment of any potential remains of former bridges which may predate the Pyrmont Bridge structure or punt crossing sites	Provided in the Historical Archaeological Assessment (Curio Projects 2016), and will be further developed in relation to potential archaeological impacts, once exact development impacts are confirmed through Stage 2 DA process.	<i>Historical Archaeological Assessment</i> (Curio Projects 2016), Section 4.3
	Analysis of additional site information including geotechnical data, when available, and service plans should be undertaken to refine the understanding of the site's archaeological potential and level of significance.	Agreed. Will be integrated once available into Stage 2 DA documentation and archaeological research design.	Section 3.4
	Impacts to archaeological relics of local or State significance associated with the development should be managed in accordance with current NSW Heritage Division guidelines.	Agreed. Addressed in Historical Archaeological Assessment	<i>Historical Archaeological Assessment</i> (Curio Projects 2016), Section 6.2
Maritime Archaeology	The site has archaeological potential. There is potential for former seawalls, wharves, piers and jetties and also possible ship or boat wrecks.	Agreed. Addressed in Historical Archaeological Assessment report. Historical Archaeological Assessment has acknowledged moderate potential for archaeological evidence for such features	<i>Historical Archaeological Assessment</i> (Curio Projects 2016), Section 4.2.1

TOPIC	COMMENT	CURIO RESPONSE	RELEVANT SECTION OF REPORT
	<p>Procedures for the treatment of any newly discovered archaeological sites, prior to the commencement of any works at the site.</p> <p>Remote sensing and/or diver surveys of the seabed under any piled areas that currently form waterfront or paved areas of the proposed development</p> <p>Demolition works and any proposed excavation works should be monitored and undertaken by a suitably qualified and experienced maritime archaeologist.</p>	<p>to survive. The potential impact to these potential features, and proposed mitigation measures, will be addressed through the Stage 2 DA, once exact development impacts have been confirmed.</p> <p>To be addressed through the Stage 2 DA process and ARD.</p> <p>To be addressed for relevance/requirement for the development following confirmation of specific development impacts, to be addressed through the Stage 2 DA process and ARD.</p> <p>Should demolition and/or proposed excavation works be required to extend into the harbour/to the seabed (the requirement for which will be determined/confirmed through the Stage 2 DA process), a suitably qualified and</p>	

TOPIC	COMMENT	CURIO RESPONSE	RELEVANT SECTION OF REPORT
		experienced maritime archaeological would be nominated to undertake these works.	
Aboriginal Archaeology	<p>Assessment of any potential submerged Aboriginal sites or relics in current and formerly submerged sections of the site.</p> <p>Should unexpected finds such as Aboriginal stone artefacts or shell middens be located during the development, work should cease in the immediate vicinity of the find and the project archaeologist must be notified, in accordance with the unexpected finds protocol.</p>	<p>The Aboriginal Heritage Due Diligence assessment for the subject site has identified that the development site is almost wholly located within reclaimed land, and therefore would have previously been submerged, with low to nil potential for Aboriginal objects or sites to be present.</p> <p>Recommended through the Aboriginal Heritage Due Diligence Assessment. To be developed through Stage 2 DA.</p>	<p>Section 3.3 <i>Aboriginal Due Diligence Assessment</i> (Curio Projects 2016), Section 2.1.3</p> <p>Section 3.3 <i>Aboriginal Due Diligence Assessment</i> (Curio Projects 2016), Section 3.2</p>
Built Heritage/Visual Impact	The proposed new level 02 direct pedestrian bridge connection from the shopping centre podium to Pyrmont Bridge shall be deleted to avoid visual and physical impacts on Pyrmont Bridge.	Pedestrian bridge has been deleted in amended concept plan.	Section 2.2.1

TOPIC	COMMENT	CURIO RESPONSE	RELEVANT SECTION OF REPORT
	<p>All building elements above the Pyrmont Bridge deck should be further set back from the Bridge to allow open views to and from the bridge and minimise visual impact of new buildings.</p> <p>Specific assessment of the visual impact of the proposed works on the identified heritage values of SHR-listed sites (e.g. the SHR listed Pyrmont Bridge) and the removal of the SS South Steyne from the area. If there is no visual impact on these items, this should be specifically stated.</p>	<p>Retail podium envelope height has been reduced by 4.5m and has a setback of c.10m from Pyrmont Bridge. Tower envelope has been relocated to be c.135m from Pyrmont Bridge.</p> <p>The amended proposal will have a neutral visual impact to the State heritage listed Pyrmont Bridge.</p>	<p>Section 2.3.2</p> <p>Section 2.3.1</p>
National Trust of Australia (14 February 2017)			
Impact to Pyrmont Bridge	The proposed development will have a major visual impact on the of Pyrmont Bridge, concern about trivialization of the bridge by tower development, removal of some of Bridge stone balustrade for new pedestrian bridge access from original tower location.	<p>Amended development proposal relocates tower envelope to c.135m setback from Pyrmont Bridge, removing visual impact.</p> <p>Deletion of proposed new pedestrian access bridge connecting to Pyrmont Bridge, therefore no longer requiring removal of any stone balustrade or physical impact to the bridge.</p>	Section 2.2 and 2.3

TOPIC	COMMENT	CURIO RESPONSE	RELEVANT SECTION OF REPORT
Public Submissions			
Tower and Pymont Bridge	<p>The development is too tall for an area that has height restrictions, it is overpowering the Pymont Bridge, a place of historical beauty.</p> <p>Pymont Bridge is the busiest bridge in the Southern Hemisphere, there is opportunity to celebrate it with a new development.</p> <p>The development will detract from the significant heritage values of the bridge.</p> <p>The massing across the site to the north is inconsistent with the proximity of the Bridge</p> <p>Any development must preserve and enhance the heritage values of the bridge. It will destruct the architectural and heritage history of Pymont Bridge.</p>	<p>Reduction of the height of tower, relocation of tower envelope towards centre of site, creating c.135m setback from Pymont Bridge, removing any visual connectivity between heritage item and tower. No physical or visual impacts proposed to Pymont Bridge through amended proposal.</p> <p>Public domain proposal and improved interface between shopping complex and bridge (c.10m setback) will improve setting of western side of Pymont Bridge, allowing improved celebration of its heritage values and setting.</p> <p>Heritage Interpretation Plan to be developed through Stage 2 DA to enhance heritage values of Bridge in harbour context.</p>	Section 2.3 and 3.1
Podium	<p>The building envelope appears to depict the podium as being much closer to the Pymont Bridge than the existing building. The proposal</p>	<p>The proposed setback from the new retail podium envelope will increase the setback from Pymont Bridge from c.4.7m (current</p>	Section 2.3.2

TOPIC	COMMENT	CURIO RESPONSE	RELEVANT SECTION OF REPORT
	includes the removal of the heritage bridge railing which should be protected.	setback of existing Harbourside shopping complex) to c.10m setback.	
100-year lease	The Harbourside site is owned by the people of NSW and its usage, stipulated in its 100-year lease, signed in 1988 is as a Festival Market including retail, restaurant and entertainment complex. Residential usage is not included in the lease. The proposed development is contrary to the terms of the lease.	NSW Government will remain the landowner of the site. Mirvac will only be a long-term leaseholder.	N/A

4. Conclusions and Recommendations

4.1. Conclusions

Since exhibition of the Harbourside Redevelopment Stage 1 SSDA proposal, given the nature and range of submissions made from agencies and the public, Mirvac has reviewed the overall approach and elements of the Concept Proposal for the project. This led to the development of an Amended Concept Proposal, in order to address matters raised in the submissions and deliver an overall significantly improved outcome on the site and for the broader Darling Harbour precinct.

The main modifications as relevant to heritage, proposed through the amended concept plan include:

- Relocation of the new tower envelope further away from Pyrmont Bridge (c.135m south of the Bridge);
- Reduction in the height of the new tower envelope from RL 166.35 to RL 153.75;
- Deletion of the proposed pedestrian access way between the new development and Pyrmont Bridge;
- Setback of retail podium envelope to 10m from Pyrmont Bridge; and
- Reduction in height of the retail podium envelope adjacent to Pyrmont Bridge.

Other key concepts and elements of the original concept plan including proposed basement excavation and landscaped open space and public domain concepts remain unchanged through the amended concept plan.

These key changes have been assessed in relation to their potential heritage impacts, and the following conclusions have been made:

- The deletion of the pedestrian access bridge linking to Pyrmont Bridge means that the amended proposal will have no physical impact on any heritage item or significant heritage fabric.
- The re-positioning of the tower envelope in the centre of the site provides a generous distance of c.135m between the tower and Pyrmont Bridge, effectively removing any visual relationship (or potential negative visual impact) between the new tower and the heritage item.
- The placement of the new proposed tower envelope in the centre of the Harbourside development site places the tower out of the immediately viewline along the west-facing approach across Pyrmont Bridge, and therefore will not alter the aesthetic of the existing modern western backdrop to Pyrmont Bridge, nor detract from the reading of the bridge in its harbour setting when viewed from key public spaces around the harbour.

- Therefore, the amended location and form of the new tower envelope is considered to have a neutral visual impact to the heritage listed Pyrmont Bridge, within the context of the Darling Harbour foreshore.
- The reduction in height of the retail podium envelope adjacent to Pyrmont Bridge, the increase in the setback of the retail complex from the current c.4.7m, to c.10m, and the improvement of public domain works and general interface zone between the new retail podium and Pyrmont Bridge will present a positive visual impact on the relationship between the retail complex, and the heritage listed Pyrmont Bridge.
- The improvement of the relationship between the retail complex and Pyrmont Bridge from existing will also serve to provide new opportunities for site users to appreciate visually, and interact with, the heritage form and industrial architecture of the Pyrmont Bridge, which is currently greatly obscured and undervalued.

Overall, the amended concept plan is assessed to present no adverse physical or visual impact to any identified local or State heritage listed items.

The potential impacts to historical archaeological resources will require further assessment through the Stage 2 DA process, once specific development impacts are known.

4.2. Recommendations

The following recommendations are made for the proposed Harbourside Development, revised from the 2016 HIS report, to address the amended concept proposal and the response to submissions relating to heritage.

4.2.1. Built Heritage

The proposal to design the new shopping centre with many public domain spaces that open up views to the harbour, Pyrmont Bridge, the city and its surrounds, should continue to form the basis of future detailed design.

The relocation of the tower envelope has removed any significant visual relationship between the tower and Pyrmont Bridge, and therefore should be supported on heritage grounds. While the development will present no physical or visual impact to the State heritage listed Pyrmont Bridge, the final tower design should still seek to provide sensitive design solutions in consideration of the final form and materiality of the tower, in order to ensure that the tower does not detract from views from the east towards, and along the eastern approach to the Pyrmont Bridge.

The proposal to improve the spaces in and around the Pyrmont Bridge approach, and possible bridge surface works (i.e. conservation of Pyrmont Bridge) is to be further developed and encouraged as part of more detailed designs. In particular, the proposal to improve the intrusive interface between the existing Harbourside Shopping Complex and Pyrmont Bridge, should be supported on heritage grounds, improving the readability of the bridge from the western foreshore below the bridge, as well as along the eastern approach.

4.2.2. Aboriginal Archaeology

While it is assessed that there is low to nil potential for Aboriginal objects to be present within the study area, nor impacted through the proposed redevelopment, the potential for unexpected relics and/or Aboriginal objects to be discovered will be managed through the instigation of an Unexpected Finds Protocol for Aboriginal Objects for the site during development works (to be developed through the Stage 2 SSDA). This would be managed via the appointment of an overseeing project archaeologist for the site who will ensure that any unexpected finds are managed appropriately in accordance with the Unexpected Finds Protocol, and reported to the statutory authorities in accordance with the provisions of the NSW National Parks and Wildlife Act, as required.

4.2.3. Historical Archaeology

The following recommendations are made for the Harbourside Redevelopment project, with reference to historical archaeology:

- Any future historical archaeological works should be undertaken and supervised by an appropriately qualified archaeological consultant.
- A Heritage Induction, specific to archaeological potential and context for the site, should be developed and presented to all on-site contractors and personnel during the course of the development.
- A historical Archaeological Research Design (ARD) and Excavation Methodology, prepared in accordance with Heritage Council Guidelines, should be developed for the site as part of the Stage 2 DA.
- Following any archaeological works undertaken at the site (as guided by the ARD), a final excavation report should be prepared, to a suitable standard for submission to the NSW Heritage Division, and providing details such as (but not limited to), identification of all recovered relics, resources and artefacts, including nomination of long term storage solutions.

4.2.4. Heritage Interpretation

A Heritage Interpretation Plan for the subject site should be prepared as part of the Stage 2 DA, in accordance with the relevant heritage guidelines issued by the NSW Heritage Division, to address all heritage elements and values of the site, including built heritage items, historical archaeology and Aboriginal cultural heritage.



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