

Attachment A – Detailed Response to Submissions Table

A response to submissions received in response to the public exhibition of SSD-11099584 is set out in detail below. A total of five (5) submissions were received, all from government agencies. These included submissions from:

- Heritage NSW – Aboriginal Cultural Heritage;
- Biodiversity and Conservation Division;
- Transport for NSW (**TfNSW**);
- Environment Protection Authority (**EPA**); and
- Sutherland Shire Council (**SSC**).

No submissions were received from members of the public.

In addition, an Issues Letter was received from the Department of Planning, Industry and Environment (**DPIE**).

Architecture
Urban Design
Planning
Interior Architecture

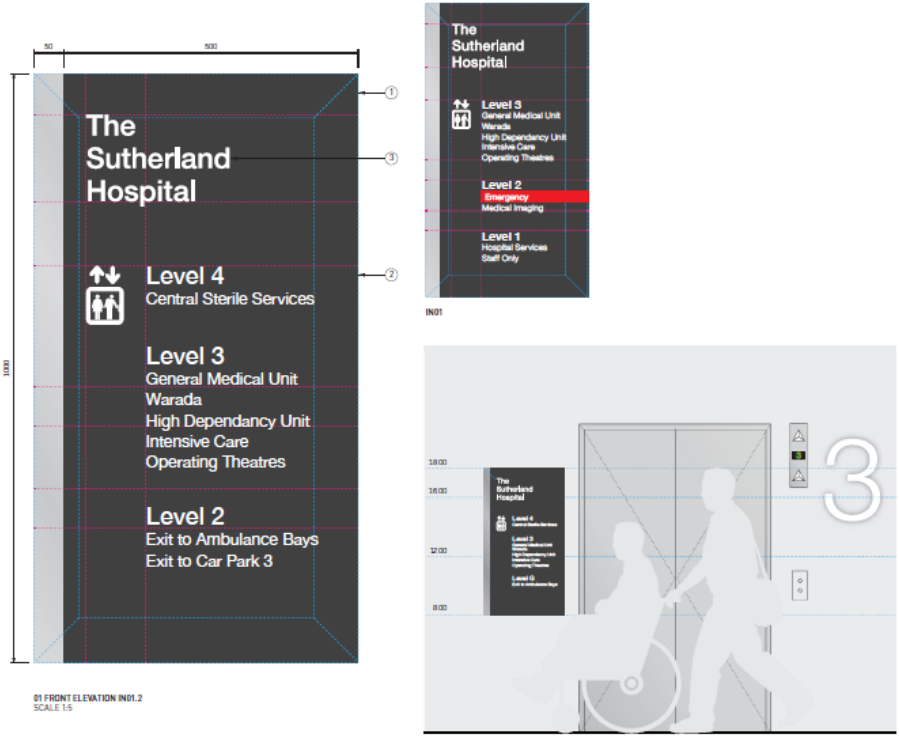
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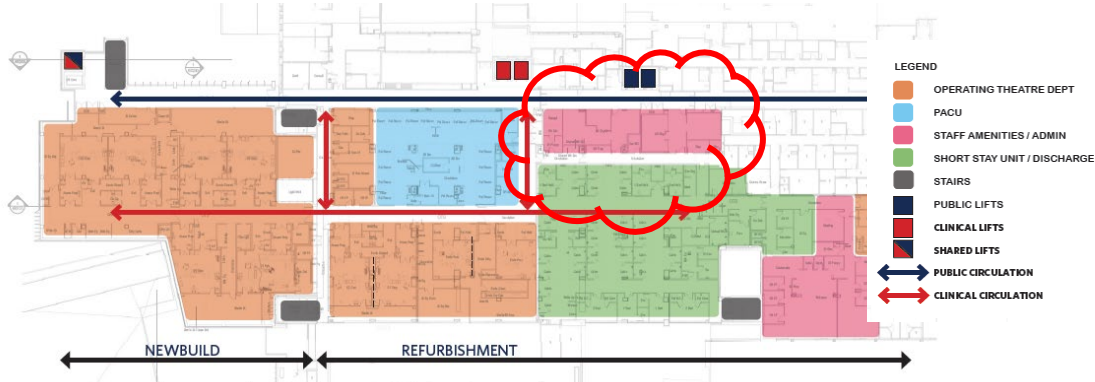
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Attachment A – The Sutherland Hospital Operating Theatre Upgrade (SSD-11099584) – Detailed Response to Submissions Table

Issue raised in agency submission	Proponent's response
Department of Planning Industry and Environment (DPIE)	
<p>1 Access</p> <p><i>While the main hospital entrance is not being altered or changed in the proposal, the proposal also includes a smaller entrance, which may cause confusion when patients are entering the site. No signage has been included in this proposal but should be incorporated to improve wayfinding across the site.</i></p>	<p>A wayfinding strategy has been developed for the proposed development by Brandculture and is provided at Attachment C and Figure 1 below.</p> <p>The wayfinding strategy provides further information on the legibility of the entrance to the building including the signage to be used at the entrance fronting the car park. As depicted below the signage on this frontage is quite minimal so as to not appear as a key entrance to the public. Note also that the signage notes that Level 1 is for staff only.</p>  <p>Figure 1 Proposed wayfinding signage at the entrance to the operating theatre building. Source: Brandculture</p>

Issue raised in agency submission	Proponent's response
<p>2 End-of-trip facilities</p> <p><i>No increase in bicycle parking or end-of-trip facilities are provided in this application. Clarification is to be provided on whether the existing facilities are sufficient for any increase in demand as a consequence of the proposed development.</i></p>	<p>The Landscape Plans submitted within the SSDA package, prepared by Arcadia note the location of the proposed bicycle parking spaces. Drawing L-203 (Rev 1) proposes 11 new bicycle rails. Refer to extract of Drawing L-203 at Figure 2 below, illustrating bicycle parking.</p>  <p>Figure 2 Landscape detail plan, with the location of bicycle parking shown in yellow. Source: Landscape Plans – Drawing L-203, Rev 1, prepared by Arcadia</p> <p>Additionally, within the upgrade new end-of-trip facilities will be included within the refurbished area of Level 3. The facilities will include 2 male and 2 female showers, as well as male and female lockers. Refer to Figure 3 and the Architectural Design Statement at Attachment B for end of trip facilities.</p>

Issue raised in agency submission	Proponent's response
	 <p>Figure 3 Staff amenities on Level 3 of The Sutherland Hospital, shown in pink within red cloud. Source: HDR Architects</p>
<p>3 Construction hours</p> <p><i>Detailed justification is to be provided for any works proposed outside of recommended construction hours. If works outside of recommended construction hours are proposed, a works plan must be included to detail how often works would occur outside the recommended times, the activities permitted and the period of time these works would continue.</i></p>	<p>It is considered that the proposed bicycle parking and end-of-trip facilities will be sufficient to service hospital staff.</p> <p>Noted. The construction hours being sought are those noted as standard construction work hours and recommended in the Interim Construction Noise Guidelines (ICNG), being:</p> <ul style="list-style-type: none"> Monday to Friday – 7am to 6pm Saturday – 8am to 1pm Sunday/public holidays – Nil <p>Section 3.2 of the attached CTMP has been updated to reflect these hours. Refer Attachment J.</p> <p>Following approval of the SSDA and the appointment of a contractor, a detailed works plan will be prepared prior to commencement of works.</p>
<p>4 Crime Prevention through Environmental Design (CPTED)</p> <p><i>It is noted that the undercroft landscaped area will be open to the public. The EIS states that CPTED principles have been applied but further detail is required on the specific measurements taken to address CPTED in this space, noting that the western façade has limited external facing windows and so passive surveillance from the building will be limited.</i></p>	<p><u>CPTED – Undercroft</u></p> <p>The undercroft area is not intended for use by the general public. The landscaped area is intended as a place where staff and escorted patients can take respite. Through extensive community consultation with staff and indigenous groups on the space, groups have already formed an effective guardianship of the space which will allow people to feel spatially 'connected' to a place and feel an association with, or responsibility for it. The space will be regularly maintained by the Local Health District (LHD).</p> <p>On the ground plane and within the undercroft area, surveillance is provided as passive surveillance from the immediate surrounds supported by the use of CCTV.</p>

Issue raised in agency submission	Proponent's response
	<p>The landscaped public domain area is open to the car park, and the proposed building includes several clearly marked, open, visible pedestrian access ways that link to destination points while movement of traffic provides more constant natural surveillance of the landscape.</p> <p>Lighting will be used in the open space area and the car park to ensure passive surveillance and safety at all hours of the day.</p> <p><u>CPTED – Western façade</u> To provide opportunities for passive surveillance large sections of glazing have been provided in transient spaces, however windows have not been provided where clinically inappropriate to do so and as a result no further windows can be provided on the western elevation than have been proposed. More specifically the uses along the western frontage of the building include the fire stairs, an MRI room, patient change room and a reporting room. The fire stairs are likely to be used during fire incidents, given patient transfer would have to occur via the lift and the lift is also predicted to be the more popular option for staff for general use. As a result, a window looking from the fire stairs would provide very limited opportunities for passive surveillance of the car park. A window is not appropriate for the MRI rooms as they required faraday cage protection (to block electromagnetic radiation) and a window in the patient area would be counterintuitive for privacy reasons. For these reasons, it is considered there are no further opportunities for more windows on this façade.</p> <p>A large panel of perforated metal screening and a glazing panel are shown on the western façade in Figure 4 below and provide views to and from the carpark from circulation corridors on the western building edge to promote surveillance of the surrounds.</p> <p>In addition, the main stair and lift core have direct views across the car park and to the north and west.</p> <p>On Level 3 natural surveillance will be provided from the main circulation corridor and touch down space. This area will be the most occupied area on the western building edge and so will provide the best opportunities for passive surveillance. From this area surveillance will be provided from within the building, over the western car park and towards Kareena Rd.</p> <p>Refer to page 86-88 of the Revised Architectural Design Statement prepared by HDR Architects (Attachment B) for a discussion of the CPTED design principles incorporated into the proposal.</p>

Issue raised in agency submission	Proponent's response
	 <p>Figure 4 Building materiality detail for the northern and western elevations Source: HDR Architects</p>
<p>5 Noise</p> <p><i>The Noise and Vibration Impact Assessment includes multiple references to St George Private Hospital. Please update the report with correct references.</i></p>	<p>Noted. This reference has been removed and an updated Noise and Vibration Impact Assessment is provided at Attachment I.</p>
<p>6 <i>The noise associated with the construction work is expected to exceed the highly noise affected level from the ICNG Guideline at the nearest affected residential receivers with no acoustic screening. It is unclear at this stage how many affected sensitive receivers will be impacted by construction noise. Please provide further clarification on the impacted receivers.</i></p>	<p>JHA has amended the Noise and Vibration report to include more detail on noise impacts to residential receivers. Refer to the revised Report at Attachment I. The report it confirms that predicted noise levels from construction to the residential receivers has been assessed and the number of noise-affected residential buildings has been approximated as 60 dwellings at the following locations:</p> <ul style="list-style-type: none"> – North of the site across Kingsway Road – Approximately 20 noise affected residential dwellings – East of the site – Approximately 25 noise affected residential dwellings – West of the site across Kareena Road– Approximately 15 noise affected residential dwellings. <p>The majority of the dwellings are attached homes however there are a couple of apartment buildings as well.</p>

Issue raised in agency submission	Proponent's response
	<p>It is noted this figure is a preliminary estimation given the affected receivers would be more accurately known once a detailed construction management plan is finalised given the number of affected residences would be based on specific construction methodology and program.</p> <p>However as recommended, mitigation measures will be put in place, such as the use of acoustic screening and sheds to ensure that the noise levels stay within an acceptable noise range, as noted in the ICNG Guidelines. This will minimize as much as is possible the number of affected sensitive receivers. The Noise and Vibration report includes a list of noise attenuation methods that could be adopted to stay within this acceptable range (refer Section 7.5.3 of Attachment I).</p> <p>As detailed in the Acoustic report, the ICNG notes that noise levels, measured at the most exposed boundary of any affected residential receiver when the construction site is in operation, must not exceed the background noise level (RBL) by more than 10 dB(A) during standard hours, and must not exceed background noise level (RBL) by more than 5 dB(A) outside of standard hours. Standard hours are Monday to Friday 7am to 6pm and Saturday 8am to 1pm.</p> <p>As noted in the Preliminary CMP (Appendix Q in the EIS), noise from the site will not exceed the limits set out in the EPA's <i>Interim Construction Noise Guidelines</i> and all relevant Australian Standards.</p> <p>A detailed <i>Construction Noise and Vibration Management Plan</i> will be prepared as a condition of consent by the Contractor. It is anticipated this will include a requirement for maintaining reasonable noise levels for sensitive receivers, to the effect of:</p> <p>Construction Noise Limits <i>The development must be constructed to achieve the construction noise management levels detailed in the Interim Construction Noise Guideline (DECC, 2009). All feasible and reasonable noise mitigation measures must be implemented and any activities that could exceed the construction noise management levels must be identified and managed in accordance with the management and mitigation measures identified in the approved Construction Noise and Vibration Management Plan.</i></p>
<p>7 Architectural and Landscape Plans</p> <p><i>Updated architectural / landscape plans are to be submitted providing:</i></p> <ul style="list-style-type: none"> a) <i>Further clarity on the entrance to the hospital, in particular clarifying the main entrance and if the new entrance is for patients or the public</i> b) <i>Provide a section view of the undercroft space including plans that illustrate the solar access to this space.</i> 	<p><u>Building entrances</u></p> <p>The main entrance to the hospital is not affected by the proposed development. The entrances into the proposed operating theatre building are intended for use by staff only. There are three points of access into the new building; two to the west end of the building near the carpark and one entrance into the MRI. All three entrances are intended for use by staff only and will have card / swipe card access. The new building is not intended for use by visitors. Figure 6 below provides a pedestrian scale perspective of the new building as viewed from the existing car park, looking towards the entrance of the new building.</p> <p>This perspective demonstrates that the entrance does not read as a main hospital entrance.</p>



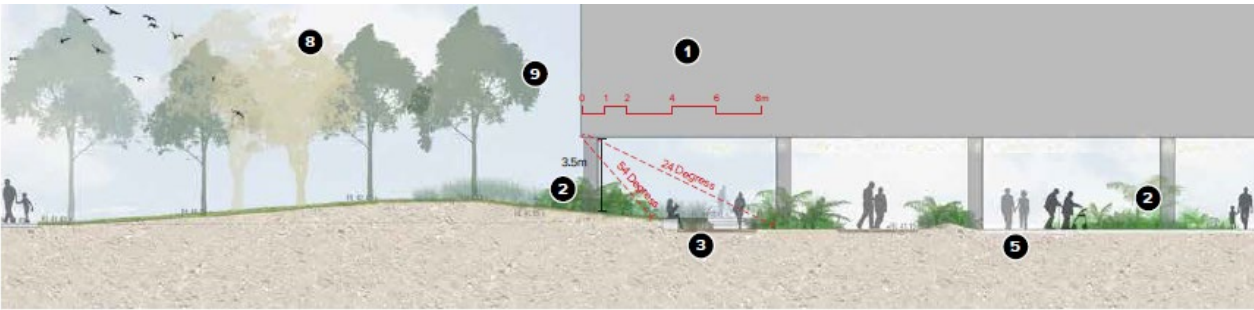
Figure 5 Excerpt from the Landscape report showing the entrances into the new building and the main hospital building, marked as arrows.

Source: Arcadia

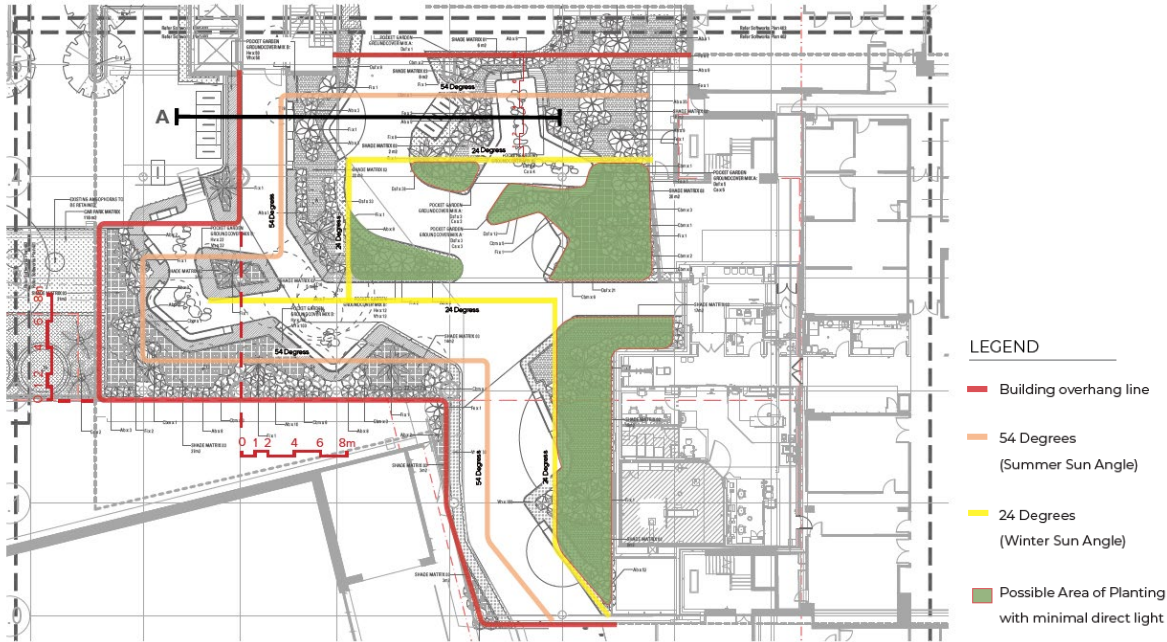


Figure 6 Perspective taken from pedestrian scale looking towards the entrance (for staff only) and the existing ambulance bays.

Source: HDR Architects

Issue raised in agency submission	Proponent's response
	<p>Undercroft – Solar access</p> <p>The undercroft area will receive sun in both summer and winter months. Sun access in summer months is modelled with an angle of 54 degrees and winter sun access is modelled with an angle of 24 degrees. This is to reflect the sun path being higher in the summer compared to winter. A greater amount of sunlight is achieved in winter when the sun is lower in the sky, however the solar access in the summer months is also of a reasonable level, as demonstrated below.</p>  <p>Figure 7 Plan excerpt showing sun access to the undercroft. Source: Arcadia</p>
Sutherland Shire Council	
<p>Following review of the plans and accompanying documentation, the following concerns are noted for your consideration in the assessment of the proposal.</p>	<p>Refer to responses to each item below.</p>
<p>8 Building entrance</p> <p>The external expression and chosen materials for the proposed addition relate comfortably with the existing buildings. The only aspect which may confuse the public is the prominence placed on the new entry along the northern elevation as it seems to be more for staff access. As the public's main entry will remain unchanged, this new entry may confuse the public.</p>	<p>For the reasons noted above, the northern elevation does not read as a main or public entrance to the building.</p>
<p>9 On-site parking</p> <p>The loss of 12 on-site parking spaces will see additional pressure placed on the parking demand on surrounding streets.</p>	<p>The loss of 12 on-site parking spaces was detailed in Section 5.3 of the Transport and Accessibility Impact Assessment, dated 18 March 2021, at Appendix M in the SSDA submission. The report notes the loss of 12 on-site parking spaces will still provide sufficient on-site capacity to accommodate future parking demands. The on-site parking capacity will be reduced but will still retain on-site demand, and avoid overflow demand impacts to the surrounding streets. The current car parking surplus as well as the predicted surplus during construction and post construction are summarised below.</p>

Issue raised in agency submission		Proponent's response																								
		Table 1 – Site parking demand and supply <table><tr><th>Stage</th><th>2020 (Existing)</th><th>2021/22 (Construction/ Transition)</th><th>2023/26 (Phase 1)</th><th>2026/31 (Phase 2)</th></tr><tr><td>Site supply</td><td>853</td><td>816</td><td>841</td><td>841</td></tr><tr><td>Site demand</td><td>718</td><td>738</td><td>772</td><td>797</td></tr><tr><td>Surplus</td><td>135</td><td>78</td><td>69</td><td>44</td></tr></table>					Stage	2020 (Existing)	2021/22 (Construction/ Transition)	2023/26 (Phase 1)	2026/31 (Phase 2)	Site supply	853	816	841	841	Site demand	718	738	772	797	Surplus	135	78	69	44
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Site demand	718	738	772	797																						
Surplus	135	78	69	44																						
10	Noise and Vibration <i>The subject site adjoins a rail corridor. Pursuant to clause 87(1)(c) of the State Environmental Planning Policy (Infrastructure) 2007, development for the purposes of a hospital must consider the likely adverse impacts of rail noise or vibration.</i>	As noted in the Noise and Vibration report (Attachment I), a noise and vibration assessment are only required under <i>Development Near Rail Corridors or Busy Roads – Interim Guideline</i> where the site is less than 100 metres from the rail line, which is not the case for the site.																								
11	<i>While a Noise and Vibration Impact Assessment Report has been prepared, on occasions incorrect references to St George Private Hospital appear within the report. Upon rectification, recommendations made within the report should form conditions of consent.</i>	The references to St Georges Hospital were in error and have been removed. The recommendations made within the Noise and Vibration Impact Assessment Report (Attachment I) will be implemented, with a condition of consent to the effect of: <i>Prior to the commencement of construction, the Applicant will incorporate noise mitigation recommendations detailed in the Noise and Vibration Impact Assessment Report prepared by JHA dated 30 June 2021, into the detailed drawings. The certifying authority will verify that all reasonable and feasible noise mitigation measures have been incorporated into the design to ensure noise measures do not exceed those recommended operational noise levels noted in the aforementioned report.</i>																								
12	Landscaping <i>The landscape proposal is sited beneath the upstairs theatre rooms and is unlikely to gain any sunlight, with northern sun blocked by the lift core. A communal landscape area would better service the community if re-located to the roof.</i>	As discussed above, a solar access study was completed to analyse the solar access to the undercroft area. The plans are included at Attachment B and an excerpt of the plans is shown in Figure 7 above. The figure outlines that there is adequate access to sunlight beneath the upstairs theatre rooms. The landscape proposal has included a variety of plant species that are robust and can survive in low light conditions. A schedule of the proposed plant species is included at Attachment F (Drawing reference: Landscape Drawing ALA-LA-DG-400). A rooftop landscaped area was not proposed for the following reasons: <ul style="list-style-type: none">- A landscape area connected to the ground was a preferred space for indigenous groups throughout the consultation process;- The landscape proposal was resolved around existing constraints such as overland flow and existing services connected to the hospital; and																								

Issue raised in agency submission	Proponent's response
<p>13 <i>Plant species below the cantilever will need to be shade and wind tolerant as this area will become a wind tunnel. Irrigation will be essential due to its enclosed nature.</i></p>	<p>- Locating a landscape area on the roof precludes any future expansion of the building on an already constrained site.</p> <p>As per the response above, there is adequate sunlight to the cantilever to support the species proposed for this area. Further, the existing NSW Ambulance station creates a wind buffer for the site from south to west which shields the undercroft area. Additionally, tree planting within the car park will provide a wind buffer from the wind from the west of the site. The landscaping of the undercroft has been chosen based on how much sunlight the various areas of the undercroft will receive. Figure 8 below is an excerpt of Attachment E and demonstrates the landscaped area receives variable levels of sunlight during summer and winter months. The landscaped area shown in green is identified as receiving minimal direct light and as a result hardy species have been chosen in this location.</p>  <p>Figure 8 – Landscaping plan demonstrating varying sun exposure. Source: Arcadia</p> <p>In summary, the landscape proposal includes a variety of plant species that are robust and can survive in low light conditions (where appropriate) and can withstand prevailing wind. Proposed plant species are referred to in Landscape Drawing ALA-LA-DG-400 and is attached at Attachment F.</p>

Issue raised in agency submission		Proponent's response
		Detailed information on irrigation measures within the proposal are provided within Chapter 5 of the Landscape Specification Report (L-SP001), attached at Attachment H . The irrigation system's quality, function, materials, components and installation are detailed within this chapter.
14	<p>10 trees including <i>Syzigium</i>, <i>Eucalyptus</i> and <i>Corymbia</i> species are to be removed. Council's tree replacement ratio of 8:1 is recommended to form a condition of consent as recommended in the addendum to this letter. Further, proposed tree species require substitution to better reflect species that are endemic to the Sutherland Shire. This is also detailed within a recommended condition of consent.</p>	<p>In response to Council's comment, Arcadia has provided a Landscape Statement which can be found at Attachment D. Refer to an extract of Arcadia's response below:</p> <p><i>With regards to the design, the trees selected are of the Sydney turpentine-ironbark forest, North Hinterland Wet Sclerophyll Forests and Coastal enriched sandstone dry forest which is found from Arcadia Background research. No professional ecological input has been provided. We would be more than happy to change the planting species to better reflect the request from Sutherland Shire Council.</i></p> <p><i>The physical landscape area provided after accommodating the existing car park and building footprint t is limited. This limited physical scope only allows for a certain number of trees to be planted - 19 Trees. The landscape design looks to provide as many trees as possible.</i></p> <p>Arcadia has provided a planting schedule (at Attachment F), which provides information on the proposed tree species. A condition of consent could be included to require an amended planting palette to be submitted prior to commencement of works.</p>
Sutherland Shire Council – Recommended Conditions of Consent		
1. Landscaping Works		
<p>A. Design</p> <p>The landscaping works must be designed in accordance with the approved Landscape Plan except where modified by the following:</p> <ol style="list-style-type: none"> Substitute trees proposed on site with the following species: <i>Eucalyptus paniculate</i> (Grey Ironbark), <i>Eucalyptus globoides</i> (White Stringybark), <i>Eucalyptus racemosa</i> (Narrow Leaved Scribbly), <i>Syncarpia glomulifera</i> (Turpentine) and <i>Backhousia myrtifolia</i> (Grey Myrtle). Amend the landscape plan in accordance with the approved architectural plans. Clearly show on plan existing trees to be removed /retained including tree numbering in accordance with the arborist report OR provide a separate existing tree plan and schedule. Tree Protection Zones (TPZ) / the location of tree protective fencing must be shown on plan for all 		<p>The landscaping works incorporate, at the request of Council, a number of species that are endemic to the Sutherland Shire Council including some of the species listed at left. Further amendments can be accommodated however it is noted as well that the final landscaping will depend on the views of aboriginal stakeholders. Refer to letter at Attachment D from Arcadia confirming there is some flexibility within the planting palette. It is recommended that a condition of consent be included that requires the finalisation of the landscape plan and planting palette as follows:</p> <p>Landscaping</p> <p><i>The applicant will develop the planting palette and landscaping plan in consultation with Sutherland Shire Council and aboriginal stakeholder groups and will submit the final plan and schedule to the Secretary prior to commencement of works.</i></p>

Issue raised in agency submission	Proponent's response
<p>existing trees and/or natural site features to be retained and protected.</p> <p>v. Provide minimum soil depths in planter boxes as follows:</p> <ul style="list-style-type: none"> - 1200mm for large trees. - 900mm for small trees and tall shrubs. - 600mm low shrubs. - 450mm grass and ground covers. <p>vi. All landscape retaining walls and planter boxes must be constructed in masonry, stone or gabions. Timber is not acceptable.</p> <p>vii. All landscaped areas and all planter boxes on slab must be provided with a waterefficient irrigation system and taps at 25m centres, connected to a pump and the rainwater tank, to enable effective landscape maintenance.</p> <p>viii. To improve coverage and reduce weeds and maintenance, planting densities in all planting areas including planter boxes must achieve a minimum of plants per square metre.</p> <p>ix. To reduce long term maintenance of planting beds, turf species must be native grass such as <i>Zoysia macrantha</i> 'Nara' or Buffalo grass varieties.</p> <p>x. As the subject site is identified as being within a Greenweb Restoration area, all new tree plantings must be indigenous species and 50% of understorey plants must be indigenous species. All indigenous species must be selected from Council's 'Native Plant Selector' available on Council's website (www.sutherlandshire.nsw.gov.au and search for Native Plant Selector).</p> <p>The applicant must engage a suitably qualified Landscape Designer or Landscape Architect to oversee any design changes to the approved Landscape Plan and amendments required above.</p> <p>Notes:</p> <p>A Landscape Designer is a person eligible for membership of the Australian Landscape Designers and Managers and a Landscape</p>	

Issue raised in agency submission	Proponent's response
<p><i>Architect is a person eligible for membership of the Australian Institute of Landscape Architects as a Registered Landscape Architect.</i></p>	
<p>B. Ongoing</p> <p><i>All landscaping works required by 'A' above must be maintained for 12 months following the final landscape inspection date. Trees required by this condition must be maintained and protected until they are covered by Council's Controls for Preservation of Trees and Bushland Vegetation (Sutherland Shire Development Control Plan 2015 Chapter 38). Any plants found faulty, damaged, diseased or dead shall be replaced with the same species in the same sized container within one month with all costs borne by the owner.</i></p> <p>Note: <i>If difficulty is experienced sourcing suitable indigenous plants from other suppliers, plants grown from locally provenance seed may be available from:</i></p> <p><i>Sutherland Shire Council Nursery 345 The Boulevard, Gympie Ph: 02 9524 5672</i></p>	
<p>2. Trees on Private Land</p>	
<p>A. Tree Removal</p> <p><i>The removal of the following trees is approved:</i></p> <ul style="list-style-type: none"> <i>i. Trees identified on the approved Landscape Plan as "existing tree to be removed"</i> <i>ii. Trees growing within the 3 metres of the building footprint of the approved structures.</i> <i>iii. Any declared noxious plant. The applicant is to ensure that all noxious plants are properly identified and controlled / removed.</i> <i>iv. Any tree species exempted by the Sutherland Shire Local Environmental Plan 2015.</i> <i>v. All other vegetation that would require approval to be removed must be protected.</i> 	<p>The applicant does not accept this condition. A replacement ratio of 8:1 is not considered reasonable nor does it reflect the approach ordinarily taken on similar projects. In accordance with Arcadia's Landscape Architectural Statement (attached at Attachment D), <i>"the physical landscape area provided after accommodating the existing carpark and building footprint is limited. This limited physical scope only allows for a certain number of trees to be planted – 19 trees. The landscape design looks to provide as many trees as possible"</i>.</p> <p>In order to meet this requirement, Health Infrastructure is willing to offer payment for the planting of 60 trees off site.</p>

Issue raised in agency submission	Proponent's response
<p>Note: For the 80 replacement trees required by "B ii)" (less the requirements of "B iii)" above), Council offers offsite planting under a 'Deed of Agreement' as an alternative to on site planting, at a cost specified in Council's Schedule of Fees and Charges. Offsite planting will be undertaken as part of Council's Green Street Program. 'Deed of Agreement' forms can be downloaded from Council's website at www.sutherlandshire.nsw.gov.au</p> <p>B. Design</p> <ul style="list-style-type: none"> i. 10 trees are approved for removal as part of this consent. Where trees are proposed to be removed, Sutherland Shire Council requires indigenous replacement canopy tree planting at a ratio of 8:1 on private land (Council Resolution EHR003-17 of 18 July 2016). ii. 80 replacement trees are required to be planted. iii. A minimum of 20 trees must be replanted across the Hospital site. iv. Trees must have a minimum container size of 5 litres. <p>An amended Landscape Plan/Tree Location Plan showing the location of all replacement trees on the site and/or in the street must be provided prior to the commencement of construction.</p> <p>C. Ongoing</p> <p>D. Trees required by this condition must be maintained and protected until they are covered by Council's Controls for Preservation of Trees and Bushland Vegetation (SSCDP 2015 Chapter 38). Any replacement trees found damaged, dying or dead must be replaced with the same species in the same container size within one month with all costs to be borne by the owner.</p>	
3. Street Tree Planting	
<p>Sutherland Shire Council Development Control Plan 2015 requires street tree planting at a rate of 1 tree per 7.5m of frontage.</p> <p>A. Design</p>	<p>The applicant accepts this requirement and will plant 4 trees on Kareena Road verge to Council's requirements prior to occupation.</p>

Issue raised in agency submission	Proponent's response
<p><i>The applicant must plant 4 street trees upon the Kareena Road verge fronting the subject property. Trees are to be Eucalyptus paniculata (Grey Ironbark) Street trees must have a minimum container size of 5L. Street tree planting must be in accordance with the "Street Tree Planting Specifications" which are located on Council's website through 'search'. These specifications include the provision of a timber border, tree cage, staked with star pickets and mulching of the trees.</i></p> <p>B. Ongoing</p> <p><i>Street trees must be maintained and protected until they are covered by Council's Controls for Preservation of Trees and Bushland Vegetation (Sutherland Shire Development Control Plan Chapter 38 part 4). Any replacement trees found damaged, dying dead must be replaced with the same species in the same container size within 1 month with all costs to be borne by the owner.</i></p>	
4. Tree Retention and Protection	
<p><i>The following condition applies to all trees on the subject site, trees on the adjoining sites (which are potentially affected by the development works), as well as trees on the adjoining Council land that are not approved for removal.</i></p> <p>A. Before Works</p> <p><i>Prior to the commencement of any demolition, excavation or construction works on site the applicant must engage a suitably qualified and experienced Supervising Consulting Arborist to oversee the measures for the protection of existing trees as listed below.</i></p> <p>Note: A Consulting Arborist is a person with a current membership of the Institute of Australian Consulting Arboriculturalists (IACA) or alternatively a person who has obtained an Australian Qualifications Framework AQF Level 5 in Arboriculture.</p>	<p>It is anticipated DPIE will impose standard conditions of consent for tree protection.</p>

Issue raised in agency submission	Proponent's response
<p><i>All trees not approved for removal must be protected by the following measures:</i></p> <ul style="list-style-type: none"> <i>i. Protective fencing constructed of 1.8m high chain wire mesh supported by robust posts must be installed in accordance with A54970- Protection of Trees on Development Sites</i> <i>ii. The tree protection zone within the protective fencing must be mulched with a maximum depth 75mm of suitable organic mulch (woodchips or composted leaf chip mulch) and kept regularly watered for the duration of the works subject to this consent.</i> <i>iii. No development or associated activity is permitted within the fenced tree protection zone for the duration of works subject to this consent. This includes vehicular or pedestrian access, sheds, washout areas, excavations, backfilling, installation of services (including stormwater), removal of top soil, stockpiling of soil or building materials.</i> <i>iv. Where site access/egress is required over the roots of trees identified for retention and protection, provide hardwood rumble boards over a 200mm thick layer of wood chip.</i> <i>v. Where it is impossible to install protection fencing to the full extent of the specified Tree Protection Zone- install trunk and branch boarding protection as shown in Figure 4 of the Australian Standards AS4970- Protection of Trees on Development Sites (Page 17).</i> <p><i>B. During Works</i></p> <ul style="list-style-type: none"> <i>i. The tree protection measures detailed in 'A' above must be maintained during construction.</i> <i>ii. The supervising Consulting Arborist must be present during any approved hand excavation or under boring works within the Tree Protection Zone (TPZ) of any tree identified for retention and protection and have the authority to direct works to ensure the trees long term preservation.</i> <i>iii. The supervising Consulting Arborist must strictly supervise that there is no disturbance or severing of roots greater than 50mm diameter and to cleanly cut those roots between 10-50mm in diameter.</i> 	

Issue raised in agency submission	Proponent's response
<p>iv. If the tree/s identified for retention in 'A' above are damaged or destabilised during construction then works must cease and Council's Tree Assessment Officer (ph. 9710 0333) must be contacted to assess the tree/s and recommend action to be taken.</p> <p>v. Ensure each hold point outlined below within the Tree Protection Schedule is signed off and dated progressively by the Consulting Arborist throughout the various development stages, including preconstruction, construction and post construction. Photographic evidence must also be provided (Refer to table in Sutherland Shire Council's submission).</p>	
5. Site Management Plan	
<p>A. Before Commencement of Works including demolition</p> <p>An Environmental Site Management Plan must be prepared. These plans must satisfy the Objectives and Controls of Sutherland Shire Development Control Plan 2015 relating to environmental site management and must incorporate the following throughout demolition and construction:</p> <ul style="list-style-type: none"> i. Safe access to and from the site during construction and demolition. ii. Safety and security of the site, road and footpath area including details of iii. proposed fencing, hoarding and lighting. iv. Method of loading and unloading excavation machines, building materials. v. How and where, construction materials, excavated and waste materials will be vi. stored. vii. Methods to prevent material being tracked off the site onto surrounding roadways. viii. Erosion and sediment control measures. ix. All trees and their protection zones on and around the site identified for retention x. are to be protected according to Australian Standard AS 4970 - 2009 Protection of 	<p>These matters will be covered by the final CEMP required for the project as a condition of consent.</p> <p>Suggested condition wording is as follows:</p> <p>Construction Environmental Management Plan</p> <p>Prior to the commencement of construction, the Applicant must submit a Construction Environmental Management Plan (CEMP) to the Certifier and provide a copy to the Planning Secretary. The CEMP must include, but not be limited to, the following:</p> <ul style="list-style-type: none"> a) Details of: <ul style="list-style-type: none"> i. hours of work; ii. 24-hour contact details of site manager; iii. management of dust and odour to protect the amenity of the neighbourhood; iv. stormwater control and discharge; v. measures to ensure that sediment and other materials are not tracked onto the roadway by vehicles leaving the site; vi. groundwater management plan including measures to prevent groundwater contamination; vii. external lighting in compliance with AS 4282-2019 Control of the obtrusive effects of outdoor lighting; viii. community consultation and complaints handling; b) Construction Traffic and Pedestrian Management Sub-Plan (see condition B12); c) Construction Noise and Vibration Management Sub-Plan (see condition B13); d) Construction Waste Management Sub-Plan (see condition B14); e) an unexpected finds protocol for Aboriginal and non-Aboriginal heritage and associated communications procedure; waste classification (for materials to be removed) and validation (for materials to remain) be undertaken to confirm the contamination status in these areas of the site

Issue raised in agency submission	Proponent's response
<p><i>Trees on Development Sites using the methods outlined in that Standard.</i></p>	
<p><i>A. During Works</i></p> <p>The site management measures set out in the above plan must remain in place and be maintained throughout the period of works and until the site has been stabilised and landscaped.</p> <p><i>B. Before Occupation</i></p> <p>Before occupation, all foundations / materials associated with construction works (that do not form part of the approved works) must be removed. This includes but is not limited to foundations for tower cranes, vehicle access ways, stockpiles, building waste etc.</p>	
<p>6. Internal Driveway, Parking and Manoeuvring</p>	
<p><i>A. Design</i></p> <p><i>The internal driveway profile, parking and manoeuvring areas must be designed in accordance with the approved architectural plans except where modified by the following:</i></p> <ul style="list-style-type: none"> <i>i. The ingress and egress crossing must be clearly identified by signage.</i> <i>ii. The car park must be line marked to accommodate 25 vehicles.</i> <i>iii. The internal driveway and car parking area must be concrete or asphalt.</i> <i>iv. Provide adequate sight distance for the safety of pedestrians using the footpath area.</i> <i>v. Comply with AS2890.1 (2004) user class 3, in relation to the design of vehicular access, parking and general manoeuvring for the B85 vehicle.</i> <i>vi. The maximum longitudinal grade of the driveway must not exceed 12.5%.</i> <p><i>B. Occupation</i></p>	<p>The development will be designed in accordance with the architectural drawings, including the arrangement for the internal driveway profile, parking and manoeuvring areas. It is anticipated a condition of consent will be imposed requiring compliance with relevant parts of Australian Standard AS2890.</p>

Issue raised in agency submission	Proponent's response
<p><i>Prior to the occupation of the development, a suitably qualified engineer must certify that the works required in A. above were undertaken and completed to their satisfaction and in accordance with the requirements of this Development Consent.</i></p> <p>C. Ongoing</p> <p><i>The approved parking must be used exclusively for car parking and approved for the life of the development.</i></p>	
7. Stormwater Drainage	
<p>A. Design</p> <p><i>The stormwater drainage system must be designed in accordance with the approved stormwater drainage design drawing; Australian Standard A53500.3:2015; Sutherland Shire Environmental Specification - Stormwater Management.</i></p> <p>B. Before Occupation</p> <p><i>Prior to occupation:</i></p> <ul style="list-style-type: none"> i. <i>A Works-As-Executed drawing (WAED) of the stormwater drainage system must be prepared by a Registered Surveyor. This drawing must detail the alignment of pipelines, pits, the rainwater tanks and the detention facilities. An original or a colour copy must be submitted to Sutherland Shire Council.</i> ii. <i>The supervising engineer must certify the WAED of the stormwater drainage system that the stormwater drainage works, rainwater harvesting facility and rainwater reuse systems were constructed to their satisfaction and in accordance with the Development Consent, and Public Domain Technical Manual. Prior to the</i> 	<p>This condition is accepted.</p>

Issue raised in agency submission	Proponent's response
<p><i>occupation or use of the building the Applicant / Owner must submit to Council a copy of the aforementioned letter of certification.</i></p> <p>C. Ongoing</p> <p>i. <i>The operation of all devices or appliances installed within the development approved by this consent as required by conditions pertinent to rainwater harvesting and rainwater reuse must be maintained in good operating order at all times.</i></p> <p>ii. <i>The stormwater detention facility must be:</i></p> <ul style="list-style-type: none"> - <i>Kept clean and free from silt, rubbish and debris.</i> - <i>Be maintained so that it functions in a safe and efficient manner.</i> - <i>Not be altered without prior consent in writing of the Council.</i> <p>Note: Upon submission of the Works-As-Executed drawing for the stormwater drainage system a notation will be added to the Section 10.7 certificate advising future owners that their property is burdened by a stormwater detention facility.</p>	
8. Sydney Water – Notice of Requirements	
<p>A. Before Any Works</p> <p><i>Prior to the commencement of any works on site, including demolition or excavation, the plans must be approved by Sydney Water to determine if sewer, water or stormwater mains or easements will be affected by any part of your development. Please refer to the web site www.sydneywater.com</i></p>	<p>This condition is accepted.</p>
9. Noise Control and Permitted Hours for Building and Demolition Work	
<p>A. General</p> <p><i>To manage noise impacts upon the surrounding properties and occupants, demolition,</i></p>	<p>As mentioned previously, the construction activities will remain within reasonable levels as per the ICNG. This condition accepted.</p>

Issue raised in agency submission	Proponent's response
<p><i>excavation, or construction activities must be managed in accordance with the NSW Department of Environment and Climate Change (now Environment Protection Authority). Interim Construction Noise Guideline (ICNG) 2009 and Australian Standard 2436 -2010 Guide to Noise Control on Construction, Maintenance and Demolition Sites.</i></p> <p>B. Before Excavation</p> <p><i>Prior to any excavation works involving rock breakers and similar earthmoving equipment, the builder must notify in writing all property owners/tenants within a minimum of 20m of all boundaries of the development site of the works being undertaken, a minimum of 7 days prior to the commencement of such works. The notification must provide details of the type of work being carried out, the time of day, its anticipated duration and a contact number to log any complaints or to make enquiries.</i></p> <p>C. During Works</p> <p><i>To minimise the noise impact on the surrounding environment, all building and demolition work must be carried out only between the hours of 7.00am and 6.00pm Monday to Friday inclusive, 8.00am and 3.00pm Saturdays. No work is permitted on Sundays and Public Holidays.</i></p>	
10. Toilet Facilities	
<p>A. During Works</p> <p><i>Toilet facilities must be available or provided at the work site at a ratio of one toilet plus</i></p>	<p>This condition is accepted.</p>

Issue raised in agency submission	Proponent's response
<p>one additional toilet for every 20 persons employed at the site before works begin and must be maintained until the works are completed. Each toilet must:</p> <ul style="list-style-type: none"> i. be a standard flushing toilet connected to a public sewer, or ii. have an on-site effluent disposal system approved under the Local Government iii. Act 1993, or be a temporary chemical closet approved under the Local Government Act 1993. 	
NSW Environmental Planning Authority	
<p>15 In view of these factors, the EPA has no comments to provide on this project and no follow-up consultation is required, Sutherland Shire Council should be consulted as the appropriate regulatory authority for the Protection of the Environment Operations Act 1997 in relation to the proposal.</p>	<p>Noted.</p>
Heritage NSW – Aboriginal Cultural Heritage	
<p>16 Aboriginal Cultural Heritage Assessment Report (ACHAR)</p> <p>The Aboriginal Cultural Heritage Assessment Report (ACHAR) identifies that the proposed development will have limited subsurface impact and is located within an area previously mapped by the Sutherland Shire as of low archaeological potential. Additionally, due to the highly disturbed nature of the study area, confirmed by a site inspection and background research, intact archaeological deposits are unlikely to be present. The report concludes that as a result the proposal is unlikely to impact any Aboriginal heritage items.</p>	<p>Noted.</p>
<p>17 During consultation with the Registered Aboriginal Parties no specific cultural values directly related to the proposal area were identified. Monitoring during groundworks was requested by Kadibulla Khan, however due to the limited impacts and disturbed nature of the area this was not supported by Artefact Heritage (2021).</p>	<p>Noted.</p>

Issue raised in agency submission	Proponent's response
<p>18 <i>The ACHAR recommends that no further work is required and that an unexpected finds protocol should be implemented.</i></p> <p><i>Heritage NSW supports these recommendations and notes that the requirement for an Unexpected Finds Protocol is reflected in Table 19, Section 8 of the EIS (page 93) and must be in place prior to and throughout the proposed works.</i></p>	<p>It is anticipated an Unexpected Finds Protocol will be included as a condition of consent and the protocol will form part of the CEMP package. Refer to a draft condition of consent below.</p> <p><i>In the event that surface disturbance identified a new Aboriginal object, all works must halt in the immediate area to prevent any further impacts to the object(s). A suitably qualified archaeologist and the registered Aboriginal representatives must be contacted to determine the significance of the object(s). The site must be registered in the Aboriginal Heritage Information Management System (AHIMS) which is managed by Heritage NSW and the management outcome for the site included in the information provided to AHIMS. The Applicant must consult with the Aboriginal community representatives, the archaeologists and Heritage NSW to develop and implement management strategies for all objects/sites. Works may only recommence with the written approval of Heritage NSW.</i></p>
Biodiversity and Conservation Division	
<p>20 <i>EES has reviewed the relevant documentation and make the following comments.</i></p>	<p>Refer to responses below.</p>
<p>21 Biodiversity</p> <p><i>A Biodiversity Development Assessment Report (BDAR) Waiver Request was approved 3 December 2020.</i></p>	<p>Noted.</p>
<p>22 Flooding</p> <p><i>No flooding comments.</i></p>	<p>Noted.</p>
Transport for NSW (TfNSW)	
<p>23 <i>TfNSW has reviewed the EIS and provides the following advisory comments for the Department's consideration below.</i></p>	<p>Refer responses provided below.</p>
<p>24 Construction Traffic Management Plan (CTMP)</p> <p><i>Comment</i> <i>The CTMP does not indicate the largest/longest vehicle size will be needed and the frequency of this movement. Under Part 5.2 Public Infrastructure, the plan states that large vehicles may need to mount or cross public kerbs and median devices if necessary, to which the builder will account for any damages to any public infrastructure.</i></p> <p><i>Recommendation</i> <i>Details of the largest/longest vehicles accessing the site are required to be submitted as part of the Response to Submissions, as well as a swept path analysis for these vehicles accessing the site.</i></p>	<p>The largest typical vehicle expected is a 12.5 metre Heavy Rigid Vehicle however larger vehicles may be required occasionally. These will be defined and analysed with the appointed Contractor in the final Construction Traffic Management Plan as a condition of consent.</p> <p>Under a separate works package, the existing median strip on Kareena Road was recently removed and a new right-turn lane into the site has been constructed. This will assist larger vehicles accessing the site and will avoid the need for larger vehicles to mount or cross public kerbs or median devices.</p> <p>TTW has provided an updated Construction Traffic Management Plan at Attachment J. The existing median strip on Kareena Road was removed as part of the Review of Environmental Factors (REF) upgrade works and will include a new right-turn lane into the site. This will assist larger vehicles accessing the site and will not require mounting or crossing public kerbs or median devices.</p>

Issue raised in agency submission		Proponent's response																				
		A Swept Path Analysis for a 12.5m Heavy Rigid Vehicle has been appended to the updated Construction Traffic Management Plan, prepared by TTW (Attachment J). The swept path analysis demonstrates there is adequate clearance for a vehicle of this length.																				
25	<p>Green Travel Plan</p> <p><u>Comment</u> The Travel Demand Management Implementation team has reviewed the Green Travel Plan (GTP) and has a number of recommendations to improve the initial GTP and the proposed initiatives to encourage sustainable transport to the site.</p> <p><u>Recommendation</u> During the Response to Submissions stage, the Applicant shall provide an updated Green Travel Plan for TNSW's consideration that:</p> <ul style="list-style-type: none">• Incorporates data from available sources including:<ul style="list-style-type: none">○ Surveys of staff and visitors to find out barriers and facilitators to modal shift○ Quantification and analysis of staff shift times and numbers on the site and analysis of workforce○ residential postcode data to develop effective strategies in response, as well as help to inform○ service planning considerations○ Relevant transport strategies and proposed network changes including planned cycleway along Kingsway– Includes aspirational, achievable and specific mode targets, identified by avoiding the use of ranges, by clarifying the data and by increasing the targets for active and public transport.– Provides details and maps of end of trip facilities, access points and site permeability for active travel, including number and location of all secure bike parking, casual bike parking, showers and lockers.– Identifies lighting or other issues around hospital access points and routes from nearby public transport stops and other points of interest.	<p>The Green Travel Plan (GTP) has been updated to include:</p> <ul style="list-style-type: none">– Additional information on pedestrian links and routes to public transport– Additional information on existing bicycle and end-of-trip facilities– External transport strategies such as the Sutherland to Cronulla Active Transport Link– New actions and checklist items such as a public transport information screen, promotion of health benefits by healthcare workers, and nominating a sustainable transport champion. <p>Refer to the updated GTP at Attachment K.</p> <p>A detailed GTP is expected to be required as a condition of consent prior to occupation. This would be expected to include more detailed information such as staff travel surveys, and additional consultation, prior to preparing the operational document. The document at Attachment K is a preliminary document only.</p> <p>The revised Green Travel Plan includes staff mode share targets, summarised in the table below. The mode shift is intended to reduce for private vehicles by 3-6 %, and this mode share will be split across carpooling, public transport and active transport which are expected to increase 2%, 2% and between 1-2% respectively. A 2% increase in cycling to work would translate to 8 people based on a projected 400 staff members. 11 bicycle racks are proposed which is considered to be adequate to accommodate the mode shift, noting each bicycle rack can be used by 1-2 bicycles. The location of the bicycle racks is shown in Figure 2 in yellow.</p> <p>Table 2 – Mode share current and targets</p> <table><tr><th></th><th>Existing mode share</th><th>Target mode share</th><th>Change</th></tr><tr><td>Private vehicle (single person)</td><td>81%</td><td>75–78%</td><td>- 3–6%</td></tr><tr><td>Private vehicle (carpool / drop-off)</td><td>2–3%</td><td>4–6%</td><td>2%</td></tr><tr><td>Public transport</td><td>1-2%</td><td>3–4%</td><td>2%</td></tr><tr><td>Active transport</td><td>4–8%</td><td>5–10%</td><td>+ 1–2%</td></tr></table> <p>Three items suggested by Transport for NSW are not intended to be included as part of the GTP and it is requested that they not be included in the condition relating to the GTP.</p> <p>These items and the reasons for them being excluded are noted below.</p> <p><u>Item 1:</u> A strategy for staff (through flexible working and shift times) and visitors (through visitor's hours) to retime their travel to avoid peak hours where possible.</p>		Existing mode share	Target mode share	Change	Private vehicle (single person)	81%	75–78%	- 3–6%	Private vehicle (carpool / drop-off)	2–3%	4–6%	2%	Public transport	1-2%	3–4%	2%	Active transport	4–8%	5–10%	+ 1–2%
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Active transport	4–8%	5–10%	+ 1–2%																			

Issue raised in agency submission	Proponent's response
<ul style="list-style-type: none"> – Considers whether further bike parking is required. – Addresses potential to reduce car parking spaces for staff and therefore reserve the majority of parking for patients and visitors. – Includes a completed travel access guide and communications strategy. – Considers innovative ways to incorporate public transport and active transport into the fabric of life at the hospital such as: <ul style="list-style-type: none"> ○ Promotion of the benefits of cycling, walking and using active transport for people's health, through information provided by Doctors, nurses and allied health staff; ○ A screen with public transport information near reception area for staff and visitors to see when the next buses and trains are arriving; ○ A strategy for staff (through flexible working and shift times) and visitors (through visitor's hours) to retime their travel to avoid peak hours where possible. ○ Special transport provision for night shift workers (such as shuttle to local train station); ○ Sustainable transport champions to advocate and model desired behaviour and promote through newsletter or other internal channels; and ○ Alternatives provided for staff who use public or active transport in case of an emergency. <p>Further guidance for a Travel Plan Toolkit for Hospitals is available here: https://www.mysydney.nsw.gov.au/travelchoices/tdm</p> <p>The Applicant should prepare an updated Green Travel Plan in consultation with TfNSW to discuss the proposal via email development.sco@transport.nsw.gov.au</p>	<p>Response: There are three primary shift times meaning that is already a natural staggering of start and finish times amongst hospital staff. There is a morning start at 7am, an afternoon starts at 1pm and evening shifts commencing between 7-11pm. Non-clinical staff start between 7am and 9:30am. Visiting hours are from 10am to 8pm. It is necessary to have clinical staff on-site 24 hours per day and so naturally some staff will have to start within peak hours and some will not, to ensure a constant rotation of staff. It is considered that there is already a good amount of shift timings that are outside of peak times, and changes to hospital staff rostering is not considered within the scope of this project and will have broader repercussions for the ongoing operations of the hospital. As a result, it is requested this item not be required as part of the GTP.</p> <p>Item 2: Special transport provision for night shift workers (such as shuttle to local train station) Response: NSW Health as a state-wide entity does not currently provide this service to staff. The Sutherland Hospital is within a short walking distance of heavy rail and on most major bus transport routes and therefore a shuttle bus does not seem necessary in the circumstances.</p> <p>Item 3: Alternatives provided for staff who use public or active transport in case of an emergency. Response: NSW Health as a state-wide entity does not currently provide this service to staff and does not intend to include this as part of the project.</p> <p>The suggested wording for the condition of consent is:</p> <p>Green Travel Plan Prior to the commencement of operation, a Green Travel Plan (GTP) must be submitted to the satisfaction of TfNSW to promote the use of active and sustainable transport modes. The plan must:</p> <ul style="list-style-type: none"> – Be prepared by a suitably qualified traffic consultant in consultation with Council and TfNSW – incorporate data from available sources including: <ul style="list-style-type: none"> ○ Surveys of staff and visitors to find out barriers and facilitators to modal shift ○ Quantification and analysis of staff shift times and numbers on the site and analysis of workforce ○ residential postcode data to develop effective strategies in response, as well as help to inform ○ service planning considerations ○ Relevant transport strategies and proposed network changes including planned cycleway along Kingsway ○ Includes aspirational, achievable and specific mode targets, identified by avoiding the use of ranges, by clarifying the data and by increasing the targets for active and public transport. – Provide details and maps of end of trip facilities, access points and site permeability for active travel, including number and location of all secure bike parking, casual bike parking, showers and lockers. – Identify lighting or other issues around hospital access points and routes from nearby public transport stops and other points of interest. – Consider whether further bike parking is required.

Issue raised in agency submission		Proponent's response
		<ul style="list-style-type: none"> – Address potential to reduce car parking spaces for staff and therefore reserve the majority of parking for patients and visitors. – Include a completed travel access guide and communications strategy. – Consider innovative ways to incorporate public transport and active transport into the fabric of life at the hospital for example: <ul style="list-style-type: none"> ○ Promotion of the benefits of cycling, walking and using active transport for people's health, through information provided by Doctors, nurses and allied health staff; ○ A screen with public transport information near reception area for staff and visitors to see when the next buses and trains are arriving; and ○ Sustainable transport champions to advocate and model desired behaviour and promote through newsletter or other internal channels.