

Sydney Football Stadium

**SSD 9835 Sydney Football Stadium Redevelopment,
Section 4.55 Modification -**

Precinct Village and Car Park (MOD 7):
Design Integrity Assessment Report
(Revision 9, September 2021)

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HOW TO READ THIS DOCUMENT:

This Design Integrity Assessment Report ('the Report') was first prepared to support SSD 9835 to demonstrate how the proposed design for the Sydney Football Stadium Stage 2 State Significant Development Application was equivalent to the winning competition scheme.

The Report, as endorsed by the Design Integrity Assessment Panel at the time, focussed on the stadium and identified the MP Car Park as an at grade facility capable of accommodating 540 spaces. Since then, the Report has been modified numerous times to reflect design changes that have occurred as part of modifications to SSD 9835 and to reflect the design development process.

The Report is now being updated as Version 8 to consider the Precinct Village and Car Park proposal. The structure and content of the Report (as the approved Version 7) has been retained for context. All content related to the Precinct Village and Car Park project is contained within the addendum which commences at page 27.

1. Introduction

This Design Integrity Assessment details how the proposed design for the Sydney Football Stadium Stage 2 State Significant Development Application is equivalent to the winning competition scheme. The Design Integrity Assessment Panel (Panel) has considered the design modifications outlined in this assessment and confirmed that the design proposes design excellence qualities consistent with the winning competition scheme.

This Design Integrity Assessment was exhibited publicly as part of the State Significance Development (SSD) Development Application (DA) for the Stage 2 Sydney Football Stadium Redevelopment (SSD 9835). The Assessment was amended to reflect the design changes to respond to feedback received and to reflect the design development process. It documents the competition winning scheme, the exhibited proposal and outlines the design modifications. This Design Integrity Assessment should be read in conjunction with the Response to Submissions prepared by Ethos Urban dated August 2019, as well as modifications to SSD 9835.

This Design Integrity Assessment has been updated to consider the Precinct Village and Car Park proposal via an addendum as the Design Integrity Assessment report as initially submitted and approved did not consider the MP1 Car Park site on which the Precinct Village and Car Park is now proposed. The addendum is provided from page 27. The earlier parts of this Design Integrity Assessment remain unchanged and have not been amended for the purposes of the Precinct Village and Car Park proposal.

The Precinct Village and Car Park proposal was presented to the Design Integrity Panel on 13 August 2021 and provided feedback. The design team is continuing to engage with the Design Integrity Panel to address feedback. Accordingly, this Design Integrity Assessment Report will be refined during the public exhibition period and finalised before determination to ensure the Precinct Village and Car Park proposal contributes towards the Precinct’s overall achievement of design excellence.

Table 1: Condition of Consent B4 - SSD9249

Condition Requirement	Where addressed
B8. Prior to the lodgement of the future development application, a Design Integrity Assessment Panel (DIAP) must be established. The members of the DIAP must comprise of the members of the assessment panel responsible for selecting the winning design through the Competitive Design Process.	See section 2.
B9. Any reports endorsed by the DIAP, prior to the lodgement of a future development application, must be presented to the CCC and be referenced in any minutes.	See Appendix A.
C2. The future development application must be supported by a Design Integrity Assessment (DIA) to inform whether the proposal is equivalent to or is an improvement upon the design excellence qualities of the winning competition scheme. The DIA must be prepared, and the DIA report, signed by the members of DIAP required by Schedule 2, condition B8.	See section 3 in relation to design amendments. See section 4 in relation to assessment by the Panel. See section 5 for Panel endorsement.
C3. Details of the presentation of the DIA report to CCC, as required by Schedule 2 condition B9, and feedback as a result of the presentation, must be submitted as part of the future development application.	See Appendix A and B.

Table 2: SEARs Requirements

Condition Requirement	Where addressed
4. Design Excellence Provide details of the Design Integrity Assessment Panel as required by condition B8 of Schedule 2 of the development consent for SSD 9249.	See section 3 in relation to design amendments. See section 4 in relation to assessment by the Panel. See section 5 for Panel endorsement.
In accordance with Schedule 2, Condition C2 and C3 of SSD 9249, provide a Design Integrity Assessment Report.	This Report.

2. Design Integrity Assessment Panel

Design Integrity Assessment Panel was established comprising membership of:

- Peter Poulet - NSW Government Architect representative (Chair)
- Kim Crestani - Independent
- John Perry - Independent
- Tom Gellibrand - Infrastructure NSW (Proponent Representative).



3. Design Amendments

3.1. Landscape Design

Enhancements have been made to the landscape design as a result of design development. Since the approval of SSD 9835, Infrastructure NSW and the appointed contractor have worked with the design team to document the approved scheme. Figure 1 provides the landscape concept from the winning competition entry. Figure 2 provides the landscape proposal for the Stage 2 development application. Table 3 provides the details of the changes from the competition winning scheme to the current proposal, as reflected in the Response to Submissions, that were submitted during the assessment of SSD 9835.



Figure 1: Competition winning landscape concept

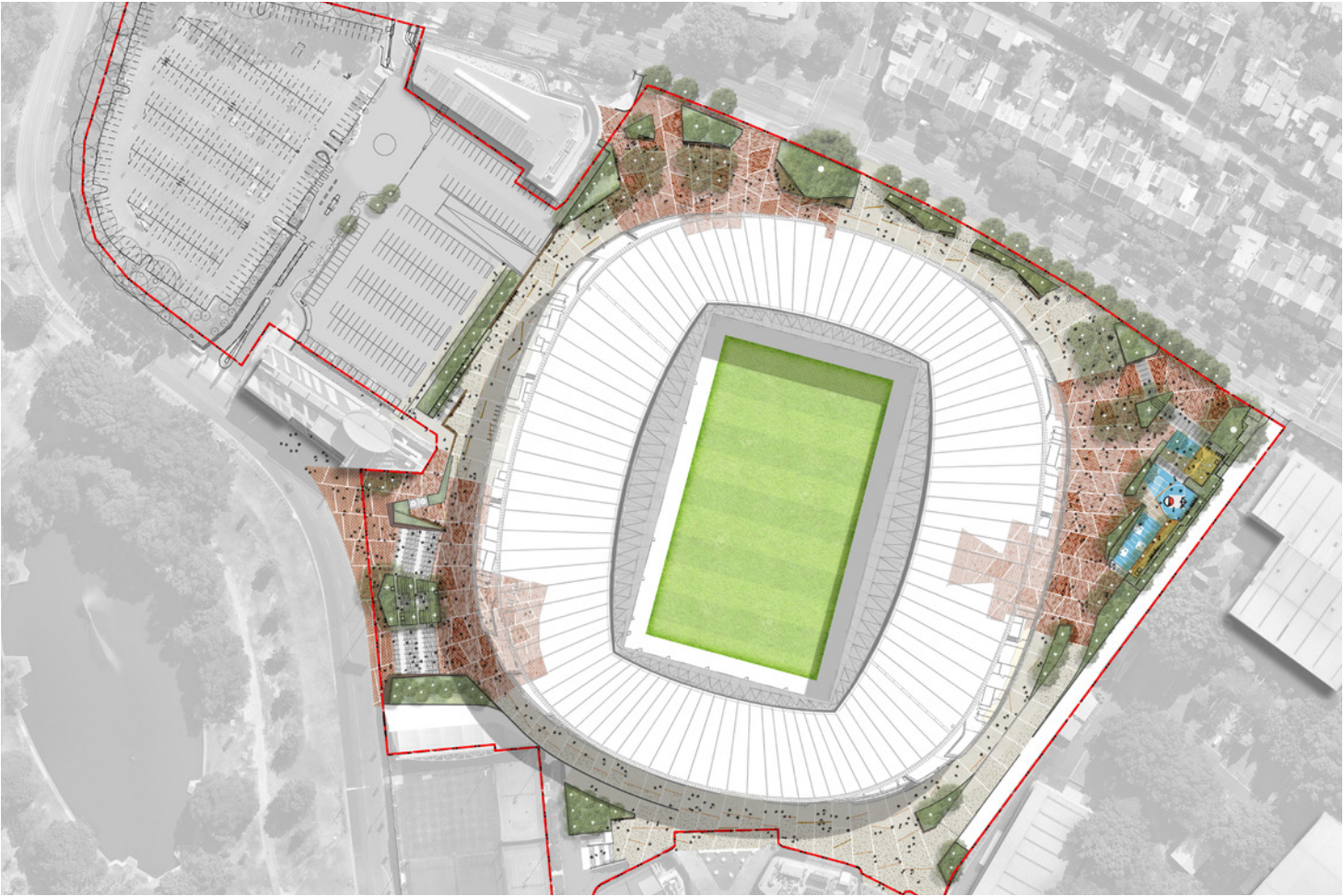
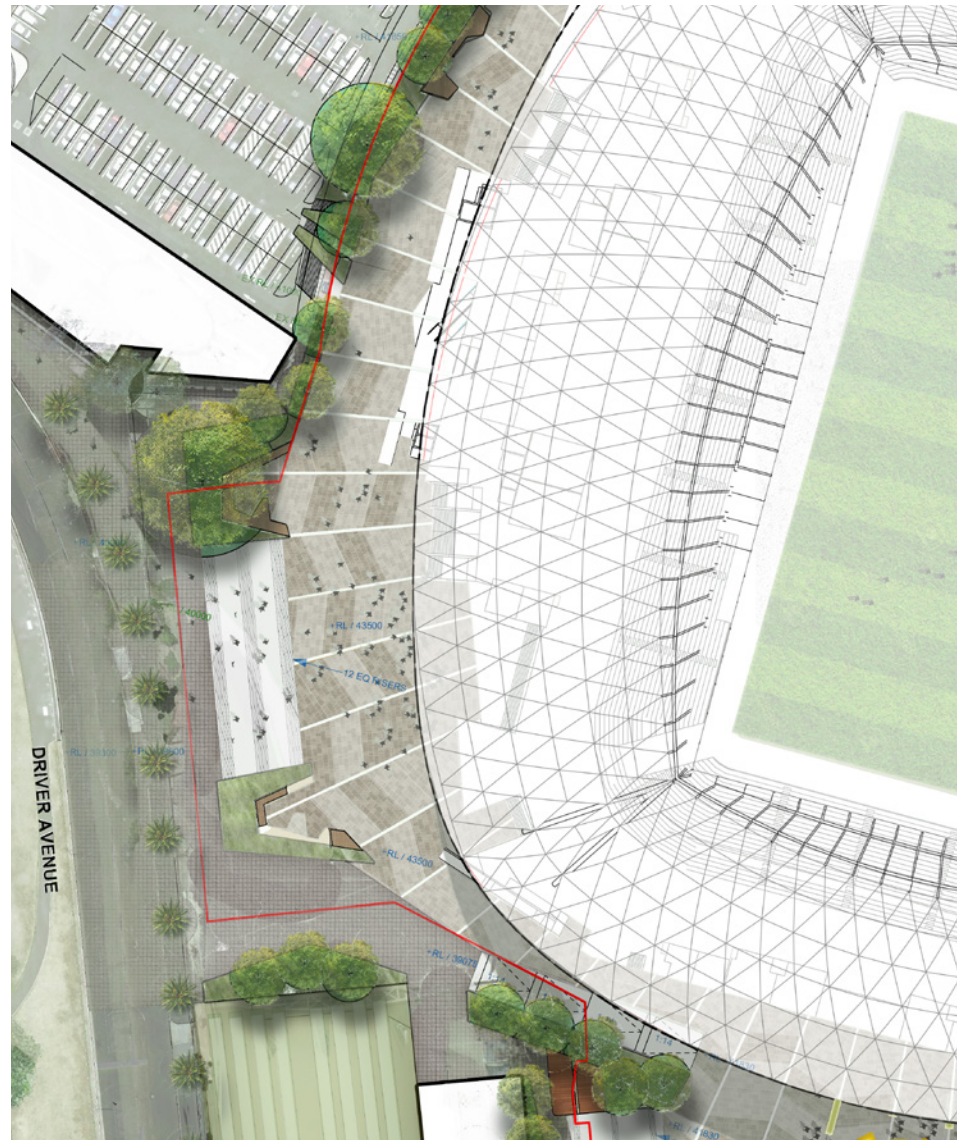
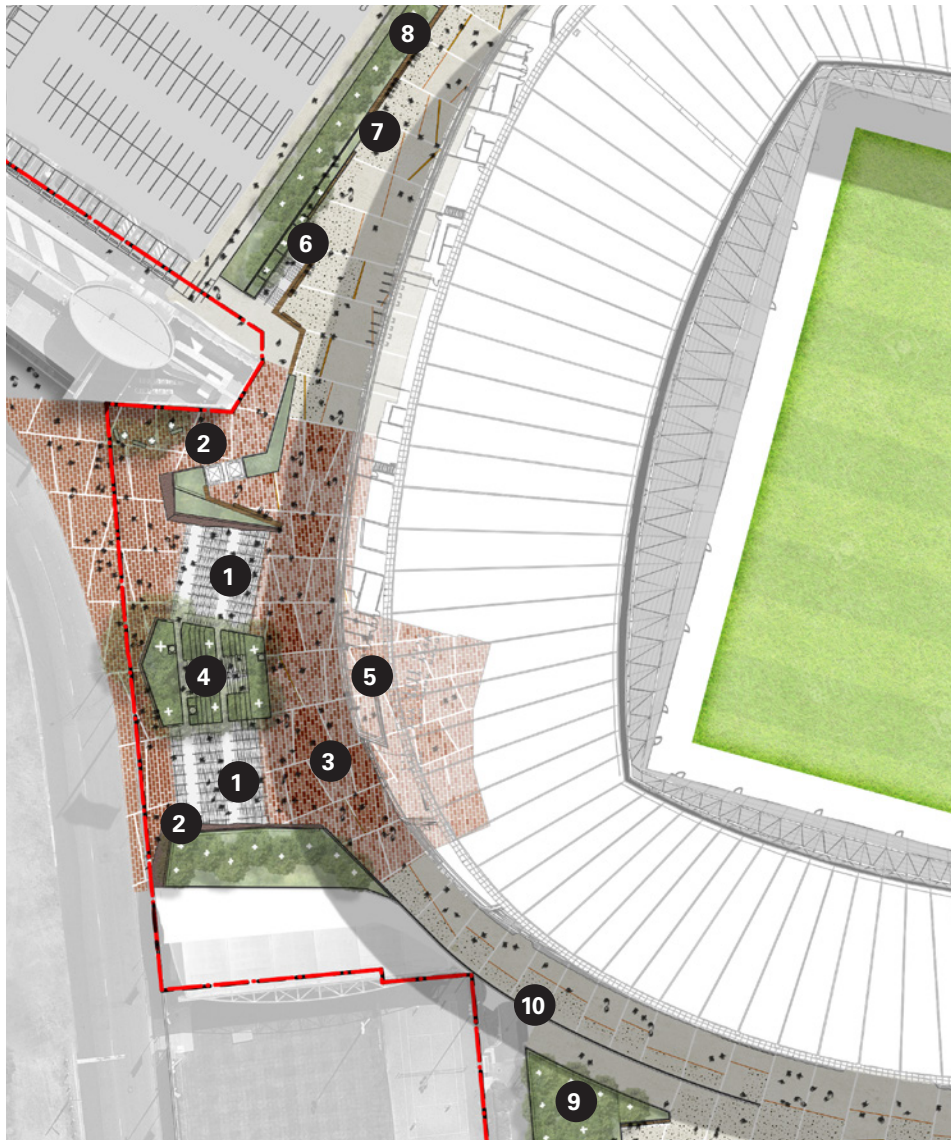



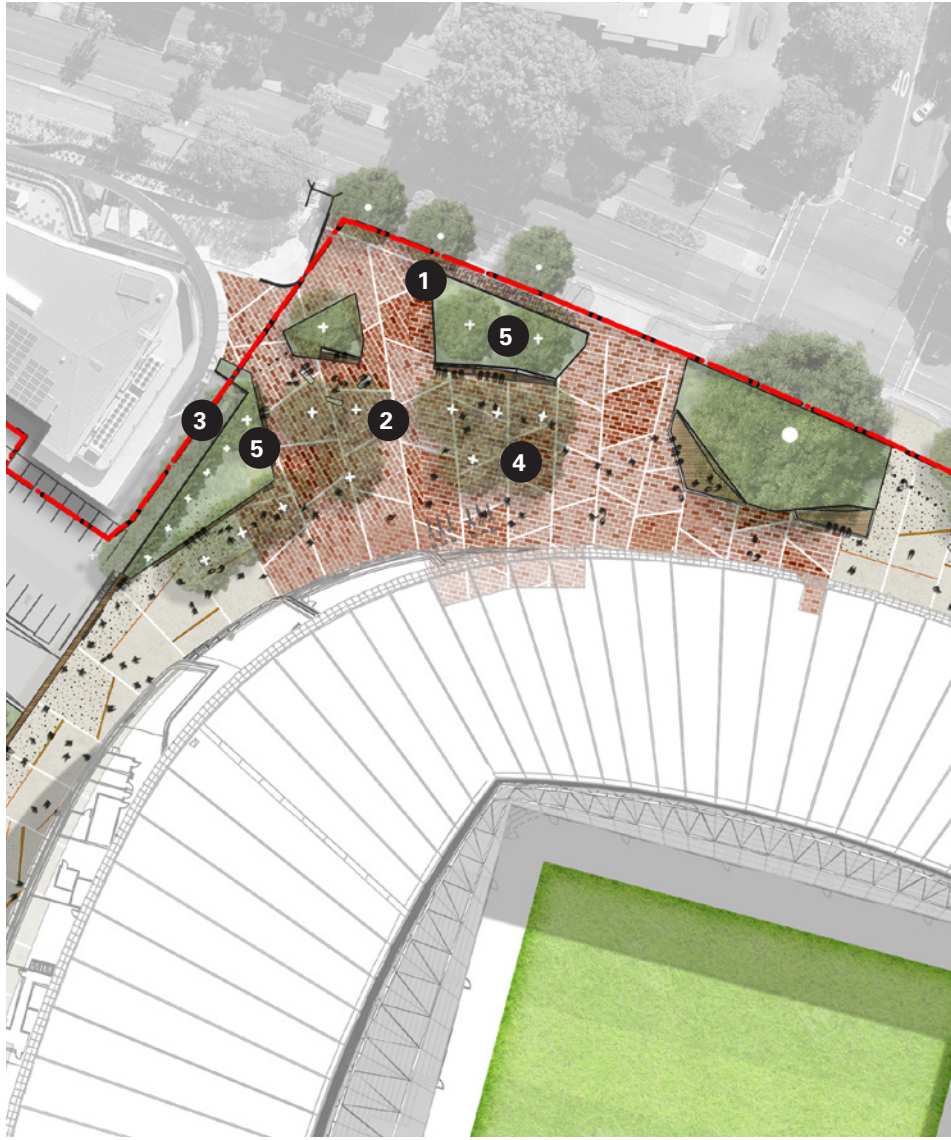
Figure 2: Landscape Design

Table 3: Changes to the Landscape Design

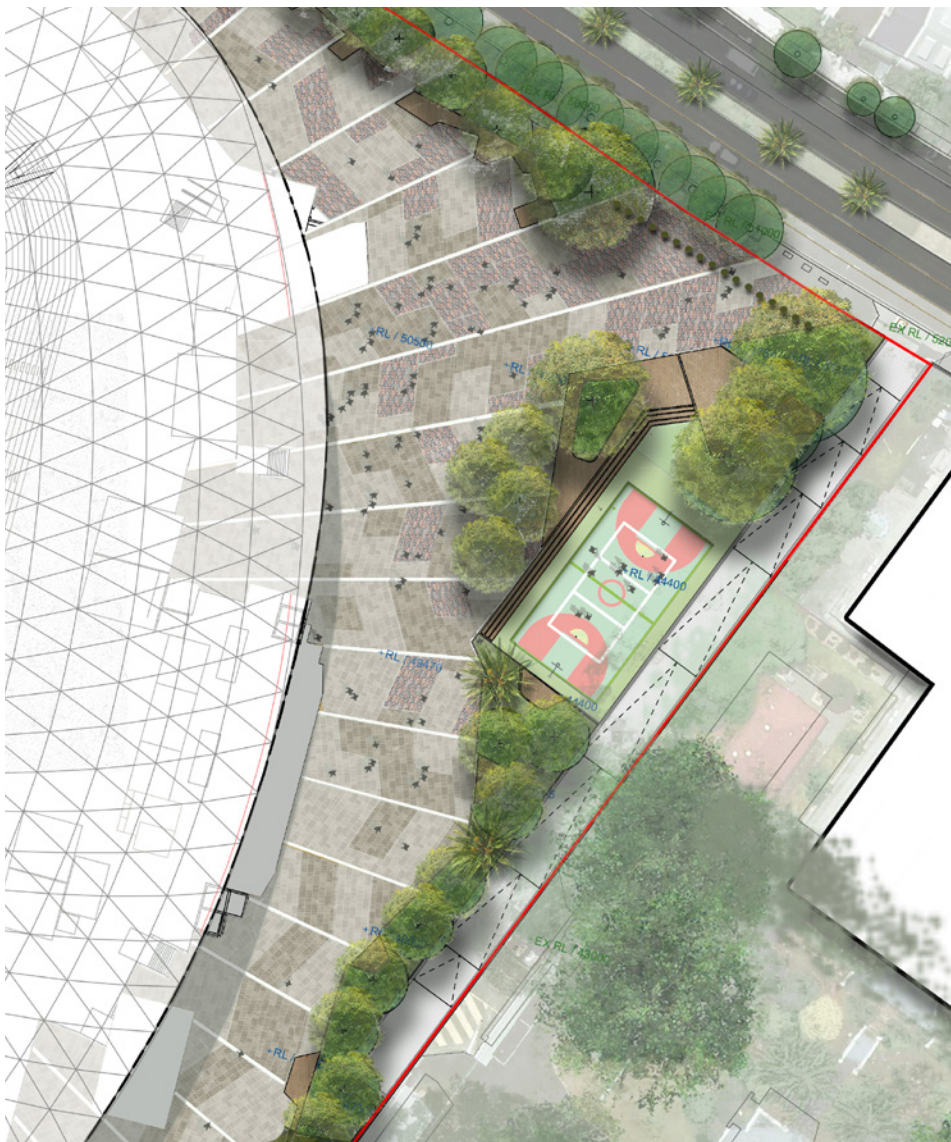
Driver Avenue Steps		
Competition Concept Design	Design (2019)	Design Changes
		<div>1. Split Movement Paths into 2 streams<ul style="list-style-type: none">– Stairs have been orientated to respond to desire lines– 3 flights of stairs respond better to human scale– 2 glass lifts added at the north end to provide enhanced accessibility across the precinct</div> <div>2. Folding Brick stair abutments<ul style="list-style-type: none">– Create a site Gateway which references the language of gateways in the precinct– Responds to Driver Avenue materiality– Planting at upper level will cascade down walls– <i>Proposed tree planting between abutment wall and NRL building adjusted to avoid diverted Sydney Water infrastructure</i>– <i>Additional planting proposed within concourse</i></div> <div>3. Removal of the Oculus<ul style="list-style-type: none">– Removal accommodates additional public realm and an entrance arrival more appropriate to the parkland setting</div> <div>4. Establish central landscaped area with seating terraces<ul style="list-style-type: none">– Provides for amenity and views over Moore Park and Kippax Lake– Forms a secondary movement path– <i>Central landscape space in Driver Avenue stair reduced in width and rationalised</i></div> <div>5. Detailed paving to mark site and Stadium entry</div> <div>6. Stair width rationalised</div> <div>7. Western edge of concourse reduced. Removal of planted edge and trees at concourse edge to ensure pedestrian flows are accommodated.</div> <div>8. Mass planting with trees proposed on edge of MP1 and stair to MP1 removed</div> <div>9. Layout adjusted to include ramp to members area</div> <div>10. Loss of planting and trees to accommodate pedestrian flows</div>

Note: A Section 4.55 modification being assessed by the DPIE that proposes to revise the landscaping in this portion of the site. When the modification has been approved the DIR will be updated.

3. Design Amendments

Fig Tree Place		
Competition Concept Design	Design (2019)	Design Changes
		<ul style="list-style-type: none">1. Site extended to include the existing carpark area on Moore Park Road to increase permeability at the north west corner2. Additional trees for passive shade3. <i>The steps adjacent to the Rugby Australia Building retained.</i>4. Detailed paving to mark site and Stadium entry5. Geometry and forms of planted landscaped areas rationalised

Competition Concept Design



Design (2019)



Design Changes

1. Stadium Entry moved from north east Corner to Paddington Lane
 - Activity zone restructured to direct pedestrians to stadium entry
 - Structured to allow high volumes of pedestrian egress
2. Arrangement of community area
 - Steps are now on desire line from Moore Park Road
 - Creates stepped activity platforms following concourse grade with flush connections at landings
 - Removal of the full basketball court to be replaced with basketball hoops, exercise provision and informal recreation as a more diverse community offering
 - Number of stairs reduced to accommodate revised stadium levels
3. Maintain accessible connection from Moore Park Road
4. Detailed paving to mark site and Stadium entry
5. Width of stairs adjusted to accommodate pedestrian flows

3. Design Amendments

South East Corner		
Competition Concept Design	Design (2019)	Design Changes
		<p>Design Changes</p> <ol style="list-style-type: none">1. Removal of southern stairs on Paddington Lane resulting in 'ramped concourse'<ul style="list-style-type: none">– 1:21 from Moore Park Road to stadium entry– 1:14 from Stadium Entry to SCG2. Additional planting proposed

3.2. Architectural Design

Enhancements have been made to the architectural design as a result of design development and to respond (in part) to feedback received during the public consultation process, as well as to respond to conditions of consent imposed under SSD 9835. Figure 3 provides the architectural concept from the winning competition entry. Figure 4 provides the architectural proposal for the Stage 2 development application. Table 4 provides the details of the changes from the competition winning scheme to the proposed design.




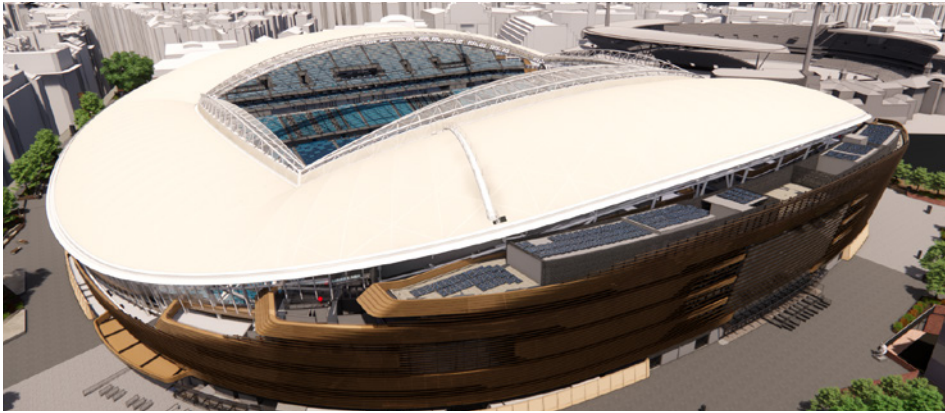
Figure 3: Competition winning concept design

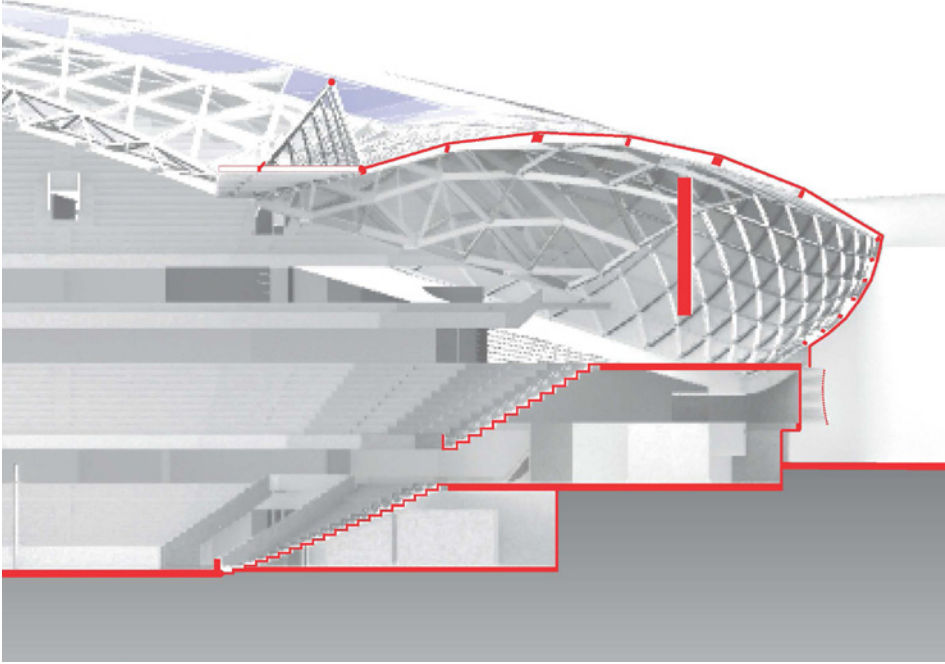
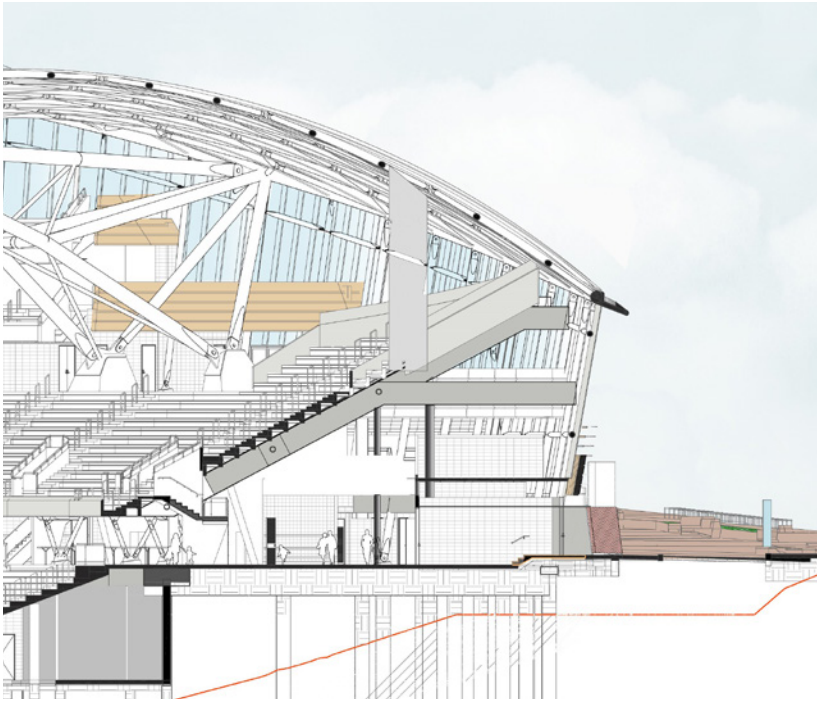



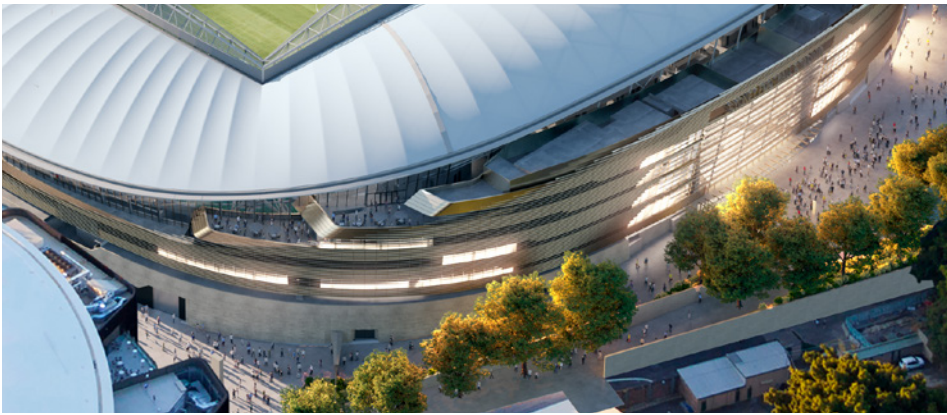
Figure 4: Proposed Design



3. Design Amendments

Table 4: Changes to the Architectural Design

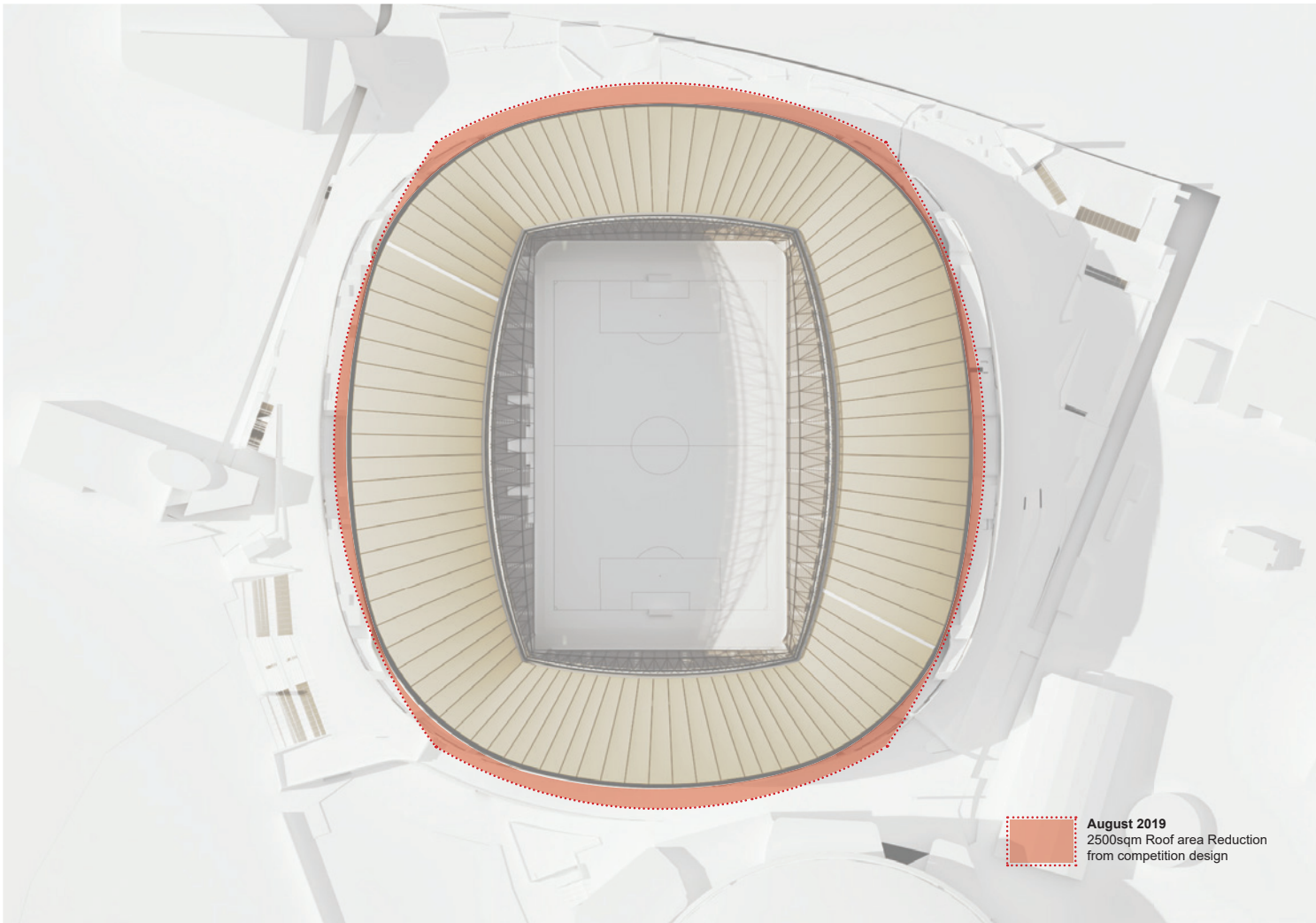
Solar Photovoltaic Array		
Competition Concept Design	Design (2019)	Design Changes (November 2020 changes <i>italicised</i>)
		<p><i>Distribution of the photovoltaic array moved to the roofs of Level 5 on the East and West Stands to provide for more efficient operation.</i></p>

Roof Line		
Competition Concept Design	Design (2019)	Design Changes
		<p>Roof height at northern and southern ends lowered approximately 1-2 metres to assist in reducing scale of the building.</p> <p>Roof height at northern and southern end reduced approximately 5.5 metres from competition winning scheme to reduce scale of the Stadium.</p>

Extension to Stepped Terraces		
Competition Concept Design	Design (2019)	Design Changes
		<p>Removal of landscape from the terraces to the east and west on levels 2, 3 and 4.</p> <p>Removal of level 5 occupiable terraces. Building plant only.</p>

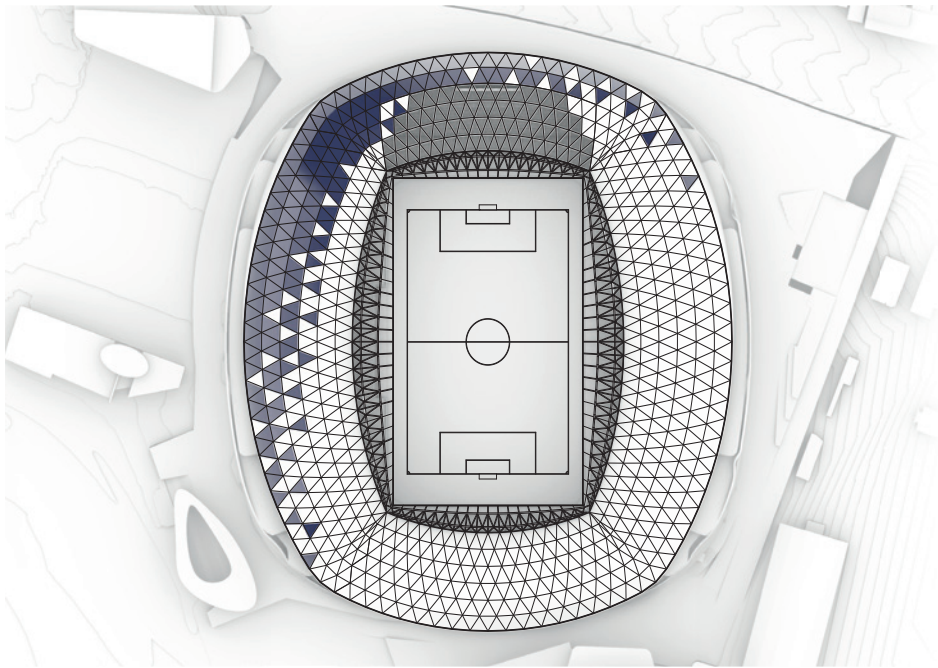
Louvre Screen		
Competition Concept Design	Design (2019)	Design Changes
		<p>Simplification of louvred screen around the Stadium and change of colour to a bronze metallic finish to better integrate with the surrounds.</p>

3. Design Amendments

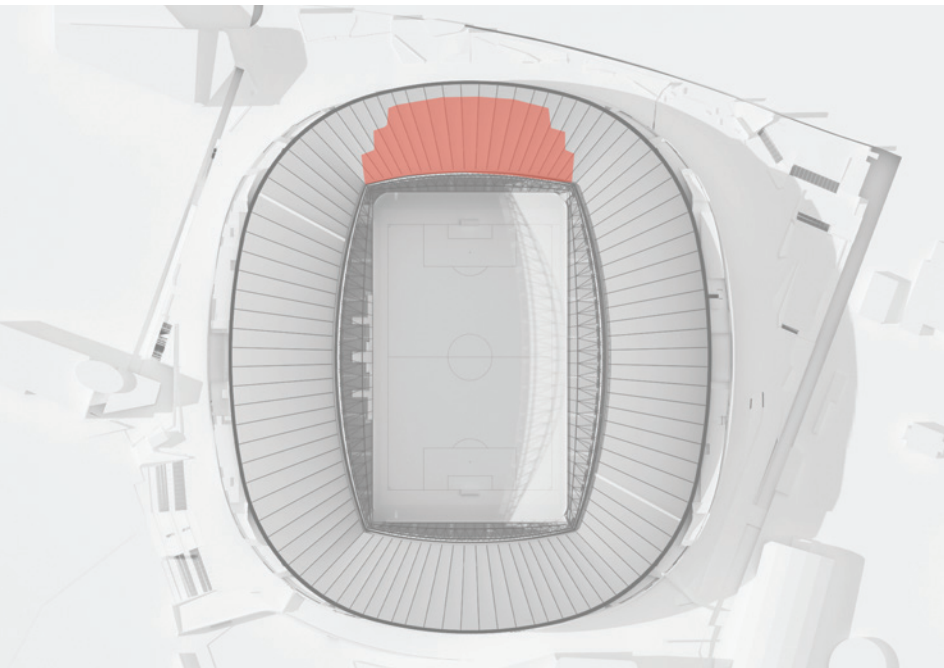
Reduction of Overall Roof Area	
<div>Design (2019)</div>  <p>August 2019 2500sqm Roof area Reduction from competition design</p>	<div>Design Changes</div> <p>The current roof design has reduced the overall plan area by 2500sqm since the competition design. With a 5.5m reduction north and south and 3m East and West.</p>

Reduction of Extent of ETFE

Competition Concept Design





Design (2019)



Design Changes

In the competition design, a zone of clear ETFE existed on the northern side of the stadium intended to maximise daylight penetration to the grass pitch. Through further daylight analysis during the value engineering process, it was identified that this zone was providing little overall benefit to the grass growth. As a result, this area was replaced with translucent PTFE. This alteration represents an overall reduction of 3800sqm of ETFE from the competition design to the December 2019 design.

3. Design Amendments

Removal of Media Mesh		
Competition Concept Design	Design (2019)	Design Changes
		<p>All areas of media mesh represented on the external facade of the building at competition phase have been removed.</p>

Material Changes

Competition Concept Design





Design (2019)



Design Changes

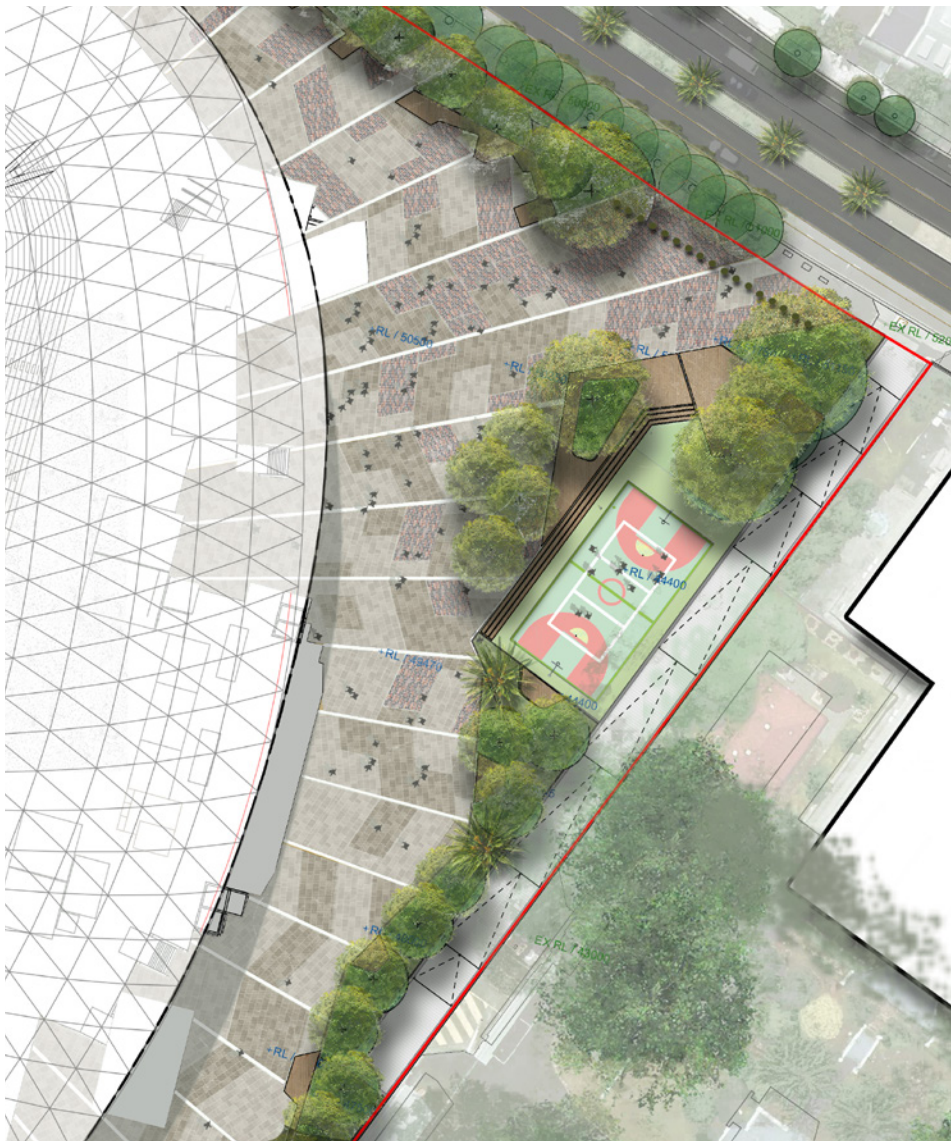
The Façade materials in the design are consistent with the intent expressed in the competition. The areas indicated as sandstone in the competition have been replaced with a coloured and textured concrete, evocative of sandstone. Aluminium façade fins were nominated in the competition as the predominate façade material. This remains the same. However, the tone and colour of this has been warmed up significantly to be more sympathetic with the character of the surrounding context.

3. Design Amendments

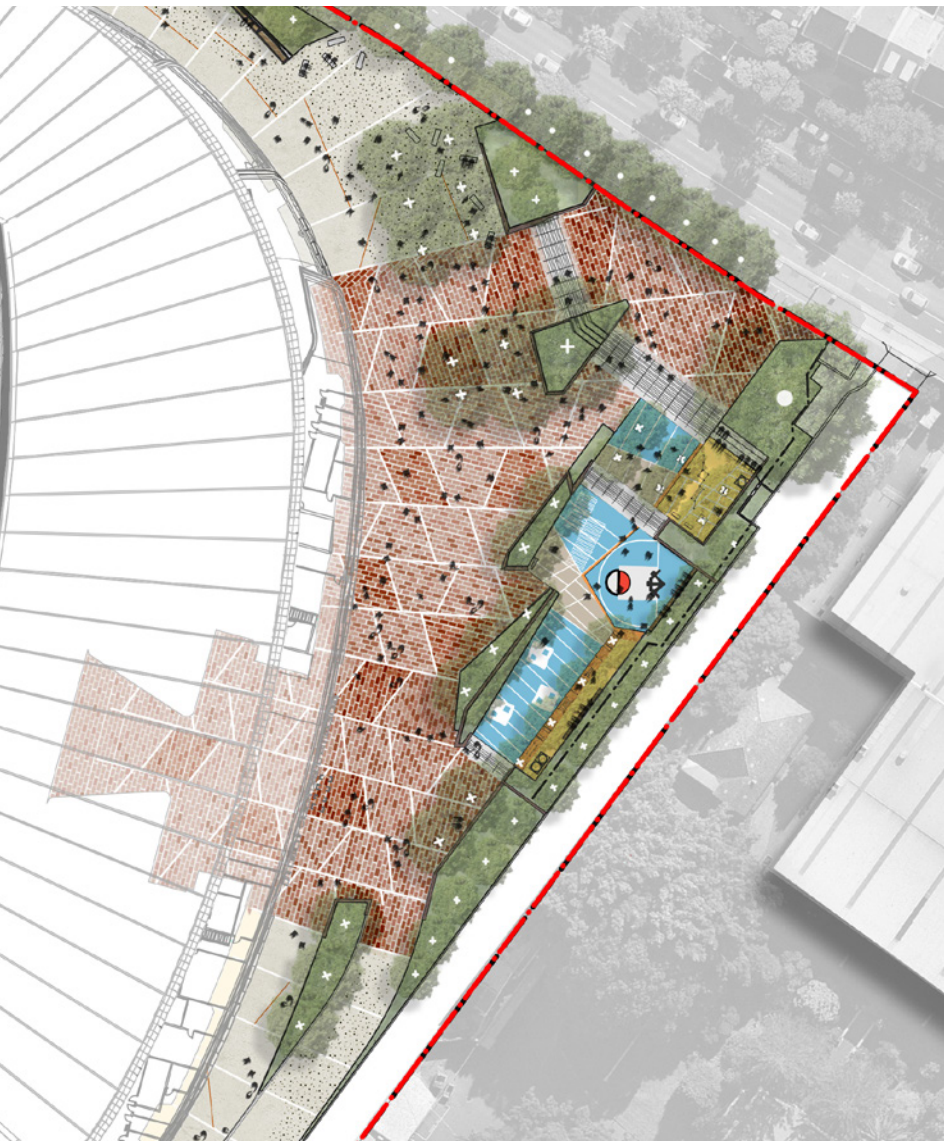
Roof Skirt		
Competition Concept Design	Design (2019)	Design Changes
		Reduction of the roof 'skirt' on the underside of the stadium roof to reduce visual bulk.

North Eastern Entry

Competition Concept Design



Design (2019)



Design Changes

Amalgamation of the north east and south east entries to one entry on the eastern elevation.

4. Panel Assessment

The Design Integrity Assessment Panel has reviewed the design changes from the competition winning scheme to the (as approved and now proposed) design and is of the opinion that the changes proposed are consistent.

The changes do not alter the Panel’s conclusion that the design demonstrates design excellence as outlined in the Competitive Design Alternatives Report. The changes are considered to be consistent with the Panel’s review of the design in relation to the Sydney Local Environmental Plan 2012, the objectives of Better Placed or the project Urban Design Guidelines.

5. Panel Endorsement

Peter Poulet (Chair)

Kim Crestani

John Perry

Tom Gellibrand

This page has been left intentionally blank as the CCC has not been required to comment on this section of the Design Integrity Assessment Report.

Appendix A

Minutes of Community Consultative Committee Meeting

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Appendix B

Feedback from the Community Consultative Committee

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Appendix C

Addendum

Precinct Village and Car Park

1. Design Amendments

1.1. Landscape Design

Since the approval of SSD 9835, Venues NSW has worked with the design team to develop the scheme for a new Precinct on the MP1 Car Park site. Figure 1 provides the landscape design from the Stage 2 development application. Figure 2 provides the landscape design of the current Precinct Village and Event Plaza proposal. Table 1 provides the details of the changes from the Stage 2 development application to the current proposal.

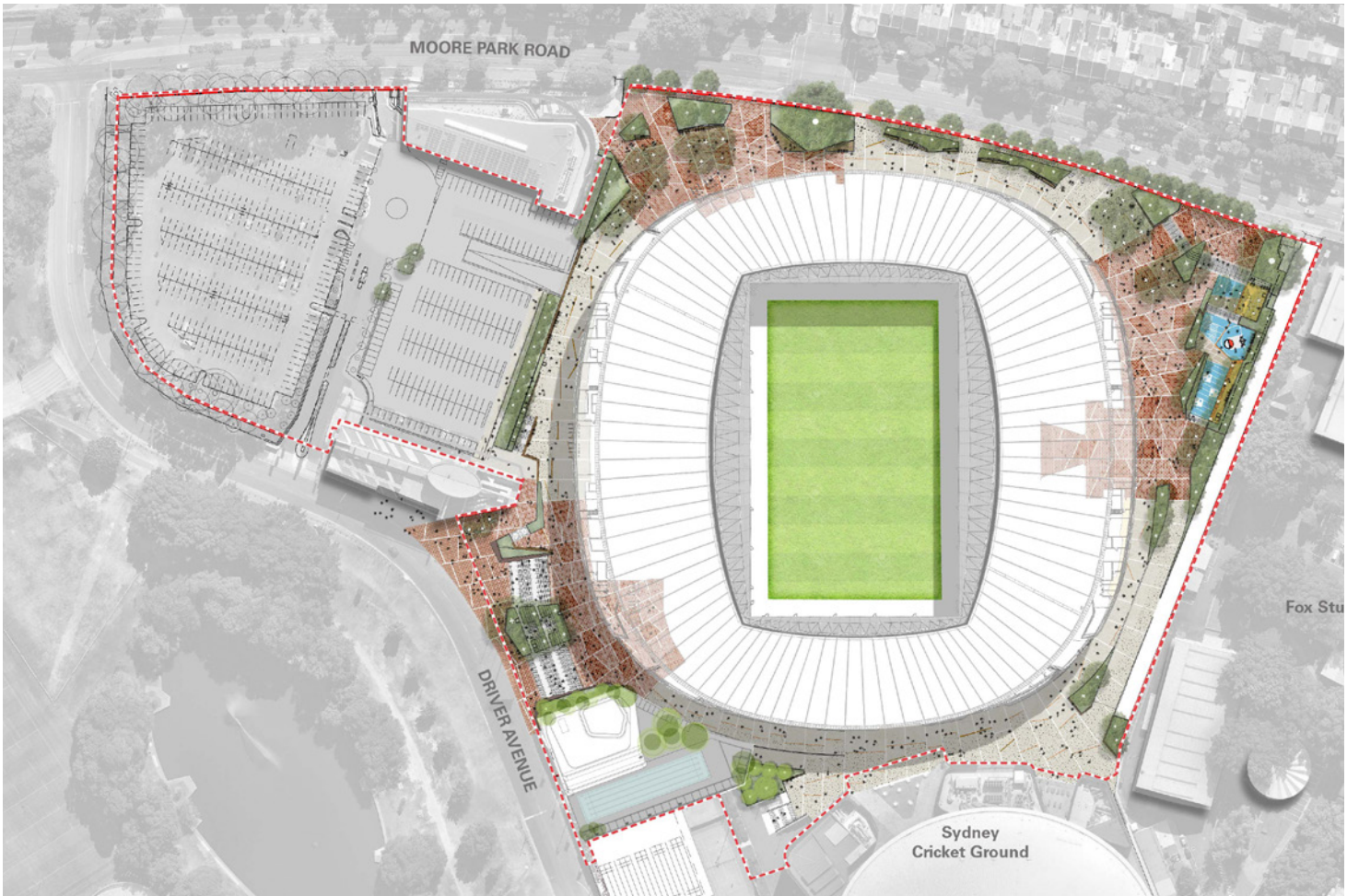


Figure 1: Stage 2 Development Application Design

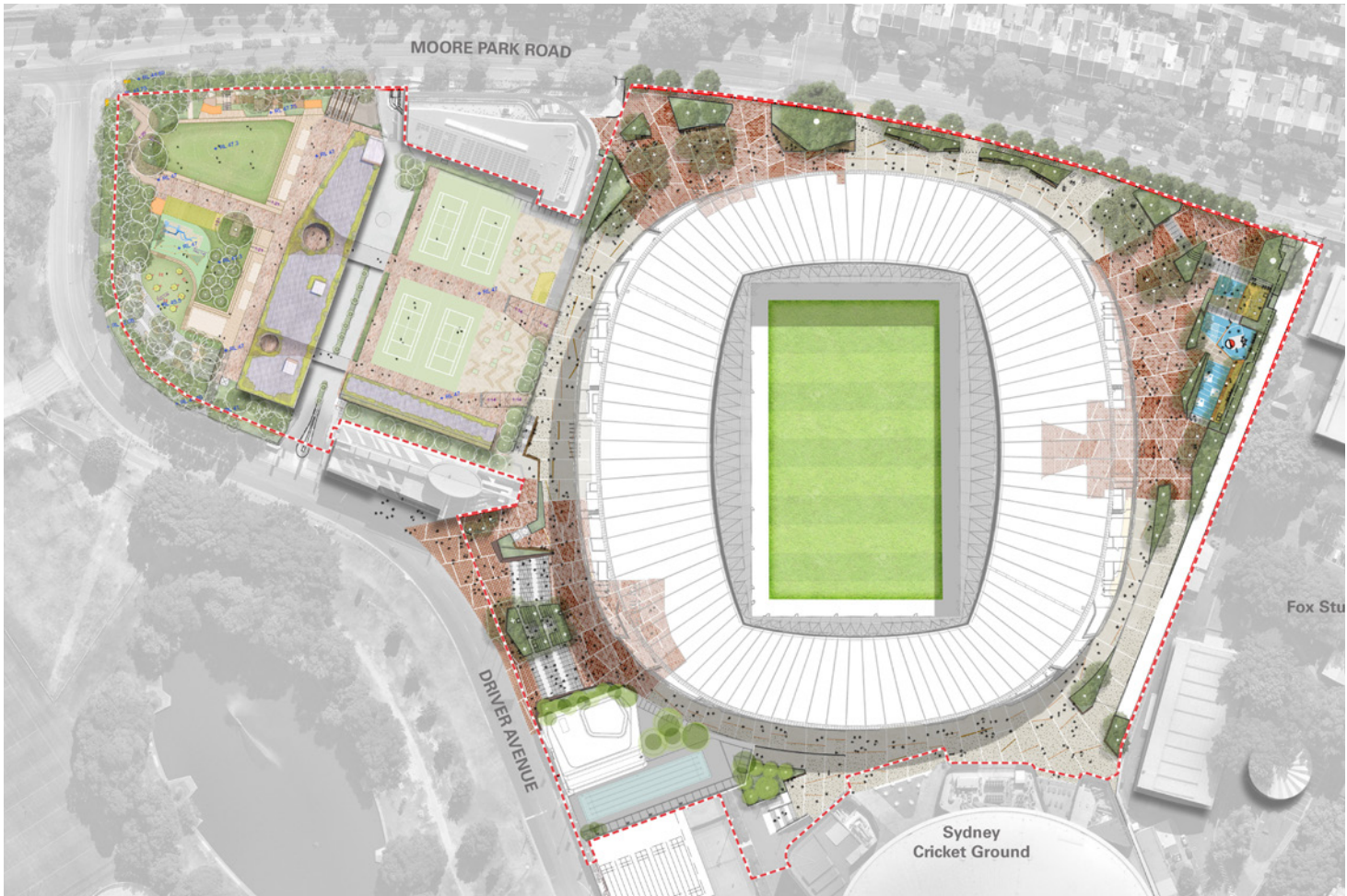


Figure 2: Proposed Landscape Design (September, 2021)

Venues NSW (VNSW) is proposing to introduce a village community space, event plaza and multi level car park to complement the SFS and adjoining Moore Park and Centennial Parklands. The proposed development will facilitate the permanent closure of the EP2 on-grass parking areas within Moore Park opposite the MP1 car park once the development is commissioned and enable its use for open space purposes consistent with the Moore Park Master plan.

The vision for the Precinct Village and Car Park is set out below:

The Precinct Village and Car Park provides a platform and canvas for an exceptional community asset and iconic design that visually and physically connects to the adjacent Moore Park East and Kippax Lake. It provides patrons with quality café and dining experiences in an idyllic parkland setting, and well-being play and relaxation nodes which engage with all ages. An event plaza, connected to the Stadium plaza, provides a seamless opportunity for greater patron and community engagement through non-event and event day functions. Key design areas of the plaza includes:

- ① Village Terraces + Lawn
- ② Eat Street with pergolas + Tree Grove
- ③ Village lawn + terraces + compliant meandering ramp
- ④ Nature play under Feature Pavilion
- ⑤ Tennis Court + flexible spectating

Located to the west of Moore Park, the Village Precinct utilises deep soil opportunities with new endemic and native vegetation, character and permeable finishes treatment to continue the parkland typology. Thus, creating a strategic urban connection between the Stadium and its surrounding context.



Key areas offer additional community amenity, complimenting and responding to design considerations raised within the Moore Park Master Plan 2040 (including the revitalisation of green and built spaces), supporting movement structure and hierarchy, incorporating adaptive landscape elements and extending a world class sports precinct to the community. The Village Precinct also integrates material palettes for the Stadium development. The Village Precinct will feel connected inherently to the Stadium, as well as embodying a sense of parkland and community.



Figure 3: Proposed Landscape Design (September, 2021)

1. Design Amendments

Table 1: Changes to the Landscape Design

1. Changes to MP1 Car Park West		
Stage 2 Development Application MP1 Car Park West (as approved)	Precinct Village and Car Park (September, 2021)	Design Changes
		<p>1. The Village Terraces are elevated above Driver Avenue. The space lends itself to integrated terraces underneath a rich heritage tree canopy, as well as panoramic views to Kippax Lake, to create an active gathering space for people to watch and spectate over Moore Park East.</p> <p>The green area is framed with additional timber terraces adjacent to the proposed Eat Street as well as pergolas to encourage informal seating and eating opportunities, whilst retaining a more intimate scale and privacy.</p> <p>2. The new 6.2m wide pedestrian Eat Street is the primary North South site connection. Eat Street is designed to facilitate people in event modes, whilst serving as a visual and physical gesture to provide legibility. With commercial, food and beverage opportunities, and pergolas, Eat Street will feel like a vibrant, slow environment to meander within a parkland setting.</p> <p>The grove comprises of a compressed grove of native evergreen trees to provide shade all year round. Underneath the grove of trees are a series of custom seats, adaptable for small groups and individuals. Facilitating a carpark underneath, proposed planting and trees are raised to allow for sufficient volume for tree growth and lawn.</p> <p>3. The Village Lawn is located immediately south of Moore Park Road and takes advantage of the good year round solar access to provide a lush, green local scaled turf area for local residents and visitors alike. Lifted nominally 300mm above the adjacent Eat Street, the turf area is accessed by short steps and ramps around its circumference.</p> <p>To the west, the Village Lawn is flanked by generous planting and a ramp pathway that negotiates the level change between Driver Avenue and the Village Precinct. The northern edge of the turf is characterised by generous timber plinths providing seating opportunities, as well as negotiating existing level conditions with the retention of existing trees and the subterranean car park.</p> <p>4. The community pavilion is located between the two lawn areas and comprises of a free standing structure with seating and tables underneath a canopy cover. The canopy creates a strong edge for the east/west connection, giving definition to the spaces within and creating opportunities for activation. It also provides additional shade to the adjacent play area, as well as housing access stairs to the carpark underneath. The pavilion will retain material proposed throughout the Stadium Precinct. A play area is proposed adjacent to the pavilion to the north of the terraced lawn area, extending a sense of parkland and community. Proposed play elements will encompass its natural setting through rope and climbing elements, water and sand play.</p>

2. Changes to MP1 Car Park East

Stage 2 Development Application MP1 Car Park East (as approved)



Precinct Village and Car Park (September, 2021)



Design Changes

5. The Village Precinct extends the sports amenity offering in the Stadium Precinct with the provision of four tennis courts, a flexible spectator area, pavilion and tennis club house. The spectator area proposes removable seating to provide amenity, comfort and respite in a flexible format that can be customised to suit differing event needs.

By extending the Stadium's concourse feature paving treatment, the area will read as the connector between the Stadium, Stage 2 of the Precinct Village and Moore Park East Parkland. The Tennis courts in Stage 1 have all access 1:14 ramps from the Stadium concourse.

1. Design Amendments

3. Tree Retention and Replacement Plan

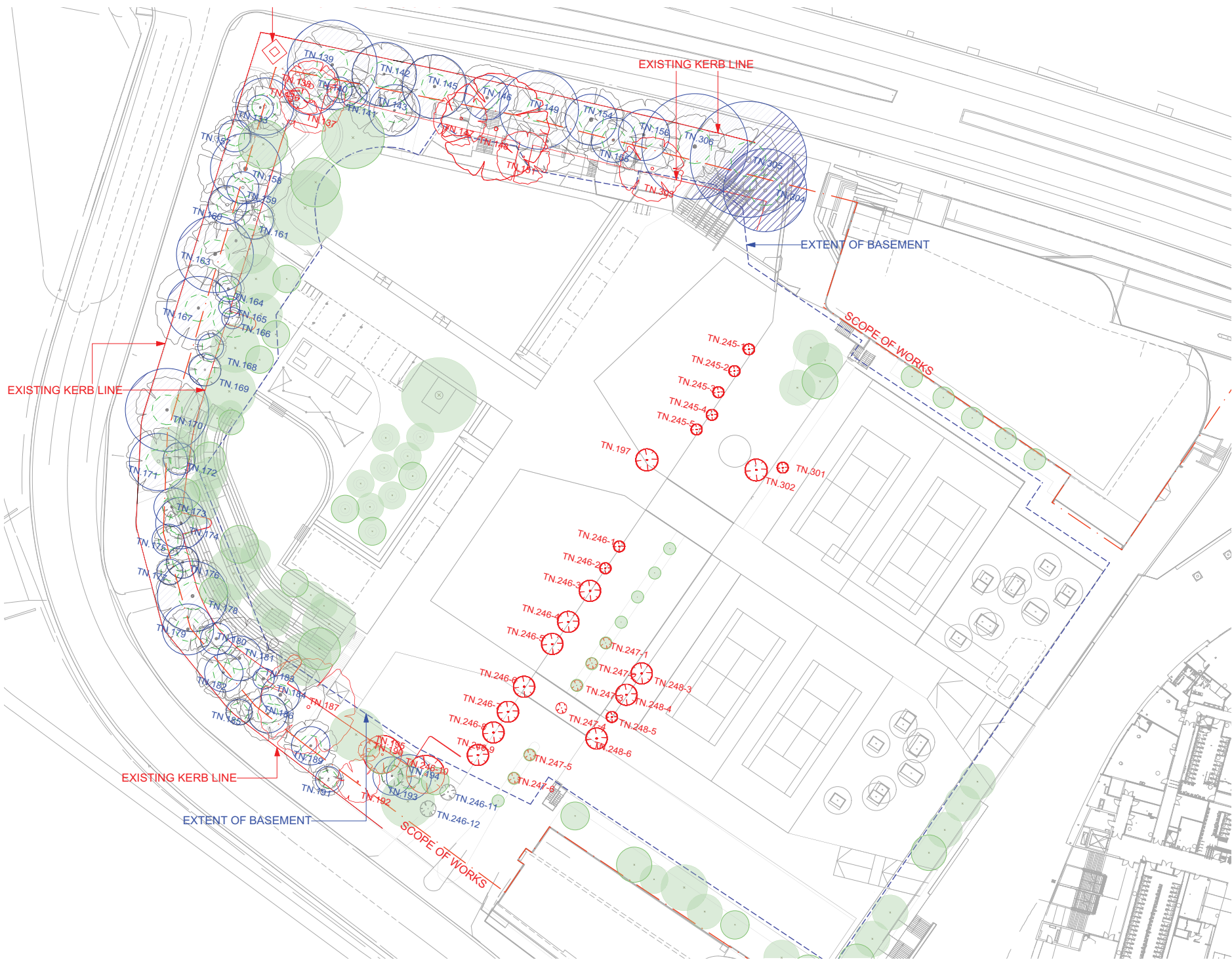
Precinct Village and Car Park (September, 2021)

Design Changes

SSD 9835 provides for an overall tree replacement strategy across the SFS development. The modification for the Stadium Fitness Facilities increased the total to 120 trees.

The Precinct Village and Car Park modification proposes planting of an additional eighty-nine (89) trees and the removal of thirty-nine (39) existing trees. Retention of existing fig species (*Ficus macrophylla* and *Ficus rubiginosa*) were of high priority to retain and extend the Moore Park parkland character and protection of local habitats (bat habitats).

Proposed trees are native Australian species, many of which are endemic to the Eastern Suburbs Banksia Scrub.



- Legend
- Existing Trees to be retained and protected
 - Proposed New Trees
 - Trees proposed for Demolition

4. Legibility and Way finding

Design as shown in DIP Presentation (August 2021)



Precinct Village and Car Park (September, 2021)



Design Changes

In response to DIP feedback, benchmark threshold details have been investigated for the north-western entry to the Precinct Village. The Proposed Design image shows indicative locations of these monumental features on either side of the pathway leading into the Precinct. See following page for examples of potential thresholds.

The detailed design of the future entry portals will be presented to the DIP following consent and prior to construction commencing. The entry portals will be detailed in the final landscaping plan.

- Legend
- Indicative location of monumental entry 'portals'



1. Design Amendments

Potential threshold detail benchmarks

4. Legibility and Way finding

Monumental Entry Portal Benchmarks



Digital Tower at Yagan Square, Perth



Centennial Park Reservoir Gates, Sydney



Centennial Parklands Gates, Sydney



Centennial Parklands Gates, Sydney

1.2. Architectural Design

Enhancements have been made to the architectural design as a result of the design development for the Precinct Village and Car Park proposal. Figure 4 provides the architectural design from the Stage 2 development application. Figure 5 provides the architectural design of the current proposal. Table 2 provides the details of the changes from the Stage 2 development application to the current proposal.

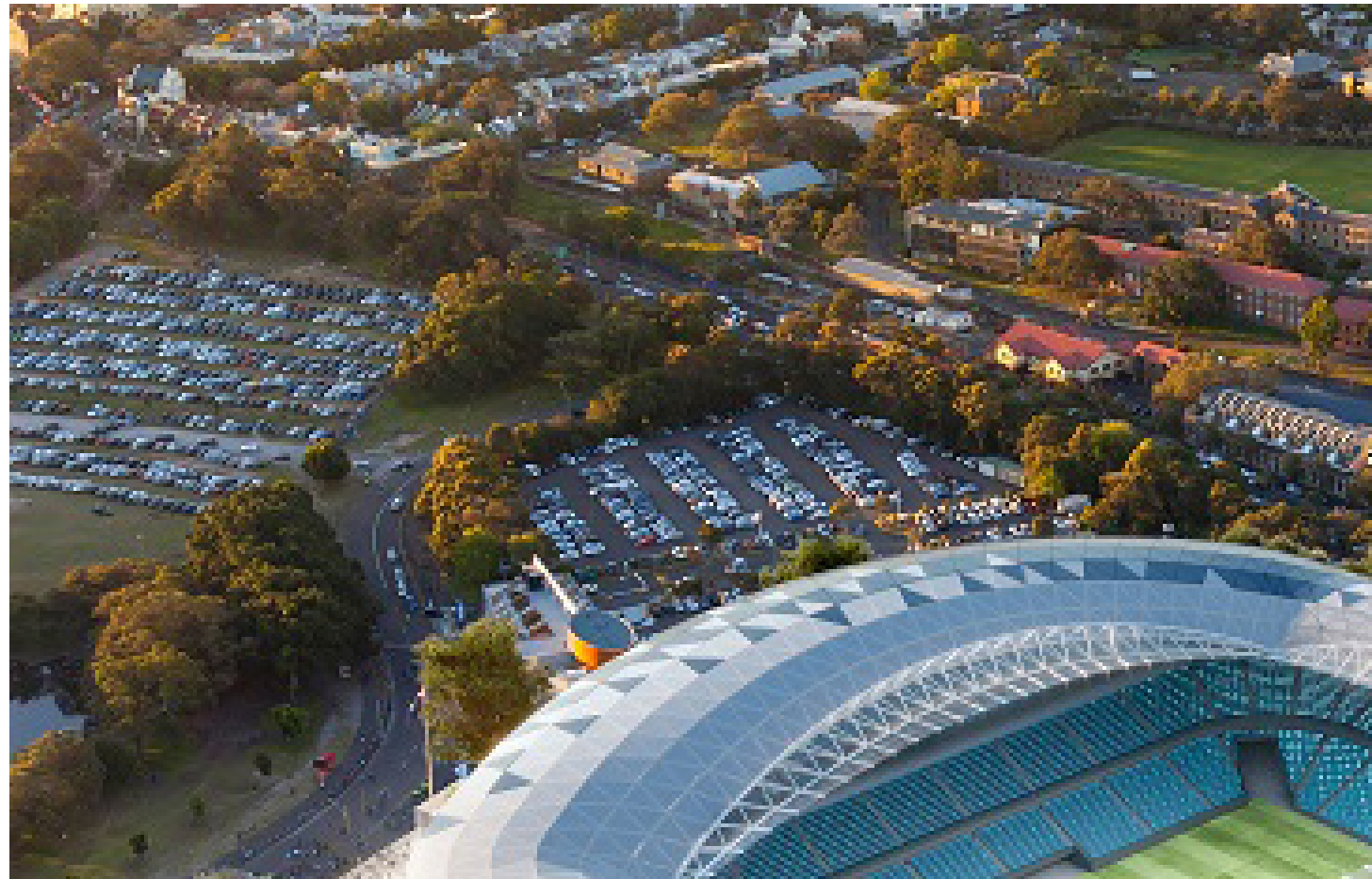


Figure 4: Stage 2 Development Application Design



Figure 5: Precinct Village and Car Park (September, 2021)

1. Design Amendments

Table 2: Changes to the Architectural Design

1. Development interface with the UTS and NRL Buildings

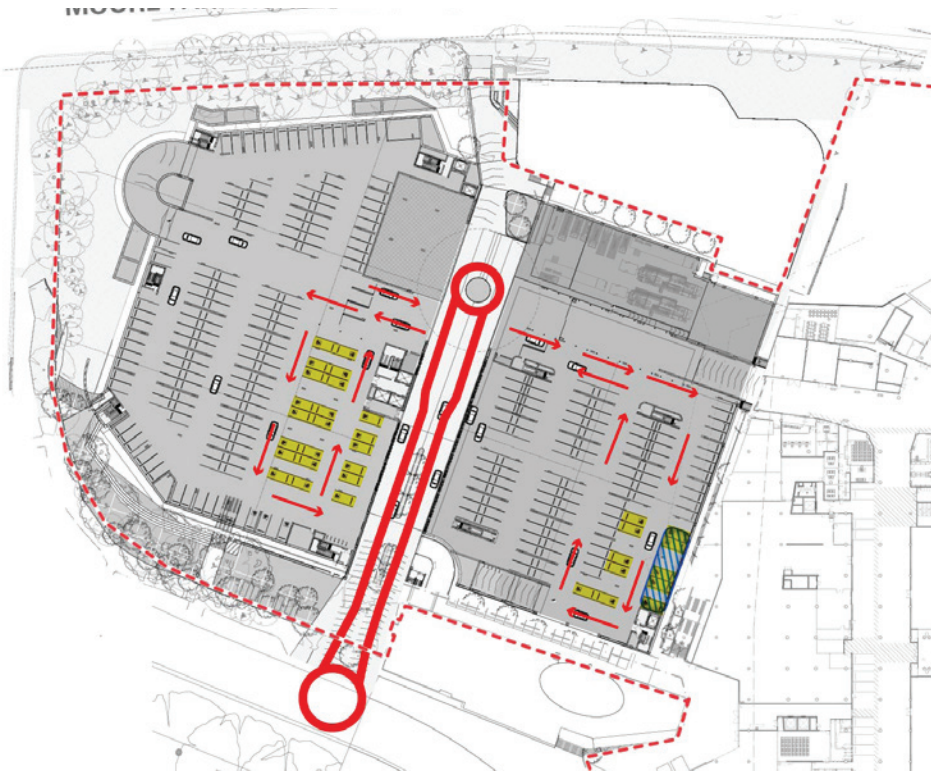
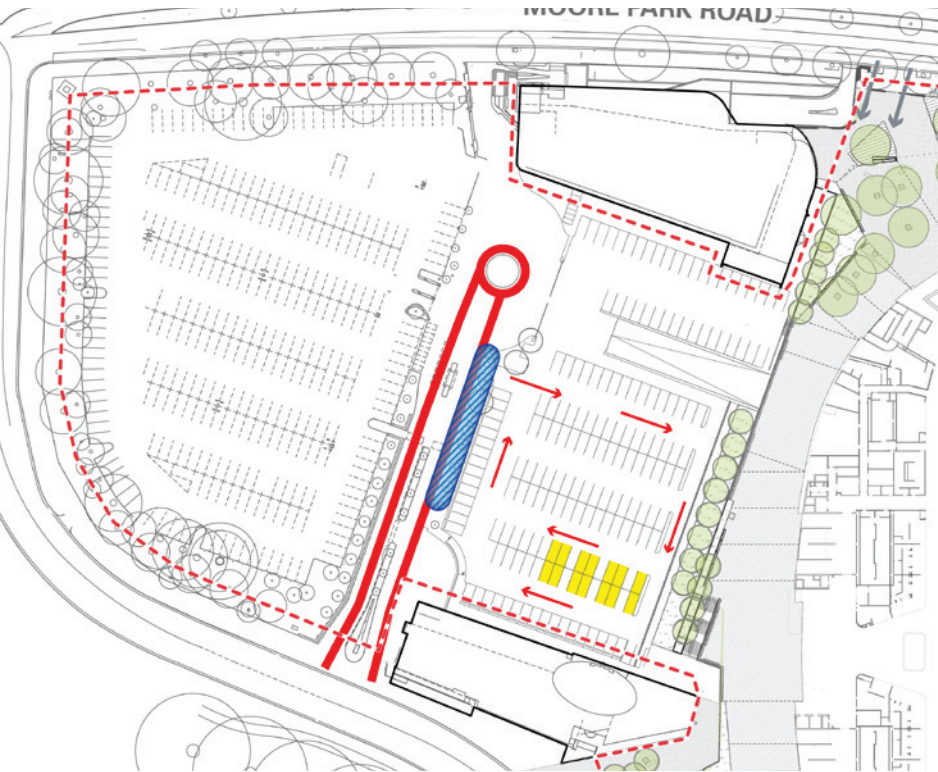
Section A of Plaza Interface with UTS Edge	Section B of Plaza Interface with NRL Edge	Design Changes
		<p>The Village Precinct and Car Park sits adjacent to the Stadium concourse, the NRL and UTS buildings. There is a setback by a minimum 6m from all edges of existing buildings and the SFS concourse.</p>
<p>Section C of Plaza Ramp Interface with Concourse</p>	<p>Section D of Plaza Ramp Interface with Concourse</p>	

3. Accessible Parking Strategy

Stage 2 Development Application MP1 East (as approved)

Precinct Village and Car Park - Car Park Level 0 (September, 2021)

Design Changes



In response to a comment made in the DIP the proposed design promotes universal accessibility, safety and security such that the Village Precinct and Car park is welcoming, inclusive and safe for all users.

Provisions for DDA compliant access and parking within the Precinct Village and Car Park has been incorporated through the following:

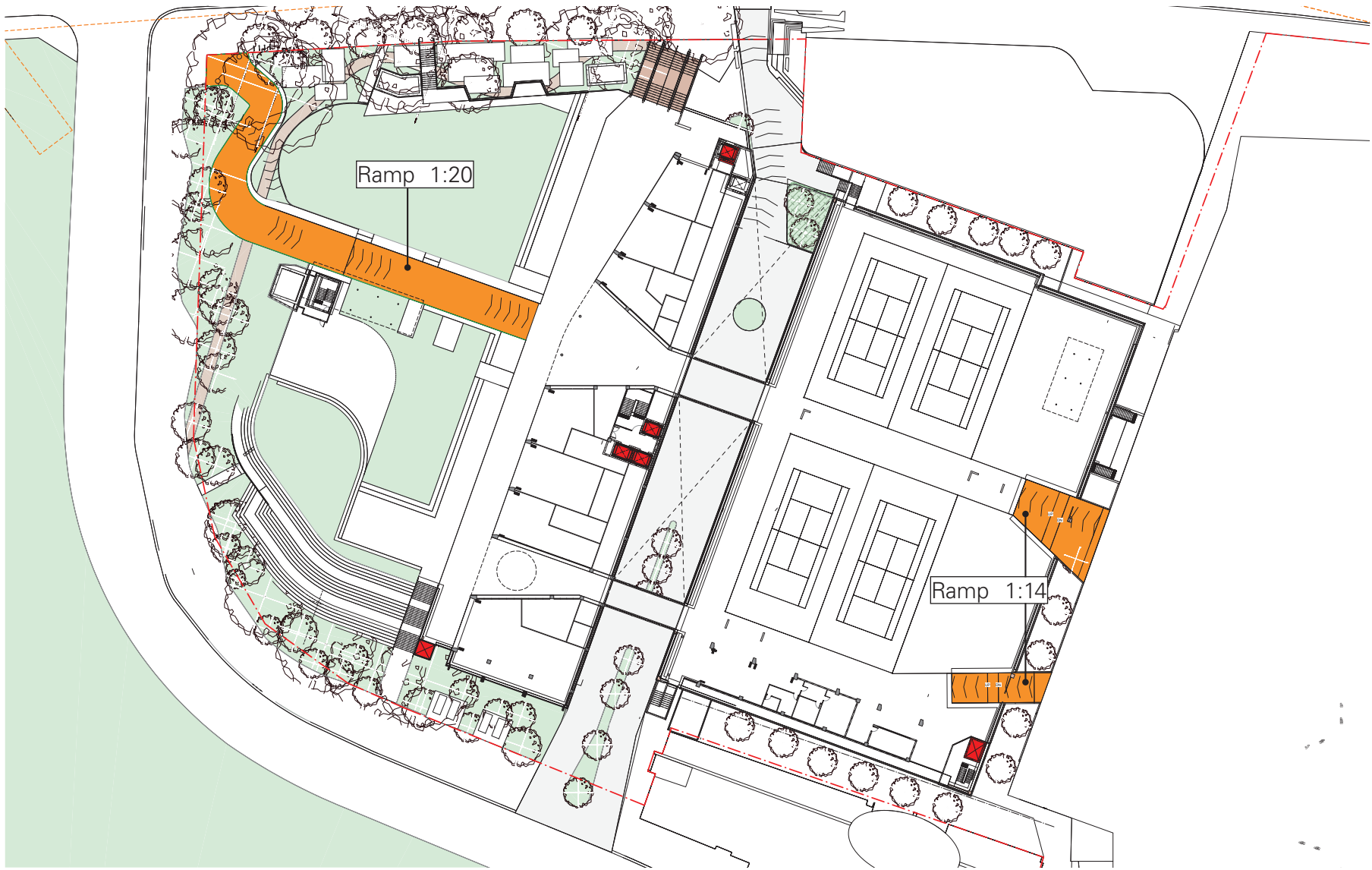
- A Persons Requiring Universal Access (PRUA) drop off area has been located within the Precinct Car Park on Level 0.
- Vehicles will enter off Driver Avenue turning right into the East Car Park to circulate to the drop off area.
- The current proposal has retained 11 accessible car parking spaces in the MP1 East Car Park location and allocated a further 24 spaces in the western car park to serve to the Precinct Village.

- Legend
- Accessible Parking Spaces
 - Car Park Access Loop
 - PRUA Drop off
 - Vehicle Circulation to Accessible Parking and PRUA Drop off

1. Design Amendments

4. Accessibility / DDA Access onto Plaza Level

Plaza Level Plan outlining DDA Access to Precinct Village and Car Park (September, 2021)



Design Changes

- In response to a comment made in the DIP, provisions for a DDA compliant access to and within the site has been incorporated through the following:
- PRUA patrons have compliant access from the drop off area to lifts located within the Level 0 Car Park with direct access to the Precinct Village Plaza level. Six accessible lifts have been provided.
 - Compliant circulation paths are provided throughout the Precinct Plaza level with areas to stop and rest to ensure the site remains accessible for those with impairments.
 - There are three compliant entry access ramps into the Precinct Village. The pathway entering from the corner of Driver Avenue and Moore Park Road is 1:20. The two access ramps leading onto the Stadium concourse from the tennis courts are 1:14.

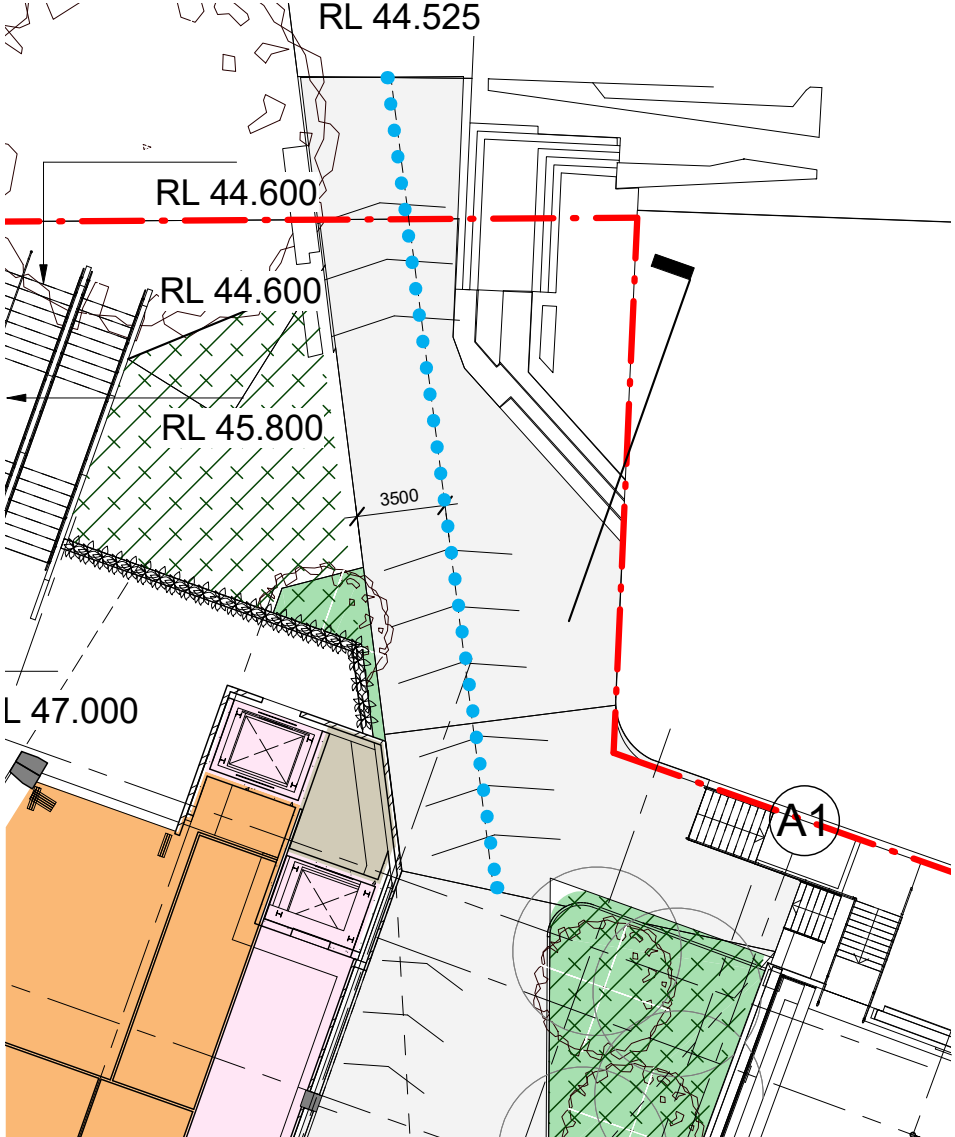
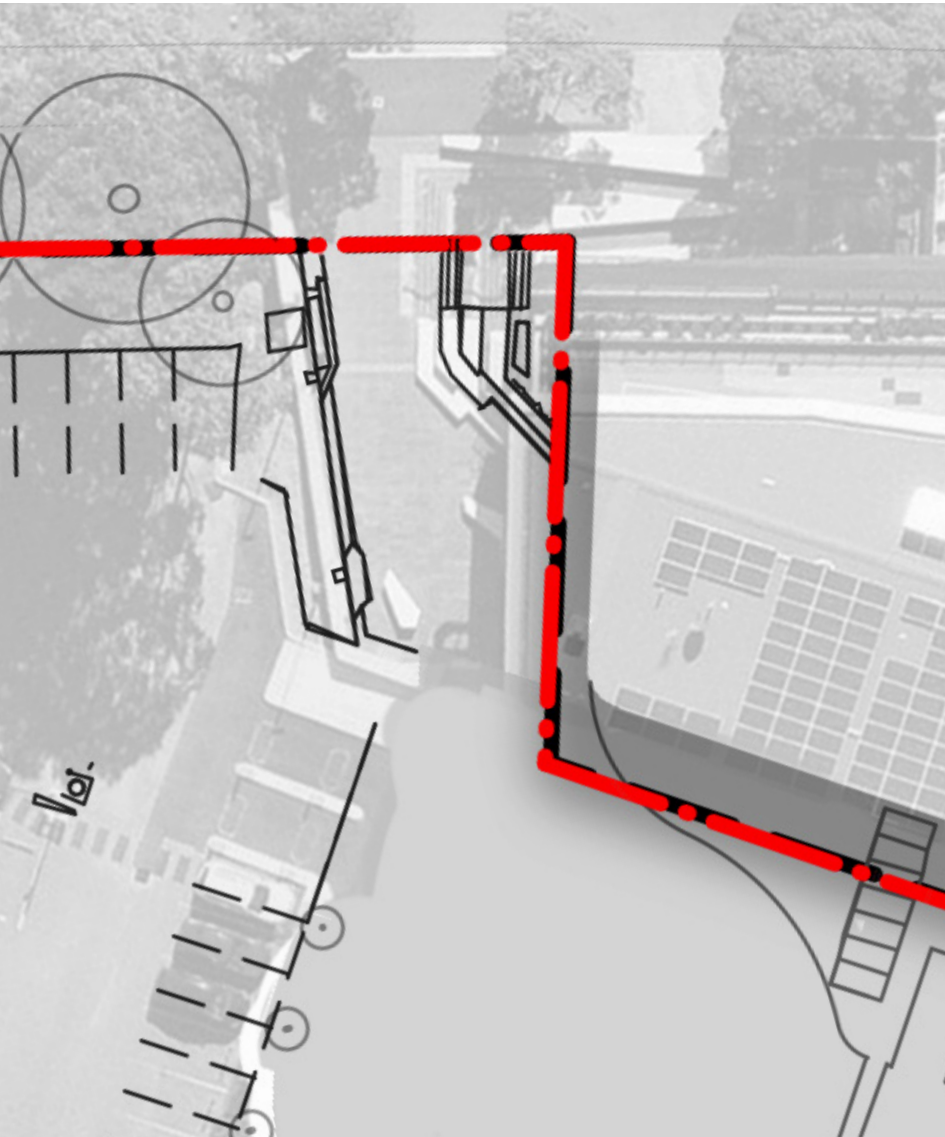
- Legend
- Accessible Entry Ramps
 - Accessible Lifts

5. Roadway width through the UTS Precinct

Stage 2 Development Application (as approved)

Precinct Village and Car Park (September, 2021)

Design Changes



The roadway width adjacent to the UTS Rugby Australia Building is approximately 3.5m. This egress will only be used to cater for additional egress of vehicles out of the car park on major event days. At all other times pedestrians take precedent within this Precinct.

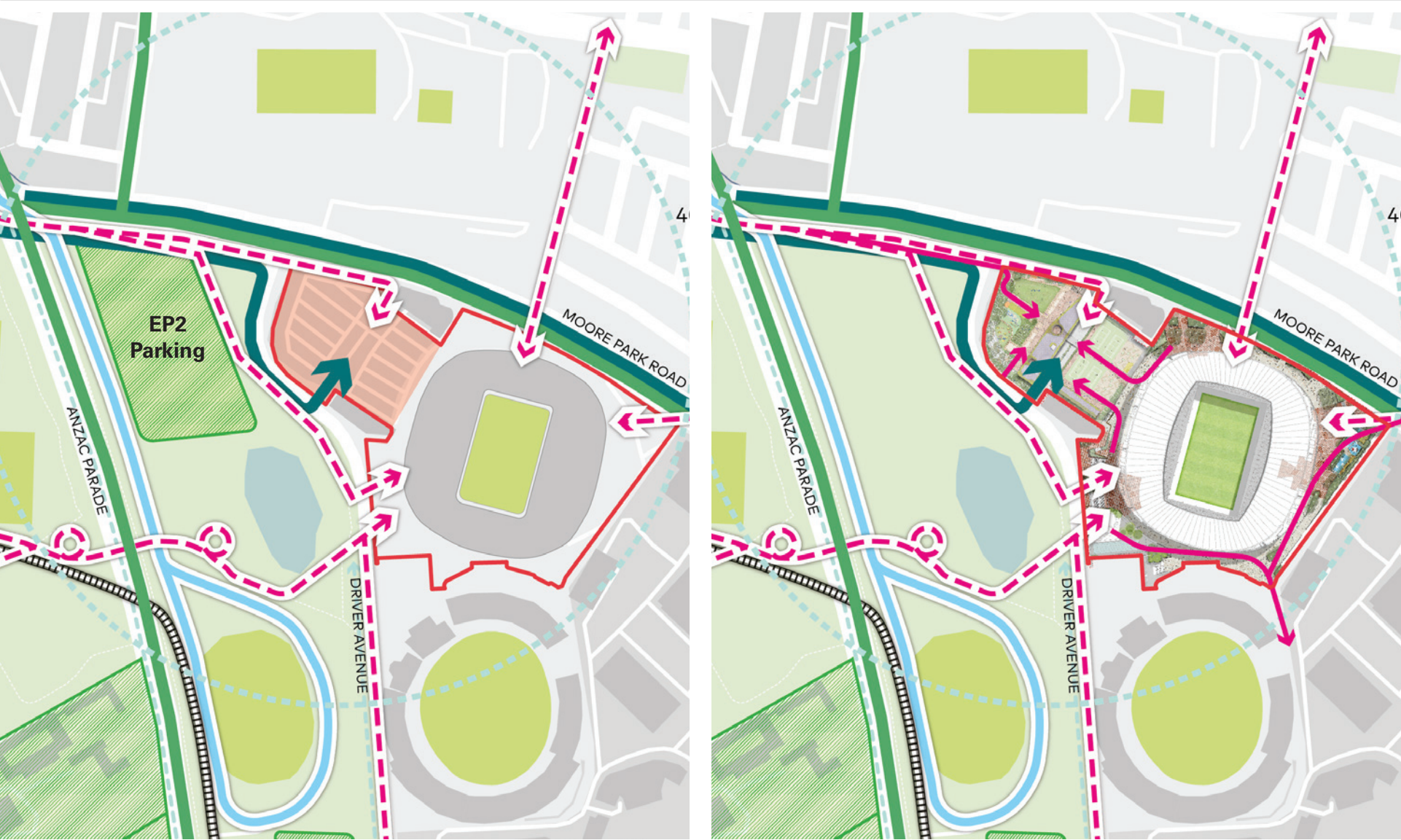
1. Design Amendments

6. Removing Car Parking from Moore Park East

Stage 2 Development Application (as approved)

Precinct Village and Car Park (September, 2021)

Design Changes



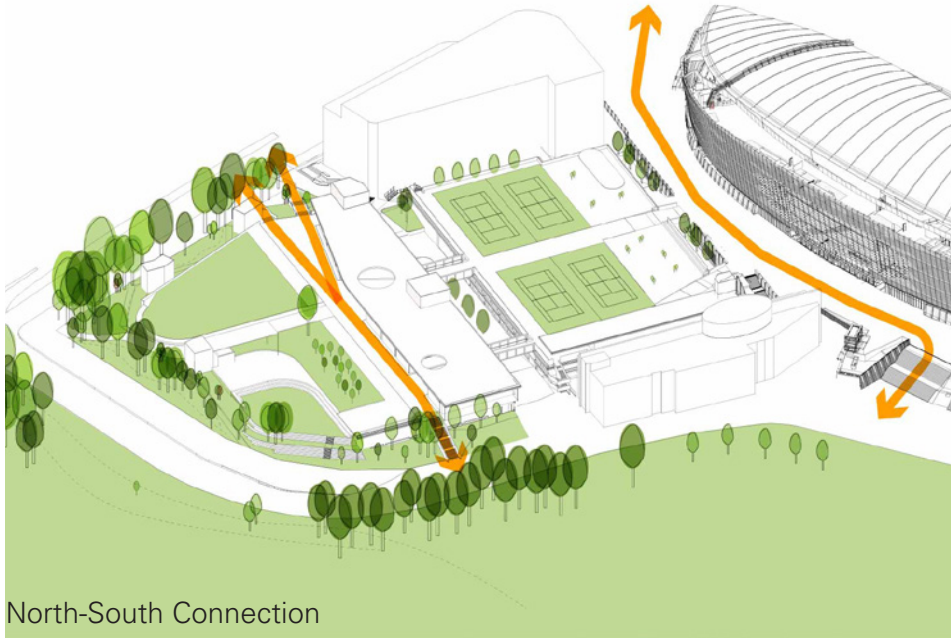
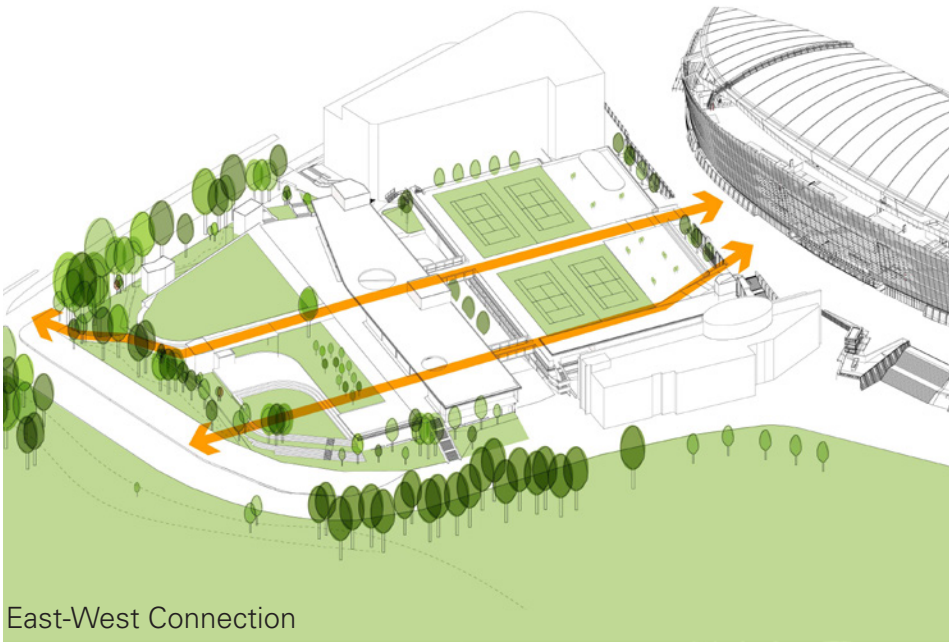
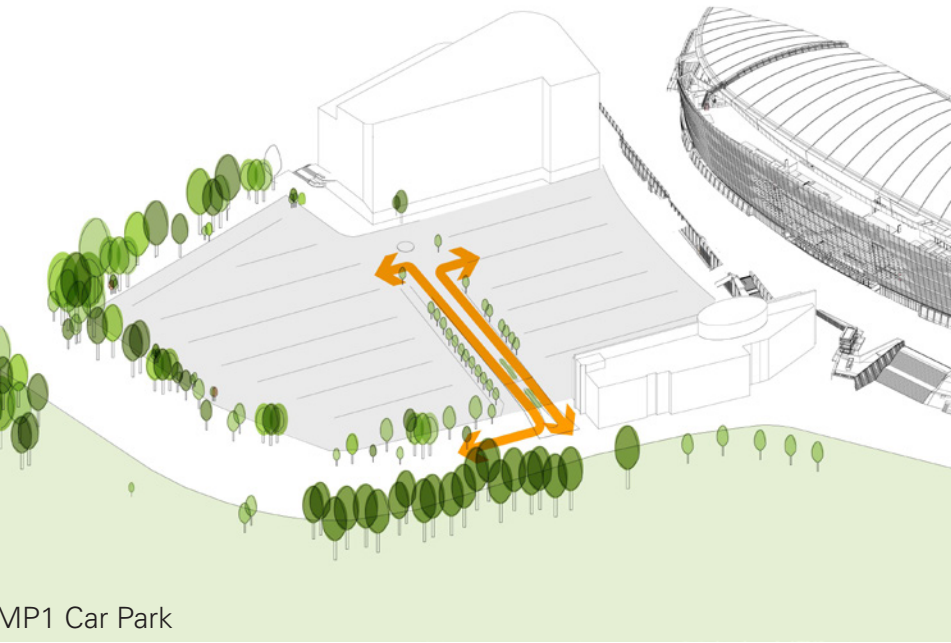
The Precinct Village Car Park design allows for the removal of parking from the Moore Park East Parklands (EP2) because these cars are relocated to the new Car Park on MP1.

7. Increased Permeability through Site

Stage 2 Development Application (as approved)

Precinct Village and Car Park (September, 2021)

Design Changes



The proposed Precinct Village provides significantly improved pedestrian permeability and amenity across the MP1 Car Park site, due to the public Precinct Village and Event Plaza providing seamless connectivity from Moore Park in the west to the Stadium Plaza in the east and from Moore Park Road in the North to Driver and Avenue and Kippax Lake in the south. These pedestrian routes are distinctly separated from the vehicle routes within the site and also enjoy the amenity of F&B outlets, toilets and play and activity areas.

1. Design Amendments

8. Scale and Legibility of Access in the South West Corner		
Design as shown in DIP Presentation (August 2021)	Precinct Village and Car Park (September, 2021)	Design Changes
		<p>An open green area, sun filled facing Moore Park East is scaled to facilitate informal recreation and areas for green respite. The space lends itself for integrated terraces underneath rich heritage tree canopy as well as panoramic views to Kippax Lake to create an active gathering space area, people watching and spectating over Moore Park East.</p> <p>The green area is framed with additional timber terraces adjacent to proposed Eat Street and pergolas to encourage informal seating and eating opportunities whilst retaining a more intimate scale and privacy. It also creates opportunities to act as an amphitheatre to create an informal audience or spectator zones for small events.</p>

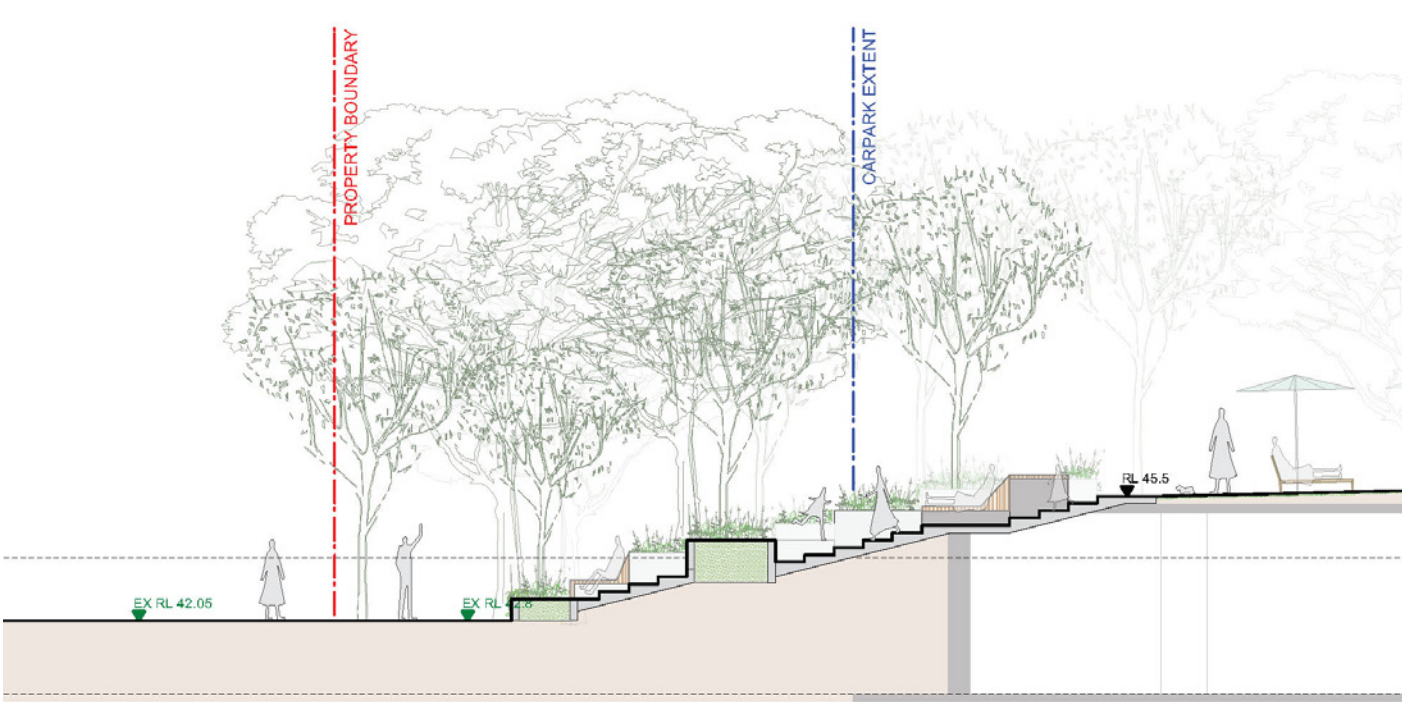


8. Scale and Legibility of Access in the South West Corner

Precinct Village and Car Park (September, 2021)



Render looking towards the South West Corner



Section of Village Terraces



Marrickville Library, Sydney



UTS Alumni Green, Sydney



The Goods Line, Sydney

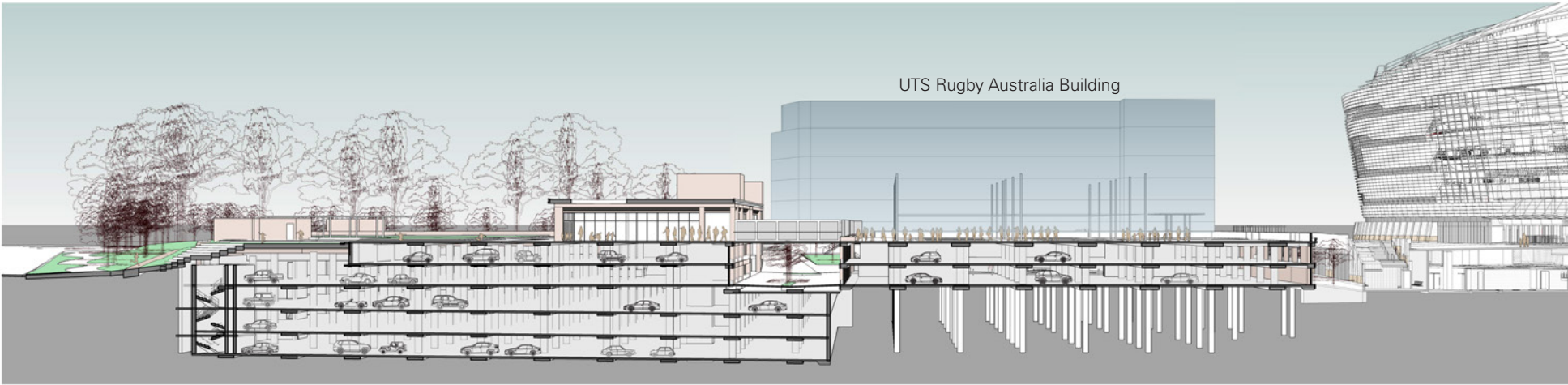


1. Design Amendments

9. Size and Transitions of the Car Park Gully

East - West Perspective Section of Car Park Gully (September, 2021)

Design Changes



The proposed Precinct Village and Car Park integrates seamlessly in terms of mass and form to gently extend the adjacent parkland into the precinct and over the western car park below. The Eastern car park is surrounded on 3 sides by the much larger built form of the UTS/ARU building, the NRL building and the Sydney Football Stadium podium.



West Elevation through Gully (September, 2021)



2. Panel Assessment


The Design Integrity Assessment Panel has reviewed the design changes from the Stage 2 development application to the (as approved and now proposed) design and is of the opinion that the changes proposed are consistent.

The changes do not alter the Panel’s conclusion that the design demonstrates design excellence as outlined in the Competitive Design Alternatives Report. The changes are considered to be consistent with the Panel’s review of the design in relation to the Sydney Local Environmental Plan 2012, the objectives of Better Placed or the project Urban Design Guidelines.

3. Panel Endorsement



Peter Poulet (Chair)



John Perry



Kim Crestani



Tom Gellibrand

