



VIRTUS HERITAGE



SHOP-TOP HOUSING: 494 & 516 MILITARY ROAD,

Mosman, NSW

Preliminary Historical Heritage Assessment

Final / June 2026

Prepared for Abadeen

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Cover design: Cake

Cover image: Virtus Heritage 2026

Document controls

Client:	Abadeen
Local Government Area:	Mosman Municipal Council Local Government Area

Version History

Version	Date	Prepared by	Reviewed by	Comments
1	28 May 2026	Antonella Skepasianos – Primary Author Sarah Carter – Secondary Author Liam Clerke – Figures	Dr Mary-Jean Sutton	Draft for client review
2	10 June 2026	Antonella Skepasianos	Mikhaila Chaplin	Final
3	16 June 2026	Mikhaila Chaplin	Dr Mary-Jean Sutton	Final Additional client comments incorporated

Acknowledgement of Country

Virtus Heritage acknowledges the Traditional Custodians of the land on which this project was undertaken and pays respect to Elders past, present and emerging.

Content Warning

Aboriginal and Torres Strait Islander people are warned that this publication may contain names and images of deceased people, descriptions of traumatic historic events and parts of Country that have been impacted by development.

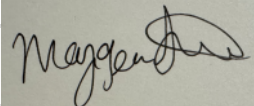
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SEARs Declaration

Declaration		
Qualified Professional Name	Dr Mary-Jean Sutton	
Qualified Professional Qualifications	Bachelor of Arts (Hons.) Archaeology, University of Sydney PhD Archaeology, University of Queensland MAACAI, MEIANZ, FAAS, CEnvP-Heritage, SEAC – Heritage The undersigned declares that this 494–500 and 516 Military Road, Mosman has been prepared in response in response to the following SEARs requirements issued for the project on 20 October 2025 (EIS) for SSD– 93787211:	
SEARs item no.	SEARs Issue and Assessment Requirement	Relevant Section of this Report that addresses SEARs Requirement
23	Environmental Heritage Where there is potential for direct or indirect impacts on environmental heritage, provide a Statement of Heritage Impact and Archaeological Assessment (where required), in accordance with the relevant guidelines.	This PHAA addressed "Archaeological assessment (where required)" in SEARs condition 23, Environmental Heritage only and recommends that no further assessment is required. The SoHI is not addressed in this report, it is being undertaken by Heritage 21 for the proponent.
Signed		
Dated	16 June 2026	

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1. Introduction

This preliminary historical heritage assessment has been prepared by Virtus Heritage on behalf of Abadeen for the proposed mixed-use redevelopment at 494 – 500 & 516 Military Road, Mosman NSW (the project area). The project is being progressed as a State Significant Development (SSD) and is currently at the early concept design stage.

This report has been prepared to provide early, proportionate archaeological advice to support the Secretary's Environmental Assessment Requirements (SEARs) process and broader SSD pathway. The purpose of this assessment is to undertake an initial review of the historical archaeological potential of the project area and identify whether the proposed works are likely to impact potential historical archaeological resources. The assessment also provides preliminary advice regarding whether further historical archaeological assessment or statutory approvals may be required as the project progresses.

1.1 BACKGROUND

The project area is located at 494 – 500 & 516 Military Road, Mosman, within the Mosman Local Government Area (LGA). The site occupies a prominent corner allotment bounded by Military Road, Hale Road, Lang Street and Mackie Lane (Figure 1). The project area currently contains a nursery/garden centre, a mechanic workshop and a community garden.

The proposal is being progressed as a State Significant Development (SSD) under the title *Shop-Top Housing – Military Road, Mosman* (SSD-93787211). The proposal includes demolition of the existing site improvements and construction of a part five-storey and part nine-storey mixed-use development over a two-level basement. The development is understood to comprise ground-floor retail premises with residential shop-top housing above, together with basement parking, servicing and associated public domain works.

The proposed works are expected to involve substantial excavation associated with basement construction, piling, shoring, footing installation and underground services. Preliminary geotechnical investigations indicate excavation into Hawkesbury Sandstone and the likely requirement for significant excavation support measures and vibration management during construction.

1.2 LIMITATIONS

This report provides preliminary historical archaeological advice only and does not constitute a detailed Historical Archaeological Assessment (HAA) or Statement of Heritage Impact (SoHI). The assessment has been prepared to provide early-stage advice regarding potential historical archaeological constraints and whether further assessment may be required as the project progresses.

The assessment is limited to historical archaeological considerations within the mapped project area and surrounding locality. Detailed built heritage and visual impact assessment have not been undertaken as part of this report. Aboriginal cultural heritage has been assessed separately through a Preliminary Aboriginal Cultural Heritage Due Diligence assessment.

The assessment is based on the information available at the time of reporting, including concept design documentation and publicly available historical sources. The site inspection was limited to areas accessible at the time of inspection and no subsurface archaeological investigation was undertaken.

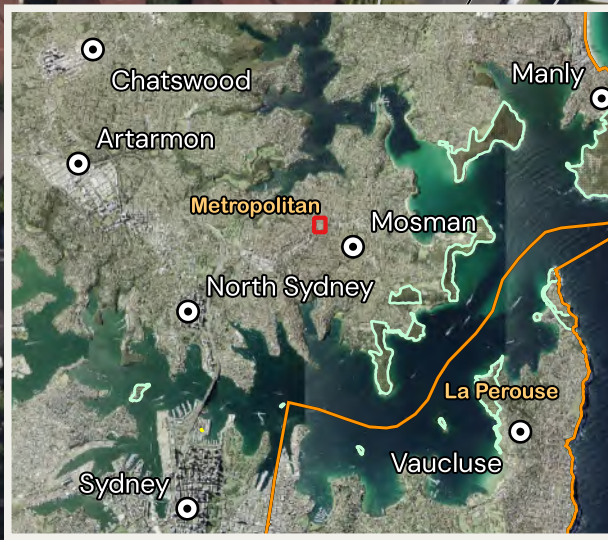
1.3 AUTHORSHIP

This report was authored by Antonella Skepasianos (Senior Heritage Specialist, Virtus Heritage), with assistance from Anya Graubard (Senior Heritage Specialist, Virtus Heritage). The report was reviewed by Dr Mary-Jean Sutton (Director, Virtus Heritage). GIS mapping and figure preparation were undertaken by Liam Clerke (Senior Geomorphologist/GIS, Virtus Heritage).

The site inspection was undertaken by Mikhaila Chaplin (Senior Archaeologist, Virtus Heritage) on 18 May 2026.

Project information, architectural drawings and development details referenced throughout this report were provided by Adrian Giardina of Abadeen. Relevant concept design information and façade drawings prepared for the proposed redevelopment were also reviewed as part of this assessment.

Virtus Heritage would like to acknowledge the assistance of Adrian Giardina and Abadeen in facilitating access to project information and the site inspection for this assessment.



851 - 494 & 516 Military Road
Mosman, NSW
Figure 1.1. Project Area and Locality

Author: Liam Clerke
Date: 28/05/2026
Scale: 1:1020 @ A4
Datum: GDA2020 / MGA zone 56
Source: NSW Spatial Services

Legend

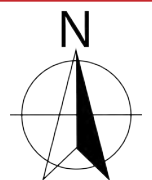
- Project Area
- NSW Lot Boundaries
- Local Aboriginal Land Council
- Locality

NSW Roads

- Minor Road
- Local Road

NPWS Reserves

- National Park



0 10 20 30 m

2. Statutory Framework

In NSW, heritage protection and management for historical archaeological sites, buildings, relics, and culturally significant landscapes are governed primarily by the NSW Heritage Act 1977 (Heritage Act), and the Environmental Planning and Assessment Act 1979 (EP&A Act). These acts establish the framework for identifying, assessing, and conserving heritage places and objects of local or State significance, as well as managing heritage impacts from development projects. Additionally, heritage protection can be further reinforced under the Commonwealth Environment Protection and Biodiversity Conservation Act 1999 (EPBC Act), especially for sites with nationally significant heritage values or actions involving Commonwealth land or agencies.

Please see Appendix A for more information on legislation and guidelines.

2.1 HERITAGE LISTINGS

The following heritage registers were searched on 13 May 2026.

- World Heritage List (WHL)
- National Heritage List (NHL)
- Commonwealth Heritage list (CHL)
- State Heritage Inventory (SHI) Sections 1–3, which includes:
 - State Heritage Register (SHR)
 - Aboriginal Places
 - Heritage and Conservation Register (s170 register)
 - Schedule 5 of the Mosman Local Environmental Plan 2012 (2011 EPI 647)
- The Register of the National Estate (RNE), which is now static but provides useful information.

A 300 m search radius was adopted to identify heritage items, heritage conservation areas and archaeological listings with potential relevance to the preliminary archaeological assessment, having regard to the scale of the proposed redevelopment, the surrounding urban context and the preliminary nature of this assessment.

The heritage register searches identified multiple heritage items and heritage conservation areas in proximity to the project area at Federal, State and local levels. The project area is located immediately south of the Lang Street Heritage Conservation Area (C6), with the conservation area boundary situated approximately 5 m north of the project area. The Lang Street Divided Road (I394), which forms part of the conservation area, is located approximately 200 m north of the project area.

At the Federal level, the project area falls within the nominated extent of the Sydney Cultural Crescent Rock Art Site listed on the National Heritage List as a nominated place. The Sydney Harbour Landscape Area (RNE ID 14308) is also located approximately 28 m south-east of the project area. Aboriginal cultural heritage values are addressed separately through the Preliminary Aboriginal Cultural Heritage Due Diligence assessment.

At the State level, Alma House (SHR ID 00070) is located approximately 258 m south of the project area. Two Section 170 listed items are located within the surrounding locality, including Electricity Substation No. 149 (s170 Ausgrid ID 3430494), located approximately 53 m south-east of the project area, and Middle Harbour Public School – Building BOOK (School Infrastructure s170 ID 5065613), located approximately 122 m north-west of the project area.

At the local level, the Mosman LEP identifies several heritage items and conservation areas within proximity to the project area, including the Montague Road Heritage Conservation Area (CA04), Glover and Nathan's Estates Heritage Conservation Area (C12), Electricity Substation No. 149 (I136), 'Aotea Roa' (I95), 'The Cottage' Arts Education Centre (I110), 'Oreel' (I111), Sacred Heart Church (I154) and several individually listed residential properties.

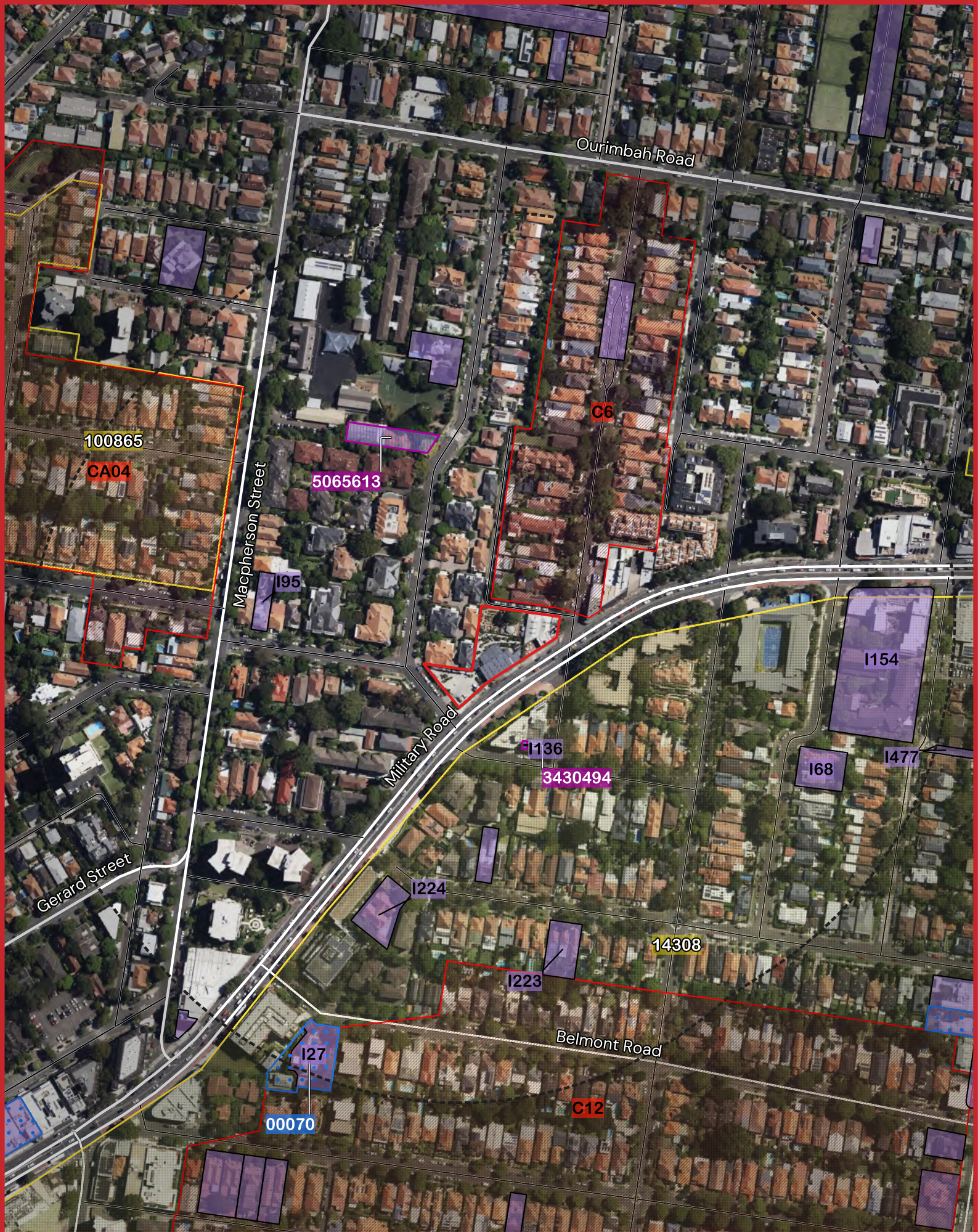
The heritage register searches indicate the project area is located within a historically sensitive urban context containing numerous heritage items and conservation areas. The results of the heritage register searches are summarised in Table 2.1. All register searches include listed heritage values, which are presented in

Figure 2.1, illustrating their spatial relevance to the project area.

Table 2.1. Heritage register search.

Jurisdiction	Heritage Register	Place ID #	Listing	Proximity to project area
Federal	World Heritage List	-	-	-
	Commonwealth Heritage Register	-	-	-
	National Heritage Register	106369	Sydney Cultural Crescent Rock Art Site (nominated place)	Project area is within this nominated place
	Register of the National Estate (non-statutory)	14308	Sydney Harbour Landscape Area	~28 m south-east
		100865	Montague Road Conservation Area	~169 m west
State	State Heritage Register	00070	Alma House	~258 m south
	S170 Register – Ausgrid	3430494	Electricity Substation No. 149	~53 m south-east
	S170 Register – School Infrastructure	5065613	Middle Harbour Public School – Building BOOK	~122 m north-west
	Aboriginal Places	-	-	-
Local	Mosman Local Environmental Plan 2012	C6	Lang Street Heritage Conservation Area	~5 m north
		CA04	Montague Road Heritage Conservation Area	~165 m west
		C12	Glover and Nathan's Estates Heritage Conservation Area	~188 m south
		I136	Electricity Substation No. 149	~53 m south-east
		I222	House (divided into 2 dwellings)	~87 m south
		I95	'Aotea Roa', House	~121 m west
		I110	'The Cottage', Arts Education Centre	~122 m north-west
		I111	'Oreel' (14) House with stone fence and garden Stone fence (12A)	~166 m north-west
		I394	Land Street Divided Road	~200 m north

Jurisdiction	Heritage Register	Place ID #	Listing	Proximity to project area
		I154	Sacred Heart Church	~216 m east
		I68	Group of 2 houses	~198 m east
		I223	Group of 3 houses	~211 m south
		I224	'Clumba Court'. Flats	~133 m south
		I27	'Alma', Former house, now offices	~258 m south



851 - 494 & 516 Military Road
Mosman, NSW
Figure 2.1. Heritage Register Searches

Author: Liam Clerke
Date: 28/05/2026
Scale: 1:3538 @ A4
Datum: GDA2020 / MGA zone 56
Source: NSW Spatial Services

Legend

- Project Area
- Project Area - 300m Buffer

NSW Roads

- Minor Road
- Local Road

- State Heritage Register Curtilage
- Section 170 Items
- Register of the National Estate

EPI Heritage Items

- Conservation Area - General
- Item - General



Maps and figures contained within this document may be based on third party data, may not be to scale and are intended for use as a guide only. Virtus Heritage does not warrant the accuracy of such maps or figures.

3. Historical summary

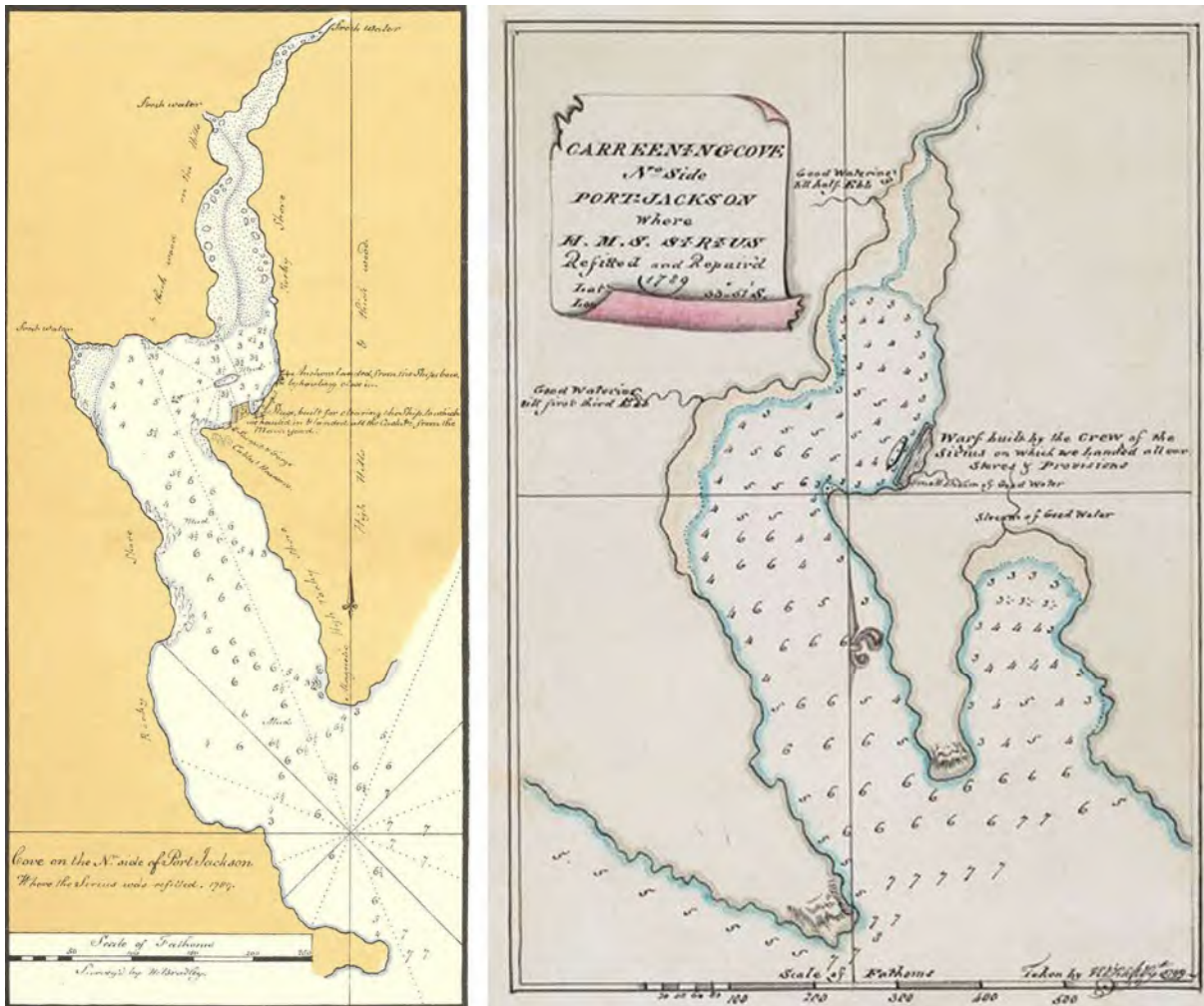
3.1 ABORIGINAL OCCUPATION AND EARLY EUROPEAN SETTLEMENT

Prior to European colonisation, the Mosman peninsula formed part of the traditional lands of the Borogegal and Cammeraygal (Gamaraigal) peoples, who maintained long-standing cultural, social and economic connections with the Sydney Harbour foreshore and surrounding ridgelines. Archaeological evidence of Aboriginal occupation throughout the Mosman locality includes rock shelters, middens, rock engravings and axe grinding grooves, reflecting thousands of years of occupation and resource use associated with the harbour environment (Mosman Council 2026a). Aboriginal cultural heritage values associated with the broader locality are addressed separately within the accompanying Aboriginal heritage assessment.

European settlement within Mosman commenced during the early nineteenth century following the expansion of colonial Sydney across the northern harbour foreshore. The Mosman peninsula remained relatively isolated and sparsely occupied during the early colonial period due to its rugged topography and reliance on maritime transport routes connecting the harbour foreshores with Sydney Cove.

One of the earliest documented European activities within the Mosman locality occurred in 1789 when HMS *Sirius*, flagship of the First Fleet, was careened and repaired within what later became known as Sirius Cove (Plate 3.1). Historical charts prepared during the repair works depict the sheltered harbour environment and associated temporary wharf and repair infrastructure established within the cove (Mosman Historical Society 2020).

Early European settlement within the Mosman locality remained limited during the early nineteenth century. Thomas O'Neil is recorded as one of the earliest settlers in the area, establishing a small farm at Hunters Bay in 1814 (Mosman Memories 2010). In 1831 Archibald Mosman received a land grant at Great Sirius Cove, now Mosman Bay, where he established a whaling station which later gave the suburb its name (Mosman Council 2026a; Mosman Memories 2010). Subsequent subdivision and promotion of the district occurred following the acquisition of large holdings by Richard Hayes Harnett Snr in 1859. Harnett actively promoted Mosman as a recreational and residential destination, subsidising transport services and encouraging visitors to the area's beaches, waterfalls and harbour foreshores (Mosman Memories 2010).



Source: Mosman Historical Society 2020 (<https://mosmanhistoricalsociety.org.au/blogs/mhs-blog/hms-sirius-the-mosman-connection>)

Plate 3.1. Historical charts depicting Carreening Cove (later Sirius Cove), where HMS Sirius underwent repairs in 1789

3.2 DEVELOPMENT OF MOSMAN AND MILITARY ROAD

The development of Mosman accelerated during the late nineteenth century as transport infrastructure improved and large estates were progressively subdivided for residential occupation. Early roads in Mosman developed incrementally in response to settlement patterns rather than formal town planning principles, resulting in the irregular and steep road alignments that continue to characterise parts of the suburb today (Mosman Memories 2010).

Military Road, which forms the primary frontage to the project area, was established in 1871 as a strategic route connecting Milsons Point with the military fortifications at Middle Head, Georges Head and Bradleys Head (Mosman Memories 2010). The road followed the ridge line across the peninsula and subsequently became the principal transport and commercial corridor through Mosman. The extension of tram services from North Sydney to Mosman in 1893 significantly improved accessibility and stimulated further subdivision and urban development throughout the locality (Mosman Memories 2010). The tram corridor reinforced the commercial role of Military Road, resulting in the concentration of shops, services and professional premises along the ridgeline corridor throughout the late nineteenth and twentieth centuries.

Mosman formally became the Municipality of the Borough of Mosman in 1893, coinciding with increasing residential subdivision and urban expansion associated with Sydney's Federation-era growth (Mosman

Memories 2010). The suburb experienced substantial residential development during the Federation and inter-war periods, resulting in the construction of a diverse range of architectural styles including Federation Queen Anne, Federation Arts and Crafts, Californian Bungalow and Inter-war revival styles, many of which continue to define the heritage character of the locality today (Staas 1999).

During the early twentieth century, Military Road developed as Mosman's principal commercial spine, accommodating shops, medical practices, service industries and public transport infrastructure (Plate 3.2). Historical accounts indicate the corridor historically contained a highly mixed and utilitarian character, including grocery stores, timber yards, bakeries, service stations, nurseries, workshops and professional rooms associated with residential premises fronting the roadway (Holmes 2010).

Road infrastructure within Mosman continued to evolve throughout the twentieth century. Large-scale road widening, resurfacing and concrete paving works were undertaken during the 1920s and 1930s under Mosman Council engineer Jack H. Tonkin, who pioneered the use of concrete road construction within the municipality (Mosman Memories 2010). These works substantially altered portions of the original road corridor and adjacent allotments along Military Road. The progressive widening and commercial intensification of Military Road throughout the twentieth century contributed to substantial modification of original allotment boundaries, landforms and subsurface conditions along portions of the corridor.



Source: Mosman Memories 2010. (<https://mosmanmemories.net/about/15/brief-history-of-mosman-and-its-streets>)

Plate 3.2 Spit Junction, 1921, looking west along Military Road towards Cremorne with Spit Road to the right and Vista Street on the left.

3.3 HISTORICAL DEVELOPMENT OF THE PROJECT AREA

The project area, comprising 494 and 516 Military Road, is located within Portion 116 of the Parish of Willoughby, County of Cumberland. Crown Plan 460–690, dated to 1884, provides the earliest identified cadastral depiction of Portion 116 within the locality (Plate 3.4). The Crown Plan illustrates the early subdivision framework surrounding Military Road and confirms the project area formed part of a larger 20 acre allotment during the late nineteenth century.

Historical parish and cadastral mapping identifies Portion 116 as a 20 acre allotment associated with "C. Robertson" and later "Chas (Charles) Robertson" (Plate 3.4; Plate 3.5). These maps demonstrate the broader

pattern of large landholdings and progressive subdivision occurring throughout Mosman during the late nineteenth and early twentieth centuries as the suburb expanded around the developing Military Road corridor.

The establishment of Military Road and associated tram infrastructure significantly influenced the urbanisation of the surrounding locality during the late nineteenth and early twentieth centuries. As Mosman developed into an increasingly suburban and commercially active municipality, Military Road evolved into the primary transport and commercial spine servicing the peninsula. Historical accounts indicate the corridor accommodated a diverse range of commercial, industrial and service-related land uses during the early and mid-twentieth century, including workshops, service stations, shops, medical practices and vehicle-related businesses (Holmes 2010). The project area formed part of this broader commercial evolution of the Military Road ridgeline corridor, which developed as Mosman's principal transport and commercial spine throughout the twentieth century.

Consistent with the broader commercialisation of Military Road during the early twentieth century, historical newspaper records indicate that 516 Military Road became associated with automotive and service-related land uses during the inter-war period. Newspaper notices published in 1935 reference tenders being called for the "erection [of a] modern drive-in service station" at 516 Military Road, Mosman (Plate 3.6). This notice reflects the increasing importance of motor vehicle infrastructure and roadside commercial development along the Military Road corridor during the early twentieth century. Establishment of the service station and associated automotive infrastructure would likely have involved localised excavation and ground disturbance associated with underground fuel infrastructure, hardstand construction and associated servicing works.

Subsequent newspaper advertisements published in *The Sydney Morning Herald* on 17 April 1937 reference "Mosman Motors" operating from 516 Military Road, Mosman, advertising both Chevrolet and Essex motor vehicles (Plate 3.7). These advertisements indicate that vehicle sales and/or servicing activities had become established at the site by at least 1937 and demonstrate the continued commercial and transport-oriented character of the project area during the inter-war period.

Contemporary development application records indicate the project area has continued to undergo substantial redevelopment and modification throughout the late twentieth and early twenty-first centuries. At 494 Military Road, development consent was granted in 2014 for construction of a single-storey retail premises for use as a garden centre, including associated car parking, landscaping and signage (Mosman Council 2014). A subsequent modification application approved changes to retaining walls, awnings and pedestrian access arrangements in 2015 (Mosman Council 2015).

At 516 Military Road, development applications lodged during the early 2000s proposed redevelopment of the site incorporating commercial premises, a drive-through liquor outlet, office development and associated car parking while retaining an existing muffler repair workshop; however, these applications were either withdrawn or refused (Mosman Council 2002; Mosman Council 2003; Mosman Council 2004).



The approximate location of the Project Area is indicated in red

Source: Hazlett Information Services

Plate 3.3. Extract from Crown Plan 460-690 dated 1884 showing Portion 116 and surrounding allotments within the Parish of Willoughby.



The approximate location of the Project Area Is Indicated in red

Source: HLRV

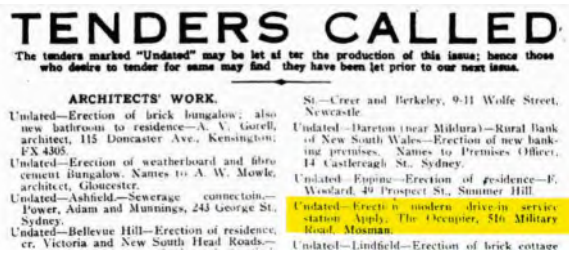
Plate 3.4. Extract from the 1899 Parish of Willoughby map showing Portion 116 associated with "C. Robertson".



The approximate location of the Project Area Is Indicated in red

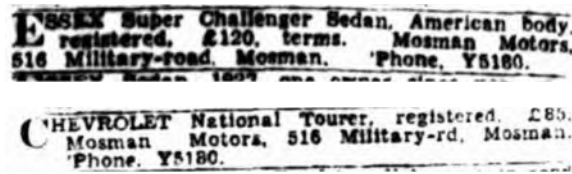
Source: HLRV

Plate 3.5. Extract from the 1971 Land Titles Office charting map showing Portion 116 associated with "Chas Robertson".



Source: Construction and Real Estate Journal 1935

Plate 3.6. Newspaper notice referencing tenders for erection of a modern drive-in service station at 516 Military Road, Mosman



Source: The Sydney Morning Herald 1937

Plate 3.7. Newspaper advertisements for "Mosman Motors" at 516 Military Road, Mosman, advertising Chevrolet and Essex motor vehicles

3.4 HISTORICAL AERIAL ANALYSIS

Historical aerial imagery (Table 3.1) demonstrates that the project area had already been substantially developed by at least 1943, with a commercial or service-related building complex established fronting Military Road together with associated hardstand and ancillary structures. The aerial sequence indicates that the project area formed part of the broader commercial evolution of the Military Road corridor during the twentieth century, reflecting the increasing concentration of transport, automotive and service-related land uses throughout Mosman.

By 1965, the earlier building complex visible in 1943 had been substantially altered, demolished or reconfigured, with the project area appearing consistent with automotive or service-related commercial use incorporating extensive hardstand surfaces, vehicle access arrangements and dispersed built structures across much of the site. Historical newspaper records further support this transition, indicating the establishment of a modern drive-in service station and vehicle-related commercial activities within the project area during the inter-war period. The extent of building footprints, hardstand establishment, vehicle circulation areas and probable subsurface servicing associated with these commercial and automotive land uses implies that substantial excavation and landform modification likely occurred across much of the project area during the twentieth century.

Historical aerial imagery from 1986 indicates the project area remained intensively developed for automotive or commercial-related purposes, including workshop or garage-style structures, extensive sealed surfaces and vehicle circulation areas. The aerial sequence demonstrates ongoing modification and redevelopment throughout the twentieth century, including alterations to buildings, expansion of hardstand areas and removal of earlier structures.

By 2024, the former automotive and service-related development visible in earlier aerials had largely been removed or repurposed, with the project area accommodating a garden centre or nursery use incorporating landscaped planting beds, open-air nursery infrastructure and associated hardstand areas. Contemporary development application records additionally indicate continued redevelopment and site modification throughout the late twentieth and early twenty-first centuries, including construction works, retaining walls, landscaping, vehicle access arrangements and associated servicing infrastructure.

The historical mapping, aerial imagery and development history indicate the project area has undergone sustained redevelopment and site modification throughout the twentieth and early twenty-first centuries associated with roadside commercial, automotive and service-related land uses. Historical evidence indicates the site functioned predominantly as a highly modified commercial corridor associated with the twentieth-century development of Military Road, rather than land uses typically associated with substantial relic-bearing archaeological deposits under the Heritage Act 1977.

Table 3.1. Historical aerials of the project area.

Plate	Aerial photograph (project area outlined in red)	Description
Plate 3.8. 1943 aerial imagery		1943 • The project area is already developed by 1943. • A large building complex is visible within the central-eastern portion of the project area fronting Military Road. • Open or hardstand areas are visible adjacent to the building complex with access from Military Road. • Smaller structures are visible within the south-western portion of the project area. • Residential development is well established surrounding the project area. • The layout visible in 1943 is consistent with commercial or service-related land use fronting Military Road.
Plate 3.9. 1965 aerial imagery		1965 • The project area has been substantially redeveloped since 1943. • The large building complex visible in 1943 appears to have been demolished, altered or reconfigured by this period. • Development within the project area appears consistent with automotive or service-related commercial use, including extensive hardstand areas and vehicle access from Military Road. • Buildings and structures are established across much of the project area by this period. • Earlier structures visible within the south-western portion of the project area appear to have been removed.

*Plate 3.10. 1986
aerial imagery*



1986

- The project area remains intensively developed for commercial or automotive-related use.
- The service station-style development fronting Military Road appears more established by this period, with alterations or extensions visible.
- Buildings within the northern and eastern portions of the project area appear to have been altered or extended since 1965.
- A long rectangular structure is visible along the northern boundary of the project area, consistent with workshop, garage or covered storage-related use.
- The project area remains predominantly hardstand with vehicle access and parking areas throughout the site.
- The south-western portion of the project area appears open and sealed by this period.

*Plate 3.11. 2024
aerial imagery*



2024

- The project area has been substantially reconfigured since 1986.
- Earlier automotive or service-related commercial development visible in previous aerials has largely been removed or repurposed.
- A garden centre or nursery use is established across the central and northern portions of the project area.
- Landscaped garden beds, planting areas and associated open-air nursery / community garden are visible throughout the site.
- The building within the north-eastern portion of the project area remains.
- The south-western portion of the project area remains predominantly sealed and utilised for vehicle access, parking or hardstand purposes.

4. Site Inspection

A site inspection of the project area was undertaken by Mikhaila Chaplin (Virtus Heritage, Senior Archaeologist) on Monday 18 May 2026.

The project area comprises three properties currently occupied by a nursery/garden centre, automotive mechanic, and community garden, located on Military Road in Mosman, NSW. All outdoor areas within each property were inspected.

The portion of the project area occupied by the nursery/garden centre (street address 500 Military Road) comprises a large shed-type commercial structure, surrounded by an open-air carpark covered with concrete hardstand and enclosed by fencing (Plate 4.1). Modification of the ground surface level was apparent, including in the slope of the northern side of the carpark, likely implemented to facilitate drainage of the hardstand surface (Plate 4.2). The introduction of the concrete itself would likely also have required levelling of the site. A stormwater drain was also observed within the carpark, and it can be assumed that other sub-surface utilities and services are also present within the property.

The portion of the project area occupied by the automotive mechanic (street address 516 Military Road) comprises a large structure with an awning adjacent to its southeastern side that covers the majority of the property, and a hardstand surface covering the remainder of the property along Military Road (Plate 4.3 and Plate 4.4). The majority of floorspace within the building is in use as a garage, with a small portion utilised as a reception/administration office. Modification of the ground surface within this portion of the project area has likely occurred in association with the introduction of the hardstand surface, construction of the building, and installation of services and utilities. Additionally, this property was previously the location of a service station which included underground fuel storage tanks, which remain in situ.

The portion of the project area occupied by Mackie Lane Community Garden (street address Mackie Lane) comprises a mix of hardstand surfaces, grass, raised garden beds, and small structures including toolsheds and a gazebo, although the majority of the ground surface within the area is covered by a dense layer of mulch (Plate 4.5). In some locations where the mulch had been disturbed, a black, loamy imported topsoil was visible (Plate 4.6). Modification of the ground surface through levelling has likely occurred within this property, with fills including imported topsoil also having been imported. Localised disturbance associated with the large eucalypt tree (planted after 1994) is also likely to have occurred.

No exposures of the natural soils were visible in any part of the project area due to the complete coverage of the ground surface by hardstand, and, within the community garden, a combination of hardstand, grass, and mulch. Whilst some soils were visible in isolated instances within the community garden, these comprised imported topsoil only. Existing disturbances were pervasive throughout the project area, and included ground surface modification, including levelling, for the introduction of the hardstand surfaces, the construction of commercial buildings, and existing sub-surface services and utilities. These activities are likely, at a minimum, to have removed or significantly truncated the relatively shallow natural A-horizon soils within the project area.



Plate 4.1: Nursery/garden centre and adjacent hardstand carparking area, facing ~northeast.



Plate 4.2: Nursery/garden centre carparking area, facing ~north.



Plate 4.3: Automotive mechanic building and adjacent hardstand surfaces, facing ~west.



Plate 4.4: Automotive mechanic building and adjacent hardstand surfaces, facing ~north-northeast.



Plate 4.5: Mackie Lane Community Garden, view ~west including gazebo.



Plate 4.6: Area of greater visibility within Mackie Lane Community Garden showing black imported soil

5. Archaeological Potential

5.1 GEOTECHNICAL INVESTIGATIONS

Geotechnical investigations were undertaken within the project area in 2025 by NEO Consulting and included drilling of three boreholes within the eastern portion of the project area at 516 Military Road (NEO Consulting 2025).

Two boreholes located outside the principal building footprint identified a concrete slab approximately 20 cm thick overlying between 5 cm and 20 cm of sandy fill, underlain by sandstone bedrock at depths between approximately 25 cm and 40 cm below ground level. A third borehole, located within the building footprint, identified sandy clay fill beneath the concrete slab, with the borehole terminating within fill material at 1 m depth (NEO Consulting 2025).

The results of the geotechnical investigations indicate substantial modification of the original soil profile within the tested areas, including removal or truncation of natural soil horizons associated with earlier commercial redevelopment, hardstand construction and subsurface servicing works.

Historical plans and contamination investigation mapping associated with the former service station use at 516 Military Road additionally identify underground fuel storage tanks, oil sumps and associated subsurface infrastructure remaining within the project area (Plate 5.1). Installation of this infrastructure would have required localised excavation and subsurface disturbance associated with tank emplacement, fuel lines and associated servicing works.



Source: NEO Consulting 2025

Plate 5.1. Contamination investigation and inferred underground fuel storage tank locations within 516 Military Road

5.2 HISTORICAL ARCHAEOLOGICAL POTENTIAL

The historical background research, aerial imagery review, site inspection observations and available geotechnical information collectively indicate that the project area has undergone substantial and sustained modification associated with twentieth-century commercial redevelopment fronting Military Road.

Historical mapping and documentary evidence demonstrate that the project area formed part of the broader commercial and transport-oriented evolution of the Military Road corridor during the late nineteenth and twentieth centuries. By at least the 1930s, the site was associated with automotive and service-related land uses, including establishment of a modern drive-in service station and operation of “Mosman Motors” from 516 Military Road. Historical aerial imagery further demonstrates that the project area had already been substantially developed by 1943 and remained intensively occupied throughout the twentieth century, with successive phases of redevelopment, demolition, hardstand establishment, building alteration and vehicle-related commercial use evident across the site.

The site inspection confirmed that the project area is extensively modified and largely covered by concrete hardstand, buildings, landscaping, imported soils and associated commercial infrastructure. Existing disturbances identified across the project area include levelling, hardstand construction, building foundations, retaining walls, underground services, stormwater infrastructure and landscaping works. The former service station component of the site additionally retains underground fuel storage tanks, oil sumps and associated subsurface infrastructure.

The geotechnical investigations undertaken by NEO Consulting further indicate that natural soil horizons within tested areas have been substantially removed or truncated. Borehole results identified shallow sandstone bedrock underlying concrete slabs and fill materials within portions of the project area, confirming previous excavation and significant ground disturbance associated with commercial redevelopment and servicing works.

The historical evidence indicates the project area functioned predominantly as a highly modified roadside commercial allotment associated with automotive, service-related and retail land uses throughout the twentieth century, rather than land uses typically associated with substantial relic-bearing archaeological deposits under the Heritage Act 1977. Accordingly, the project area is considered to demonstrate *low* historical archaeological potential.

6. Conclusions and recommendations

Historical research, aerial imagery review, site inspection observations and geotechnical investigations indicate that the project area has undergone extensive redevelopment and ground disturbance associated with long-term commercial, automotive and service-related land uses fronting Military Road.

The project area is presently characterised by commercial buildings, concrete hardstand, landscaping, underground services and former service station infrastructure, with geotechnical investigations confirming substantial truncation and removal of natural soil profiles within tested areas. Historical aerial imagery further demonstrates successive phases of redevelopment, demolition and reconfiguration throughout the twentieth century.

The identified historical land uses and extent of redevelopment are not considered likely to have generated or retained a substantial archaeological resource capable of meeting the threshold for relic significance under the Heritage Act 1977. Accordingly, the project area is assessed as demonstrating *low* historical archaeological potential.

Based on the findings of this assessment, the following recommendations are made:

- No further historical archaeological assessment is considered warranted for the project area at this stage.
- The proposed redevelopment is not considered likely to impact significant historical archaeological relics as defined under the *Heritage Act 1977*.
- No historical archaeological excavation permit under Sections 139 or 140 of the Heritage Act 1977 is considered likely to be required for the proposed works.
- Should unexpected historical archaeological remains be encountered during works, works in the immediate area should cease and a suitably qualified historical archaeologist should be consulted to provide further advice regarding the significance and management of the finds.

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Appendix A Statutory framework

A.1. LEGISLATION

NSW Heritage Act 1977

The Heritage Act provides a legislative framework to promote and protect heritage places, items, and relics throughout NSW. Administered by the Heritage Council of NSW, the Act supports the identification, conservation, and appropriate management of heritage assets.

i. State Heritage Register

Under the Act, heritage items of exceptional importance to the State are listed on the State Heritage Register (SHR). Inclusion on the SHR means that these heritage items cannot be damaged, altered or developed without approval from the Heritage Council of NSW, under Section 59 of the Act.

ii. Archaeology and relics

The Heritage Act also provides protection to archaeological 'relics', regardless of their listing status. It applies to all NSW land that is not listed on the SHR. Section 4(1) of the Heritage Act (as amended 2009) defines 'relic' as follows:

A "relic" means any deposit, artefact, object or material evidence that:

- (a) relates to the settlement of the area that comprises New South Wales, not being Aboriginal settlement, and
- (b) is of State or local heritage significance.

Section 139(1) of the Heritage Act states that:

A person must not disturb or excavate any land knowingly or having reasonable cause to suspect that the disturbance or excavation will or is likely to result in a relic being discovered, exposed, damaged or destroyed unless the disturbance or excavation is carried out in accordance with an excavation permit.

Under Section 146 of the Act, individuals must notify the Heritage Council of NSW within a reasonable timeframe if an unanticipated relic is discovered. Any suspected relic cannot be removed without obtaining the necessary approval. The Act distinguishes between 'works,' which encompass historical infrastructure, and archaeological 'relics.' While 'works' may be buried and possess archaeological characteristics, their exposure does not require reporting under the Heritage Act unless they are determined to hold demonstrable heritage significance.

iii. State Government Heritage and Conservation (s170) Registers

Under Section 170 of the Heritage Act, NSW State government agencies are required to maintain a Heritage and Conservation Register. This section ensures that government agencies actively manage listed heritage items and notify the Heritage Council of NSW prior to changes such as removal, transfer, or demolition.

NSW Environmental Planning and Assessment Act 1979

The EP&A Act provides the primary legislative framework for planning and assessing environmental impacts, including heritage considerations, within the development process in NSW. This Act ensures environmental, and heritage impacts are evaluated and appropriately managed through local planning instruments, such as

Local Environmental Plans (LEPs) and Development Control Plans (DCPs). Development applications (DAs) or environmental assessments under Division 5.1 of the Act are required to demonstrate that heritage impacts have been appropriately addressed.

i. Waverley Local Environmental Plan 2012

Part 5, Section 5.10 of the *Waverley Local Environmental Plan 2012* (WLEP 2012) addresses the conservation of heritage significance within the Waverley Council LGA. The objectives of the NLEP 2012 in relation to heritage are:

- a) to conserve the environmental heritage of the Waverley LGA,
- b) to conserve the heritage significance of heritage items and heritage conservation areas, including associated fabric, settings and views
- c) to conserve archaeological sites
- d) to conserve Aboriginal objects and Aboriginal places of heritage significance.

To achieve these objectives, development consent is required to demolish, move, alter, disturb or excavate a heritage item, an Aboriginal object or a building, work, relic or tree within a heritage conservation area. Schedule 5 of the NLEP 2012 provides a list of heritage items, conservation areas and archaeological sites within the Waverley LGA.

Commonwealth Environment Protection and Biodiversity Conservation Act 1999

At the Commonwealth level, the EPBC Act offers protection for natural, Indigenous, and historic heritage values. This Act mandates assessments for actions impacting places listed on the:

- World Heritage List (WHL) – places inscribed on the United Nations Educational, Scientific and Cultural Organisation (UNESCO) World Heritage List.
- National Heritage List (NHL) – nationally significant heritage places.
- Commonwealth Heritage List (CHL) – heritage items owned or managed by the Commonwealth or its agencies.

Under the EPBC Act, any activity on Commonwealth land (Section 26) or action performed by a Commonwealth agency (Section 28) must undergo an assessment to evaluate potential environmental impacts. These impacts may involve heritage places listed on formal registers (WHL, NHL, CHL, or state-based registers) or heritage values identified independently by Commonwealth agencies.

Furthermore, the Act mandates assessment of actions that could significantly affect Matters of National Environmental Significance (MNES), including heritage sites listed on the WHL or NHL. Actions considered likely to have a significant impact on MNES are classified as 'controlled actions' and require approval from the Commonwealth Minister for the Environment. Projects that may significantly impact MNES must be referred to the Commonwealth Department of Climate Change, Energy, the Environment and Water (DCCEEW) for a determination on whether the project qualifies as a controlled action and thus necessitates further assessment and approval under the EPBC Act.

A.2. GUIDELINES

This historical heritage constraints assessment has been conducted in accordance with applicable government heritage, guidelines, policies and assessment criteria. The report and associated site inspection were guided by principles outlined in The Australian International Council on Monuments and Sites, Charter

for Places of Cultural Significance (also known as the Burra Charter, Australia ICOMOS 2013) and informed by methodologies from the New South Wales (NSW) Heritage Manual (Heritage Office 1996 with regular additions).

The Burra Charter (ICOMOS (Australia) 2013) provides best practice standards for custodians involved in advising on or managing places of cultural significance. Cultural significance is broadly defined by the Charter as encompassing aesthetic, historical, scientific, social, and spiritual values relevant across generations (Australia ICOMOS 2013, Article 1.2). The Charter advocates for conservation actions tailored specifically to the identified significance, recommending minimal intervention to ensure heritage values are preserved effectively.

Structured into five key sections—definitions, conservation principles, processes, conservation practices, and implementation—the Burra Charter outlines a structured approach to heritage management. Its key principles include:

- Conservation is based on the identified significance
- A cautious and minimal intervention approach, undertaking only necessary changes
- Maintenance is a fundamental conservation measure.

The full text of the Burra Charter is accessible online at the ICOMOS Australia website: <http://icomos.org/australia>

Appendix B Concept Plans

Parking Provision

Parking Rates

Residential				
Type	Units	Rate	Req	Prop
1 Bed	32	1	32.0	32
2 Bed	56	1	56.0	64
3 Bed	12	2	24.0	24
Adaptable	100	20%	20.0	20
Visitor	100	1 per 4 units	25.0	17
Carwash Bay	100	1 per 12 units	8.3	9
Total			137	137

*Adaptable parking/ carwash bay numbers are excluded in the total

B2	
Level	Prop
UB	13
B1	52
B2	72
	137

Non-Residential				
Area	Rate	Req	Prop	
Retail	776	1 per 74m ²	10.5	10
Total:			147	

Bicycle				
Units	Rate	Req	Prop	
Bicycle	103	1 per 4 units	25.8	26
Visitor				
Motorbikes	103	1 per 25 parking	4.1	5



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P8	27/05/2026	KL/KG	KL/MR	Issue for Consultants

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- Refer to site survey for all information relating to existing site conditions.
- All Boundary information to be confirmed by registered surveyor before commencing works on site.
- Refer to Arborist Report and Landscape Documentation for all information relating to trees and their retention/removal, and all landscape works.
- Drawings to be read in conjunction with all Specifications and Schedules; all specialist consultant documentation; BASIX, NCC/BS, Section J Certificates.
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PRELIMINARY

Drawing Name
Plan - Basement 2

Drawing Scale
Drawing No.
DA200

@ A1
Revision
P8

Parking Provision

Parking Rates

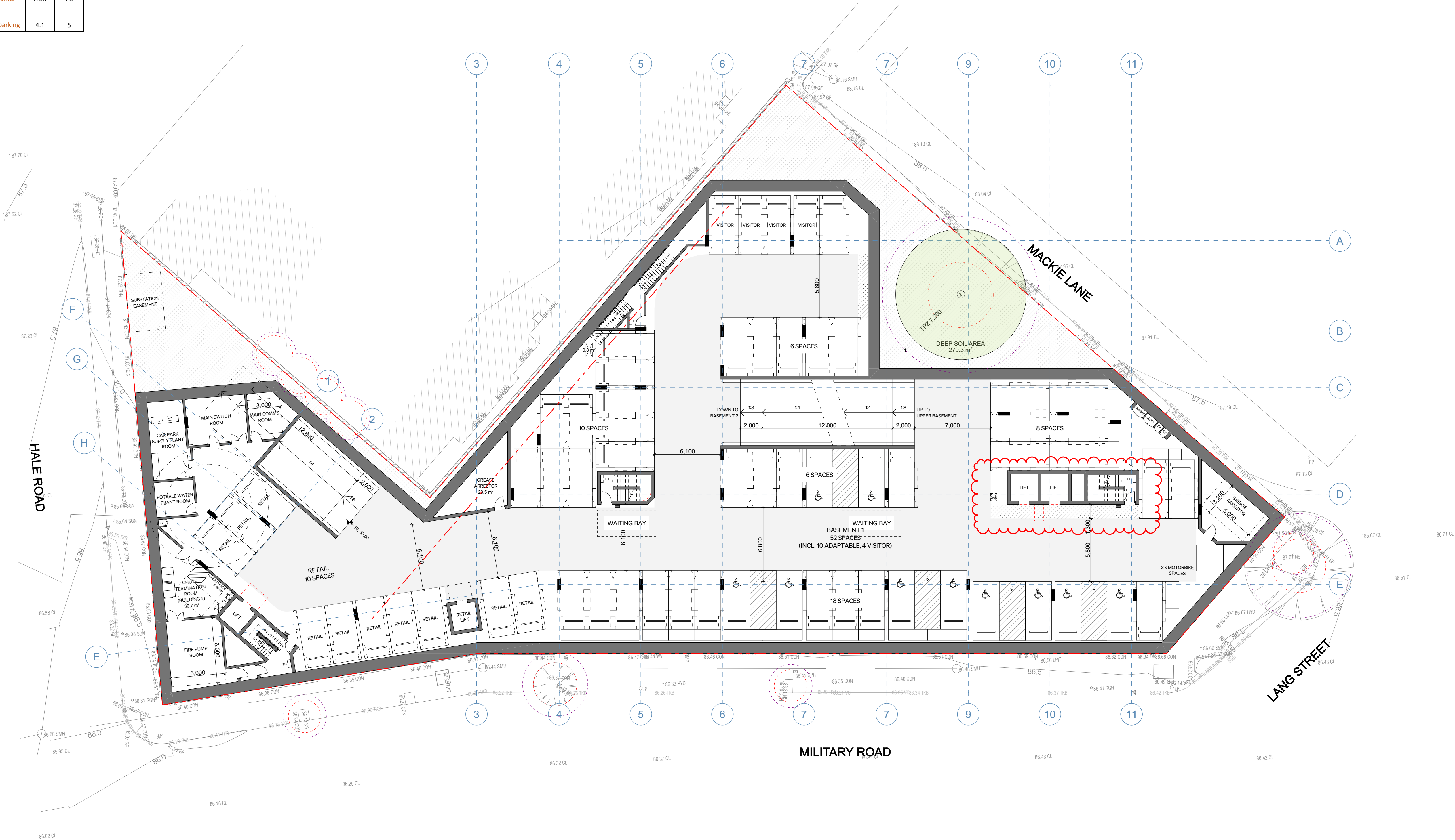
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Retail	776	1 per 74m ²	10.5	10
Total:			147	

Bicycle	Units	Rate	Req	Prop
Bicycle	103	1 per 4 units	25.8	26
Visitor				
Motorbikes	103	1 per 25 parking	4.1	5

B2	
Level	Prop
UB	13
B1	52
B2	72
137	



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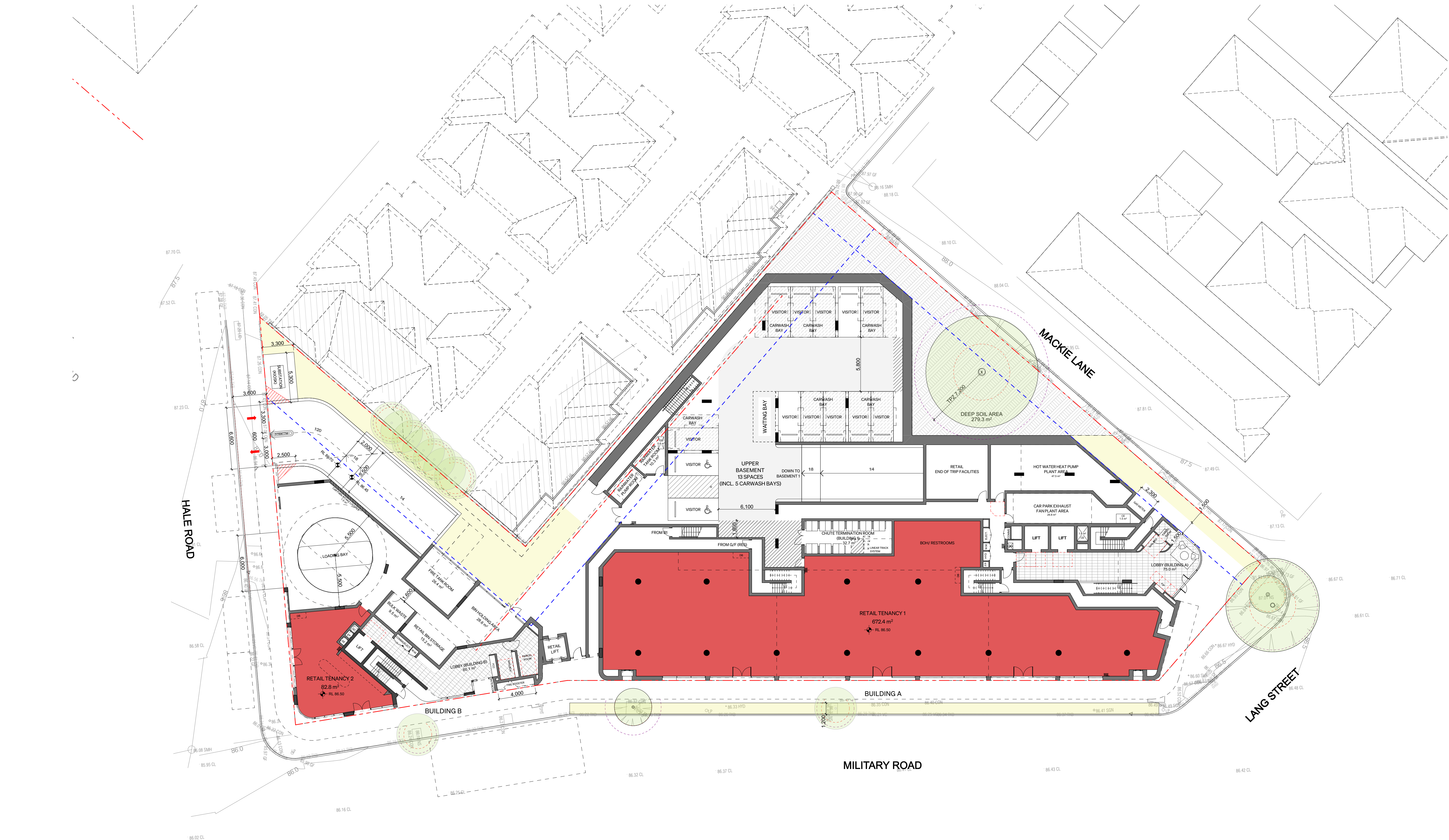
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Plan - Basement 1

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DA201

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Drawing Name
Plan - Lower Ground Floor

Drawing Scale
Drawing No.
DA202

@ A1
Revision
P8



Rev	Date	By	Chk	Description
P4	9/05/2026	KL/KG	KL/MR	Issue for Client Review
P5	14/05/2026	KL/KG	KL/MR	Issue for Consultants
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Plan - Ground Floor

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DA203

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DA205

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- Do not scale from drawing; figured dimensions only to be used.
- Building Contractor to verify all dimensions before commencing work.

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PRELIMINARY

Drawing Name
Plan - Level 3

Drawing Scale
Drawing No.
DA206

1:200 @ A1
Revision
P8

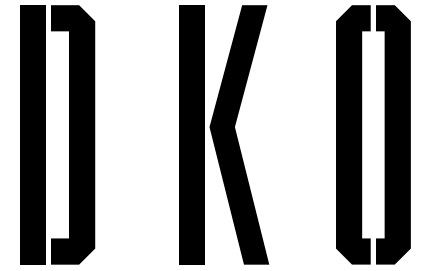


Rev	Date	By	Chk	Description
P4	9/05/2026	KL/KG	KL/MR	Issue for Client Review
P5	14/05/2026	KL/KG	KL/MR	Issue for Consultants
P7	22/05/2026	KL/KG	KL/MR	Issue for Client
P8	27/05/2026	KL/KG	KL/MR	Issue for Consultants

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- All works to be in accordance with authority & statutory approvals.
- Refer to site survey for all information relating to existing site conditions.
- All Boundary information to be confirmed by registered surveyor before commencing works on site.
- Refer to Arborist Report and Landscape Documentation for all information relating to trees and their retention/removal, and all landscape works.
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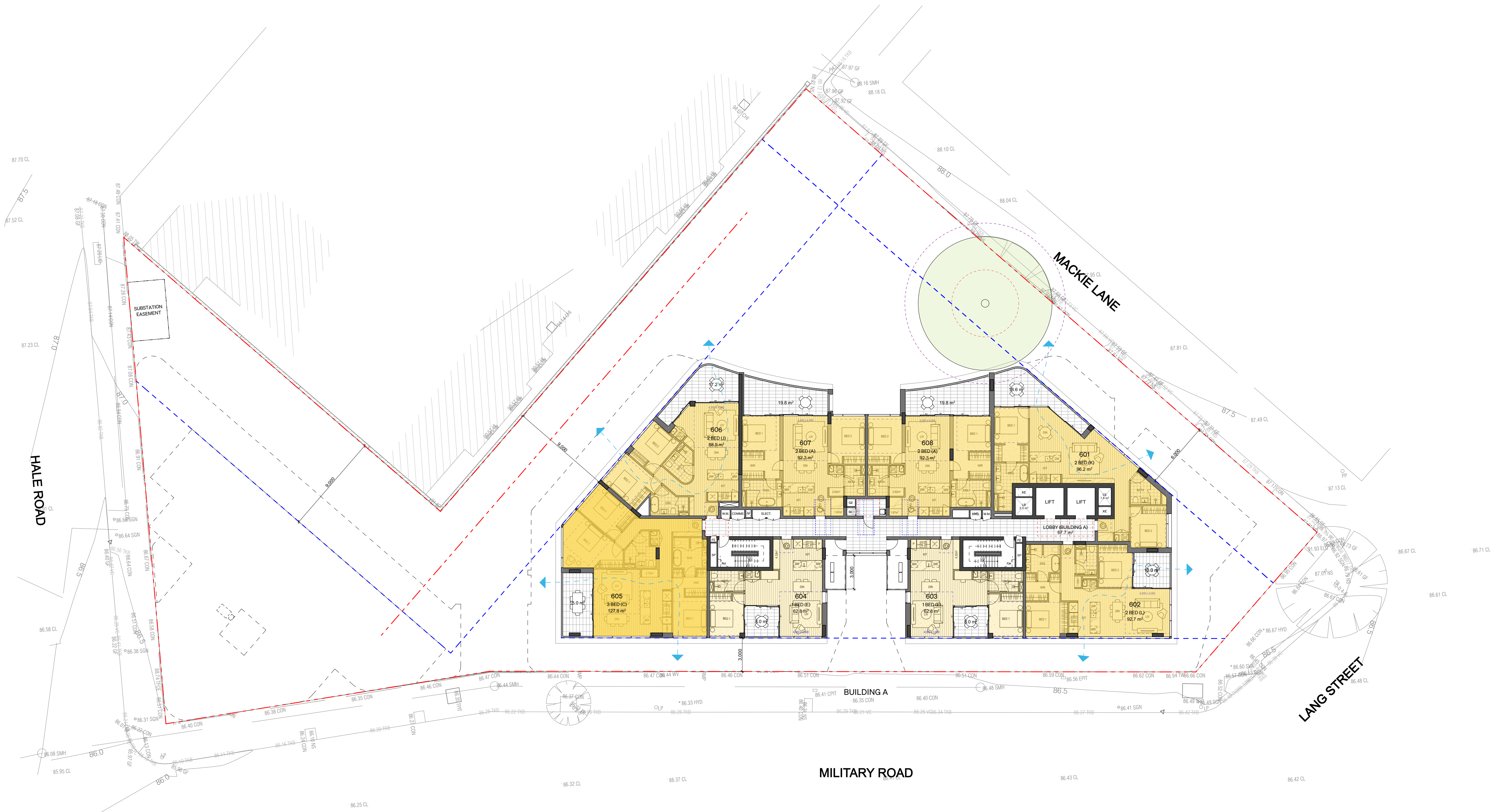
Country
Cammeraygal

PRELIMINARY

Drawing Name
Plan - Level 5

Drawing Scale
Drawing No.
DA208

1:200 @ A1
Revision
P8



PRELIMINARY

Rev	Date	By	Chk	Description
P4	9/05/2026	KL/KG	KL/MR	Issue for Client Review
P5	14/05/2026	KL/KG	KL/MR	Issue for Consultants
P7	22/05/2026	KL/KG	KL/MR	Issue for Client
P8	27/05/2026	KL/KG	KL/MR	Issue for Consultants

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Project Number
Project Address

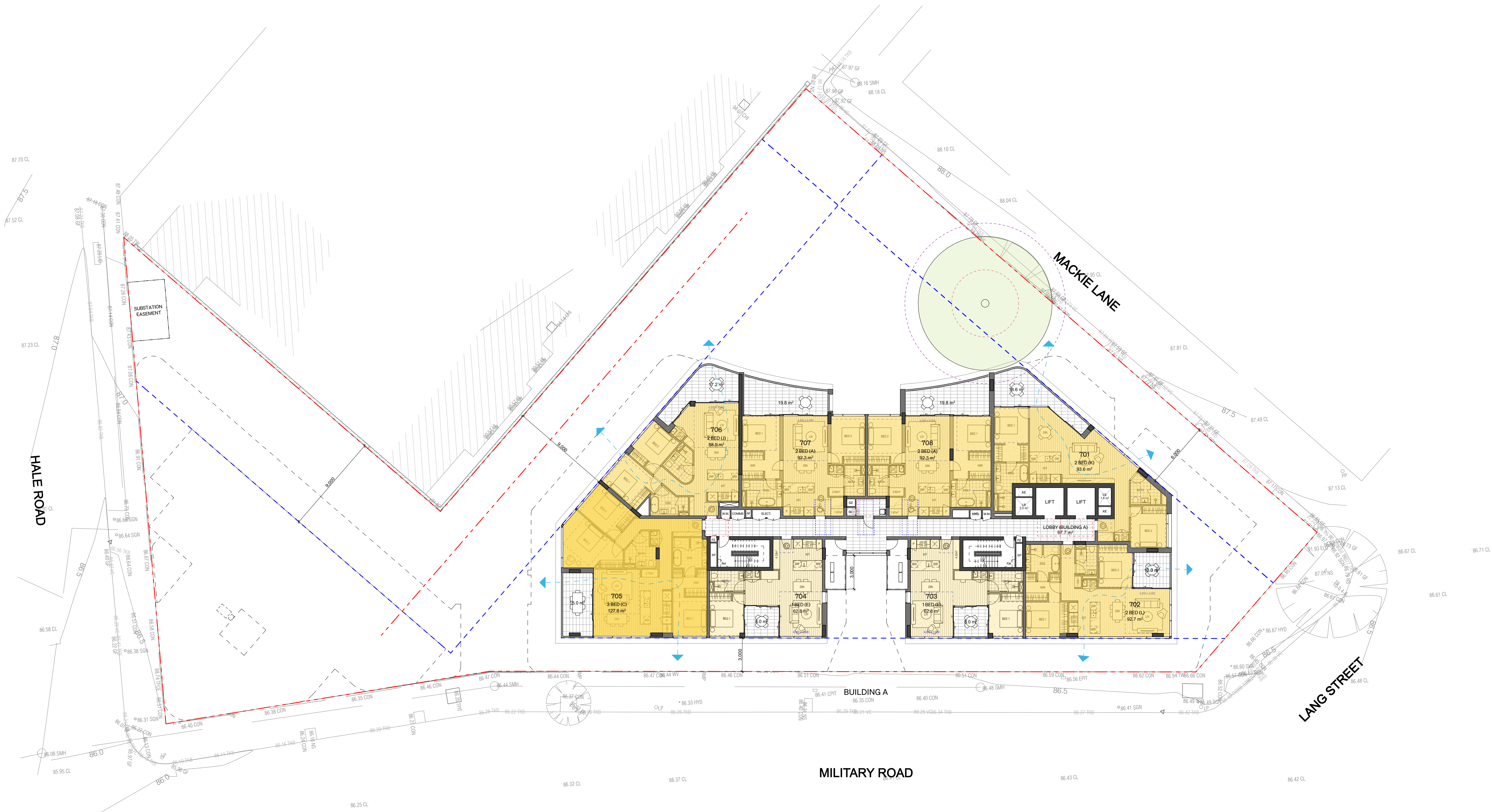
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Drawing Name
Plan - Level 6

Drawing Scale
Drawing No.
DA209

1:200 @ A1
Revision
P8



PRELIMINARY

Rev	Date	By	Chk	Description
P4	9/05/2026	KL/KG	KL/MR	Issue for Client Review
P5	14/05/2026	KL/KG	KL/MR	Issue for Consultants
P7	22/05/2026	KL/KG	KL/MR	Issue for Client
P8	27/05/2026	KL/KG	KL/MR	Issue for Consultants

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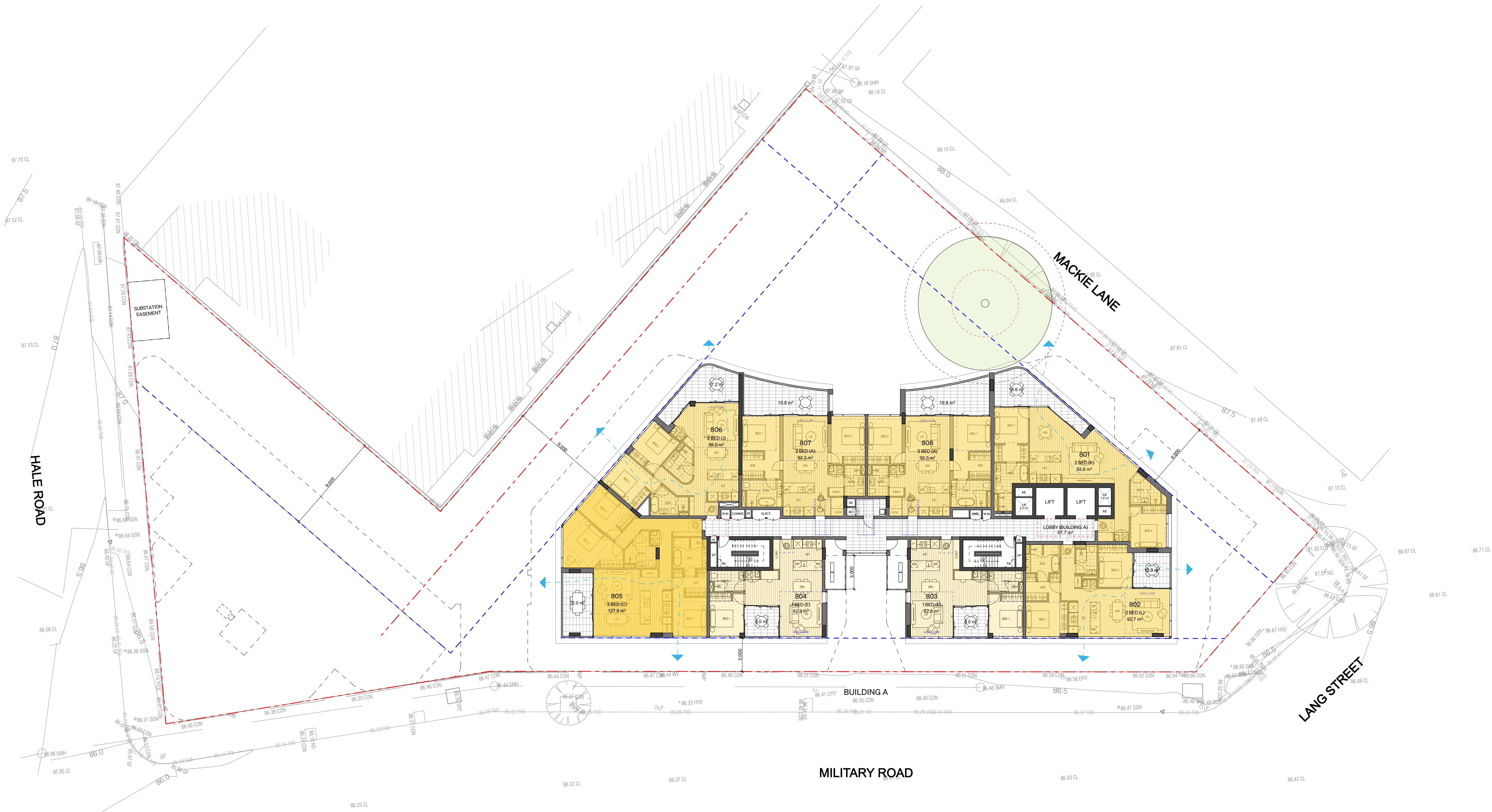
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Drawing Name
Plan - Level 7

Drawing Scale
Drawing No.
DA210

1:200 @ A1
Revision
P8



Rev	Date	By	Chk	Description
P4	9/05/2026	KL/KG	KL/MR	Issue for Client Review
P5	14/05/2026	KL/KG	KL/MR	Issue for Consultants
P7	22/05/2026	KL/KG	KL/MR	Issue for Client
P8	27/05/2026	KL/KG	KL/MR	Issue for Consultants

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PRELIMINARY

Drawing Name
Plan - Level 8

Drawing Scale
Drawing No.
DA211

1:200 @ A1
Revision
P8