

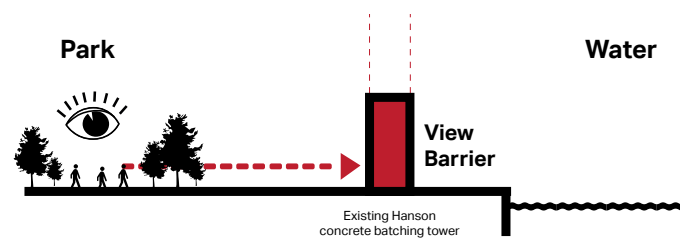
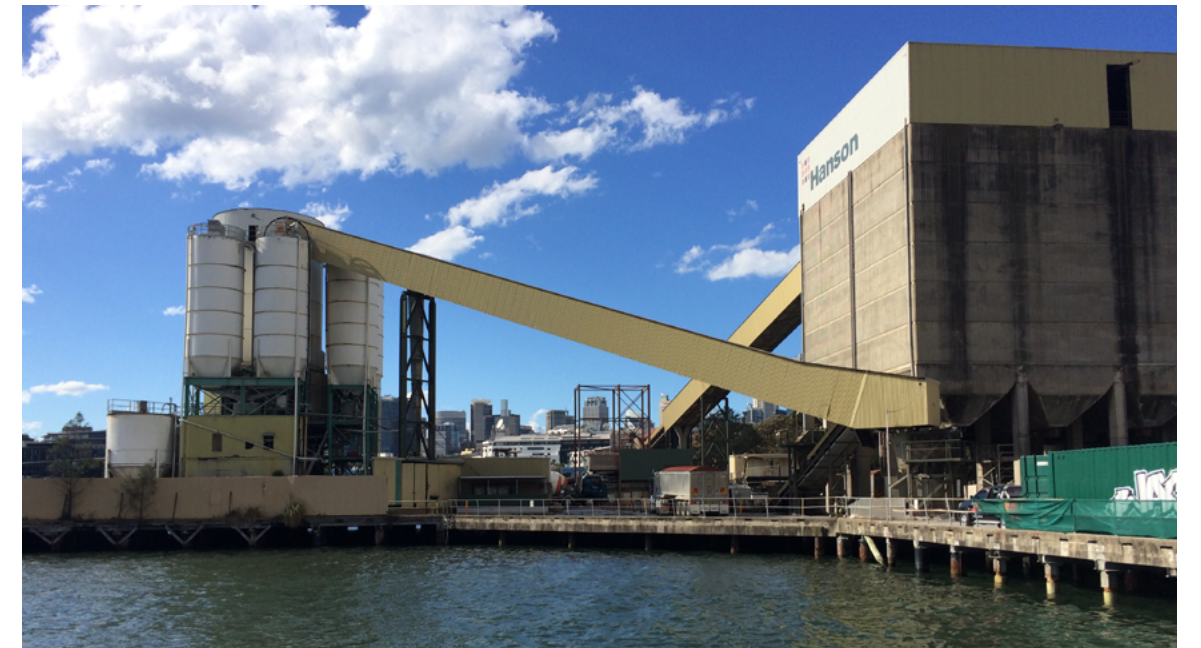
2.1 Built Form & Urban Design Analysis

Site Conditions - Existing Context

View of existing concrete batching tower from Wentworth Park Road to Blackwattle Bay.



View of existing industrial plant looking east from Sydney Secondary College Blackwattle Bay playfield.



Existing Context: Hanson Concrete Batching facility to be demolished

Current industrial infrastructure on site including the Hanson Concrete Batching facility will be demolished to create desired visual connection between Wentworth Park and Blackwattle Bay. With the concrete batching tower at 28.7 meters high, its large scale adds to the disconnection to the site and surrounding.

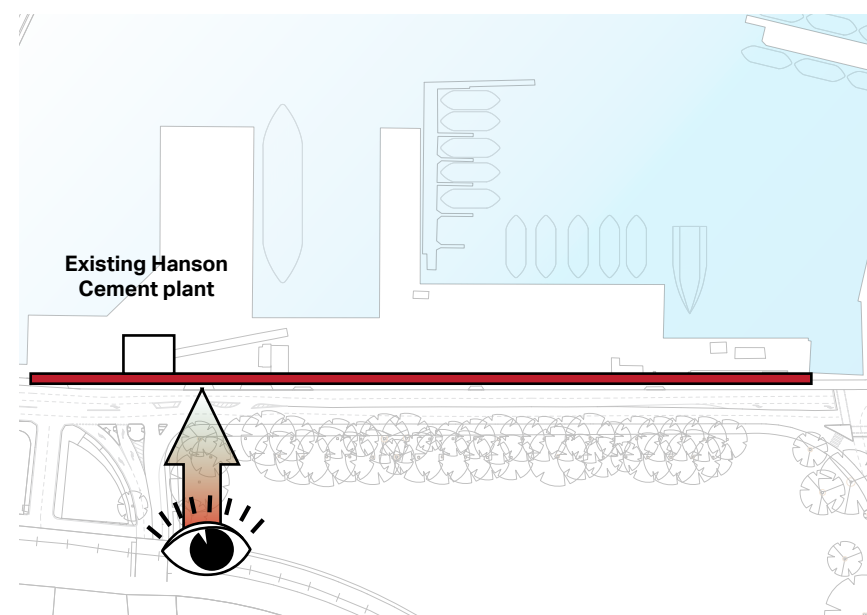


Diagram - Existing Hanson Cement plant limit views to water. Indicative only.

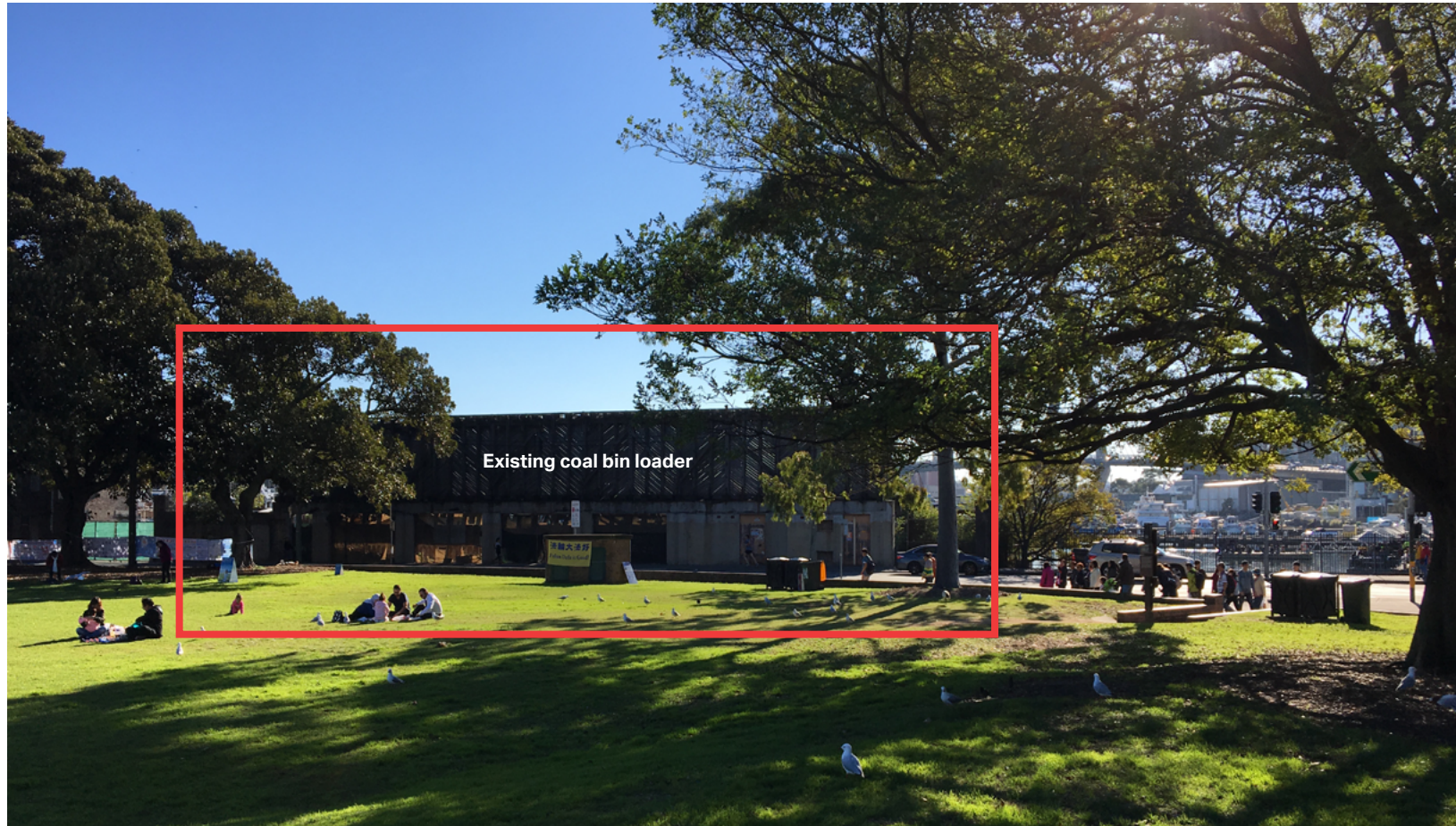


View to existing cement plant looking west from Blackwattle Bay.

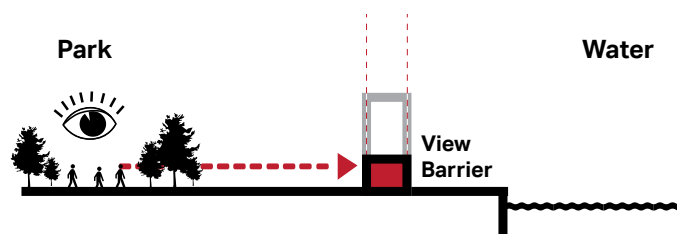
2.1 Built Form & Urban Design Analysis

Site Conditions - Existing Context

View of existing coal bin loader looking to Blackwattle bay from Wattle Street. The structure is at approximately 9.17 meters high.



Rear view of existing coal bin loader looking east towards the existing Fish Market.



Existing Context: Jones Brothers Coal Bins and brick building

Current former Jones Brothers coal bin loader and two storey brick building located at the eastern end of the proposed site will be demolished to create views and vistas to the water.

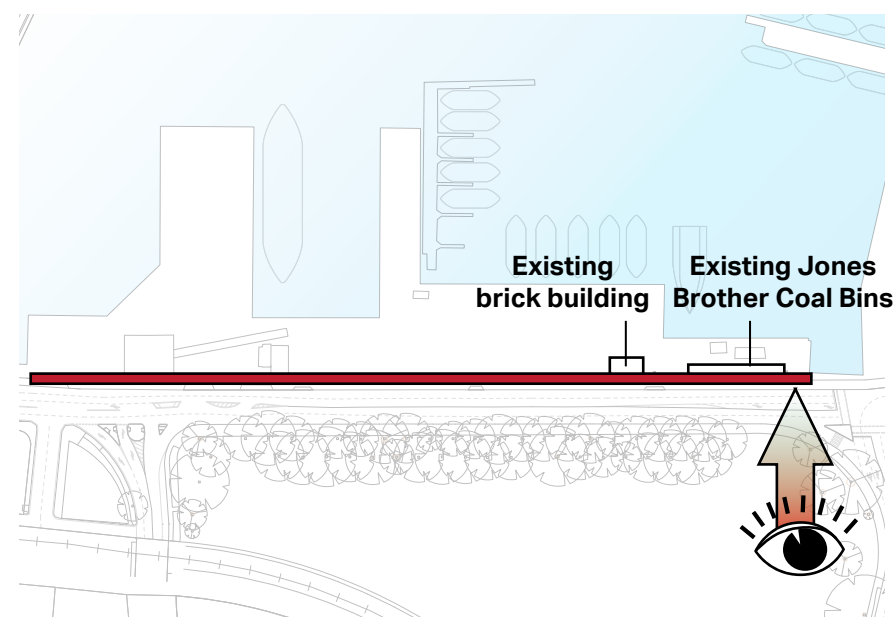


Diagram - Existing coal bin loader limit views to water. Indicative only.



Rear view of existing two storey brick building. Roof pitch at approximately 8.53 meters high.

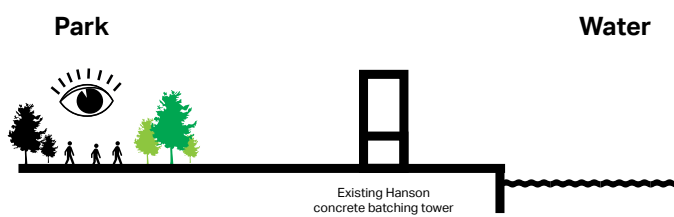
2.1 Built Form & Urban Design Analysis

Site Conditions - Existing Context

View of existing Bridge Road looking west from northern edge footpath of Wentworth Park. Existing brick building and coal bin loader on right.



View of existing significant Moreton Bay fig trees along Bridge Road.



Existing Context: Significant Moreton Bay fig trees

The avenue of Moreton Bay fig trees along the northern edge of Wentworth park besides Bridge Road are significant. The height and view of the trees present as opportunity for the proposed development.

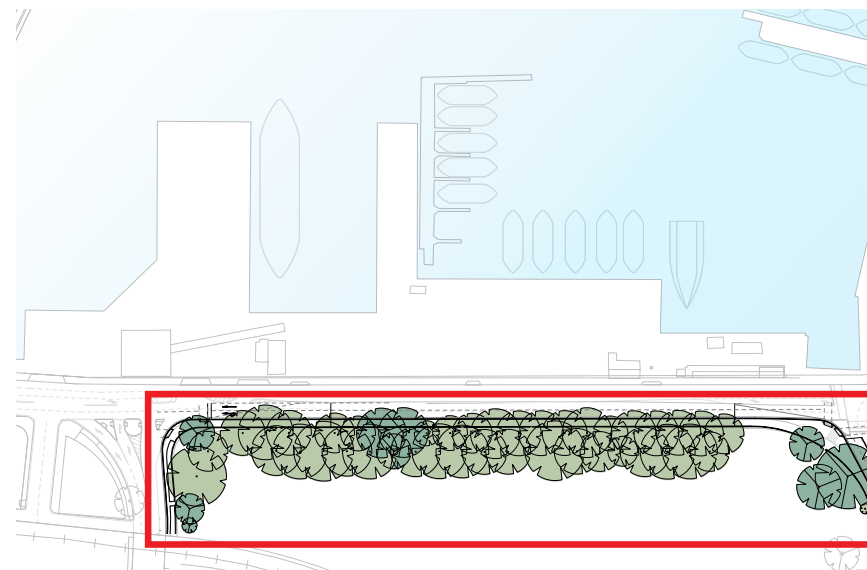


Diagram - Existing Moreton Bay fig trees at northern side of Wentworth Park.
Indicative only.



View to west from Bridge Road and Wattle Street traffic intersection.

2.1 Built Form & Urban Design Analysis

Site Conditions - Existing Context

The following images highlight existing elements located at or within close proximity to the site.



1. Coal Bin Loader



2. Moreton Bay Fig Trees



3. Glebe Viaducts



4. Former Caretaker's Cottage



5. Remnant Sandstone Sea Walls

2.1 Built Form & Urban Design Analysis

Site Conditions - Existing Context

Wentworth Park Road at Wentworth Park Centre & Greyhounds Racecourse

The image above outlines the existing flooding conditions and instance of flood-waters along the wider area of the Wentworth Park Centre & Greyhounds Racecourse. In such events, excess water is not currently discharged into the bay with falls, but rather handled by stormwater drain pipes.



2.1 Built Form & Urban Design Analysis

Site Conditions - Existing Context

The following diagrams illustrate the current site conditions of the existing Fish Market. The new development seeks to address and improve existing pedestrian safety, accessibility and view issues.

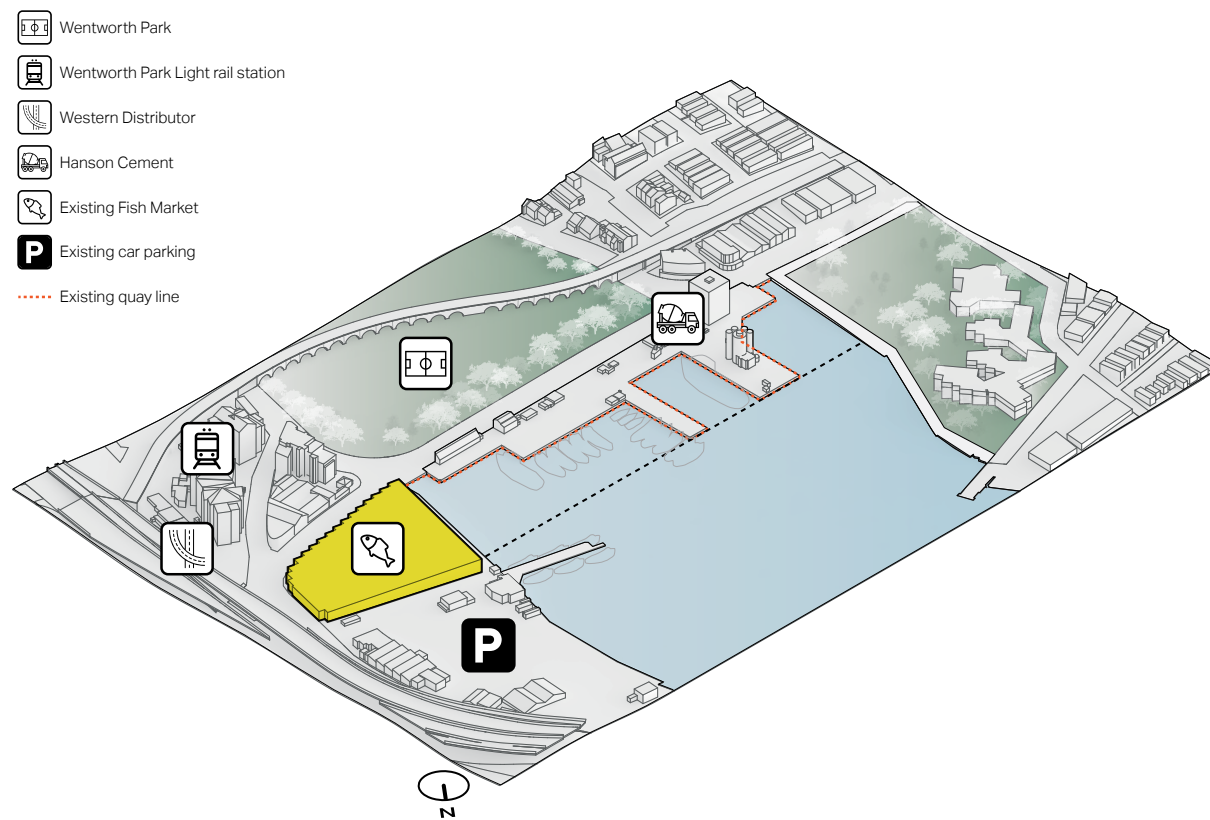


Diagram - Current Site Conditions

The proposed site is flanked by the existing Fish Market facility to the east, Wentworth Park to the south, the Glebe foreshore promenade to the west and Blackwattle Bay to the north. The proposed site has historically been used for industrial purposes such as a commercial marina and concrete batching.

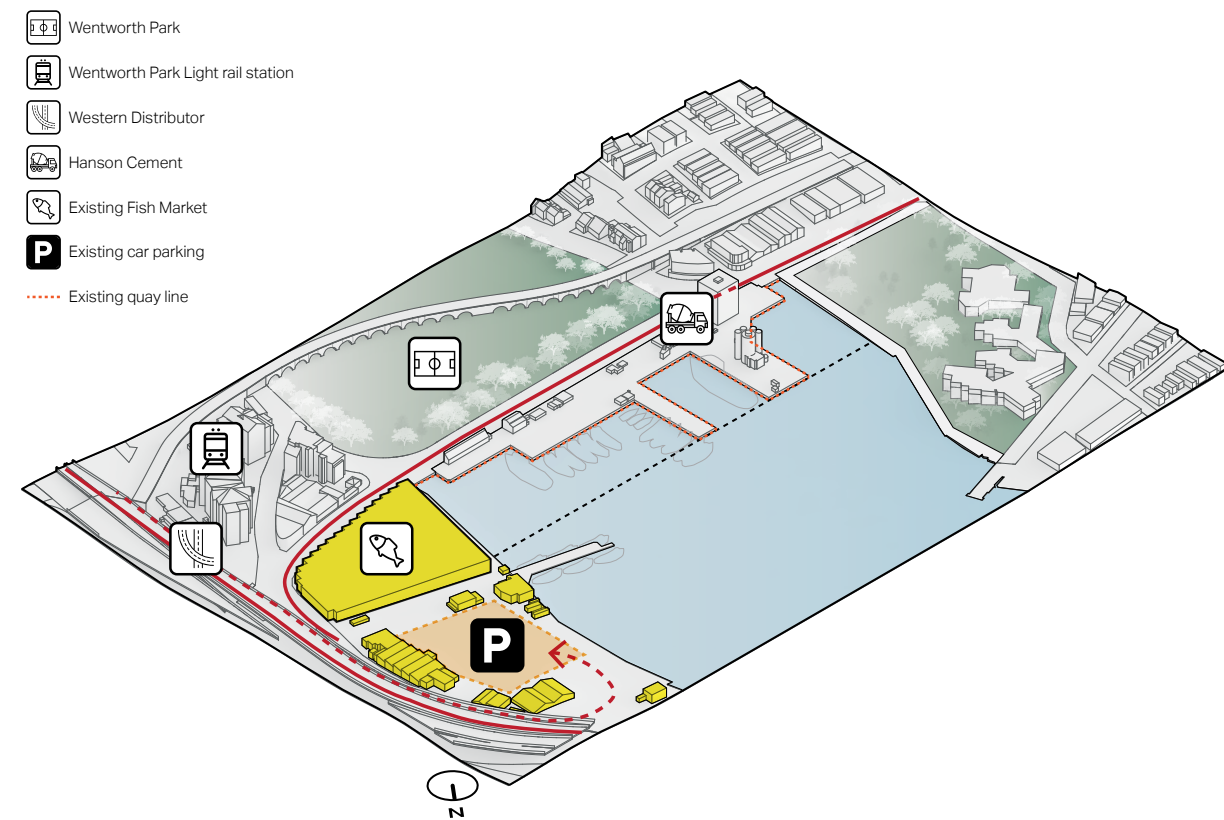


Diagram - Existing Vehicular Access and Parking

There are currently six access points to the site from Bridge Road. None of the access points are signalised making site access dangerous for both pedestrians and vehicles.

The existing Fish Market has approximately 417 car park spaces. The existing Fish Market site is accessed from one signalised intersection at Bank Street and Miller Street.

The primary vehicular access point for the proposed site will occur at a new signalised intersection at Wentworth Park Road and Bridge Road.

2.1 Built Form & Urban Design Analysis

Site Conditions - Existing Context

With reference to Bays Market District urban design principles, the following diagrams illustrates why the head of Blackwattle Bay was selected as the site for the new development.

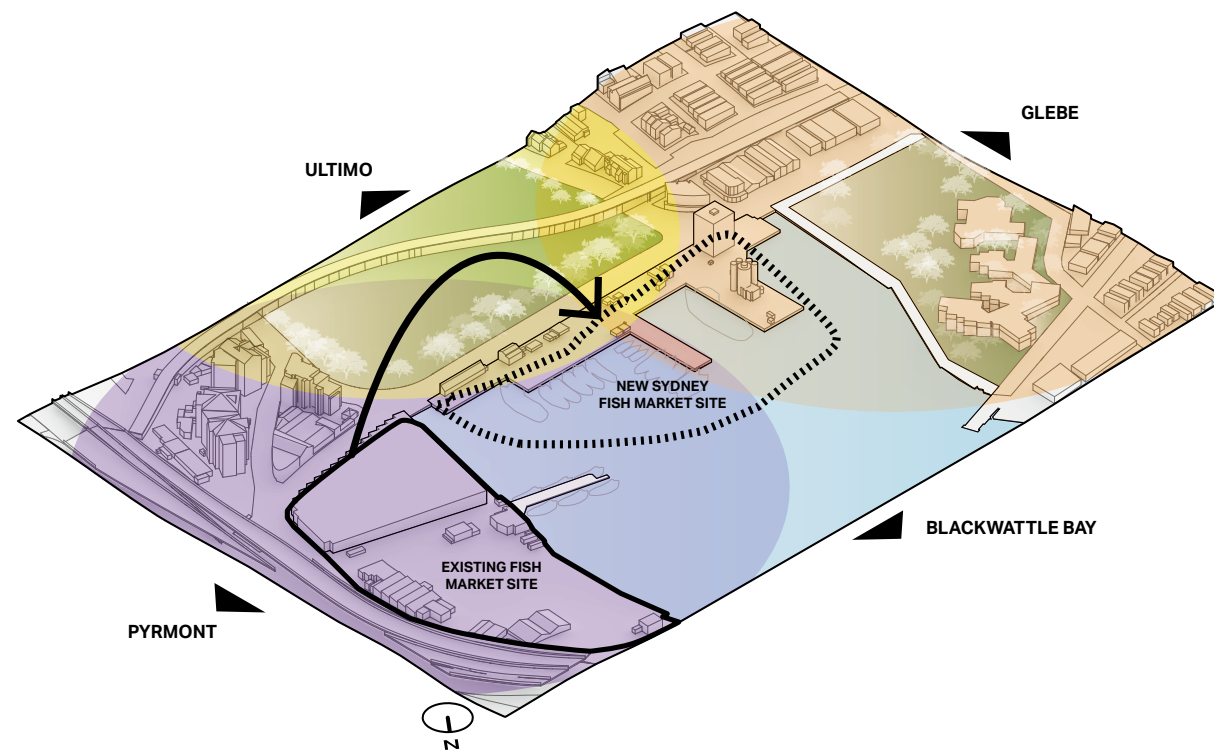


Diagram - Strategic relocation

The current Fish Market is a popular destination for both locals and visitors with approximately three million visitors per year. After decades of continuous operations, the non-purpose-built facility that currently house the market is aging.

With reference to Bays Market District transformation urban design principles, the proposed development location is envisioned as a logical step for inner city urban renewal and civic amenity enhancement. The new Sydney Fish Market will be established as a unique civic destination to rejuvenate surrounding district such as Pyrmont, Broadway and Darling Harbour.

The relocation approach will also minimise impacts on the business operations for the existing Fish Market.

Source: Infrastructure NSW (2016). Bays Market District Urban Design Framework.

2.1 Built Form & Urban Design Analysis

Site Conditions - Existing Context

The following diagrams illustrate the existing site conditions at the head of Blackwattle Bay, the new development seeks to address and improves on the views and access conditions.

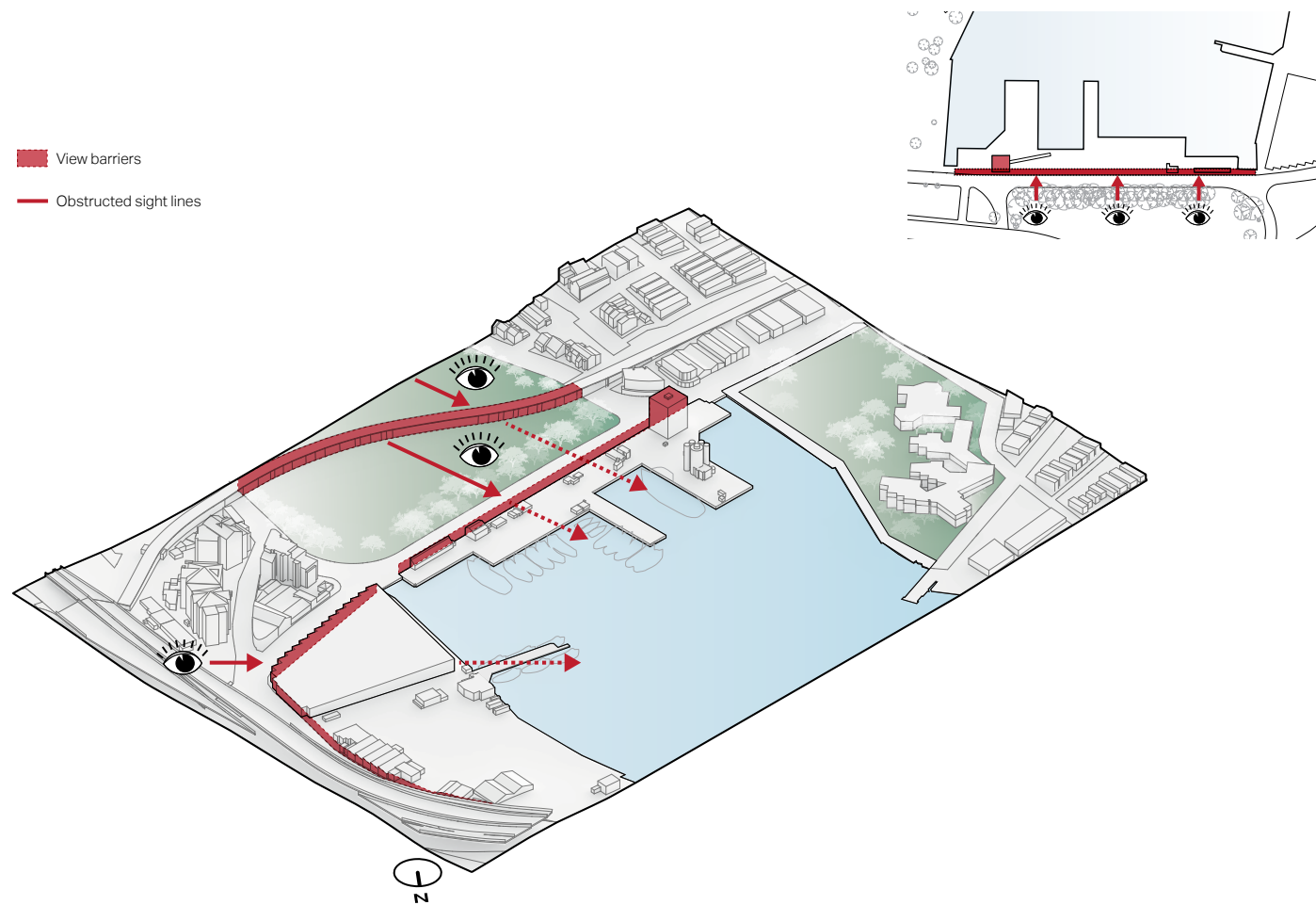


Diagram - Views from Wentworth Park

Views to the bay from the park and surrounding streets are currently blocked by the remnants of Hanson's concrete batching plant and a series of concrete and wire fences around the site, demarcating the prior private property lines. The light rail viaducts present a partial obstruction of views from the south side of Wentworth Park, while framing view corridors from the streets on either side.

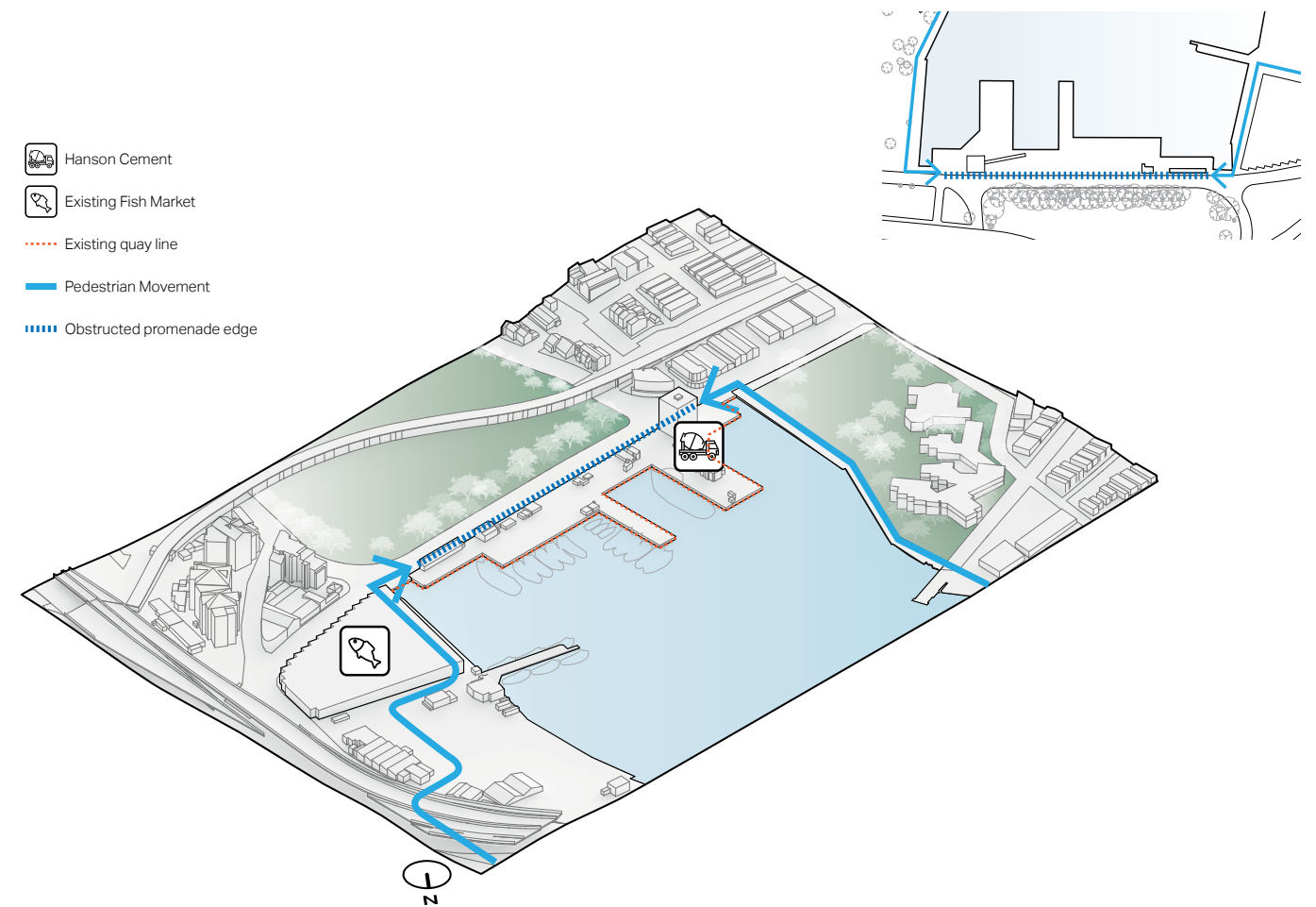


Diagram - Obstructions to Pedestrian Access

Due to the prior uses of the bay current configuration, pedestrian passage is blocked across the face of Blackwattle Bay, where industrial remains and a privately operated marina uses prevent public access to the waterfront.

2.1 Built Form & Urban Design Analysis

Site Conditions - Existing Context

The following diagrams illustrate the existing site conditions at the head of Blackwattle Bay, the new development seeks to address and improves on these conditions.

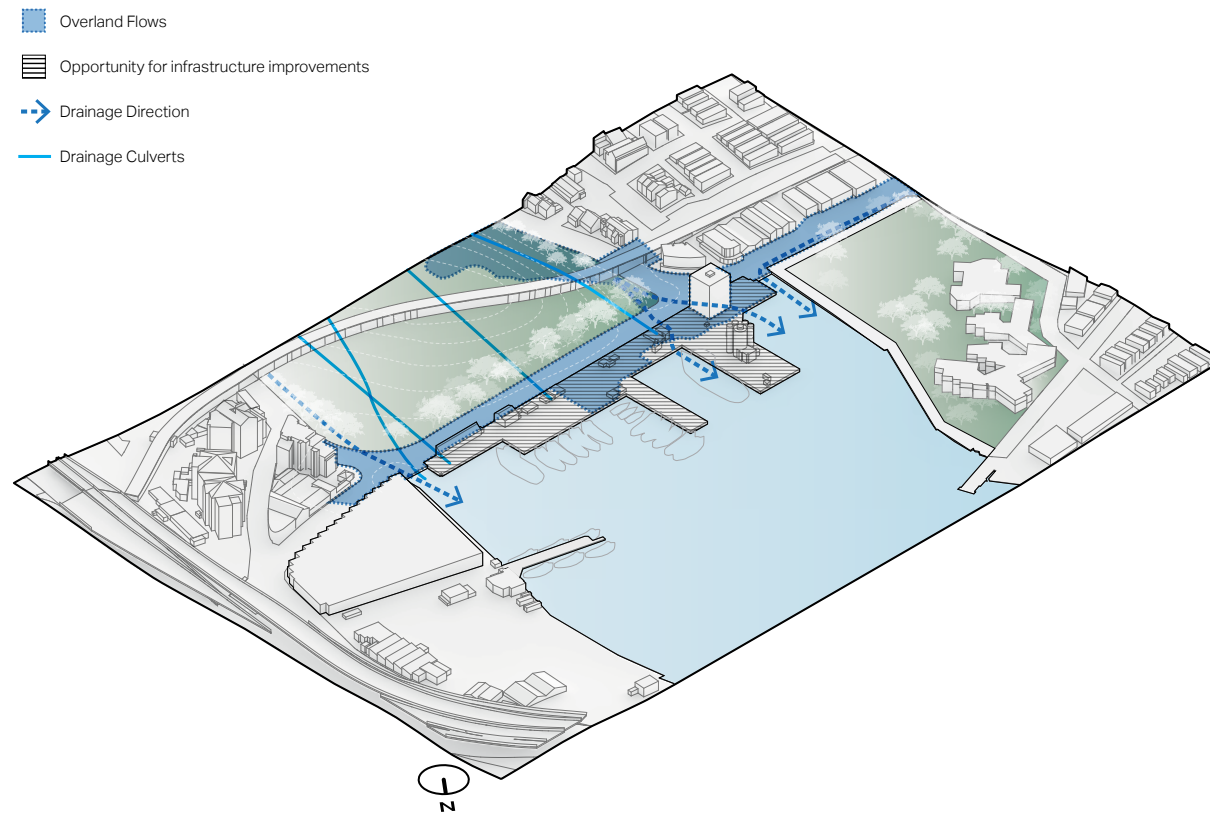


Diagram - Indicative Flood Zones

The diagram above indicates overland flows. At present, flows are dealt with by storm-water culverts on the east side of Wentworth Park that discharge into the bay. Bridge Road and the west side of the park exhibit a propensity for flooding which carries over to the existing piers.

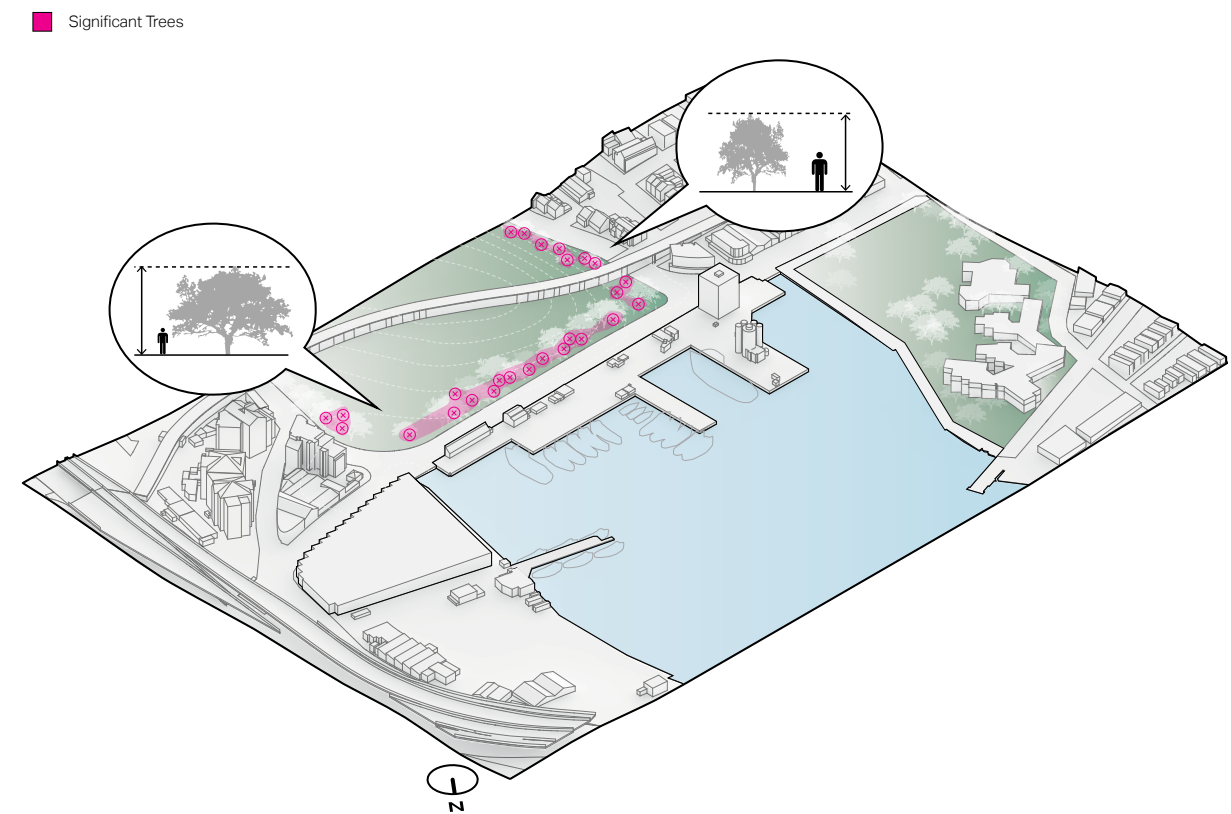


Diagram - Retainment of Wentworth Park Trees

The double row plantation of Moreton Bay Figs are listed as significant trees by the City of Sydney, as they exemplify the stylistic approach and influence of Charles Moore. The trees have been planted to define Wentworth Park's boundary, a technique typical of 1890s. The height of the trees provides an opportunity to conceal the scale of a potential development north of Bridge Road.

2.1 Built Form & Urban Design Analysis

Site Conditions & Responses

The following diagrams illustrates how the site was defined by existing developments at the head of Blackwattle Bay.

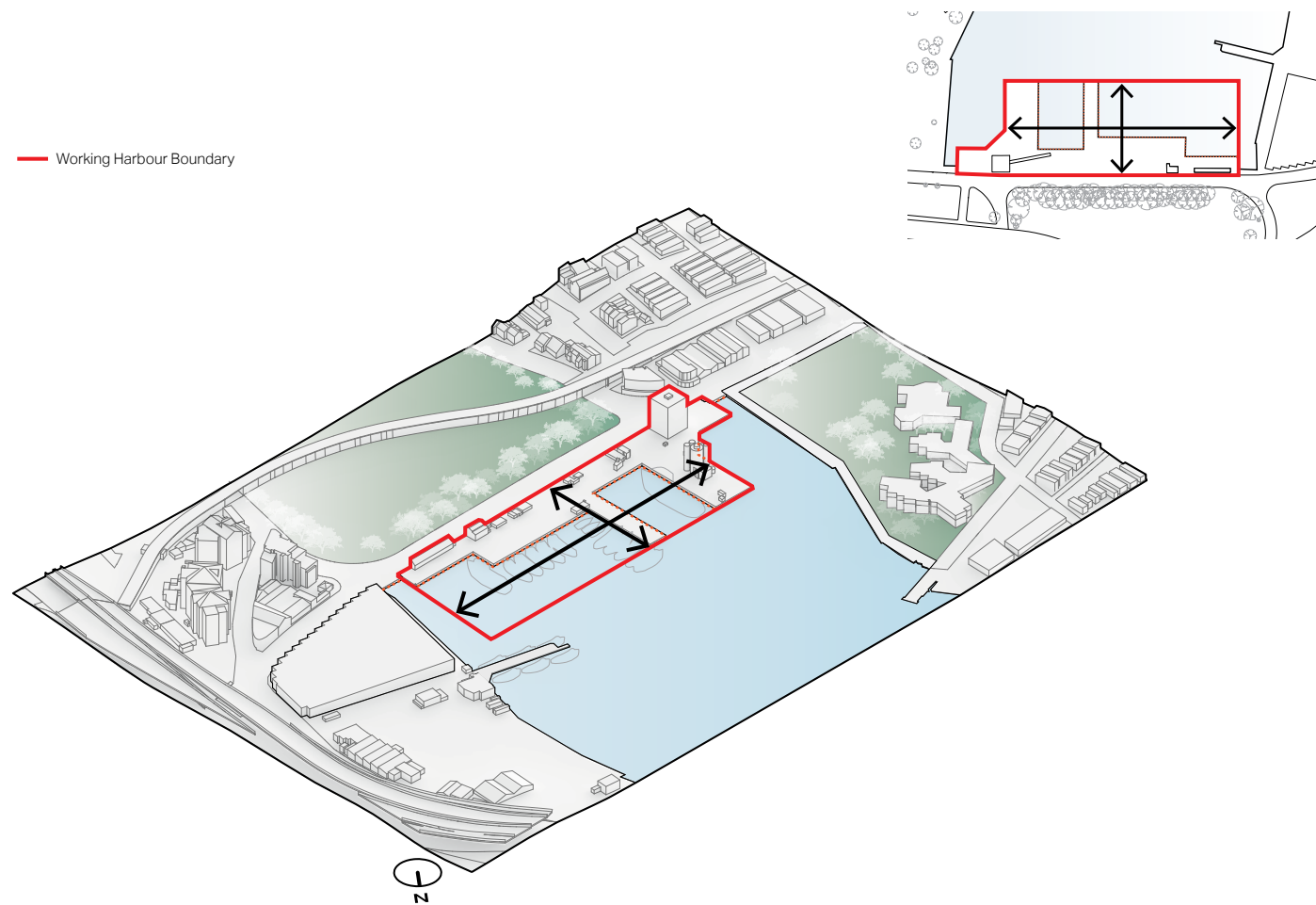


Diagram - Existing Harbour Infrastructure

The proposed site location is determined from the area of existing working harbour infrastructure which currently occurs at the head of Blackwattle Bay.

The Hanson Cement plant is characterised by hard-surfaced industrial wharves, a car-park and appended marina jetties.

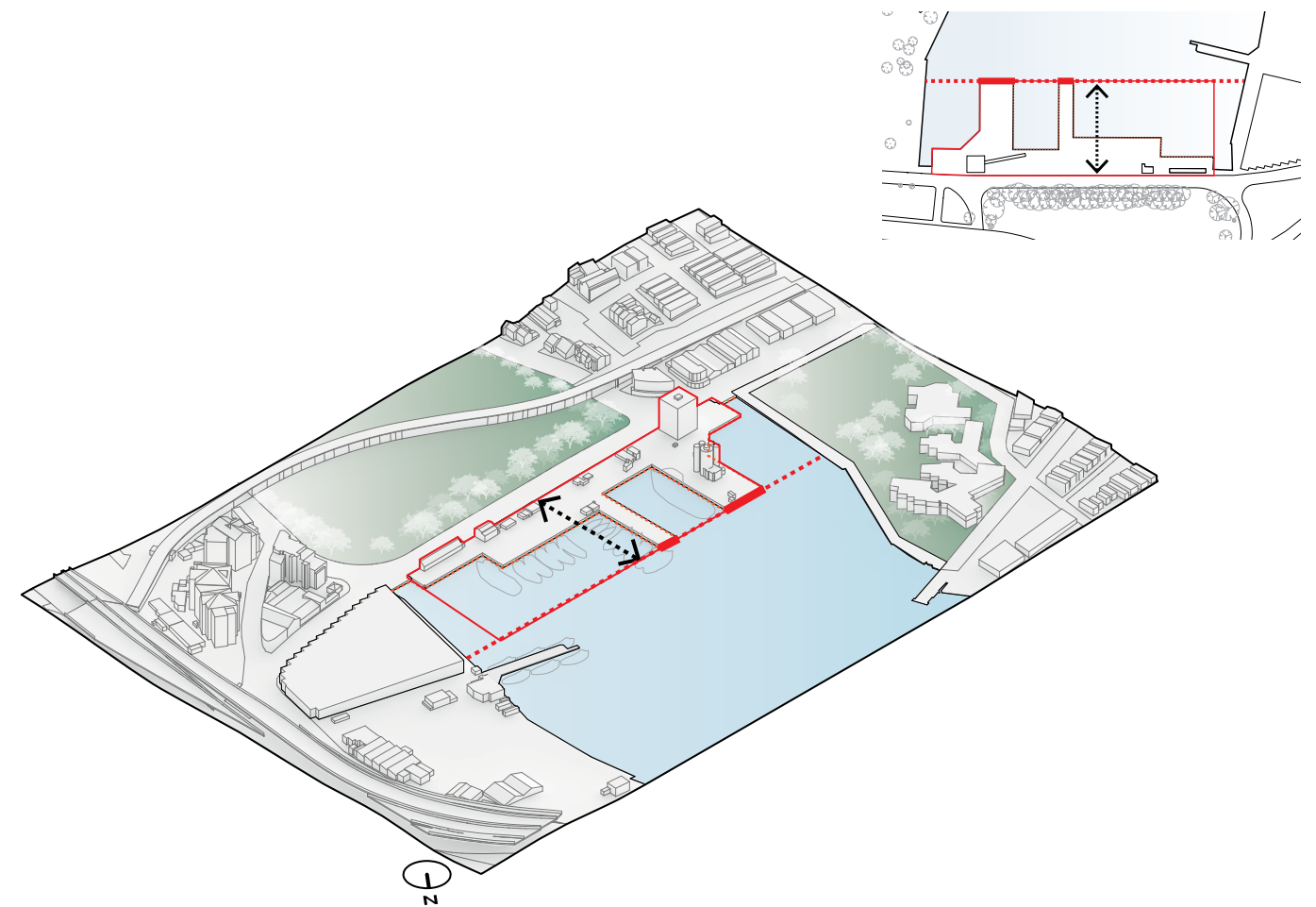


Diagram - Extent of Site

The outer perimeter edge is defined as a distance from the roadside boundary which contains the required programme as compactly as possible. The site attempts to match the lengths of the existing piers.

2.1 Built Form & Urban Design Analysis

Site Conditions & Responses

The following diagrams illustrate how the site envelope was shaped based on considerations regarding program and context.

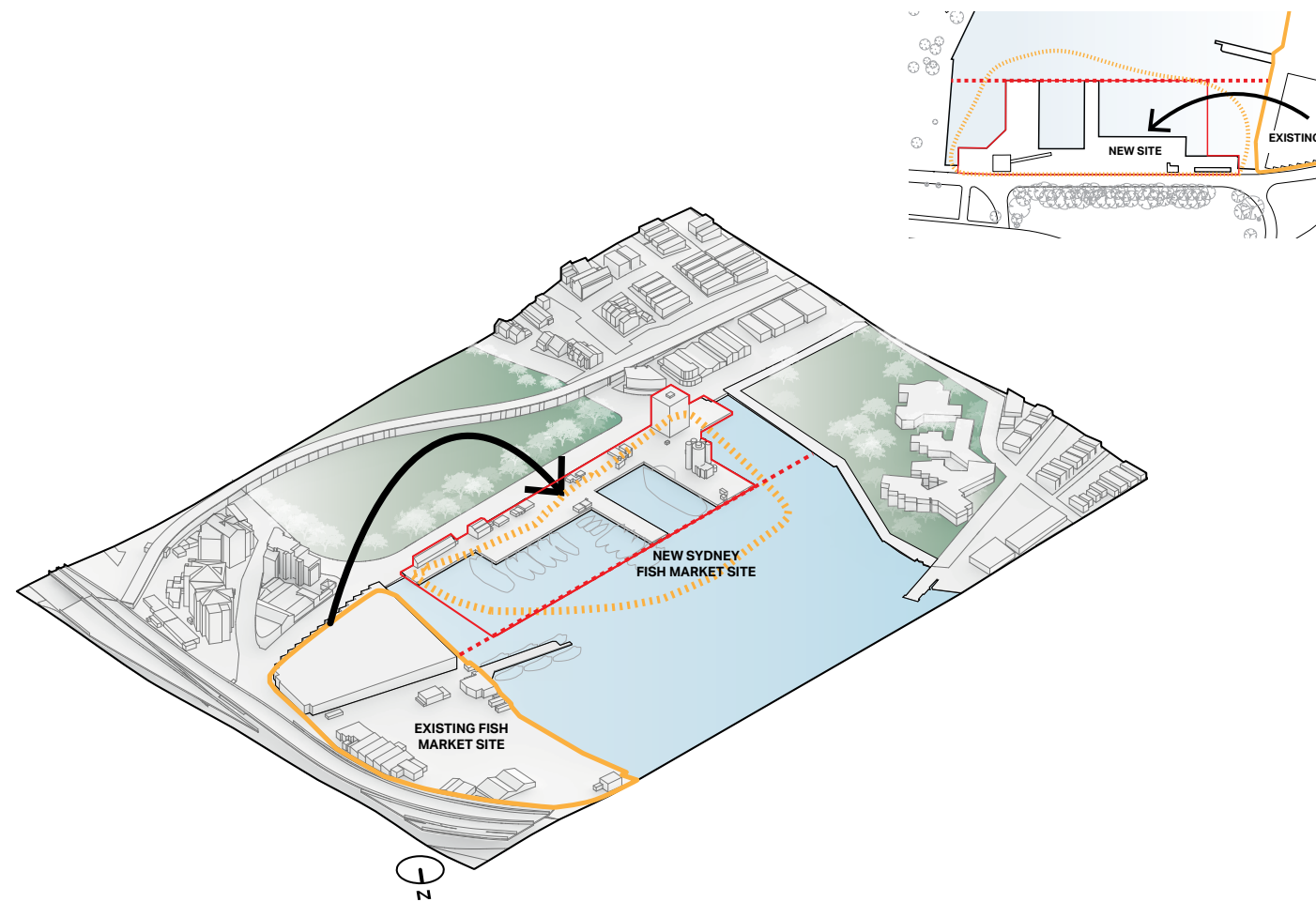


Diagram - Site Equivalency

The transposition of the existing Fish Market site is used as a basis for the new Sydney Fish Market site area located along Bridge Road.

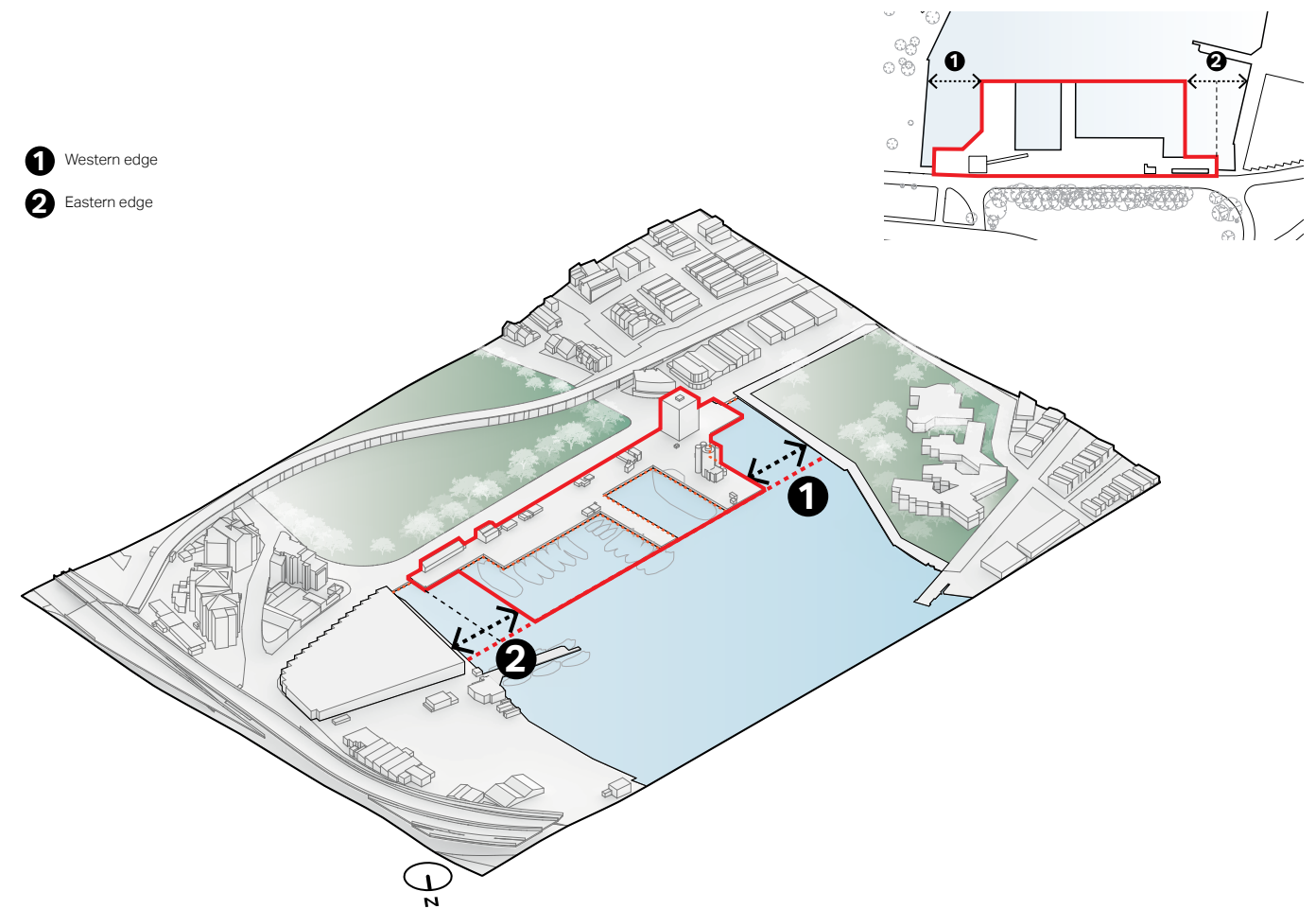


Diagram - Edge Boundaries

To preserve flow and functionality in the bays on either side of the new development, a distance precedent is set for either side of the development based on existing infrastructure. The new strategically placed location provides separation to existing Fish Market, as well as creating linkage north (Blackwattle Bay) and south (Wentworth Park); east (Glebe) and west (Pyrmont).

2.1 Built Form & Urban Design Analysis

Site Conditions & Responses

The following diagrams illustrate the proposed development's response to existing site conditions, and how these have influenced its form. The development seeks to minimise its impact to the site whilst improving upon existing site conditions by creating new view corridors, expanding the public's access to the water's edge,

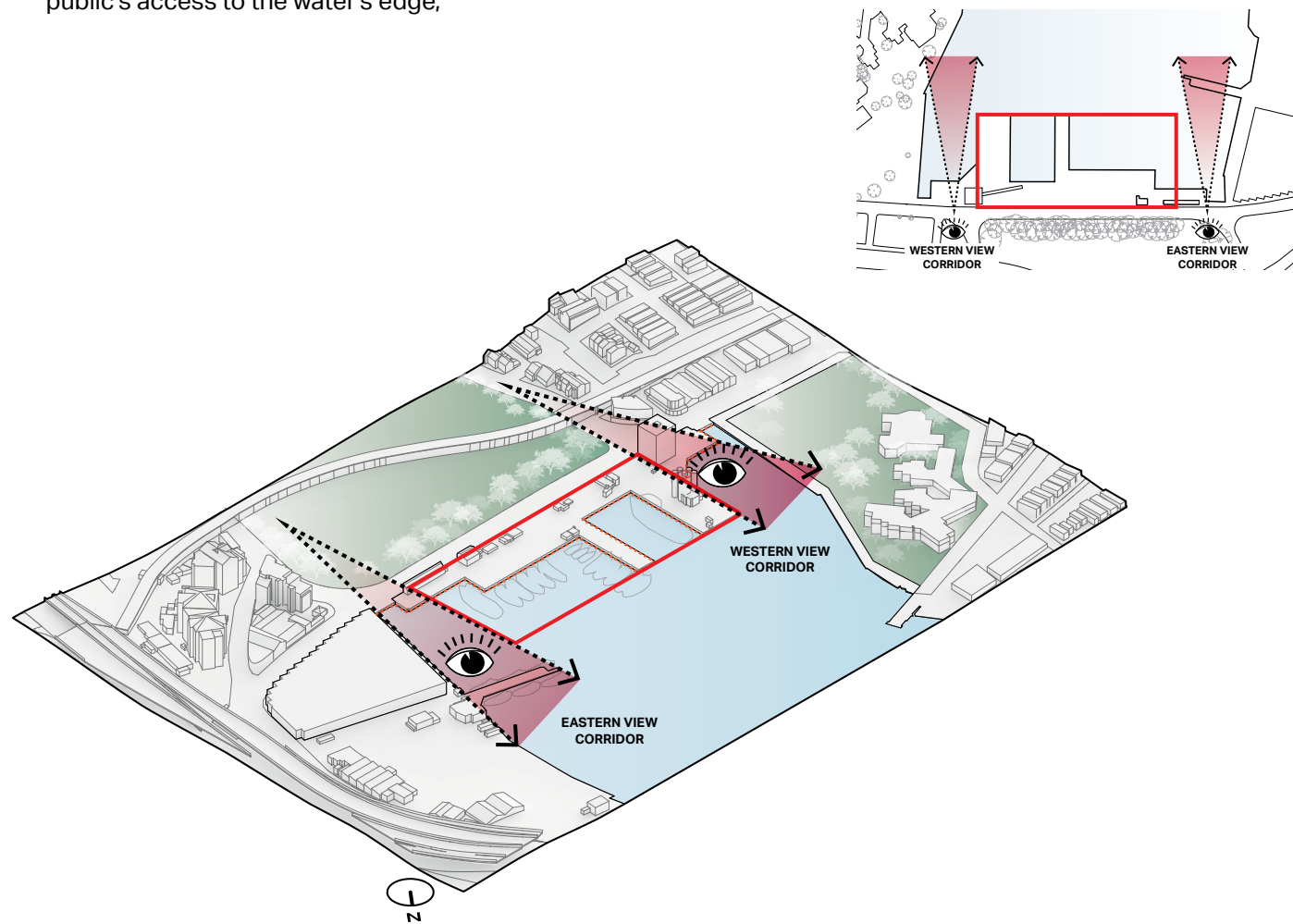


Diagram - Views and vistas to Blackwattle Bay

Preserving boundaries to the east and west side of the new Sydney Fish Market development offers new view corridors from the park and surrounding streets to Blackwattle Bay once the fences and disused industrial infrastructure is removed.

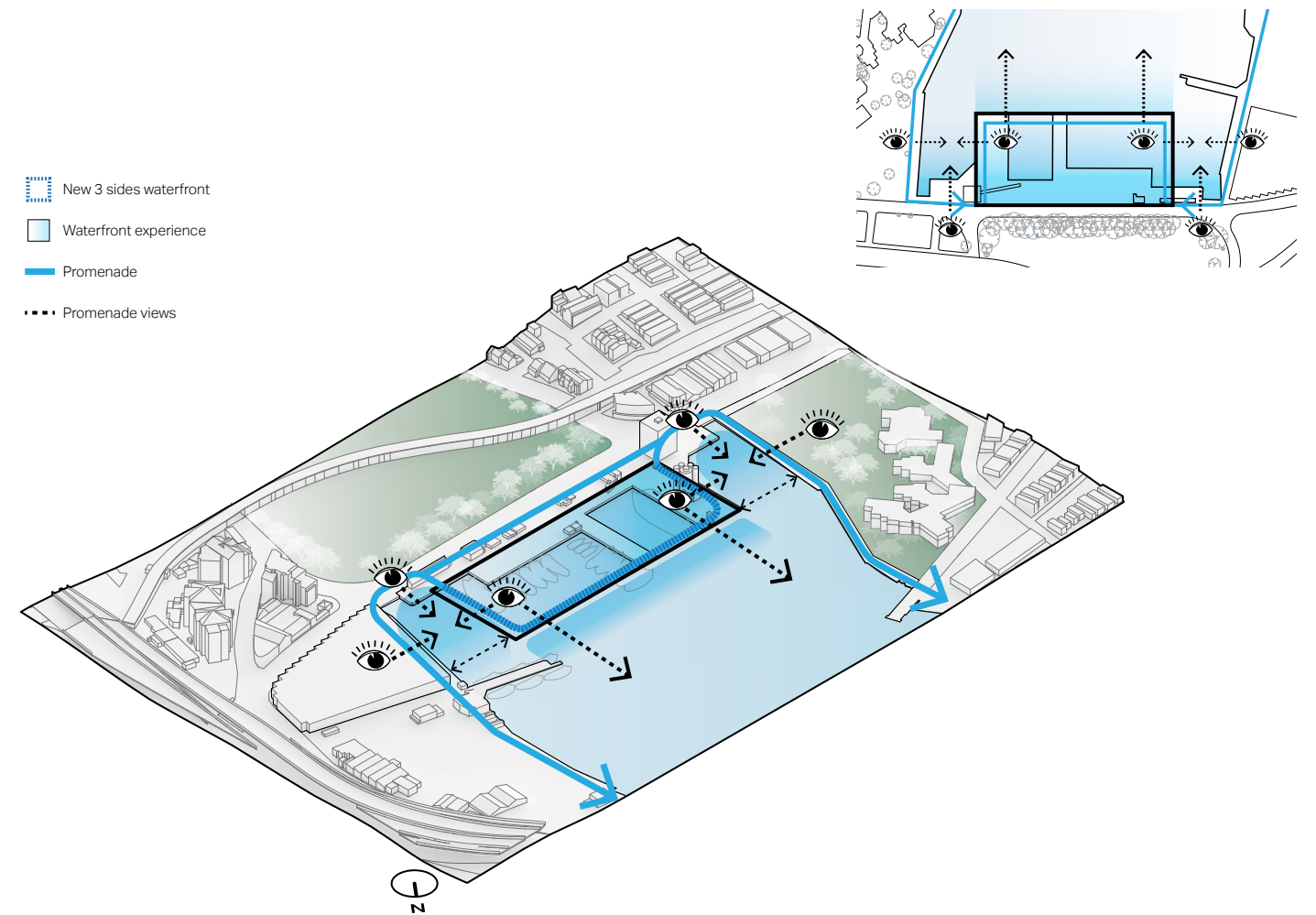


Diagram - Perimeter Circulation

A primary design driver for the new Sydney Fish Market development is public access to the water's edge and the ability to pedestrianise the entire waterfront promenade of Blackwattle Bay. The circuit of public access increases the waterfront edge, offering greater opportunity to connect with the harbour waterfront along the eastern and western edges.

2.1 Built Form & Urban Design Analysis

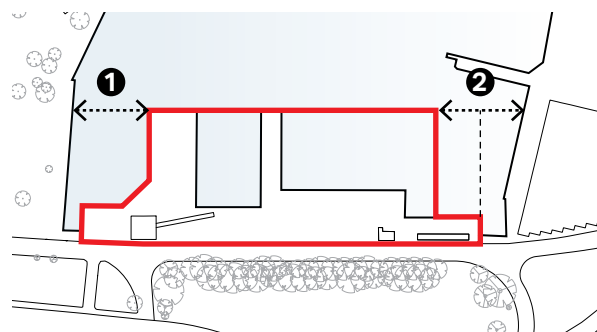
Site Conditions - Existing Distance Between Edge and Pier



1. Existing Distance Between Western Edge and Pier



2. Existing Distance Between Eastern Edge and Pier



Legend Diagram. Indicative Only.

2.1 Built Form & Urban Design Analysis

Site Conditions & Responses

The following diagrams illustrate how the site envelope was setback for public access and activation.

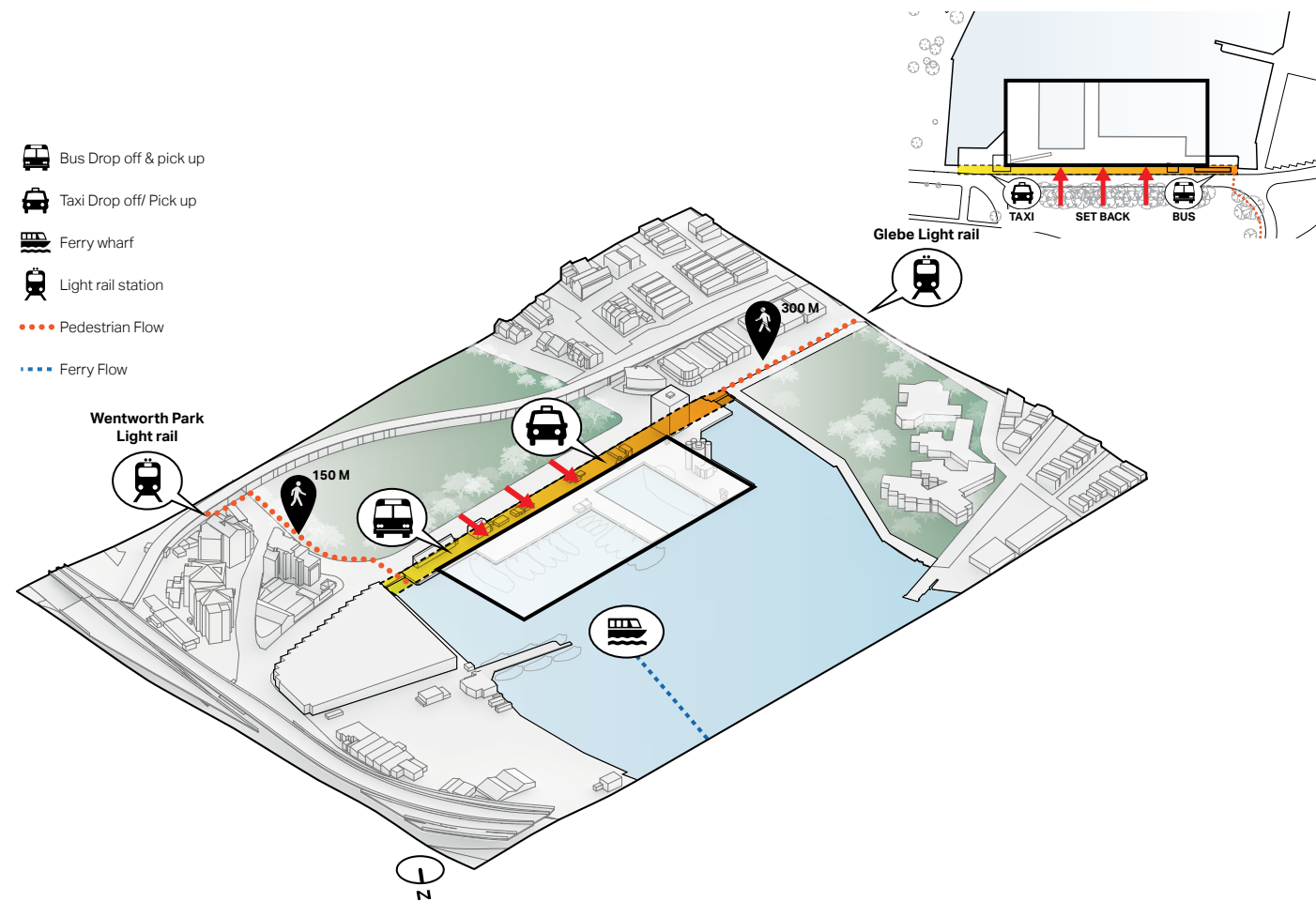


Diagram - Connectivity

The proposed site for the new Sydney Fish Market is foreseen to be a node for connectivity. The design of the new facility will accommodate all modes of transport but will give preference to active transport (walking and cycling) and public transport (light rail, bus, taxi, Uber, and ferry).

Wentworth Park, the Fish Market and Glebe light rail stations are in walkable distance to the proposed site, and the pedestrian movement will encourage movement across park and along water.

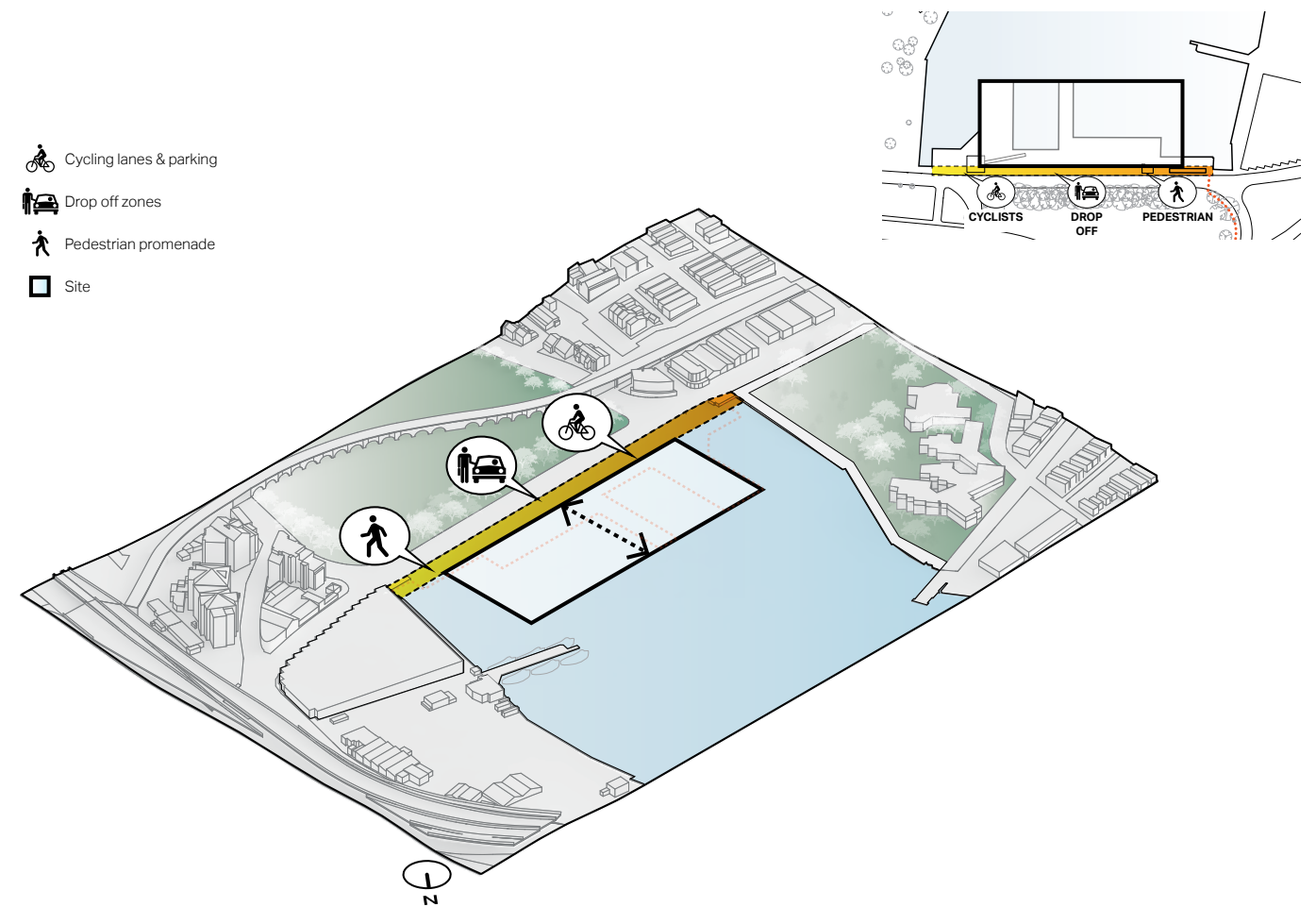


Diagram - Potential Building Setback

Given the anticipated high number of pedestrian and cyclist movements to and around the new Sydney Fish Market, the building will be setback from Bridge Road to accommodate a wider footpath.

2.1 Built Form & Urban Design Analysis

Site Conditions & Responses

The following diagrams illustrates how the site envelope was shaped based on considerations to immediate context and public amenity.

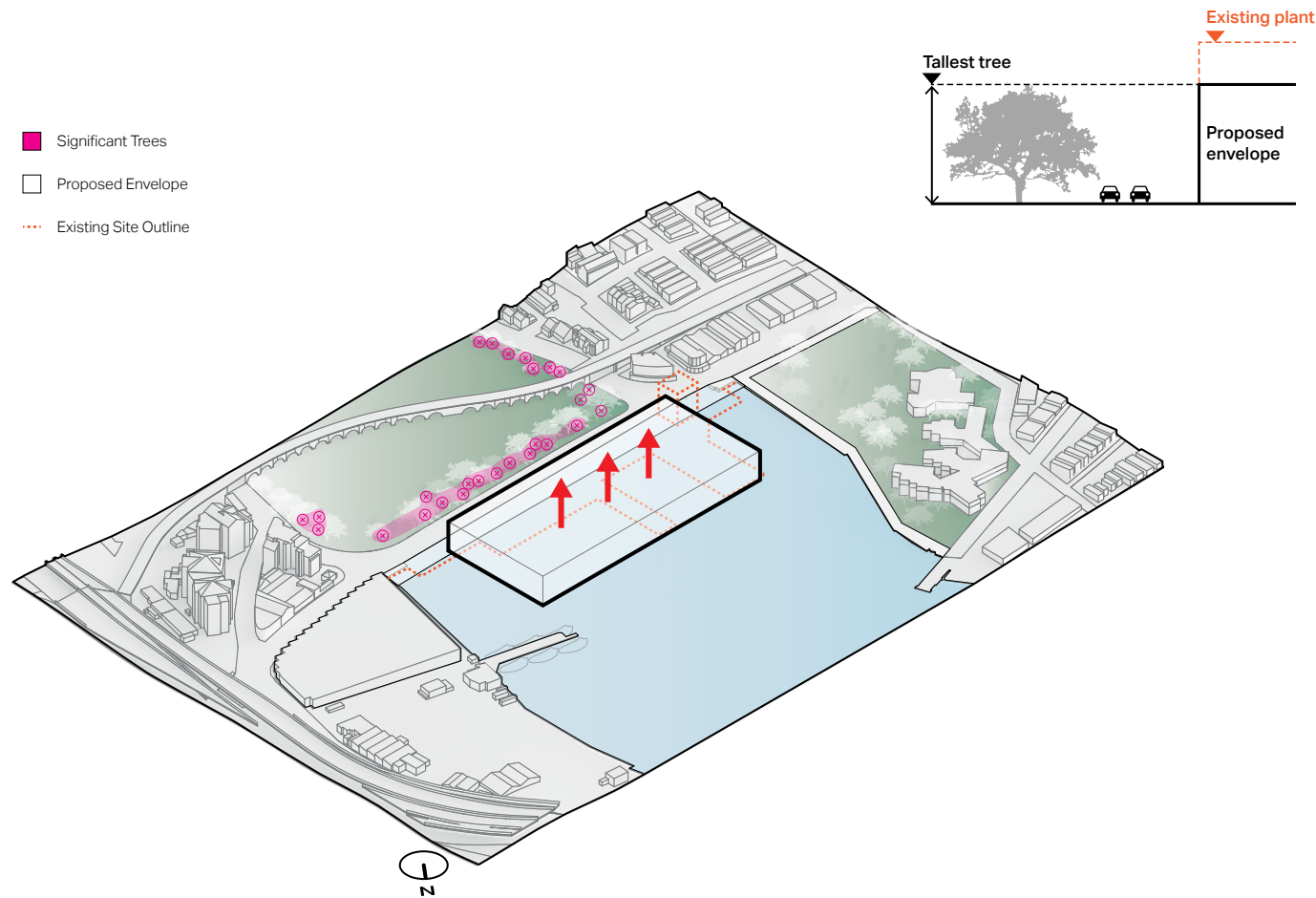


Diagram - Height and scale of envelope

The significant trees in Wentworth Park informed the height of the proposed building envelope with the roof edge seeking to relate to the tree line, and not exceed the height of existing cement batch plant (28.7 meters high) as a reference datum. This also reduces the visual and shadowing impacts on Wentworth Park.

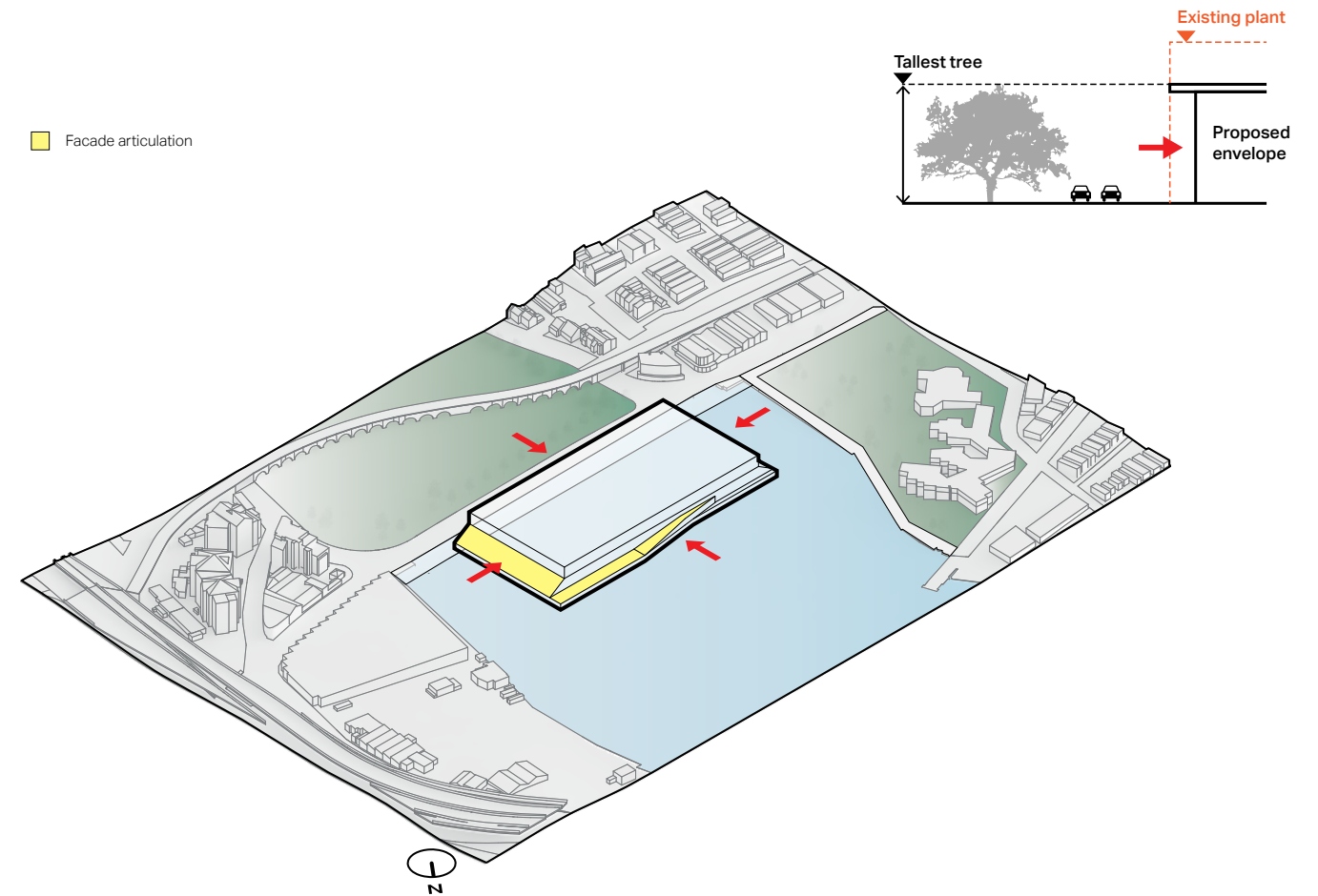


Diagram - Articulation of envelope bulk and scale

The proposed facades are set back to reduce the scale of its massing to Bridge Road. In particular the Bridge Road elevation of the new development will feature a creation of green promenade with new planting references.

Each facade provides an opportunity as a connector through featured landscape stairs in order to relate the promenade and building. This will also break the bulk of the mass to a more human street scale to activate the public realm.

2.1 Built Form & Urban Design Analysis

Site Conditions & Responses

The following diagrams illustrates how the site envelope was shaped based on considerations to immediate context and public amenity.

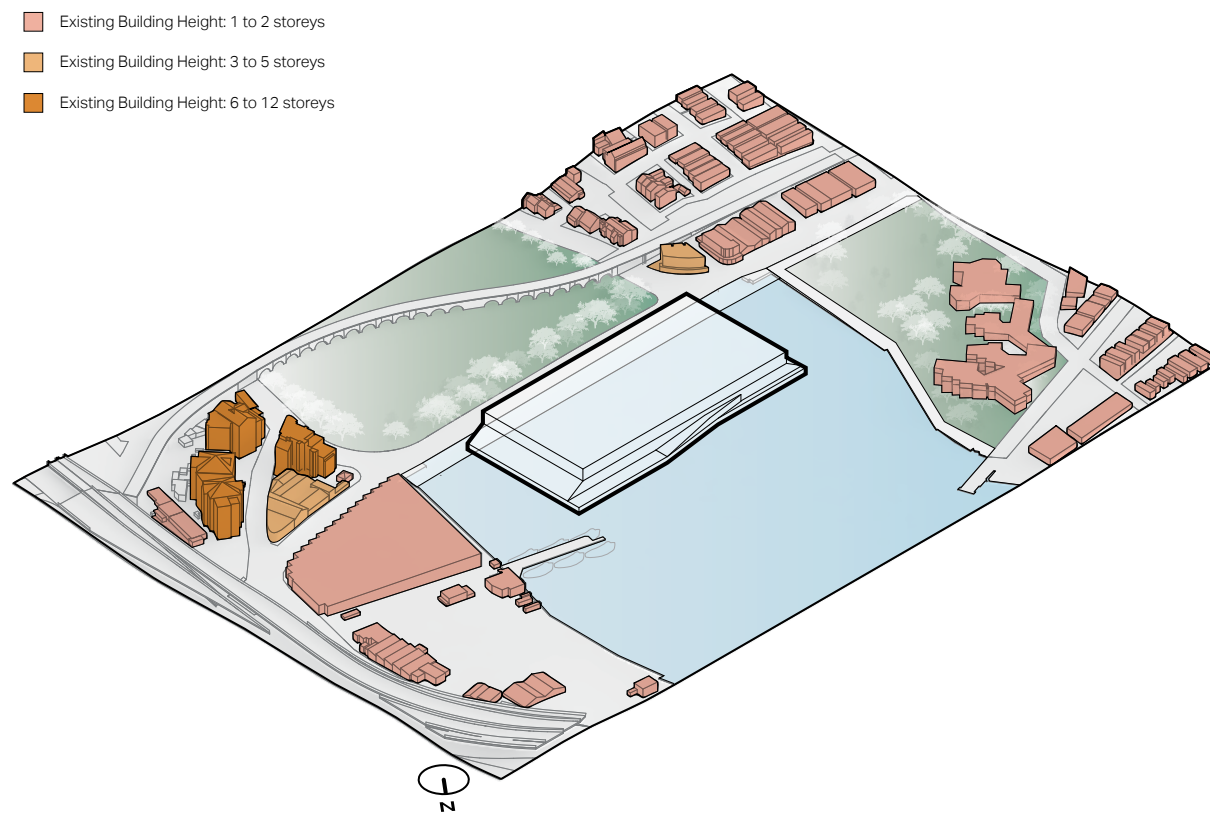


Diagram - Built Form

The built forms to the west of Wentworth Park (Glebe) are made up of small lots with majority of terrace houses at 1 to 2 storeys high with some more recent multi-storey apartment developments. The built form to the east of Wentworth Park (Ultimo and Pyrmont) consist of buildings with larger footprints, mainly in the 3 to 12 storey height range.

The proposed development will consist of 3 levels above ground.

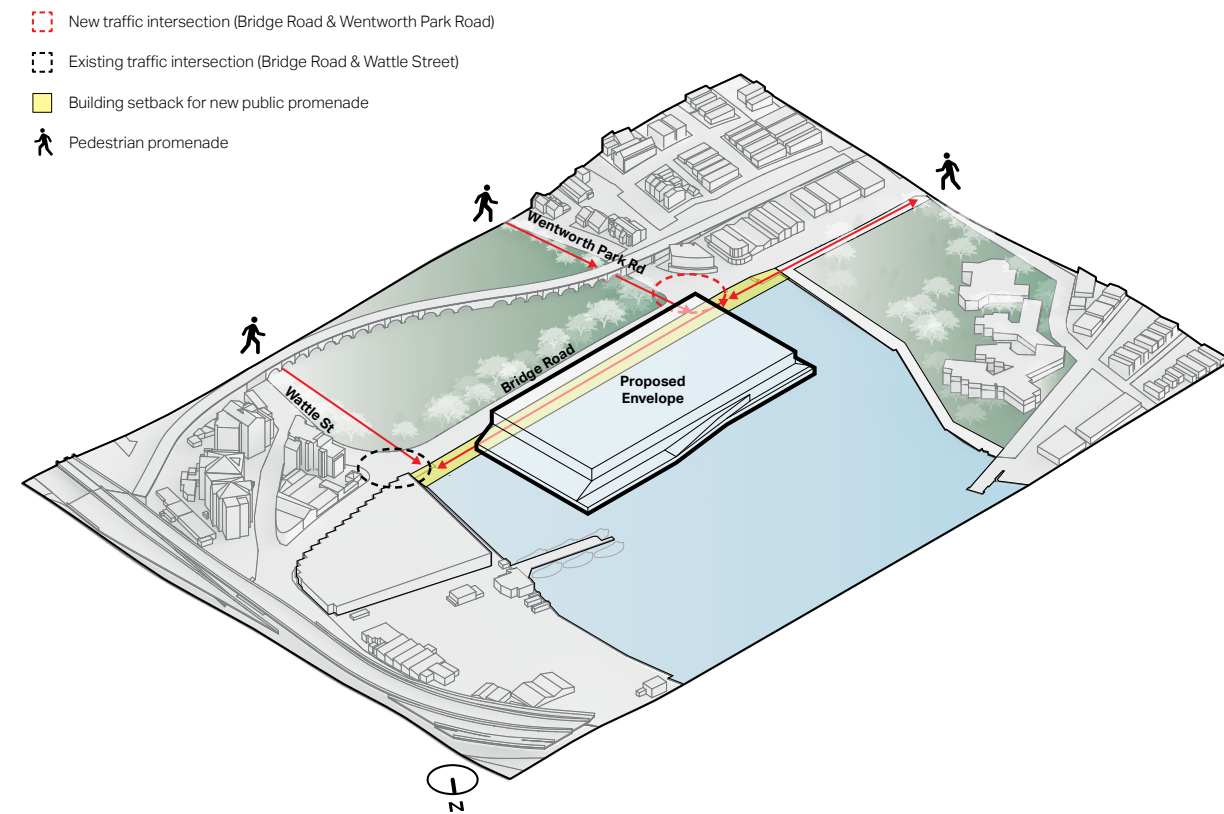


Diagram - Street activation and public accessibility

New signalised crossing at Bridge Road and Wentworth Park Road intersection, and changes to Bridge Road and Wattle Street intersection will be implemented for safe pedestrian crossing and vehicle movements in and out of the proposed development.

2.2 Built Form & Urban Design

Blackwattle Bay - Desired Future Character

2.2 Built Form & Urban Design Analysis

Desired Future Character

The following outlines the desired future character of the proposed development, as a working fish market which also connects people to place and provides a new waterfront destination for the city.



Future oriented, yet authentic Fish Market

A key theme behind creating the new Sydney Fish Market is to allow advancement in design and technology to improve the operability and sustainability of the fish market, while preserving, and even enhancing the essential character of the market itself. Authenticity will be achieved through the sensory and experiential qualities of a bustling industrial fish market. It includes the sounds of competing sellers, the visual connection to the boats hauling their catch, the theatre of the auction in process, the connection to the outdoor environment and the rapid movement of goods.



Continuing Sydney's connection with the water

Sydney is a city celebrated for its connection to the harbour and ocean. A key theme behind the new Sydney Fish Market is to re-establish this connection at Blackwattle Bay, not only visually, but physically in many ways. New sustainability and leisure initiatives offer opportunities for this intersection of land and harbour to educate the public about marine ecology, provide opportunities to contact the water and initiatives to experience an increase in aquatic biodiversity.

2.2 Built Form & Urban Design Analysis

Desired Future Character

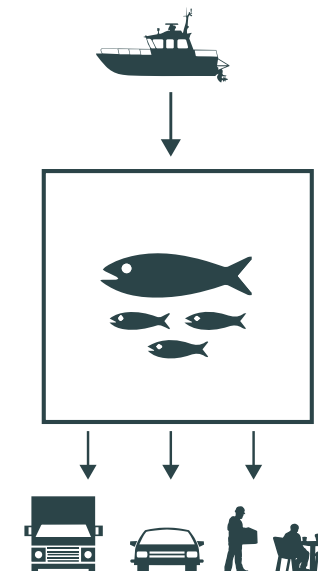
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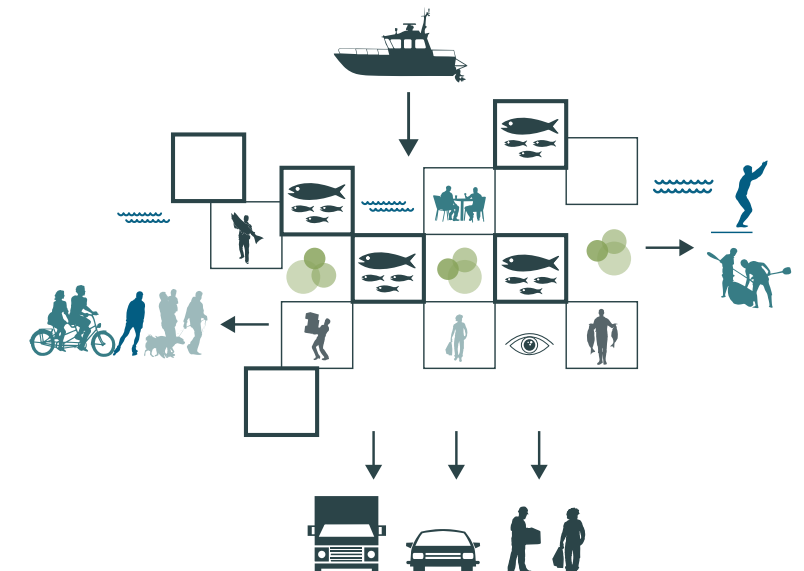
Combining two territories; Operations and public access

The new Sydney Fish Market design will successfully provide public access to the bay while separating them from the dangers of industrial operations and goods handling. Public safety is an essential quality of the design that preserves experiential connectivity between the public and industrial processes while separating their paths.

What is a fish market?



What could a new fish market be?



The new Sydney Fish Market as an urban connector

The new Sydney Fish Market is seen as an opportunity to diversify the functions and experiential character of the fish market. The open, flexible structure with injections of nature, education and leisure experiences will allow more visitors to participate in the activities offered by the harbour and fish market.

2.2 Built Form & Urban Design Analysis

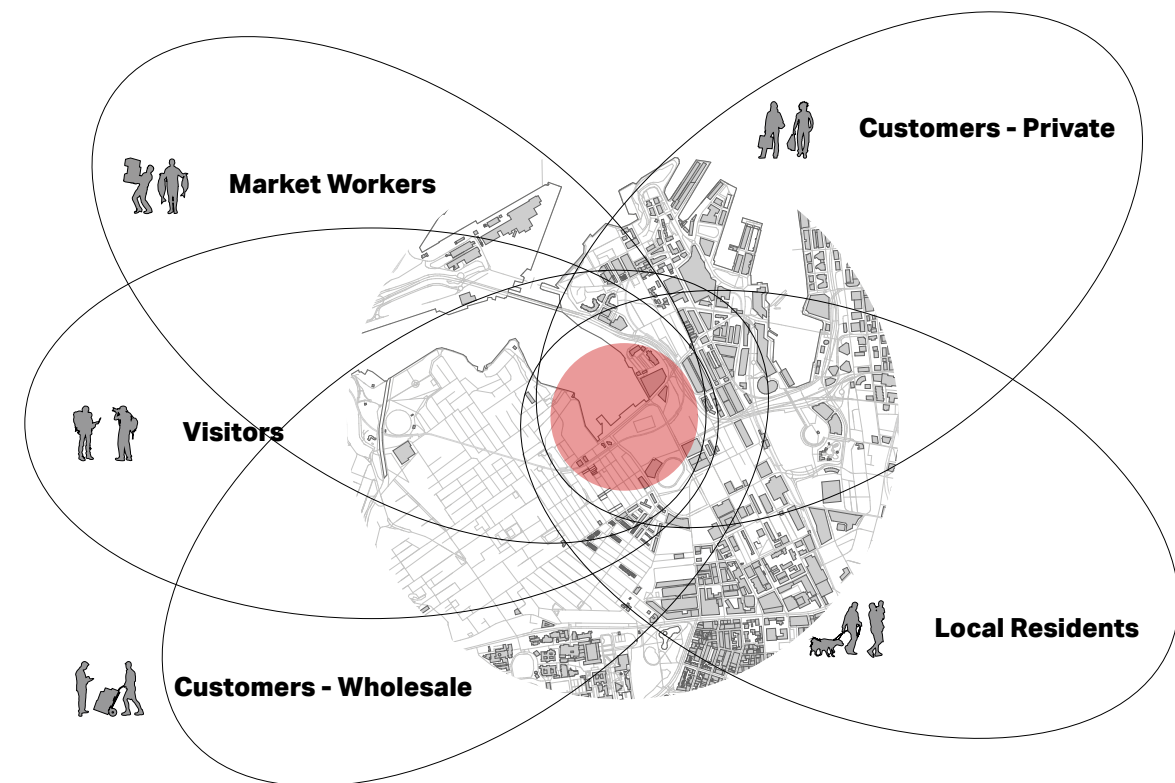
Desired Future Character

The following outlines the desired future character of the proposed development, as a working fish market which also connects people to place and provides a new waterfront destination for the city.



A new public anchor on the Sydney harbour front

Identified as the first development in 7 key actions of the " NSW Government's Bays Precinct Sydney: Transformation Plan (October 2015) ", the new Sydney Fish Market will form the primary connective node for Sydney and the Bays District, which includes Roselle Bay, White Bay, and Glebe Island.



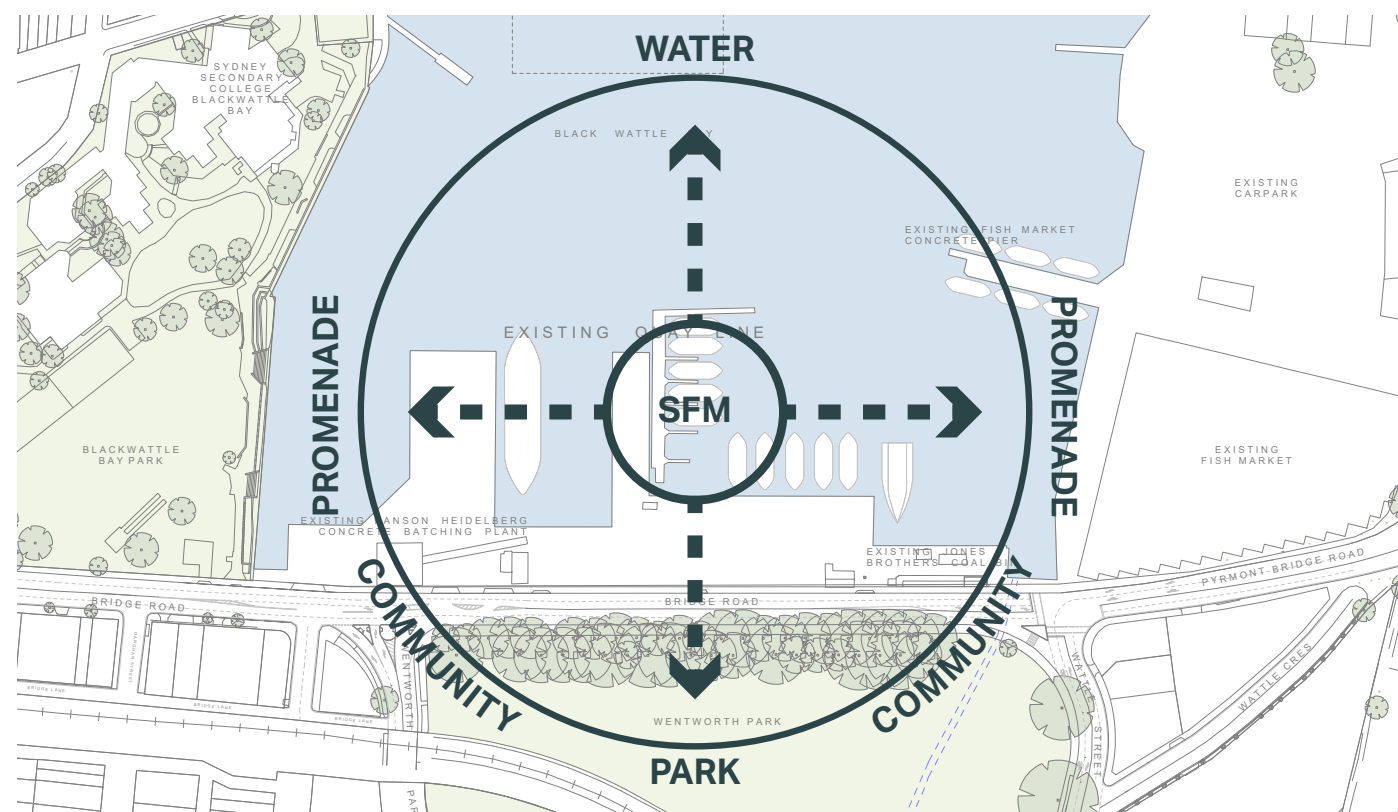
A project that is relevant for different user groups

As an effective anchor, the new Sydney Fish Market design aims to address a variety of user groups, each with specific needs and desires. The open structure of the market design seeks to democratise the space and offer value to each group.

2.2 Built Form & Urban Design Analysis

Desired Future Character

The following outlines the desired future character of the proposed development, as a working fish market which also connects people to place and provides a new waterfront destination for the city.



The new Sydney Fish Market connects the water, park and promenade

A key principle of the new Sydney Fish Market design is to operate as a connective agent, linking the pedestrian promenades developed on either side and linking the park with the water for the benefit of both visitors and the local communities.



Maintain Fish Market authenticity and improve operational efficiency and safety

Various activities such as goods handling, wharf operations, waste handling and food-processing are unfit for public interaction. Other processes such as the auction and wholesale activities may be double programmed or provide some opportunities for crossover. The new Sydney Fish Market design is formulated in such a way as to mediate industrial, semi-public and public activities in a way that is mutually beneficial to all, achieving authenticity through connection and safety through spatial organisation and separation.

2.2 Built Form & Urban Design Analysis

Provide A Variety of Experiences Along The Foreshore



Diagram: Operations and Public - Wholesale and Retail.



Diagram: Operations and Public - Loading Dock and Retail.



Diagram: Tiered Seating.



Diagram: Promenade Boardwalk.

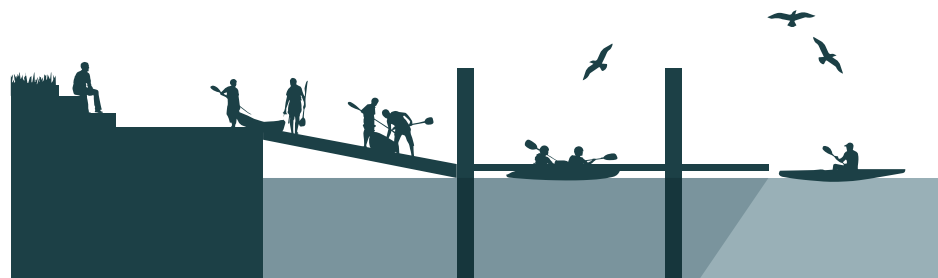


Diagram: Pontoon Platform.

2.3 Built Form & Urban Design

Design Strategy