

125 & 145-175 LAWSON ROAD, BADGERYS CREEK

URBAN DESIGN REPORT

Prepared For

FORMUS PROPERTY PTY LTD

27 August 2025

FINAL



ACKNOWLEDGEMENT OF COUNTRY

Urbis acknowledges the Traditional Custodians of the lands we operate on.

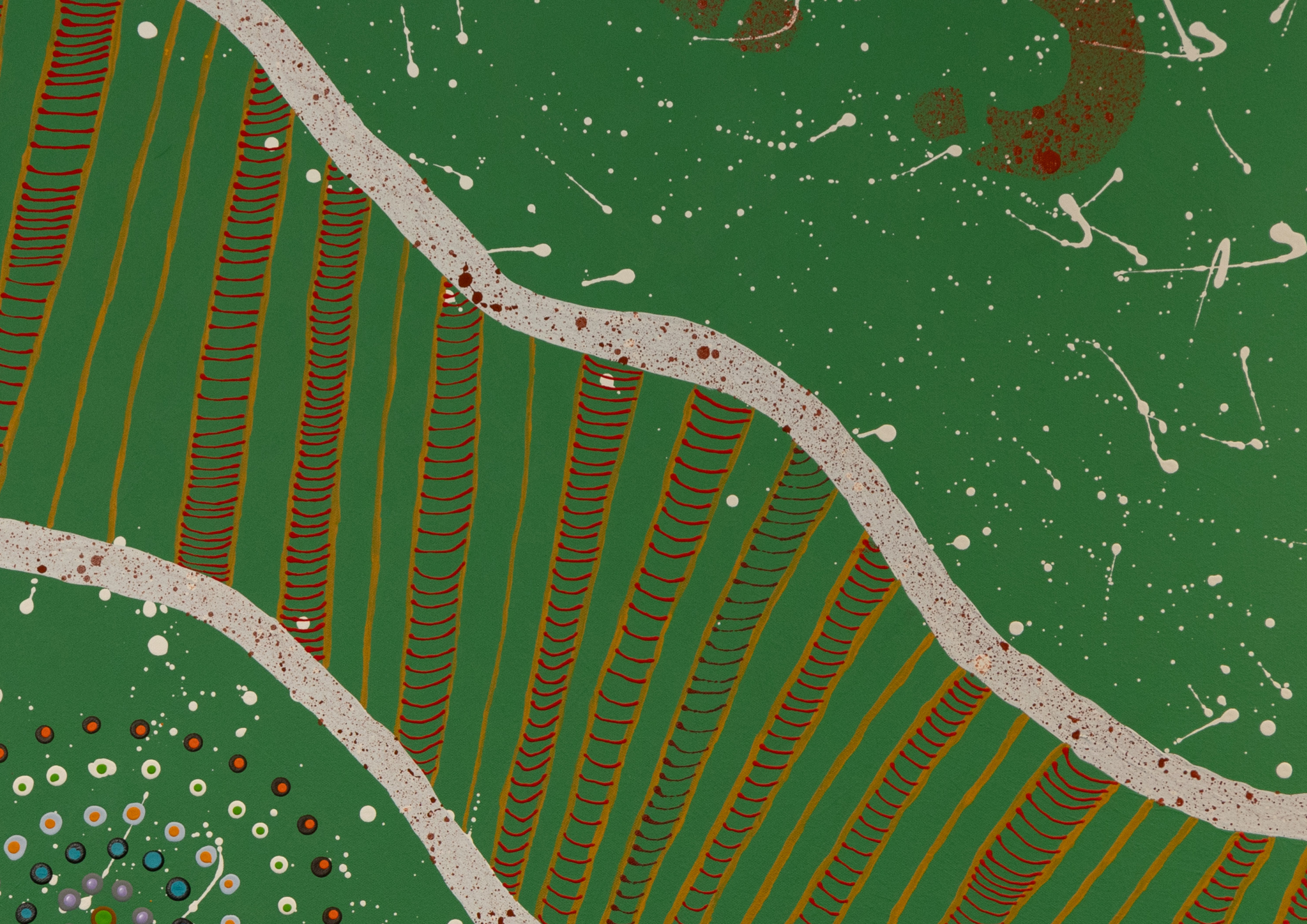
We recognise that First Nations sovereignty was never ceded and respect First Nations peoples continuing connection to these lands, waterways and ecosystems for over 60,000 years.

We pay our respects to First Nations Elders, past and present.

The river is the symbol of the Dreaming and the journey of life. The circles and lines represent people meeting and connections across time and space. When we are working in different places, we can still be connected and work towards the same goal.

Title: Sacred River Dreaming
Artist: Hayley Pigram
Darug Nation
Sydney, NSW





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EXECUTIVE SUMMARY

The site at 125-175 Lawson Road, Badgerys Creek spans approximately 13.9 hectares and is part of the Badgerys Creek Precinct within the Western Sydney Aerotropolis. This location is prioritised for development due to its strategic position and access to utility services.

Formus has engaged Urbis to prepare an Urban Design Report summarising the Master Plan outcomes. The Master Plan underscores the site's development potential by evaluating its context, opportunities, and constraints.

The proposed Master Plan supports the growth of the Western Sydney region by creating employment opportunities through the delivery of warehouses, consistent with the land use plan's intentions. The plan includes a large warehouse (Warehouse 1) of 40,505.5 m² at the southern end and three smaller warehouses at the northern end. The size and scale of Warehouse 1 align with other developments in the Aerotropolis, reflecting the high demand for such facilities in Western Sydney.

A corridor for the future Park Edge Road has been incorporated into the landscape design, with potential long-term realisation, as there are currently no commitments to deliver this road north of the site.

The Master Plan strategically considers an east-west connection to enhance permeability and connectivity, providing safe public access for walking and cycling and activating the creek edge. This approach balances the Enterprise Zone objectives, which advocate for higher-order uses, large-format employment typologies, and freight-capable development outcomes.

Setbacks are broadly consistent with the Precinct Plan, with building setbacks to Lawson Road being approximately double the requirement of the Aerotropolis Development Control Plan (DCP), allowing for a better transition and interface to Lawson Road. Although Pitt Street is assumed to function as a secondary street due to the existing corner lot configuration, a significant building

setback of 16.5 metres has been provided. To support the desired future street character, a continuous 10-metre landscape setback has been included.

The Urban Design Report outlines the intended outcomes for the site, aligning with the objectives of the Aerotropolis Precinct Plan.



Source: SBA



1. INTRODUCTION

1.1 Purpose of this Report

The purpose of this Report is to demonstrate urban design excellence of the proposed Master Plan, which:

- Summarises the local and site context analysis, including the key constraints and opportunities for its future development.
- Illustrates and summarises the key findings of consultants' analysis and advice to inform the development proposal for the development extent.
- Demonstrates and reinforces the merits of the proposed Master Plan and Landscape Design.

1.2 Background Studies

Technical studies undertaken as part of this process to inform this urban design report, include the following:

- Connecting with Country Framework - Ngurra
- Aboriginal Cultural Heritage Assessment Report (ACHAR) - City Plan
- Aviation Impact Assessment - AVLAW
- Arboricultural Impact Assessment - Heartwood
- Land Contamination Assessment - EGA
- Geotechnical Assessment - EGA
- Wildlife Hazard Assessment and Biodiversity Management Plans - Eco Logical
- Noise and Vibration Impact Assessment - SLR
- Bush Fire Assessment - Blackash
- Flood Impact and Risk Assessment - AT&L
- Architectural Drawings and Design Report - SBA
- Landscape Report - Urbis
- Visual Impact Assessment - Urbis
- Environmental Impact Assessment - Urbis

1.3 Report Structure

For ease of understanding, the report is divided into 9 Key sections:

Section 1: Introduction

Provides an overview of the development extent and location. It also includes a response table to the SDRP advice.

Section 2: Connecting with Country

Provides an overview of the community engagement process and the principles established .

Section 3: Planning Context

Provides an overview of the strategic planning and planning controls applied to the development extent.

Section 4: Place Analysis

Collates the key findings from the technical studies to inform a consolidated constraints and opportunities map.

Section 5: Key Directions

Vision and guiding principles that inform the Master Plan.

Section 6: Master Plan

Provides an overview of the proposed Master Plan.

Section 7: Design Directions

Provides high-level design directions to guide the Master Plan.

Section 8: Master Plan Features

Discusses the master plan features that contribute to place making.









Section 9: Response to Precinct Plan

Provides an overview of how the proposed Master Plan responds to the Precinct Plan.

1.4 Strategic Context

The site is located approximately 40km southwest of Parramatta CBD, and 63km southwest of Sydney CBD, and 12km west of Bradfield.

LEGEND

-  The Site
-  Trade Gateway
-  Economic Corridor
-  Agribusiness Area
-  Strategic Centre
-  Train Line
-  Metro Line
-  Freight Line
-  Motorway

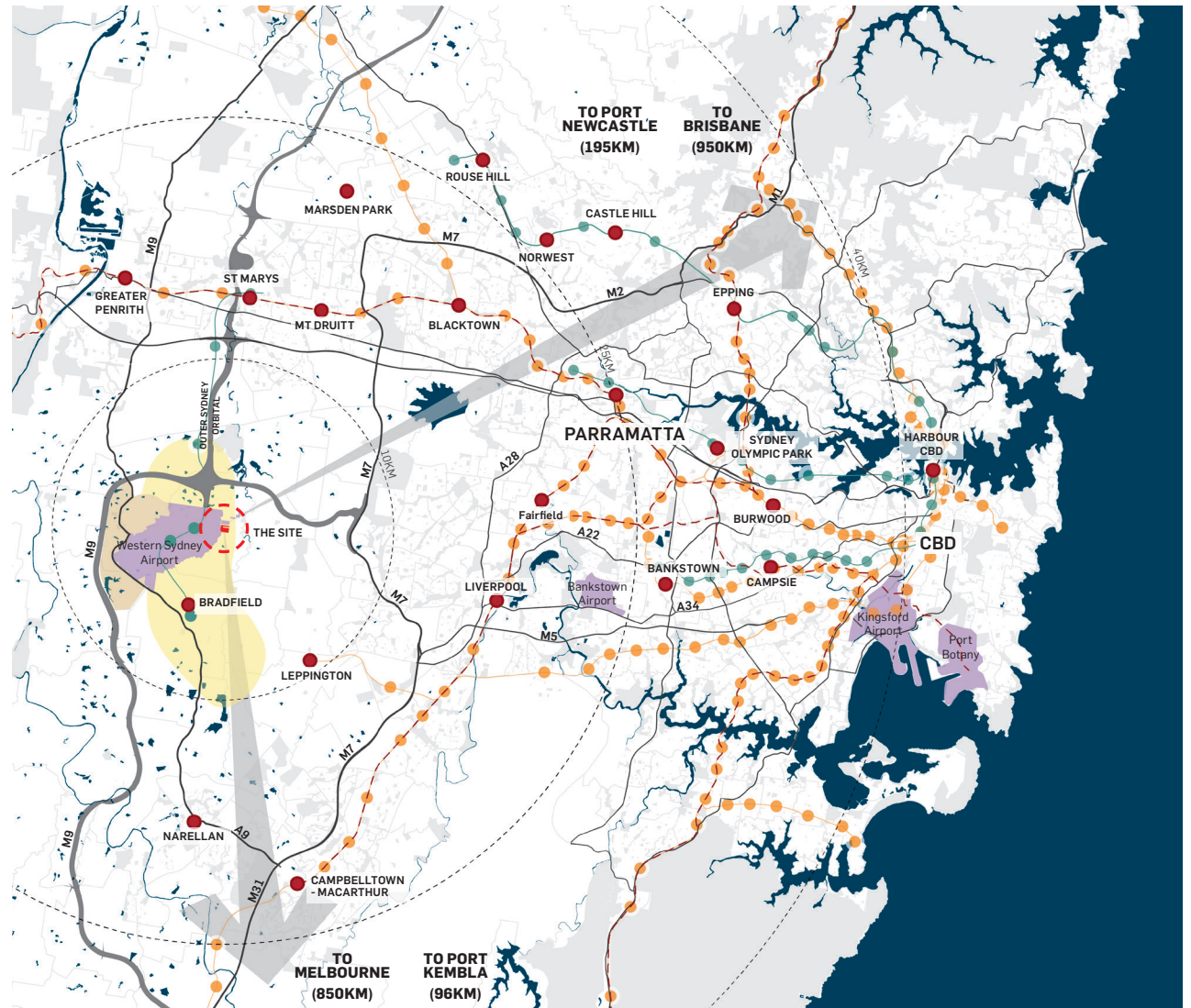






Figure 1 Strategic Context Map

1.5 Aerotropolis Context

The site is located within the Badgerys Creek Precinct, one of the five precincts in the Western Sydney Aerotropolis Precinct Plan. One of the objectives of this Plan is to transform lower density and less intensive land uses, buildings and structures to higher order employment-focused technology, advanced manufacturing and industry uses.

Therefore, the land use of the Badgerys Creek Precinct will focus on a range of employment generating uses that will also benefit from proximity to the Western Sydney Airport. The site also shares a boundary (western boundary) and will be a key location for employment uses.

LEGEND

-  Site Boundary
-  Wianamatta South Creek Non-initial Precincts
-  Land to which the Precinct Plan Applies
-  Precinct Boundary

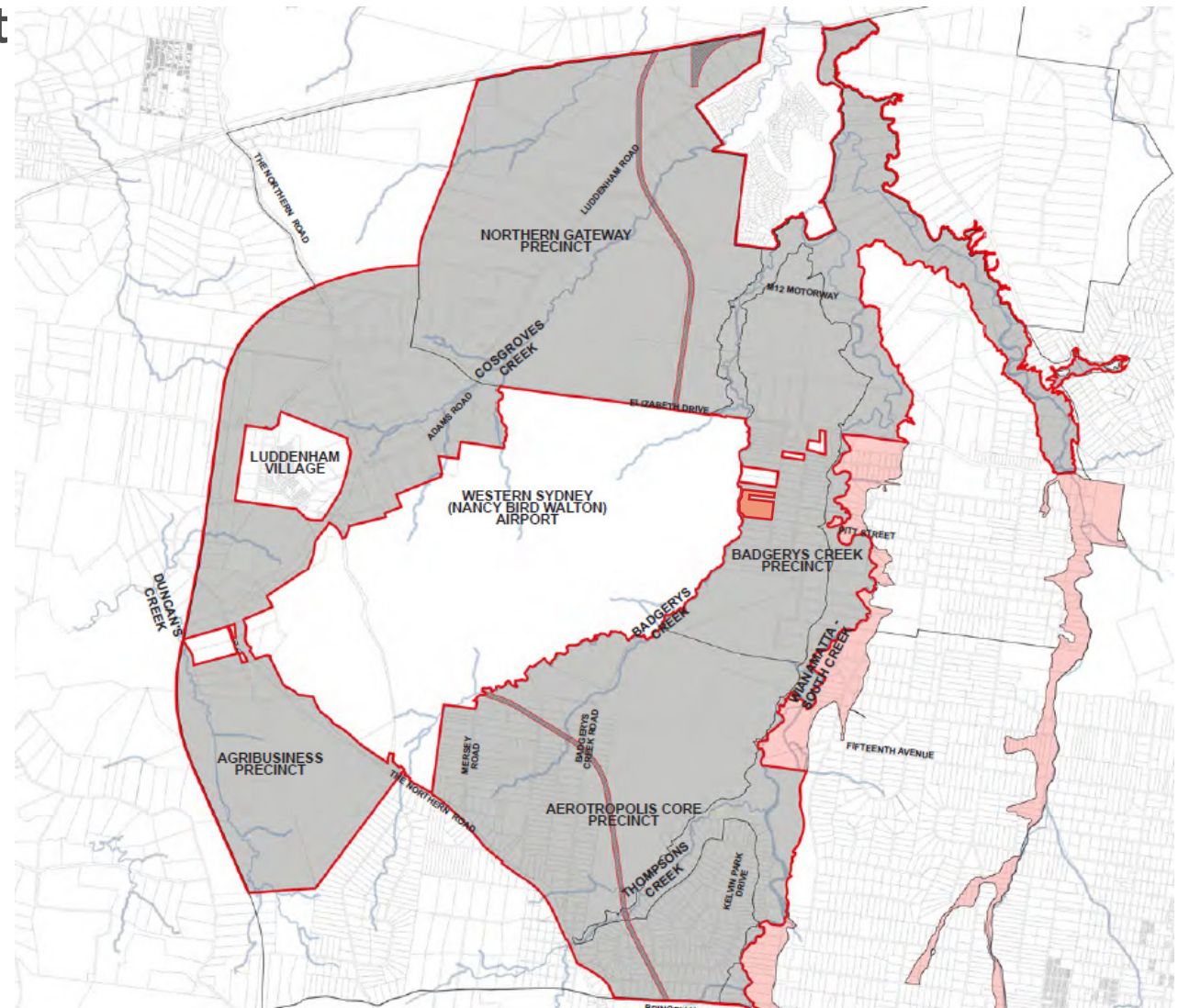


Figure 2 Site Location Map (Overlaid on Five Precincts Plan of the Precinct Plan)

1.6 Site Description

The subject site occupies 13.9ha and is located within the Badgerys Creek Precinct.

The site comprises five (5) lots which are described as Lots 1, 3, 4, 5, and 6 in DP226912.

The site is bounded by:

- Lawson Road to the east
- Pitt Street to the south
- Western Sydney Airport to the west

Immediate Context

The site shares a boundary with the Western Sydney Airport and is located approximately 900m away from Elizabeth Drive.

To the north, 135 Lawson Road has a development application that was refused by the Liverpool City Council Planning Panel.

Current Use

The site has been historically used for rural residential purposes.

Land Form Characteristics

The site is relatively flat and gradually slopes down towards Badgerys Creek. The site contains a high point along Lawson Road.

Zoning

The site is zoned as an Enterprise zone.

LEGEND

- Site Boundary
- x Site Photo Locations



Figure 3 Aerial Map of the site

1.7 Site Photos



1.8 SDRP Response Table

The NSW State Design Review Panel (SDRP) provided an initial advice letter dated 4 June 2025 with advice and recommendations on the proposed master plan. The following pages illustrate the responses to the queries raised by the SDRP in relation to urban design at the first meeting.

SDRP RESPONSE TABLE			
SDRP COMMENT	FINAL RESPONSE	REFERENCE PAGES	
Site strategy and landscape			
4. Consider shifting the main warehouse westward to allow for a larger setback along Lawson Road with substantial planting.	The setback along Lawson Road not only meets but exceeds the requirements fronting Warehouse 1, as outlined in the Aerotropolis DCP. A 10m landscaped setback has been established to accommodate a landscape buffer planting. While the required building setback is 12m, a generous 23.5m setback has been provided. This additional space includes parking areas, offering further opportunities for interspersed tree planting.	Refer to page 71 and the Landscape Report	
5. The proposed reduction in setback along Pitt Street is not supported. A minimum of 20 metres building setback should be maintained following the development of the Pitt Street sub-arterial to ensure sufficient space for planting, active transport, and visual amenity.	The proposed master plan provides a 16.5m setback along Pitt Street which includes a 10m landscape setback. Given that the site is a corner lot, Lawson Road has been treated as the primary frontage and Pitt Street is considered a secondary frontage which has the requirements of a 5m building setback and a 3m landscape setback. However, to maintain the intended character of the sub-arterial road, the 10m setback is maintained. Additionally, this 16.5m setback is provided in addition to the 21.65m allocated for road widening. The building line has been established to avoid any conflict with the future road corridor. The sub-arterial road will include planting and active transport links.	Refer to page 72 and the Landscape Report	
6. Ensure the Pitt Street setback and planting support the development of an ecological corridor from Badgerys Creek, along Cuthel Road, toward South Creek.	The reduction of the setback applies to the building only and the 10m landscape setback has been maintained throughout the length of Pitt Street. This landscape setback is embellished with planting, a linear swale and meandering pathways to encourage biodiversity and pedestrian movement.	Refer to page 72 and the Landscape Report	

SDRP RESPONSE TABLE		
SDRP COMMENT	FINAL RESPONSE	REFERENCE PAGES
<p>8. Provide an east-west road, as indicated in the Precinct Plan, that enables long-term public access to the future Creek Edge Road and enhance the site's integration with the broader public realm.</p> <p>Consider an adaptation of 'Option A', which shows a east-west 'private road'.</p> <p>Explore opportunities for the road to enhance the site's connectivity and provide direct pedestrian access for workers to the creek on site.</p>	<p>The master plan for the site has strategically considered the option for an east-west connection and the primary objective of permeability and connectivity, to provide safe public access (walking and cycling) and activation of the creek edge. Importantly, this has been balanced against the Enterprise Zone objectives which advocates for higher order uses, large-format employment typologies and freight capable development outcomes.</p> <p>Introducing any local road(s) connecting with Lawson Road, and in proximity to the Pitt St/ Lawson Rd intersection, is not considered a desirable outcome from traffic planning/ modelling perspective.</p> <p>Surplus local roads would introduce additional hardstand with increased setbacks, internal driveways, carparks etc. Furthermore it would increase conflict between freight movements and pedestrians/ cyclists conflicting with the strategic intent to promote connectivity and permeability.</p>	<p>Refer to page 67, Landscape Report and Architectural Design Report</p>

A second meeting with the SDRP was held on the 20th of August to recap on the matters set out in SDRP 1 and provided further feedback. As a result of the SDRP 2 meeting, further refinements have been made including additional pedestrian movements from carparks, permeable pavements, refined fencing location and canopy cover. Refer to the EIS for a detailed response.



2. CONNECTING WITH COUNTRY

2.1 Community Engagement

Ngurra's approach to community engagement for the 125 & 145 – 175 Lawson Road Badgerys Creek development is grounded in a deep respect for the Dharug people.

The consultations to date have been instrumental in shaping the themes of this project, ensuring that the voices, perspectives, and cultural knowledge of the local First Nations community, particularly the Traditional Custodians, are meaningfully embedded in the design process. Through ongoing collaboration with the Custodians and the project team, these engagements have created opportunities to listen, learn, and articulate key ideas that reflect both cultural values and contemporary needs. This process has fostered a shared understanding of place, connection, and responsibility, forming a strong foundation for the design principles and considerations that will guide the development.

Building on the insights and considerations shared by the community, a Design Principles Framework has been developed and validated in close collaboration with the local First Nations community. This foundational document outlines core design principles and themes, grounded in the cultural responsibilities, aspirations, and needs of the Dharug people. The framework ensures the project remains true to their enduring connection to Country and serves as a guiding reference throughout the entire life cycle of the development. These community insights have enabled the architectural team to respond with designs that respectfully reflect cultural values and community aspirations, embedding integrity, sustainability, and respectful engagement at every stage of the project.

Source: Ngurra Advisory

1 Walk on Country with the Project Team

A culturally led Walk on Country was held with Traditional Custodians Aunty Julie Jones, Matt Morrison and the project team, providing a foundational understanding of the site's cultural significance and the Dharug people's enduring connection to Country.

2 Visioning Workshop with Traditional Custodians and Project Team

A collaborative workshop was held with Traditional Custodians Aunty Julie Jones, Aunty Justine Coplin and the project team to explore cultural values, aspirations, and opportunities for embedding these into the project's vision.

3 Development of the Connecting with Country Design Framework

Grounded in community insights and cultural knowledge, the Connecting with Country Design Framework outlines core principles and design approaches for engaging meaningfully with Country. The First Nations Working Group will guide its implementation to uphold cultural integrity and ensure accountability.

4 Validation Workshop with Traditional Custodians and Project Team

A follow-up workshop was conducted to review and validate the Design Themes Framework, ensuring it accurately reflects cultural priorities and is supported by the community. Drawing from community insights and cultural knowledge, four

core design themes were developed to guide the design approach: Night Sky Country, Brown Snake Country, Water as Memory, and Regeneration of Cumberland Plain Woodland.

5 Next Steps In Community Engagement

The next phase of engagement will involve establishing a First Nations Working Group. This group will play a central role in overseeing the implementation of the Design Principles Framework, ensuring cultural integrity and community priorities are upheld throughout the project. Comprised of Traditional Custodians, Elders, and key representatives, the Working Group will provide cultural guidance, support decision-making, and ensure that the project remains grounded in the values, knowledge systems, and aspirations of the Dharug community.

6 Future Engagement

As the project evolves, the project team is committed to continuing engagement with the wider Dharug community. This includes;

- Broader Community Consultation: To ensure diverse Dharug voices are heard.
- Specialist Aboriginal Consultants, such as:
 - Language experts for naming and interpretation
 - Local artists to lead public art and storytelling features
 - Cultural knowledge holders to advise on heritage, water, landscape, and healing.

2.2 Design Themes

The built form and landscape vision has been driven by the Connecting with Country Framework, Walk on Country and following design workshops facilitated by Ngurra Advisory with community. The following design themes and considerations have been developed by Ngurra Advisory to be incorporated where possible into the design.

NIGHT SKY COUNTRY AND LIVING CREEK



Celebrate the deep spiritual and cultural connection the Dharug (South Creek Wianamadagal and Cabrogal) people hold with the land, sky, and waterways, particularly the creek system and the concept of "Night Sky Country," where constellations, ceremony, and natural wayfinding intersect. The creek is not only a physical feature but also a cultural pathway and holder of stories.

BROWN SNAKE COUNTRY AND LOCAL WILDLIFE



Acknowledge and reflect the cultural significance of Marragawan—the brown snake and the broader relationship between the Dharug (South Creek Wianamadagal and Cabrogal) people and the local non-human kin that inhabit the site.

Recognise the land as a shared living space, where animals, plants, and people coexist in a delicate balance guided by cultural lore, respect, and observation.

Source: Ngurra Advisory

WATER AS MEMORY, LIFE, AND CONNECTION



Honour water as a living, cultural presence one that holds memory, sustains life, and connects people across generations and geographies. For the Dharug (South Creek Wianamadagal and Cabrogal) people, the creek is not just a waterway, but a storyline, a place of ceremony, refuge, and knowledge transfer.

REGENERATING CUMBERLAND PLAIN WOODLAND



Restore and celebrate the ecological and cultural richness of the Cumberland Plain Woodland, an endangered ecosystem that once thrived across Dharug Country.

Recognise the deep knowledge Dharug people hold about local plant species and their use in weaving, medicine, ceremony, and daily life.

Source: Ngurra Advisory

3. PLANNING CONTEXT

This chapter of the report provides a summary of the key planning directions for (the site) as set out by the relevant planning documents.

3.1 Strategic Planning Context

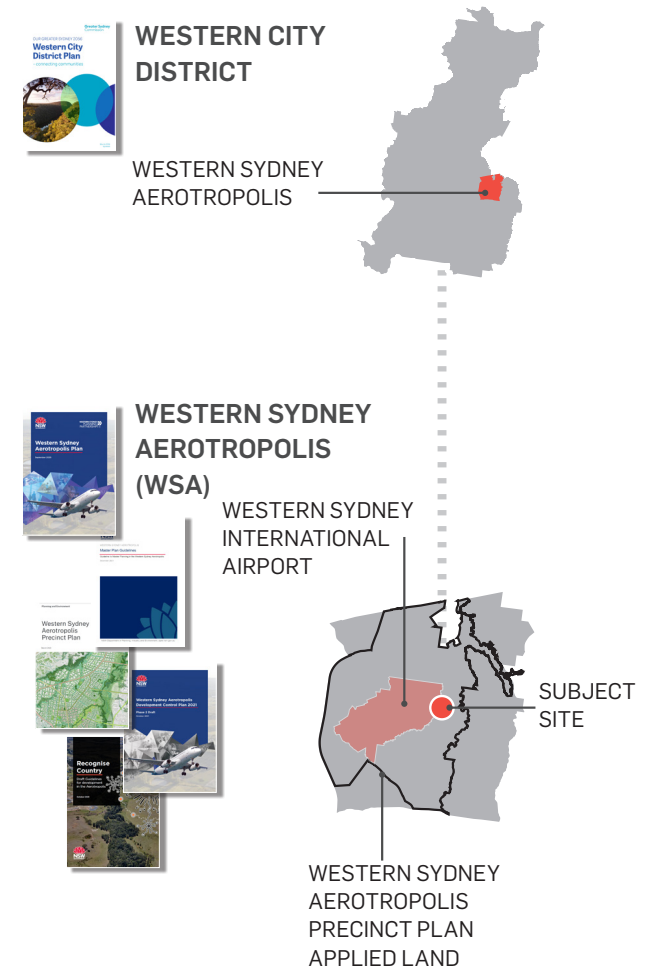
Planning in NSW is underpinned by a series of cascading strategic planning documents which align land use, transport and infrastructure between three tiers of government and across State agencies for the first time in a generation.

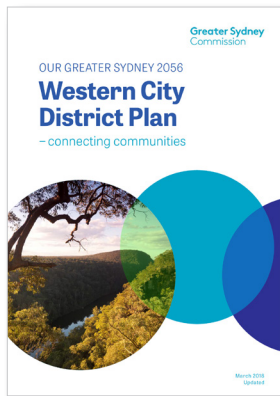
Together these documents set out the existing and future context along with development and design considerations to inform future development outcomes on a site.

Aerotropolis Planning Framework

A review of strategic planning requirements for the development extent identified the following plans, strategies and requirements are relevant to the site:

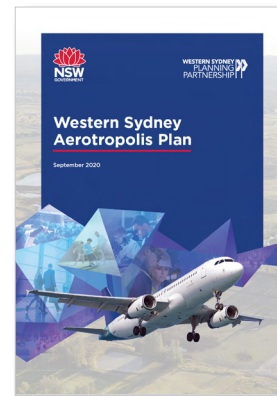
- Western City District Plan
- Western Sydney Aerotropolis Plan 2020
- Western Parkland City SEPP
- Western Sydney Aerotropolis Precinct Plan 2024
- Western Sydney Aerotropolis Development Control Plan 2022-Amendment 1





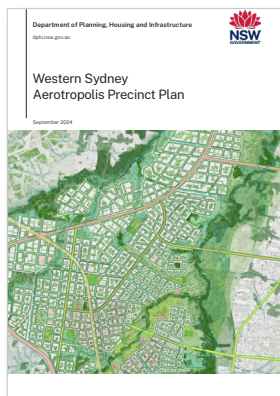
Western City District Plan

Western City District Plan is a 20-year strategic planning document published by the Greater Sydney Commission. It unlocked the planning priorities and actions to achieve connectivity, liveability, productivity, and sustainability. Western Sydney International Airport-Badgerys Creek Aerotropolis has been identified as one of the metropolitan clusters in the district plan and an economic catalyst for Western Parkland City. It will enable to support the well-connected city - a 30-minute city initiative.



Western Sydney Aerotropolis Plan 2020

The Western Sydney Aerotropolis Plan was published in 2020 by the Western Sydney Planning Partnership. It sets the planning framework for the Western Sydney Aerotropolis with the vision, 11 objectives, and 50 principles for the Aerotropolis.



Western Sydney Aerotropolis Precinct Plan 2024

Western Sydney Aerotropolis Precinct Plan (the Precinct Plan) has been endorsed by DPHI in September 2024. The Precinct Plan is applied to five precincts within the Aerotropolis, providing place-based visions, objectives, and setting out the finer grain detail to support the Aerotropolis SEPP.



Western Sydney Aerotropolis Development Control Plan 2022

As a supplementary document to the Aerotropolis planning framework, the DCP Phase 2, Amendment 1 aims to provide detail guidelines to master plans and DAs in the Aerotropolis area with the objectives, performance outcomes and benchmark solutions.

3.2 Western Sydney Aerotropolis Precinct Plan (2024)

The subject site is located within the Badgerys Creek Precinct, one of the five precincts focused in the Western Sydney Aerotropolis Precinct Plan.

Western Sydney Aerotropolis Precinct Plan (Precinct Plan) 2024 provides the detail guidelines and criteria for the overall vision for five precincts of the Aerotropolis including Aerotropolis Core, Badgerys Creek, Wianamatta-South Creek, Northern Gateway and Agribusiness.






BADGERYS CREEK

The Badgerys Creek Precinct will transform from lower density and less intensive land uses, buildings and structures to higher order employment-focused technology, advanced manufacturing and industry uses with the opportunity for between 9,000 – 11,000 jobs (estimated by the WSAP).

Land use of the Badgerys Creek Precinct will focus on a range of employment generating uses that will benefit from proximity to the Western Sydney Airport.

The proposed development is estimated to provide additional jobs during the operational phase which further supports the objectives of the Precinct Plan.

LEGEND

-  Site Boundary
-  Land Application Boundary
-  Land to which the Precinct Plan Applies
-  Precinct Boundary
-  Property Boundary

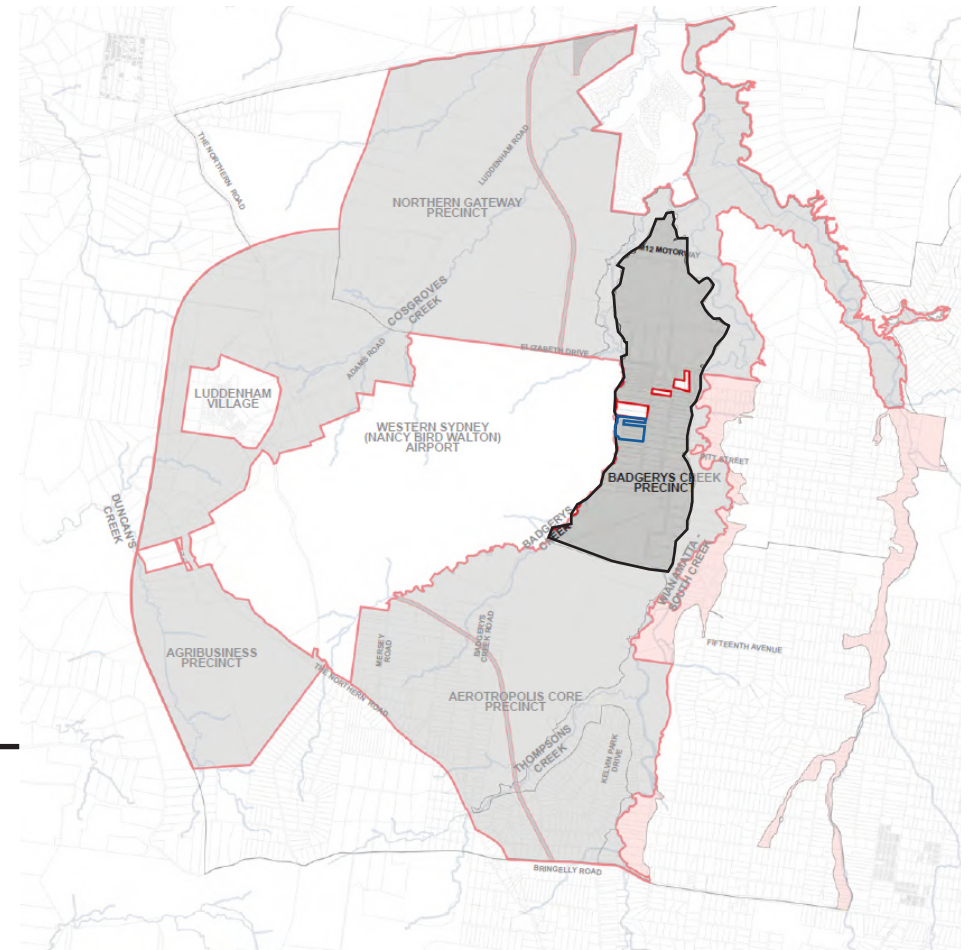


Figure 4 Five Precincts within Aerotropolis which the Precinct Plan Applies

Source: Precinct Plan








3.2.1 Future Surrounding Uses

The site has been identified for employment including enterprise, business and industry uses. Along the western boundary, land has been identified under WSA proposed biodiversity links.

KEY TAKEAWAY

Opportunity to deliver on the increased demand of warehouses in Western Sydney.

LEGEND

-  Site Boundary
-  Creek
-  Metro Line
-  Western Sydney International Airport (WSA)
-  Business Development / Aviation Support Facilities / Parking (within WSA)
-  Employment (Enterprise/Business/Industry)
-  WSA Proposed Biodiversity Links

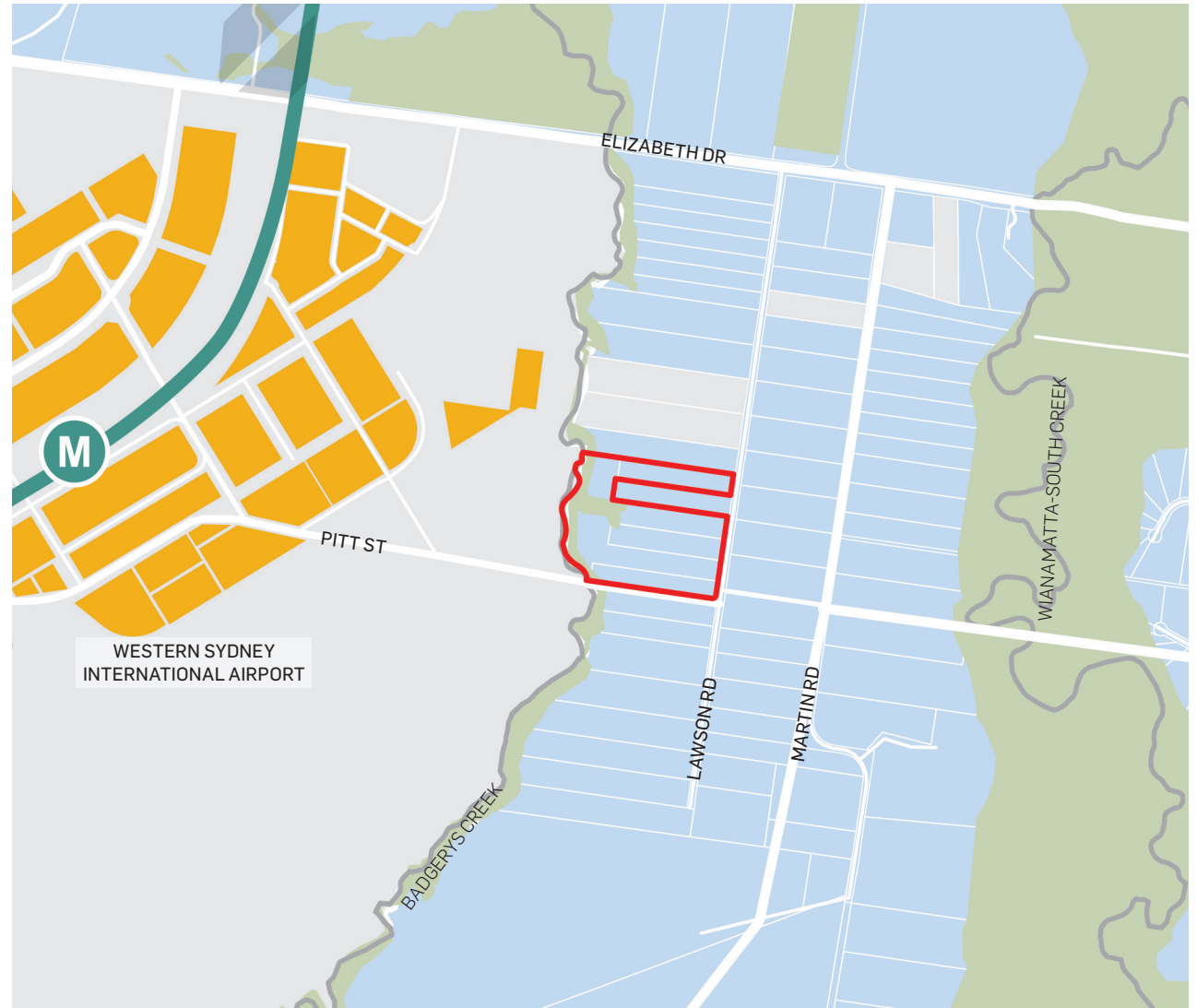


Figure 5 Future Surrounding Uses Plan (as per the Precinct Plan)

3.2.2 Street Hierarchy

The Precinct Plan proposes that:

Pitt Street: is a proposed sub-arterial road to enhance the east-west connection from the Western Sydney International Airport to Badgerys Creek Local Centre.

Lawson Road: is a proposed collector road with local bus corridor.

There are additional local streets identified within the site, however as per the Precinct Plan they are indicative only. A park edge street has been identified to the west. The delivery of this road will be dependent on the outcomes on the WSA sites.

KEY TAKEAWAY

Vehicular access only available from Lawson Road.

LEGEND

- Site Boundary
- Creek
- Western Sydney International Airport (WSA)
- M Metro Line
- Primary Arterial Road
- Sub Arterial Road
- Collector Road
- Park Edge Street
- Local Street
- Key Signalled Intersection
- Key Signalled Intersection (subject to investigation)

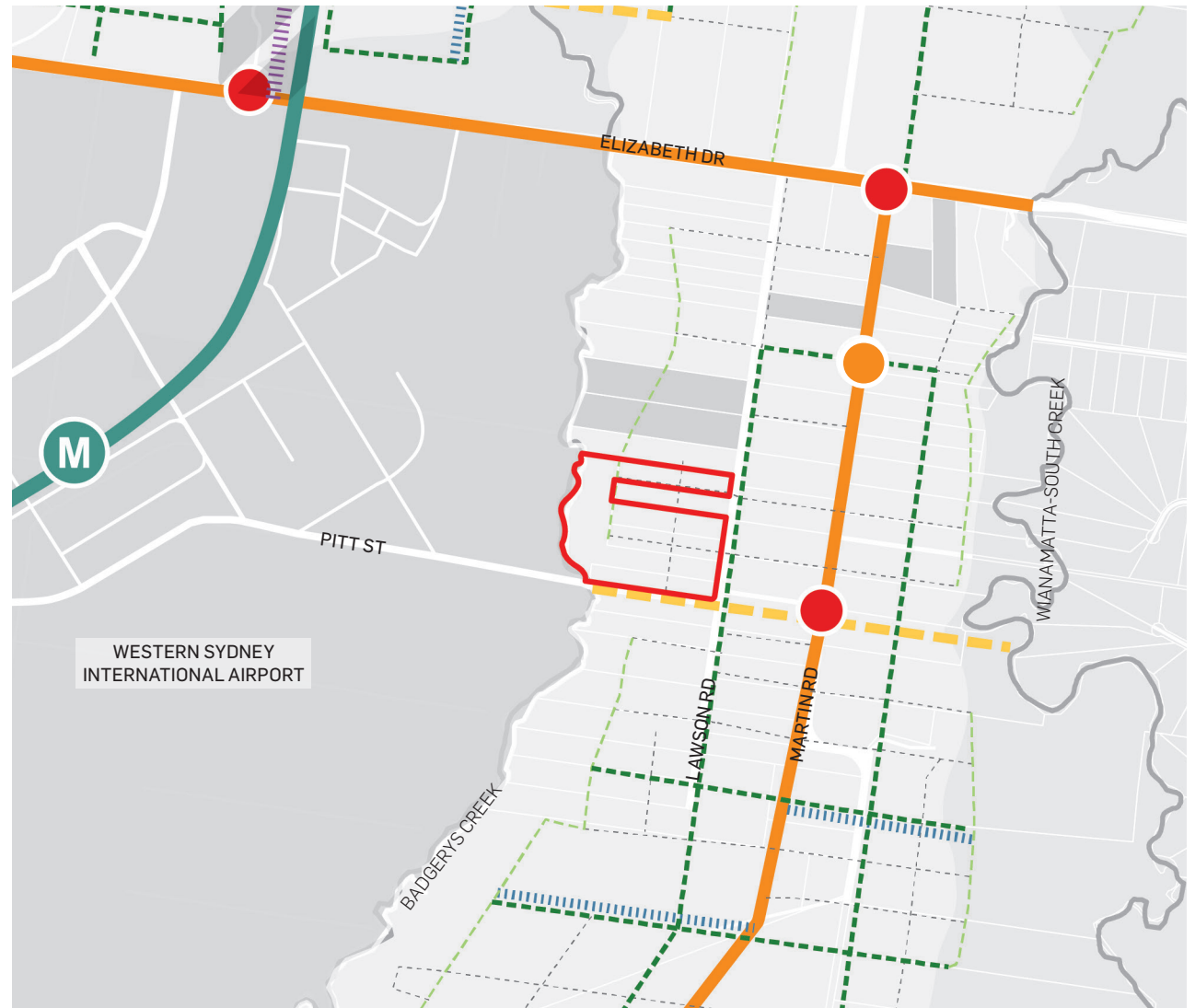


Figure 6 Street Hierarchy Plan (as per the Precinct Plan)



3.2.3 Total Water Cycle Management

Stormwater infrastructure has been identified along the site's western boundary to manage land affected by 1% AEP flood inundation.

KEY TAKEAWAY

Opportunity to consolidate basin design to ensure amenity and landscape outcomes.

LEGEND









-  Site Boundary
-  Western Sydney International Airport
-  Creek
-  Riparian Corridors
-  Riparian Streets
-  Stormwater Infrastructure
-  1% AEP Flood
-  Water body



Figure 7 Water Cycle Management Plan (as per the Precinct Plan)
















3.2.4 Active and Public Transport Network

A principle regional cycle path runs north-south through the site adjacent to the open space and a cycle path on collector road runs along the Lawson Road frontage. There is a local bus route which runs along Lawson Road and a rapid bus corridor which runs eastward along Pitt Street from Martin Road towards the WSA.

KEY TAKEAWAY

There is opportunity for the principal regional cycle path to be integrated with open space and to provide views to Badgerys Creek.

LEGEND

-  Site Boundary
-  Creek
-  Western Sydney International Airport
-  Metro Line
-  Principle Regional Cycle Path
-  Cycle Paths on Collector Roads
-  Rapid Bus Corridor
-  Frequent Bus Corridor
-  Indicative Local Bus Network
-  Key Signalled Intersection
-  Key Signalled Intersection (subject to investigation)
-  Primary Arterial Road
-  Sub-arterial Road

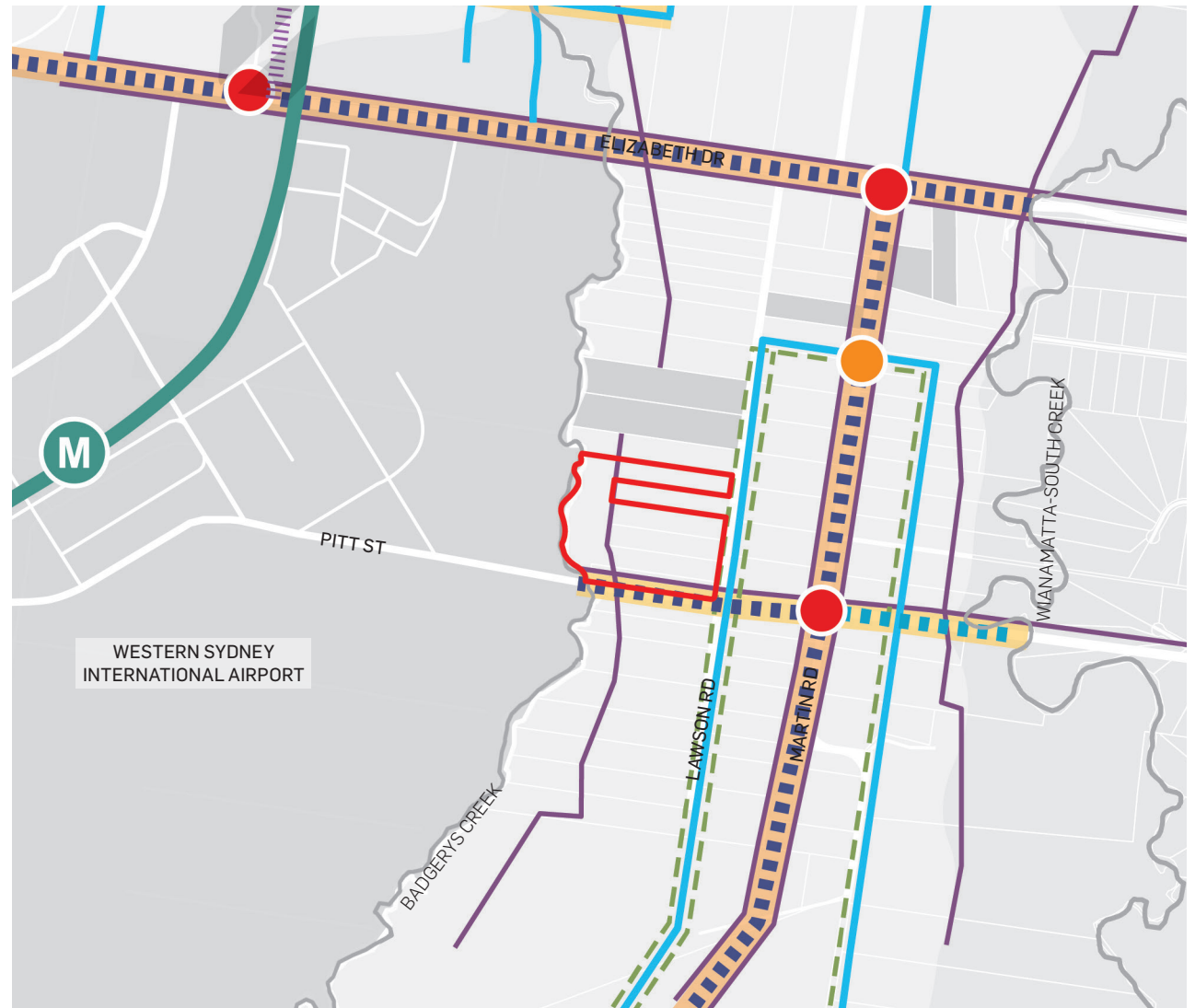


Figure 8 Active & Public Transport Network Plan (as per the Precinct Plan) 1:20,000 @ A4










3.2.5 Biodiversity and Riparian Corridors

There is one major riparian corridor which runs along the site's western boundary, following the Badgerys Creek alignment along the eastern WSA boundary. The corridor provides an environmental buffer between the Badgerys Creek Precinct and the WSA. Existing sensitive vegetation and open spaces are integrated with the riparian corridors.

KEY TAKEAWAY

Enhance connectivity to the surrounding natural features through open spaces and active transport corridors.

LEGEND

-  Site Boundary
-  Western Sydney International Airport
-  Open Spaces
-  WSA Proposed Biodiversity Links
-  High Biodiversity Value Area
-  Creek
-  Riparian Corridors
-  Scenic Protection
-  Water body

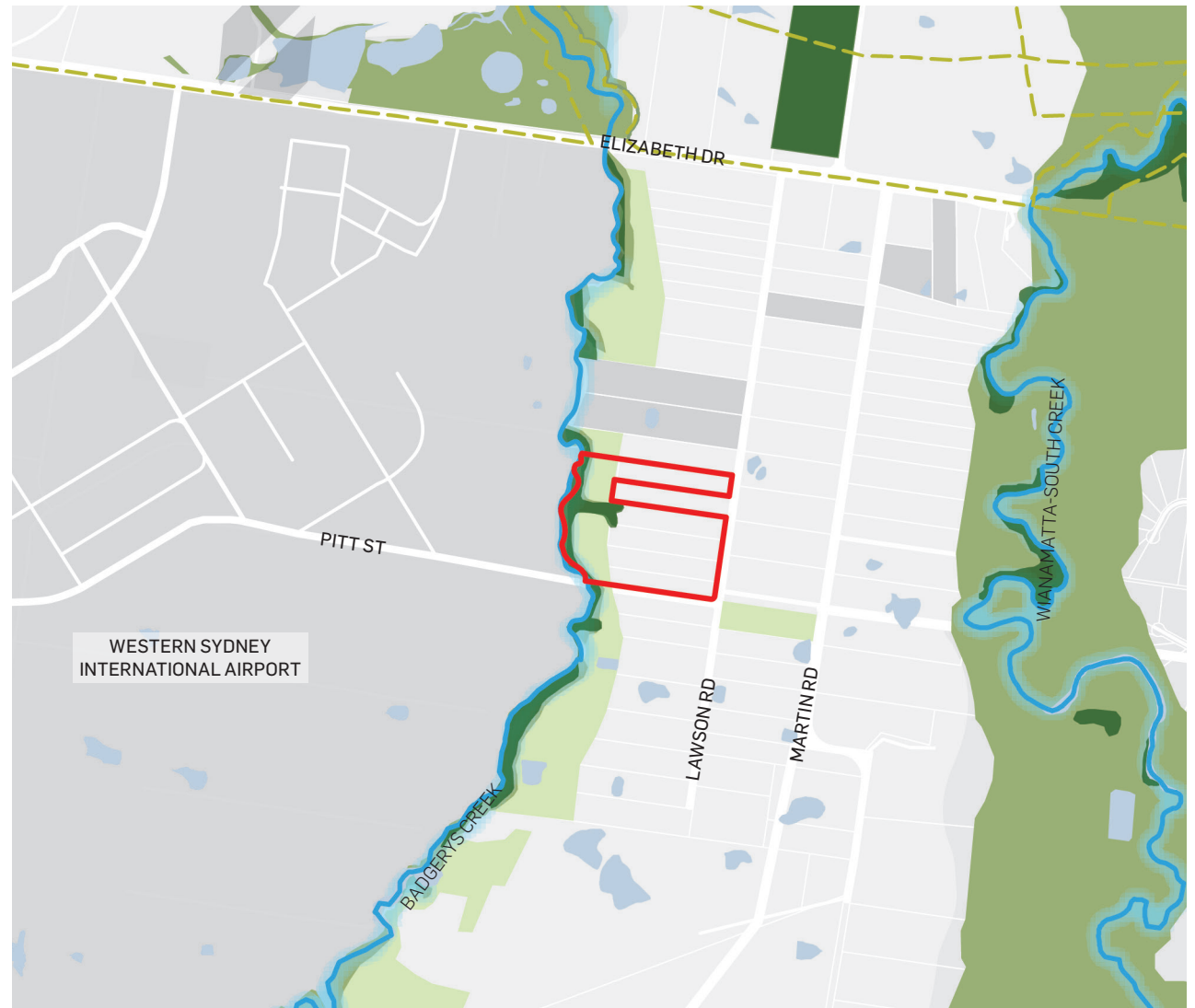


Figure 9 Biodiversity and Riparian Corridors Plan (as per the Precinct Plan) 1:20,000 @ A4






3.2.6 Centres Hierarchy

There is no local/neighbourhood centre identified within the site, with the closest centre to the east along the Wianamatta-South Creek corridor.

KEY TAKEAWAY

- There is no local/neighbourhood centre identified within the site.
- The scheme is within walking distance to the proposed local centre.
- Opportunity to provide a potential temporary cafe within the site, preferably along Lawson Road to enhance amenity for the future users and visitors, subject to operator requirement.

LEGEND

-  Site Boundary
-  Metropolitan Centre
-  Local Centre
-  Neighbourhood Centre
-  Local / Neighbourhood Centre Land Use

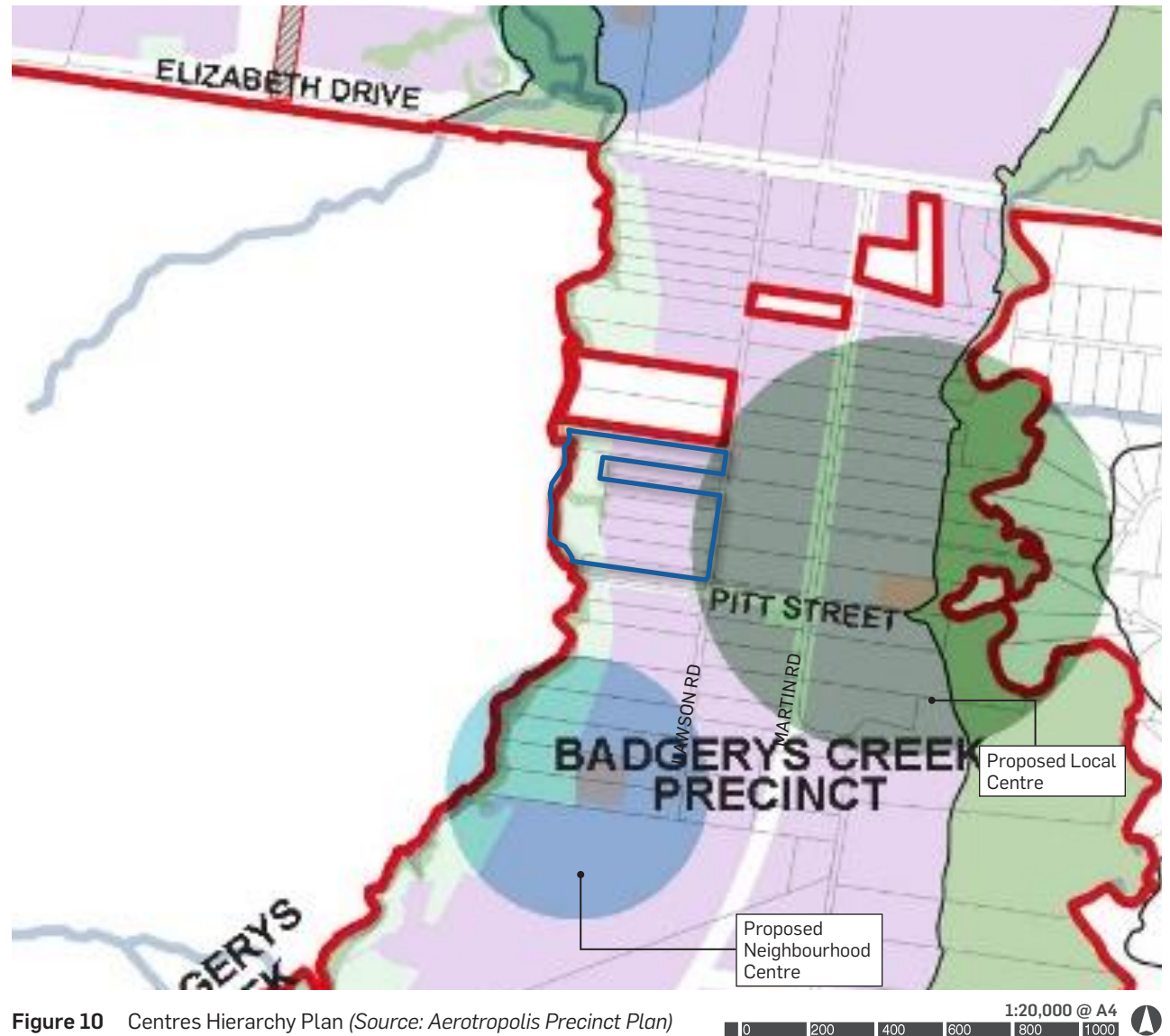


Figure 10 Centres Hierarchy Plan (Source: Aerotropolis Precinct Plan)









3.2.7 Heritage

A potential heritage item has been identified to be located within the site. Additionally, there are high-moderate Aboriginal cultural sensitivity areas along the western boundary of the site.

KEY TAKEAWAY

An ACHAR report has been prepared by City Plan. The future development should respond to the recommendations provided.

LEGEND

-  Site Boundary
-  Aboriginal Cultural Sensitivity - High
-  Aboriginal Cultural Sensitivity - Moderate
-  SEPP Heritage Item
-  State Heritage Register Item
-  State Heritage Item
-  Potential Heritage Item
-  Local Heritage Item

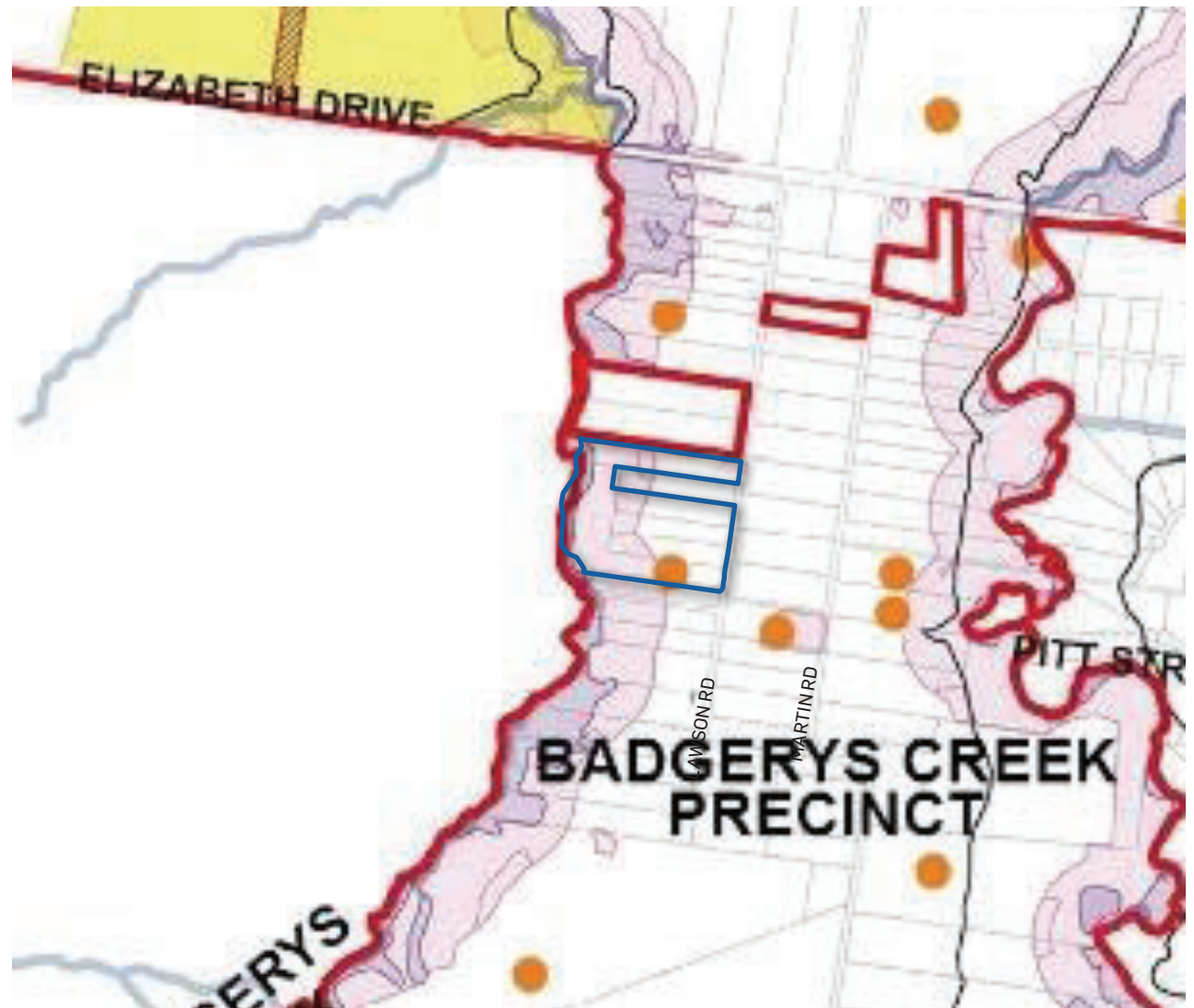


Figure 11 Heritage Plan (Source: Aerotropolis Precinct Plan)






3.2.8 Maximum Building Height

The maximum building height for majority of the site is 24m.

KEY TAKEAWAY

The current building height allocated is consistent with the provision of large format warehouse on the site.

LEGEND

-  Site Boundary
-  24m
-  Watercourses

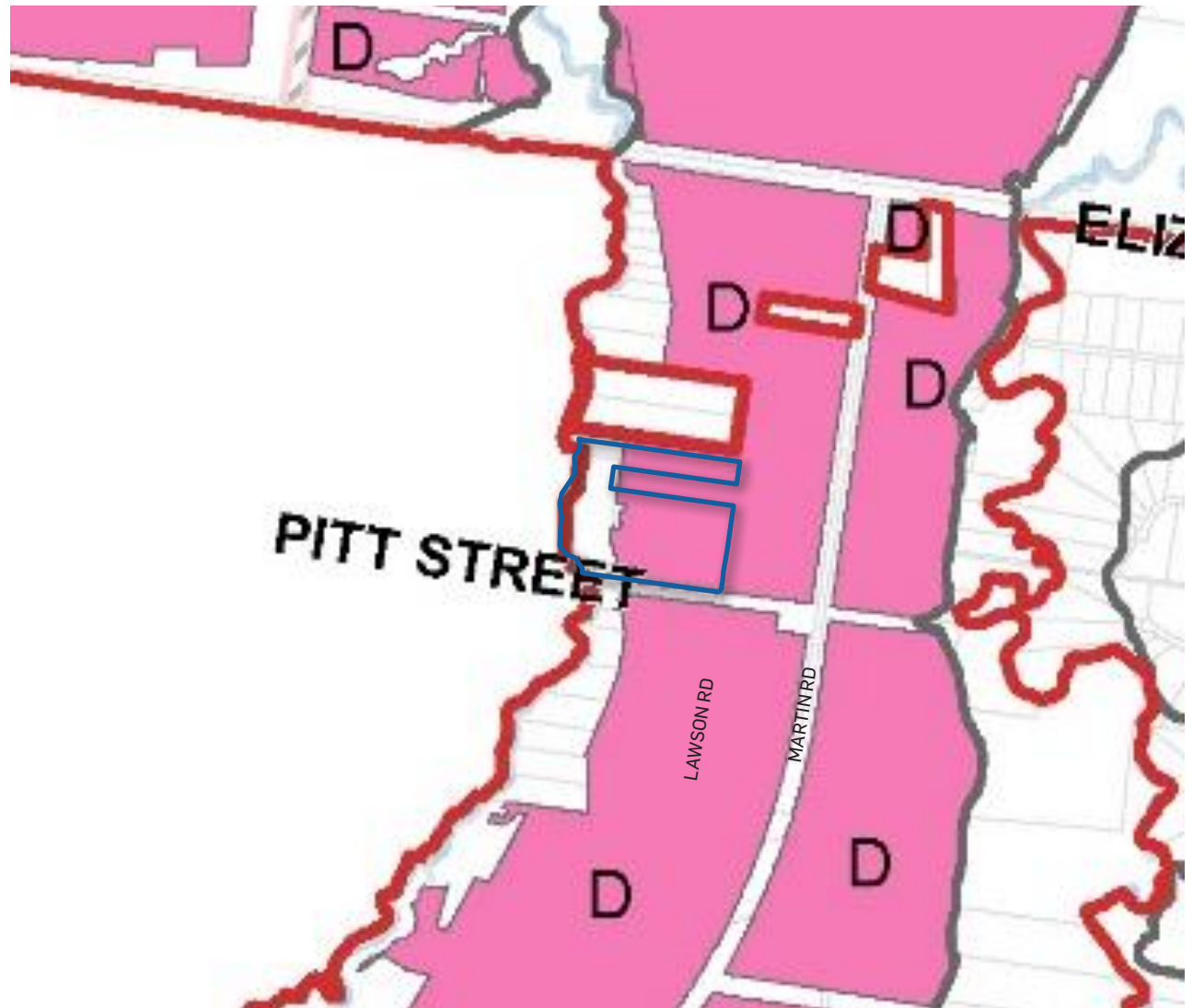


Figure 12 Max. Building Height Plan (Source: Aerotropolis Precinct Plan)

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0 200 400 600 800 1000

Surrounding Developments

- ① **85 Martin Road, Badgerys Creek Industry Park (SSD Under Assessment)** - Construction of a warehouse and distribution facility for ESR Developments consisting of three warehouse buildings with ancillary offices and associated services, infrastructure, on-site parking and landscaping.
- ② **230 Martin Road Industrial Development (SSD Under Assessment)** - Construction and operation of two large format warehouses.
- ③ **1669-1723 Elizabeth Drive, Badgerys Creek - Elizabeth Enterprise Stage 1 (SSD Under Assessment)** - Concept Master plan and Stage1 Development for general industry and warehouse distribution centres.
- ④ **Western Sydney International Airport (Under Construction)** - Staged proposal comprising the construction and operation of a new international airport.
- ⑤ **135 Lawson Road, Badgerys Creek (DA Refused)** - Construction of a concrete manufacturing facility.
- ⑥ **475 Badgerys Creek Road, Bradfield (WSA_MP01) (TAP process completed)** - Ingham Property Group are preparing a master plan for this 182-hectare site. A master plan request was lodged in March 2022. The site will predominately comprise 'enterprise and light industry' uses. 'Business and enterprise' and a 'local/ neighbourhood centre' are also envisaged.
- ⑦ **Burrah Park, 1953-2109 Elizabeth Drive (SSD Under Assessment)** - Concept proposal for an industrial warehouse and logistics estate with a Stage 1 development including bulk earthworks, road access and internal road construction, civil infrastructure and utilities, and construction and operation of 3 warehouses.

Key Considerations

- **Proximity to the WSA** - Opportunity to leverage the airport's connectivity and infrastructure, creating a supportive environment for businesses and the community.
- **Emerging Character** - The demand for large format warehouses can be observed with the recent surrounding proposals.
- **Location at a Major Intersection** - Improves visibility and access for the future users.
- **Improve Landscape** - Opportunity to enhance and support the existing creek corridor through a place responsive design solution.
- **Enhance Amenity** - A proposed local and neighbourhood centre are in proximity to the site. However, the site presents an opportunity to incorporate a potential temporary cafe along Lawson Road that can be accessible to the future users of the site.
- **Land Ownership** - There are two lots located to the north of the site that are under WSA ownership. The delivery of the park edge street identified within the site will be dependent on the outcomes of the WSA sites. There is currently no indication from WSA on the delivery of this street.

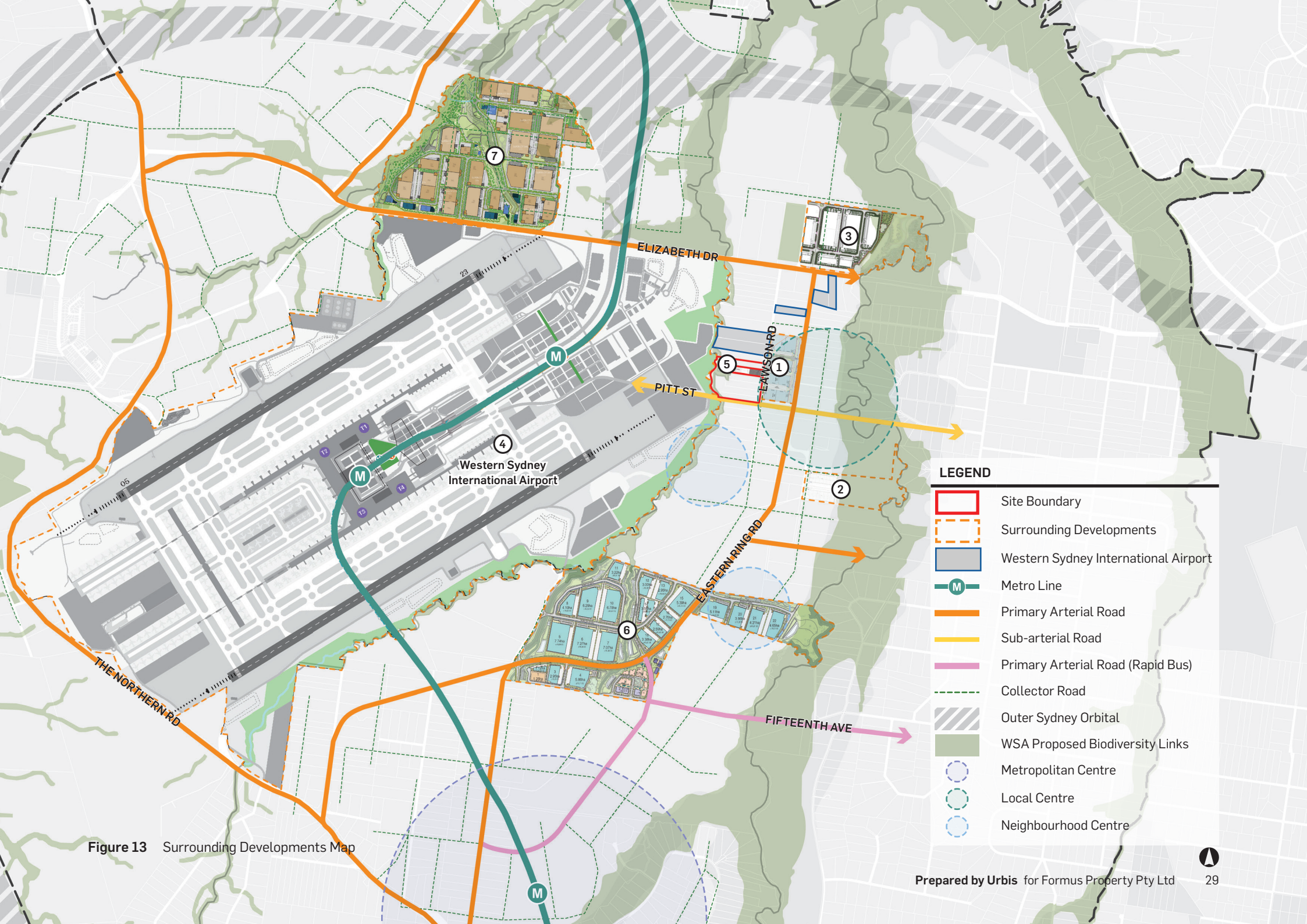


Figure 13 Surrounding Developments Map

LEGEND

- Site Boundary
- Surrounding Developments
- Western Sydney International Airport
- M Metro Line
- Primary Arterial Road
- Sub-arterial Road
- Primary Arterial Road (Rapid Bus)
- Collector Road
- Outer Sydney Orbital
- WSA Proposed Biodiversity Links
- Metropolitan Centre
- Local Centre
- Neighbourhood Centre

4. SITE ANALYSIS

This chapter of the report provides an understanding of the site and surrounding area. It also investigates the existing constraints and opportunities of the site.

4.1 Topography and Views









The subject site gently slopes in an east-west direction towards the creek.

The topography within the site varies by 12m from the low point along the western boundary (50m) and the high point along Lawson Road (62m).

KEY TAKEAWAY

- Enhance the views to the Badgerys Creek.

LEGEND

	Site Boundary
	Slope greater than 15%
	Creek
	Contours
	Ridgeline
	Immediate Views to Badgerys Creek and Existing Vegetation
	High Point
	Low Point

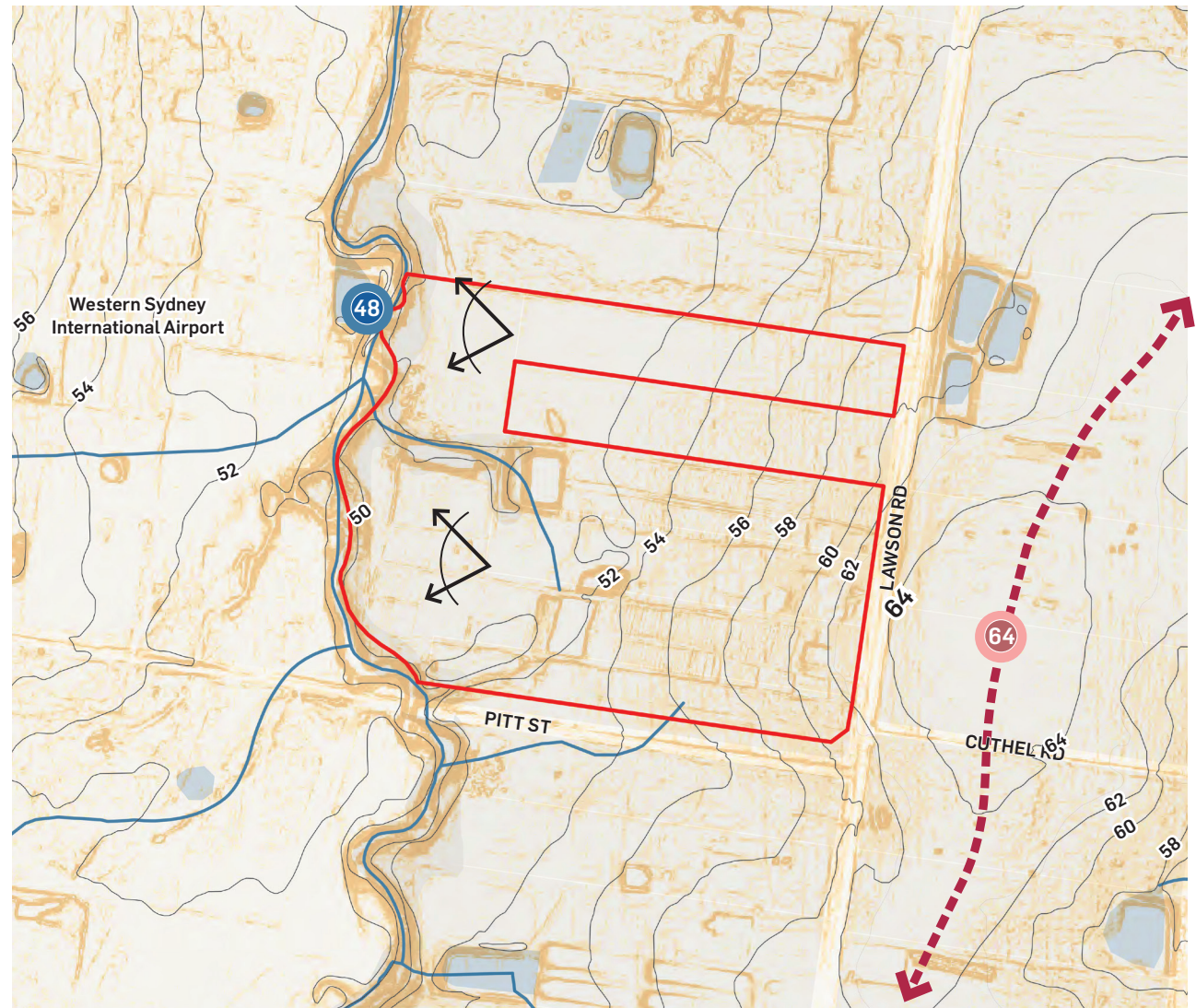


Figure 14 Topography and Views Map

1:6,000 @ A4

4.2 Road and Transport Network

The Aerotropolis Precinct Plan proposes:

Pitt Street: to be a sub-arterial road proposed to enhance the west-east connection from the Western Sydney International Airport to Badgerys Creek Local Centre.

Lawson Road: to be a proposed collector road with local bus corridor.

Local and park edge streets have been proposed to run through the site.

KEY TAKEAWAY

- Limited access opportunities from Pitt Street.
- The local street layout is not suitable for the site and the intended large format warehouse.
- Delivery of park edge street is dependent on the outcomes of the WSA site.

LEGEND







-  Site Boundary
-  WSA sites
-  Sub-arterial Road
-  Collector Road
-  Local Street
-  Park Edge Street



Figure 15 Road and Transport Network Map (Source: Aerotropolis Precinct Plan)



4.3 Access and Movement

A regional cycle way is proposed to run along the creek towards the west of the subject site and along Pitt Street.






Cycleways are also nominated to be located along Lawson Road.

Pitt Street is nominated to be a rapid bus corridor and a local bus route is identified along Lawson Road.

KEY TAKEAWAY

- Opportunity to integrate active transport networks within the proposed landscaping design.
- Buildings should front onto Lawson Road as it has access to public and active transport routes.
- Detailed investigation to be undertaken to review the proposed intersection along Pitt Street. The active transport connections are more likely to occur along Lawson Road and the creek.

LEGEND

-  Site Boundary
-  Rapid Bus Corridor
-  Indicative Local Bus Network
-  Principal Regional Cycle Path
-  Cycle Paths on Collector Roads

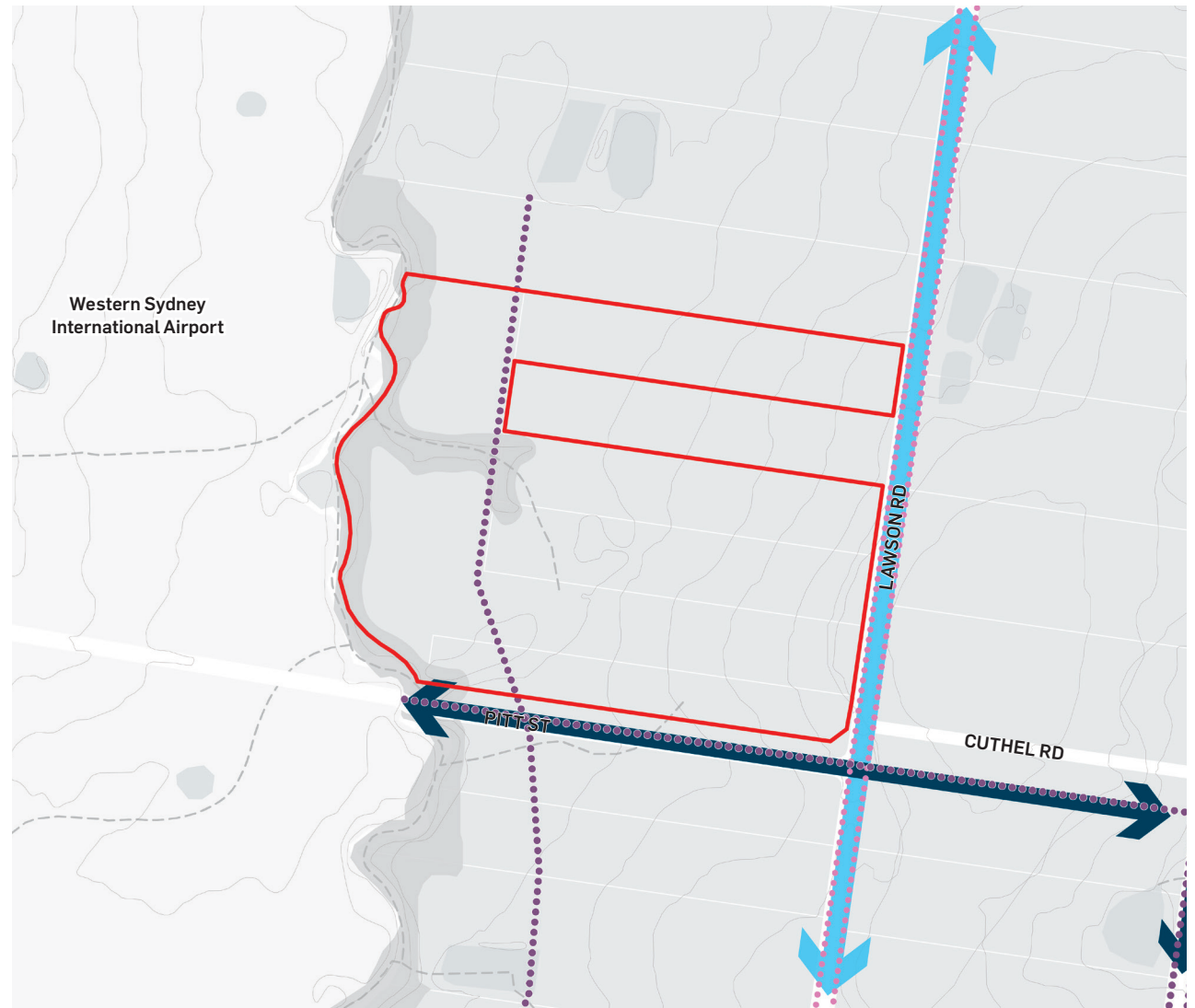


Figure 16 Access and Movement Map (Source: Aerotropolis Precinct Plan)








4.4 Riparian Corridor & Blue Infrastructure

The existing watercourses have been assigned a number based on the Strahler Stream Order. The various stream orders require specific landscape buffers to protect the corridor.

KEY TAKEAWAY

- Protect and enhance the riparian corridor whilst maintaining the landscape buffer.

LEGEND

-  Site Boundary
-  1st Stream Order - 10m VRZ each side
-  2nd Stream Order - 20m VRZ each side
-  4th Stream Order - 40m VRZ each side
-  Riparian Corridor

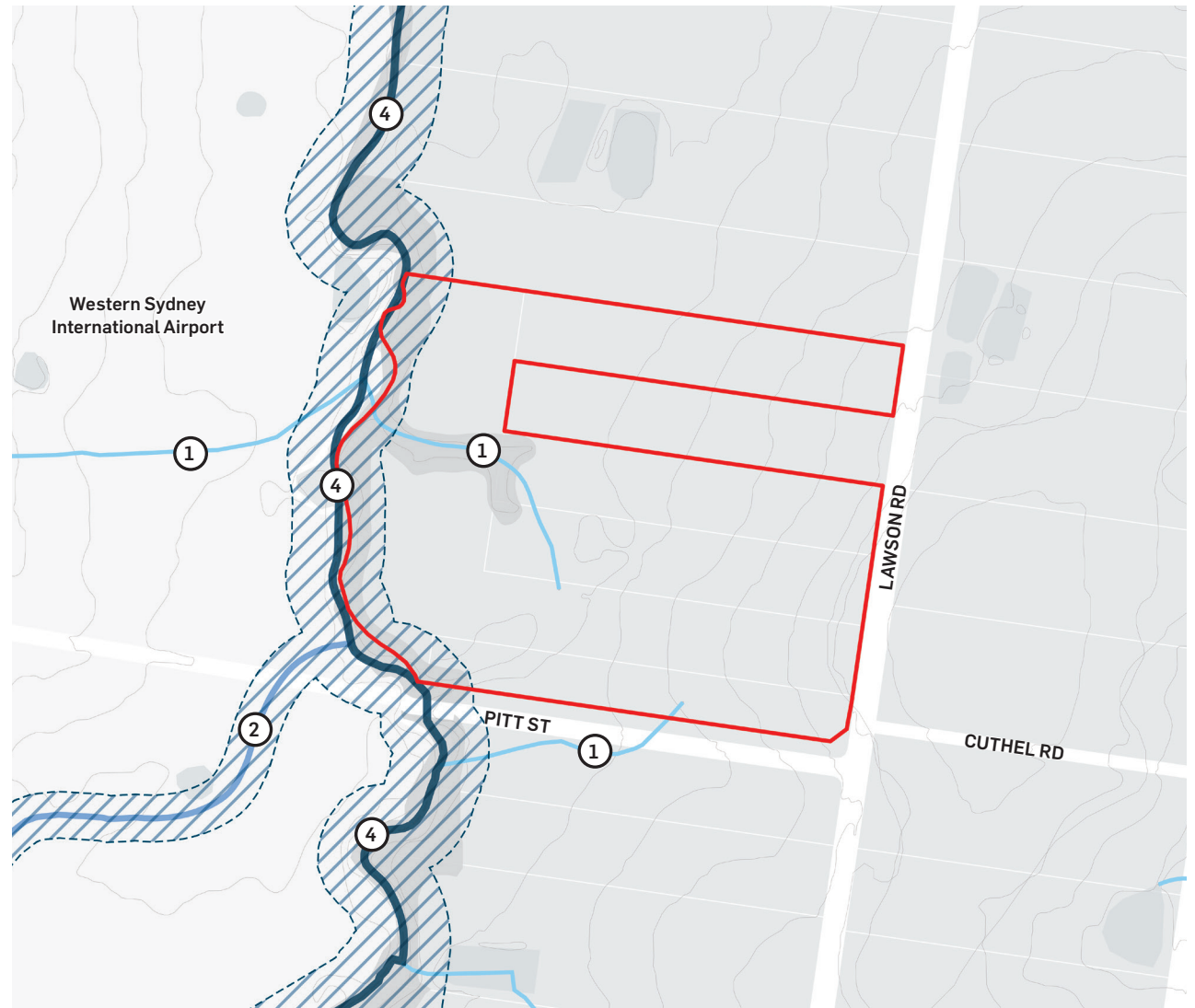


Figure 17 Riparian Corridor and Blue Infrastructure Map

4.5 Biodiversity and Ecology

ELA have been engaged to prepare the Biodiversity Assessment Report for the site. Accordingly, the site is located on biodiversity certified land. Four (4) Plant Community Types (PCT) were identified and include:

- PCT 3320 Cumberland Shale Plains Woodland (Moderate and Derived Native Grassland)
- PCT 3975 Southern Lower Floodplain Freshwater Wetland (Moderate)
- PCT 4023 Coastal Valleys Riparian Forest (Moderate, low and regenerating)
- PCT 4025 Cumberland Red Gum Riverflat Forest (Moderate and low)

A small portion to the west of the site has been identified as High Biodiversity Value (HBV) land.

KEY TAKEAWAY

- Provide landscape open spaces along the riparian corridor and enhance the existing vegetation.
- Retain and protect the HBV land.
- Majority of impacts on identified ENZ land are sited within previously cleared land or exotic grass.

LEGEND














	Site Boundary		PCT 4023 Coastal Valleys Swamp Oak Riparian Forest
	High Biodiversity Value Land		Regenerating
	Planted Native		Low
PCT 3320 Cumberland Shale Plains Woodland			Moderate
	Derived Native Grassland	PCT 4025 Cumberland Red Gum Riverflat Forest	
	Moderate		Disturbed
PCT 3975 Southern Lower Floodplain Freshwater Wetland			Moderate
	Low		Cleared



Figure 18 Biodiversity and Ecology Map (Source: ELA)



4.6 Flooding and Stormwater

Establishing the existing flood conditions allows an understanding of the availability of developable land and identification of regional stormwater constraints associated with the development of the site.






The site is situated between two major catchment areas which include Badgerys Creek to the north-west and South Creek to the east.

The entire western boundary is affected by the indicative 1% AEP area, as shown in the figure across.

KEY TAKEAWAY

- Utilise the presence of the riparian corridor to manage the existing flow path and provide stormwater management facilities that integrate with the natural environment.
- Limited earthworks will allow the rationalisation of the development footprint above the 1% AEP.

LEGEND

-  Site Boundary
-  Indicative 1% AEP Flood Extent
-  Creek-line
-  Hydro-line
-  Riparian Corridor

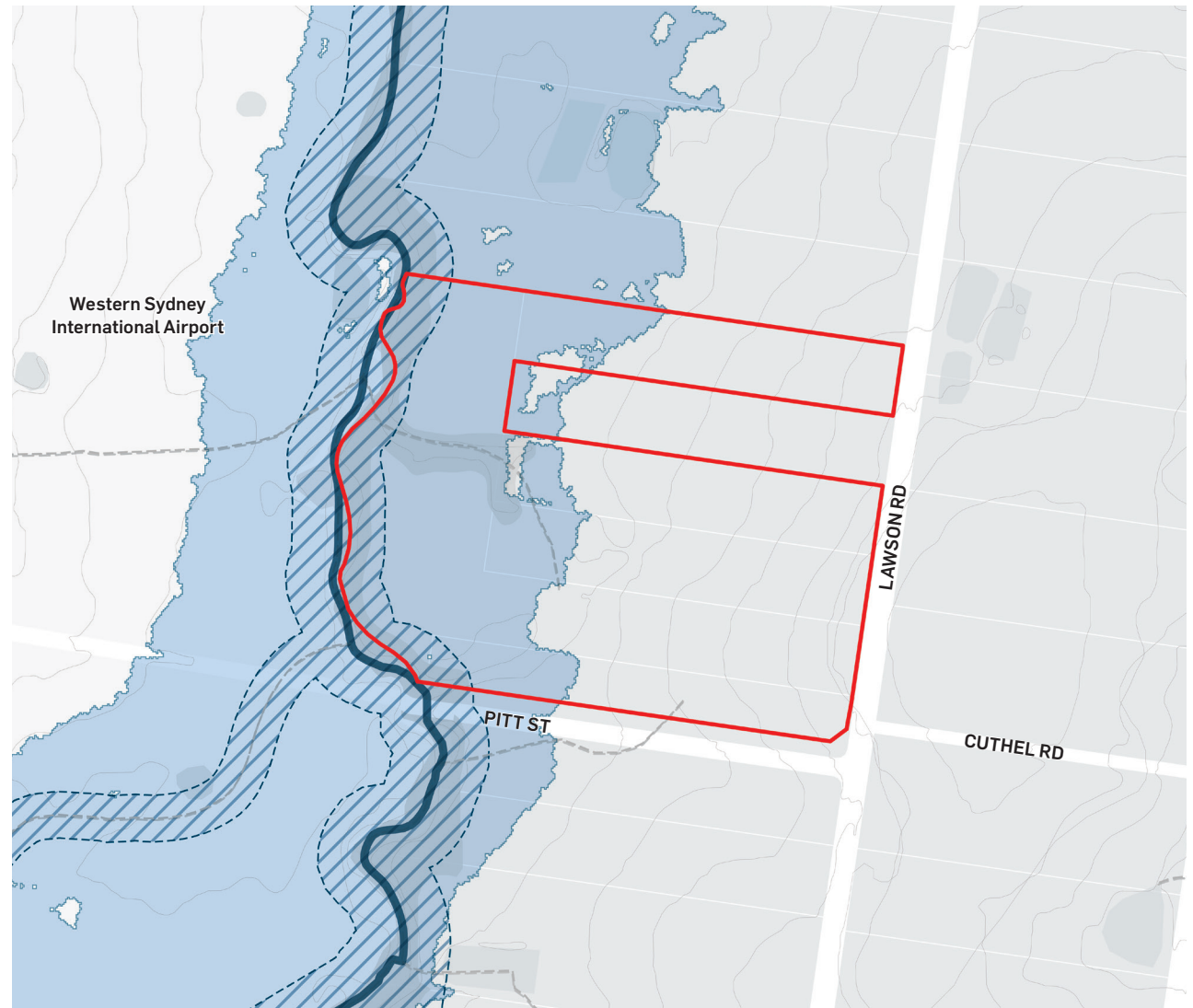


Figure 19 Flooding and Stormwater Map



4.7 Bushfire

The Bushfire Prone Land (BPL) has been identified in figure 20. The site includes areas Category 1 and 3 and associated vegetation buffer.





The proposed Asset Protection Zone's (APZ) by Blackash include >12m to the north* and west.

Adequate bushfire protection measures are provided to reduce the residual risk to an appropriate level.

KEY TAKEAWAY

- Respond to the APZ requirements through the development of the built form.
- *A 4m APZ zone to the north has been identified as dependable by Blackash.

LEGEND

-  Site Boundary
-  Vegetation Category 1
-  Vegetation Category 3
-  Vegetation Buffer

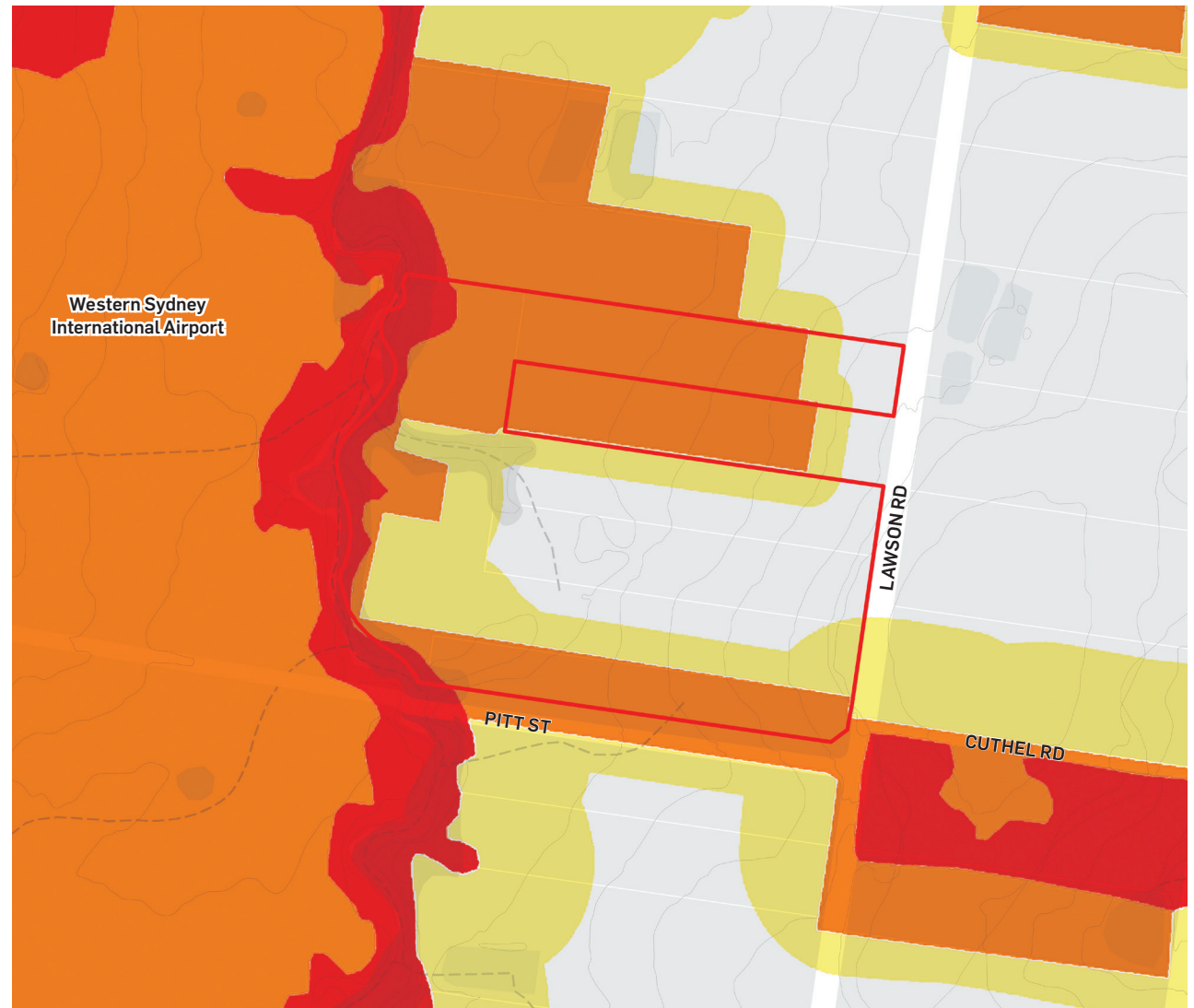


Figure 20 Bushfire Map

4.8 Indigenous Cultural Heritage

City Plan have been engaged to prepare an Aboriginal Cultural heritage Assessment for the site. The assessment summarises the following:

- Three archaeological sites were found on site including;
 - An artefact scatter across the entire extent of the site which has been subjected to stripping of topsoil
 - An area of potential archaeological deposit associated with remnant topsoil which roughly aligns with riverflats along Badgerys Creek
 - A potential hearth or ground oven.
- There is potential for the sub-surface of the potential archaeological deposit along Badgerys Creek to be preserved as it sits within the area on the Precinct Plan mapped as 'Aboriginal cultural sensitivity - high'.
- Acknowledges that development activity on the site will result in unavoidable harm to Aboriginal objects and recommends a series of heritage management measures to mitigate potential impacts.

KEY TAKEAWAY

- Once approval has been issued, an Aboriginal Heritage Management Plan should be developed in consultation with the Registered Aboriginal Parties.

LEGEND






-  Site Boundary
-  Artefact (isolated finds)
-  Possible hearth
-  AHIMS #45-5-5696: Artefacts
-  AHIMS #45-5-5703: PAD



Figure 21 Indigenous Cultural Heritage Map (Source: City Plan)



4.9 Noise

The site's adjacency to the Western Sydney International Airport (WSA) presents issues in relation to noise.





The SEPP (Precincts - Western Parkland City) identifies portions of the site being affected by the ANEC 30-35 and also ANEC 35-40 buffers. The areas subject to these ANEC controls have restrictions on residential development and require permissible development, which include employment uses, to adopt appropriate design and construction standards in order to mitigate the impacts of aircraft noise.

ANEC: Australian Noise Exposure Concept. An ANEC rating above 25 is unacceptable for residential development.

KEY TAKEAWAY

- Appropriate noise mitigation measurements are to be considered at construction stage to minimise the noise impacts of the future transport corridors on future users.
- Warehouse and light industrial will be a suitable use under the ANEC buffers.

LEGEND

-  Site Boundary
-  ANEC between 25 - 30
-  ANEC between 30 - 35
-  ANEC exceeding 35

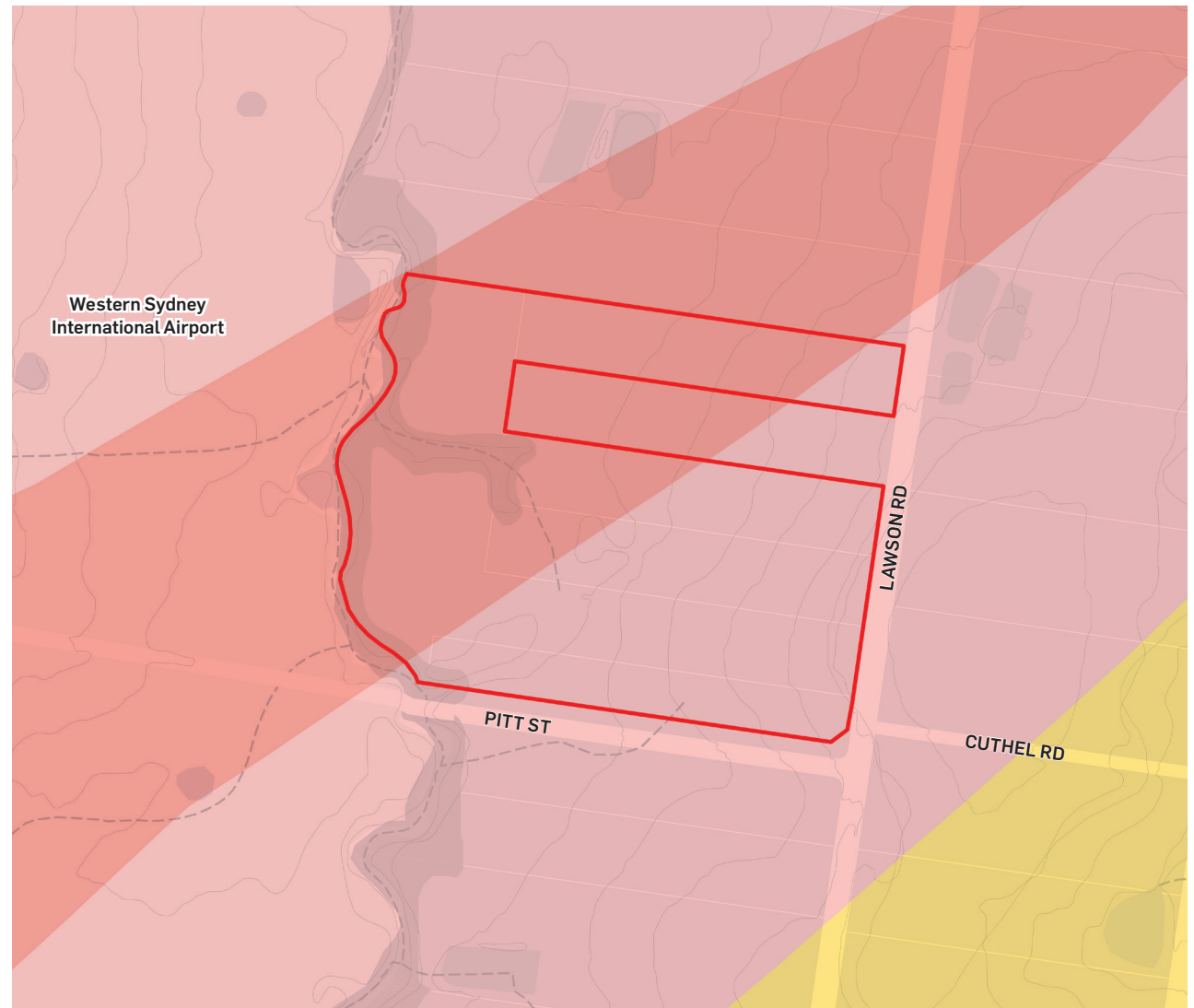


Figure 22 Noise Contour Map

(Source: State Environmental Planning Policy (Precincts-Western Parkland City) 2021 Noise Exposure Contour Map)



4.11 Aviation Restrictions

AVLAW Aviation Consulting have been engaged to prepare an Aviation Safeguarding Assessment Report assess the aviation considerations related to the Western Sydney International Airport.

The assessment provides a review of the proposed master plan and identifies that all the proposed structure will remain below the airspace protection surfaces and therefore will not require the controlled activity approval prior to construction.

However, it notes that the site is covered by lighting plane B of the second runway which limits the maximum lighting intensities to 50 candelas. Additionally, the Public Safety Runway of the second runway intersects part of Warehouse 5. At the detailed design stage, these conditions need to be addressed, with mitigation strategies, if required.

4.10 Geotechnical and Acid Sulphate Soils

Sydney Environmental Group has undertaken geotechnical, contamination, and soil salinity assessments to determine the on-site ground conditions.

The assessment report summarises that:

- Significant changes have occurred on site between March 2025 and March 2022 when an initial study was undertaken. Previously identified contamination risks have now been removed and remaining contamination has been localised to soil material stockpiles.
- Asbestos had been detected on the site, however, the concentration levels have been considered unlikely to present an unacceptable human health exposure risk.
- Other detected contaminants identified on the site are unlikely to present an unacceptable human health exposure risk.
- No significant groundwater or surface water risks were identified for the site.

The assessment report recommended that:

- The site can be made suitable for the proposed land use subject to further characterisation and remediation works.
- These further assessments / surveys should be undertaken:
 - Prepare a Remedial Action Plan
 - Asbestos Characterisation Assessment
 - Hazardous building materials survey of the site's existing structures
 - Clearance inspection - following the removal of hazardous building materials and the demolition of existing structures
 - Waste classification assessment of proposed soil materials to be disposed of.



4.12 Consolidated Opportunities and Constraints

SITE ACCESS	BUILT FORM	LANDSCAPE AND BIODIVERSITY
<p>Opportunity:</p> <p>Lawson Road is identified as a Collector Road with an indicative local bus route and provides the opportunity to:</p> <ul style="list-style-type: none"> Provide pedestrian access to/from the site to enhance the connectivity to Badgerys Creek Provide pedestrian access to/from the site to future bus stops and bike routes. Provide an internal road access for loading and vehicular access. <p>Constraints:</p> <p>Pitt Street has been identified as a Sub-arterial Road and for the purposes of this development, it is treated as a secondary frontage and therefore has the opportunity to only include pedestrian access only to improve connections to the Creek.</p>	<p>Opportunity:</p> <p>The site presents an opportunity to provide a sensitive response to the unique site features:</p> <ul style="list-style-type: none"> Provide sensitive interfaces with the natural environment. Provide high quality architectural expression that contributes to the public domain and streetscapes along Lawson Road and Pitt Street. Suitable for the current demand of large format warehouses >40,000 m2 in prevalent in Western Sydney. <p>Constraints:</p> <p>Due to the location of the site and frontages to a proposed Collector and Sub-arterial road, increased setback requirements apply to the site.</p> <p>Accommodate for the intended road widening of Pitt Street.</p> <p>Locate building entries to enable convenient pedestrian access to/from bus stops on Lawson Road.</p> <p>At a detailed design stage, if required, provide mitigation strategies in the areas where the site intersects with the Public Safety Area (PSA).</p>	<p>Opportunity:</p> <p>Enhance the connection to Country through a landscape design that celebrates the Creek.</p> <ul style="list-style-type: none"> Maintain and enhance the green and blue character through the provision of open spaces, gathering spaces and active transport. Promote pedestrian activity and connectivity. <p>The provision of landscape setbacks along Lawson Road and Pitt Street presents the opportunity to:</p> <ul style="list-style-type: none"> Deliver a high-quality landscape design that enhances the streetscape, softens the built environment, and aims to meet the minimum tree canopy target, while considering aviation safety requirements. Incorporate deep soil planting. <p>Constraints:</p> <p>Existing flooding impacts along the Creek, require appropriate mitigation measures.</p> <p>The western edge along Badgerys Creek is zoned Environmental and Recreation under the WPC SEPP which is to be enhanced and preserved.</p> <p>Provide appropriate measures to spread awareness and mitigate any impacts to the brown snake habitat.</p>


LEGEND

 Site Boundary

Key Opportunities

 Potential Truck Access


 Potential Pedestrian Access Points to the Site

 Opportunity for High-Quality Landscape along Lawson Road and Pitt Street Landscape Setbacks

Key Constraints

 Public Safety Area (as per WSA SEPP Map)

 Road Widening

 Indicative 1% AEP Flood Extent

 Riparian Corridor

 ENZ Environment and Recreation

 High Biodiversity Value Land

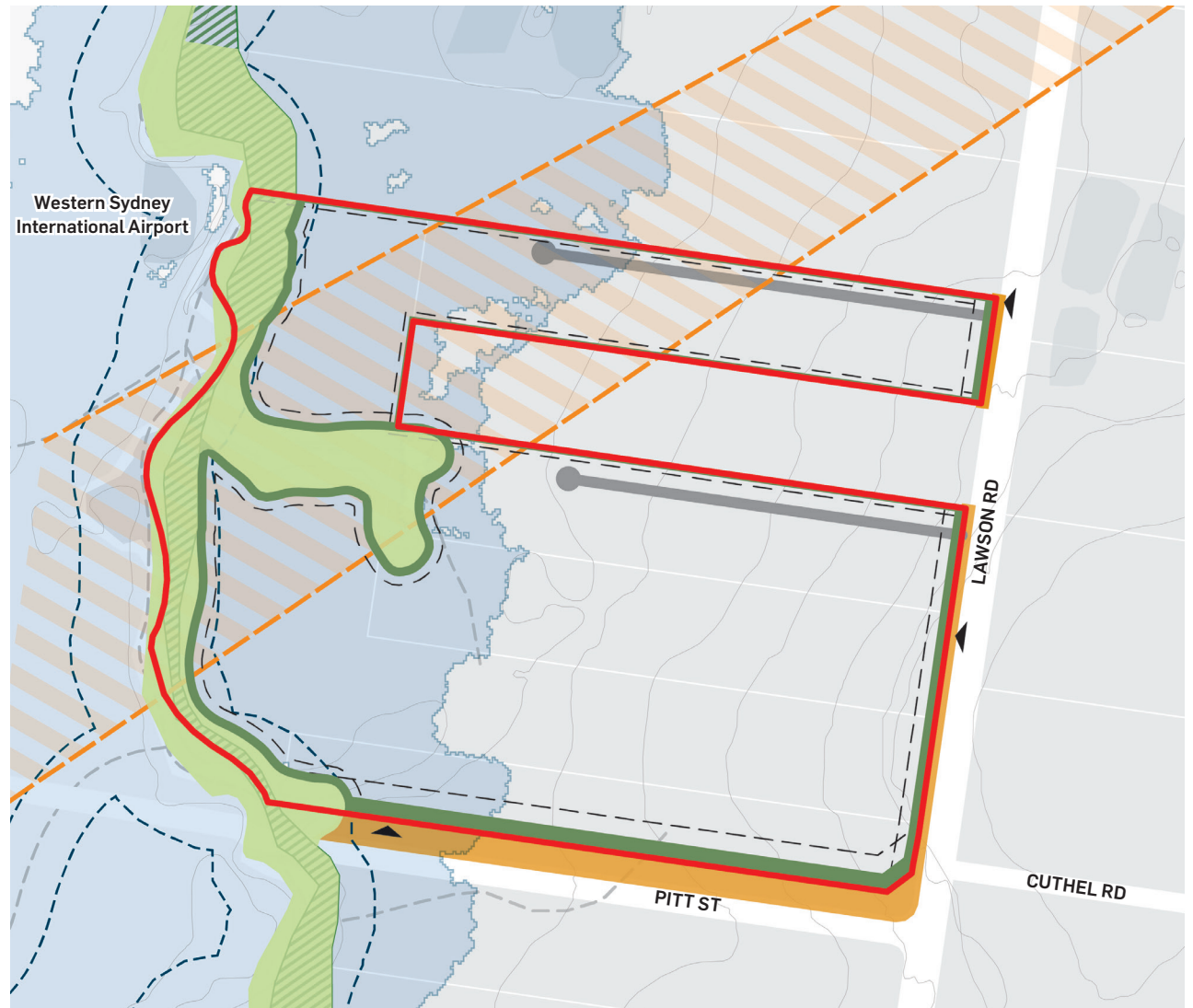


Figure 23 Consolidated Opportunities and Constraints Map



4.13 Alignment with the Precinct Plan

The subject site and its context lie within the Enterprise and Light Industry land use which provides an opportunity to develop the built form and connectivity layout under the assumption that they would primarily function as large to medium sized industrial or warehouse buildings.

The fine-grained road network as identified in the Precinct Plan compromises the delivery of large format warehouse. A similar approach of minimising local streets to deliver the market demand has been taken on sites such as the IPG Badgerys Creek and Burrah Park.

The proximity to the Western Sydney International Airport triggers aviation safety requirements that prescribe building heights restricting buildings to be under 24m, which the proposed master plan is consistent with.

Local Street Network

The delivery of the proposed east-west road in the Precinct Plan is currently constrained by the high market demand for large format warehouses. However, east-west pedestrian links have been provided in the master plan to increase connectivity and permeability to the creek and associated open space.

145-175 provides the optimal development potential for the site (free of constraints) with land to the north considered better suited for any future local road due to it being heavily constrained/ burdened by the PSA (airport safeguarding).

Introducing any local road(s) connecting with Lawson Road, and in proximity to the Pitt St/ Lawson Rd intersection, is not considered a desirable outcome from traffic planning/ modelling perspective.

Park Edge Street

The landscape design includes provisions for the park edge street. However, the delivery and timeline for this road are uncertain, as they largely depend on the development of the lots to the north of the site, including the one owned by the Western Sydney International Airport.

Pedestrian Connectivity

According to the Precinct Plan, the location and layout of the Collector and Local Streets are indicative (*Requirement SH3*). This Master Plan proposes a variation to the Precinct Plan in terms of the Local Street layout however, the provision of the east-west pedestrian links from Lawson Road:

- Provides a permeable pedestrian network from Lawson Road and along Pitt Street, increasing connectivity to the creek
- Minimises walking distance from Lawson Road to the creek and proposed open spaces
- Responds to the existing topography
- Minimises isolation of any neighbouring properties and does not impact on their development potential

The public pedestrian link has been strategically located along Pitt Street in proximity to the pedestrian crossing at the junction of Pitt Street and Lawson Road to facilitate uninterrupted pedestrian movement.



Source: SBA

5. KEY DIRECTIONS

VISION

"A Country and landscape led design approach that responds to the Creek and its associated riparian corridor, placing architecture at the forefront. The design seamlessly integrates employment opportunities with vibrant placemaking, enriching the experience for both visitors and the workers."

6. MASTER PLAN



Source: SBA

The proposed Master Plan is informed by a Country and landscape-led approach and takes into consideration the key conditions of the site. SBA and Urbis have prepared the Master Plan that embraces a holistic approach to create a balance between the proposed warehouses, open spaces, connectivity and cultural significance.

SETBACKS

The built form setbacks from Pitt Street allows for the future road widening. The setbacks also enable the provision of high quality landscape, deep soil zones, surface parking and driveways.

ACCESS & MOVEMENT

The proposed movement provides vehicular access from Lawson Road and pedestrian access from both Lawson Road and Pitt Street. The layout of the warehouses have been designed to accommodate adequate truck movements, car parking and perimeter driveways.

BUILT FORM & INTERFACE

The office components are located at the corners of the proposed warehouses to have visibility from the street, ease of access and specifically for Warehouse 1 to take advantage of the views to the Creek. The long and monolithic façades along the streets have been well articulated to create visual interest and respond to Country.

LANDSCAPE

The proposed master plan creates significant landscape opportunities to enhance the connection to the Creek. It provides gathering spaces for both visitors and workers to connect with Country.

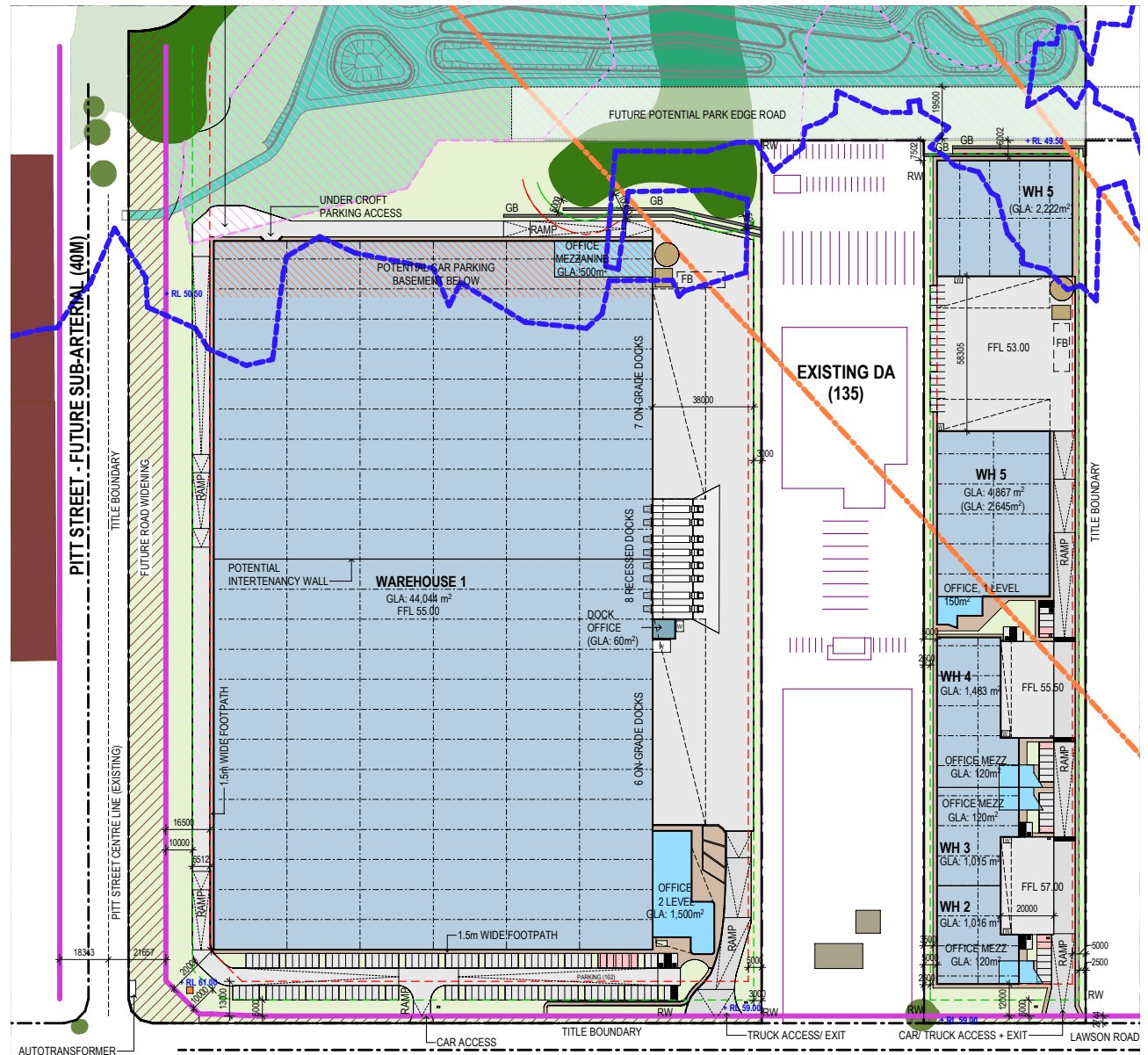
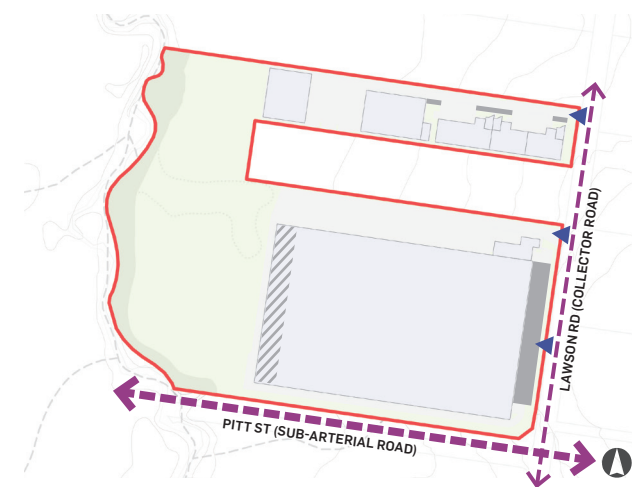


Figure 25 Proposed Master Plan
Source: SBA

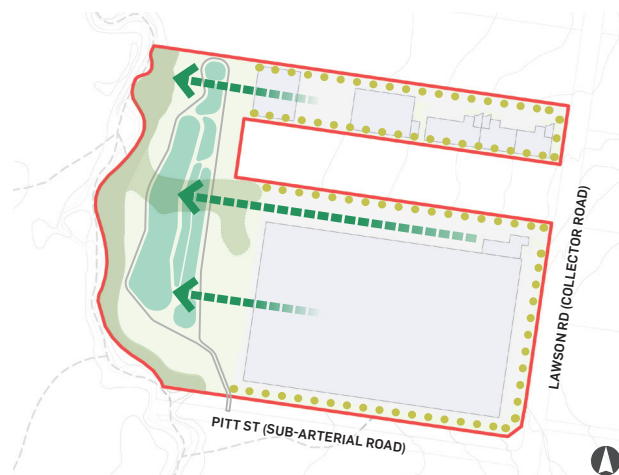
7. DESIGN DIRECTIONS

ACCESS AND MOVEMENT



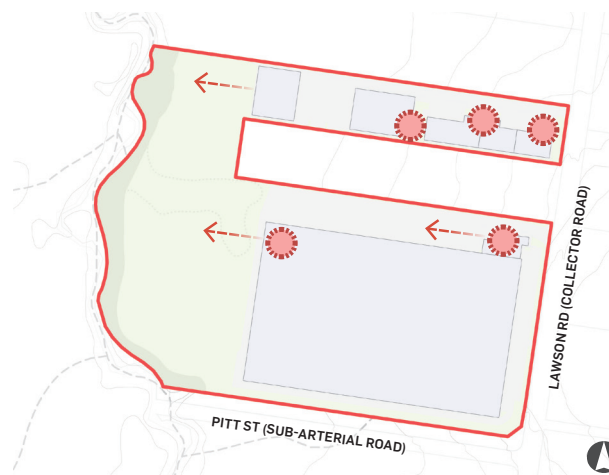
- Ensure vehicular access is provided off collector roads (Lawson Road).
- Allow for the widening of Pitt Street and Lawson Road to deliver an upgraded road network servicing the wider Aerotropolis precinct.
- Integrate tree planting and landscaping within the parking areas.

LANDSCAPE AND VIEWS



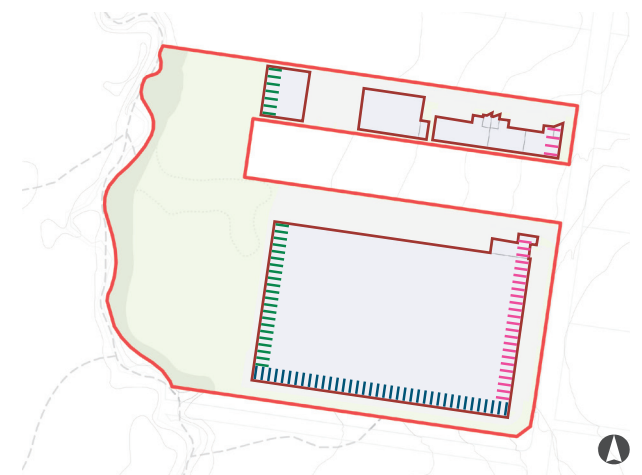
- Maintain and enhance views to Badgerys Creek and the Blue Mountains from the public domain.
- Provide landscaping and tree canopy cover within setbacks.
- Create a consolidated water management system and passive open space along Badgerys Creek.

OFFICE COMPONENT LOCATION



- Locate office components at the interface of Lawson Road and away from any major intersections.
- Locate office components such that they can take advantage of the view lines to Badgerys Creek and the Blue Mountains.

INTERFACES



- Support and provide active frontages along the road edges to improve the interface to the public domain.
- Create visual interest along Lawson Road and Pitt Street with special facade treatment.
- Enhance frontage to the creek through landscaping and facade treatment.



8. MASTER PLAN FEATURES

8.1 Design Excellence

The proposed master plan achieves key design excellence, from an urban design perspective, in accordance with SEPP Precincts-Western Parkland City 2021 (Cl. 4.33) as follows:

- The proposed built form and landscape design appropriately supports and enhances the connection to the surrounding landscape, biodiversity, site features and Country.
- The use of varied materiality and colours on the warehouses creates visual interest and respond to the emerging character of the area. Additionally, the detail and resolution of the office components elevates the quality of architecture.
- Landscape features, gathering spaces and planting improves the amenity of the public domain. The nodes and continuous green network along the Creek, provides places for education, cultural practices and social interaction.
- The proposal allows for pedestrian access by balancing access from both Lawson Road and Pitt Street. The site security will be maintained through strategically placed, visually permeable gates. The pedestrian experience is further enhanced through the provision of interconnected green open spaces, pathways and boardwalks.
- The proposed architectural and landscape design strategies celebrates views to the creek and enhances the connection.
- The proposal demonstrates landscape architectural excellence by enhancing the existing blue-green corridor by incorporating landscape within all setback zones as well as on-site deep soil zones.









Source: SBA

8.1.1 Land Use and Built Form

In keeping with the site's zoning as employment land, the proposed scheme provides the following built form:

- Six buildings single storey warehouses with footprints ranging from 1,000 m² - 44,000 m².
- Associated with Warehouse 1 is a 1,500 m² office at the Lawson Road frontage as well as a 500 m² mezzanine office at the rear.
- Associated with Warehouses 2 - 5 are a series of smaller offices ranging between 120 m² and 150 m².
- Hardstand areas are provided for truck manoeuvring, vehicle access and surface parking.

LEGEND

	Site Boundary
	Warehouse
	Office
	Hardstand
	Undercroft Parking
	Warehouse ID

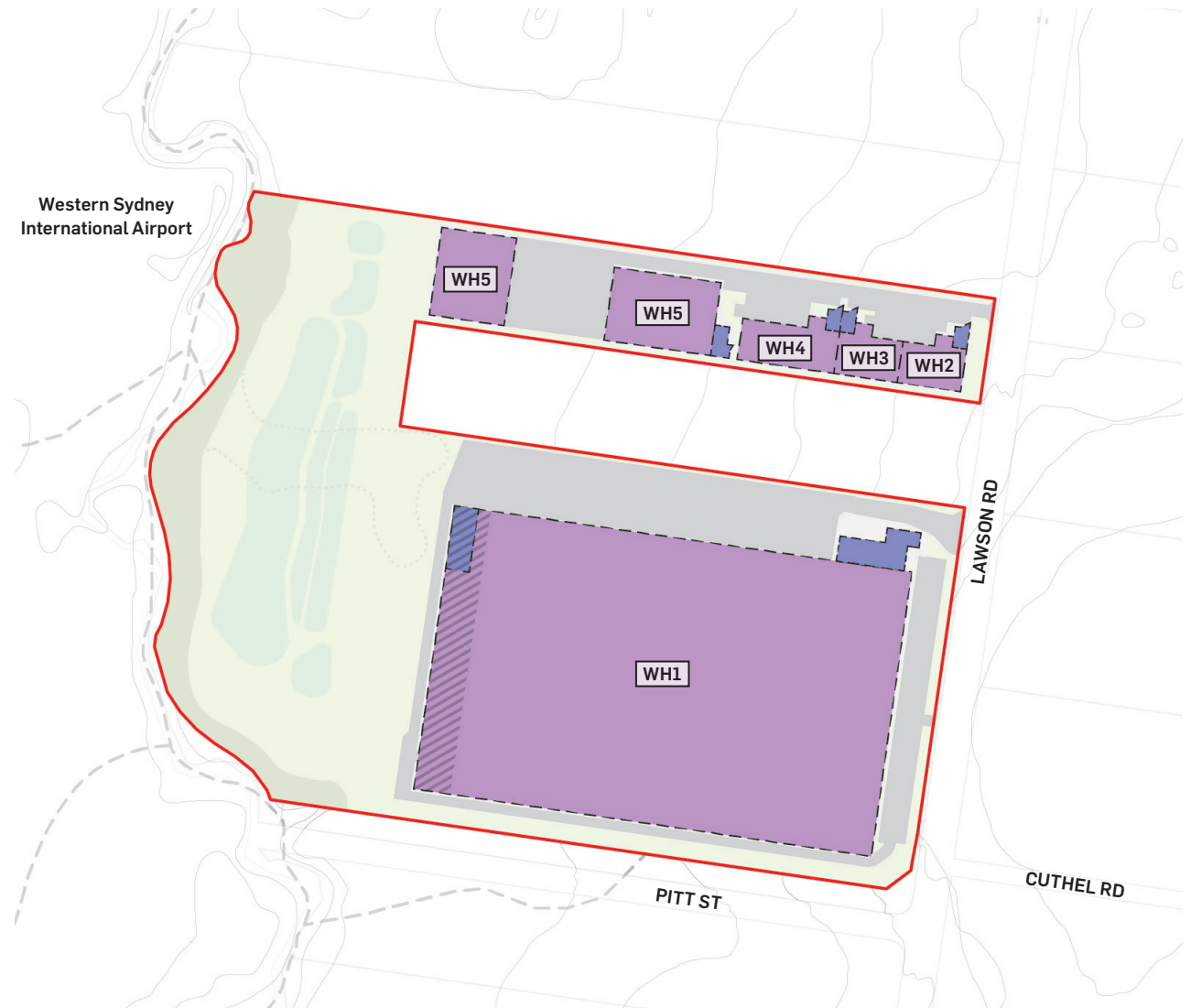


Figure 26 Land Use and Built Form Plan



8.1.2 Site Access












The following provisions have been made for vehicular access to the site:

- Vehicular access off Lawson Road.
- Separate heavy and light vehicle circulation surrounding to provide for efficient dock access and minimise conflict.
- Allowance has been made within the site for the widening of Pitt Street and Lawson Road.
- An internal road has not been provided to allow for the future subdivision of the land in accordance with the proposed road layout of the Precinct Plan.

The following provisions have been made for pedestrian access:

- Public access to the site on Pitt Street to connect to the shared path - provided as per the Precinct Plan.
- Pedestrian access points off Lawson Road to increase permeability of the site.
- The building entries to the office components of Warehouse 1 and 2 face Lawson Road to reinforce passive surveillance and activation.

LEGEND

	Site Boundary
	Pedestrian Circulation (Public)
	Pedestrian Circulation (Private)
	Light Vehicle Circulation
	Heavy Vehicle Circulation
	Sub-arterial Road (Pitt Street)
	Collector Road (Lawson Road)
	Pedestrian Access Points
	Vehicular Access Points
	Undercroft Parking
	Road Widening along Pitt St and Lawson Rd

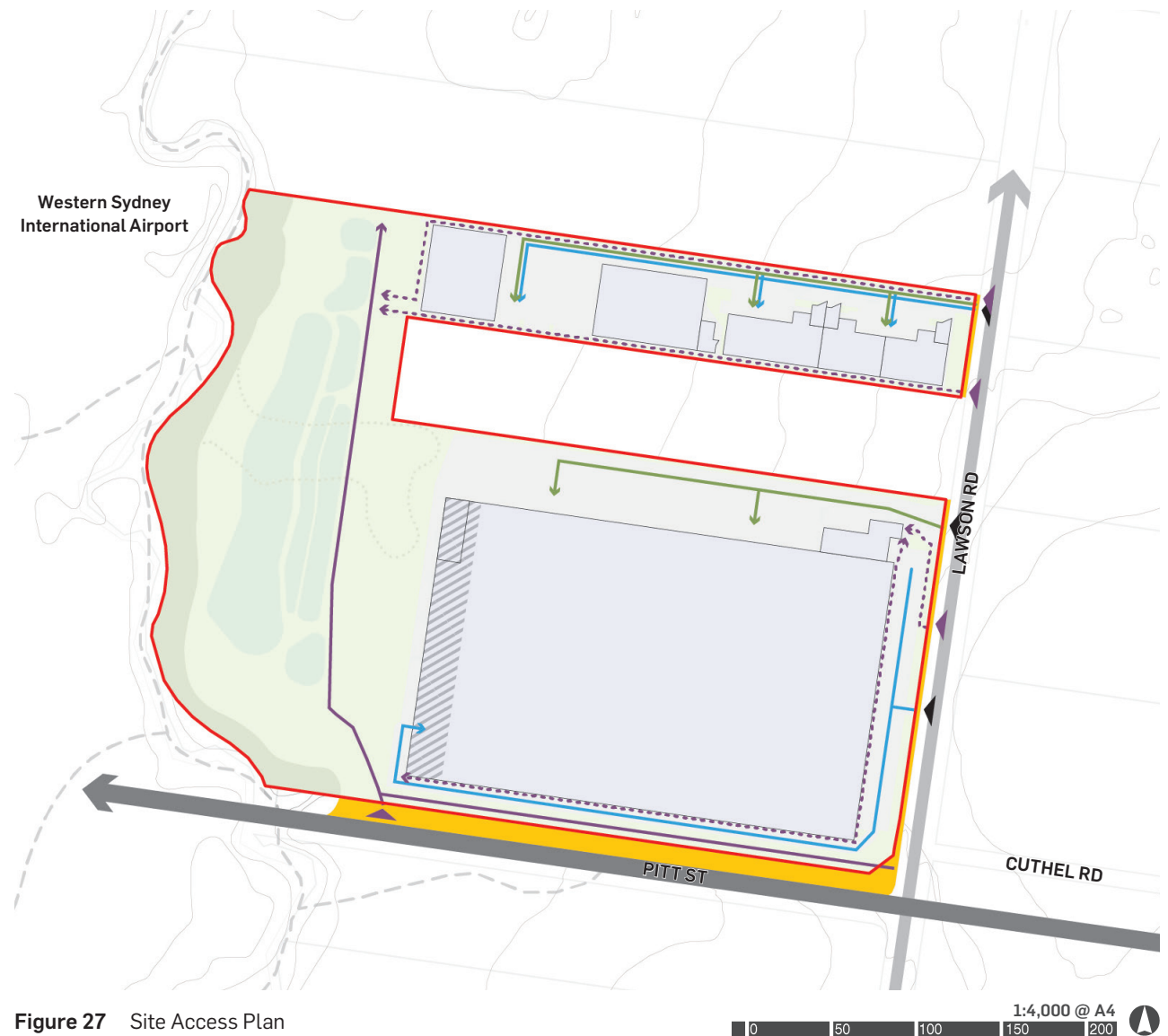


Figure 27 Site Access Plan

8.1.3 Frontages and Interface

Due to the nature of warehouse built form and functions:

- Long stretches of blank wall are minimised through articulation and perimeter landscaping which reinforces narratives of Country.
- For Warehouses 1 and 2 which face Lawson Road, their office components and entries are street facing to provide active frontages. .
- Built form frontages along the site's entire perimeter are softened by generous landscape setbacks.
- All façades facing streets and the riparian corridor are architecturally presented to be of high quality, fine-grain detailed and visually pleasing - see typical architectural details over page.

LEGEND






-  Site Boundary
-  Single Storey Warehouse Interface
-  2 Storey Office Interface
-  Landscape Interface
-  Interface to Proposed Landscape and Stormwater Infrastructure



Figure 28 Interfaces Plan





Figure 29 View of Warehouse 1 Ancillary Office from Lawson Road frontage



Figure 30 View of Warehouse 1 from public open space at the rear of the site



Figure 33 View of Warehouse 5 from public open space at the rear of the site



Figure 31 View of Warehouse 1 from Pitt Street



Figure 32 View of Warehouse 2 Ancillary Office from Lawson Road frontage

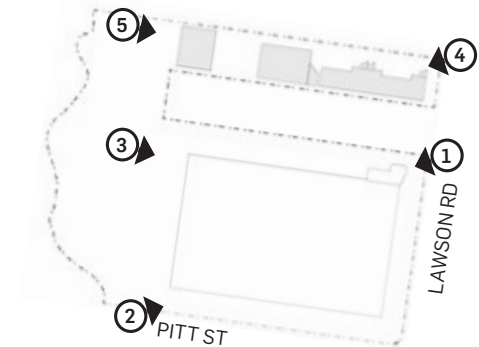




Figure 34 Proposed Office Concept Scheme (3D) - Warehouse 1

Source: SBA



Figure 35 Proposed Office Concept Scheme (3D) - Warehouses 2-4

Source: SBA

8.2 Landscape

The development proposes a high quality landscape response, consisting of:

- Landscape buffer planting to the adjoining properties.
- Breakout spaces and dining areas for the future workers.
- Central gathering space and yarning circle to enhance the connection with Country.
- Gabion wall with associated buffer planting to repurpose the existing stones.
- Pedestrian and cycle track to promote and enhance active transport.
- Flexible lawn and amphitheatre space, creating spaces to promote social interaction.
- Sensory garden, shelter, recreation and BBQ space enhancing the amenity provision of the site.
- Timber platform, to connect with the water.

PRECEDENT IMAGERY





Programs & Destinations

- 1 Landscape buffer planting with proposed trees
- 2 Accessible path connection for staff and workers
- 3 Viewing breakout space with tables and chairs
- 4 Sensory garden and shelter structure
- 5 Central gathering area and yarning circle
- 6 Elevated boardwalk / platform
- 7 Large flexible lawn / amphitheatre
- 8 Through site link - Meandering path to creek with linear swale planting typology
- 9 Maintenance track / Running / Cycle loop
- 10 Future opportunities to extend ecological creek trail
- 11 Vegetation Management Zone - Enhancement and restoration of riparian corridor
- 12 Sculpture and educational garden
- A Sedimentation pond
- B Wetland
- C Bio Basin
- D Storage Pond

Figure 36 Landscape Concept Plan

8.3 Indicative Shadow Diagrams

The shadow analysis has been undertaken by SBA for the proposed Master Plan on the 21st of June between 9am and 3pm.

The high-level analysis demonstrates that there is no adverse impacts to the Creek and proposed landscape areas, with minimal impact to the proposed planting along the Pitt Street frontage.

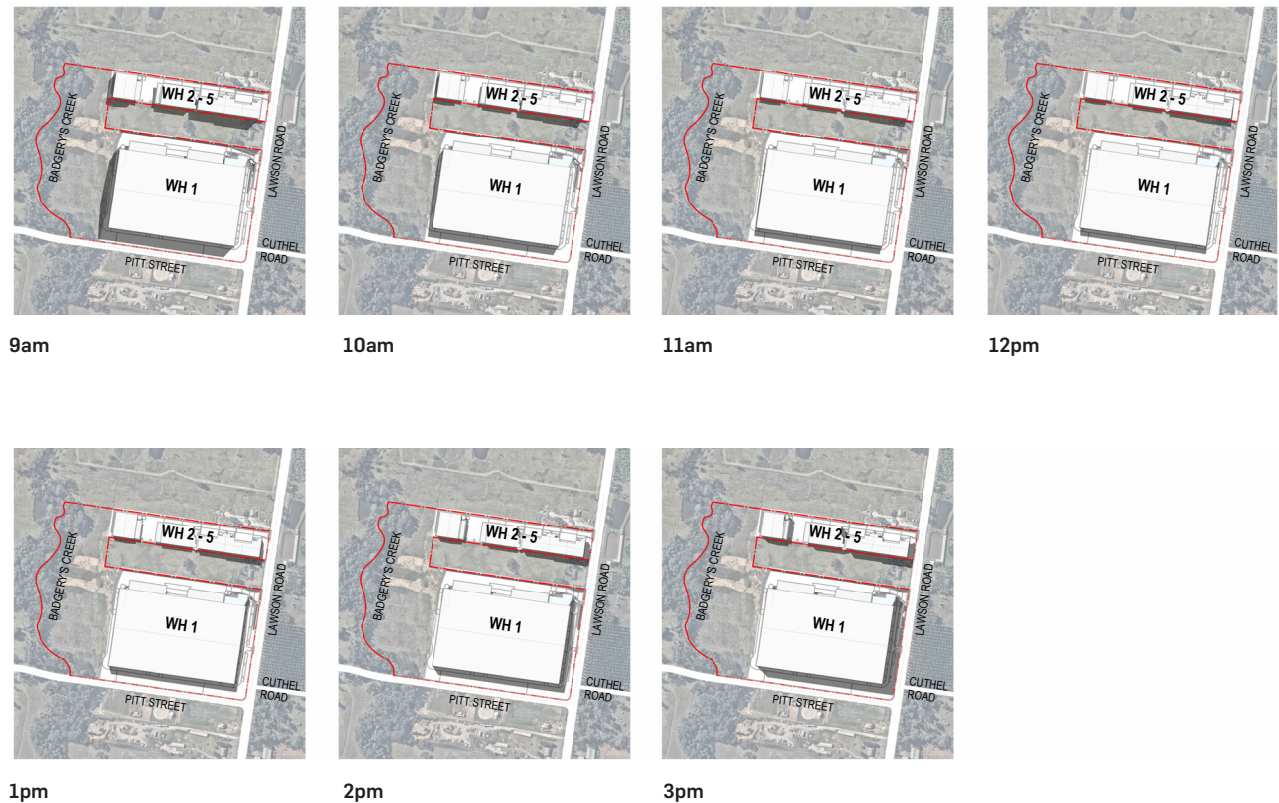


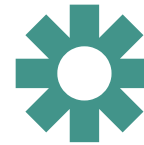
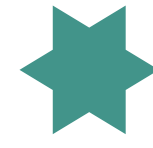
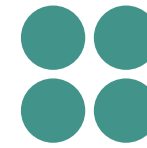
Figure 37 Shadow Diagrams - Winter Solstice


Source: SBA

8.4 Response to Better Place Objectives

The proposed built form, massing and landscape achieves the GANSW Better Place objectives in an integrated manner as follows:

- The proposed master plan responds to the context, location, and connection to Badgerys Creek, while balancing the industrial and workplace land use objectives of the Western Sydney Aerotropolis Plan.
- The proposed workplaces are set within a safe, attractive and comfortable public domain that balances on-site security requirements with connectivity to future public and active transport.
- The blue-green network is maintained and enhanced by
- Sustainability initiatives such as carbon offsets are achieved through the provision of generous landscape areas, reuse of the existing stones for the retaining walls and permeable pavers.
- The orientation of the building, potential PV panels on the roof and sun-shading devices will contribute to the sustainability approach.
- The master plan also aims to meet landscape deep soil zones requirements to support healthy blue-green networks and improved tree canopy to address reduction of carbon and energy footprint.





9. RESPONSE TO PRECINCT PLAN

9.1 Land Use

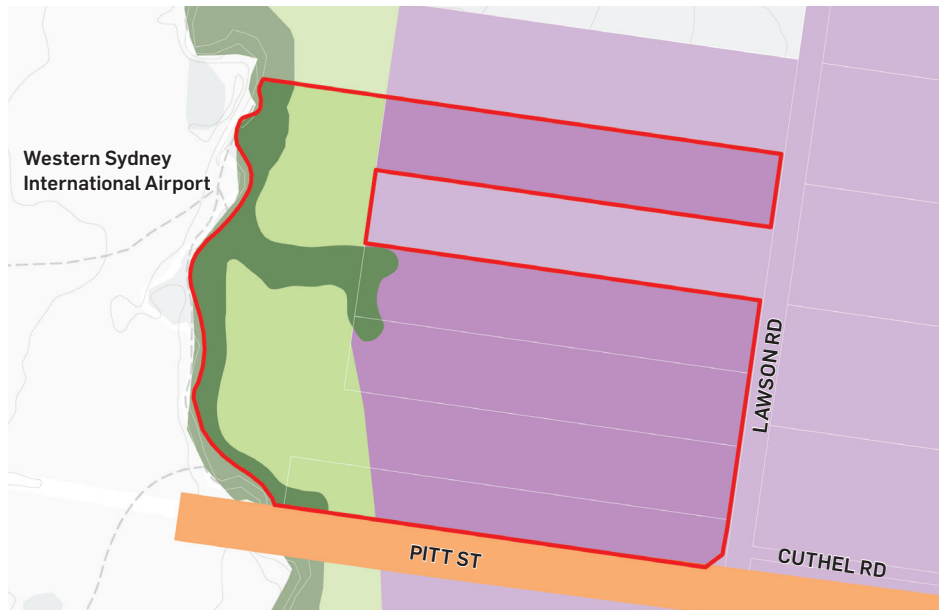


Figure 38 Aerotropolis Precinct Plan 2024 - Land Use Plan

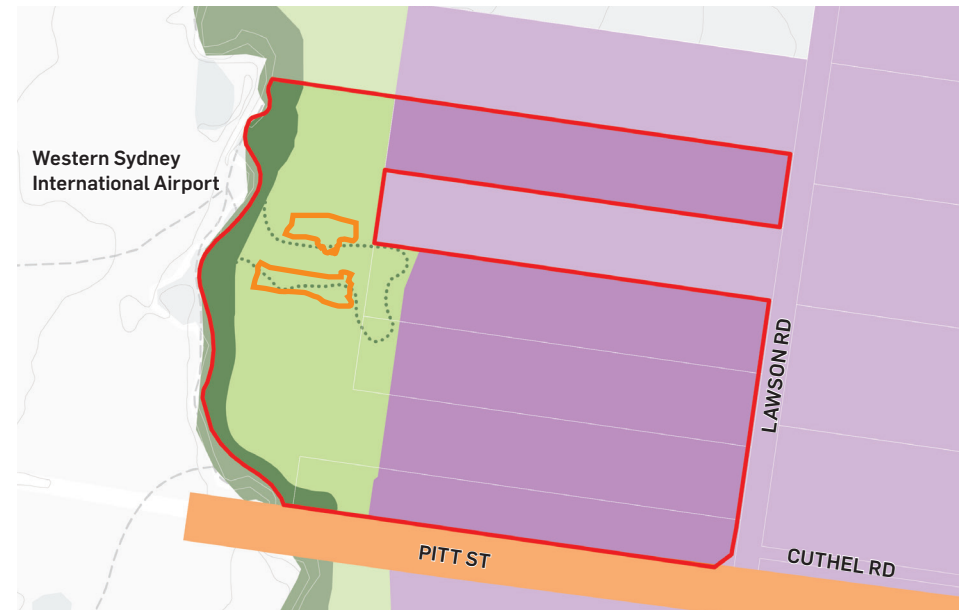








Figure 39 Proposed Concept Scheme - Land Use Plan

LEGEND

-  Site Boundary
-  Enterprise and Light Industry
-  Open Space / Stormwater Land
-  Environment and Recreation
-  Sub-arterial Road
-  Cleared Land (Source ELA)

DESIGN RATIONALE

- The use distribution specifically for the enterprise and light industry area has been altered to respond to the format of a large warehouse.
- Based on the biodiversity assessment undertaken, majority of the identified environment and recreation area east of the creek are sited with previously cleared land or exotic grass.



9.2 Total Water Cycle Management

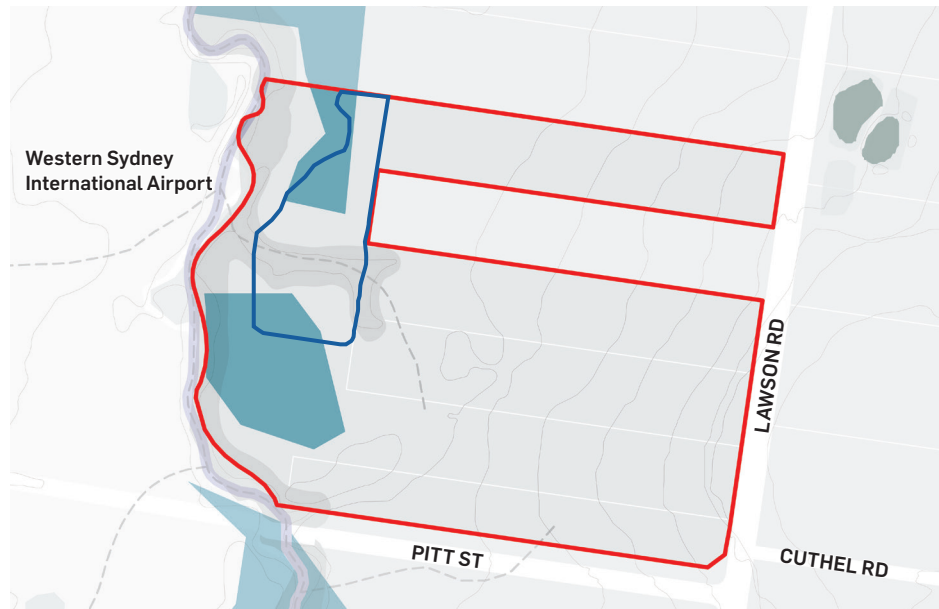


Figure 40 Aerotropolis Precinct Plan 2024 -Total Water Cycle Management Plan

LEGEND

- Site Boundary
- Creeks
- Stormwater Infrastructure
- 1% AEP Flood
- Water body
- Proposed Sydney Water Stormwater Basin

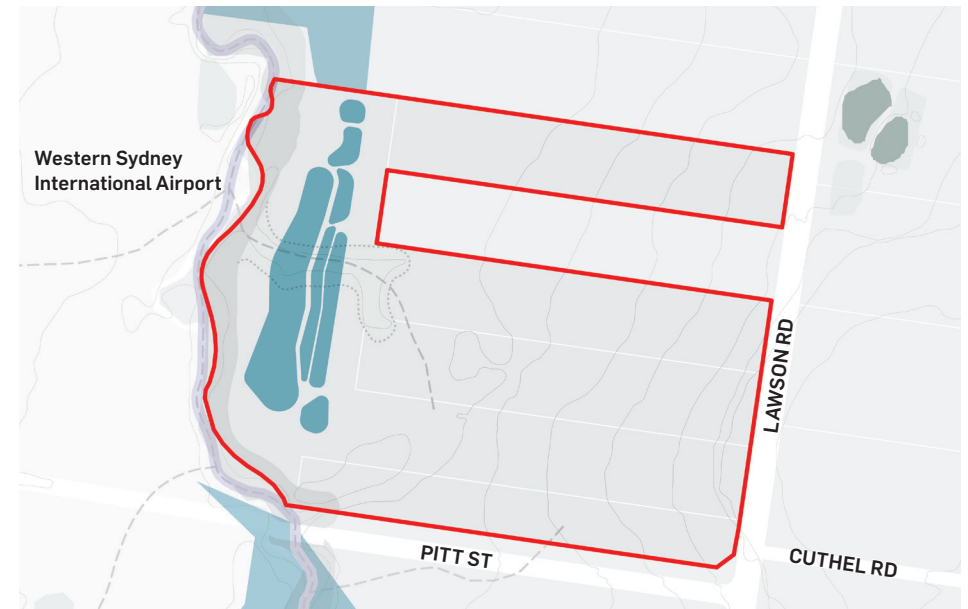


Figure 41 Proposed Concept Scheme - Total Water Cycle Management Plan

DESIGN RATIONALE

- The proposed stormwater basin by Sydney Water currently encroaches the identified environment and recreation zone of the Precinct Plan. However, as identified in the land use comparison map, this area has been cleared.
- The design of the proposed stormwater infrastructure within the western landscape zone has been rationalised to enhance the pedestrian movement/ experience and interface to the creek.



9.3 Street Hierarchy

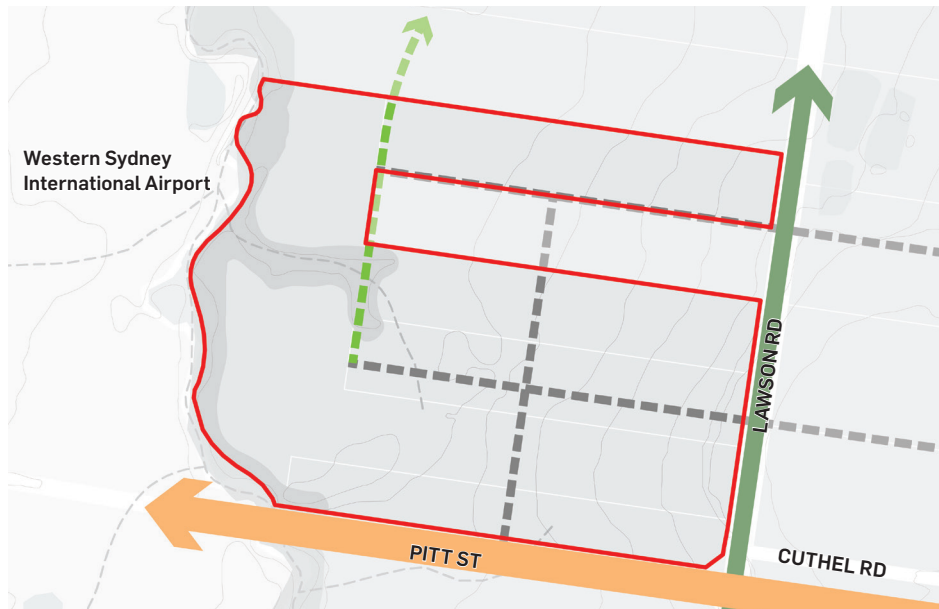


Figure 42 Aerotropolis Precinct Plan 2024 - Street Hierarchy Plan

LEGEND

	Site Boundary
	Sub-arterial Road
	Collector Road
	Local Street
	Park Edge Street
	Provision for Park Edge Street

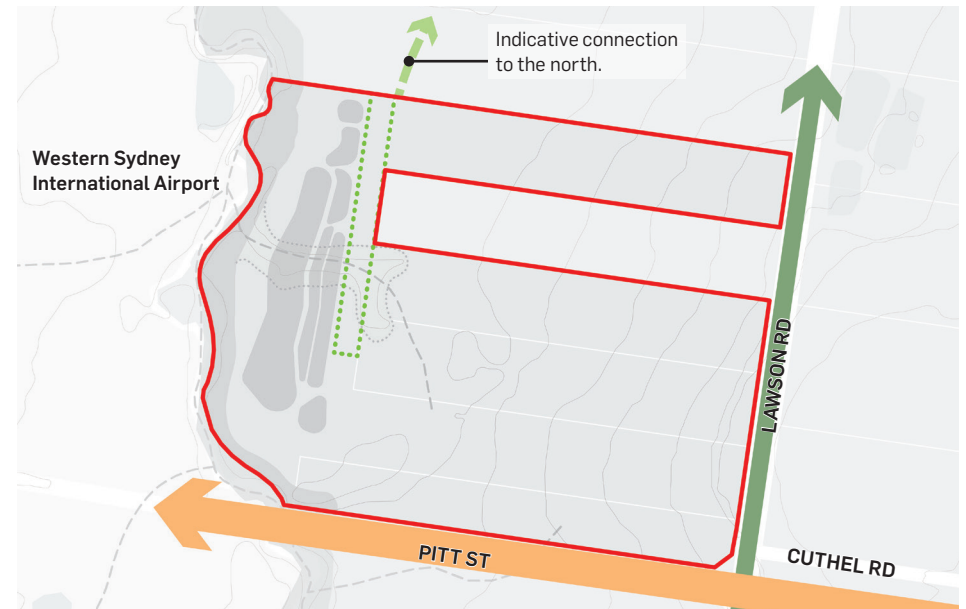


Figure 43 Proposed Concept Scheme - Street Hierarchy Plan

DESIGN RATIONALE

- Local streets removed as they would result in subdivision of the land which would limit land use efficiency by preventing large industrial warehouse development.
- Introducing any local road(s) connecting with Lawson Road, and in proximity to the Pitt St/ Lawson Rd intersection, is not considered a desirable outcome from traffic planning/ modelling perspective.
- The Park Edge Street provision has been safe guarded and future proofed by providing a landscaped buffer. The short-term outcome is to deliver a landscaped area to maximise public amenity.



9.3.1 Park Edge Road

The Precinct Plan proposes a Park Edge Road that runs through the western quadrant of the site. However, the delivery and indicative timeframe for this road remains uncertain, as they are largely dependent on the development of the lots to the north of the subject site, including the Western Sydney International Airport site. There is no indication of the street being provided in the WSA land.

In the short term, a provision for the road has been incorporated into the landscape design, which has the potential to realise the vision of the Precinct Plan.

The proposed corridor for the Park Edge Road has been shifted further west to reduce reliance on 135 Lawson Road for the road's delivery. This adjustment aims to facilitate a more efficient and feasible implementation of the road, aligning with the overall goals of the Precinct Plan.

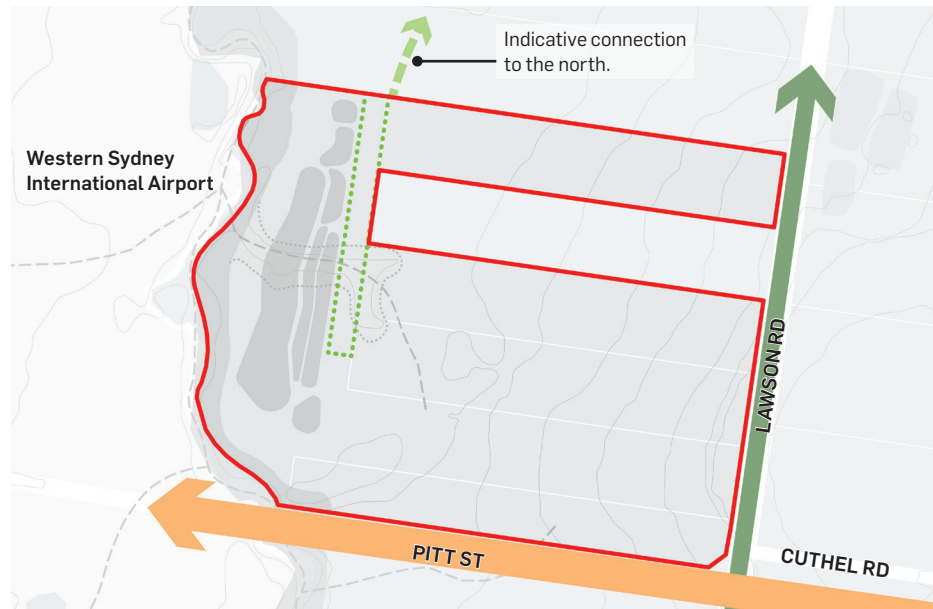








Figure 44 Proposed Concept Scheme - Street Hierarchy Plan

LEGEND

-  Site Boundary
-  Sub-arterial Road
-  Collector Road
-  Local Street
-  Park Edge Street
-  Provision for Park Edge Street



9.4 Public and Active Transport

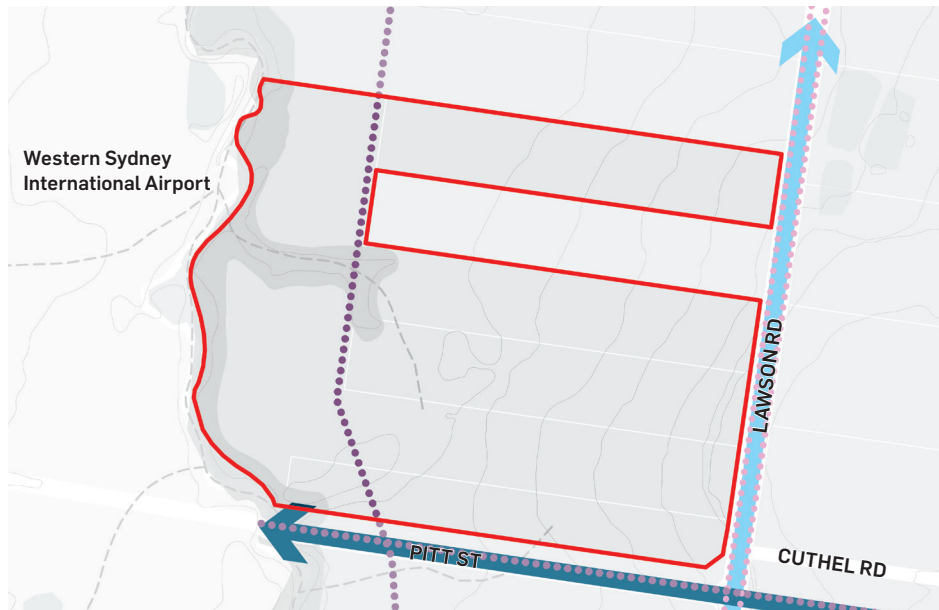


Figure 45 Aerotropolis Precinct Plan 2024 - Transport Network and Active Transport Network Plan

LEGEND

- Site Boundary
- ↔ Rapid Bus Corridor
- ↔ Indicative Local Bus Network
- Principal Regional Cycle Path
- Cycle Paths on Collector Roads

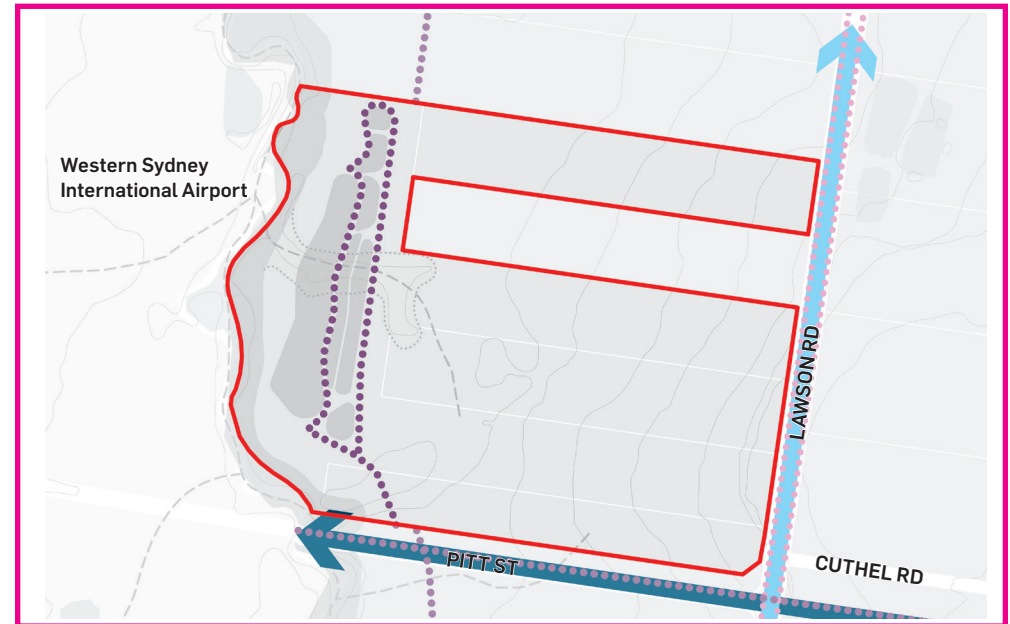


Figure 46 Proposed Concept Scheme - Transport Network and Active Transport Network Plan

DESIGN RATIONALE

- The proposed scheme aligns with the Aerotropolis Precinct Plan and provides additional active transport paths which weave between the basin system.



9.5 Setbacks

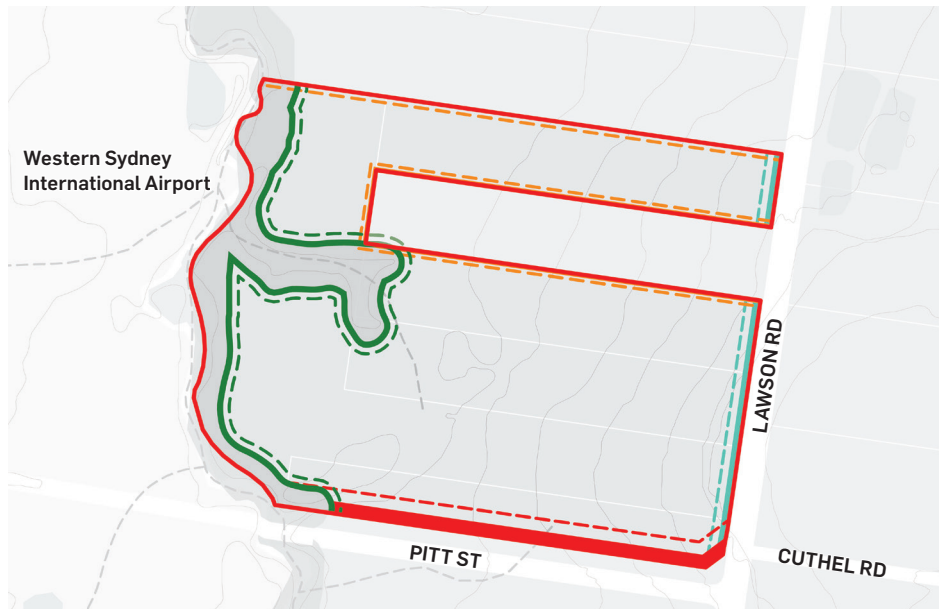


Figure 47 Western Sydney Aerotropolis DCP 2022 - Minimum Building and Landscape Setbacks

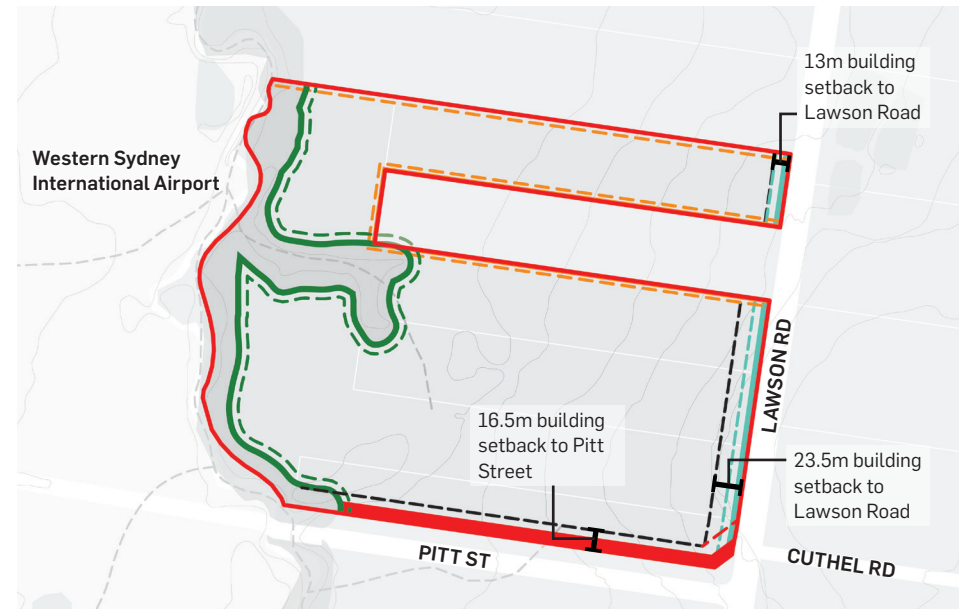


Figure 48 Proposed Concept Scheme - Building and Landscape Setbacks Plan

LEGEND

	Site Boundary		5m building setback to side boundaries
	20m building setback to sub-arterial roads		10m building setback to Environment and Recreation land
	10m minimum landscape width to sub-arterial roads		5m minimum landscape width to Environment and Recreation land
	12m building setback to collector streets		Variation to DCP setbacks (dimension noted on plan)
	6m minimum landscape width to collector streets		

DESIGN RATIONALE

- Reduced setback to Pitt Street due to treatment as a secondary frontage.
- All other proposed building and landscape setbacks respond to the DCP requirements.



9.5.1 Setback along Lawson Road

A 12m building setback and a 6m landscape setback are to be provided from the site boundary for Lawson Road (Collector Roads) in accordance with the Aerotropolis DCP.

The proposed master plan exceeds these requirements by providing a 23.5-metre building setback fronting Warehouse 1, while maintaining the 6-metre landscape setback as stipulated by the DCP. The additional 11.5 metres in the building setback is designed to accommodate surface car parking. This parking area will feature bays interspersed with tree planting, enhancing the landscape character along Lawson Road and contributing to a greener, more aesthetically pleasing environment.

The proposed development across Lawson Road offers a building setback of 19.5 metres, which creates an appropriate scale and streetscape interface.

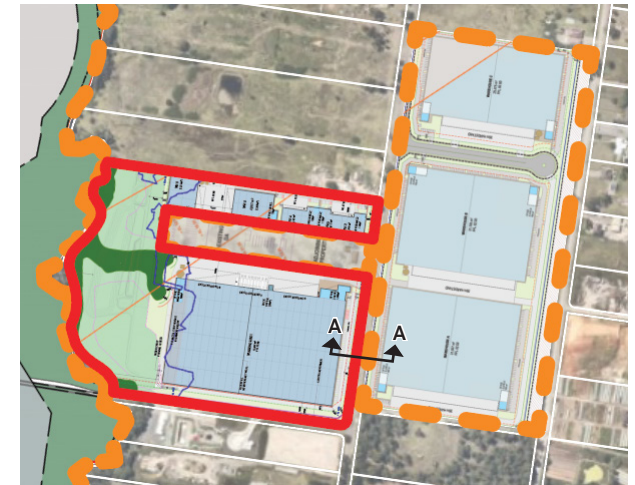


Figure 49 Proposed Concept Scheme and Surrounding Developments

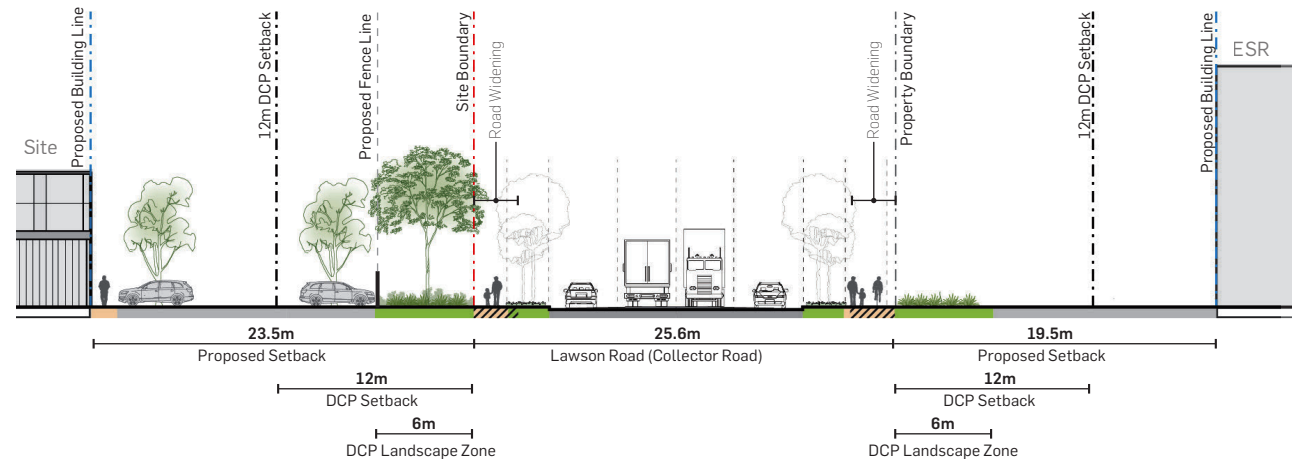


Figure 50 Indicative Section A-A

9.5.2 Setback along Pitt Street

The Precinct Plan identifies Pitt Street as a 40m wide sub-arterial road. This requires a land dedication of approximately 21.65m along Pitt Street. The sub-arterial road is designed to include active transport and landscaped areas along its edges.

Given the site's corner location, Lawson Road is treated as the primary frontage, while Pitt Street is the secondary frontage. According to the DCP, a secondary road frontage should have a 5m building setback and a 3m landscape setback from the site boundary.

A 16.5m setback has been provided along Pitt Street, which includes a 10m landscape setback to align with the future character of Pitt Street including proximity to the pedestrian crossing at the junction of Pitt Street and Lawson Road.

The design treatment of the setback provides for a landscape led and high-quality pedestrian orientated connection to the creek (and importantly clear of any vehicle interactions/ interfaces as there are no street intersections or driveways anticipated along Pitt St as advised by TfNSW). Hence active transport and visual amenity has been prioritised along this frontage.

Additionally, a fence has been provided along the landscape setback to incorporate this area as part of the publicly accessible public domain.

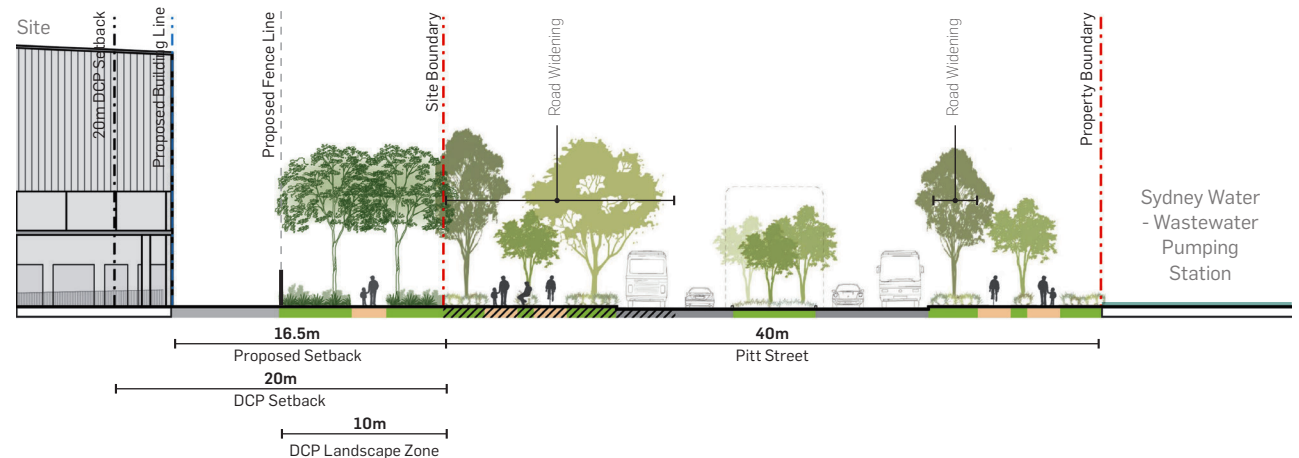


Figure 52 Indicative Section B-B

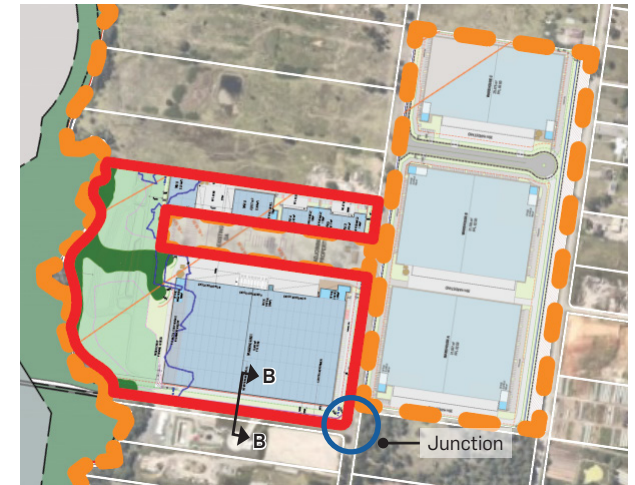


Figure 51 Proposed Concept Scheme and Surrounding Developments



Source: SBA

