

Preliminary Construction Traffic Management Plan

Mixed use development including infill
affordable housing

Prepared for

Southern Cross Community Housing

Site address

Lot 1 DP 520502

Lot 129 DP3060

Lot 110 DP131219

25 Moss Vale Road, Bomaderry

Date

30th October 2024

allen price & scarratts pty ltd
land and development consultants

Surveying



Town Planning



Civil Engineering



Project Management



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Nowra Office: 75 Plunkett Street • PO Box 73, Nowra 2541

Kiama Office: 1/28 Bong Bong Street

Wollongong Office: Suite 1, Level 2, 83-85 Market Street

tel 02 4421 6544 • email consultants@allenprice.com.au

ABN 62 609 045 972

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Construction Traffic Management Plan for Early Works

Mixed use development including infill affordable housing

Prepared for

Southern Cross Community Housing

Location

Lot 1 DP 520502
Lot 129 DP3060
Lot 110 DP131219

25 Moss Vale Road, Bomaderry

Prepared by

Allen, Price & Scarratts Pty Ltd
Land and Development Consultants
75 Plunkett Street
NOWRA NSW 2541

Author



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Benjamin Innis
Civil Engineer

Table of Contents

1.0 Introduction	5
2.0 Project Site & Locality	5
3.0 Project Description	6
4.0 Construction & Traffic Management Plan Items	6
4.1 Haulage routes	6
4.2 Truck Movements	6
4.3 Load Protection	7
4.4 Sequencing	8
4.5 Proposed Management Methods	8
4.6 Timeframe Commencement & Completion	8
4.7 Impact Mitigation On Other Road Users	9
4.8 Retain Amenity Of Adjoining Neighbours	9
4.9 Parking Arrangements For Construction Employees and Contractors	9
4.10 Monitoring & Maintenance of Haulage Routes	9
4.11 Details of fill Storage, Disposal and Materials Haulage Routes	9
4.12 Traffic Impact on Adjoining Lots	10
5.0 Relevant Contact Numbers	10

Table of Revisions

Rev	Date	Details
0	October 2024	ISSUED FOR CONCEPT APPROVAL

1.0 Introduction

Allen Price & Scarratts are assisting Southern Cross Community Housing to oversee a package which has been prepared in accordance with the Planning Secretary's Environmental Assessment Requirement's (SEAR's) as shown in Figure 1.

- Provide a Construction Traffic Management Plan detailing predicted construction vehicle routes, access and parking arrangements, coordination with other construction occurring in the area, and how impacts on existing traffic, pedestrian and bicycle networks would be managed and mitigated.

Figure 1 – SEARs Excerpt (Ref SSD-69257218)

It is understood that the CTMP will form part of the approved documentation which is to be held on site and always be readily available during the preliminary remedial works.

2.0 Project Site & Locality

The site is located at 25 Moss Vale Road, Bomaderry within the Shoalhaven City Local Government Area (LGA). The site comprises three allotments, which are all owned by Southern Cross Community Housing and are legally described as Lot 110 in DP 131219, Lot 1 in DP520502 and Lot 129 in DP3060. The lots have a combined area of 8.5 hectares.

Formerly, the site was used as a cattle yard, however, as of 31/10/2024, it lies vacant. As detailed within the EIS prepared by Beam Planning, the site is subject to a deferred development consent (SF10851) for Torrens Title subdivision and a series of site preparation works, which was approved by Shoalhaven City Council in December 2022.

The site is located approximately 2.2km northwest of Bomaderry Station and 350m west of the A1 Princes Highway. Approximately 5.2km south of the site is the suburb of Nowra, a major commercial centre servicing the south coast suburbs of NSW. A summary of the surrounding development is provided below:

- North: The site is bound by Moss Vale Road to the north. Directly across the site is a large area of R5 Large Lot Residential zoned properties. Further north of the site is farmland, which is earmarked for future residential development as part of the Moss Vale Road North Urban Release Area.
- East: Immediately east of the site is bushland and Moss Vale Road. To the northeast of Moss Vale Road is detached residential dwellings, a kindergarten and highway retail uses along the Princes Highway. The Bomaderry Town Centre is located approximately 550m southeast of the site and comprises major services and facilities, including a supermarket, high school, aquatic centre, sporting oval and fast-food retailers. This area also contains slightly denser residential uses within low and medium residential zoned land.
- South: Immediately south of the site is the Bomaderry Creek nature reserve and Bomaderry Creek Regional Park. Further to the south is the suburb of North Nowra, which extends down to the Shoalhaven River, a perennial river running 327km in a north easterly direction from Braidwood to the Pacific Ocean east of Nowra.
- West: The Bomaderry creek riparian corridor runs along the western boundary of the property. Further west of this is the Waymount Farm, a rural property including tourist and visitor accommodation surrounded by Bomaderry Creek. Beyond this is the Moss Vale Road South Urban Release Area, which is a new subdivision currently being developed.

3.0 Project Description

The proposed development is to create 198 dwellings, including attached dwellings, six residential flat buildings and shop top housing, commercial tenancies, a boarding house, a childcare centre, a medical centre, at grade parking, communal open space and public domain works from Lot 1 DP 520502, Lot 129 DP 3060 and Lot 110 DP 131219 2 DP 1093337 (as shown in supporting plans).

The proposal involves the construction of roads, drainage (and associated water quality devices), sewer, water, electrical and telecommunications to service the proposed development

This CTMP is for the SEAR's package, Application number SSD-69257118.

- Sediment & erosion controls.
- Construction of roads, drainage (and associated water quality devices), sewer, water, electrical and telecommunications to service the proposed development.

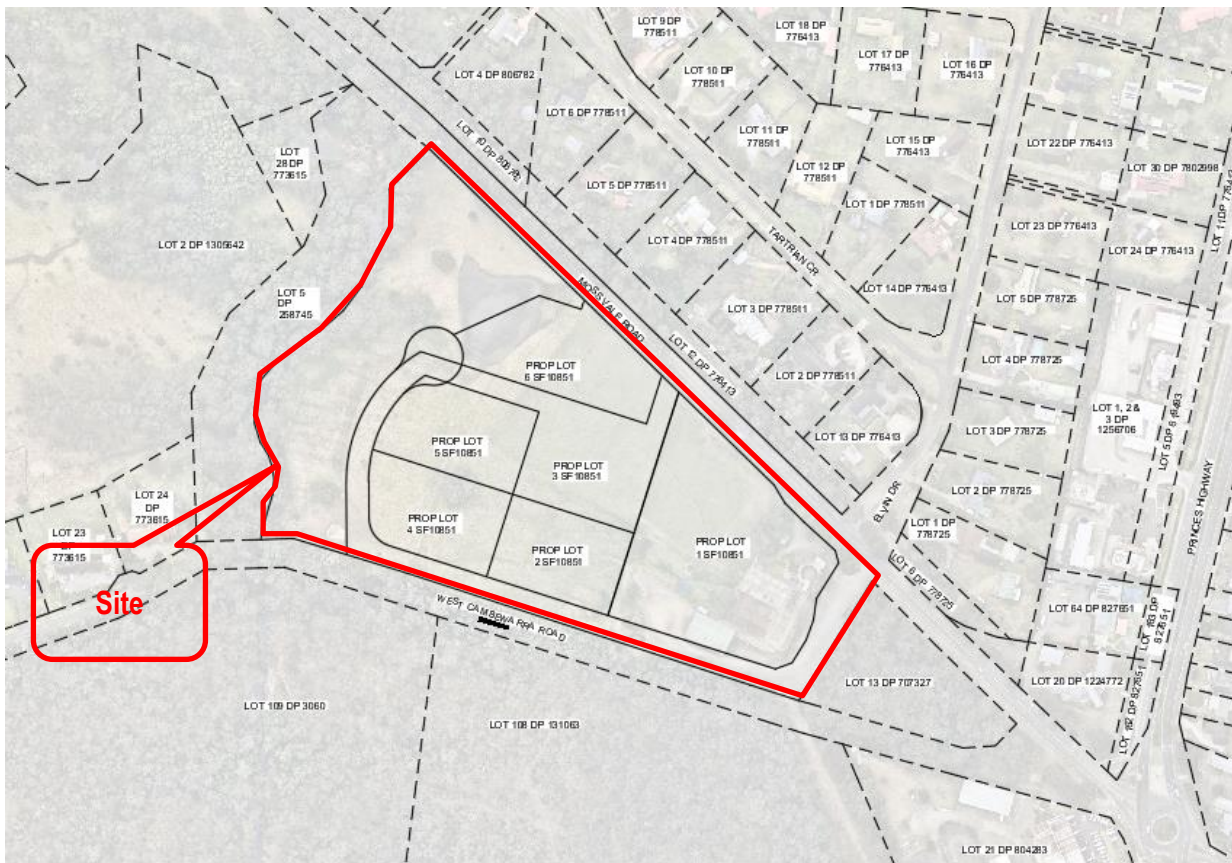


Figure 2 – Site Location Plan (Ref Near Maps)

4.0 Construction & Traffic Management Plan Items

4.1 Haulage Routes

Figure 3 below shows the proposed haulage routes to be used to enter and exit site.

4.2 Truck Movements

Trucks will be moving both internally on site and on the public road network.

Internal movements will include but not necessarily limited to:

- Distribution of materials within the site.
- Movement of plant and equipment within the site.
- Employees traffic between parking areas and point of access from Moss Vale Road.

External movements will include but not necessarily limited to:

- Plant and equipment delivery associated with site establishment and disestablishment. These movements will be limited to the start and completion of the works .
- Delivery of construction materials, short term usage plant and removal of construction wastes. These movements will occur periodically throughout the construction period generally during work hours.
- Site employees arriving and departing the site. These movements will be daily during the duration of the works.

The Princes Highway and Moss Vale Road are major roads with sufficient capacity to cater for traffic generated by construction activities. Local temporary traffic control plans can be established to mitigate risks associated with construction traffic.

4.3 Load Protection

All loads travelling off site will be covered with appropriate load coverings.

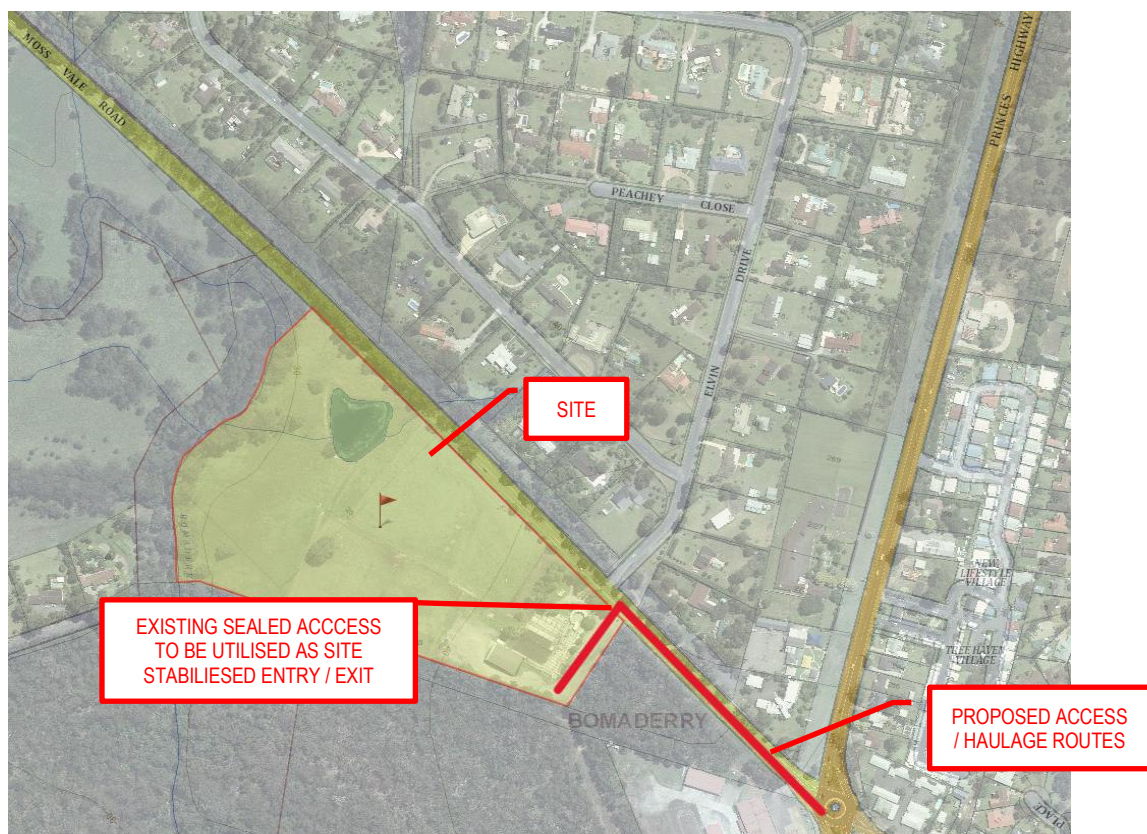


Figure 3 – Site Haulage Route. Source SixMaps

4.4 Sequencing

It is envisaged that works will take place in the following order:

- a) Site Establishment (including site office and compound).
- b) Maintenance of proposed site haul roads to ensure trafficability (by proposed vehicles).
- c) Establishment of sediment and erosion controls.
- d) Construction of roads, drainage, sewer, water, electrical and telecommunications to service the proposed development.
- e) Re-vegetate/rehabilitate areas construction zones.
- f) Dis-establish from site.

4.5 Proposed Management Methods

The Contractor will comply with the relevant management plans specified in the future development consent.

Contractors are to comply with the following rules when travelling to/from and on-site.

External to Site

- Obey all road rules.
- Access the site via the site entrance on Moss Vale Road.
- Drive in a manner that minimises noise, e.g. do not necessarily rev motors, minimise use of engine brakes, do not sound horns unnecessarily.
- Watch for pedestrians and cyclists which often use Moss Vale Road.
- When exiting the site, stop at the property boundary and check for oncoming traffic before entering Moss Vale Road.
- Before exiting the site ensure all loads are properly secured and covered.
- Always be courteous to local traffic and pedestrians.
- Always exit the site in a forward direction.

On Site

- Site speed limit is 20km/hr, unless stated otherwise.
- No use of engine brakes on site.
- Use designated haul roads. Do not drive on lots/conservation lot unnecessarily.
- Give way to heavy vehicles.
- Minimise reversing.
- Do not leave unattended vehicles running.
- Staff vehicles are to park in the construction parking area only.

Contractors will be required to stick to the proposed haulage routes whilst on site – these routes are to be determined by the contractor.

4.6 Timeframe for Commencement & Completion

It is envisaged that the project will take approximately 30 weeks to complete. Actual works for completion will be confirmed by the contractor prior to commencing works.

4.7 Impact Mitigation On Other Road Users

A single point of access from Moss Vale Road is proposed. Access to individual building sites will be from internal roads subject to traffic control plans developed for the relevant construction certificates.

Impact on existing road users will be mitigated by ensuring that vehicles entering/exiting the site comply with road rules, and establishment of traffic control at and within the site (details to be finalised as construction proceeds).

4.8 Retain Amenity of Adjoining Neighbours

The following actions will be taken to retain the amenity of adjoining neighbours:

- Compliance with road rules.
- Stabilised site access/rumble grid.
- Unless otherwise approved or restricted by the development consent, working hours shall be between 7am to 6pm (Monday to Friday) and 8am to 3pm (Saturday). No works are to take place on Sunday or Public Holidays.
- Compliance with work practices as outlined in the NSW Department of Environment & Climate Change Interim Construction Noise Guideline.

4.9 Parking Arrangements for Construction Employees and Contractors

The proposed parking area is to be determined by the contractor at a later stage, but generally should be contained to an area on-site accessible during all-weather events and near provided amenities.

4.10 Monitoring & Maintenance of Haulage Routes

Prior to commencement of works, a dilapidation report shall be prepared by the developer in accordance with Condition 15 of SF10851 and any other relevant condition of development consent.

Prior to commencement of works the developer shall nominate the organisation / person responsible to undertake monitoring and maintenance of both public and internal haul roads.

A monitoring inspection shall be undertaken each day at the start and end of each work shift. The details of the inspection shall be recorded and shall include, date, time, weather, condition of the haul road, immediate maintenance required, and undertaken, future maintenance required and scheduled. Records to be always maintained on site.

Maintenance of both public and internal haul roads shall be undertaken promptly in order ensure progressive removal of debris ensure prompt rectification of damage and maintain pedestrian and traffic safety.

4.11 Details of Fill Storage, Disposal and Materials Haulage Routes

Imported fill stockpile will be stored in dedicated locations that will minimise sediment and erosion run-off.

The external and initial site haulage routes are shown in Figure 3 above.

4.12 Traffic impact on adjoining lots

All contractors, sub-contractors shall ensure that all traffic generated by their respective site operations minimises traffic impact on adjoining lots.

Measures include but are not limited to:

- Timing deliveries to avoid impacts with pedestrian and traffic peaks associated with school bus movements
- Ensuring deliveries do not occur outside approved working hours
- Minimising dust generation resulting from vehicle movements on unsealed parts of the site
- Implementing short duration traffic controls as required to minimise disruption of access to and from adjoining lots

5.0 Relevant Contact Numbers

ORGANISATION	CONTACT NUMBER
Emergency Services	000
Shoalhaven City Council	(02) 4429 3111
Allen Price & Scarratts	(02) 4421 6544
SafeWork NSW	131 050
EPA	131 555
Principal Certifying Authority (TBA)	TBA prior to commencing works
Developers Nominated Rep for Monitoring and Maintaining Haul Routes	TBA prior to commencing works