

Option 5:		Concept Plans
Summary	Option 5 design concept retained the built form and general planning of Option 4. However, to address the programming and operational concerns noted in Option 4, this concept proposed to relocate the Secondary School facilities across the two (2) storeys of the western wing. This adjustment allowed for all Primary School facilities to be co-located across the two (2) storeys of the southern wing, and directly connected with the newly formed, two (2) storey eastern wing.	100 Gavenlock (Dushing) Control of the state
Advantages	 Clearly identifiable entry visible from Keefers Glen Clearly defined and independent Primary and Secondary facilities Independent access to Secondary School (ie. not through Primary Circulation/Common area) Deletion of Northern Wing footprint in response to SDRP commentary Reduced cut & fill to existing site impacted by acid sulphate soils Staff visibility of playground areas from Staff Room Removal of Northern Wing footprint increases opportunity to soften transition/levels through playground from south to north Opportunity for additional staff/building facilities and/or COLA at Lower Ground level below northern end of Eastern Wing Increased solar access to playground with removal of Northern Wing 	Secondary Secondary Primary 10 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1
Disadvantages	 8 GLA's/80 students with direct level access from GLA's to playground/outdoor spaces for play and/or regulation 8 GLA's/80 students with direct level access to main entry Reduced landscaped buffer zone to Keefers Glen not supported by Central Coast Council Removal of Northern Wing footprint results in: increased viewing cone to northern neighbour residences reduced acoustic attenuation and privacy to northern neighbour residences reduced viewing cone to north-eastern bushland 	2 Option 5 - Level 2 1:500 Gavenlock Road (Jushland) Dahl Architects Stanton Dahl Architects Stanton Connor Certificial School Stroken Bay Fillen O'Connor Certificial School Elicen O'Connor Catholic School Elicen O'Connor Catholic School
Conclusion	Following review and discussion between the project team, consultants and CSBB, this concept was selected as the preferred option to move forward with due to: o Option 5 best addressing the functional and operational requirements of the School o The concept best addressing the SDRP and Central Coast Council comments with regards to removal of the northern wing This concept was therefore progressed to form Phase 5 of the Design Development.	Catholic Schools Charlotte Scho



	Option 6a:	Concept Plans
Summary	Option 6a tested a significantly different approach by relocating all parking from the western and eastern carpark to form a single, consolidated carpark on the southern side of the project site. This concept included a predominately two (2) storey built form to the western, southern and eastern wings, with a portion of the northern end of the western wing three (3) storeys to accommodate service and maintenance areas on the Lower Ground level. To accommodate the revised parking scheme, the building was required to shift further north, reducing the length of the western and eastern wings. This scheme also reduced the depth of open, outdoor area for the playground between the southern wing and the existing northern boundary. It was noted that this would present a significant challenge to levels in the playground due to the shorter distance to manage the now steeper site falls from the south to north. Removal of the western carpark allowed the building footprint to be extended further west, with a reduced setback to the western boundary. In doing so, this offset the shortening of the western and eastern wings to provide sufficient footprint to accommodate all functional requirements for the Primary and Secondary Schools.	The last being bei
Advantages	 Deletion of Northern Wing footprint in response to SDRP commentary Car parking consolidated and relocated along southern boundary in response to SDRP commentary No vehicular traffic along northern boundary Staff visibility of playground areas from Staff Room Opportunity for additional staff/building facilities and/or COLA at Lower Ground level below northern end of Eastern Wing Increased solar access to playground with removal of Northern Wing Increased landscaped buffer zone to Keefers Glen in response to Central Coast Council commentary 	Option 6a - Level 1 1:500 Gavenlock Road (bushland) Formation Shorter Street Secondary Administration Primary Secondary Administration First Secondary Option 6a - Level 1 Inches Secondary Option 6a - L
Disadvantages	 Main entry to School shifted to southern Wing and out of direct line of sight from the Keefers Glen Increased building height at northern end of Eastern and Western Wings in close proximity to northern boundary Access to Secondary School through Primary Circulation Common area 9 GLA's/90 students with direct level access from GLA's to playground/outdoor spaces for play and/or regulation 9 GLA's/90 students with direct level access to main entry Building footprint further north decreased dimension to northern boundary which in turn reduced area for playground and increased steepness in level changes/transition from south to north. Removal of Northern Wing footprint results in: increased viewing cone to northern neighbour residences reduced acoustic attenuation and privacy to northern neighbour residences reduced viewing cone to north-eastern bushland Restricted area for bushfire emergency vehicle access along northern boundary 	2 Option 6a - Level 2 Ti.500 Stanton Dahl Architects Fileen O'Connor Controls School Graventock Road Architects Fileen O'Connor Controls School Catholic
Conclusion	Following review and discussion between the project team, consultants and CSBB, this concept was deemed unsuitable and therefore excluded due to: o reduced setbacks to the northern boundary o the significantly reduced area for playground and impact on outdoor levels/transitions	And sufficient and the state of



	Option 6b:	Concept Plans
Summary	Option 6b design concept followed the similar built form and general planning of Option 6a. This concept, however, tested the SDRP's recommendation of cutting and setting the lower level of the building into the southern slope of the site in order to reduce the overall building height and provide a single, level, albeit smaller, playground area at the lower level. This converted the building into a two (2) storey form across all 3 wings, and did not allow for onsite service and maintenance areas. The carpark area on the southern side of the project site at street level was maintained and the main entry to the School shifted to the upper level of the southern wing	To the thing has been been been been been been been bee
Advantages	 Deletion of Northern Wing footprint in response to SDRP commentary Car parking consolidated and relocated along southern boundary in response to SDRP commentary No vehicular traffic along northern boundary Staff visibility of playground areas from Staff Room Playground all at same level (Level 1) Increased solar access to playground with removal of Northern Wing Increased landscaped buffer zone to Keefers Glen in response to Central Coast Council commentary 	Option 6b - Level 1 1:500 Gavenlock Road (Dushland) For Francisco Buttle Signature Control Administration Primary For Management Control Secondary For Francisco F
Disadvantages	 Main entry to School shifted to southern Wing and out of direct line of sight from the Keefers Glen Increased cut & fill to existing site impacted by acid sulphate soils Reduced natural light to lower level Access to Secondary School through Primary Circulation Common area 9 GLA's/90 students with direct level access from GLA's to playground/outdoor spaces for play and/or regulation 9 GLA's/90 students with direct level access to main entry Building footprint further north decreased dimension to northern boundary which in turn reduced area for playground and increased steepness in level changes/transition from south to north. Removal of Northern Wing footprint results in: increased viewing cone to northern neighbour residences reduced acoustic attenuation and privacy to northern neighbour residences reduced viewing cone to north-eastern bushland Restricted area for bushfire emergency vehicle access along northern boundary 	Stanton Dahl Architects Eleen O'Connor Contols School Eleen O'Connor Catholis School Disconting Contols School Disconting
Conclusion	Following review and discussion between the project team, consultants and CSBB, this concept was deemed unsuitable and therefore excluded due to: o reduced setbacks to the northern boundary o the significant extent of cut required to the site which is impacted by acid sulfate soils o the significantly reduced area for playground and impact on outdoor levels/transitions o the reduction of natural light opportunities due to the lower level being set into the slope	December 2018 Control 2018 Cont



	Option 7:	Concept Plans
Summary	Option 7 tested an alternative floor planning arrangement based on the Option 6 concepts with all car parking areas consolidated to the southern side of the site. The planning adjustments included: Predominantly two (2) storey southern and eastern wings, with a portion of the northern end of the eastern wing being three (3) storeys to accommodate service and maintenance areas at the Lower Ground level. Consolidating all administration and shared facilities such as Kitchen/Canteen and the Multi-Purpose Hall to the western end of the southern wing. Locating all Secondary facilities on a single level (upper floor)	The state of the s
Advantages	 Clearly defined and independent Primary and Secondary facilities Deletion of Northern Wing footprint in response to SDRP commentary Car parking consolidated and relocated along southern boundary in response to SDRP commentary Increased solar access to playground with removal of Northern Wing Emergency vehicle access along northern boundary 	1 Option 7 - Level 1 1:500 Gavenlock Road (bushand) Permutual Short Individual Short Indiv
Disadvantages	 Main entry to School shifted to southern Wing and out of direct line of sight from the Keefers Glen Increased building height at northern end of Eastern Wing in close proximity to northern boundary 8 GLA's/80 students with direct level access from GLA's to playground/outdoor spaces for play and/or regulation 8 GLA's/80 students with direct level access to main entry Building footprint further north decreased dimension to northern boundary which in turn reduced area for playground and increased steepness in level changes/transition from south to north. Removal of Northern Wing footprint results in: increased viewing cone to northern neighbour residences reduced acoustic attenuation and privacy to northern neighbour residences reduced viewing cone to north-eastern bushland Reduced landscaped buffer zone to Keefers Glen not supported by Central Coast Council 	2 Option 7 - Level 2 1:500 Gaveniock (bushland) Dahl Architects Stanton Dahl Architects Eileen O'Connor Control: School Broken Bay We WANAGERICHURE Eileen O'Connor Catholic School Control: School Contro
Conclusion	Following review and discussion between the project team, consultants and CSBB, this concept was deemed unsuitable and therefore excluded due to: o reduced setbacks to the northern boundary o the significantly reduced area for playground and impact on outdoor levels/transitions	March, NSW Activation of the control of the contro



4.4.6 Phase 5: Option 5

Summary:

As noted in Phase 4 of the Design Development, Option 5 was selected by the project team and CSBB to be the preferred scheme to progress into the next phase of the design.

The Option 5 proposal encompassed the following works:

- Part 2-3 storey western Secondary School wing comprising:
 - o 6 General Learning Areas across two (2) levels
 - Staff facilities
 - o Specialised Learning Areas including
 - Gym/Fitness, Science, CAPA, TAS and Life Skills
 o Maintenance and storage areas to lower ground
- 2 storey southern and eastern Primary School wings comprising:
 - nprising:

 o 14 General Learning Areas across two (2) levels
 - o Administration and staff facilities
 - o Multi-purpose Hall
 - o Library
- Two (2) new vehicle entry points via Keefers Glen with western and eastern staff carpark areas (connected via a northern driveway)
- Outdoor playground and landscaping

In addition to the above, Option 5 continued to incorporate elements identified during Phase 2 of the Design Development including:

- Increased landscaping buffer along northern boundary
- Increased retention of existing trees.
- Reduction of hard surfaced, paved landscaping through:
 - Reducing onsite staff parking spaces from 71 to 61 parking spaces and 2 onsite School bus overnight parking bays.
 - o Increased landscaped garden areas to western and eastern carparks
- Improved engagement with public domain with the introduction of a public arrival/meeting garden at the Keefers Glen pedestrian entry and increased landscaping to the western frontage
- Simplified playground design, sympathetic and complementing the site terrain.
- Revised roof concept with split roof design incorporated into each wing to allow for additional natural light to GLA's

The northern driveway was retained as part of this concept noting the following:

- Parking spaces to the eastern carpark were proposed to be exclusively used by staff only, resulting in reduced traffic movement, concentrated to staff arriving and departing at the start and end of School hours.
- Similarly, the proposed School bus parking bays would be utilised by the School for overnight parking with the intention being the buses would be offsite on excursions for the majority of the day before returning to site at the end of the school day in time for student pick-ups.

Together with input from the traffic, acoustic and waste management consultant teams, it was deemed the infrequent use of the northern driveway would result in minimal impact to the northern neighbours.





4.4.7 Phase 6: RFS Redesign – Options 8a to 10b

To address access for emergency vehicles in the event of a bushfire, the development for EOCCS proposed that vehicle access could be provided onto the new School site via the Keefers Glen entry and onto the northern driveway interconnecting the western and eastern carparks. It was then proposed that access from the EOCCS northern driveway could extend and connect into the existing St Peters Catholic College site and the College's internal road system to form a complete perimeter access road around both the EOCCS and St Peters Catholic College sites as a whole.

To support this approach, an application was submitted by Bushfire Consulting Services on behalf of CSBB to the NSW Rural Fire Services, based on the bushfire assessment report:

"....seeking New South Wales Rural Fire Service (NSW RFS) approval for an exemption from the National Construction Code (NCC) Part 5 & Planning for Bushfire Protection 2022 Table 3 for Access to exclude the following acceptable solutions for access roads;

- Roads must be capable of providing continuous access for emergency vehicles to enable travel in a forward direction from a public road around the entire building;
- Roads must have a minimum unobstructed width of 6m with no part of its furthest boundary more than 18m from the building and in no part of the 6m width be built upon or used for any purpose other than vehicular or pedestrian movement

On the 28th August 2024, the project team received the adjacent advice letter from the NSW Rural Fire Services, in response to the above application noting that the proposed exemption could not be supported on the basis that:

- the EOCCS development incorporated a new school within a newly created lot and
- in their opinion, the site characteristics and development had the ability "to comply with the acceptable solutions and support a continuous access road for emergency vehicles around the entire building".

To address this feedback, the project team re-visited the current Option 5 design, to further explore opportunities to now incorporate a 6m wide, emergency vehicle access roadway to the full perimeter of the EOCCS building.

To facilitate this discussion, another package of planning and massing studies were undertaken, resulting in the following Options 8a to 10b (14 Options in total)





Catherine Gorrie PO Box 1020 Penrith NSW 2751

NSW 2751 Our reference: DA20240712002859-Original-1

Date: Wednesday 28 August 2024

ATTENTION: Catherine Gorrie

Dear Sir/Madam,

Integrated Development Application s100B - SFPP - School 84 Gavenlock Road Mardi NSW 2259, 9/4/DP3368

I refer to your correspondence dated seeking general terms of approval for the above Integrated Development Application.

The New South Wales Rural Fire Service (NSW RFS) has reviewed the information provided and advises the following:

The bush fire assessment report is seeking New South Wales Rural Fire Service (NSW RFS) approval for an exemption from the National Construction Code (NCC) Part 5 & *Planning for Bushfire Protection* 2022 Table 3 for Access to exclude the following acceptable solutions for access roads;

- Roads must be capable of providing continuous access for emergency vehicles to enable travel in a forward direction from a public road around the entire building;
- Roads must have a minimum unobstructed width of 6m with no part of its furthest boundary more than 18m from the building and in no part of the 6m width be built upon or used for any purpose other than vehicular or pedestrian movement.

The bushfire report seeks an exemption from the acceptable solutions listed above based on existing building location within the school to the south and east, proposed pedestrian access to the south and provision of fire fighting access from the proposed carparks which are stated to be located within 70m of the carparks which provide fire fighting access.

Based on the above, further clarification is required to confirm if the proposal is for a new school in a newly created allotment (i.e. not affiliated with the existing St Peters Catholic College) or if the proposal forms part of the existing school.

If the proposal involves the creation of a new school in a newly created lot, the proposed exemption outlined in the bushfire assessment report cannot be supported. This is based on the site characteristics and the ability of development to comply with the acceptable solutions and support a continuous access road for emergency vehicles around the entire building.

Postal address

NSW Rural Fire Service Locked Bag 17 GRANVILLE NSW 2142 Street address

NSW Rural Fire Service 4 Murray Rose Ave SYDNEY OLYMPIC PARK NSW 2127 T (02) 8741 5555 F (02) 8741 5550 www.rfs.nsw.gov.a ____



	Option 8a:	Concept Plans
Summary	Option 8a was the first concept testing the introduction of a 6m wide access roadway to the full perimeter of the EOCCS building. This concept was established and developed from the current design proposal at the time, being Option 5 (as per Phase 5 of the Design Development) A number of initial steps were taken in order to accommodate the new roadway as part of the design development including: • relocating the Secondary School gym from the western wing to the southern wing, allowing the western wing to be shortened in length, and the southern and eastern wings as a whole to move north to accommodate an asphalted access roadway and carpark along the southern boundary (running in an east/west direction) • a row of parking was removed from the western carpark and relocated to the southern boundary. This allowed the building as a whole to be shifted to the west to accommodate the new, non-asphalted access roadway along the eastern boundary (running in a north/south direction). A turning bay was added to the eastern end of the carpark aisle to assist with vehicle movements through the carpark. • the northern boundary asphalted driveway currently providing access between the western and eastern carparks was retained to form the northern section of the emergency access road	10 Gavenlock (Dushard) To residuate the second of the sec
Advantages	 The design incorporates a 6m wide emergency access road to the full perimeter of the new School building as per the NSW RFS requirements. The overall building form and planning remained generally consistent with the current design proposal, Option 5. The reduced dimension and capacity of the western carpark further reduces the extent of hard pavement surfaces to the School frontage and presentation to Keefers Glen. The introduction of the southern portion of the access road and associated carpark results in an increased building setback to the southern boundary with St Peters Catholic College. An increase in total carparking spaces from the currently proposed 61 to 70 bays. 	Secondary Secondary Detion 8a - Level 2
Disadvantages	 Relocation of the Gym results in a reduction of the administration floor area at ground level Shifting the building further north decreases the extent of available area for the playground (measured between the northern face of the southern wing and the northern access road) 	2 Option 8a - Level 2 15:500 Gavenic Road (Justinan) Per Parallel School Schools Schools Schools Schools Bay CATHOLIC SCHOOLS Broken Bay Eileen O'Connor
Conclusion	Following review and discussion between the project team, consultants and CSBB, this concept was not selected as a preferred option and therefore excluded.	Primary Primary Gatholic School Catholic School Catholic School Catholic School Discover Birthers Bay 84 Gavenhock Road, Martin, NSW Catholic School C



	Option 8b:	Concept Plans
Summary	Option 8b evolved from Option 8a with the same built form and planning arrangement. This option simply sought to enhance the extent of landscaping to the western frontage on Keefers Glen by increasing the area for a public arrival/meeting garden within the public domain. This increased garden area was achieved through the removal of excess parking bays to the western carpark.	100 Gavenick (Rushing) Ga
Advantages	 The design incorporates a 6m wide emergency access road to the full perimeter of the new School building as per the NSW RFS requirements. The overall building form and planning remained generally consistent with the current design proposal, Option 5. The reduced dimension and capacity of the western carpark further reduces the extent of hard pavement surfaces to the School frontage and presentation to Keefers Glen. The introduction of the southern portion of the access road and associated carpark results in an increased building setback to the southern boundary with St Peters Catholic College. An increase in total carparking spaces from the currently proposed 61 to 65 bays. 	Secondary Secondary Primary Figure Actions
Disadvantages	 Relocation of the Gym results in a reduction of the administration floor area at ground level. Shifting the building further north decreases the extent of available area for the playground (measured between the northern face of the southern wing and the northern access road) 	Stanton Dahl Architects Secondary Secondary Secondary Signature School Second Seco
Conclusion	Following review and discussion between the project team, consultants and CSBB, this concept was not selected as a preferred option and therefore excluded.	Primary Catholic School Catholic School Catholic School Discose of Broken Bay 84 Governick Road, March, NSW Change, Did Prof. of School Catholic School



	Option 8c:	Concept Plans
Summary	Option 8c evolved from Option 8b with the same built form and planning arrangement. This option tested the concept of providing access to the eastern carpark via an asphalted driveway along the newly formed southern and eastern portions of the access roadway. The main driver for this adjustment was to increase the available area for the playground by converting the northern portion of the roadway from asphalt to a more suitable, playground friendly surface, such as turf, to be designed in compliance with the NSW RFS Planning for Bushfire Protection guidelines and therefore absorbing the northern portion of the roadway within the playground area.	Option 8c - Level 1 Option 8c - Level 1 Option 8c - Level 1
Advantages	 The design incorporates a 6m wide emergency access road to the full perimeter of the new School building as per the NSW RFS requirements. The overall building form and planning remained generally consistent with the current design proposal, Option 5. The reduced dimension and capacity of the western carpark further reduces the extent of hard pavement surfaces to the School frontage and presentation to Keefers Glen. The introduction of the southern portion of the access road and associated carpark results in an increased building setback to the southern boundary with St Peters Catholic College. An increase in total carparking spaces from the currently proposed 61 to 65 bays. The proposal removes daily vehicle movements along the northern boundary, in line with SDRP recommendations. 	Secondary Primary 100 2 Option 8c - Level 2
Disadvantages	 Relocation of the Gym results in a reduction of the administration floor area at ground level. Northern portion of emergency access road must be designed to accommodate vehicle movement and must be maintained clear at all times, limiting playground/landscape design. 	Stanton Dahl Architects Secondary Secondary Secondary Sign and the second control of
Conclusion	Following review and discussion between the project team, consultants and CSBB, this concept was not selected as a preferred option and therefore excluded.	Primary Catholic School Catholic School Catholic School Catholic School Catholic School Dicessed Rinner Bay 84 Carentok Road, Maral, NSW Carentok Catholic School Catholic School Catholic School Catholic School Dicessed Rinner Bay 84 Carentok Road, Maral, NSW Carentok Catholic School Catholic School Catholic School Dicessed Rinner Bay 84 Carentok Road, Maral, NSW Carentok Catholic School Catholic School Catholic School Dicessed Rinner Bay Bay Catholic School Catholic School Catholic School Catholic School Catholic School Catholic School Dicessed Rinner Bay Bay Catholic School Catholic School



	Option 8d:	Concept Plans
Summary	Option 8d evolved from Option 8c with the same built form and planning arrangement. This option expanded on the concept of the asphalted eastern section of the emergency access road by relocating the western carpark bays to the eastern roadway. To accommodate this adjustment, the building was shifted further west to provide the required area for the eastern parking bays adjacent the access road along the eastern boundary. The western road was retained to form part of the emergency road in addition to maintaining function as the prime entry and exit for student vehicles movements for pick-up and drop-off as well as visitor and service/maintenance vehicle access.	100 Gavenlock (Rock (Roc
Advantages	 The design incorporates a 6m wide emergency access road to the full perimeter of the new School building as per the NSW RFS requirements. The overall building form and planning remained generally consistent with the current design proposal, Option 5. The removal of carparking to the west further reduces the extent of hard pavement surfaces to the School frontage and presentation to Keefers Glen. The introduction of the southern portion of the access road and associated carpark results in an increased building setback to the southern boundary with St Peters Catholic College. The introduction of the eastern car parking bays results in an increased building setback to the eastern boundary with St Peters Catholic College. An increase in total carparking spaces from the currently proposed 61 to 72 bays. The proposal removes daily vehicle movements along the northern boundary, in line with SDRP recommendations. 	Secondary Primary B 8 8 8 8 8 8 8 8 8 8 8 8 8 8 8 8 8 8
Disadvantages	 Reduced setback to western frontage (Keefers Glen). Relocation of the Gym results in a reduction of the administration floor area at ground level. Northern portion of emergency access road must be designed to accommodate vehicle movement and must be maintained clear at all times, limiting playground/landscape design. 	Stanton Dahl Architects Secondary Secondary Catholic School Eileen O'Connor Catholic School Eileen O'Connor Catholic School
Conclusion	Following review and discussion between the project team, consultants and CSBB, this concept was not selected as a preferred option and therefore excluded.	Primary Primary Brown of Roberts Bay Brow



	Option 9a:	Concept Plans
Summary	One of the key drivers for a reduced setback to the southern boundary across previous schemes was to maximise the available playground area available for the development. This was further demonstrated through Options 8a to 8d, with the introduction of the southern access road and parking combined with shifting the building north impacting the available playground area. Option 9a sought to offset the loss in playground area caused by the building shifting north through two main concepts: 1. Flipping the southern road and associated car parking spaces to now position the roadway against the southern boundary and with the parking spaces closer to the building. 2. Splitting the eastern end of the southern wing and pulling the southern eastern corner of the building, including the entirety of the eastern wing, back towards the south. Through these modifications, this allowed for an increased setback from the northern boundary to the northern end of the eastern wing and therefore, increased playground area. Option 9a utilised Option 8a as a starting point, including the retention of the northern driveway and turning bay at the western end of the southern driveway.	100 Gavenlock Road (Dushland) To Plantage of The American State o
Advantages	 Increased playground area in comparison to Options 8a to 8d. The design incorporates a 6m wide emergency access road to the full perimeter of the new School building as per the NSW RFS requirements. The overall building form and planning remained generally consistent with the current design proposal, Option 5. The reduced dimension and capacity of the western carpark further reduces the extent of hard pavement surfaces to the School frontage and presentation to Keefers Glen. The introduction of the southern portion of the access road and associated carpark results in an increased building setback to the southern boundary with St Peters Catholic College. An increase in total carparking spaces from the currently proposed 61 to 63 bays 	Secondary Secondary Primary 2 Option Sa- Level 2 100
Disadvantages	 Relocation of the Gym results in a reduction of the administration floor area at ground level Shifting the building further north decreases the extent of available area for the playground (measured between the northern face of the southern wing and the northern access road) 	Secondary Secondary Secondary Secondary Secondary Secondary Signature regulation of the street
Conclusion	Following review and discussion between the project team, consultants and CSBB, this concept was not selected as a preferred option and therefore excluded.	Primary Catholic School Cantolic School Catholic School Dicease of Blocks Blocks of Blocks all y Be Gavenlock Road, Manda, NSW Per date; 13 97:2034 Solids, and



	Option 9b:	Concept Plans
Summary	Option 9b evolved from Option 9a with the same built form and planning arrangement. This concept, however, aligned with the roadway profile of previous Options 8c and 8d, with access to the eastern carpark diverted via the southern and eastern roads and incorporation of the northern driveway into the playground area.	To Option 9b - Level 1 100 Gavenlock To Option 9b - Level 1
Advantages	 Increased playground area in comparison to Options 8a to 8d. The design incorporates a 6m wide emergency access road to the full perimeter of the new School building as per the NSW RFS requirements. The overall building form and planning remained generally consistent with the current design proposal, Option 5. The reduced dimension and capacity of the western carpark further reduces the extent of hard pavement surfaces to the School frontage and presentation to Keefers Glen. The introduction of the southern portion of the access road and associated carpark results in an increased building setback to the southern boundary with St Peters Catholic College. An increase in total carparking spaces from the currently proposed 61 to 65 bays. The proposal removes daily vehicle movements along the northern boundary, in line with SDRP recommendations. 	Secondary Secondary Frimary Frimary
Disadvantages	 Relocation of the Gym results in a reduction of the administration floor area at ground level. Northern portion of emergency access road must be designed to accommodate vehicle movement and must be maintained clear at all times, limiting playground/landscape design. 	2 Option 9b - Level 2 1500 Gaveniock Road (Justinand) Architects Stanton Dahl Architects CATHOLIC SCHOOLS Broken Bay Catholic School Eileen O'Connor Catholic School
Conclusion	Following review and discussion between the project team, consultants and CSBB, this concept was not selected as a preferred option and therefore excluded.	Primary Primary Beginning State Control Cont



	Option 9c:	Concept Plans
Summary	The modifications proposed in Options 9a and 9b achieved a positive result in seeking to address and offset the reduction in playground area generated by the introduction of the 6m wide emergency access road to the perimeter of the building. Option 9c, therefore, sought to address one of the main operational requirements of the School and the negative impact of the reduction in administration areas through Option 8a to 9b. With Option 9b as the starting point, Option 9c proposed to pull back the south-eastern corner of the upper floor level, cantilevering over the ground floor, southern car parking spaces. In doing so, this adjustment brought the southern façade in alignment at the upper level with the setback of the south-eastern corner of the building. In addition, it increased the floor area within the upper level Library, allowing for a new, enlarged administration area to offset the loss of administration area on the ground floor.	10 Gavenlock (Rushard) (Part Russell) (Part Russell
Advantages	 Increased playground area in comparison to Options 8a to 8d. Increased administration area in comparison to Options 8a to 9b. The design incorporates a 6m wide emergency access road to the full perimeter of the new School building as per the NSW RFS requirements. The overall building form and planning remained generally consistent with the current design proposal, Option 5. The reduced dimension and capacity of the western carpark further reduces the extent of hard pavement surfaces to the School frontage and presentation to Keefers Glen. The introduction of the southern portion of the access road and associated carpark results in an increased building setback to the southern boundary with St Peters Catholic College. An increase in total carparking spaces from the currently proposed 61 to 62 bays. The proposal removes daily vehicle movements along the northern boundary, in line with SDRP recommendations. 	Secondary Secondary Primary 388888
Disadvantages	 Main administration areas split across two (2) storeys. Northern portion of emergency access road must be designed to accommodate vehicle movement and must be maintained clear at all times, limiting playground/landscape design. 	Stanton Dahl Architects Secondary Secondary Catholic School Eileen O'Connor Catholic School Eileen O'Connor Catholic School
Conclusion	Following review and discussion between the project team, consultants and CSBB, this concept was not selected as a preferred option and therefore excluded.	Primary Registrate in the control of the control o



	Option 9d:	Concept Plans
Summary	Option 9d was developed from Option 9c with the same built form, however, modified the internal planning arrangement, seeking to test opportunities to consolidate the administration area into a single level, consistent with the original design concepts of Options 1a and Option 5. In order to achieve this, Option 9d proposed the following planning amendments: 1. Relocation of the Secondary School specialist learning area (Science/CAPA/TAS/Life Skills/Independent Learning) from the ground floor to the upper floor. 2. Relocating the Kitchen/Canteen area on ground floor (currently adjacent the main administration area) into the former Secondary School specialist learning area. Through these planning adjustments, this allowed for the area occupied by the Kitchen/Canteen to be re-incorporated into the main administration area as per the original Option 1a and Option 5 designs.	10 Gave lock Cushand Or For American The State S
Advantages	 Increased playground area in comparison to Options 8a to 8d. Consolidated main administration area at ground floor level consistent with the original, Option 5 design The design incorporates a 6m wide emergency access road to the full perimeter of the new School building as per the NSW RFS requirements. The overall building form and planning remained generally consistent with the current design proposal, Option 5. The reduced dimension and capacity of the western carpark further reduces the extent of hard pavement surfaces to the School frontage and presentation to Keefers Glen. The introduction of the southern portion of the access road and associated carpark results in an increased building setback to the southern boundary with St Peters Catholic College. An increase in total carparking spaces from the currently proposed 61 to 62 bays. The proposal removes daily vehicle movements along the northern boundary, in line with SDRP recommendations. 	Secondary Primary 2 Option 9d - Level 2
Disadvantages	Northern portion of emergency access road must be designed to accommodate vehicle movement and must be maintained clear at all times, limiting playground/landscape design.	Secondary
Conclusion	Following review and discussion between the project team, consultants and CSBB, this concept was selected as the preferred option to move forward with due to: o Option 9d best addressing the functional and operational requirements of the School o The concept best addressing the NSW RFS advice and incorporation of the 6m wide emergency access road to the full perimeter of the building. o Removal of daily vehicle movements along the northern boundary, in line with SDRP recommendations. This concept was approved by CSBB to proceed, with the following minor modifications summarised hereafter through Options 9e to 9h.	Primary Gatholic School Catholic School Catholic School Catholic School Catholic School Dicesses of Blockses



	Option 9e:	Concept Plans
Summary	Option 9e evolved from Option 9d with the same built form and planning arrangement. The proposal incorporated a minor amendment to re-instate the increased landscaped area and arrival/meeting garden within the public domain as per Option 8b.	100 Gavenlock Road (Dushland) For Resource and Annual Control of the Control of t
Advantages	 Increased playground area in comparison to Options 8a to 8d. Consolidated main administration area at ground floor level consistent with the original, Option 5 design The design incorporates a 6m wide emergency access road to the full perimeter of the new School building as per the NSW RFS requirements. The overall building form and planning remained generally consistent with the current design proposal, Option 5. The reduced dimension and capacity of the western carpark further reduces the extent of hard pavement surfaces to the School frontage and presentation to Keefers Glen. The introduction of the southern portion of the access road and associated carpark results in an increased building setback to the southern boundary with St Peters Catholic College. Total of 61 car parking bays consistent with the original Option 5 design proposal The proposal removes daily vehicle movements along the northern boundary, in line with SDRP recommendations. 	Secondary Primary 10 10 10 10 10 10 10 10 10 10 10 10 10 1
Disadvantages	Northern portion of emergency access road must be designed to accommodate vehicle movement and must be maintained clear at all times, limiting playground/landscape design.	2 Option 9e - Level 2 1500 Gavenlock Road (Jushland) Architects Stanton Dahl Architects Secondary Secondary Fillen O'Connor Catholic School Eileen O'Connor Catholic School
Conclusion	This concept was approved by CSBB to proceed, with minor modifications summarised hereafter in Option 9f.	Primary 8 General Roberts Brooks Disconser of Rochers Bay 8 General Roberts Mark, NSW Andrews Andrews Control of Control of Control Disconser of Roberts Disconser of Roberts Disconser of Roberts Mark, NSW Andrews Control Disconser of Roberts Disconser of



	Option 9f:	Concept Plans
Summary	Option 9f evolved from Option 9e with the same built form. The proposal incorporated a detailed planning assessment of the ground floor administration area with a review of operational and functional requirements together with spatial relationships within the staff areas.	100 Gavenlock Road (Dushland) For Requirement of the Section of th
Advantages	 Increased playground area in comparison to Options 8a to 8d. Consolidated main administration area at ground floor level consistent with the original, Option 5 design The design incorporates a 6m wide emergency access road to the full perimeter of the new School building as per the NSW RFS requirements. The overall building form and planning remained generally consistent with the current design proposal, Option 5. The reduced dimension and capacity of the western carpark further reduces the extent of hard pavement surfaces to the School frontage and presentation to Keefers Glen. The introduction of the southern portion of the access road and associated carpark results in an increased building setback to the southern boundary with St Peters Catholic College. Total of 61 car parking bays consistent with the original Option 5 design proposal The proposal removes daily vehicle movements along the northern boundary, in line with SDRP recommendations. 	Secondary Primary 3868888
Disadvantages	Northern portion of emergency access road must be designed to accommodate vehicle movement and must be maintained clear at all times, limiting playground/landscape design.	2 Option 9f - Level 2 1:500 Gavenlock Road (bushland) Information distributed School Secondary Secondary Secondary Fileen O'Connor Catholic School Elieen O'Connor Catholic School
Conclusion	This concept was approved by CSBB to proceed, with minor modifications summarised hereafter in Option 9g.	Primary Registration of Broken Bay Registration of Broke



	Option 9g:	Concept Plans
Summary	Option 9g evolved from Option 9f with the same built form. The proposal incorporated a minor planning re-configuration to the ground floor administration area.	10 Gaventock (Cushand) Office and the state of the state
Advantages	 Increased playground area in comparison to Options 8a to 8d. Consolidated main administration area at ground floor level consistent with the original, Option 5 design The design incorporates a 6m wide emergency access road to the full perimeter of the new School building as per the NSW RFS requirements. The overall building form and planning remained generally consistent with the current design proposal, Option 5. The reduced dimension and capacity of the western carpark further reduces the extent of hard pavement surfaces to the School frontage and presentation to Keefers Glen. The introduction of the southern portion of the access road and associated carpark results in an increased building setback to the southern boundary with St Peters Catholic College. Total of 61 car parking bays consistent with the original Option 5 design proposal The proposal removes daily vehicle movements along the northern boundary, in line with SDRP recommendations. 	Secondary Primary Primary Bill BBB
Disadvantages	Northern portion of emergency access road must be designed to accommodate vehicle movement and must be maintained clear at all times, limiting playground/landscape design.	2 Option 9g - Level 2 100 Gavenlock Road (Jushiand) Permatural Permatural Architects Stanton Dahl Architects Secondary Fileen O'Connor Elicen O'Connor Elicen O'Connor Elicen O'Connor
Conclusion	This concept was approved by CSBB to proceed, with minor modifications summarised hereafter in Option 9h.	Catholic School Catholic School Catholic School Catholic School Catholic School Discose of Broken Bay 84 Governick Road, March, NSW Orace Dat Orace



	Option 9h:	Concept Plans
Summary	Option 9h evolved from Option 9g with the same built form. The proposal incorporated a minor planning re-configuration to the ground floor administration area.	100 Gavenlock Road (Dushland) The Reputer of the Common Co
Advantages	 Increased playground area in comparison to Options 8a to 8d. Consolidated main administration area at ground floor level consistent with the original, Option 5 design The design incorporates a 6m wide emergency access road to the full perimeter of the new School building as per the NSW RFS requirements. The overall building form and planning remained generally consistent with the current design proposal, Option 5. The reduced dimension and capacity of the western carpark further reduces the extent of hard pavement surfaces to the School frontage and presentation to Keefers Glen. The introduction of the southern portion of the access road and associated carpark results in an increased building setback to the southern boundary with St Peters Catholic College. Total of 61 car parking bays consistent with the original Option 5 design proposal The proposal removes daily vehicle movements along the northern boundary, in line with SDRP recommendations. 	Secondary Primary 38888
Disadvantages	Northern portion of emergency access road must be designed to accommodate vehicle movement and must be maintained clear at all times, limiting playground/landscape design.	Stanton Dahl Architects Secondary Secondary Secondary Finance Read Cartholic School Cartholic School Cartholic School Secondary Finance Read Finan
Conclusion	Following review and discussion between the project team, consultants and CSBB, this concept was selected as the preferred option to move forward with due to: Option 9h best addressing the functional and operational requirements of the school The concept best addressing the NSW RFS advice and incorporation of the 6m wide emergency access road to the full perimeter of the building. Removal of daily vehicle movements along the northern boundary, in line with SDRP recommendations. This concept was therefore progressed to form Phase 7 of the Design Development.	Catholic School Cacholic School Discuss of Broken Bay 84 Carenick Board, Manual, NSW Primary 1



	Option 10a:	Concept Plans
Summary	Option 10a tested a hybrid concept taking inspiration from Options 6a, 6b, 7 and 8d. This option re-visited the concept of locating all carparking on the southern side of the proposed site, a concept previously explored in Options 6a, 6b and 7. To allow this, both the western and eastern wings were required to be shortened to allow the entire building to be shifted further north. In addition, the setback of the western wing to the western boundary was reduced, with the western wing and entry driveway (and associated pick-up/drop-off area) moving closer to the Keefers Glen boundary (similar to Option 8d) allowing an extension in length and footprint of the southern wing to offset the loss of floor area through the shortening of the western and eastern wings.	Toplion 10a - Level 1 Option 10a - Level 1 Option 10a - Level 1 Option 10a - Level 1
Advantages	 The design incorporates a 6m wide emergency access road to the full perimeter of the new School building as per the NSW RFS requirements. The overall building form and planning remained generally consistent with the current design proposal, Option 5. The removal of carparking to the west further reduces the extent of hard pavement surfaces to the School frontage and presentation to Keefers Glen. Increased building setback to the southern boundary with St Peters Catholic College. An increase in total carparking spaces from the currently proposed 61 to 62 bays. The proposal removes daily vehicle movements along the northern boundary, in line with SDRP recommendations. Retains the main entry and pick-up/drop-off area directly off Keefers Glen. 	Secondary Primary Primary 2 Option 10a - Level 2 1:500
Disadvantages	 Building footprint further north decreases dimension to northern boundary which in turn, reduces area for playground and will increase steepness in level changes/transition from south to north. Locates all staff carparking away from the main entry which increases walking distance No onsite visitor parking area 	Stanton Dahl Architects Secondary Secondar
Conclusion	Following review and discussion between the project team, consultants and CSBB, this concept was not selected as a preferred option and therefore excluded.	Primary Catholic School Catholic School Catholic School Catholic School Dicesses of Birchen Bay B4 Cacenbock Road, Martin, NSW Catholic School Catholic Sch



	Option 10b:	Concept Plans
Summary	Option 10b evolved from Option 10a, combined with elements from the Option 9 series of concepts. Similar to Option 10a, this concept located the majority of parking to the southern side of the proposed site. However, in addition, the eastern carpark was retained to provide a second staff parking area. The concept of "splitting" the southern wing as tested in the Option 9 series was incorporated into this design, stepping the south-western corner of the building towards the south. This provided for an increased administration area on the ground floor and increased Library and administration area on the upper floor.	Commence of the state of the st
Advantages	 The design incorporates a 6m wide emergency access road to the full perimeter of the new School building as per the NSW RFS requirements. The overall building form and planning remained generally consistent with the current design proposal, Option 5. The removal of carparking to the west further reduces the extent of hard pavement surfaces to the School frontage and presentation to Keefers Glen. Increased building setback to the southern boundary with St Peters Catholic College. An increase in total carparking spaces from the currently proposed 61 to 62 bays. The proposal removes daily vehicle movements along the northern boundary, in line with SDRP recommendations. Retains the main entry and pick-up/drop-off area directly off Keefers Glen. 	Secondary Primary 18 8 8 8 8 2 Option 10b - Level 2
Disadvantages	 Building footprint further north decreases dimension to northern boundary which in turn, reduces area for playground and will increase steepness in level changes/transition from south to north. Locates the majority of staff parking away from the main entry which increases walking distance No onsite visitor parking area 	Stanton Dahl Architects Secondary Secondar
Conclusion	Following review and discussion between the project team, consultants and CSBB, this concept was not selected as a preferred option and therefore excluded.	Catholic School Catholic School Catholic School Discored Finchers Blay 8 of Goverhock Road, March, NSW Discored Blay Control Cont

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4.4.8 Phase 7: Option 9h

Summary:

As noted in Phase 6 of the Design Development, Option 9h was selected by the project team and CSBB to be the preferred scheme to progress into the next phase of the design.

The Option 9h proposal encompassed the following works:

- Part 2-3 storey western Secondary School wing comprising:
 - o 6 General Learning Areas across two (2) levels
 - Enclosed Circulation and Common Areas on both levels to provide extended learning and regulation areas
 - Staff facilities
 - Specialised Learning Areas including Gym/Fitness, Science, CAPA, TAS and Life Skills
 - o Maintenance and storage areas to lower ground
- 2 storey southern and eastern Primary School wings comprising:
 - o 14 General Learning Areas across two (2) levels
 - Enclosed Circulation and Common Areas on both levels to provide extended learning and regulation areas
 - o Administration and staff facilities
 - o Multi-purpose Hall
 - o Library
- Two (2) new vehicle entry points via Keefers Glen with western and eastern staff carpark areas (connected via a northern driveway)
- Outdoor playground and landscaping
- 6m wide emergency access road to the full perimeter of the building in accordance with NSW RFS Planning for Bush Fire Protection guidelines.

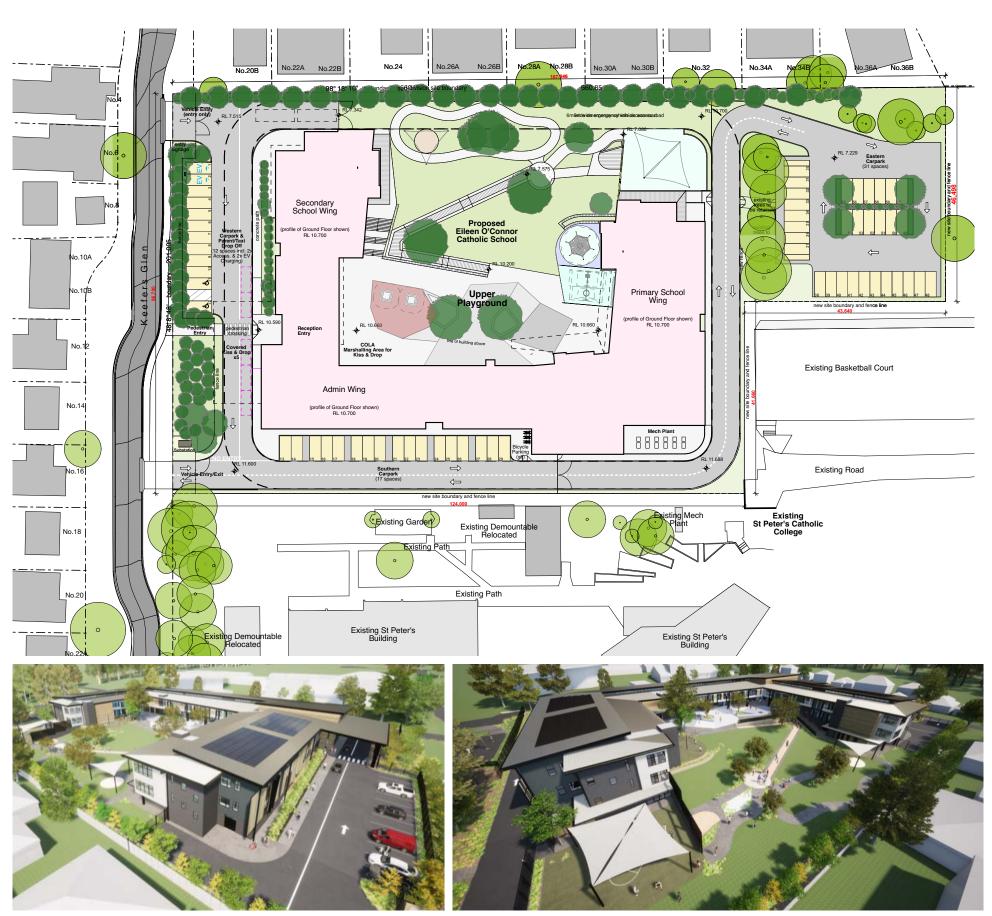
In addition to the above, Option 9h continued to incorporate elements identified during Phases 2 and 5 of the Design Development including:

- Increased landscaping buffer along northern boundary
- Reduction of hard surfaced, paved landscaping through:
 - Reducing onsite staff parking spaces from 71 to 61 parking spaces and 2 onsite School bus overnight parking bays.
 - Increased landscaped garden areas to western and eastern carparks
- Improved engagement with public domain with the introduction of a public arrival/meeting garden at the Keefers Glen pedestrian entry and increased landscaping to the western frontage
- Simplified playground design, sympathetic and complementing the site terrain.
- Revised roof concept with split roof design incorporated into each wing to allow for additional natural light to GLA's

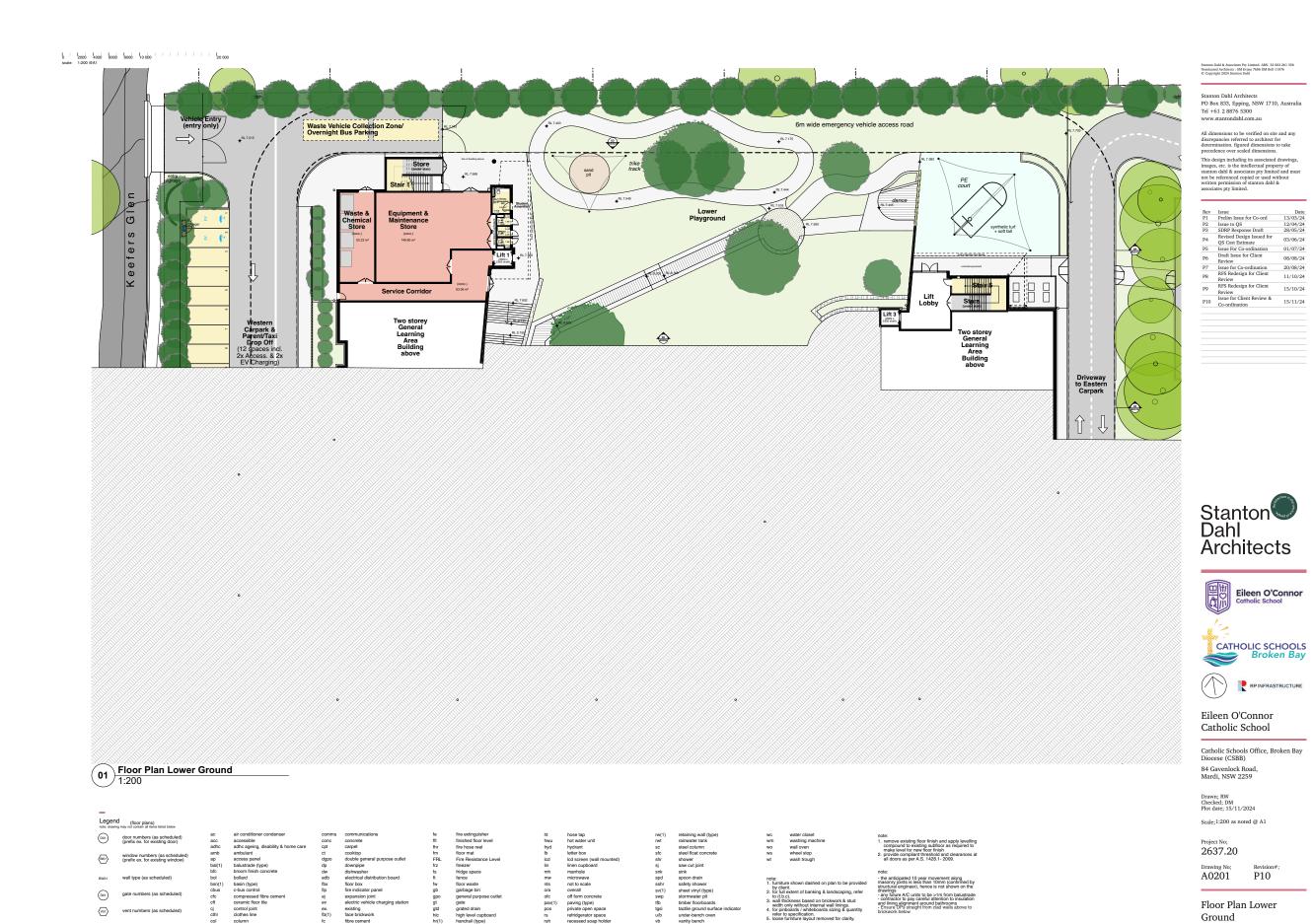
Adjacent is the revised Option 9h Proposed Site Plan and preliminary perspective images.

Following are the revised architectural drawings detailing the Option 9h design including:

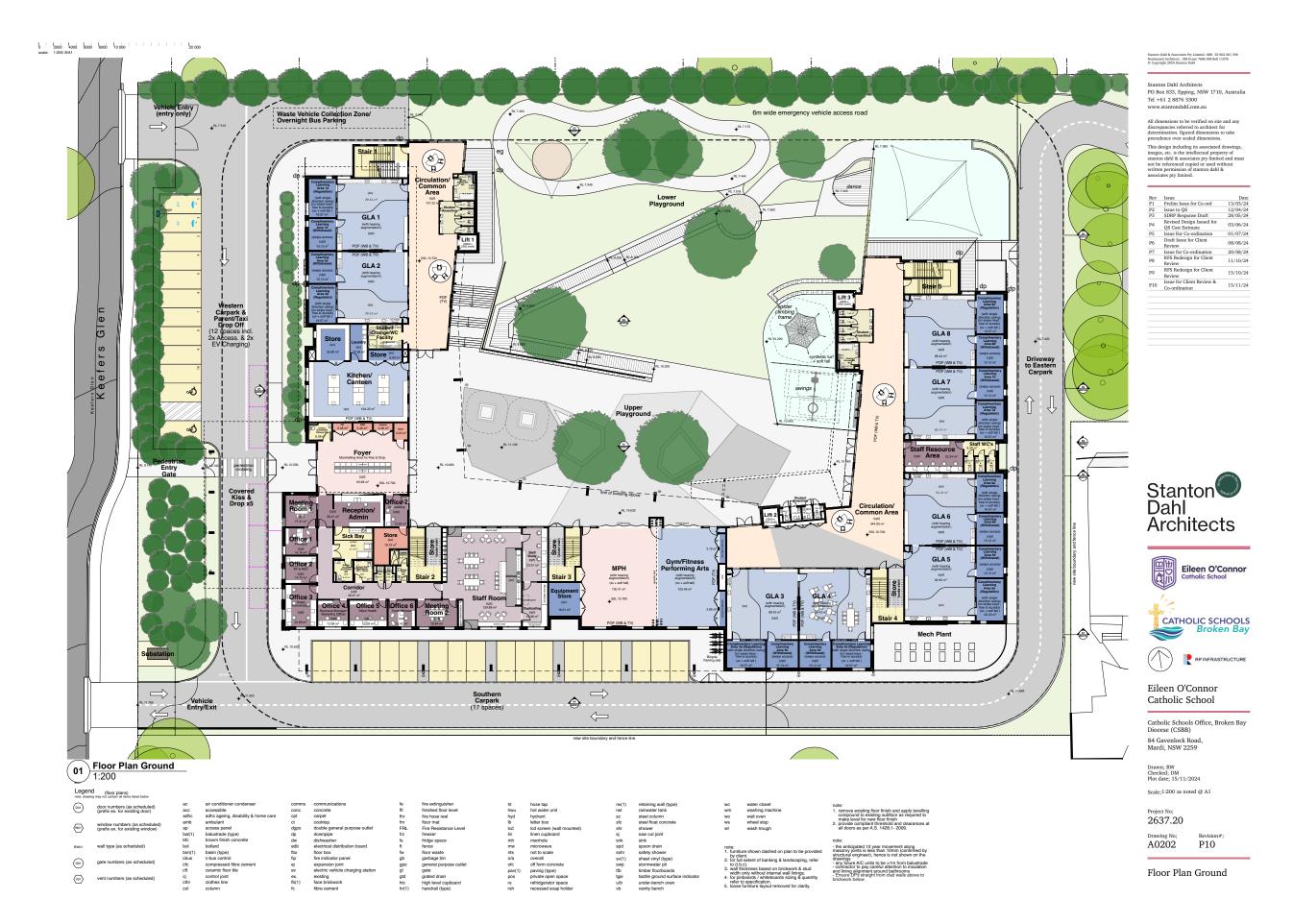
- Lower Ground Floor Plan
- Ground Floor Plan
- Level 1 Plan
- Roof Plan
- Elevations







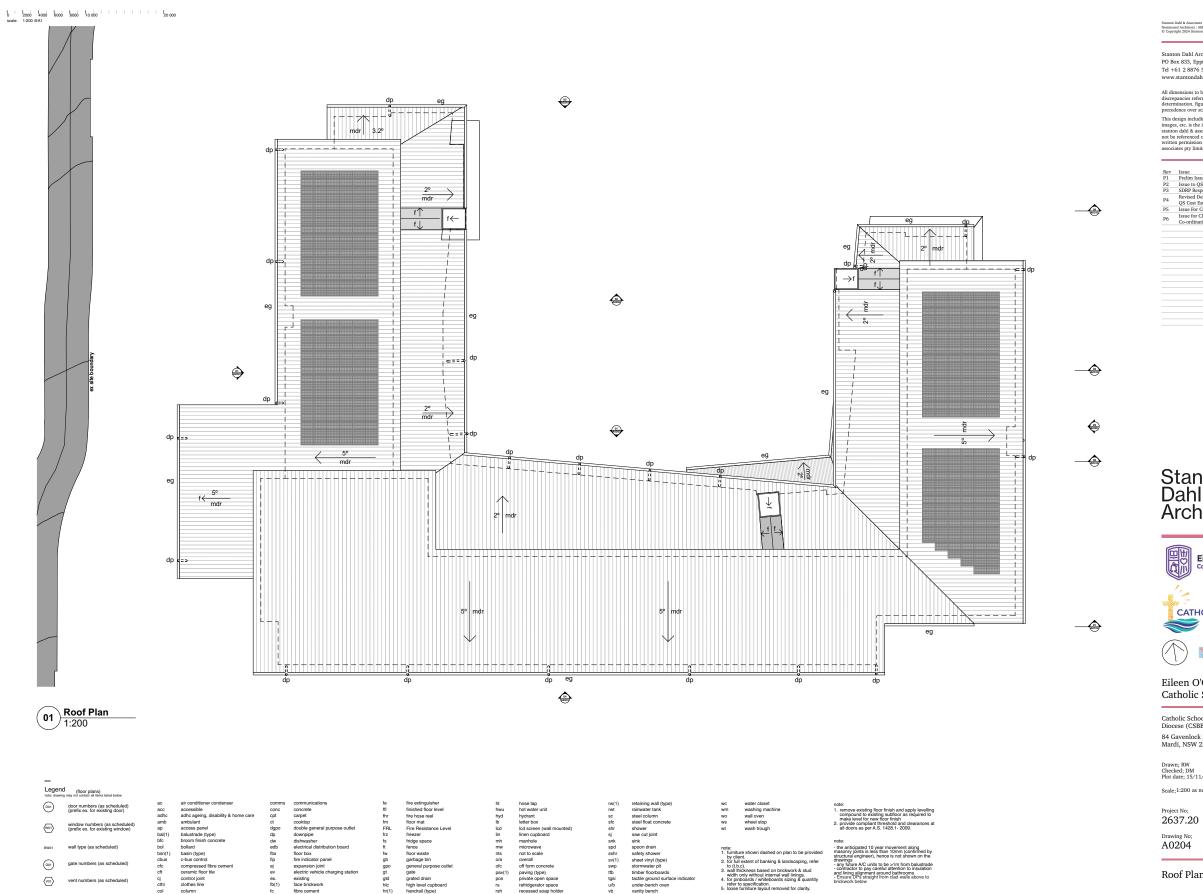












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Rev	Issue	Da
P1	Prelim Issue for Co-ord	13/03/2
P2	Issue to QS	12/04/2
P3	SDRP Response Draft	28/05/2
P4	Revised Design Issued for QS Cost Estimate	03/06/2
P5	Issue For Co-ordination	01/07/2
P6	Issue for Client Review & Co-ordination	15/11/2
	CO Ordination	









Eileen O'Connor Catholic School

Catholic Schools Office, Broken Bay Diocese (CSBB)

Drawn; RW Checked; DM Plot date; 15/11/2024

Scale;1:200 as noted @ A1

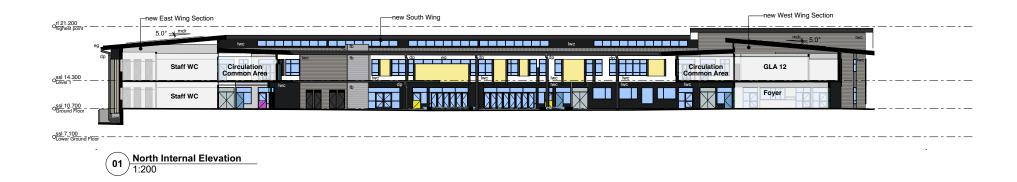
Roof Plan

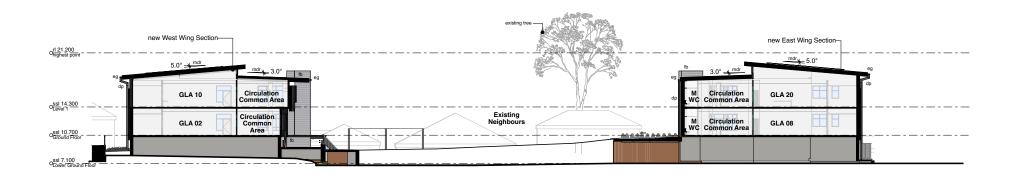






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South Internal Elevation 1:200

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Rev	Issue	
P6	Issue for Client Review & Co-ordination	









Eileen O'Connor Catholic School

Catholic Schools Office, Broken Bay Diocese (CSBB)

84 Gavenlock Road, Mardi, NSW 2259

Drawn; RW Checked; DM Plot date; 15/11/2024

Scale;1:200 as noted @ A1

Project No; 2637.20

A0302

Elevations - Sheet 2



4.4.9 Phase 8: Option 9h (Modified)

The final design development phase was undertaken following a detailed review of the Option 9h design and input from the broader consultant team. In particular, the modifications to the design during this phase focused on advice and commentary received from the fire engineer and BCA consultant and sought to address the following:

- Smoke separation and smoke management between the GLA's with consideration of egress via the enclosed Circulation & Common Areas/corridors
- Natural ventilation considerations
- Natural light considerations

To assist in addressing the above items, the following modifications were implemented and Option 9h revised as follows:

- Circulation & Common Areas/corridors previously enclosed have been converted to open verandahs by removing the external walls to all corridors.
- To create a permeable façade, the modified Option 9h design incorporates steel framed screens with perforated mesh infill to the perimeter of the verandahs. The screens are proposed to perform both as screening and as fall protection, with full height screens at the Ground Floor level proposed for the northern end of the western and eastern wings and, on Level 1, the top of the screens is proposed to be 3600mm above the external finished floor level.
- For simplification of construction, the roof profile for all three wings has reverted to the original Option 1a scheme with each wing covered by a single plane, skillion roof. In addition, this modification allows for an increased fascia height to the internal, playground facing façades which allows for:

 o increased light penetration into the open verandahs and GLA's on Level 1

 - o increased ceiling height to the open verandahs on Level 1
- Additional natural light and ventilation to the Level 1 GLA's has been provided through skylights and high level, openable windows to the open verandah.

The above amendments and modified Option 9h concept forms the final design and is represented in the accompanying architectural drawings (as listed on page 8 of this report) to support the SSDA submission for the new Eileen O'Connor Catholic School

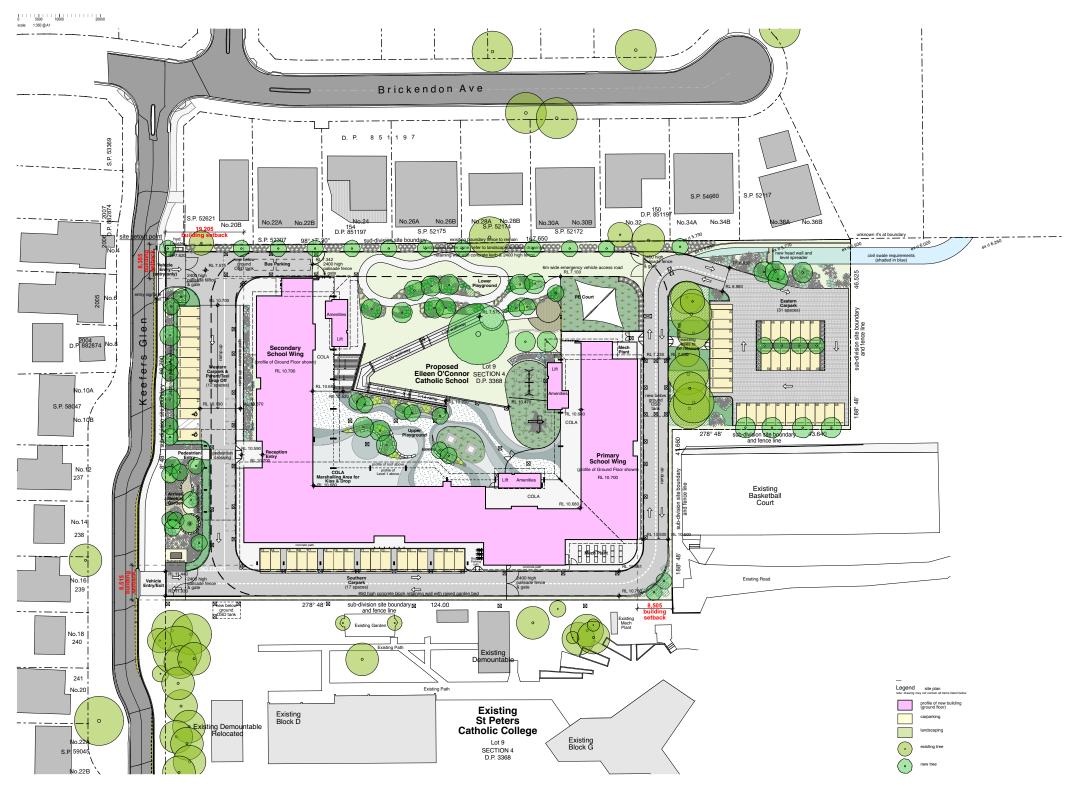


Original Option 9h - View from Playground Looking West (with enclosed Circulation & Common Areas)



Modified Option 9h - View from Playground Looking West (with open verandahs and mesh screens)





Eileen O'Connor Catholic School – Proposed Site Plan (Modified Option 9h)



4.5 Site Planning

The planning arrangement of the new campus has been designed to accommodate the brief requirements of the School and respond to the site conditions and surrounding context. Encompassing three (3) distinct "Wings", the building siting has evolved during the extensive design development phase as demonstrated in *Part 4.4* of this report. Through each phase, the siting for the connected wings has been driven by:

- Meeting the specific functional and relational requirements for the new School
- Consideration of the operational opportunities and limitations triggered by rise in storeys and massing within the site context (refer to Part 4.6 Rise in Storeys and Part 4.7 Massing below)
- Achieving good, accessible connectivity to and within the campus

Functional Relationships:

To facilitate the operational requirements of the School, the functional planning and inter-spatial relationships have incorporated the following principles and design strategies:

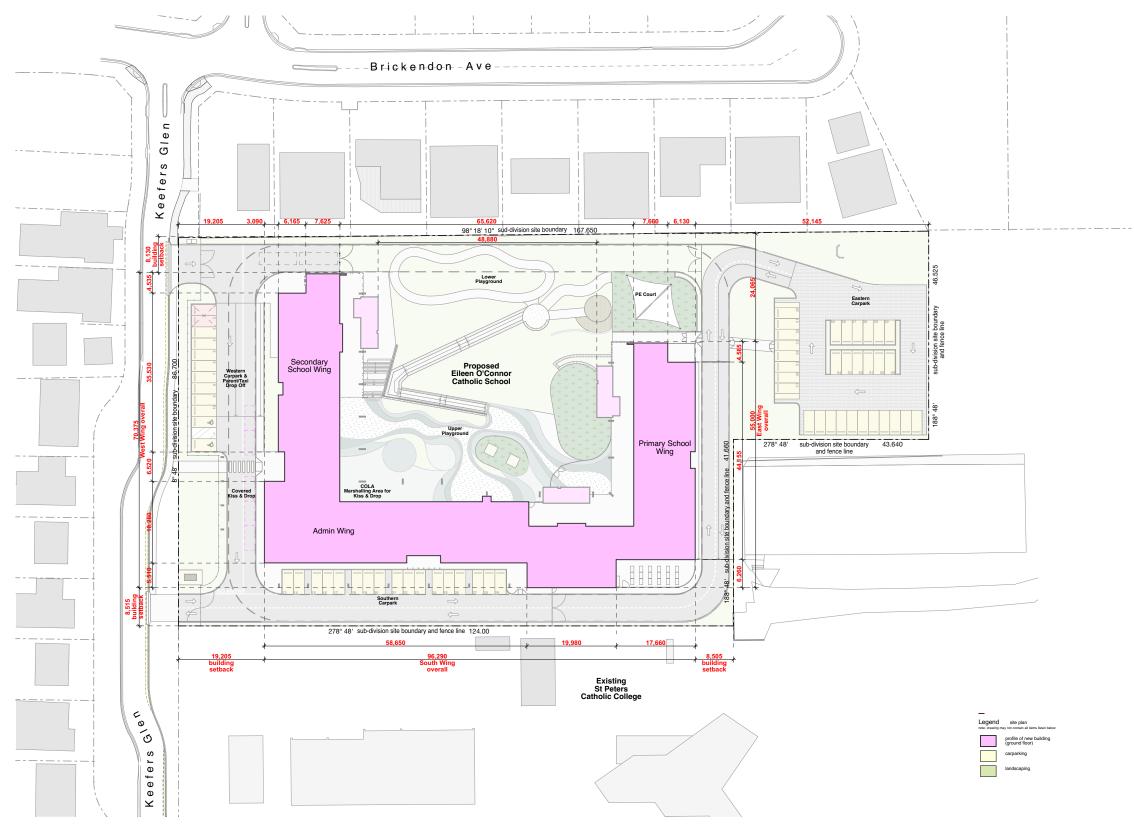
- The School has been provided with an undercover, pick-up and drop-off area to facilitate student transfers during morning and afternoon peaks in a weather protected area directly accessed from the Foyer which is the main entry/exit point of the campus. The Foyer is also used as a supervised marshalling area to assist staff with management of student pick-ups and drop-offs. In addition, the Upper Playground COLA area immediately adjacent the Foyer provides an expanded marshalling area for management of larger groups of students.
- To assist with security and safety of students, the Primary and Secondary Schools are accessed independently of each other once inside the campus proper.
- The Library has been positioned in the south-western corner of the building on Level 1 to provide independent access from both the southern Primary School wing and western Secondary School wing. Stair 3 also provides staff access between Ground Floor administration area and the Library and Staff Resource Areas on Level 1.
- Stair 1 and Lift 1 have been extended to the Lower Ground floor to provide the following:
 - An alternative, covered path of travel for Secondary School students accessing the Lower Playground
 - Lift 1 is proposed to be a double-sided lift, with staff only access to the Service Corridor and Maintenance and Waste Stores, and student access to the Lower Playground.
- Stair 5 and Lift 3 have been extended to Lower Ground floor to form an independent lobby area providing:
 - An alternative, covered path of travel for Primary School students accessing the Lower Playground
 - Staff direct access to and from the Eastern Carpark
- Services and maintenance areas have been located on the Lower Ground floor of the western wing with direct access off the main School vehicular entry via Keefers Glen.
- The Multi-purpose Hall has been located adjacent the Gym to provide flexibility and accommodate two seating modes. In standard mode, the MPH has capacity for up to 100 seats. The dividing, operable wall between the MPH and Gym, when opened, creates one large space that allows accommodation for the full 200 student population.
- The MPH has been positioned directly adjacent the Upper Playground gathering space for breakout and pre-event functions.
- Each GLA is proposed to include the following facilities:
 - Point of focus wall with provisions for digital screen and/or whiteboard
 - Wet area with vinyl flooring including fixed joinery cabinetry provisioned with storage, twin sinks, and benchtops at accessible and non-accessible heights
 - Teacher Resource Room for staff amenity and focused one-on-one tuition for students
 - Regulation Zone to provide students with a comfortable, quiet space to regulate
 - Inter-connecting door to adjacent GLA to allow for additional staff support and/or provide an alternative exit path for staff and students in the event of an emergency and blockage of the main GLA entry door
- The Student Amenities and Lift blocks have been distributed to ensure a maximum group of four (4) GLA's are serviced by a bank of toilets and lift.
- Stairs have been positioned to ensure both the Primary and Secondary Schools are serviced by two (2) independent stairs as follows:
 - Stairs 1 and 2 servicing the Secondary School western wing
 - Stairs 4 and 5 servicing the Primary School southern and eastern wings
- High use, high activity playground equipment, including proposed swings, climbing equipment and trampolines have been located in the Upper Playground for ease of access immediately from the Ground Floor and to provide clear vision for supervision of students.
- The accessible pathway from the Upper Playground to the Lower Playground has been designed with the lower portion at a 1:20 gradient to allow for the exclusion of handrails therefore providing a clear, open playground area.

Setbacks:

As noted in *Part 3.9 Heights and Setbacks*, there are no minimum setback envelope controls nominated within the Central Coast Council LEP and DCP. The planning arrangement and setbacks for the new EOCCS have therefore been generated to respond to the site conditions and surrounding context, in addition to compliance with relevant codes as follows: (refer following architectural Setbacks and Building Dimensions Plan)

- Northern Boundary 8.130 metre setback (northern face of western wing) including:
 - Landscaped buffer zone to the northern residences
 - o 6m wide emergency vehicle access road
- Western Boundary 19.205 metre setback incorporating:
 - o 1.5m wide concrete pedestrian pathway to perimeter of building
 - o Kiss & Drop lane and passing lane (forming the 6m wide emergency access road)
 - Staff parking with associated parking aisle
 - o Arrival/Meeting Garden
 - o Landscaped buffer zone to Keefers Glen
- Southern Boundary 8.515 metre setback incorporating:
 - o 1.5m wide concrete pedestrian pathway to perimeter of building
 - o 6m wide emergency access road/carpark driveway
 - o Retaining wall with raised landscaped garden to southern boundary fence line
- Eastern Boundary 8.505 metre setback incorporating:
 - o 1.5m wide concrete pedestrian pathway to perimeter of building
 - o 6m wide emergency access road/carpark driveway
 - o Landscape zone to eastern boundary fence line





Eileen O'Connor Catholic School – Setback and Building Dimensions Plan