DOCUMENTATION OF CIVIL WORKS IN ROAD RESERVE

NEW DEVELOPMENT EILEEN O'CONNOR CATHOLIC SCHOOL

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	CIVIL DRAWING LIST			
Sheet No.	t No. Sheet Name			
C.1	ROAD DESIGN - COVER SHEET			
C.2	GENERAL NOTES			
C.11	PLAN - OVERVIEW			
C.12	SETOUT PLAN 1			
C.13	SETOUT PLAN 2&3			
C.14	SETOUT PLAN 4&5			
C.15	SETOUT PLAN 6			
C.21	SECTIONS SHEET 1			
C.22	SECTIONS SHEET 2			
C.23	SECTIONS SHEET 3			
C.24	SECTIONS SHEET 4			
C.31	ROAD LONGITUDINAL SECTIONS SHEET 1			
C.32	ROAD LONGITUDINAL SECTIONS SHEET 2			
C.35	ROAD CROSS SECTIONS			
C.101	VEHICLE SWEPT PATH SHEET 1			
C.102	VEHICLE SWEPT PATH SHEET 2			
C.103	VEHICLE SWEPT PATH SHEET 3			







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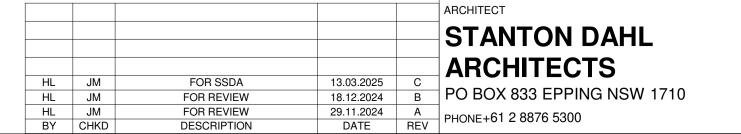
3. ARCHITECTURAL DRAWINGS PREPARED BY: STANTON DAHL Ph. +61 2 8876 5300 Project No. 2637.20

4. GEOTECHNICAL REPORT PREPARED BY: NEPEAN GEOTECHNICS PH: 0447 280 042 Report No. R23169.Rev0 DATED: 04/09/2023

5. STORMWATER ASSET PLANS PREPARED BY: CENTRAL COAST COUNCIL Job No. 36418098 DATED: 06/05/2024

6. NSW DEPOSITED PLAN BY: OFFICE OF THE REGISTRAR-GENERAL REF: EC2251 DOC: DP 0857182 REV: 27-FEB-1996

7. STORMWATER DRAWINGS BY: JAMES TAYLOR & ASSOCIATES PROJECT NO. 6588 DRAWING NO. SW.1-SW.45







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ROAD DESIGN - COVER SHEET

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GENERAL

- G1. THESE DRAWINGS ARE TO BE READ IN CONJUNCTION WITH ALL OTHER RELEVANT DOCUMENTS INCLUDING ALL WORKING DRAWINGS AND SPECIFICATIONS, AND WRITTEN INSTRUCTIONS AS MAY BE ISSUED PRIOR TO OR DURING THE COURSE OF CONSTRUCTION. ALL DISCREPANCIES AND VARIATIONS SHALL BE REFERRED TO THE ENGINEER
- BEFORE PROCEEDING WITH THE WORK. ALL CIVIL WORK SHALL BE IN ACCORDANCE WITH THE REQUIREMENTS OF ALL RELEVANT AND
- CURRENT RMS SPECIFICATIONS/CODES. STRUCTURAL DRAWINGS SHALL NOT BE SCALED IN ORDER TO OBTAIN DIMENSIONS. DIMENSIONS WHERE SHOWN ON STRUCTURAL DRAWINGS SHALL BE CO-ORDINATED WITH ALL OTHER RELEVANT
- DURING CONSTRUCTION, THE STRUCTURE SHALL BE MAINTAINED IN A STABLE CONDITION AND NO PART SHALL BE OVERSTRESSED.

CONSTRUCTION NOTES

GENERAL

- NOT ALL TACKCOATS AND CURING COMPOUNDS ARE SHOWN ON THESE DRAWINGS.
- DRAWINGS TO BE READ IN CONJUNCTION WITH TECHNICAL GUIDE P-G-002.

ASPHALT

- AS PER R116, EACH ASPHALT COURSE DEPTH MUST ALLOW FOR INDIVIDUAL ASPHALT LAYER THICKNESSES. THE RANGE OF ALLOWABLE ASPHALT LAYER THICKNESS DURING PAVING IS LISTED IN TABLE 3.1
- ONLY DENSE GRADED ASPHALT AND STONE MASTIC ASPHALT LAYERS ARE INCLUDED IN THE 175 mm MINIMUM ASPHALT THICKNESS OVER LEAN-MIX CONCRETE OR HEAVILY BOUND MATERIAL. SPRAYED SEALS AND OPEN GRADED ASPHALT WEARING COURSE LAYERS ARE NOT TO BE CONSIDERED AS CONTRIBUTING TO THE 175 mm MINIMUM THICKNESS.

GRANULAR

AS PER R71, EACH COURSE THICKNESS IS TO COMPRISE COMPACTED LAYERS THAT ARE NOT MORE THAN 150 mm OR LESS THAN 100 mm THICK (A SPECIFIED COURSE THICKNESS OF 150 - 200 mm DOES NOT COMPLY).

PRIMERSEAL, SPRAYED SEAL AND LOW CUTTER SEAL

- FOR ASPHALT OVER UNBOUND GRANULAR MATERIAL. REFER TO TECHNICAL GUIDE P-G-002.
- USE THE AVERAGE LEAST DIMENSION (ALD) OF SEALING AGGREGATE TO DETERMINE THE CONTRIBUTION OF A SPRAYED SEAL THICKNESS TO THE TOTAL PAVEMENT THICKNESS.
- A LOW CUTTER SEAL IS TO BE PROVIDED AT ALL LOCATIONS WHERE A SPRAYED SEAL IS TO BE PLACE DIRECTLY UNDER AN ASPHALT LAYER.
- THE APPLICATION OF A LOW CUTTER SEAL IS AS FOLLOWS: - AGGREGATE SPREAD RATE FOR 10 mm AGGREGATE AS PER RMS FORM 395K. AGGREGATE DESIGN. AGGREGATE SPREAD RATEFOR 7 mm AGGREGATE IS BETWEEN 200 - 230 m²/m³ - A MAXIMUM OF 2% CUTTER OIL MAY BE USED - USE 1% ADHESION AGENT
 - DOUBLE THE AMOUNT OF ROLLING FOR A SPRAYED SEAL - TO BE PLACED IN ACCORDANCE WITH R106.
- A 7 mm SPRAYED SEAL IS TO BE PROVIDED OVER THE SMZ LAYER, **EXCEPT AT THE FOLLOWING LOCATIONS:** (a) DIRECTLY UNDER AN ASPHALT LAYER, WHERE A LOW CUTTER SEAL IS TO BE PROVIDED
- (b) DIRECTLY UNDER A GRANULAR LAYER (AS APPROVED BY THE PRINCIPAL)
- 11. FINAL SPRAYED SEAL IS TO BE APPLIED 12 MONTHS AFTER APPLICATION OF PRIMERSEAL. PAVEMENT DESIGNER TO NOMINATE FINAL SPRAYED SEAL TYPE.

BITUMINOUS CURING COMPOUND OVER CONCRETE PAVEMENT

12. IF A NON-BITUMINOUS CURING COMPOUND IS USED AS PER R83, IT IS TO BE REMOVED IMMEDIATELY PRIOR TO THE APPLICATION OF A BITUMINOUS QUICK DRYING PRIME. FOLLOWED BY A SPRAYED SEAL OR ASPHALT LAYER.

BITUMINOUS WATERPROOFING MEMBRANE

13. APPLY BITUMINOUS WATERPROOFING MEMBRANE IN ACCORDANCE **WITH B344.**

DEBONDING TREATMENT

14. APPLY DEBONDING TREATMENT FOR CONCRETE IN ACCORDANCE

BITUMINOUS PRIMER OVER BRIDGE DECK OR APPROACH SLAB

15. APPLY BITUMINOUS PRIMER IN ACCORDANCE WITH B344.

REFERENCES

RMS 3051:GRANULAR BASE AND SUBBASE MATERIALS FOR SURFACED ROAD PAVEMENTS

RMS B344:SPRAYED BITUMINOUS WATERPROOFING MEMBRANE FOR CONCRETE BRIDGE DECKS

RMS B80: CONCRETE WORK FOR BRIDGES

RMS R44: EARTHWORKS RMS R71: CONSTRUCTION OF UNBOUND AND MODIFIED PAVEMENT COURSE

RMS R73: CONSTRUCTION OF PLANT MIXED HEAVILY BOUND PAVEMENT COURSE

RMS R82: LEAN-MIX CONCRETE SUBBASE RMS R83: CONCRETE PAVEMENT BASE

RMS R106:SPRAYED BITUMINOUS SURFACING (WITH CUTBACK BITUMEN)

RMS R116:HEAVY DUTY DENSE GRADED ASPHALT

RMS R119:OPEN GRADED ASPHALT RMS R121:STONE MASTIC ASPHALT

RMS FORM 395K:SEAL OR RESEAL DESIGN CALCULATION SHEET

RMS TECHNICAL GUIDE P-G-002 TYPICAL PAVEMENT PROFILES

RMS R141: PAVEMENT MARKING

RMS R53: CONCRETE (FOR GENERAL USE), MORTAR AND GROUT

RMS R15: KERBS AND GUTTERS RMS Q4: QUALITY MANAGEMENT SYSTEM (TYPE 4)

CONCRETE

- ALL WORKMANSHIP AND MATERIALS SHALL BE IN ACCORDANCE WITH RMS R83.R53 EXCEPT WHERE VARIED BY THE CONTRACT DOCUMENTS.
- CONCRETE COMPOSITION AND CLEAR COVER TO REINFORCEMENT SHALL BE AS FOLLOWS:

ELEMENT	AS 3600 f'c MPa	COVER mm.
FOOTINGS	32	50
FOOTPATH	25	40
ALL - UNO. PAVEMENT	l	40
KERB & GUTTER	32	-

ADMIXTURES, WHERE USED, SHALL COMPLY WITH THE REQUIREMENTS OF RMS R83.

- ALL CONCRETE SUPPLY & TESTING SHALL CONFORM WITH RMS R83.
- CONSTRUCTION JOINTS SHALL BE PROPERLY FORMED AND USED ONLY WHERE SHOWN ON DRAWINGS OR SPECIFICALLY APPROVED BY THE **ENGINEER**
- NO HOLES, CHASES OR EMBEDMENTS OF PIPES, OTHER THAN THOSE SHOWN ON THE STRUCTURAL DRAWINGS, SHALL BE MADE IN CONCRETE MEMBERS
- WITHOUT THE PRIOR APPROVAL OF THE ENGINEER. ALL CONCRETE SHALL BE PLACED AND CURED IN
- ACCORDANCE WITH RMS R83. HORIZONTAL FORMWORK SHALL BE STRIPPED WHEN
- APPROVED BY THE ENGINEER. SLABS AND BEAMS SHALL BEAR ONLY ON THE BEAMS, WALLS ETC. SHOWN ON THE STRUCTURAL DRAWINGS. ALL OTHER BUILDING ELEMENTS SHALL BE KEPT 15mm CLEAR OF SOFFITS OF STRUCTURAL ELEMENTS.
- COMPLETED STRUCTURAL ELEMENTS SHALL CONFORM WITH THE SHAPES, LINES, LEVELS GRADES AND DIMENSIONS REQUIRED BY THE CONTRACT DRAWINGS.
- C10. ALL CONCRETE REINFORCEMENT SHALL CONFORM WITH AS/NZS 4671, DUCTILITY GRADE: CLASS N. NO REINFORCEMENT SHALL BE WELDED WITHOUT PRIOR APPROVAL
- C11. R = STRUCTURAL GRADE ROUND BAR N = "TEMPCORE" DEFORMED BAR F = HARD DRAWN STEEL WIRE REINFORCING FABRIC DESIGNATION CODE OF REINFORCEMENT BARS:-

NO. OF BARS BAR GRADE AND TYPE

NOMINAL BAR SIZE -SPACING IN mm

14 N16-300

UNO - UNLESS NOTED OTHERWISE. NSOP - NOT SHOWN ON PLAN.

- LV LENGTH VARIES C12. TRANSVERSE TIE BARS N12-300, WHERE NOT OTHERWISE SHOWN. SPLICE WHERE NECESSARY AND
- LAP WITH MAIN BARS 400mm. SPLICES IN REINFORCEMENT SHALL BE MADE ONLY IN POSITIONS SHOWN ON THE STRUCTURAL DRAWINGS OR IN POSITIONS OTHERWISE APPROVED IN WRITING BY THE PRINCIPAL. LAPS SHALL BE IN ACCORDANCE WITH AS3600 AND NOT LESS THAN THE DEVELOPMENT FOR EACH BAR. SITE BENDING OF REINFORCING BARS SHALL BE DONE WITHOUT HEATING USING MECHANICAL BENDING TOOLS. WELDING OF REINFORCEMENT SHALL NOT BE PERMITTED UNLESS SHOWN ON THE STRUCTURAL DRAWINGS OR APPROVED BY THE STRUCTURAL ENGINEER. FABRICS SHALL BE LAPPED 2 TRANSVERSE WIRES PLUS 50mm. BUNDLED BARS SHALL BE TIED TOGETHER AT 30 BAR DIAMETER CENTRES WITH THREE WRAPS OF TIE WIRE.
- C14. REINFORCEMENT IS SHOWN DIAGRAMMATICALLY. IT IS NOT NECESSARILY SHOWN IN TRUE PROJECTION.
- ALL REINFORCEMENT SHALL BE MAINTAINED IN POSITION DURING CONCRETING.

ASPHALTIC CONCRETE NOTES

GENERAL

- MINERAL AGGREGATES TO COMPLY WITH CLAUSE 2.1 - MATERIALS OF RMS SPECIFICATION R116 "ASPHALT (DENSE AND OPEN GRADED)"
- MINERAL FILLER TO COMPLY WITH AS 2357 (LATEST EDITION) MINERAL FILLERS FOR ASPHALT AND CLAUSE 2.1.2 - FILLER OF RMS SPECIFICATION R116 "ASPHALT
- (DENSE AND OPEN GRADED)" BITUMEN BINDER SHALL COMPLY WITH CLAUSE 2.1.3 -BINDER OF RMS SPECIFICATION R116 - "ASPHALT (DENSE AND OPEN GRADED)"

MIX PROPORTIONS

- JOB MIX 10mm AND 20mm (AS SPECIFIED ON DRAWINGS) NOMINAL SIZE AGGREGATE MINIMUM BITUMEN CONTENT (%) BY MASS OF TOTAL MASS
- AIR VOIDS IN COMPACTED MIX BETWEEN 4% AND 7% OF THE VOLUME OF THE MIX.
- VOIDS FILLED IN BINDER 65-80% OF AIR VOIDS IN THE TOTAL MINERAL AGGREGATE FILLED BY BINDER IN ACCORDANCE WITH RMS TEST METHOD T605 AND

PAVEMENT PREPARATION_

- THE EXISTING SURFACE TO BE SEALED SHALL BE DRY AND BROOMED BEFORE COMMENCEMENT OF WORK TO ENSURE COMPLETE REMOVAL OF ALL SUPERFICIAL FOREIGN MATTER.
- ALL DEPRESSIONS OR UNEVEN AREAS ARE TO BE TACK-COATED AND BROUGHT UP TO GENERAL LEVEL OF PAVEMENT WITH ASPHALTIC CONCRETE BEFORE LAYING OF MAIN COURSE

TACK COAT

THE WHOLE OF THE AREA TO BE SHEETED WITH ASPHALTIC CONCRETE SHALL BE LIGHTLY AND EVENLY COATED WITH RAPID SETTING BITUMEN COMPLYING WITH RMS SPECIFICATIONS 3252, 3253. 3254, 3259 AND 3269. APPLICATION RATE FOR RESIDUAL BITUMEN SHALL BE 0.15 TO 0.30 LITRES/SQUARE METRE. APPLICATION SHALL BE BY MEANS OF A MECHANICAL SPRAYER WITH SPRAY BAR.

SPREADING

- ALL ASPHALTIC CONCRETE SHALL BE SPREAD WITH A SELF PROPELLED PAVING MACHINE
- THE ASPHALTIC CONCRETE SHALL BE LAID AT A MIX TEMPERATURE AS SHOWN BELOW: ROAD SURFACE TEMPERATURE IN SHADE A(°C), MIX TEMPERATURES (°C) 5-10°C: NOT PERMITTED 10-15°C: 150°C 15-25°C: 145°C
- OVER 25°C: 140°C ASPHALTIC CONCRETE SHALL NOT BE LAID WHEN THE ROAD SURFACE IS WET OR WHEN COLD WINDS CHILL THE MIX TO ADVERSELY AFFECT SPREADING AND COMPACTION.

- THE NUMBER OF JOINTS BOTH LONGITUDINAL AND
- TRANSVERSE SHALL BE KEPT TO A MINIMUM. THE DENSITY AND SURFACE FINISH AT JOINTS SHALL BE SIMILAR TO THOSE OF THE REMAINDER OF THE LAYER.

COMPACTION

- ALL COMPACTION SHALL BE UNDERTAKEN USING SELF PROPELLED ROLLERS.
- INITIAL ROLLING SHALL BE COMPLETED BEFORE THE MIX TEMPERATURE FALLS BELOW 105°C. SECONDARY ROLLING SHALL BE COMPLETED BEFORE
- THE MIX TEMPERATURE FALLS BELOW 60°C USE PNEUMATIC TYRED ROLLER.
- MINIMUM CHARACTERISTICS VALUE OF RELATIVE COMPACTION OF A LOT WHEN TESTED IN ACCORDANCE WITH CLAUSE 4.9 - COMPACTION OF RMS SPECIFICATION 116 "ASPHALT (DENSE AND OPEN GRADED)" SHALL BE 95%.

FINISHED PAVEMENT PROPERTIES

FINISHED SURFACES SHALL BE SMOOTH DENSE AND TRUE TO SHAPE AND SHALL NOT VARY MORE THAN 10MM FROM THE SPECIFIED PLAN LEVEL AT ANY POINT AND SHALL NOT DEVIATE FROM THE BOTTOM OF A 3M STRAIGHT EDGE LAID IN ANY DIRECTION BY MORE THAN 5MM.

EARTHWORKS

- THE CONTRACTOR SHALL PROVIDE PROPER FENCING. GUARDING, LIGHTING AND OBSERVATION OF ALL EARTHWORKS, TEMPORARY ROADWAYS, FOOTWAYS, GUARDS AND FENCES AS MAY BE RENDERED NECESSARY FOR THE ACCOMMODATION AND PROTECTION OF PEDESTRIANS, VEHICLES, ANIMALS
- AND THE PUBLIC. DURING THE EXECUTION OF WORKS, THE CONTRACTOR SHALL MAINTAIN THE INTEGRITY OF EXISTING SERVICES THE CONTRACTOR SHALL REPAIR ANY DAMAGE CAUSED TO THE EXISTING SERVICES TO THE SATISFACTION OF THE SUPERINTENDENT AND THE RELEVANT SERVICE AUTHORITY, AT NO COST TO THE PRINCIPAL
- WHERE IT IS NECESSARY TO REMOVE, DIVERT OR CUT INTO ANY EXISTING SERVICE, THE CONTRACTOR SHALL GIVE AT LEAST THREE (3) DAYS NOTICE OF ITS REQUIREMENTS TO THE SUPERINTENDENT, WHO WILL ADVISE WHAT ARRANGEMENTS SHOULD BE MADE FOR THE ALTERATION OF SUCH EXISTING WORKS
- THE EXCAVATION SHALL BE CARRIED OUT IN THE LOCATIONS SHOWN AND TO THE LEVELS, WIDTHS AND BATTER SLOPES INDICATED ON THE DRAWINGS.
- EXCAVATED MATERIAL NOT MEETING THE SPECIFICATION FOR FILL MATERIAL SHALL BE DISPOSED OF OFF SITE IN AN APPROPRIATE MANNER.
- WHERE EXCAVATION WORK IS REQUIRED IN THE VICINITY OF EXISTING SERVICES, THE CONTRACTOR SHALL SUPPORT ALL SERVICES DURING THE WORKS

WHERE EXCAVATED MATERIAL IS TO BE USED FOR

- FILLING, THE MATERIAL SHALL BE INSPECTED AND APPROVED BY THE SUPERINTENDENT PRIOR TO USE UNLESS SPECIFIED OTHERWISE ALL FILL SHALL BE COMPACTED TO A STANDARD MAXIMUM DRY DENSITY RATIO BETWEEN 98% - 102% MAXIMUM AT -1% TO +3%
- DETERMINED BY AS1289.5.4.1 (LATEST ADDITION) ALL WASTE MATERIALS SHALL BE DISPOSED OFF-SITE IN AN APPROPRIATE MANNER.

OF STANDARD OPTIMUM MOISTURE CONTENT AS

- WHERE ROCK IS EXPOSED DURING EXCAVATION, THE CONTRACTOR SHALL CEASE EXCAVATION AT THIS LOCALITY AND CONTACT THE SUPERINTENDENT WHO WILL THEN ADVISE ON THE LEVEL TO WHICH
- **EXCAVATION IS TAKEN.** 11. THE CONTRACTOR SHALL AT ITS OWN EXPENSE DO ALL THINGS NECESSARY TO DIVERT ANY WATER INTERFERING WITH THE PROGRESS OF WORKS, KEEP THE EXCAVATIONS AND TRENCHES FREE FROM WATER WHILE THE WORKS ARE IN PROGRESS AND PREVENT ANY DAMAGE TO THE WORKS BY WATER DUE TO FLOODS OR OTHER CAUSES. THE CONTRACTOR SHALL HAVE PUMPING EQUIPMENT FOR KEEPING THE EXCAVATION OR TRENCHES CONSTANTLY DEWATERED DURING THE TIMES THE WORKS ARE IN PROGRESS. ANY WORK OR MATERIAL DAMAGED BY WATER SHALL BE
- MADE GOOD BY THE CONTRACTOR WHERE DIRECTED BY THE SUPERINTENDENT THE BOTTOM OF TRENCHES OR EXCAVATIONS SHALL BE COMPACTED PRIOR TO THE PLACING OF ANY BEDDING OR CONCRETE MATERIALS. SHOULD, IN THE OPINION OF THE SUPERINTENDENT, THE FOUNDATION MATERIAL BE INCAPABLE OF EFFECTIVE COMPACTION, THE MATERIAL SHALL BE REMOVED AND REPLACED WITH APPROPRIATE MATERIAL

QUALITY ASSURANCE

- THE CONTRACTOR SHALL IMPLEMENT AND MAINTAIN A QUALITY ASSURANCE SYSTEM MEETING THE REQUIREMENTS OF RMS Q4 BE SUCH THAT RECORDS ARE KEPT OF ALL ASPECTS AND STAGES OF THE
- THE RECORDS FOR EACH CONSTRUCTION TASK SHALL BE STAGED AND ITEMISED TO THE SATISFACTION OF THE CONTRACTOR ADMINISTRATOR. THE PROFORMAS FOR RECORDS SHALL BE SUBMITTED TO THE CONTRACTOR ADMINISTRATOR FOR APPROVAL AND WORK SHALL NOT COMMENCE UNTIL SUCH APPROVAL HAS BEEN GIVEN.
- DURING THE COURSE OF CONSTRUCTION, THE CONTRACTOR SHALL MAINTAIN ACCURATE AND UP TO DATE RECORDS AND SHALL MAKE SUCH RECORDS AVAILABLE TO THE CONTRACTOR ADMINISTRATOR IF REQUESTED. FAILURE TO MAINTAIN RECORDS AS SPECIFIED WILL RESULT IN THE CONTRACTOR RE-INSPECTING COMPLETED WORKS IF INSTRUCTED TO DO SO BY THE CONTRACTOR ADMINISTRATOR. AT THE COMPLETION OF EACH STAGE OF THE WORKS

THE CONTRACTOR SHALL CERTIFY THAT THOSE

WORKS HAVE BEEN UNDERTAKEN AND COMPLETED IN

ACCORDANCE WITH THE DRAWINGS, SPECIFICATION

AND INSTRUCTIONS ISSUED DURING THE COURSE OF

FILL

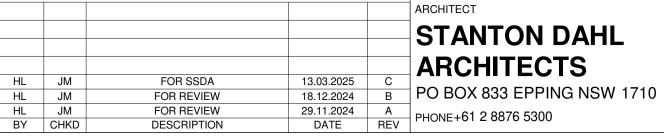
- FOUNDATION MATERIAL DEEMED BY THE SUPERINTENDENT AS UNSUITABLE TO BE REMOVED AS DIRECTED BY THE SUPERINTENDENT AND REPLACED WITH APPROVED MATERIAL SATISFYING
- THE REQUIREMENTS LISTED BELOW. UNLESS OTHERWISE APPROVED OR SPECIFIED. ALL FILL MATERIAL SHALL BE FROM A SOURCE APPROVED BY THE SUPERINTENDENT AND SHALL COMPLY WITH THE FOLLOWING: A) FREE FROM ORGANIC AND PERISHABLE MATTER B) MAXIMUM PARTICLE SIZE 75MM C) PLASTICITY INDEX BETWEEN 2% AND 20%
- D) CBR > 10SELECT FILL MATERIAL SHALL BE PLACED IN MAXIMUM 200MM LOOSE THICK LAYERS AND COMPACTED AT OPTIMUM MOISTURE CONTENT (+ OR - 2%) TO ACHIEVE A DRY DENSITY DETERMINED IN ACCORDANCE WITH AS1289E3.1 (LATEST EDITION) OF NOT LESS THAN THE FOLLOWING STANDARD MAXIMUM DRY DENSITY IN ACCORDANCE WITH AS1289E1.1 (LATEST EDITION): LOCATION STANDARD DRY DENSITY -AREAS OF SERVICE TRENCHES 98% -ROAD AND CARPARKS 100%
- -LANDSCAPED AREAS 90% THE CONTRACTOR SHALL PROGRAMME THE EARTHWORKS OPERATION SO THAT THE WORKING AREAS ARE ADEQUATELY DRAINED DURING THE PERIOD OF CONSTRUCTION. THE SURFACE SHALL BE GRADED AND SEALED OFF TO REMOVE DEPRESSIONS. ROLLER MARKS AND SIMILAR WHICH WOULD ALLOW WATER TO POND AND PENETRATE THE UNDERLYING MATERIAL. ANY DAMAGE RESULTING FROM THE CONTRACTOR NOT OBSERVING THESE REQUIREMENTS SHALL BE RECTIFIED BY THE CONTRACTOR AT HIS COST
- COMPACTION CONTROL TESTING SHALL BE CARRIED OUT BY AND AT THE COST OF THE CONTRACTOR TO CONFORM WITH LEVEL 1, AS DEFINED IN AS3798 (LATEST EDITION)
- REPLACEMENT SUBGRADE MATERIAL MAY COMPRISE GRANULAR FILL MATERIAL AND SHOULD HAVE A CBR OF AT LEAST 10%. REPLACEMENT SUBGRADE FILL SHALL BE PLACED ON PROOF ROLLED SUBGRADE IN HORIZONTAL LAYERS OF 200MM TO 250MM MAXIMUM LOOSE THICKNESS (DEPENDING ON THE SIZE OF THE EQUIPMENT) AND COMPACTED TO A MINIMUM DRY DENSITY RATIO (MDDR) OF 98% STANDARD, AT MOISTURE CONTENT WITHIN 2% OF OPTIMUM MOISTURE CONTENT.
- WHERE EXCAVATED MATERIAL IS TO BE USED FOR FILLING, THE MATERIAL SHALL BE INSPECTED AND APPROVED BY THE SUPERINTENDENT PRIOR TO

SIGNAGE & LINEMARKING

ALL SIGNAGE & LINEMARKING TO BE IN ACCORDANCE WITH RELEVANT AUSTRALIAN STANDARD & RMS SPECIFICATION.

LANDSCAPING (PUBLIC ROAD RESERVE)

- ALL LANDSCAPING TO BE UNDERTAKEN IN
- ACCORDANCE WITH THE COUNCIL REQUIREMENTS. FOR DETAILS OF ALL LANDSCAPING REFER TO
- DRAWINGS BY HASSELL.







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GENERAL NOTES

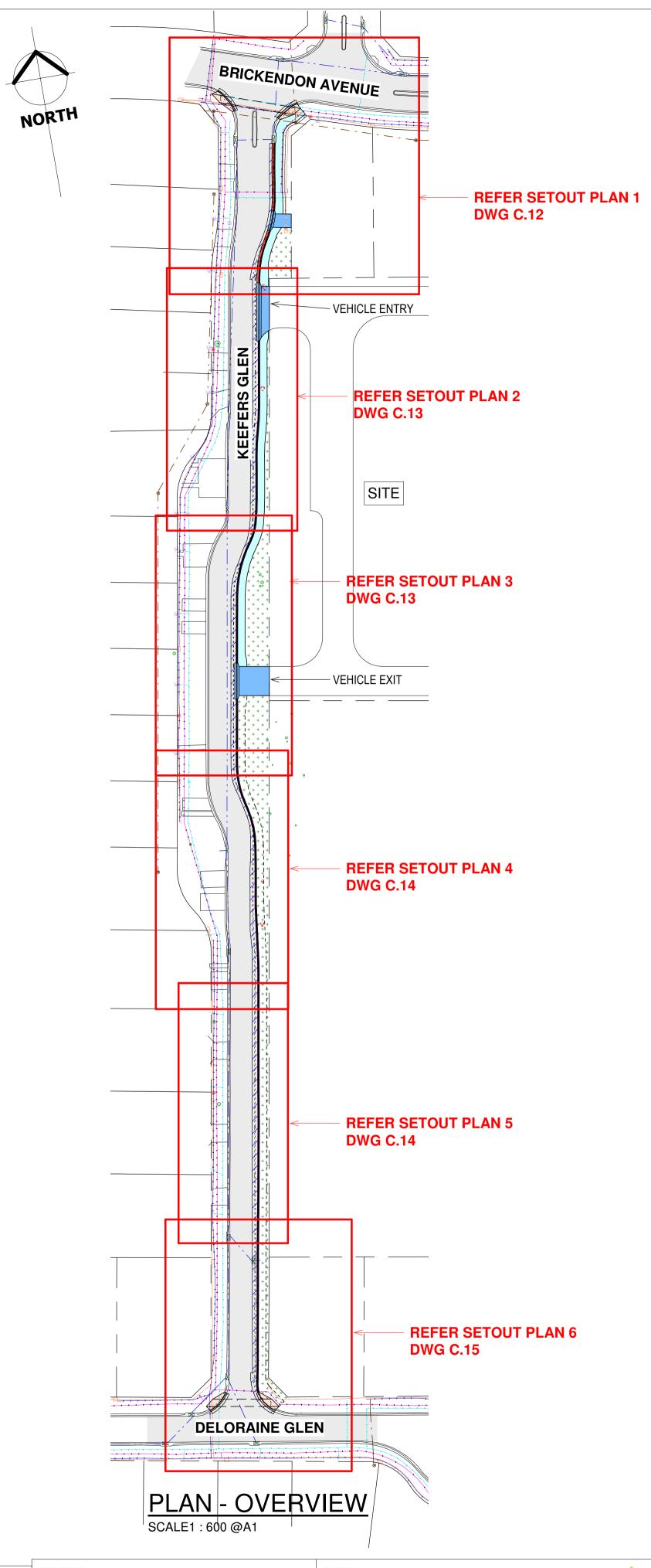
THE CONTRACT.

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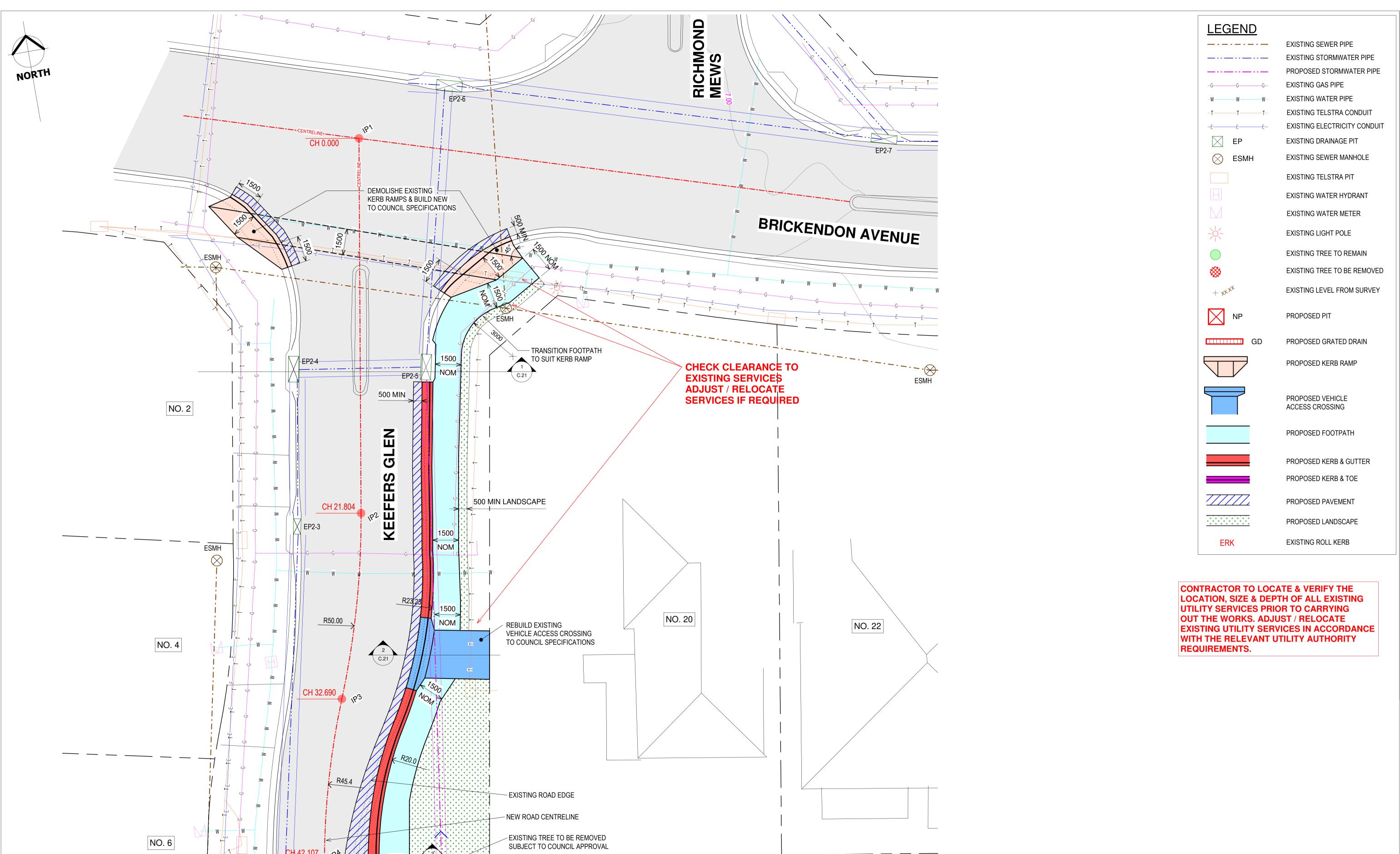
PLAN - OVERVIEW

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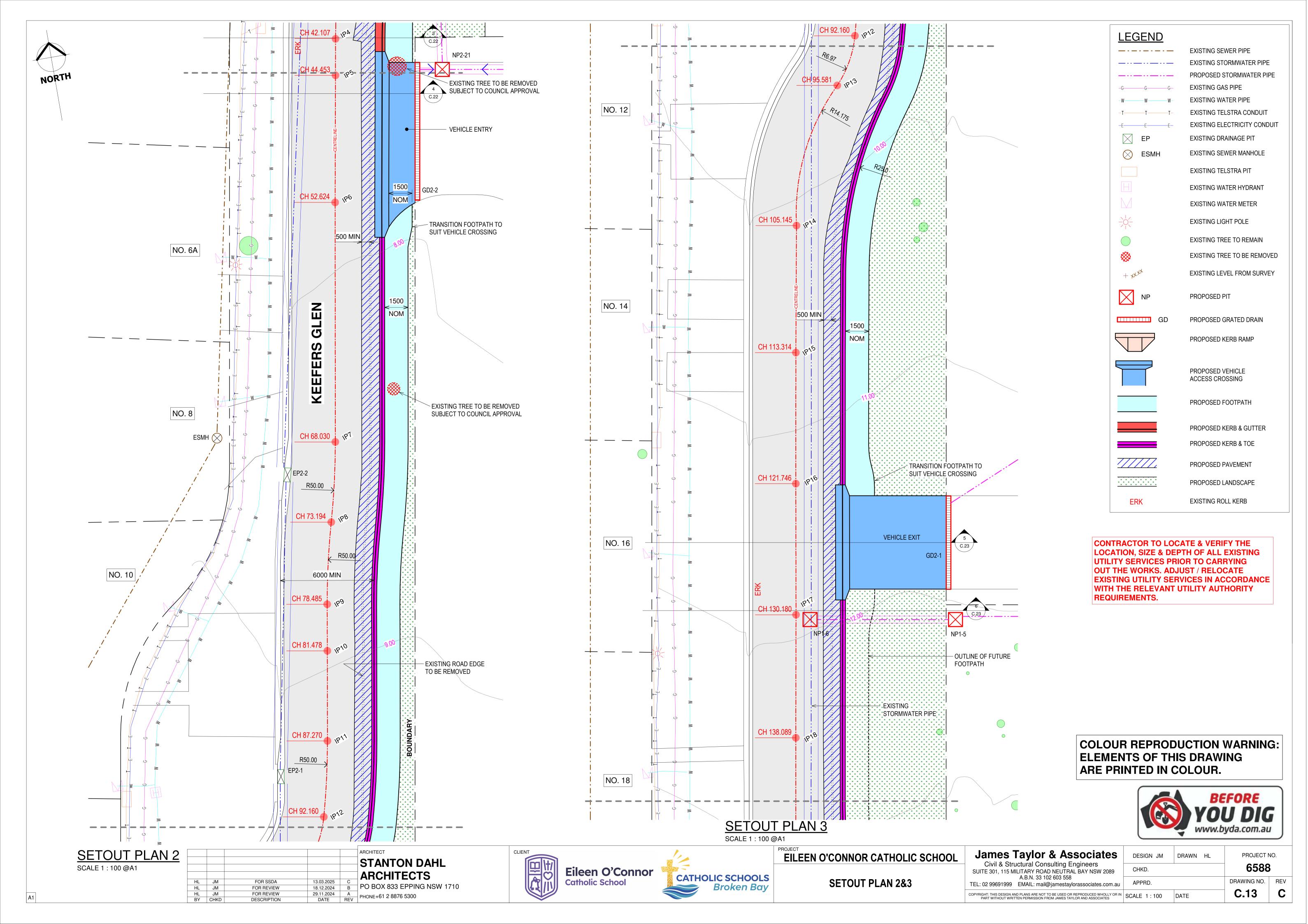
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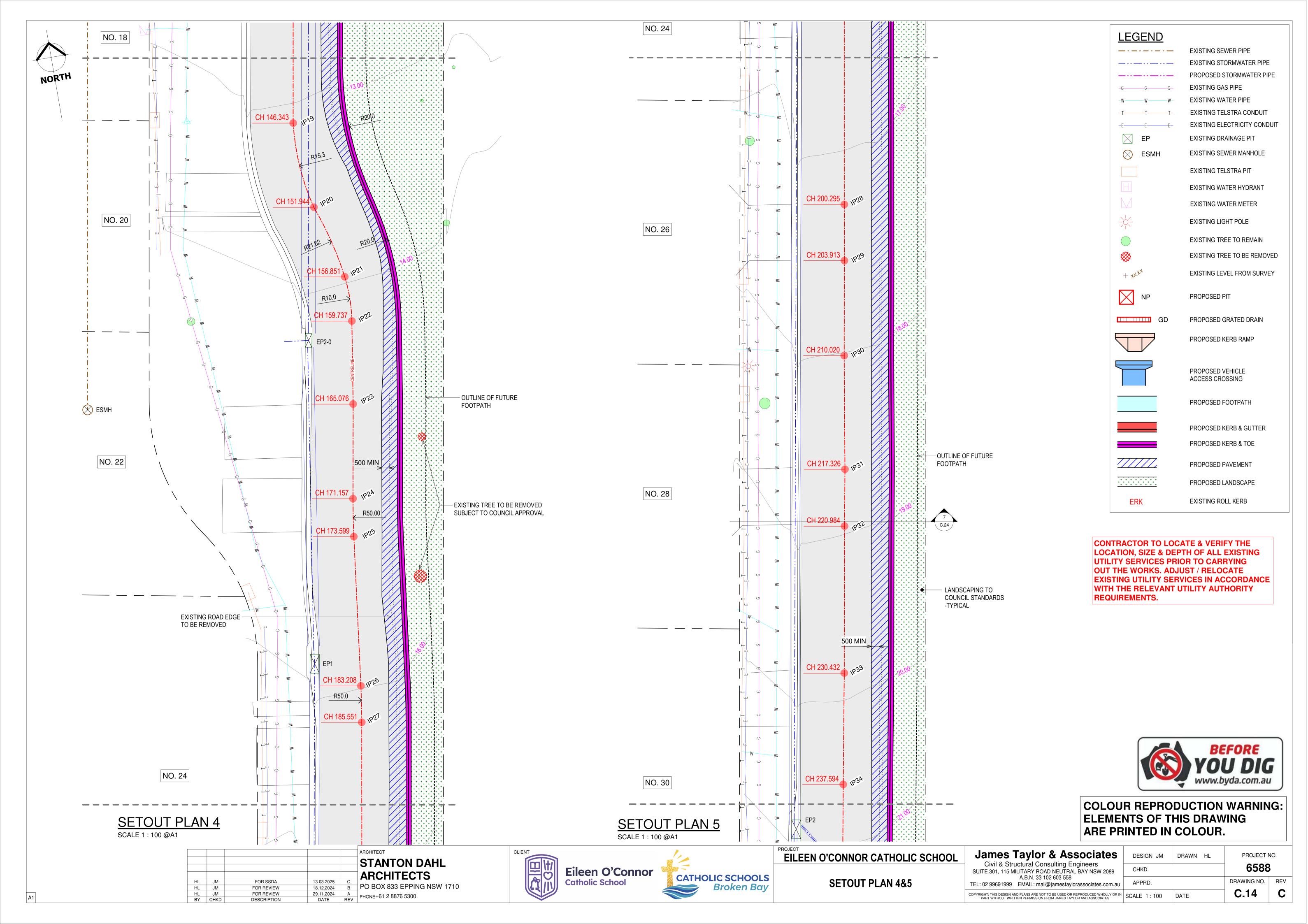
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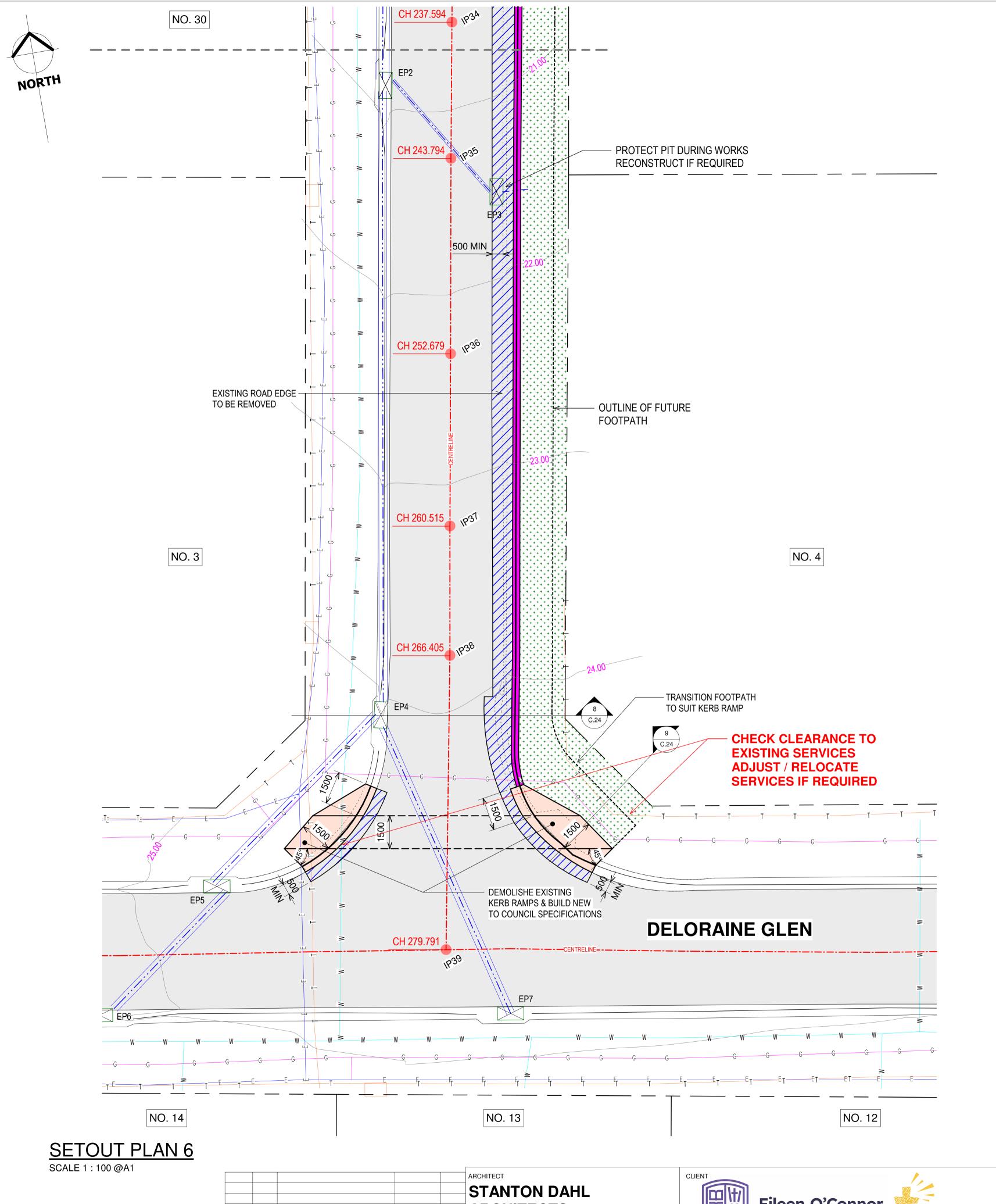
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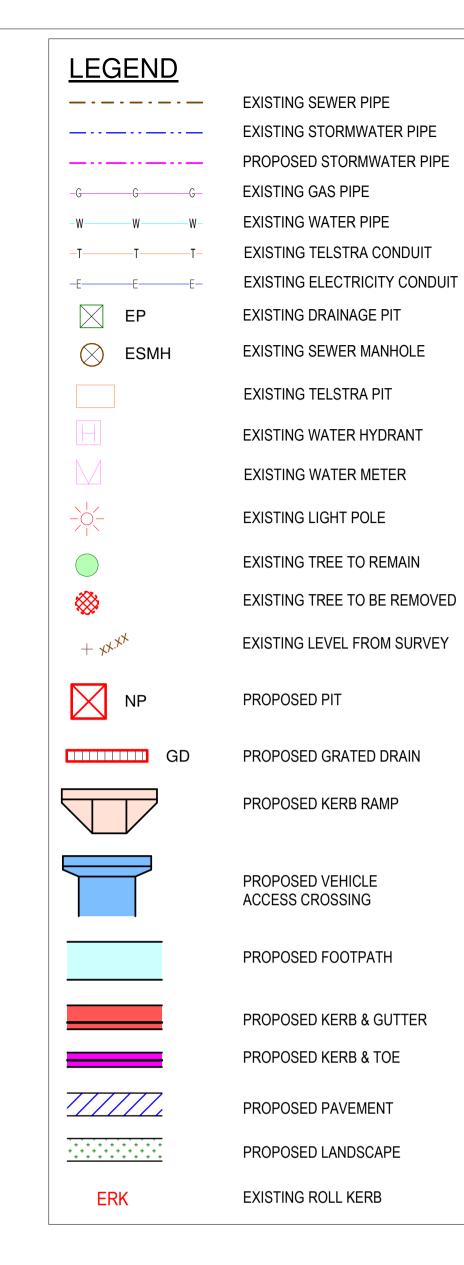
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FOR REVIEW

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CONTRACTOR TO LOCATE & VERIFY THE LOCATION, SIZE & DEPTH OF ALL EXISTING **UTILITY SERVICES PRIOR TO CARRYING** OUT THE WORKS. ADJUST / RELOCATE **EXISTING UTILITY SERVICES IN ACCORDANCE** WITH THE RELEVANT UTILITY AUTHORITY REQUIREMENTS.



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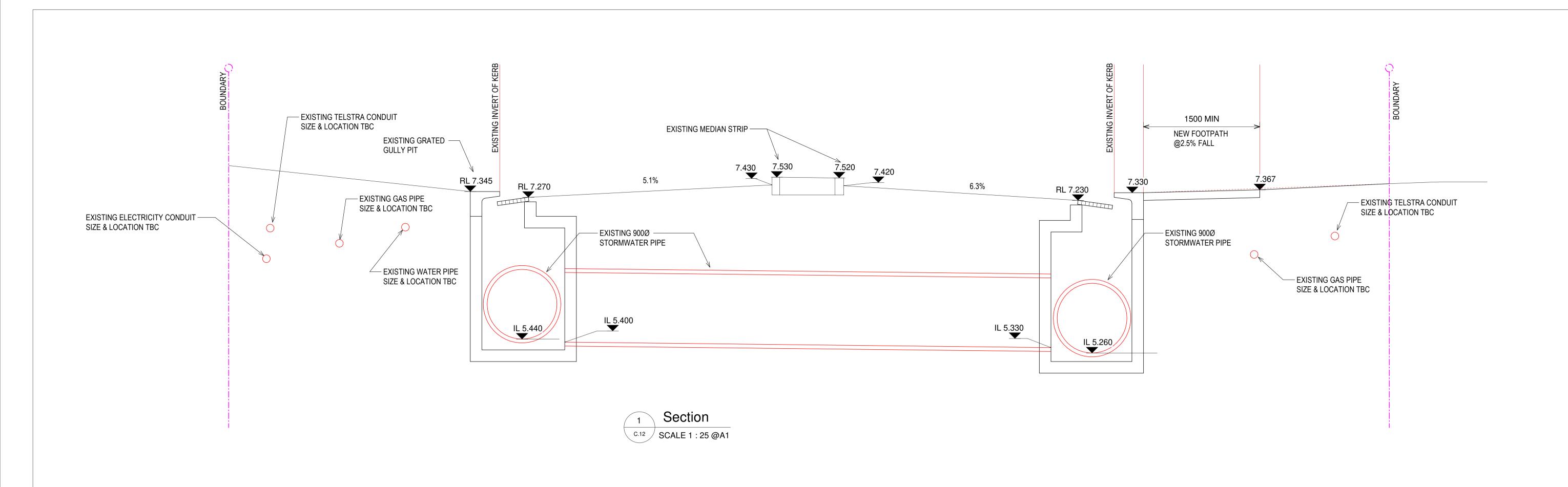
EILEEN O'CONNOR CATHOLIC SCHOOL **Eileen O'Connor ARCHITECTS CATHOLIC SCHOOLS Catholic School SETOUT PLAN 6**

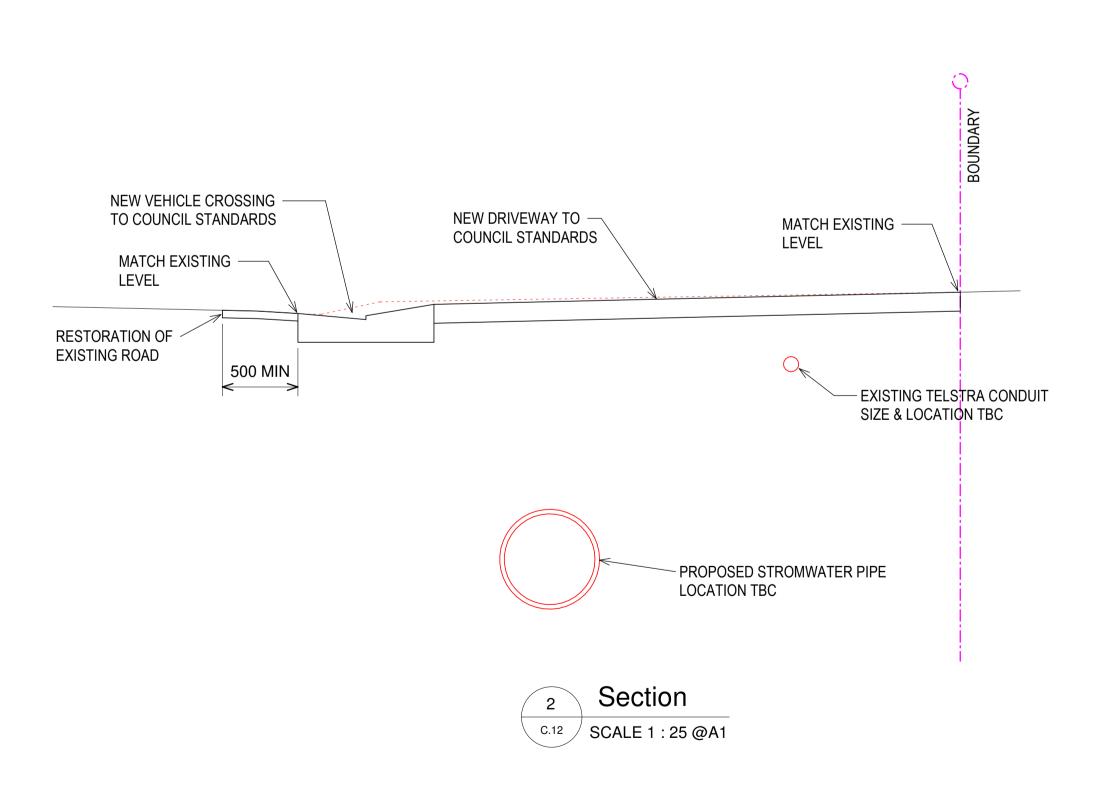
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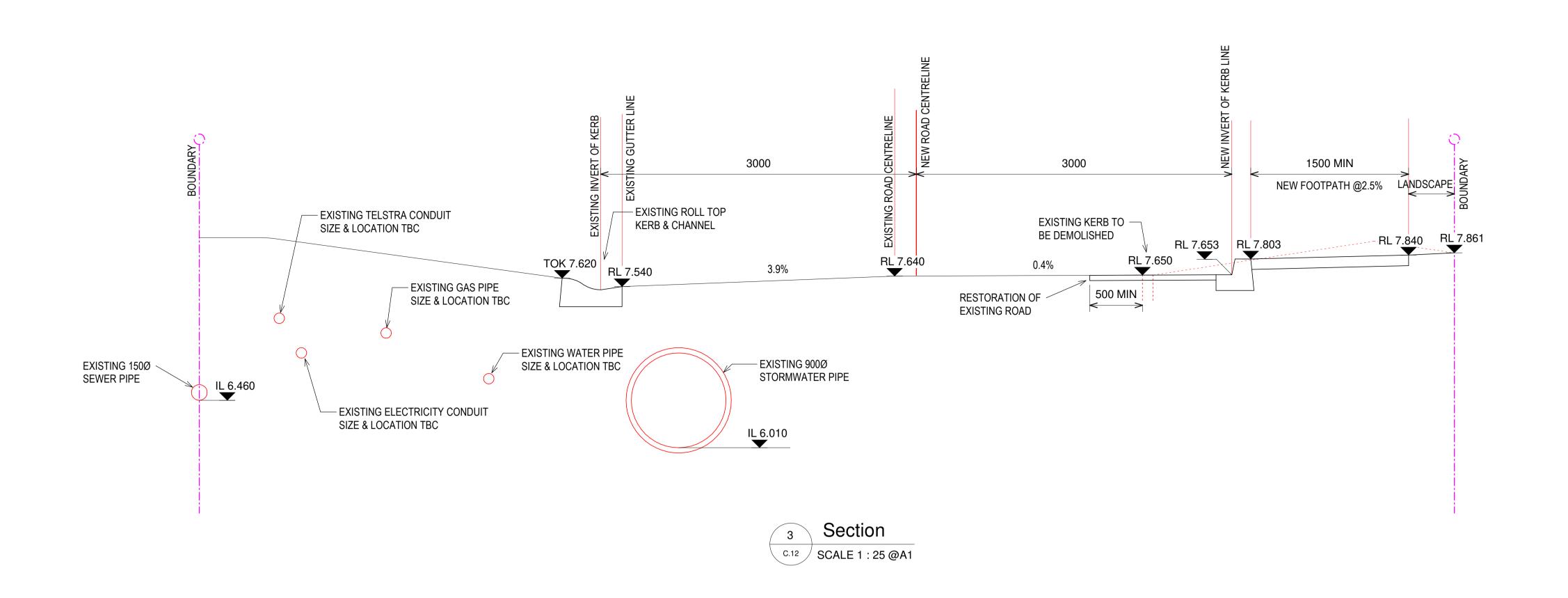
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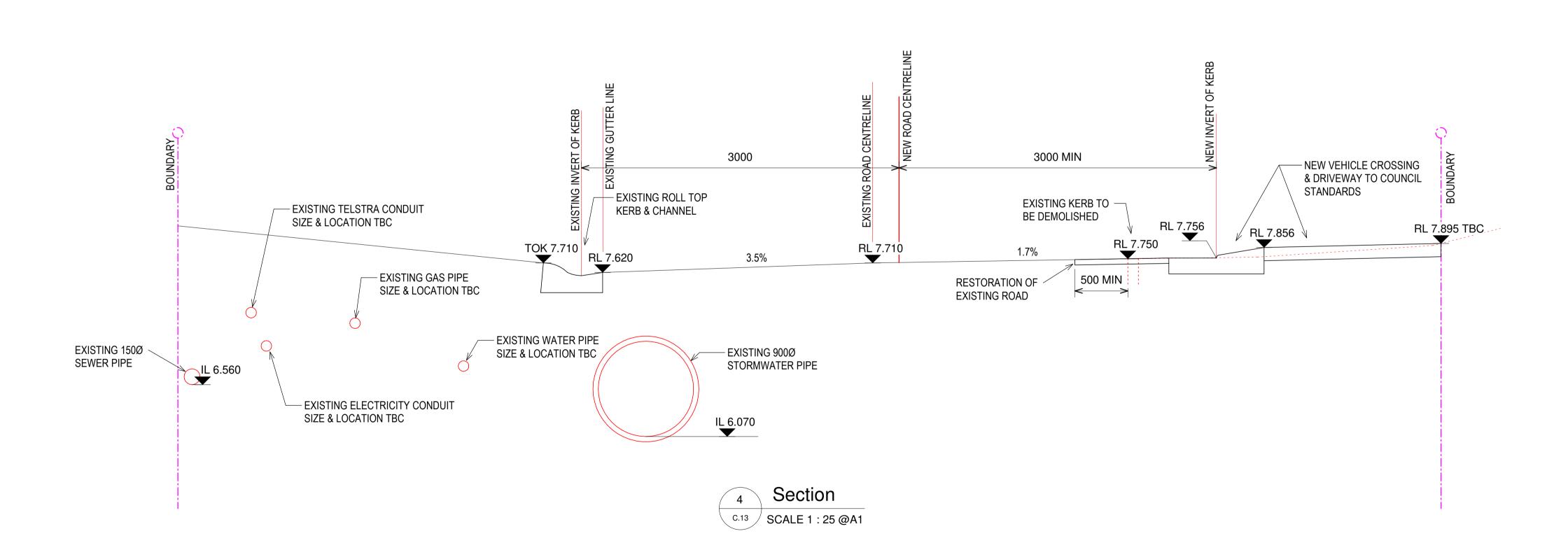




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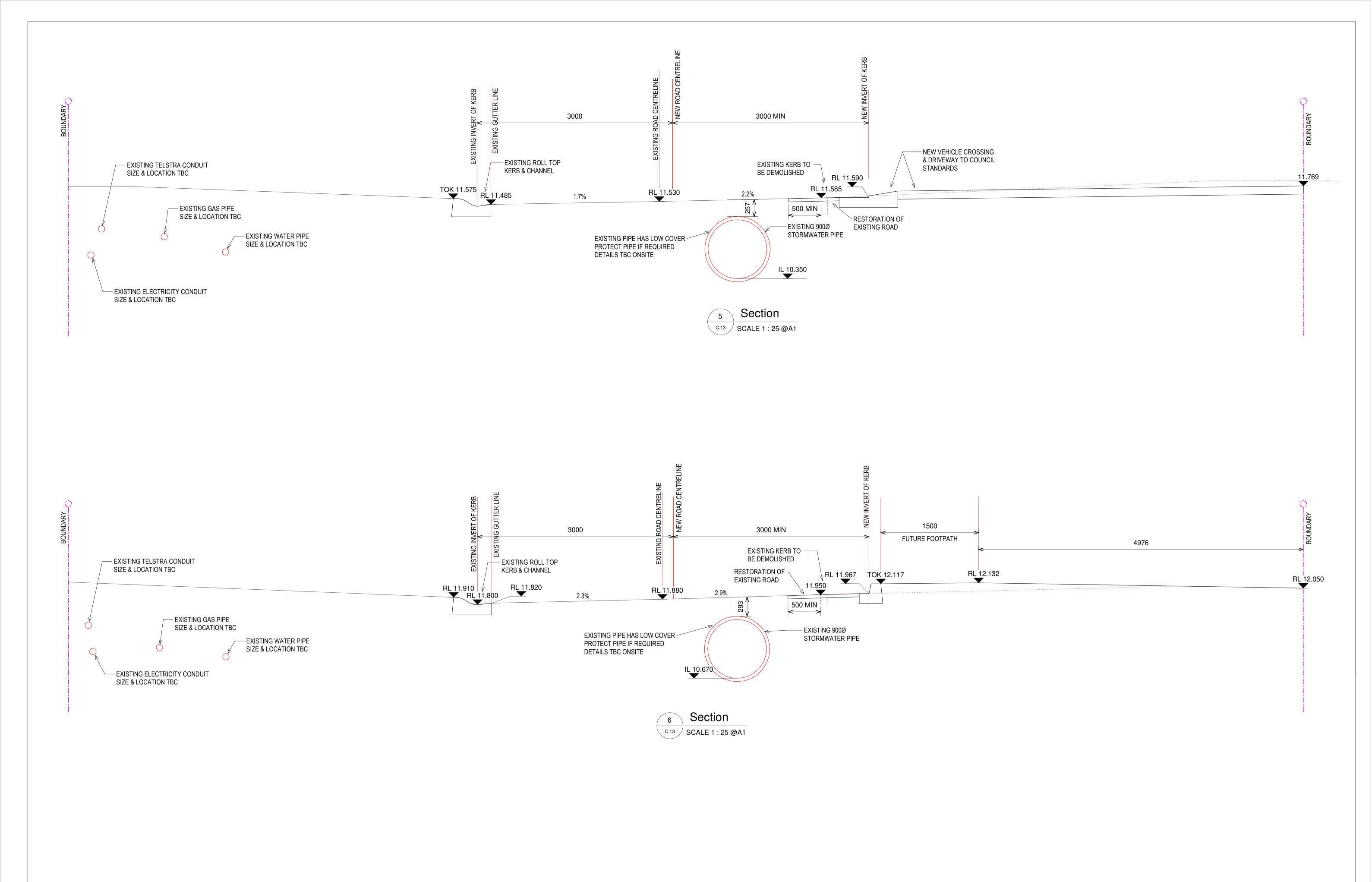
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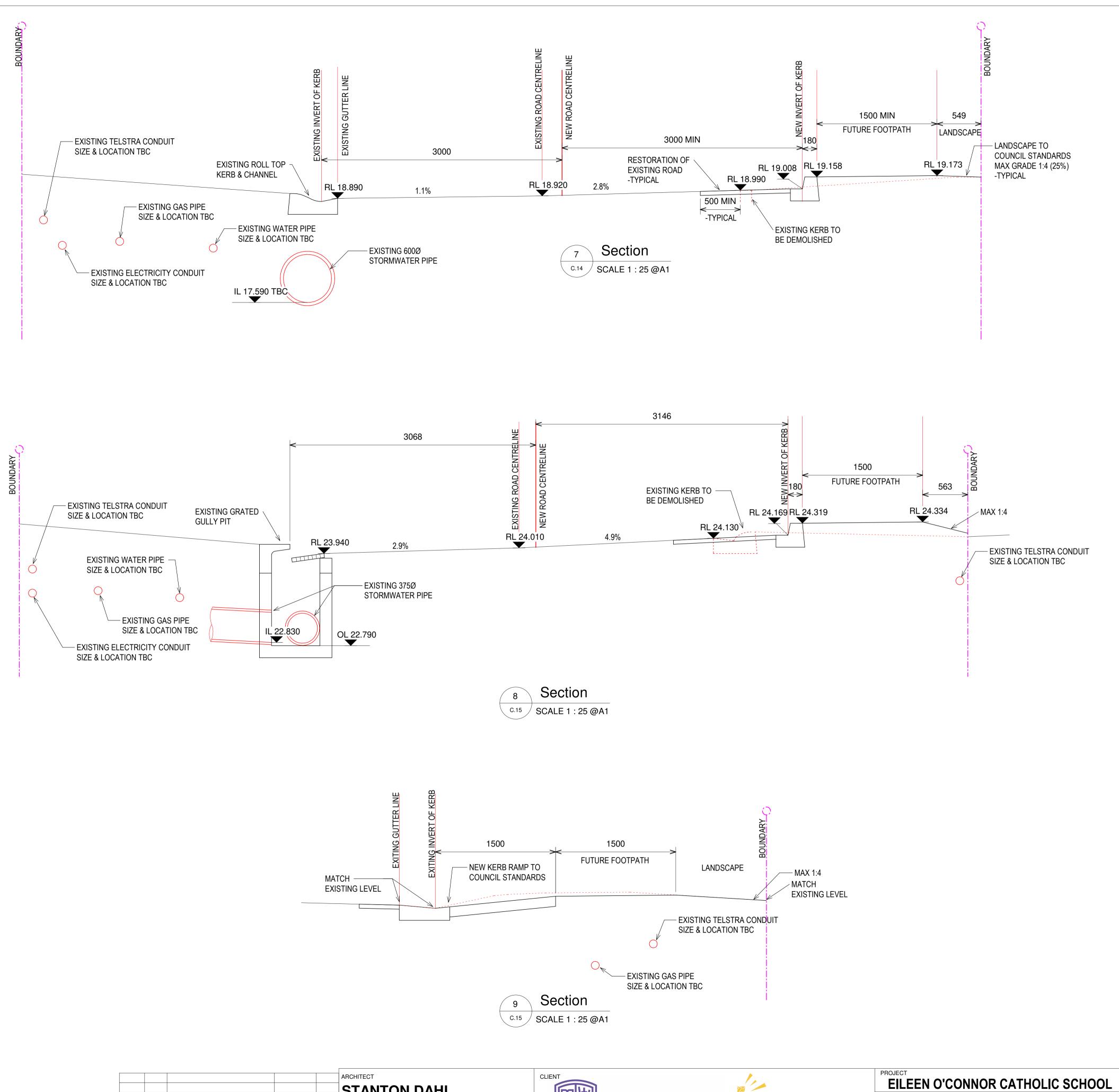
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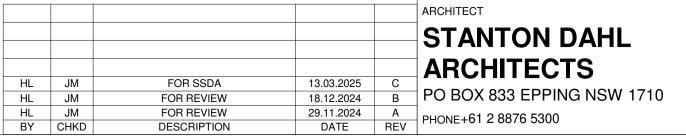




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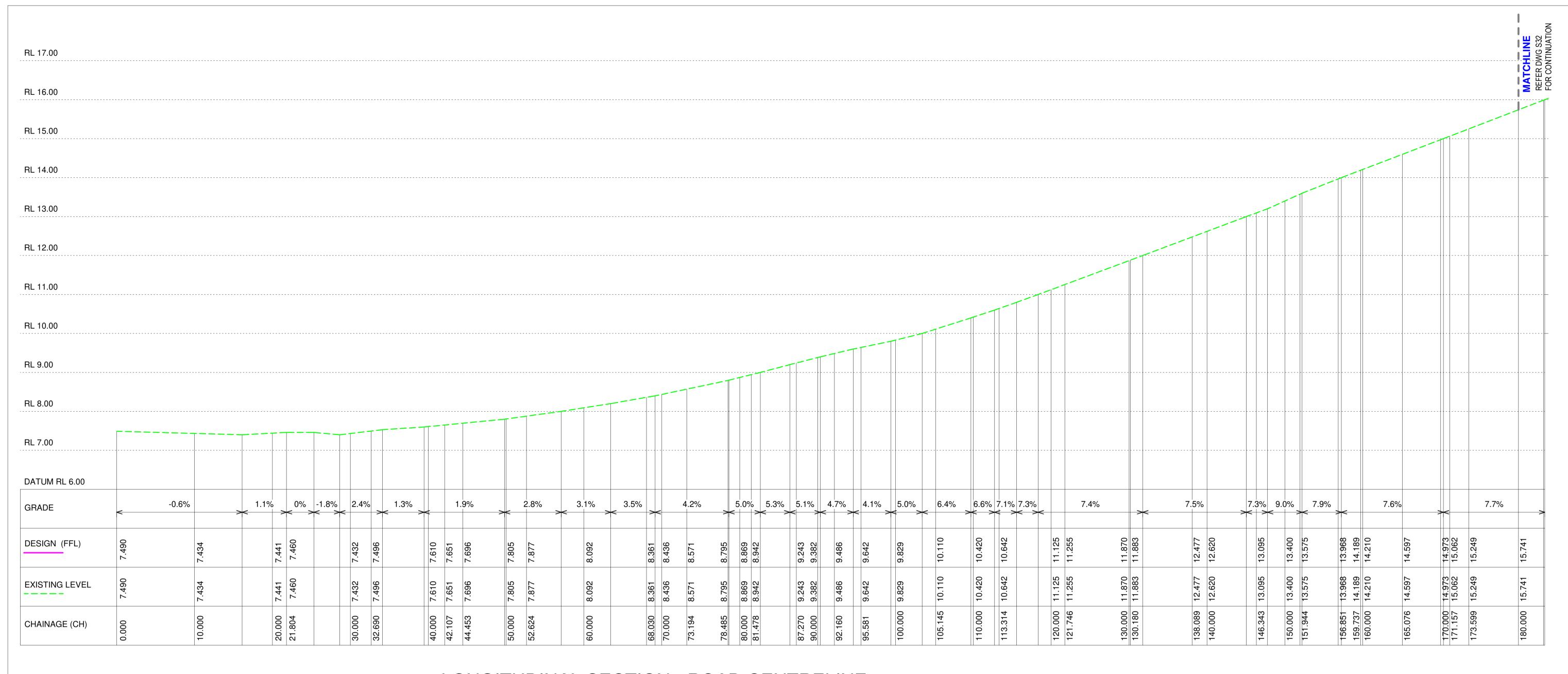
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LONGITUDINAL SECTION - ROAD CENTRELINE

1:250 HORIZONTAL 1:50 VERTICAL

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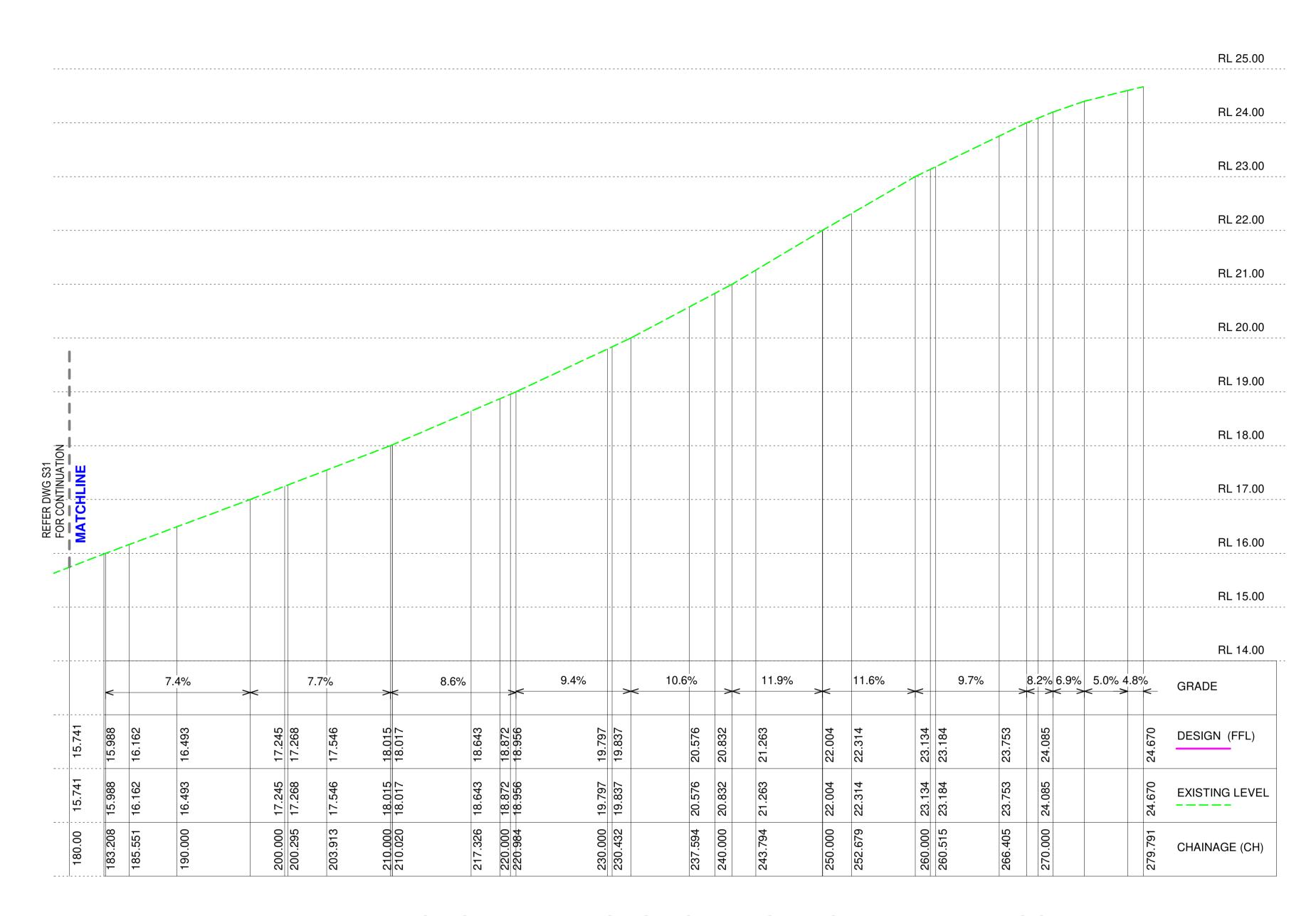


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LONGITUDINAL SECTION - ROAD CENTRELINE - CONTINUED

1:250 HORIZONTAL 1:50 VERTICAL

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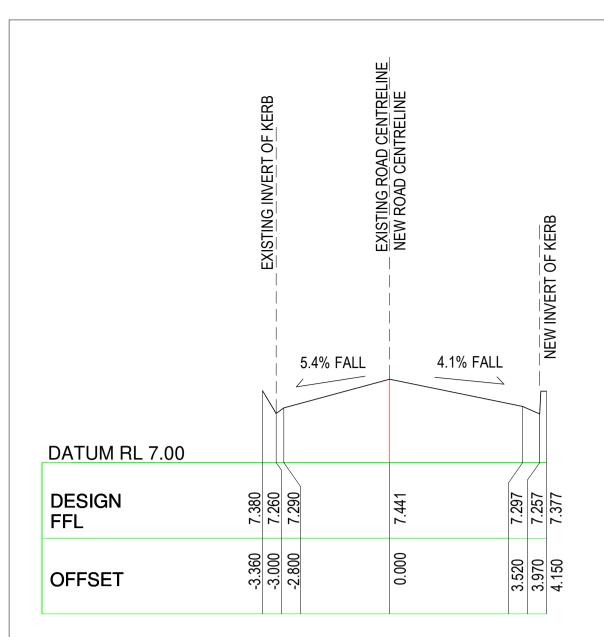
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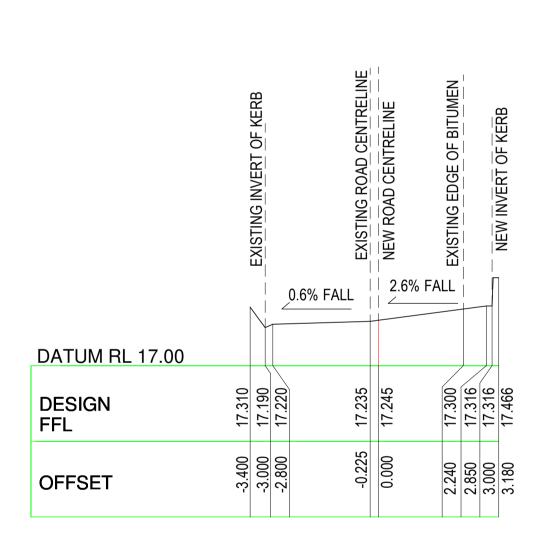
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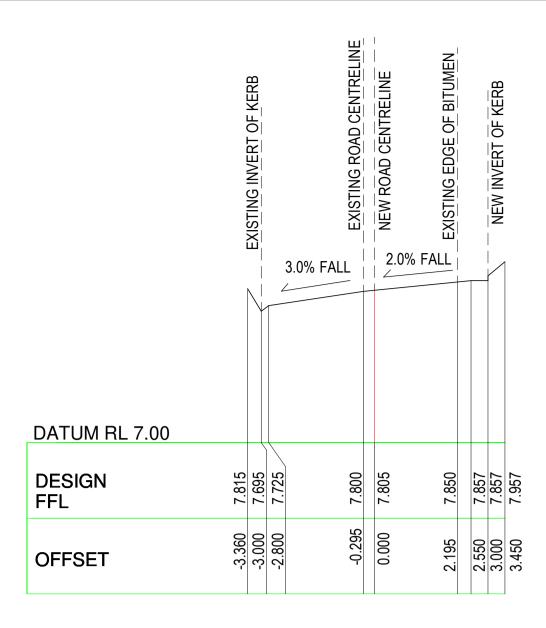




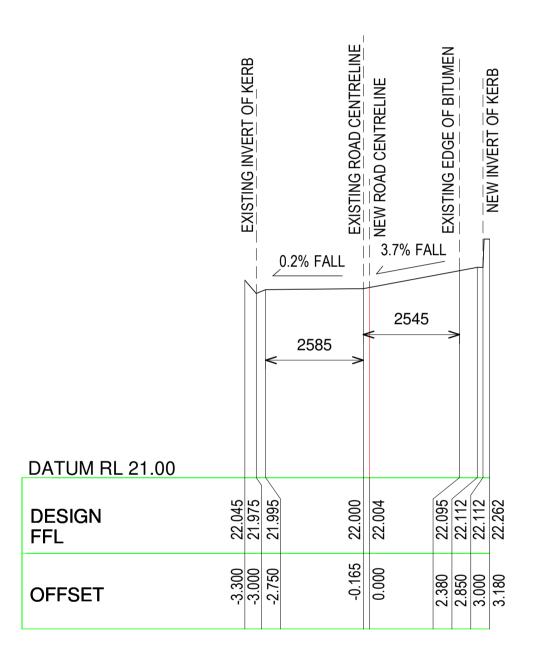


CH200.00

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VERTICAL SCALE 1:20

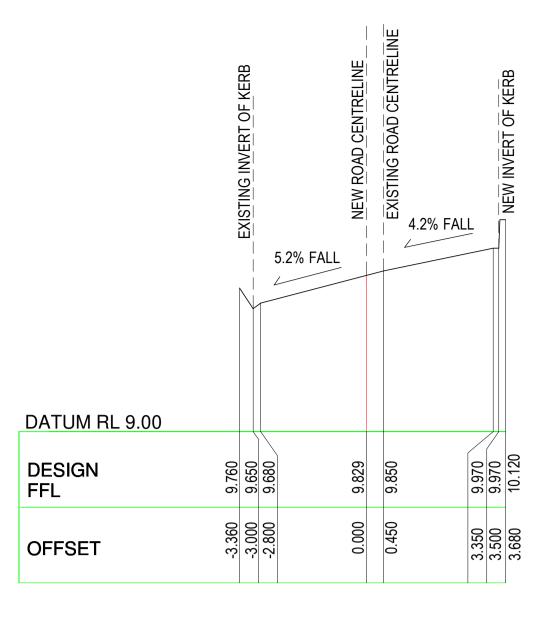


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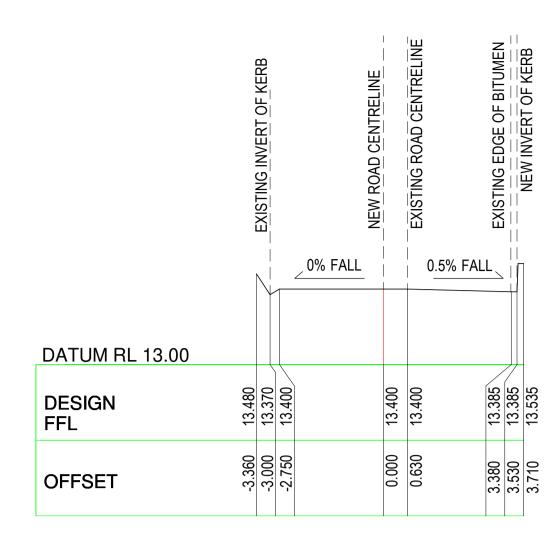
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HORIZONTAL SCALE 1:100
VERTICAL SCALE 1:20



CH100.00

HORIZONTAL SCALE 1:100
VERTICAL SCALE 1:20



CH150.00

HORIZONTAL SCALE 1:100
VERTICAL SCALE 1:20

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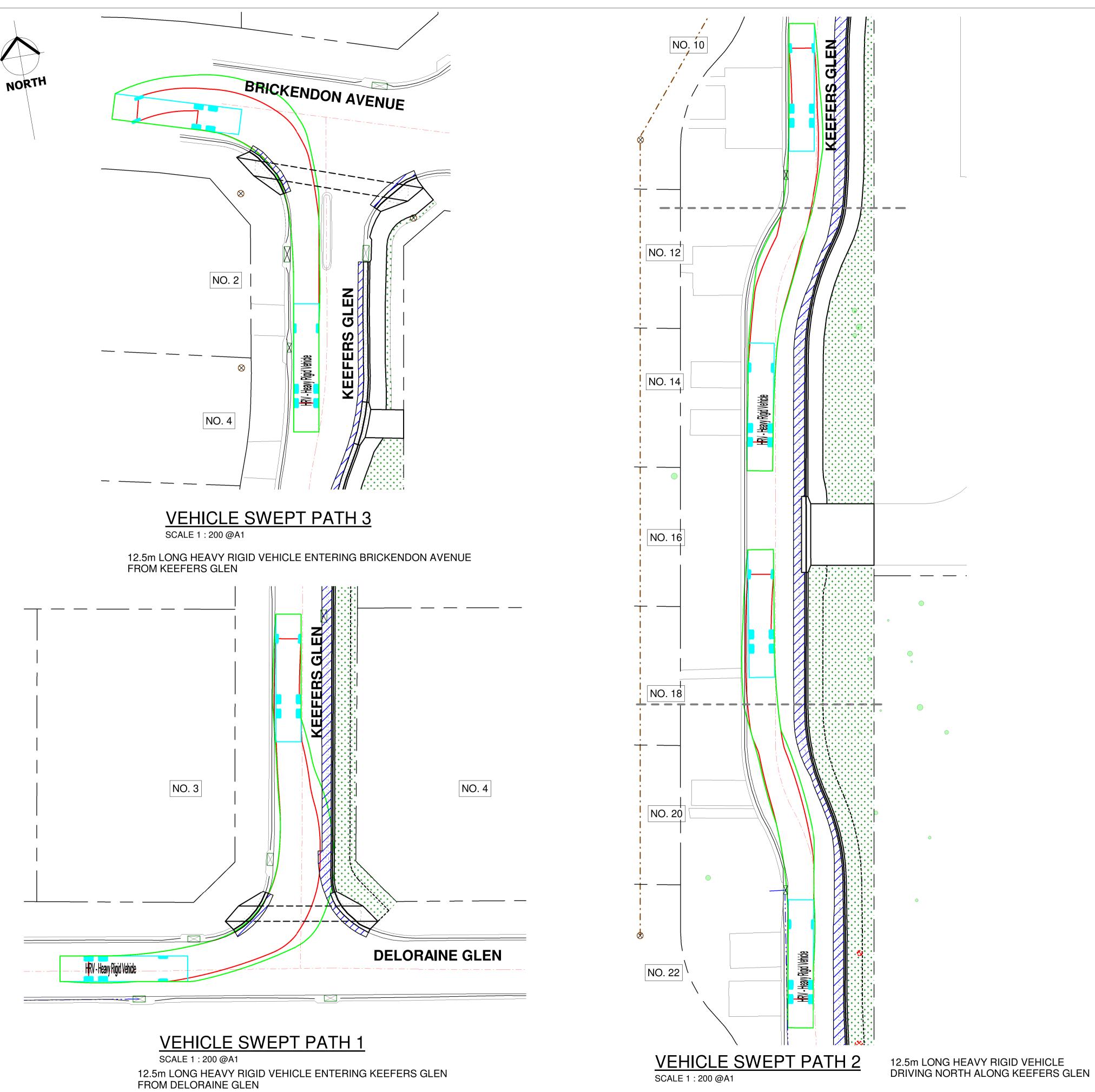




EILEEN O'CONNOR CATHOLIC SCHOOL	
ROAD CROSS SECTIONS	-

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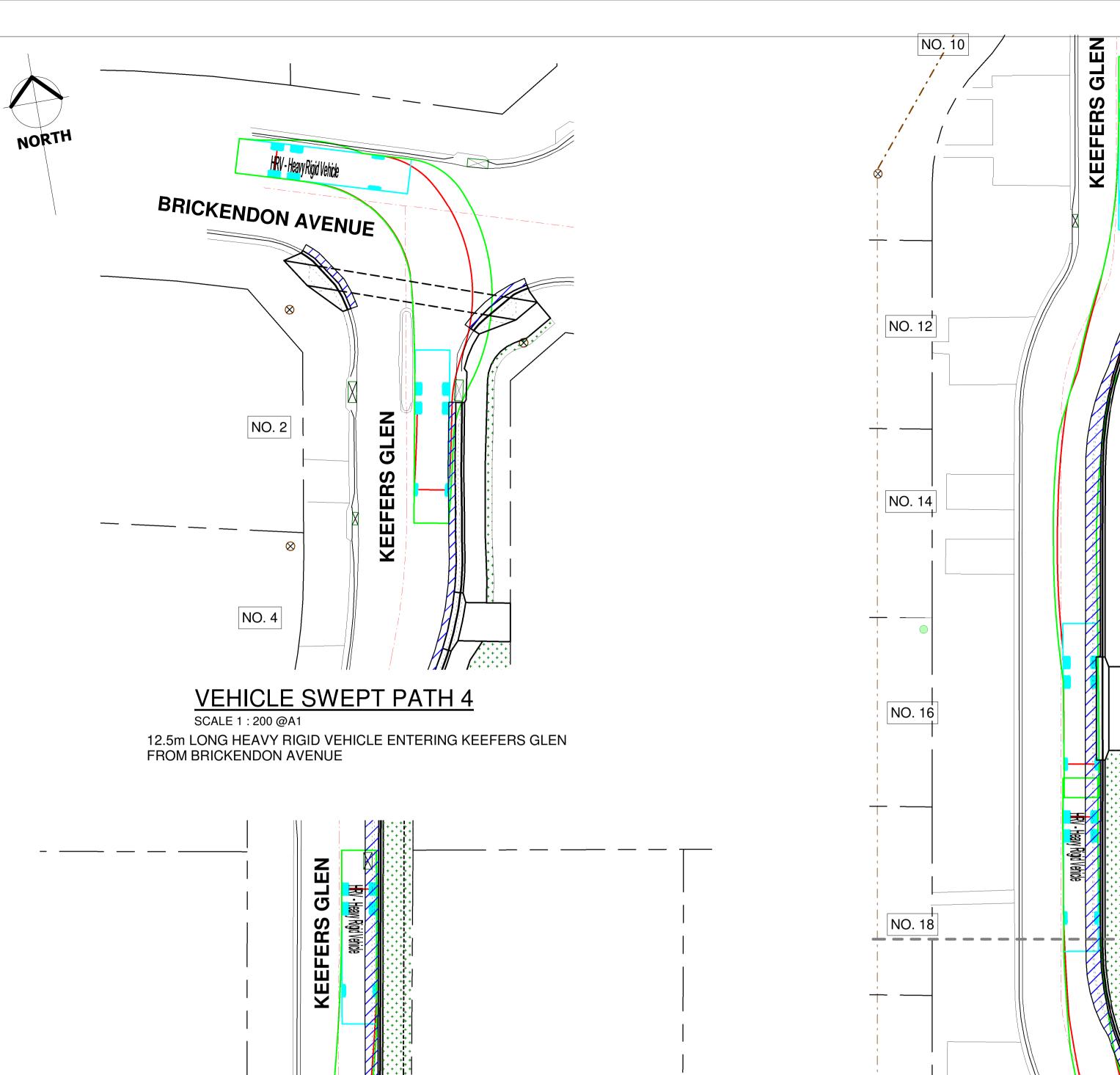


EILEEN O'CONNOR CATHOLIC SCHOOL

VEHICLE SWEPT PATH SHEET 1

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NO. 3 NO. 4 **DELORAINE GLEN**

VEHICLE SWEPT PATH 6

SCALE 1 : 200 @A1

12.5m LONG HEAVY RIGID VEHICLE ENTERING DELORAINE GLEN FROM KEEFERS GLEN

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12.5m LONG HEAVY RIGID VEHICLE DRIVING SOUTH ALONG KEEFERS GLEN

CATHOLIC SCHOOLS

Broken Bay

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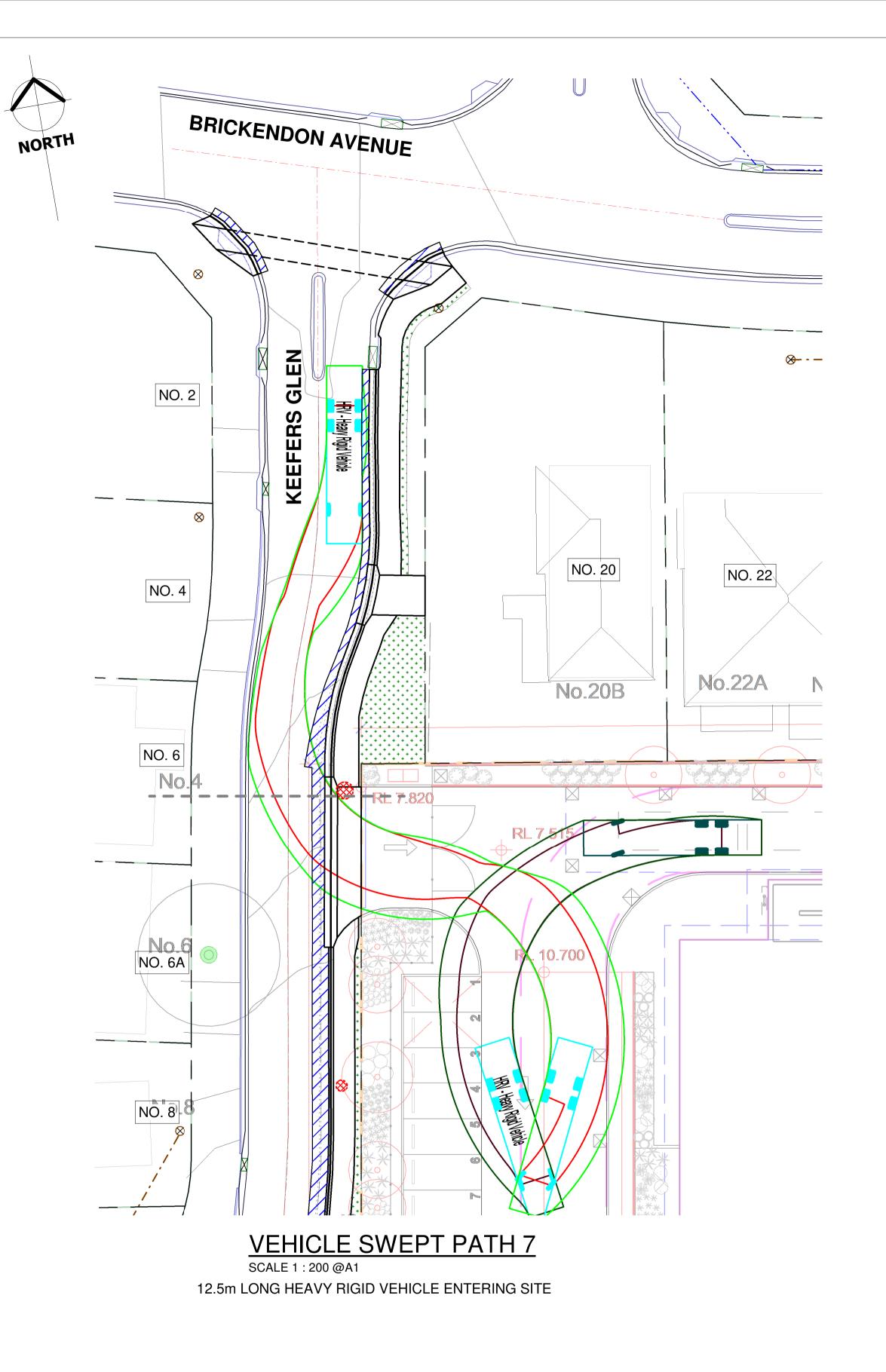


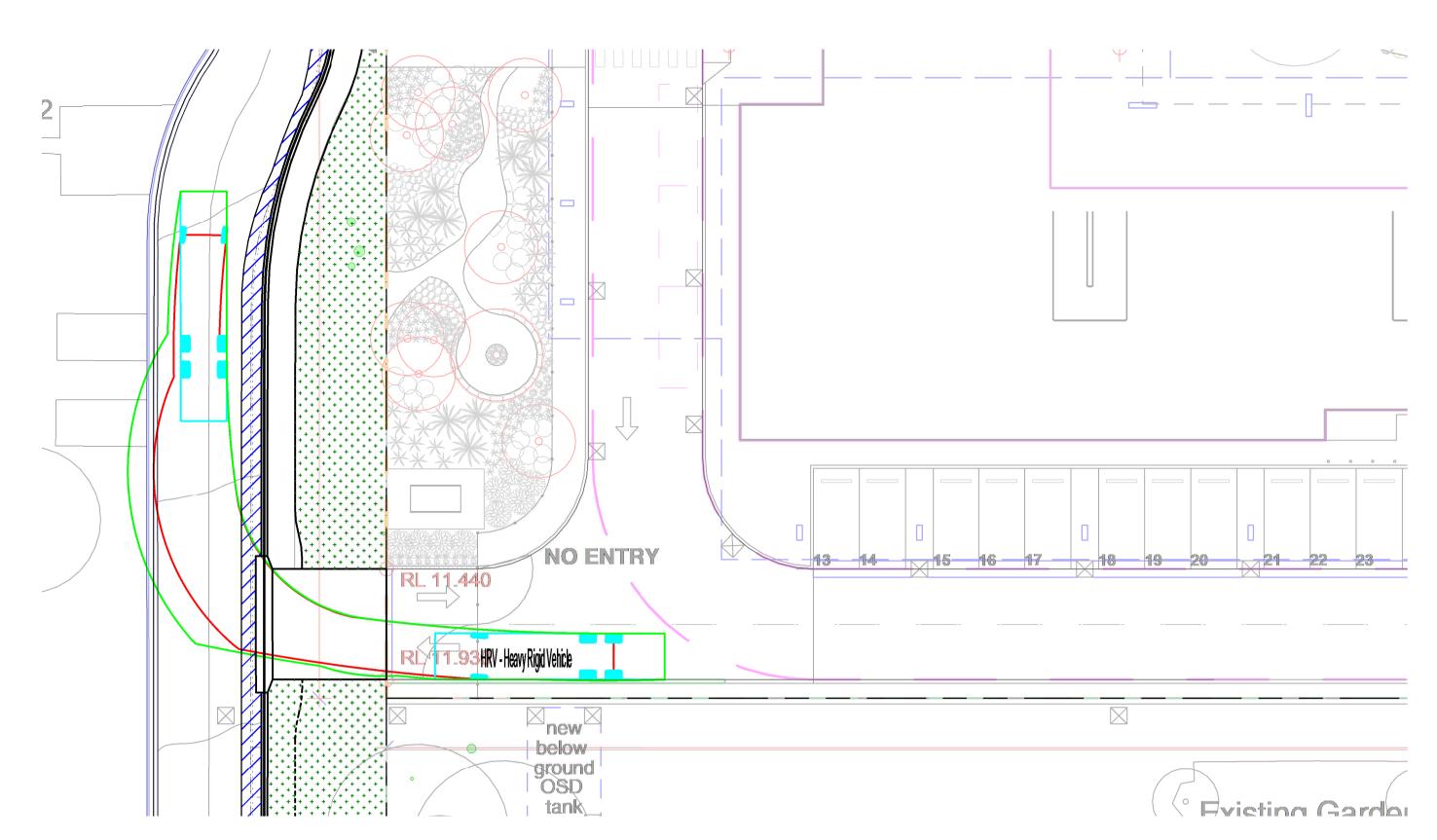
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EILEEN O'CONNOR CATHOLIC SCHOOL
VEHICLE SWEPT PATH SHEET 2

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VEHICLE SWEPT PATH 8

SCALE 1 : 200 @A1

12.5m LONG HEAVY RIGID VEHICLE EXITING SITE

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EILEEN O'CONNOR CATHOLIC SCHOOL

VEHICLE SWEPT PATH SHEET 3

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