

Bank Street Park  
Blackwattle Bay / Tjerruing

SSD-53386706

# Appendix O

## Statement of Heritage Impact (GML)



December 2023

Anzac Bridge—Transport for NSW (Roads and Maritime) S170 register (4305018)

Glebe Island Bridge—State Heritage Register (01914)

# 1A to 19 Bank Street, Pyrmont

Final Statement of Heritage Impact for Bank Street Park SSSA

Prepared by GML Heritage for Infrastructure NSW

November 2023

Issue 3

## **Acknowledgement of Country**

We respect and acknowledge the First Nations of the lands and waterways on which we live and work, their rich cultural heritage and their deep connection to Country, and we acknowledge their Elders past and present. We are committed to truth-telling and to engaging with First Nations to support the protection of their culture and heritage. We strongly advocate social, cultural and political justice and support the Uluru Statement from the Heart.

## **Cultural warning**

Aboriginal and Torres Strait Islander readers are advised that this report may contain images or names of First Nations people who have passed away.

# Report register

The following report register documents the development of this report, in accordance with GML’s Quality Management System.

Job no.	Issue no.	Notes/description	Issue date
23-0126A	1	Draft Report	8 August 2023
23-0126A	2	Final Draft	20 October 2023
23-0126A	3	Final Report	28 November 2023

## Quality management

The report has been reviewed and approved for issue in accordance with the GML quality management policy and procedures.

It aligns with best-practice heritage conservation and management, *The Burra Charter: the Australia ICOMOS Charter for Places of Cultural Significance, 2013* and heritage and environmental legislation and guidelines relevant to the subject place.

## Indigenous cultural and intellectual property

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# 1 Introduction

The purpose of this report is to assess the heritage impacts to support a State Significant Development Application (SSDA) for a new waterfront public park within Blackwattle Bay, to be known as Bank Street Park (SSD-53386706). Bank Street Park is located at 1A–19 Bank Street, Pyrmont on the shoreline of Tjerruing Blackwattle Bay and adjacent areas of Blackwattle Bay.

## 1.1 Blackwattle Bay Precinct

Bank Street Park forms part of the Blackwattle Bay Precinct, which is an area of predominantly government owned land located on the western edge of the Pyrmont Peninsula and adjoining the waters of Blackwattle Bay (Figure 1.1).



Figure 1.1 Blackwattle Bay Precinct. (Source: Infrastructure NSW)

The precinct was rezoned in December 2022 to facilitate a new mixed-use community, providing for around 2,000 new residents and 5,600 new jobs and creating a vibrant 24/7 economy. Updated planning and land use controls were incorporated into the Sydney Local Environmental Plan 2012, along with site specific design guidance in the *Blackwattle Bay Design Guidelines*.

A critical part of the Blackwattle Bay Precinct is the high quality public domain which includes a series of parks and open spaces connected by a foreshore promenade. Bank

Street Park will bring new active and passive recreation uses into a unique park environment, catering for both existing and future communities in the vicinity.

## 1.2 Site description

Bank Street Park is located at 1A–19 Bank Street, Pyrmont NSW within the City of Sydney local government area (LGA) and includes harbour development in Blackwattle Bay. The site area is 1.1 hectares. The relevant lot and deposited plans and the respective ownership for the site are detailed in Table 1.1 and shown in Figure 1.2.

Table 1.1 Summary of land title details of the site.

Street address	Lot and Deposited Plan details	Ownership
1A Bank Street, Pyrmont NSW 2009	Lot 1 DP 85206 Lot 1 DP 188671	Transport for NSW
1–3 Bank Street, Pyrmont NSW 2009	Lots 1-2 DP 1089643 Lot 1 DP 439245	Infrastructure NSW
5 Bank Street, Pyrmont NSW 2009	Lot 20 DP 803159	Transport for NSW
7 Bank Street, Pyrmont NSW 2009	Lot 19 DP 803159	Transport for NSW
9 Bank Street, Pyrmont NSW 2009	Lot 21 DP 803159	Transport for NSW
11 Bank Street, Pyrmont NSW 2009	Lot 22 DP 803159	Transport for NSW
17–19 Bank Street, Pyrmont NSW 2009	Lots 5-6 DP 803160	Transport for NSW
Sydney Harbour	Lot 5 DP 1209992	Roads and Maritime Services (Transport for NSW)
Sydney Harbour	Lot 107 in DP 1076596	Transport for NSW
Part Bank Street road reserve	N/A	City of Sydney Council

Bank Street Park is located on Gadigal Land, one of the twenty-nine clans of the great Eora Nation. It adjoins the foreshores of Glebe to the west and Pyrmont Bridge Road and Wentworth Park to the south.





Figure 1.2 Site context map. The indicative site location is outlined in red. (Source: SIX Maps with Architectus edits [2023])



Figure 1.3 Bank Street Park site location within Blackwattle Bay State Significant Precinct. The indicative site location is outlined in red. (Source: *Blackwattle Bay Design Guidelines* with Architectus edits [2023])

## 1.3 Proposed development

### 1.3.1 Overview

Development consent is being sought for a recreation area for the primary purpose of a *public park*, comprising:

- Site preparation works, including tree removal, earthworks and remediation to facilitate the proposed use;
- Demolition of three existing buildings at 1–3 Bank Street;
- New and adapted facilities for community use, including:
  - New single storey building to accommodate flexible community space, café, and marina office/store facilities, with green roof and photovoltaics;
  - Adaptive reuse of Building D for public amenities, bin and other storage;
  - Boat launching ramp and pontoon for passive watercraft, including dragon boats and kayaks; and
  - Boat storage building with change facilities for dragon boat users with publicly accessible rooftop deck.
- Public domain works, including:
  - ‘Interpretation Garden’ in existing building ‘ruins’ at 1–3 Bank Street;
  - Split level foreshore promenade;
  - Multi-purpose court with edge seating and partial fence;
  - Nature-based inclusive playspace for ages 2–12;
  - Fitness equipment;
  - Public plaza and grassed open space areas;
  - New tree plantings and planter beds; and
  - Public art, wayfinding and interpretative signage, lighting, bike parking and seating.
- Harbour works including:
  - Overwater boardwalk;
  - Land/water interface works, including sandstone terracing into water and support structure, to improve marine habitat;
  - Demolition and construction of a new timber launching ramp for dragon boats;
  - Kayak/passive craft pontoon; and
  - Restoration, repair and alterations to the existing seawall for new stormwater outlets.
- Works to Bank Street Road reserve, including:
  - Road space reallocation to provide separated cycleway;



- Cycleway transition to Bank Street to continue south as part of future works;
- Reinstatement of existing on-street parallel parking;
- Tree planting;
- Accessible parking space; and
- Loading zone adjacent 13 Bank Street.

## 1.4 Planning Secretary's Environmental Assessments Requirements

This report has been prepared in response to the relevant requirements outlined within the Planning Secretary's Environmental Assessments Requirements (SEARs) issued on 11 May 2023 for application SSD-53386706. Table 1.2 addresses the relevant SEARs requirements and provides a project response.

Table 1.2 Secretary's Environmental Assessments Requirements.

SEARs Issue	Assessment requirements	Relevant report section(s)
<b>7. Environmental Heritage</b>	<ul style="list-style-type: none"> <li>• Provide a Statement of Heritage Impact (SOHI) prepared by a suitably qualified heritage consultant in accordance with the guidelines in the NSW Heritage Manual. The SOHI outline the history of the development site and assess its significance using the standard heritage significance criteria, and assess the impacts of the proposal on the significance of the site and on nearby heritage items. The SOHI is to specifically address the following:               <ul style="list-style-type: none"> <li>- The significance of all heritage items (state and local) within the vicinity of the site including built heritage, landscapes and archaeology, with detailed mapping of these items.</li> <li>- Assess the heritage impacts on the proposal on the significance of the site.</li> <li>- Assess the heritage impacts of the proposal on all items of heritage significance in the vicinity including visual impacts.</li> <li>- Formulate recommendations/ measures to minimise and mitigate the impact of the proposal on the heritage significance or cultural heritage</li> </ul> </li> </ul>	<p>This report has been prepared to comply with the requirements of Item 7 of the SEARS.</p> <p>Site history refer Section 2.2.</p> <p>Significance assessment refer to Section 3.</p> <p>Significance of items in the vicinity and mapping refer to Section 2.1.1</p> <p>Heritage impact assessment refer to Section 5.</p> <p>Heritage impact assessment for items in the vicinity refer to 5.2.14.</p> <p>Recommendations refer to Section 6.1.</p> <p>Significance of buildings at 1-3 Bank Street refer to Section 3.2.</p> <p>Details of retained fabric at 1-3 Bank Street refer to Section 4.1.1</p> <p>Adaptive re-use principles refer to Section 3.2.4</p> <p>Justification of changes refer to Section 6.</p>

SEARs Issue	Assessment requirements	Relevant report section(s)
	<p>values of the site and nearby heritage items.</p> <ul style="list-style-type: none"> <li>– Review the heritage significance of the buildings at 1–3 Bank Street and provide design details on the retention of any significant fabric.</li> <li>– Identify adaptive re-use principles for any buildings on the site assessed to be of sufficient cultural significance to be retained.</li> <li>– Justify any changes to the heritage fabric or landscape elements including any options analysis.</li> <li>– Provide detailed photomontages of the proposal and its siting within the historically significant cultural landscape including vantage points from conservation areas.</li> </ul>	<p>Options analysis refer to Section 4.3.2.</p> <p>Photomontages of the proposal refer to Figures Figure 4.7, Figure 4.8, Figure 4.9, Figure 4.10, Figure 4.11, and Figure 4.12 in Section 4.1.1.</p>
<p><b>7. Environmental Heritage (cont.)</b></p>	<ul style="list-style-type: none"> <li>• If the SOHI identifies impact on potential historical and/or maritime archaeology, an historical and/or maritime archaeological assessment should be prepared by a suitably qualified archaeologist in accordance with the guidelines Archaeological Assessment (1996) and Assessing Significance for Historical Archaeological Sites and Relics (2009). This assessment should identify what relics, if any, are likely to be present, assess their significance and consider the impacts from the proposal on this potential archaeological resource. Where harm is likely to occur, it is recommended that the significance of the relics be considered in determining an appropriate mitigation strategy. If harm cannot be avoided in whole or part, an appropriate Research Design and Excavation Methodology should also be prepared to guide any proposed excavations or salvage program.</li> </ul>	<p>Historical archaeology and maritime archaeology for the Bank Street Park SSSA are addressed separately from this SOHI.</p> <p>Refer to the <i>'Historical Archaeological Assessment/ Archaeological Research Design and Excavation Methodology'</i> by GML (2023) and the <i>'Maritime Archaeological Assessment'</i> by Comber Consultants (2023)</p>
<p><b>7. Environmental Heritage (cont.)</b></p>	<ul style="list-style-type: none"> <li>• Address how the development incorporates heritage interpretation that integrates Connection with Country, built heritage and</li> </ul>	<p>The incorporation of heritage interpretation in the development is addressed in the 'Preliminary Heritage</p>

SEARs Issue	Assessment requirements	Relevant report section(s)
	historical archaeology considerations.	Interpretation Framework' prepared by GML for this SSDA. Refer to this document.

## 1.5 Methodology

This report has been prepared with reference to the methodology and terminology of the following documents and guidelines:

- *Guidelines for preparing a statement of heritage impact (2023)*<sup>1</sup>
- *The Australia ICOMOS Burra Charter, 2013 (the Burra Charter)*<sup>2</sup>
- *Assessing heritage significance: guidelines for assessing places and objects against the Heritage Council of NSW criteria (2023)*<sup>3</sup>

## 1.6 Authorship and acknowledgements

This report has been prepared by Andie Coulson (Heritage Consultant), Patrick Atkinson (Senior Heritage Consultant) and Kat McRae (Senior Heritage Consultant). Julian Siu (Principal) and Sophie Jennings (Associate) have provided input and review.

## 1.7 Endnotes

- <sup>1</sup> Department of Planning and Environment 2023, *Guidelines for preparing a statement of heritage impact*, Environment and Heritage, Department and Planning and Environment.
- <sup>2</sup> Australia ICOMOS Inc, *The Burra Charter: the Australia ICOMOS Charter for Places of Cultural Significance 2013*, Australia ICOMOS Inc, Burwood, VIC, 2000.
- <sup>3</sup> Environment and Heritage Group, *Department of Planning and Environment 2023, Assessing heritage significance: guidelines for assessing places and objects against the Heritage Council of NSW criteria*.

## 2 The heritage items

### 2.1 Site description

#### 2.1.1 Heritage items

The site for Bank Street Park contains two listed heritage items, Anzac Bridge and Glebe Island Bridge (Figure 2.1). The listing details of these items is provided in Table 2.1 below.

Anzac Bridge partially sits within the proposed park site. Elements of the bridge that are within the site include the south pylon beside the water, two piers on the southeast boundary and the span overhead. The eastern pylon is located on land reclaimed for the construction of the bridge, which has altered the waterfront boundary of the site.

The eastern abutment of the Glebe Island Bridge is partially located in the northern edge of the site but will not be in Bank Street Park once completed. This abutment was formed during the construction of the bridge from 1899 to 1903 and joins to Bank Street. Part of the original sandstone-faced abutment may join 1A Bank Street; however, due to vegetation growth and modifications over the years, GML was unable to identify the extent of original bridge fabric in the area. A modern gate prevents access to the roadway on the eastern abutment of Glebe Island Bridge from Bank Street.

In addition, 1–3 Bank Street is an unlisted potential heritage item within the Bank Street Park site. The site has been assessed as being of local heritage significance as a remnant maritime industrial complex in Blackwattle Bay. Refer to Section 3.2 for a significance assessment of this site.

Table 2.1 Statutory listings of heritage items within the site.

Listing type	Item name and document details	Listing number
State agency’s S170 heritage and conservation register	<i>Anzac Bridge</i> <i>Transport for NSW (Roads and Maritime) S170 register</i>	4305018
State Heritage Register	<i>Glebe Island Bridge</i>	01914
State Environmental Planning Policy (SEPP)	<i>Glebe Island Bridge, including abutments</i> <i>SEPP (Biodiversity and Conservation) 2021</i>	125

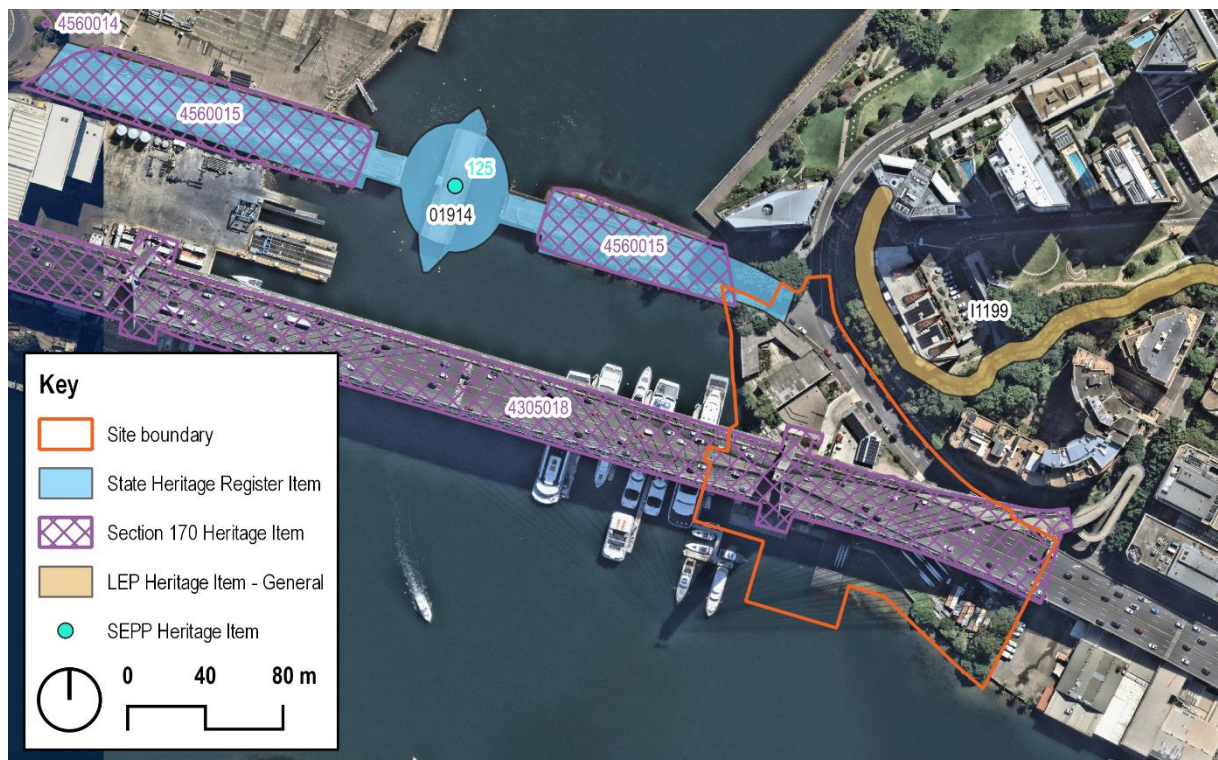


Figure 2.1 Heritage items within the site. (Source: NearMap with a GML overlay)

## Heritage items in the vicinity

There are many heritage items and conservation areas in the vicinity of the site, especially in Pyrmont to the east and Glebe to the southwest. Refer to Figure 2.2 for a map identifying heritage items in the visual curtilage of the site.

A map of the heritage overlay of the surrounding area is also provided in Appendix A.

Heritage items with a direct visual connection to the site are listed in Table 2.2 below. Heritage items with incidental or obscured views of the site are not included in this study, as there will be no anticipated impact to them resulting from the proposal.

Table 2.2 Statutory listings of heritage items in the vicinity of the site.

Listing type	Item name and document details	Listing number
Local heritage conservation area	<i>Glebe Point</i> <i>Sydney Local Environmental Plan 2012</i> <i>(Sydney LEP)</i>	C28
	<i>Glebe Point Road</i> <i>Sydney LEP</i>	C29
	<i>Lyndhurst</i> <i>Sydney LEP</i>	C31

Listing type	Item name and document details	Listing number
	<i>Annandale Heritage Conservation Area (HCA) Inner West LEP</i>	C1
Local heritage item	<i>Escarpment face from former 'Saunders' Quarry' Sydney LEP</i>	I1199
	<i>Blackwattle Bay Park Sydney LEP</i>	I649
	<i>Former incinerator &amp; semi-circular stone structure Sydney LEP</i>	I683
	<i>Pope Paul VI Reserve Sydney LEP</i>	I647
	<i>Kauri Foreshore Hotel Sydney LEP</i>	I657
	<i>Warehouse 'Greens Woolstore' Sydney LEP</i>	I658
	<i>House 'The Retreat' Sydney LEP</i>	I791
	<i>Iron/sandstone palisade fence Inner West LEP</i>	I11
	<i>Street Trees—row of Brush Box Inner West LEP</i>	I10
State agency's S170 heritage and conservation register	<i>White Bay Power Station (outlet) Canal Sydney Ports Corporation S170 register</i>	4560026
SEPP	<i>Glebe Island Wheat Silos A, B, C SEPP (Precincts—Eastern Harbour City) 2021</i>	1A, 1B, 1C
	<i>Railway Truss Bridge, Johnston Street SEPP (Precincts—Eastern Harbour City) 2021</i>	5001333
State Heritage Register	<i>Bellevue</i>	00470
	<i>Pymont and Glebe Railway Tunnels</i>	01225
	<i>Glebe and Wentworth Park Railway Viaducts</i>	01034
	<i>White Bay Power Station</i>	01015
Unlisted	<i>Ferry Road, Glebe Boatsheds: University of Sydney Rowing Club and Glebe Rowing Club</i>	



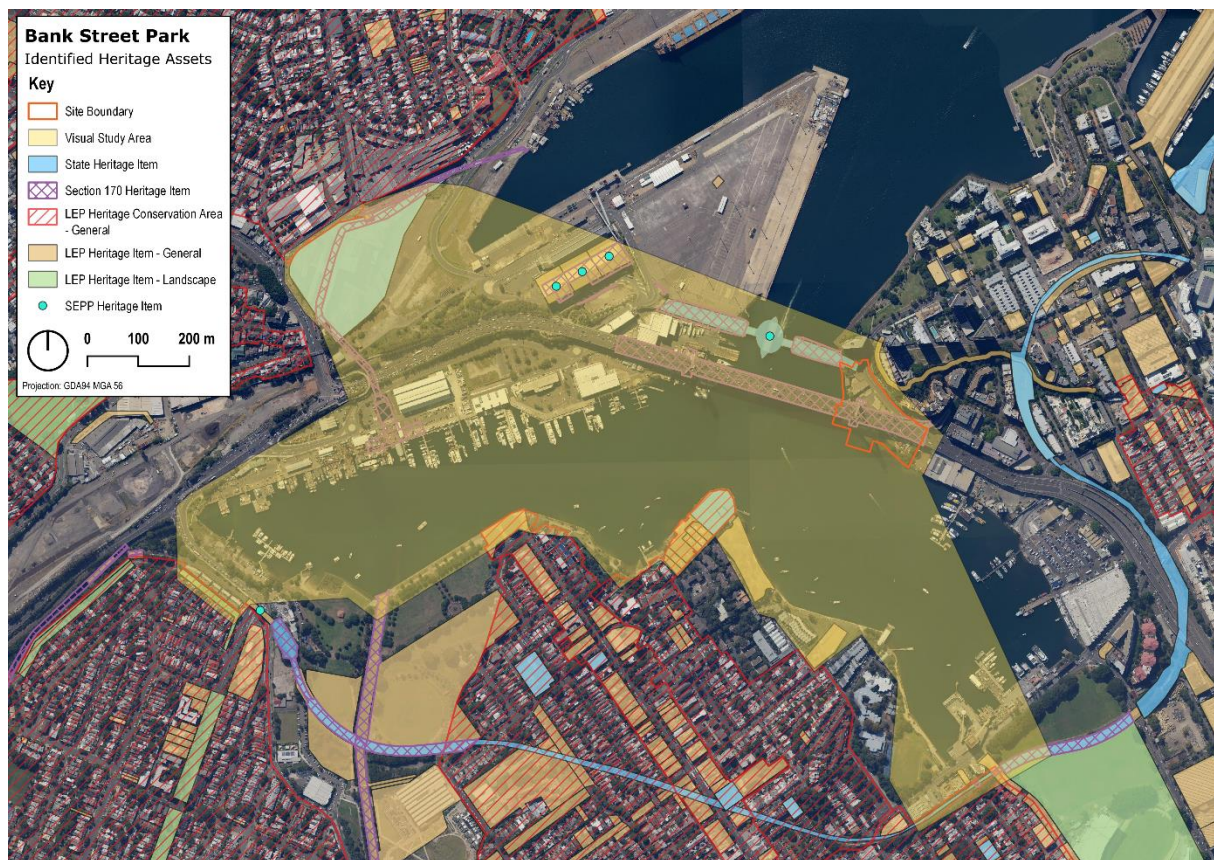


Figure 2.2 Heritage context of the subject site and the visual site. (Source: SIX Maps with GML overlay)

## 2.1.2 Site and its context

1A–19 Bank Street is a waterfront site on the eastern shore of Blackwattle Bay in Pyrmont (refer to Figure 2.3 and Figure 2.4). It is located by the entry to Rozelle Bay and Blackwattle Bay from Johnstons Bay via the narrow channel spanned by Glebe Island Bridge. The site has expansive views of the surrounding bays and to Glebe and Glebe Island on the opposite shores.

The site was formerly used for maritime industrial purposes and contains disused remnant interwar industrial buildings associated with this former use. Most of the site is now cleared of buildings and, since 2017, it has been used for the Blackwattle Bay Marina and for storage. It is also used by the community for dragon boating.

On the opposite side of Bank Street from the site is the heritage listed 'Escarpment face from former quarry (Saunders Quarry)'. The rock face has a strong presence in the site and its steep topography effectively separates the site from the rest of Pyrmont, with the only access to Bank Street being from the northwest and southeast along Bank Street.



To the south of the site are contemporary industrial buildings associated with seafood wholesale. North of the site is the eastern abutment of the Glebe Island Bridge and a contemporary apartment tower beyond that.

The site can be roughly divided into three sections: an overgrown vegetated area at 1A Bank Street, the remnant maritime industrial complex at 1–3 Bank Street, and the Blackwattle Bay Marina and open storage area at 5–19 Bank Street. Refer to Section 2.3 for a detailed description of these sections.

The site has evolved significantly over the past century and includes large areas of reclaimed land. The most significant change at the site in recent decades was the construction of Anzac Bridge and the Western Distributor motorway. This required construction of large support structures for the bridge span over the site which now visually dominate the area. Refer to Section 2.2 for a historical overview of the site.

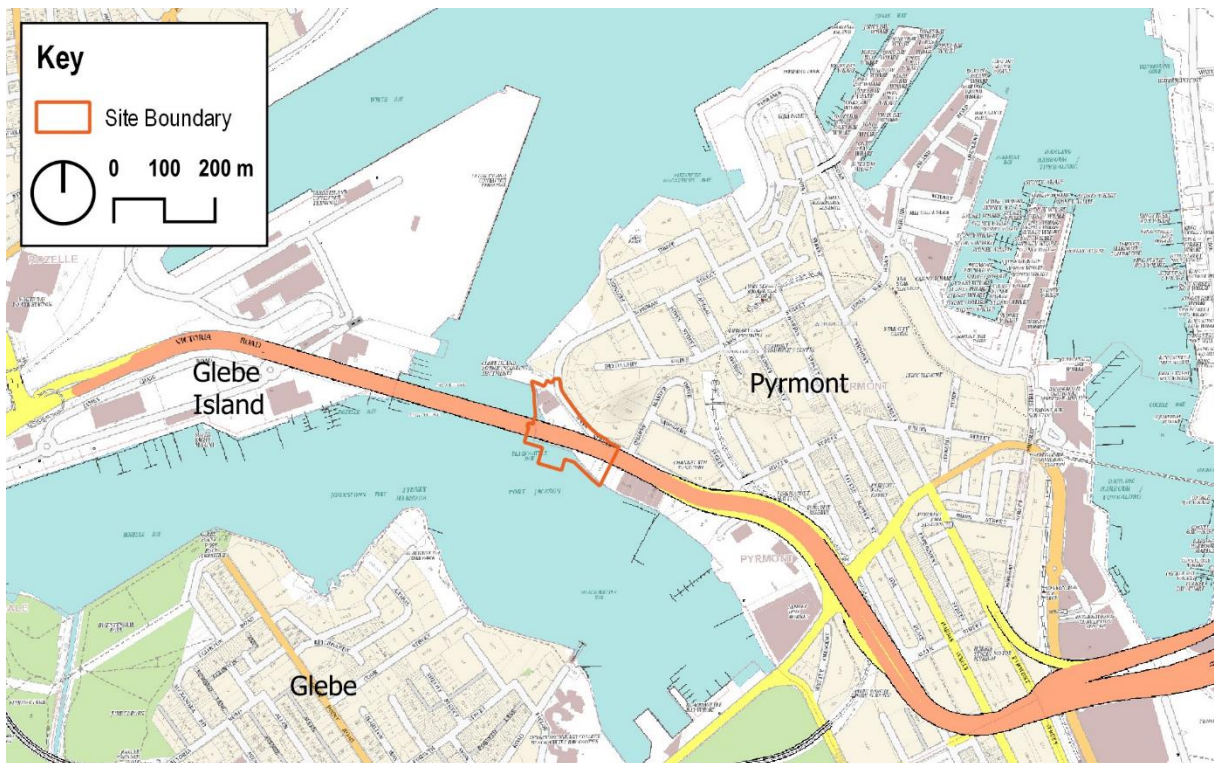


Figure 2.3 Overview of the site location. (Source: SIX Maps with GML overlay 2023)

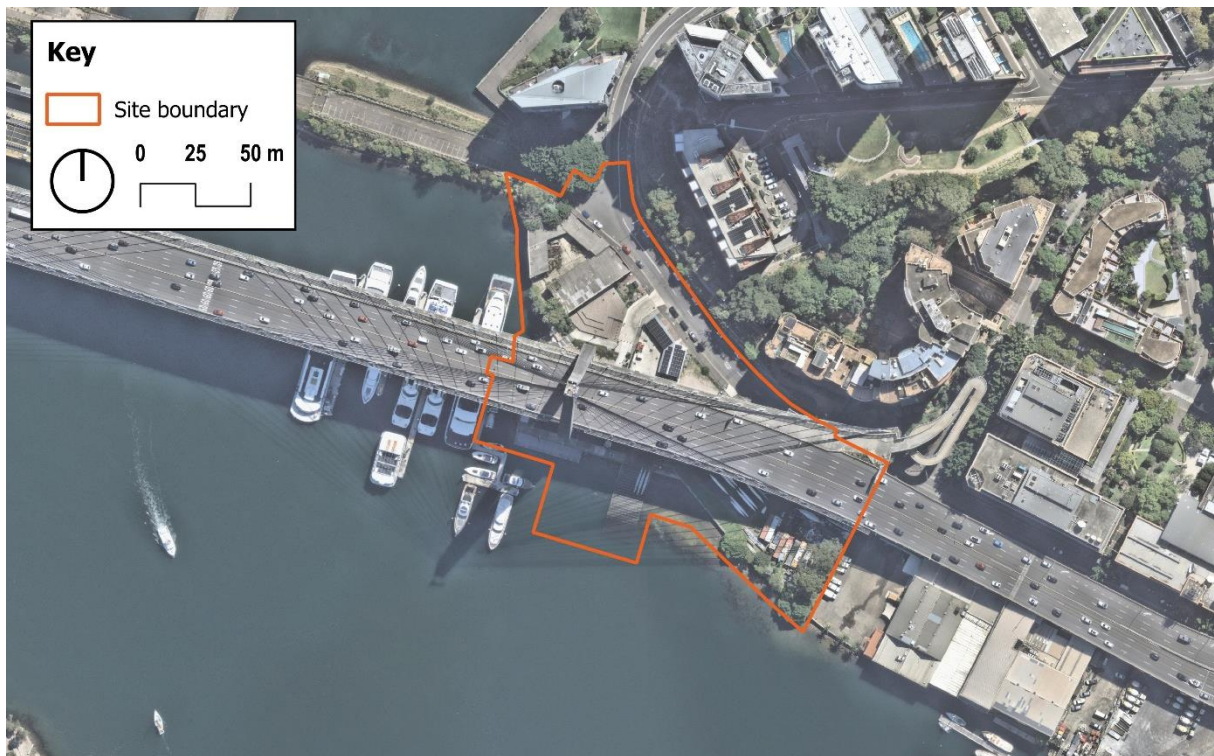


Figure 2.4 Site location on a 2023 aerial image. (Source: Nearmap with GML overlay 2023)

### 2.1.3 Proposed works area

Infrastructure NSW (INSW) has identified the site area for the proposed works (Figure 2.3). This incorporates the proposed harbour development in Blackwattle Bay and ancillary works to Bank Street.

The site area includes most of Bank Street adjacent to the site and part of the eastern abutment of the Glebe Island Bridge.

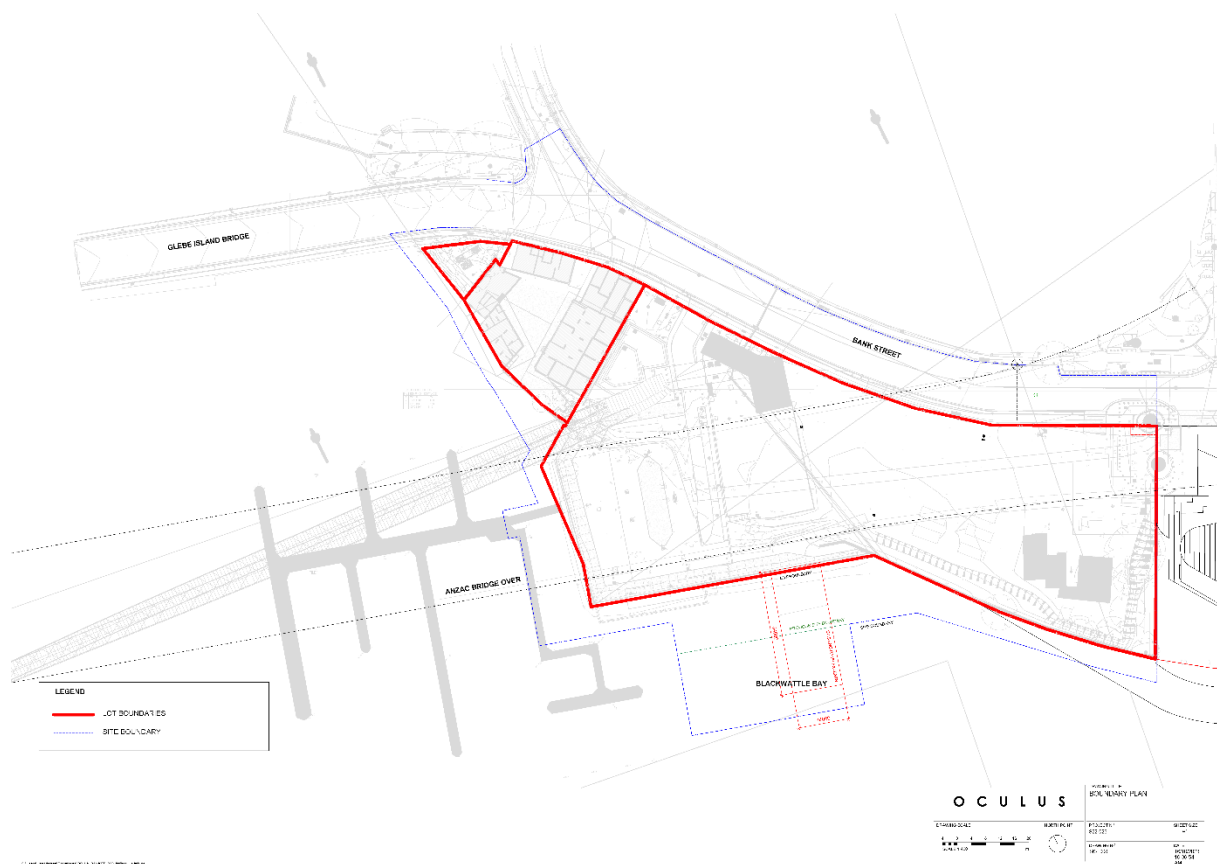


Figure 2.5 The boundary plan for the proposed Bank Street Park. Red indicates the lot boundaries boundary and blue indicates the site boundary. (Source: Oculus 2023)

## 2.2 Site summary history

This section provides an overview of the historical background of the site. It is based upon the thematic history provided in City Plan Heritage’s 2021 ‘European Heritage Assessment & Impact Statement’ for the Blackwattle Bay State Significant Precinct study, supplemented with additional primary and secondary sources research.<sup>1</sup>

### 2.2.1 Aboriginal history

The site is considered a part of the lands of the Gadi people of the Eora Nation. Gadi land extended from the South Head of Sydney Harbour to Alexandra Canal/Cooks River, and inland to Petersham in Sydney’s Inner West.<sup>2</sup> The people of Gadi (grass tree) Country are often referred to as the Gadigal, with -gal being the suffix for ‘man’.<sup>3</sup> The site intersects with the boundary with Wann (or Wanne) Country, which is described as extending from Darling Harbour to Parramatta.<sup>4</sup> However, as per the 2021 Bangawarra report, ‘Connecting with Country Framework for Tjerruing Blackwattle Bay’, a wide array of Aboriginal peoples hold ancestral connections to Sydney, including the Eora, Dharawal,

Dharug, Gundangara, Gai-maragal (often referred to as Gammeragal or Cammeragal) and Guringai peoples.<sup>5</sup> The site falls within the boundaries of the Metropolitan Local Aboriginal Land Council (MLALC).

Early dates for Aboriginal occupation in Sydney stretch back to at least 50,000 BP.<sup>6</sup> Aboriginal culture was dynamic, evidenced by changes to the archaeological record throughout this period. Rapid sea level rise in the post-glacial period (18,000 to 11,000 BP) caused a major reorganisation of social and spatial boundaries as coastal people were forced inland by rising waters.<sup>7</sup> This period also saw the beginning of a more continuous archaeological record of occupation within the Sydney Basin.

The Cumberland Plain had significant sources of raw stone materials: silcrete, IMST and quartz. Aboriginal people used these materials to manufacture stone artefacts. The presence of these materials is linked to their origins (ie source), and changing frequency and preference for use over the Pleistocene and Holocene. The sequence of stone use is referred to as the Eastern Regional Sequence (ERS), with demonstrated changes over the last 10,000-plus years.<sup>8</sup> Subtle differences in stone artefact morphologies across the wider Sydney basin likely reflect long-term territorial areas, while the long-term patterns of the ERS reflect changing preferences for materials, technology, clan and/or language boundaries and, notably, changes to trade networks.<sup>9</sup>

Changes in local technology are apparent through other economies. For instance, approximately 1,000 years ago, hook and line fishing was adopted in southern coastal New South Wales.<sup>10</sup> Fishing with hooks and lines appears to have been a gendered task, with women using this method, while men fished with pronged spears.<sup>11</sup> Women were also responsible for making shell hooks. Fishing in Port Jackson appears to have been concentrated in the lower parts of the estuary (ie east of the Sydney Harbour Bridge).<sup>12</sup>

Colonisation in 1788 significantly disrupted Aboriginal society. Impacts for Aboriginal people included the loss of access to land, food and resources, exacerbated by the introduction of new diseases and violent reprisals. As many as 1,500 Aboriginal people are estimated to have been living in coastal Sydney when the First Fleet arrived in Sydney Cove (Warrane).<sup>13</sup>

Despite dramatic changes to Aboriginal lifeways, Aboriginal people have always maintained their connections with Country and culture, and continue to practise and hand down cultural knowledge today within their communities and more broadly.

Further details on the Aboriginal history and cultural heritage of the site and surrounds are provided in the 'Connecting with Country Framework for Tjerruing Blackwattle Bay'.<sup>14</sup>



## 2.2.2 Early European occupation

Between 1803 and 1818, Surgeon John Harris was granted or acquired most of the land in Ultimo and on the western side of Pyrmont, including the subject site. In total, he acquired 233 acres in the area to form the Ultimo estate, which was overseen by Ultimo House on the hill overlooking Blackwattle Creek, near Parramatta Road. From 1821, the house and estate were leased to various groups, though the Harris family retained ownership after John's death in 1838. The Harris family's land overlooking Blackwattle Bay was steep and rocky owing to the natural topography. This made it unattractive for sale—so much so that it was not included in the first subdivisions of Ultimo Estate in the 1870s—and lent it to industrial or extractive uses.

In 1853, the Harris family leased land on the western side of Pyrmont to Charles Saunders, who operated a series of quarries extracting sandstone. In the late 1850s, the first Glebe Island Bridge was built just north of the subject site, providing easy access to Sydney's growing transport network. Jetties and wharves also appeared on the eastern side of Blackwattle Bay in the mid-1860s, related to the nascent industry developing in the area.

Further improvements to Blackwattle Bay in the 1870s and 1880s spurred the growth of industry and shipping in the bay. Between 1873 and 1880, Blackwattle Swamp was reclaimed. The Harris family also initiated land reclamation on the eastern side of Blackwattle Bay to create suitable land for development, spanning from the present Sydney Fish Market to the subject site. The Harris family's reclamation defines the current extent of most of the land at the site and it is possible that the sandstone seawalls date from around this period.

In 1895, the Harris family subdivided the land along the eastern side of Blackwattle Bay north of Miller Street into regular allotments, which included the subject site. Lots 1 and 2 were the site of 1–3 Bank Street and lots 3 to 10 were 5 Bank Street (Figure 1). This land was mostly leased to various maritime contractors and timber merchants, who had come to dominate the harbourfront in the surrounding area as importation of foreign softwoods increased. On the subject site was the Allen Taylor & Co timber yards.

In 1899–1903, the original Glebe Island Bridge was replaced with the swing-span bridge north of the site. This required resumption of some of the 1–3 Bank Street land to join to Bank Street (then Abattoir Road), which had formed below the steep sandstone cliff of Saunders Quarry. Following the invention of steam powered quarrying machines it is reputed that the area around the subject site was quarried to below road level and then backfilled.<sup>15</sup>

Images from the construction of the Glebe Island Bridge show several large timber warehouses and a central jetty on the site (Figure 2.7).

### 2.2.3 Early twentieth-century developments

The land of the subject site continued to be leased by timber yards and other minor shipping and industrial ventures throughout the early twentieth century. In 1900, Augeson & Co secured the lease of the northern edge of the subject site at 1–3 Bank Street, where they developed a timber yard and wharf.<sup>16</sup> In 1910, they were replaced by Cowlishaws timber yard and wharf. Uses in the 5 Bank Street site around the time included the Allen Taylor and Co timber yard, Wallis Brothers sawmills, McEnnally Bros and Co lightermen and tugboats, and Puech’s skin store.<sup>17</sup>

An aerial photograph from c1927 shows the site was densely packed with the buildings of these businesses, with various floating pontoons and a large central jetty for ship access and storage (Figure 2.8).

In 1928, Albert Octavius Harris placed lots 1–10 up for sale as one site. On the poster for the sale were details of the current lessees and the end dates of their leases, as well as the footprints of the various buildings on the site. From north to south, this was the Pymont Timber Storage Co, Allen Taylor and Co, Wallis Brothers, and Armand Guillhou (Figure 2.9). A large jetty on piles was shown in front of the Allen Taylor and Co lease, with a smaller punt and crane in front of the Wallis Brothers land.

Albert’s sale does not appear to have been successful and was potentially stymied by the resumption of lots 9–10 by the Municipal Council of Sydney in 1929. However, by 1932 a sale had been made to Charles Caminiti, who had purchased lots 1–4 in two separate transactions.<sup>18</sup>

Charles Caminiti (originally Carlo Caminiti and later Charles Cam) was a fish merchant who was born in 1882 or 1884 at the New Italy settlement near Richmond River in northern New South Wales to Italian immigrant parents Rocco and Catherina.<sup>19</sup> At age eleven, Charles was working on his father’s fishing boat, later operating a boat and crew himself.<sup>20</sup> By 1913, he was operating a fish shop in Drummoyne, or possibly Balmain.<sup>21</sup>

Charles proved an astute businessman, purchasing shares in the Redfern fish market company, which was later absorbed by the Municipal Fish Markets in Haymarket. In 1923, he expanded into trawling with the purchase of the steam trawler *Goorangai* from the NSW Government. Over the next decade, his trawling business expanded to eleven steam trawlers by the time he purchased the land at Blackwattle Bay. By this time, his sons had become involved in the business, which was renamed Cam & Sons.<sup>22</sup>

Charles and Cam & Sons redeveloped the land soon after purchase, demolishing the timber stores and buildings to construct a new wharf, warehouse, workshops and offices on the 1–3 Bank Street site. This became the base of operations for their steam trawler business, which was reportedly the largest in the southern hemisphere.<sup>23</sup> Fish caught by their trawlers were loaded onto shore via their wharf to be supplied to fishmongers and

the Municipal Fish Market, with Cam & Sons being one of the biggest suppliers in Sydney. In 1936 Cam & Sons began operating a coal mine to supply their ships, selling surplus coal to the Railway Department.<sup>24</sup>

In 1938, they purchased lots 5–8 of the 1928 subdivision sale, securing ownership of most of the land of the subject site.<sup>25</sup> Aerials from 1943 show the large Allen Taylor and Co wharf was retained and the land directly north redeveloped with various buildings, including those at 1–3 Bank Street (Figure 2.10).

In 1940, eight Cam & Sons steam trawlers were requisitioned by the Royal Australian Navy for use in minesweeping operations during World War II, the majority of the company’s fleet. The Cam & Sons boats commissioned by the Navy were the *Alfie Cam*, *Beryl*, *Coonambee*, *Goorangai*, *Mary Cam*, *Olive Cam*, *Patricia Cam* and *Samuel Benbow*.

Two of the Cam & Sons boats were destroyed in the war. The HMAS *Goorangai* sank in November 1940 after it was hit by the MV *Duntroon*, becoming the first vessel lost by the Navy in World War II. Later, the HMAS *Patricia Cam* was sunk Japanese Aircraft in 1943 off the coast of the Northern Territory.<sup>26</sup>

After the war, most of the vessels were returned to Cam & Sons or repurchased by them to resume regular operations.<sup>27</sup> In 1944–1945, Cam & Sons constructed an addition for ice manufacturing and storage at the site with a retaining wall.<sup>28</sup> Subsequent works included the addition of amenities in 1947.<sup>29</sup>

After Charles Cam died in 1947, his sons continued to operate the steam trawler business. By 1955 they wound up this operation, focusing instead on their coal mines and other interests.<sup>30</sup> Cam & Sons had also earlier sold a portion their lands (lots 5–8) to the Colonial Sugar Refining Co Ltd (CSR) in 1949, holding on to the northern area.<sup>31</sup>

In 1955, Cam & Sons sold the remainder of their land in the area. 1–3 Bank Street was sold to Keene & Co, a poultry business who were branching out into the fish trade.<sup>32</sup> Purchasing the site gave them ownership of a wharf, freezer, ice-making plant and offices to use in extension to their existing premises on Broadway.<sup>33</sup>

The land directly south of 1–3 Bank Street, where the Allen Taylor & Co wharf was located, was sold the same year to the Potato Marketing Board of Tasmania. In 1957, this was purchased by CSR to expand their foothold on the eastern shore of Blackwattle Bay.<sup>34</sup>

## 2.2.4 Subsequent uses

Keene & Co undertook some modifications to the building at 1–3 Bank Street during their tenure at the site. This included construction of an awning between street-facing buildings in the 1960s (Figure 2.11).



In 1973, the site of 1–3 Bank Street was sold to Fork Lift Hire Co. An image taken in 1976 from Glebe Island Bridge shows the company modified one of the four buildings at 1–3 Bank Street, opening it to Blackwattle Bay (Figure 2.12). An awning structure joining this building to the workshop building was also removed. In 1978–1979, the company applied to City of Sydney Council to register the site as a factory, likely for a light mechanical workshop and depot for the maintenance of their forklifts, replacing a premises at 56–58 Bay Street which was subject to an earlier application.<sup>35</sup>

In early 1980 the site was sold to Hendrikus Holster and Jakob Meyer.<sup>36</sup> Holster and Meyer operated Hansom Cabs of Sydney, a horse-drawn taxicab business that was a nostalgic throwback to Sydney’s horse-drawn cabs of the late nineteenth and early twentieth centuries. In 1980, their application to construct 20 stables at the site was approved. The stables were only partially built by 1981 and were in use, resulting in notices to comply from Council. These structures were demolished in 1982 after the site was abandoned by the business.<sup>37</sup>

From the mid-1980s, part of 1–3 Bank Street was used as a woodworking studio by Red Cedar Workshops, which appears to have operated until the 2000s. The warehouse was more generally used by artists as a studio, with wiring for glass and pottery kilns. The buildings at 1–3 Bank Street were again modified during this period. Modifications included enclosing the building that had been opened by the Fork Lift Hire Co, removing a tower on the warehouse building (visible in Figure 2.11), and modifying the workshop building by enlarging openings, installing shutters and refitting the interior.

In the late 1980s, the inadequacy of Glebe Island Bridge prompted the New South Wales Government to begin construction of a new suspension bridge. Construction of the bridge required the government to resume the land owned by CSR, now 5 Bank Street, as the span would cover most of the site.<sup>38</sup> This began the government’s ownership of the site, which was cleared and used for storage.

Construction of the eastern tower also required reclamation of part of Blackwattle Bay, resulting in the most substantial change to the shoreline since the 1880s. The bridge was completed in 1995 and was later renamed Anzac Bridge in 1998.

In 2004–2006, a masterplan for Bank Street was prepared to guide future redevelopment of the eastern bank of Blackwattle Bay, north of the Sydney Fish Market. At the time, 1–3 Bank Street was owned and occupied as a residence by Ms Ann Forrester, who had been living there for many years in the former office building facing Bank Street.

In 2006, the Blackwattle Bay Dragon Boat Club began to use 5 Bank Street for the storage and launching of their boats.

In 2020, the Blackwattle Bay Marina was constructed, which involved the demolition of the Cam & Sons wharf at 1–3 Bank Street. The wharf had been left derelict since at least the early 1990s, after having been reduced in size in the 1970s.<sup>39</sup>

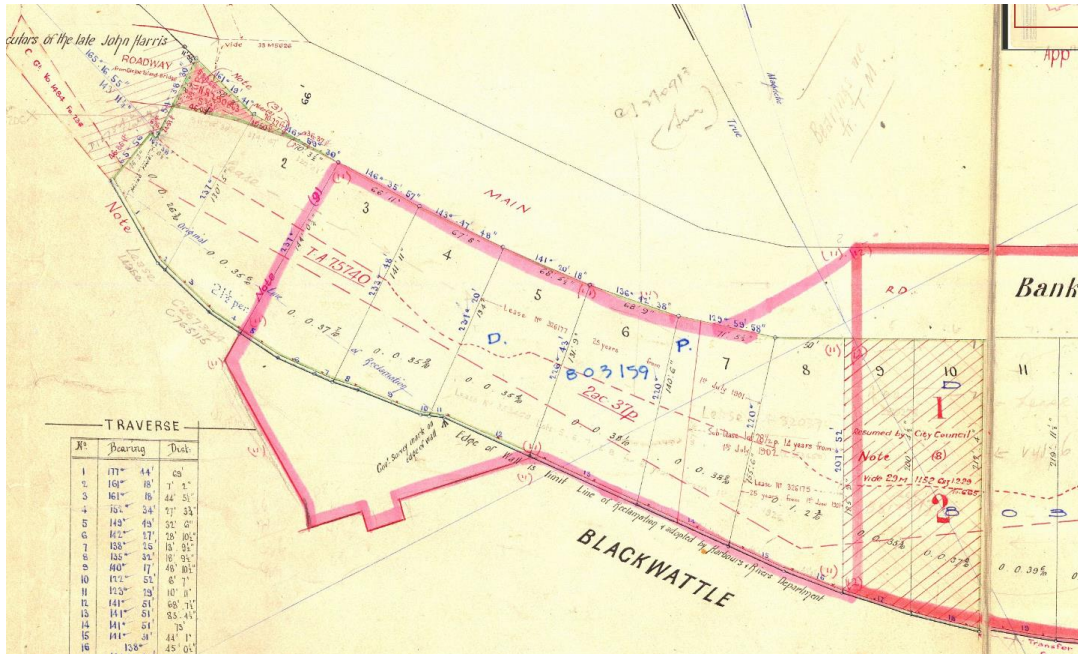


Figure 2.6 Detail of Harris’s 1895 subdivision, showing lots 1–10. (Source: NSW Lands Registry Services, Historical Lands Records Viewer [DP3176])



Figure 2.7 Detail of an image of the construction of the Glebe Island Bridge turntable c1899–1902, showing the Augeson & Co timber yard buildings and wharf in the background. (Source: State Records of NSW [AF00257768])



Figure 2.8 Detail of an aerial from Glebe Island Bridge to Pyrmont c1927. (Source: State Library of NSW [FL383183])

**BLACKWATTLE BAY** *Pyrmont*  
Torrens Title

Within 5 minutes of the City In the Hub of the Timber Trade.  
An Unsurpassed Industrial Water Frontage.

Total Frontage to Road 621' 2 1/2"  
Total Edge to Harbour 728' 0 3/4"

The buildings are charted approximately and are the property of tenants

**FOR AUCTION SALE**  
in the Rooms, Ocean House.  
36 Martin Place.  
**WED. APR. 18** 1928  
at 11 a.m.

**HARDIE & GORMAN PTY LTD.**  
**E. H. MAAS & SON**  
Auctioneers in Conjunction  
Surreyors Ray & Russell  
4 Castlereagh St

**TERMS**  
\$5000 Deposit  
\$25000 on Completion  
Balance on 1<sup>st</sup> & 2<sup>nd</sup>  
Mortgages for 5 years  
Interest at 7% & 6%

Leased to Pyrmont Timber Storage Lease Expires July 1931  
Leased to Allen Taylor & Co Lease Expires June 1932  
Leased to Wallis Bros Lease Expires June 1932  
Leased to Armond Gunton Lease Expires June 1932

Blackwattle Bay  
Abattoir  
Road  
Jetty on Piles  
LOCAL SKETCH

All dimensions are subject to Deposited Plan.  
Vendors Solicitors Sullivan Bros. Barrack House Barrack St

Figure 2.9 Harris's 1928 Blackwattle Bay subdivision sale, showing the businesses leasing the land at the time of sale. (Source: SLNSW [c041010038])





Figure 2.10 1943 aerial of the site, showing the Cam & Sons buildings at 1–3 Bank Street.  
(Source: NSW Spatial Services)



Figure 2.11 Oblique aerial of the site in 1963 by Clive Kane, showing 1–3 Bank Street intact and 5 Bank Street used for storage. (Source: SLNSW [FL14291296])

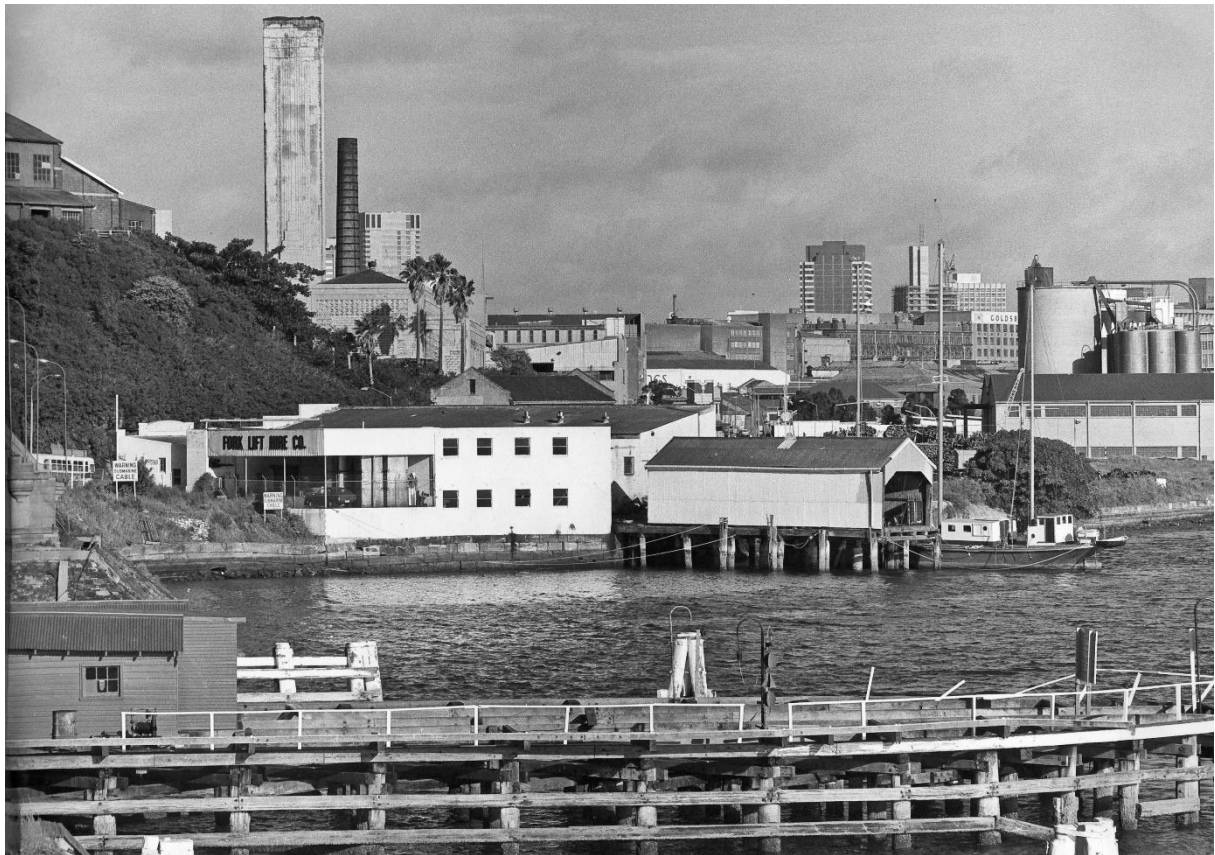


Figure 2.12 Subject site in 1976, showing the Fork Lift Hire Co sign on one of the buildings.  
(Source: City of Sydney Archives [A-0027616])



## 2.3 Physical Analysis

The site can be divided into three sections:

- 1A Bank Street
- 1–3 Bank Street
- 5-19 Bank Street

These are described separately in the section below.

### 2.3.1 1A Bank Street

1A Bank Street is on a triangular parcel of land between the eastern abutment of Glebe Island Bridge and buildings C and D of 1–3 Bank Street. It is currently overgrown with vegetation and contains several service and telecommunications pits associated with submarine Telstra and Ausgrid cabling.

1A Bank Street joins to the concrete retaining wall of the southern footpath of Glebe Island Bridge. Original sandstone blockwork of the eastern abutment of the bridge is potentially present in 1A Bank Street where it joins to the bridge. Due to vegetation growth, GML was unable to verify this. This should be confirmed prior to siteworks.



Figure 2.13 1A Bank Street, looking southeast.



Figure 2.14 1A Bank Street, looking west.



Figure 2.15 Junction between the sandstone-faced eastern abutment of Glebe Island Bridge and 1A Bank Street.



Figure 2.16 1A Bank Street western elevation, with buildings B and C of 1–3 Bank Street in the background.

### 2.3.2 1–3 Bank Street

1–3 Bank Street is a building group comprising four brick buildings on an irregular shaped site in the northern part of the proposed Bank Street Park. The site is bordered by 1A Bank Street to the north, Bank Street to the east, the Blackwattle Bay Marina site to the south and Blackwattle Bay to the west. Refer to Figure 2.17 for a breakdown of the buildings on the site.

The buildings are located on the perimeter of the site and form a central courtyard which is paved in concrete. Building A to the south and building D to the east are freestanding, while buildings B and C join to form an L-shaped structure on the north and west boundaries of the site. All buildings were initially built in the 1930s and demonstrate various levels of modification. Primary access to the site is via a gateway from Bank Street between buildings A and D. The awning covering the gateway was installed in the 1960s.

Buildings A and D face Bank Street to the east and their street façades demonstrate interwar period architectural design commensurate with their construction in the 1930s. The remaining external elevations of the building group are utilitarian, with little architectural ornamentation.

The following sections contain a more detailed description of each building, including an analysis of the interior fabric.



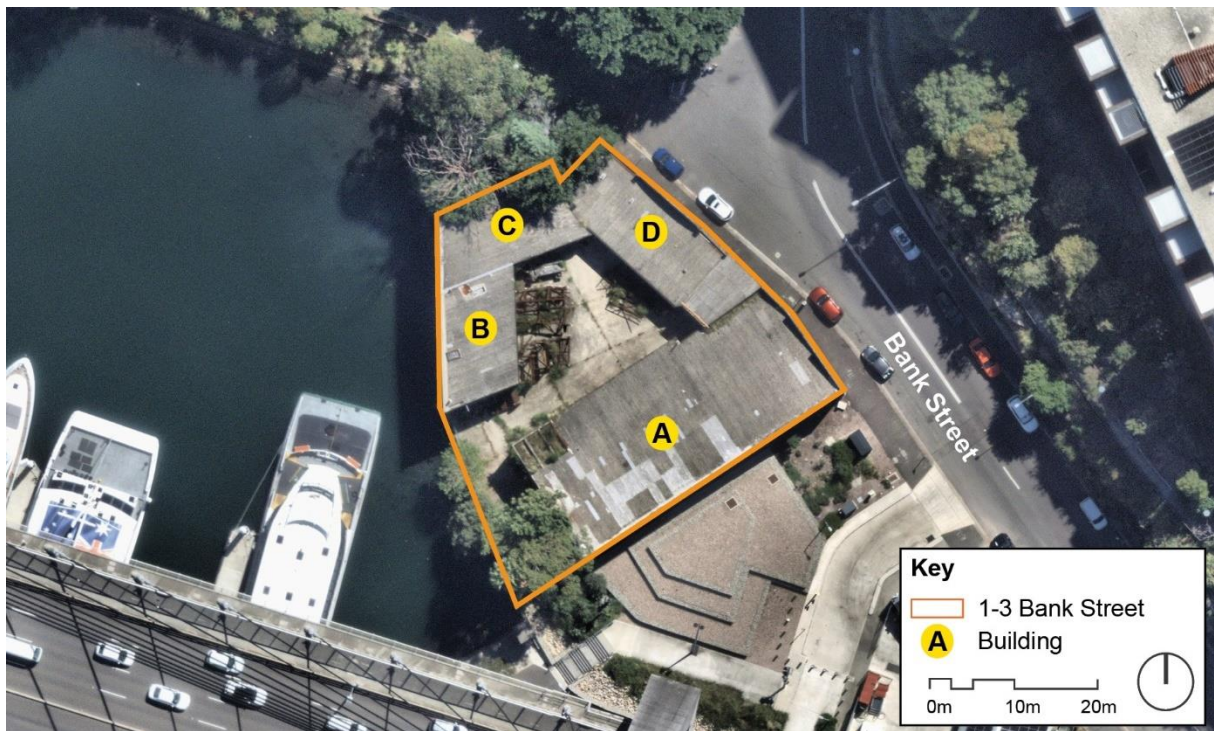


Figure 2.17 An overview of 1–3 Bank Street, showing the buildings in the site. (Source: Nearmap with GML overlay)



Figure 2.18 Survey plan of the built form at 1–3 Bank Street. (Source: Craig & Rhodes 2023)



Figure 2.19 Survey plan of the interiors of the buildings at 1–3 Bank Street. (Source: Craig & Rhodes 2023)

## Building A

Building A is a large, rectangular freestanding brick warehouse building constructed in c1932. It is one of the more intact buildings on the site and covers most of the southern boundary. The building is of double height to accommodate an internal mezzanine floor and has a skillion roof concealed by a parapet. In general, building A is in poor condition.

The eastern street elevation of the building shows elements of the Inter-War Functionalist style, including its stepped parapet, horizontal brick banding and steel-framed windows. This elevation also has a former doorway beside the covered gateway, now infilled with brick. This elevation is primarily original fabric with evidence of repairs to the brickwork.

The north elevation, facing the internal courtyard, is dominated by large modern roller doors which provided access to the warehouse storage. The façade steps back beneath the covered gateway to a rendered brick wall beneath a roof overhang. This has a series of early timber casement windows and an early timber sliding door. Closer to the water is a section of unrendered brick wall with a thin roller door, a rendered architrave and an early steel-framed window below where a former tower was located.

The west elevation of the building faces Blackwattle Bay. It is an unrendered brick façade with a large central opening, now boarded up, and a door to a small brick verandah.

The south elevation facing the Blackwattle Bay Marina is an unrendered double-height brick wall which continues to form a parapet. This elevation contains two former doorways infilled with brick. The wall dominates the surrounding area and blocks visibility into the rest of the site.

A structural condition assessment prepared by Northrop in December 2022 concluded that building A had structural issues and provided the following recommendations:

- All footings should be underpinned to the depth of sandstone and cracks repaired after the foundation is strengthened.
- The west façade wall should be demolished and rebuilt in the area which has failed around the large opening.
- All timber roof framing, sheeting and gutters should be demolished and replaced.<sup>40</sup>

## **Building B**

Building B is a c1932 two-storey brick building on the western boundary of the site, built directly over the sandstone seawall. The building is set below the height of the internal courtyard, resulting in the lower floor being substantially lower. It has several double-wide goods doors which indicate it was used as a goods store. The north elevation of building B joins to building C to form an L-shaped structure and is not visible.

The east elevation is rendered and painted and faces the internal courtyard. This elevation is symmetrical with two small eight-pane timber sash windows set around an early double-wide central timber goods door with sidelights.

The south elevation is unrendered, unpainted brick. On the lower level is a modern metal roller door with a timber lintel that reads 'Workshop', above which is a timber-framed rack associated with the goods door on the first floor. The goods door is a replacement double timber and glass door with a timber-beam goods hoist above. The opening has a concrete rendered lintel. To the side is an early single, double-hung timber sash window.

The west elevation facing the water is rendered with regularly spaced timber-framed windows with shutters. Historical photographic evidence shows that these openings were enlarged and the current windows and shutters installed before the 1990s.

A structural condition assessment prepared by Northrop in December 2022 concluded Building B's foundations were adequate. It made the following recommendations:

- Cracking was a result of subsidence in the building C footing/slab and could be resolved if the issue was corrected, with cracks stitched and bricks locally replaced.
- The RL2.65 elevation of the ground level workshop makes the room unusable for use under INSW flood planning and sea level rise guidelines.

- All structural and non-structural timber should be demolished and replaced, including the roof and timber floors.
- Windows should be repaired and doors replaced.<sup>41</sup>

## **Building C**

Building C is a single-storey rendered brick building on the northern boundary of the site. It was largely reconstructed in the 1990s when the north and west elevations were rebuilt. The east and south elevations are brick and are likely the only remaining early fabric. Building C includes a verandah overlooking Blackwattle Bay on its west elevation.

Northop's recommendations for Building C were extensive owing to its poor condition and included:

- All footings should be underpinned to the depth of sandstone.
- Existing slab-on-grade should be demolished and new supporting fill and concrete slab-on-grade installed.
- Cracks in walls can be stitched and bricks locally replaced.
- Complete demolition of all structural and non-structural timber within the building.
- All doors should be replaced.<sup>42</sup>

## **Building D**

Building D is a single-storey freestanding brick building constructed in c1932 on the eastern boundary of the site. The exterior of the building is the most intact of all buildings at 1–3 Bank Street and includes original timber-framed windows and doors to all elevations, albeit in poor condition. Building D was likely the site's office building, as it does not feature roller doors or goods doors that would indicate it was used for warehousing or storage.

The eastern elevation to Bank Street has a decorative façade which shows elements of the Inter-War Stripped Classical style. It has an unrendered face-brick façade that is divided into three bays by engaged brick piers. A parapet with a central curved cement rendered section covers the building's skillion roof. Below the parapet are concrete rendered panels. A band of concrete render runs the length of the façade at the height of the lintels of the regularly spaced double-hung timber sash windows. The brickwork on this elevation shows evidence of multiple repairs using dissimilar bricks.

The south elevation of the building is entirely below the covered gateway. It is a painted brick façade with a modern timber door and early double-hung timber sash window.

The west elevation faces the internal courtyard and is unrendered brick with a series of original double-hung timber sash windows and 15-pane timber and glass doors. Sills and lintels of the openings consist of a band of red rowlock bricks. The eaves of the skillion roof have a generous overhang on this elevation, with a timber fascia and fibro soffits.

The north elevation faces to 1A Bank Street and is painted with irregularly spaced thin double-hung timber sash windows. These join to a room which was inaccessible at the time of inspection, likely the original or early toilets.

Northrop’s structural condition assessment concluded that building D’s foundations were adequate, like those of building B. It made the following recommendations:

- The timber floor should be completely replaced, including the support framing.
- Wall cracks should be stitched.
- Windows and doors require repairs.
- The roof may need to be replaced.<sup>43</sup>

## Interiors

GML inspected the interiors of buildings A to D as part of a site inspection to inform an analysis of the intactness of the building group. The following table provides a room-by-room breakdown of what original or early fabric remains in the buildings. Refer to Figure 2.20 for a breakdown of the interior rooms of the buildings at 1–3 Bank Street.



Figure 2.20 A breakdown of the interior rooms of the buildings at 1–3 Bank Street. (Source: Craig & Rhodes 2023 with GML overlay)



Room	Analysis
<b>Building A</b>	
A1	<p>Empty room with rendered walls, concrete floor and non-original panel ceiling (ceiling in poor condition).</p> <p>Remaining original fabric includes steel-framed window to east elevation, timber-framed casement windows to the north elevation and, potentially, an internal timber window opening.</p>
A2	<p>Room accessed via the early timber sliding door on the north elevation of Building A. Joins the surrounding rooms to the main warehouse area in A7.</p> <p>Similar to A1, with internal windows to A1 and A3. No original fabric of note.</p>
A3	<p>Same as A1. Also contains a non-original sink and a collection of framed artworks, likely left over from when the building was used as a studio.</p>
A3a	<p>Small access hall to the breaker room of the building, formed by the corrugated metal eastern wall of A4. Contains services related to the use of A4.</p> <p>Original fabric is limited to a steel-framed window.</p> <p>The non-original ceiling of this room is partially collapsed, exposing the underfloor of an inaccessible mezzanine. The south wall is significantly cracked.</p>
A4	<p>Open-ended room with corrugated metal walls on a raised concrete platform from A2. Potentially used as a cool room. Contains no original fabric.</p>
A5	<p>Electrical breaker room with unpainted rendered brick walls and a timber and wire partition. Doors to A4 and A6 have been infilled with bricks.</p> <p>The room contains multiple phases of electrical meters and breakers installed in the building. This includes the original conduits for the building on the south wall. One indicates its ownership by the Municipal Council of Sydney Electricity Department, which was dissolved and merged with the Sydney County Council in 1935. Ownership by the Sydney County Council is also evident on other casings.</p> <p>Some non-original breakers indicate their use for pottery and glass studios, providing evidence that the building was used as an artists' studio for a period of time.</p> <p>Some remnant cabinets and windows are also present in the room.</p>
A6	<p>Former freezer room with insulated walls, a concrete floor and corrugated metal roof. Non-original fabric.</p>
A7	<p>Large open warehouse room externally accessible by the roller doors on the north elevation of the building. The roof structure is visible, showing the timber beams and underside of the corrugated roof cladding.</p> <p>The room has a large concrete floor, with a small, raised platform in the southeast. It joins to rooms A7a and A7b, which are storage bunkers. A brick wall between A7 and A9 appears to be non-original, indicating the spaces were formerly joined.</p> <p>A mezzanine over the rooms to the east and over rooms A7a and A7b is visible from A7. This could not be accessed during inspection.</p> <p>Limited original fabric remains, though room A7 clearly evidences the use of the building as a warehouse.</p>
A7a	<p>A rendered, painted brick storage bunker with a concrete floor. Joins to A7 by a wide opening to allow the movement of goods.</p>

Room	Analysis
A7b	Same as A7a, but with a later unrendered brick wall on its west elevation with a doorway to room A10.
A8	<p>An irregularly shaped room with half-height tiling to the walls. This room was below the former tower of building A, which was removed by the 1990s. Steel beams across the ceiling indicate the rooms operated as a goods lift.</p> <p>Machinery associated with the goods lift appears to have been removed, leaving the concrete floor uneven.</p>
A9	A warehouse room that was likely previously connected to A7. Like A7, the mezzanine floor above the southern rooms is visible from this room. The west elevation of A9 has a wide external opening which is presently boarded up. No original fabric of note remains.
A10	Similar to A7a and A7b, save for an external doorway on the west elevation.
<b>Building B</b>	
B1	<p>First floor. Contains a c1990s timber joinery fitout, including the ceiling, wall panelling, desk, staircase and banister, likely installed when the western windows were enlarged and the shutters added. Non-original bathroom and fireplace.</p> <p>Timber floorboards are likely original, as are potentially the double door and sash windows on the eastern elevation.</p> <p>Fabric is in poor condition.</p>
B2	<p>Below-ground floor workshop, set below the level of the courtyard. Consists of a concrete floor, painted brick walls, central steel beam supports, fluorescent lights, and various shelves, benches, equipment and other clutter.</p> <p>Underfloor structure of B1's original timber floor is visible, including joists and timber-cross bracing.</p> <p>Fabric is in poor condition.</p>
<b>Building C</b>	
C1	<p>An open room with a small timber mezzanine floor (inaccessible) supported by steel beams and a balcony overlooking the water. Fabric comprises a concrete floor, rendered brick walls to the east and south, a modern timber and steel wall to the north and an aluminium-framed glass wall to the west. The roof structure is exposed, showing steel and timber rafters and an underside of corrugated sheeting.</p> <p>Little if any original or early fabric remains in building C. In general, the fabric dates from the late twentieth century and is in poor condition.</p>
<b>Building D</b>	
D1	<p>Room has been converted for residential use, including use of fibro partitions to create a bedroom, kitchen and living space, and installation of timber cupboards. The ceiling is non-original fibro panels, as it is throughout the building.</p> <p>Little original or early fabric remains, save for the window and door joinery and, possibly, the floorboards. Of note are two decorative wall vents above the doorway on the west elevation which differ from the vents elsewhere in the building.</p> <p>Fabric is in poor condition.</p>
D2	Tiled bathroom with shower and a later fitout. Fabric in very poor condition.

Room	Analysis
	Remaining original fabric includes the window joinery. The walls of this room are also brick, indicating that it was an original partition space.
D3	<p>Storeroom with modern timber benches and shelves. Remaining original fabric is limited to the window joinery.</p> <p>Like D2 the walls are brick.</p>
D4	Room formed by non-original partition walls. No original fabric remains except for the door and window joinery and, potentially, the floorboards (covered with carpet).
D5	Same as D4.



Figure 2.21 Building A from Bank Street, with the covered gateway on the right.



Figure 2.22 Building D from Bank Street.



Figure 2.23 The west elevation of buildings B and C.



Figure 2.24 Inside the covered gateway. The early timber sliding door of building A is on the left.





Figure 2.25 Building A from the internal courtyard.



Figure 2.26 Building B from the internal courtyard, showing the goods door.



Figure 2.27 Building C from the internal courtyard.



Figure 2.28 Building D from the internal courtyard.



Figure 2.29 The former wharf west of building A.



Figure 2.30 Building B from beside the water, showing the 'Workshop' sign and timber rack over the roller door.





Figure 2.31 Coal loader bucket being temporarily stored in the courtyard. The bucket will be used for a future public art commission for the New Sydney Fish Market development.



Figure 2.32 Room A7 of building A, the main warehouse space.



Figure 2.33 Room A1. Typical detail of the easternmost rooms of building A.

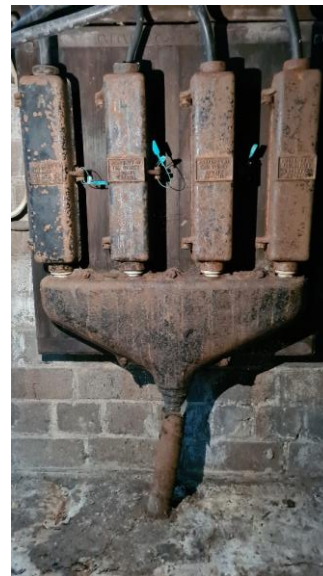


Figure 2.34 The original electrical conduit box in room A5, pre-1935.



Figure 2.35 Room B1 of building B, showing the modern timber joinery.



Figure 2.36 Room B1 continued, showing the southern goods door.





Figure 2.37 Room B2, the workshop space.



Figure 2.38 Room C1 of building C.



Figure 2.39 Room D1 of building D, showing the late twentieth-century partition for residential use.



Figure 2.40 Storage racks in room D3. Sash window joinery is typical for windows throughout the building.

### 2.3.3 5–19 Bank Street

5–19 Bank Street is an irregularly shaped parcel of land southeast of 1–3 Bank Street which covers most of the site. 5–19 Bank Street includes the eastern support structures of Anzac Bridge, as well as Blackwattle Bay Marina and an empty gravel lot used for storage. This part of the site is mostly covered overhead by the span of Anzac Bridge.

The elements of Anzac Bridge that are within 5–19 Bank Street are the eastern pylon and two piers. The eastern pylon is a substantial reinforced concrete structure located on an area of reclaimed land nearby 1–3 Bank Street created for the construction of the bridge. This is much lower than the surrounding land. On the southeast boundary of the site are two concrete bridge piers supporting the motorway. A substation is located between these piers.

Blackwattle Bay Marina comprises the marina, which extends into the water from the reclaimed land surrounding the eastern pylon of Anzac Bridge, and a contemporary storage building.

Southeast of this is an empty gravel lot which is used for the storage of boats and other material. This connects to a boat ramp adjacent to Blackwattle Bay Marina, allowing boats to be launched. The key historical feature of this area is the sandstone seawall to the empty gravel lot. This dates from the early twentieth century, perhaps earlier.



Figure 2.41 The eastern pylon of Anzac Bridge, and Blackwattle Bay Marina.

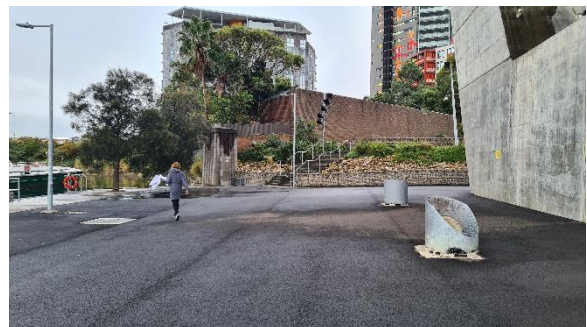


Figure 2.42 The reclaimed land of the eastern pylon of Anzac Bridge, looking towards 1-3 Bank Street.



Figure 2.43 The empty gravel lot southeast of Blackwattle Bay Marina.



Figure 2.44 View to the reclaimed land of the eastern pylon and the boat ramp.





Figure 2.45 The two Anzac Bridge piers on the southeast boundary of the site.



Figure 2.46 View southeast from the boat ramp showing the sandstone seawall of the empty gravel lot.



Figure 2.47 Boats stored in the empty gravel lot.



Figure 2.48 Detail of the sandstone seawall of the empty gravel lot.

### 2.3.4 Views

Views to the site are mostly only available from surrounding waters and foreshore areas owing to its position beneath Anzac Bridge and the steep topography of Pyrmont. Primary views to the site are available from the south and southwest. Views to the north and west are obscured by Anzac Bridge and Glebe Island Bridge, which effectively screen the site from potential vantage points. Views to the east in Pyrmont are also highly restricted by the cliff face and the Anzac Bridge span.

The clearest views to the site are from Blackwattle Bay Park on the opposite shore in Glebe. Views to the site quickly degrade as you move into the residential areas of Glebe above Blackwattle Bay Park, with the historical residences and mature trees obscuring views from the public domain. Views also become very oblique further south towards the New Sydney Fish Market.

More distant views to the site are available from foreshore areas in Rozelle Bay, including Glebe Point, Annadale and Glebe Island. Glebe Island Bridge provides clear views of the western elevations of the buildings at 1–3 Bank Street. Within Pyrmont, only close-up

views from Bank Street and the cliff face are available, with the topography and built form of the surrounding area obscuring the site.

Views within the site are also restricted, with the built form of 1–3 Bank Street and Blackwattle Bay Marina blocking views across of the site. However, the open area southeast of the marina already provides views through to the water from Bank Street and has a park-like quality which is consistent with the desired outcome of the project. Within the built form of 1–3 Bank Street, the courtyard provides close-up views of each of the buildings, contextualising it as a complex of contemporaneous former maritime-industrial buildings.

The availability of views to the site is broadly reflective of the historical views to the site, which was mainly achieved from Blackwattle Bay and Rozelle Bay.

GML has identified five key views based on an analysis of the visual catchment of the site and the location of heritage items in the vicinity, listed below:

Table 2.3 Key views to the site from the surrounding area.

View	Description
1	Clear view the site from Blackwattle Bay Park on the opposite shore in Glebe (Figure 2.49).
2	Distant view to the site from Pope Paul IV Reserve on Glebe Point, broadly representative of the views from Rozelle Bay (Figure 2.50).
3	Oblique view to the site from the University of Sydney Boatshed in Blackwattle Bay (Figure 2.51).
4	View of the waterside elevations of the buildings at 1–3 Bank Street from Glebe Island Bridge (Figure 2.52).
5	View of the buildings at 1–3 Bank Street from Bank Street (Figure 2.53).
6	Internal view within the courtyard of 1–3 Bank Street buildings (Figure 2.54).





Figure 2.49 View 1, clear view the site from Blackwattle Bay Park on the opposite shore in Glebe.



Figure 2.50 View 2, distant view to the site from Pope Paul IV Reserve on Glebe Point, broadly representative of the views from Rozelle Bay.





Figure 2.51 View 3, oblique view to the site from the University of Sydney Boatshed in Blackwattle Bay.



Figure 2.52 View 4 of the waterside elevations of the buildings at 1–3 Bank Street from Glebe Island Bridge.





Figure 2.53 View 5 of the buildings at 1–3 Bank Street from Bank Street.



Figure 2.54 View 6, internal view within the courtyard of 1–3 Bank Street buildings.

## 2.4 Endnotes

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## 3 Significance assessment

### 3.1 Statements of significance

Statements of significance for heritage items within the site are provided in the section below.

The statement of significance for the escarpment face of the former Saunders Quarry is also included due to its proximity to the site and its contribution to its setting.

#### 3.1.1 Anzac Bridge

The following statement of significance is drawn from the State Heritage Inventory listing of Anzac Bridge:

Anzac Bridge has significance at a State level because of its technical qualities; it is a world standard bridge in scale, aesthetics and design features. Anzac Bridge is a reinforced concrete cable-stayed bridge built over Johnstons Bay between Glebe Island and the inner Sydney suburbs of Pyrmont and Darling Harbour. The bridge was designed and built between 1989 and 1995 by the Roads and Traffic Authority (RTA) and its predecessor, the Department of Main Roads (DMR), and is currently the longest such bridge in Australia. The subtle sweep of the bridge's cantilevered deck, which links into the arterial road network and is supported at either end by monumental reinforced concrete towers, forms a striking and integral part of the Sydney skyline. It has quickly become one of the iconic images of Sydney, particularly for those who have views of it, cross it to work by road or bike, or use its highly visible towers as an aid to urban navigation.

Anzac Bridge is also historically significant because it is a contemporary solution to a long-term problem for government agencies responsible for road building and maintenance in Sydney. It replaces the Glebe Island Bridge of 1903, adjacent to Anzac Bridge, which was historically part of the five bridges route connecting Sydney to the north shore. This route was important in connecting Sydney to Parramatta and the north shore from the middle of the nineteenth century, and for much of the twentieth century. The design and construction of a new bridge at the Johnstons Bay crossing (along with the associated freeway road systems) from the late 1980s through to the mid 1990s reflected the desire of the road authorities (the DMR, latterly the RTA) to cut travel times for commuters, and also to limit the build up of traffic on the Glebe Island Bridge. Anzac Bridge is part of the Glebe Island Arterial, and forms an essential part of Sydney's road infrastructure.<sup>1</sup>

### 3.1.2 Glebe Island Bridge

The following statement of significance is drawn from the Conservation Management Plan (CMP) for the Glebe Island Bridge, prepared by NSW Public Works Advisory in 2022.

The Glebe Island Bridge is of State significance.

The bridge, built in 1903, has historical significance as a highly intact bridge from the early 1900s and for its role in the growth and development of Sydney. The longest operating swing span bridge in Australia, it was, until 1995, a key part of Sydney's road network as the city expanded during the 20th century. The bridge facilitated the development of industry at Blackwattle Bay and helped transform the western harbour into a modern working port.

The Glebe Island Bridge has significance for its association with its designer, engineer Percy Allan, who was a leading and highly regarded bridge designer.

The bridge has aesthetic and technical significance and is a landmark of the harbour and inner western Sydney. The bridge's highly intact materiality and detailing and overall form contribute to its distinctive aesthetic quality. As a swing span bridge, the Glebe Island bridge demonstrates highly innovative engineering for its time. It is one of the earliest examples of an electric-powered swing bridge in Australia.

The bridge has social significance for the many commuters who crossed the bridge from 1903 until it became redundant as a road bridge in 1995. It remains a well-loved part of the city as evidenced by the strong community support for its retention and conservation.

The bridge's intact condition makes it a good source of information of engineering mechanisms and systems from the early 1900s. It has the potential to yield information on construction methods and detailing from the early 20th century.

The Glebe Island Bridge is rare as one of only two electrically operated swing span bridges in Australia, the other being the Pyrmont Bridge. Located just over a kilometre apart, the two bridges form a unique set of early 20th century bridge designs, similar yet each with its unique attributes.<sup>2</sup>



State Heritage Register - SHR:01914 - Plan: 2590

Glebe Island Bridge

Bank Street, Victoria Road, Pyrmont

Gazettal Date: 29/11/2013

0 60 120 180 240 Meters

Scale: 1:4,000 @A4  
Datum/Projection: GCS GDA 1994



- SHR Curtilage
- Land Parcels
- Railways
- Roads
- LGAs
- Suburbs

Figure 3.1 State Heritage Register curtilage of Glebe Island Bridge. (Source: State Heritage Inventory, database no. 5051118)



### **3.1.3 Escarpment face of the former Saunders Quarry**

The following statement of significance is drawn from the State Heritage Inventory listing of the escarpment face of the former Saunders Quarry:

The quarry dates from the key period of Pyrmont as a source of quality construction materials used in prominent Sydney buildings and particularly those designed by Government Architect James Barnet and also internationally.<sup>3</sup>

## **3.2 1–3 Bank Street significance assessment**

The following section provides an assessment of the heritage significance of 1–3 Bank Street based on currently available information and previous assessments. The assessment has been undertaken in accordance with NSW Department of Planning and Environment assessing heritage significance guidelines.<sup>4</sup>

It includes a comparative analysis, assessment against the standard criteria and statement of significance.

1–3 Bank Street was previously assessed as being of local significance in City Plan Heritage’s 2021 ‘European Heritage Assessment & Impact Statement’ for the Blackwattle Bay State Significant Precinct study.

### **3.2.1 Comparative analysis**

This section examines 1–3 Bank Street in the context of places which share key features, characteristics or historic themes in the context of the local area, with a focus on maritime industrial complexes.

A search of the State Heritage Inventory for heritage items in the Sydney and Inner West local government areas found some places comparable to 1–3 Bank Street around Blackwattle Bay, Rozelle Bay and White Bay. However, most buildings associated with the former industrial uses of these areas have been demolished since deindustrialisation.

### **Royal Edward Victualling Yard**

The Royal Edward Victualling Yard is a State Heritage Register listed former naval yard complex located on the eastern shore of Pyrmont overlooking Jones Bay. Built between 1904 and 1912, the yard is a complex of three multi-storey Federation masonry warehouses with associated workshops and wharfage designed by government architect Walter Liberty Vernon. The site was used by the Royal Australian Navy (RAN) as a provisioning facility until the 1970s, when it was refurbished for laboratories and, later, offices.

The Royal Edward Victualling Yard has significance for its role in the development, growth and operation of the RAN for much of the twentieth century. The warehouse buildings are intact and contain evidence of the equipment and machinery associated with its historical function of providing supplies to ships. They are also fine examples of Federation warehouse buildings.

The Royal Edward Victualling Yard and 1–3 Bank Street share similarities as former maritime industrial complexes on the waterfront of Pyrmont. Both evidence the range of core and ancillary buildings which supported their use, including warehouses, workshops and offices. Both retain their relationship to the water though, in the case of 1–3 Bank Street, the primary means by which this relationship was established, the wharf, has been lost.

In comparative terms, the Royal Edward Victualling Yard is a significantly larger and more accomplished maritime industrial complex than 1–3 Bank Street. It is more intact and in better condition, with better quality buildings and a longer history of use for maritime purposes. This reflects the importance of the yard, which has significance to New South Wales. 1–3 Bank Street, while important as evidence of the former maritime industry of Blackwattle Bay, is not able to demonstrate the same level of significance which could compare to the state significant Royal Edward Victualling Yard.



Figure 3.2 Two warehouses of the Royal Edward Victualling Yard in Pyrmont, photograph by Stewart Watters 2014. (Source: State Heritage Inventory)

### **Lever Brothers Factory site, Balmain**

The former Lever Brothers Factory site in Balmain was a soap factory adjacent to White Bay which operated from 1895 to 1988. Today, the site is a former industrial complex which has been adaptively re-used for offices and residential apartment blocks. It contains several heritage items listed on the Inner West LEP which are related to the site’s use for the factory:

- Former Unilever administration building and fence (I630)
- Former Unilever vim plant, including interiors (I631)
- Former Unilever oil mill, including interiors (I632)
- Former Unilever copra store, including interiors (I633)

These buildings were constructed between 1896 and 1914 for a range of manufacturing, administrative and storage purposes within the factory, which produced various soaps, oils and cleaning products. The buildings are constructed of brick and reflect Federation-era design principles.

From the 1890s to 1988 the Lever Brothers Factory was a major industrial complex in Balmain and one of the area's largest employers. The factory had an extensive wharf frontage to White Bay for much of its history, with the remnant buildings historically being located adjacent to a series of factory wharves. This was lost with the construction of the White Bay Shipping Container Terminal in the 1970s. After its closure in 1988, the site was extensively redeveloped for residential apartments in the 1990s, with a core group of historical buildings retained closer to the water.

The buildings are significant as evidence of the historical Lever Brothers Factory and demonstrate the range of historical functions at the site. They retain a sense of their original form and purpose despite adaptive re-use, demonstrating a partly intact waterside industrial complex.

1–3 Bank Street is comparable to the Lever Brothers Factory buildings as another waterside industrial complex in the Bays precinct. The buildings of 1–3 Bank Street are more utilitarian and less sophisticated than the Lever Brothers Factory buildings. This is partly related to their later construction—c1932 in comparison to 1895–1914—but also a reflection of the smaller scale of the Cam & Sons business.

1–3 Bank Street more closely reflects the original arrangement of the site than the Levers Brothers Factory site. However, modifications to the buildings at 1–3 Bank Street and the loss of the wharf have degraded its historical fabric and value. Like the Levers Brothers Factory, 1–3 Bank Street is a visible remnant of the former maritime industrial character of Blackwattle Bay. Both have value for demonstrating this aspect of the area's history.





Figure 3.3 The former oil mill building of the Lever Brothers Factory site in Balmain. (Source: State Heritage Inventory)

## Blackwattle Bay Park

Blackwattle Bay Park is a public foreshore park located in Glebe on the opposite shore of Blackwattle Bay to the subject site. The park was created in the 1983 on former industrial land and was extended in the late 1980s to include the former Stride’s boatyard, which was run by one family for 60 years. The crane of Stride’s yard was retained and is now incorporated into the park.

Blackwattle Bay Park retains many industrial relics like the crane of Stride’s yard and landscape elements associated with its former industrial use. Much of the site has a sandstone seawall associated with the historical land reclamation of the Glebe foreshore to create suitable industrial land.

The site is significant as a public foreshore reserve constructed on former industrial land and for its historical use for industry from the nineteenth century to the late twentieth century.

Blackwattle Bay Park is less intact than 1–3 Bank Street as an industrial site but demonstrates successful repurposing of the site for public open space. It is indicative of the rarity of 1–3 Bank Street in Blackwattle Bay as an intact industrial site.



Figure 3.4 Stride’s Crane in Blackwattle Bay Park. (Source: State Heritage Inventory)

## Summary

Examples of historical maritime industrial complexes can be found in the areas around 1–3 Bank Street, though are rare in Blackwattle Bay itself. The bulk of these complexes survive in Balmain or on the eastern shore of Pyrmont. In general, these complexes are much larger and higher quality than 1–3 Bank Street. These were generally constructed in the decades prior to the 1930s.

1–3 Bank Street is utilitarian in comparison, with various phases of modifications removing significant internal fabric. It is, however, one of the last maritime industrial complexes in Blackwattle Bay. There are also few examples of maritime industrial complexes built for use by a fishing trawler fleet in the surrounding areas. In this sense, 1–3 Bank Street is rare. Its construction in the early 1930s is also notable, because

many other examples of maritime industrial complexes in the surrounding area date from the 1920s or earlier.

### **3.2.2 Assessment against criteria**

#### **Criterion A (historical significance)**

An item is important in the course, or pattern, of NSW's cultural or natural history (or the cultural or natural history of the local area).

1–3 Bank Street is a relic of the former maritime industry which once dominated Blackwattle Bay in the late nineteenth to mid twentieth century. The site has historical significance as a maritime industrial complex built in c1932 to support the fishing trawler operations of Cam & Sons. At one point, Cam & Sons reportedly had the largest fleet of steam fishing trawlers in the southern hemisphere, many of which were later requisitioned by the Royal Australian Navy during World War II. Fish trawling was an important industry in Blackwattle Bay which has persisted through to the twenty-first century with the Sydney Fish Market. 1–3 Bank Street is also more generally a remnant of the early twentieth century maritime industry of Blackwattle Bay, which has mostly disappeared.

1–3 Bank Street meets the threshold of significance at a local level under this criterion.

#### **Criterion B (associative significance)**

An item has strong or special association with the life or works of a person, or group of persons, of importance in the cultural or natural history of NSW (or the cultural or natural history of the local area).

1–3 Bank Street has strong associations with Charles Cam (Carlo Caminiti), the Caminiti family and their business, Cam & Sons. Charles Cam was an important figure in the fishing industry in Sydney in the early twentieth century, becoming a well-respected industry figure by the time of his death in 1947. Cam & Sons operated at 1–3 Bank Street for thirty years. This encouraged the growth of the fishing industry in Blackwattle Bay, prefiguring the construction of the Sydney Fish Market in the 1960s. 1–3 Bank Street retains this connection as a complex specially built for Cam & Sons.

1–3 Bank Street meets the threshold of significance at a local level under this criterion.

#### **Criterion C (aesthetic significance)**

An item is important in demonstrating aesthetic characteristics and/or a high degree of creative or technical achievement in NSW (or the local area).

1–3 Bank Street is a utilitarian maritime industrial complex. While buildings A and D demonstrate some aspects of the Inter-War Functionalist and Stripped Classical styles,

they are not significant examples of the style and do not demonstrate a noteworthy creative or technical achievement.

1–3 Bank Street does not meet the threshold of significance under this criterion.

### **Criterion D (social significance)**

An item has strong or special association with a particular community or cultural group in NSW (or the local area) for social, cultural or spiritual reasons.

There is no specific indication of 1–3 Bank Street having a strong or special association with a particular community or cultural group in the local area or beyond. However, this has not been formally tested.

1–3 Bank Street is unlikely to meet the threshold of significance under this criterion.

### **Criterion E (Research potential)**

An item has potential to yield information that will contribute to an understanding of NSW's cultural or natural history (or the cultural or natural history of the local area).

Archaeological remains associated with phase one (land reclamation in 1803–1895) and phase two (early industrial use in 1895–c1932) could contribute to our understanding of the development of Blackwattle Bay and Pymont. Artefacts and other materials contained within reclamation fills, while not easily provenanced, may provide insights into the source of the fill material and the systems by which it was procured, transported and deposited. Remains associated with later phases of the site may supplement what is known about the development of 1–3 Bank Street.

1–3 Bank Street has the potential to reach the threshold of significance at a local level under this criterion if substantive archaeological remains are encountered.

### **Criterion F (rarity)**

An item possesses uncommon, rare or endangered aspects of NSW's cultural or natural history (or the cultural or natural history of the local area).

It was determined through comparative analysis that 1–3 Bank Street is a rare example of an early twentieth maritime industrial complex in Blackwattle Bay. Few examples remain in the area, with most having been demolished in the late twentieth century after deindustrialisation. It is also unusual as a maritime industrial complex built for use by a fishing trawler fleet in Blackwattle Bay, which was dominated by other industries for much of the early twentieth century.

1–3 Bank Street meets the threshold of significance at a local level under this criterion.



## Criterion G (representativeness)

An item is important in demonstrating the principal characteristics of a class of NSW's (or a class of the local area's):

- cultural or natural places; or
- cultural or natural environments

1–3 Bank Street is not a particularly fine, intact or distinctive example of a former maritime industrial complex in the local area. The buildings of 1–3 Bank Street do not demonstrate a cohesive design, employing different architectural styles and showing different levels of modification. Little original fabric remains in the buildings which could represent its maritime industrial use, except for in buildings A and D. Superior examples of maritime industrial complexes can be found in Darling Harbour and White Bay which better represent the former working harbour of Sydney.

1–3 Bank Street does not meet the threshold of significance under this criterion.

## Integrity

The buildings at 1–3 Bank Street have all been subject to modifications which have impacted their integrity:

- Building A has been modified on its interior and partly on its exterior, but retains the layout of rooms and spaces which evidence its use as a warehouse. Some early twentieth-century fabric is also present, though disparate.
- Building B is internally modified with little to no original fabric remaining. However, the external fabric and openings of the building retain evidence of its use as a goods store and workshop, which reflects early twentieth-century industrial practice.
- Building C has been almost completely modified and no longer represents the fabric or characteristics of an early twentieth-century maritime building.
- Building D is the most intact externally, although it contains little original or early fabric on the interior, save for door and window joinery and the internal brick rooms. It is representative of a building within a maritime industrial complex used for non-industrial purposes such as offices.

Based on the above, Buildings A, B and D can be considered to be of moderate significance. Building C is of little significance.

## 3.2.3 Summary statement of significance

1–3 Bank Street, Pyrmont has historical and associative significance at a local level as a rare early twentieth-century maritime industrial complex in Blackwattle Bay. The site was built in c1932 by Cam & Sons and operated for thirty years as the base of their steam fishing trawler business. Cam & Sons were a significant fishing business in Sydney in the

early twentieth century and at one point reportedly had the largest fleet of steam trawlers in the southern hemisphere. The site has significance for its association with the company, the Caminiti family and Charles Cam (Carlo Caminiti), who played a significant role in the development of the fishing industry in Blackwattle Bay. 1–3 Bank Street is a rare early twentieth-century remnant of the industry which once dominated Blackwattle Bay but has been lost since deindustrialisation. Modifications and the removal of the wharf has affected its historical relationship to the water and to the Cam & Sons fishing trawling business.

### 3.2.4 Adaptive re-use principles

In general, the significance of the place should guide the conservation of significant areas, elements, views and fabric of the place. The treatment of all site attributes and components should be directly related to the nature and degree of their significance, with priority given to the conservation of attributes and components of highest significance.

Buildings A, B and D of 1–3 Bank Street are assessed as being of moderate significance due to the internal and external modifications to the building. The appropriate treatment for elements of moderate significance is retention and conservation where possible. Adaptation, alteration and/or removal is permissible with heritage advice.

Building C is of little significance. The appropriate treatment is the same as for moderate significance, with fewer constraints on removal.

Adaptive re-use of the buildings at 1–3 Bank Street should follow the principles of the Burra Charter, especially the maxim 'do as much as necessary and as little as possible' when adaptive re-use is proposed. Original internal and external fabric should be retained as far as possible.

## 3.3 Endnotes

- <sup>1</sup> State Heritage Inventory, 'Anzac Bridge' [4305018], Heritage NSW, accessed 31 July 2023. <https://www.hms.heritage.nsw.gov.au/App/Item/ViewItem?itemId=4305018>.
- <sup>2</sup> NSW Public Works Advisory 2022, 'Glebe Island Bridge Conservation Management Plan', report prepared for Transport for NSW, p 39.
- <sup>3</sup> State Heritage Inventory, 'Escarpment Face from former "Saunders' Quarry"' [2424701], Heritage NSW, accessed 31 July 2023, <https://www.hms.heritage.nsw.gov.au/App/Item/ViewItem?itemId=2424701>.
- <sup>4</sup> Environment and Heritage Group, Department of Planning and Environment 2023, *Assessing heritage significance: guidelines for assessing places and objects against the Heritage Council of NSW criteria*.

## 4 Proposed works

### 4.1 The proposal

Bank Street Park is a new 1.1 hectare waterfront park located around the southern pylon of Anzac Bridge at 1A to 19 Bank Street, Pyrmont, beside Blackwattle Bay. Development consent is being sought for a recreation area for the primary purpose of a public park. It will celebrate First Nations living culture together with its harbourside location and maritime history.

The preferred concept design for Bank Street Park has been developed by the design team lead by Oculus. Refer to Figure 4.1. It includes:

- site preparation works, including tree removal, earthworks and remediation to facilitate the proposed use;
- demolition of three existing buildings at 1–3 Bank Street;
- new and adapted facilities for community use, including:
  - a new single-storey building to accommodate a flexible community space, café and marina office/store facilities, with a green roof and photovoltaics;
  - adaptive re-use of Building D for public amenities, bins and other storage;
  - a boat launching ramp and pontoon for passive watercraft, including dragon boats and kayaks; and
  - a boat storage building with change facilities for dragon boat users with publicly accessible rooftop deck.
- public domain works, including:
  - an Interpretation Garden in the existing building ‘ruins’ at 1–3 Bank Street;
  - a split level foreshore promenade;
  - a multi-purpose court with edge seating and partial fence;
  - an inclusive nature-based playspace for ages 2–12;
  - fitness equipment;
  - a public plaza and grassed open space areas;
  - new tree plantings and planter beds; and
  - public art, wayfinding and interpretative signage, lighting, bike parking and seating.
- Harbour works including:
  - an overwater boardwalk;
  - land/water interface works, including sandstone terracing into the water and support structure to improve marine habitat;
  - demolition and construction of a new timber launching ramp for dragon boats;

- a kayak/passive craft pontoon; and
- restoration, repair and alterations to the existing seawall for new stormwater outlets.
- works to Bank Street road reserve, including:
  - road space reallocation to provide a separated cycleway;
  - a cycleway transition to Bank Street to continue south as part of future works;
  - reinstatement of existing on-street parallel parking;
  - tree planting;
  - accessible parking spaces; and
  - a loading zone adjacent to 1–3 Bank Street.

The present intention is that the finishes of the park complement existing materials at the site and reflect the maritime industrial history and qualities of Blackwattle Bay. This includes the use of clay masonry bricks with glazed ceramic inserts for the façade of the new building and the south elevation of Building D to complement the brickwork. Sandstone seawalls and timber decking on the water’s edge and sandstone seating in the park will also reflect the existing materials palette.

Reclaimed bricks from the demolition of buildings at 1–3 Bank Street will also be re-used in areas of the site. Most notably is their re-use on the floor of the dragon boat storage building and beside Building D.

### **4.1.1 Key impacts to heritage items**

Key impacts to listed and unlisted heritage items in the site are discussed below.

#### **Anzac Bridge**

The proposal will maintain a curtilage around the eastern pylon and piers of Anzac Bridge to allow access to these elements in line with the bridge’s operational requirements.

The ground level will be raised by 0.5m around the eastern pylon of the bridge, with associated resurfacing works. Indicative plans also include lighting around the eastern pylon, which is anticipated to be minor and requires coordination with Transport for NSW. No other works are proposed to the bridge elements.

New built elements and plantings will be introduced into the setting that includes elements of Anzac Bridge, including the dragon boat storage structure and kayak kiosk near the eastern pylon and the multi-purpose court near the piers.



## **Glebe Island Bridge**

The proposal will change the footpaths and road network to accommodate a bike path on Bank Street as it approaches the eastern abutment of Glebe Island Bridge. Works include the removal or alteration of the gate and minor changes to the footpath network and kerbs which will join to the asphalt surface of the bridge.

A staircase will be built on the west side of Building D in 1–3 Bank Street which may interact with the eastern abutment. This area contains a concrete retaining wall which supports the road as it joins to the sandstone-faced abutment. GML was unable to verify the presence of original sandstone blockwork of the abutment in this area due to the vegetation. However, it is likely that the construction of the concrete retaining wall would have removed any sandstone blockwork if it existed. Impacts to original sandstone blockwork of the abutment would need to be avoided or minimised if it is encountered, but this is considered unlikely as per the above.

It is anticipated that works will only affect the concrete retaining wall, kerbs, gate and asphalt surface of the bridge. No changes to the bridge or bridge abutment are expected.

Some trees will be removed from 1A Bank Street adjacent to the bridge abutment and will be replaced with new plantings. Several trees of some arboricultural value will be retained in this area. No other works to 1A Bank Street are proposed.

## **1–3 Bank Street**

The proposal will make significant changes to 1–3 Bank Street, including the demolition of Buildings A, B and C and the construction of a new single-storey purpose-built community and marina office/storage building.

Buildings B and C will be removed entirely to make way for a landscaped courtyard and ramp to the marina. The bottom floor of Building B will be filled to bring it to a height of RL4.20.

The majority of Building A will be demolished and replaced by the new single-storey community and marina office/storage building in the south of 1–3 Bank Street. This will involve excavation of the slab from approximately RL4.50 at its highest to RL3.50. Elements of the building's northeast rooms will be retained as 'ruins' and adapted into an Interpretation Garden. This will interpret the layout of the warehouse building by using truncated walls of the rooms and other fabric while introducing garden beds and seating for park users. The final extent of the fabric to be retained and used in the Interpretation Garden is not finalised and will be resolved in subsequent design development.

Building D will be adaptively re-used and converted into park amenities, a bin store and storage space. Proposed works to the buildings include demolition and replacement of the south wall and roof of the building. The north, east and west walls will be retained, as

will existing windows. Internal walls and fabric of Building D will be removed and replaced with new fabric and new walls to accommodate new uses.

The new south elevation of Building D will become the primary entrance and will have a metal-framed screen with clay bricks to match the new community/marina building. The new wall will be open towards to the ceiling to provide ventilation, with a metal awning overhead providing weather protection. The roof will be replaced by a standing-seam zinc roof with a new support structure. The new interior finish will use glazed ceramic tiles and terrazzo formed using reclaimed building waste and stainless steel bathroom hardware. Existing window frames will be re-used with new glazing. It is proposed that feature projector luminaires will be used to light the bays of Building D's street-facing façade.

The proposed new building at 1–3 Bank Street will be single-storey and contemporary in design, adopting a 'pebble' shape to maximise visibility through the park. It will have a green roof with photovoltaic panels. The design of the façade will use clay bricks with ceramic glazed inserts mounted on a metal frame. This will complement the existing materials of 1–3 Bank Street.

A new courtyard will be created around Building D, the new building and the Interpretation Garden in the remains of Building A. This will incorporate sandstone block walls and recycled brick paving. Where salvageable, bricks from the demolished buildings at 1–3 Bank Street will be used for paving and in the floor of the new dragon boat storage building.

## **Seawalls**

Sandstone seawalls across the site will be restored, repaired and altered to accommodate the new timber boardwalks, split-level promenade and sandstone terracing into the water.

In 5–19 Bank Street, the existing sandstone seawall will be retained and a new sandstone terracing built out from the face of the wall into Blackwattle Bay. This will be supported by a new concrete slab and piles.

In 1–3 Bank Street, the existing sandstone seawall will be retained. The wall beside Building B will be raised with new sandstone blocks and a second skin of sandstone blocks will be built in front of this seawall. The seawall behind Building A will be slightly lowered to accommodate the boardwalk and the ramp to the marina. This will affect some portions of the sandstone seawall and a section of concrete infill.

Stormwater adjustments may necessitate new outlets being formed in the seawalls, which is to be confirmed in the detailed design.



Figure 4.1 Preferred concept plan, Bank Street Park. (Source: Oculus 2023)



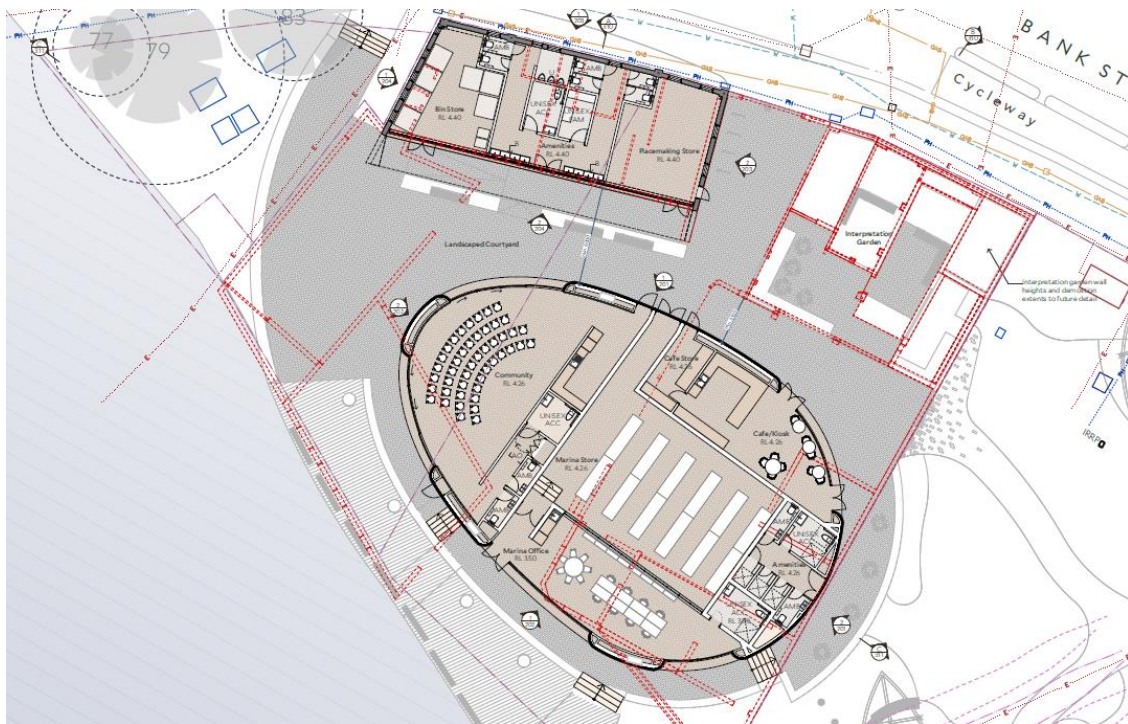


Figure 4.2 Detail of the proposed plan for works to 1–3 Bank Street showing the demolition of the buildings. (Source: Collins and Turner 2023)

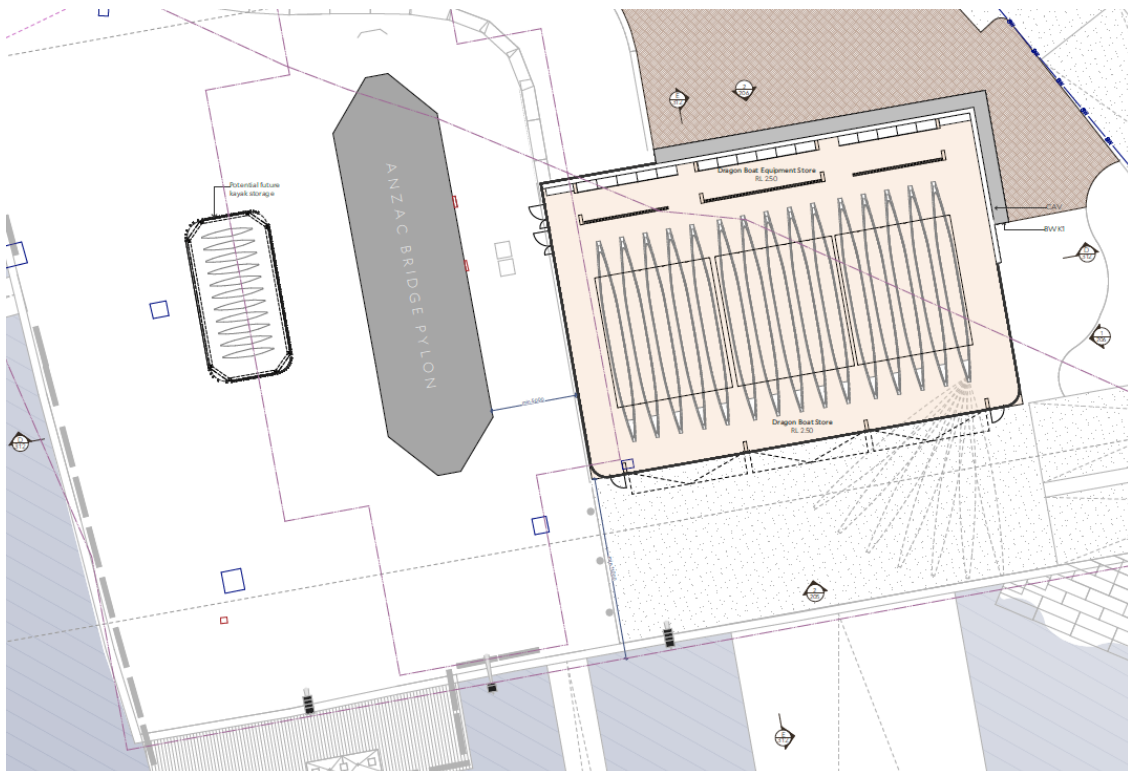


Figure 4.3 Detail of the planned dragon boat storage facility near the eastern pylon of Anzac Bridge. (Source: Collins and Turner 2023)





Figure 4.4 An indicative render of the design for 1–3 Bank Street in the proposed Bank Street Park. (Source: Oculus 2023)



Figure 4.5 An indicative render of the Interpretation Garden of Building A's north rooms (Source: Oculus 2023)





Figure 4.6 An indicative render overview of the rest of the site, showing the dragon boat storage building and sandstone block terrace seawall. (Source: Oculus 2023)

## **Views**

Refer to Figure 4.7, Figure 4.8, Figure 4.9, Figure 4.10, Figure 4.11 and Figure 4.12 below for a photomontage of the proposal from key locations within Blackwattle Bay.



Figure 4.7 Indicative render of Bank Street Park from View 1—Blackwattle Bay Park in Glebe.  
(Source: Oculus 2023)



Figure 4.8 Indicative render of the park from View 2—Pope Paul VI Reserve at Glebe Point.  
(Source: Oculus 2023)





Figure 4.9 Indicative render of the park from View 3—beside the University of Sydney Boathouse. (Source: Oculus 2023)

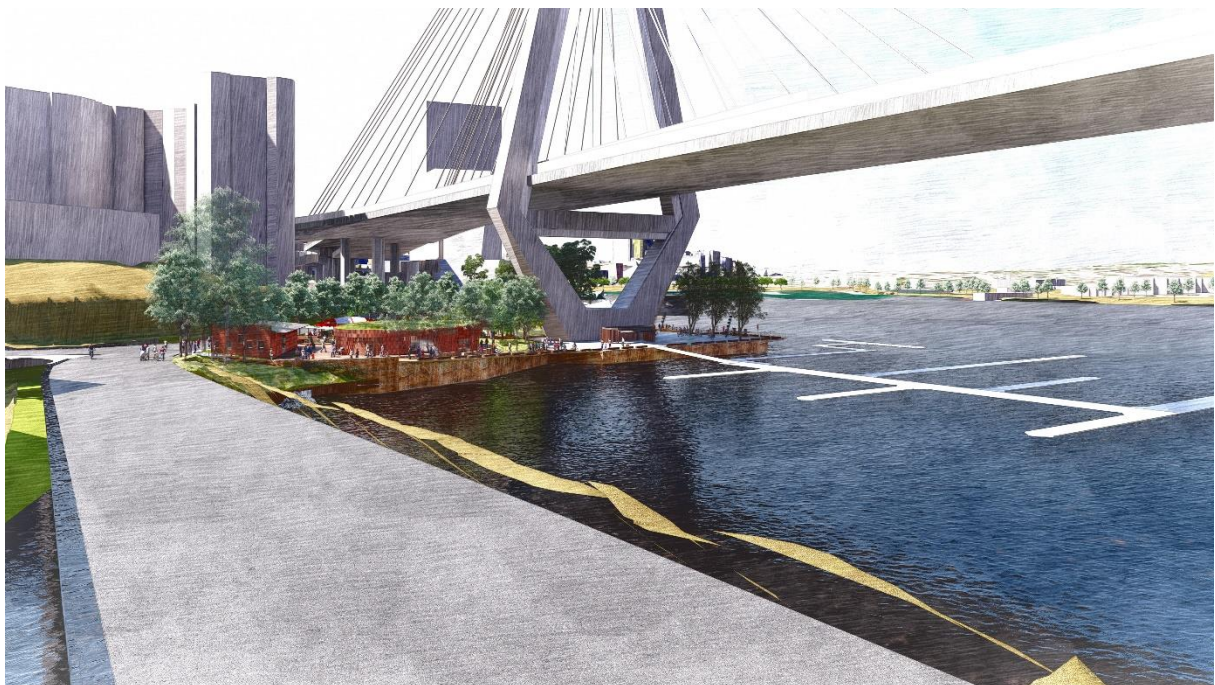


Figure 4.10 Indicative render of the park from View 4—near the swing span of Glebe Island Bridge. (Source: Oculus 2023)





Figure 4.11 An indicative render of the park from View 5—Bank Street, showing the adaptively re-used Building D. (Source: Oculus 2023)



Figure 4.12 An indicative render of site from View 6—the internal courtyard of 1–3 Bank Street. (Source: Oculus 2023).

## 4.2 Documentation

This report has assessed the impact of the proposed works on the heritage items and on items in the vicinity of the site based on the 'Bank Street Park Design Report' by Oculus (13 October 2023) and the following drawings (Table 4.1).

Table 4.1 Architectural drawing references for the proposal.

Drawing	Title	Date	Revision
<b>Collins and Turner—Bank Street Park (Tjerruing Park), 1A–19 Bank Street, Pyrmont NSW 2009—State Significant Development Application SSD-53386706—architectural drawings</b>			
346_SSDA_000	Cover Sheet	13/10/2023	—
346_SSDA_010	Title Sheet	13/10/2023	02
346_SSDA_111	Site Plan (Detail)	13/10/2023	03
346_SSDA_112	Site Analysis	13/10/2023	02
346_SSDA_113	Existing Plan	13/10/2023	04
346_SSDA_114	Existing Plan	13/10/2023	03
346_SSDA_140	Northwest Architectural Ground Floor Plan	13/10/2023	04
346_SSDA_141	Northwest Architectural Roof Plan	13/10/2023	04
346_SSDA_142	Pylon Surrounds Architectural Ground Floor Plan	13/10/2023	04
346_SSDA_143	Pylon Surround Architectural Roof Plan	13/10/2023	04
346_SSDA_201	Elevations—1–3 Bank Street	13/10/2023	03
346_SSDA_202	Elevations—1–3 Bank Street	13/10/2023	02
346_SSDA_203	Elevations—1–3 Bank Street	13/10/2023	03
346_SSDA_204	Elevations—1–3 Bank Street	13/10/2023	02
346_SSDA_205	Elevations—Dragon Boat Storage	13/10/2023	03
346_SSDA_206	Elevations—Dragon Boat Storage	13/10/2023	02
346_SSDA_310	Sections—1–3 Bank Street	13/10/2023	02
346_SSDA_311	Sections—1–3 Bank Street	13/10/2023	02
346_SSDA_312	Sections—Dragon Boat Storage	13/10/2023	02

Drawing	Title	Date	Revision
346_SSDA_411	Area Diagram	13/10/2023	02
<b>Oculus—Bank Street Park, Bank Street Pyrmont—landscape drawings—for SSDA</b>			
S22-026-L000	Cover Sheet	13/10/2023	5
S22-026-L001	Legend	13/10/2023	5
S22-026-L002	Materials Schedule	13/10/2023	5
S22-026-L003	Planting Schedule	13/10/2023	5
S22-026-L004	Site Plan	13/10/2023	5
S22-026-L005	Tree Canopy Cover	13/10/2023	4
S22-026-L101	Tree Retention and Demolition	13/10/2023	5
S22-026-L201	General Arrangement—Sheet 01	13/10/2023	5
S22-026-L202	General Arrangement—Sheet 02	13/10/2023	5
S22-026-L203	General Arrangement—Public Domain Plan	13/10/2023	5
S22-026-L501	Planting Plan	13/10/2023	6
<b>Enspire Solutions—Bank Street Park, Pyrmont—Cut and Fill Plan</b>			
SK0025-220067-00	Cut and Fill Plan	13/07/2023	4

## 4.3 Background

Bank Street Park is part of the Blackwattle Bay State Significant Precinct, which was rezoned for mixed-use development in December 2022. Updated planning and land use controls were incorporated into the Sydney Local Environmental Plan 2012, along with site specific design guidance in the *Blackwattle Bay Design Guidelines*. The new planning controls came into effect on 8 September 2023.

Bank Street Park will be the first development within the precinct after the New Sydney Fish Market.

Subsequent development south of the site will involve the construction of new mixed-use and residential buildings.



### **4.3.1 Pre-lodgement consultation**

During the rezoning application for the Blackwattle Bay State Significant Precinct, submissions by City of Sydney and Heritage NSW identified the need for additional assessment of the 1–3 Bank Street site. This site was identified as having local significance in the European Heritage Assessment and Impact Assessment for the rezoning application, prepared by City Plan Heritage in 2021.<sup>1</sup> Subsequent analysis by GML confirmed its significance during development of the preferred plan, which is discussed in the section below.

INSW and the design team also consulted with the NSW State Design Review Panel (SDRP) on 20 April, 29 June and 10 August 2023 regarding the design of Bank Street Park. Retention of fabric of the 1–3 Bank Street building was recommended by the panel and subsequently integrated into the design.

### **4.3.2 Considerations of alternatives**

INSW engaged the services of GML to provide heritage advice during development of the preferred concept design for Bank Street Park. Heritage input into the design has included detailed advice on the built form, typology, scale, character, detail and materiality.

Initial design intentions for the project proposed the demolition of the buildings at 1–3 Bank Street and replacement with a purpose-built structure. Full retention of the buildings was not considered a viable approach as it would not meet the objectives of the park and result in a poorly integrated site.

Specifically, the built form of 1–3 Bank Street was considered an impediment to movement, solar access and the integration of 1–3 Bank Street into the park. The blank double-height brick wall of Building A was considered a significant constraint, running nearly the length of 1–3 Bank Street’s southern boundary. This wall blocks movement through the site and overshadows the area of the site that has the best year-round solar access. Its retention would have resulted in a poorly integrated and dark park, with 1–3 Bank Street at 5–19 Bank Street effectively separate sites. Buildings B and C were similarly considered impediments to the creation of a waterfront promenade along the length of the park.

To aid in design development and address comments raised during the rezoning application, GML undertook a fabric analysis and preliminary heritage assessment of 1–3 Bank Street during design development. This analysis was provided in a memo to the design team at an early stage and concluded that the buildings were modified but had significance at a local level. This analysis is now integrated into this report, including the fabric analysis in Section 2.3.2 and significance assessment in Section 3.2.

GML's analysis concluded that building C was of low significance and was largely rebuilt in the late twentieth century. Buildings A, B and D were identified as being of moderate significance, with modifications having removed almost all of the internal fabric.

Of the four buildings, Buildings A and D were identified as the best candidates for retention and adaptive re-use. Retention of Building C was discounted due to its poor condition and low significance. Retention of Building B was also discounted as the ground level would be unusable under INSW flood and sea level guidelines, diminishing the viability of its re-use.

Three initial concepts design for Bank Street Park were subsequently reviewed by GML:

- Concept 1 retained buildings A and D, with the reconfiguration of building A to create a path through its centre providing greater permeability through the southern brick wall. Buildings B and C were removed to create a waterfront promenade with potential stair access to Glebe Island Bridge.
- Concept 2 was the same as Option 1, except that building D was proposed to be demolished for a planted seating area, with only its façade retained.
- Concept 3 proposed to demolish all buildings at 1–3 Bank Street and their replacement with a new, purpose-built building.

GML viewed concept 1 as the preferable option from a heritage perspective as it proposed to do the least. Demolition of all the buildings was identified as having a major and irreversible effect on the site's assessed heritage significance.

Subsequent discussions between GML and the design team identified that the south elevation of Building A, a double-height brick wall, placed significant constraints on the design of the park. This included impediments to planned facilities, movement and solar access. Adaptive re-use of Building A for the marina storage and community spaces was not viable, with the higher ground level of the building to the water preventing an adequate path of access for the marina storage. The building also blocked movement through the site, resulting in a badly integrated park with poor wayfinding outcomes. Retention would also result in significant overshadow in winter and spring, in the area with the best solar access in the site.

To meet project objectives, it was proposed to mostly demolish Building A, retaining elements of its northern rooms as 'ruins' and adapting them into an Interpretation Garden. This would interpret the layout of the warehouse building by retaining truncated dwarf walls and other fabric, and adding garden beds and seating for park users. This enabled key project objectives to be met and assisted in partially mitigating the loss of Building A.

GML considered it an acceptable solution, considering that the full retention of Building A was not compatible with project objectives. Building A is assessed as being modified on the interior and exterior while retaining the layout of rooms and spaces that evidence its

significant historical use as a maritime-industrial warehouse by Cam & Sons. The blank double-height brick wall was also not considered an especially significant element. Retention of the layout of some interior spaces as 'ruins' will enable this to be read in the design of the park, interpreting the structure while contributing to the success of the project. The final extent of fabric to be retained is yet to be determined, but present designs clearly interpret the layout of the north rooms.

Discussions between the design team and GML also resulted in the retention of Building D for adaptive re-use as amenities and storage.

GML recommended further mitigation of the loss of the unlisted buildings with archival recording prior to commencement of works, salvage of remaining significant fabric and interpretation of the history of the site in the final design. The re-use of brick masonry in the design of the park was also recommended, including in the façade of the new building. This did not proceed; however, the façade of the new building now incorporates clay bricks to complement the existing materiality of 1–3 Bank Street. These are now included as recommendations in Section 6.1 of this report, among others.

## 4.4 Endnotes

<sup>1</sup> City Plan Heritage 2021, 'European Heritage Assessment & Impact Statement: A report to support Blackwattle Bay State Significance Precinct study', prepared for Infrastructure NSW.



# 5 Heritage impact assessment

## 5.1 Introduction

Table 5.1 describes the terminology used in this report when assessing the heritage impacts of the preferred concept design.

Table 5.1 Heritage impact rating definitions.

Rating	Definition
Major adverse	<p>Actions which will have a severe, long-term and possibly irreversible impact on the heritage item.</p> <p>Actions in this category would include partial or complete demolition of a heritage item or addition of a new structure in its vicinity that destroys the visual setting of the item. These actions cannot be fully mitigated.</p>
Moderate adverse	<p>Actions which will have an adverse impact on a heritage item. Actions in this category would include removal of an important aspect of a heritage item’s setting or temporary removal of significant elements or fabric. The impact of these actions could be reduced through appropriate mitigation measures.</p>
Minor adverse	<p>Actions which will have a minor adverse impact on a heritage item. This may be the result of the action affecting only a distant/small part of the setting of a heritage place.</p> <p>The action may also be temporary and/or reversible.</p>
Neutral	<p>Actions which will have no heritage impact.</p>
Minor positive	<p>Actions which will bring a minor benefit to a heritage item, such as an improvement in the item’s visual setting.</p>
Moderate positive	<p>Actions which will bring a moderate benefit to a heritage item, such as removal of intrusive elements or fabric, or a substantial improvement to the item’s visual setting.</p>
Major positive	<p>Actions which will bring a major benefit to a heritage item, such as reconstruction of significant fabric, removal of substantial intrusive elements/fabric or reinstatement of an item’s visual setting or curtilage.</p>

## 5.2 Matters for consideration

The impact of the proposal against the matters for consideration as defined in the 2023 guidelines for preparing a statement of heritage impact<sup>1</sup> are discussed in the subsections below.

### 5.2.1 Fabric and spatial arrangements

The impacts of proposed modifications to the fabric and spatial arrangements of heritage items within the site are discussed in Table 5.2 below.

Table 5.2 Assessment of impacts of the proposal to the fabric and spatial arrangements of heritage items within the site.

Heritage item	Discussion
<p><b>Anzac Bridge</b> (Transport for NSW [Roads and Maritime] S170 register)</p>	<p>Ground level around the eastern pylon of Anzac Bridge will be raised by 0.5m. The surface finish will be like the existing surface. This will have a negligible impact on the bridge, affecting a miniscule part of the overall structure with no impact to its heritage values. Lighting will have a similar effect.</p> <p>The piers of the bridge on the southeast boundary of the site will be unaffected.</p> <p><b>The proposal will have a neutral impact to the fabric and spatial arrangements of Anzac Bridge.</b></p>
<p><b>Glebe Island Bridge</b> (SHR)</p>	<p>The proposal will make minor changes to footpaths, kerbs and the road network on Bank Street as it joins to Glebe Island Bridge. This includes introducing a new staircase beside Building D of 1–3 Bank Street.</p> <p>Works will occur on the approach to the eastern abutment of the bridge within its SHR curtilage. Works will primarily affect a modern concrete retaining wall and the road surface and kerbs. The impacts are so minor and will affect such non-significant fabric that no impact is anticipated.</p> <p><b>The proposal will have a neutral impact to the fabric and spatial arrangements of Glebe Island Bridge.</b></p>
<p><b>1–3 Bank Street, Pyrmont</b> (Unlisted)</p>	<p>The proposal will demolish Buildings A, B and C of 1–3 Bank Street and will adaptively re-use Building D. This site is unlisted but has been identified as having local significance as a rare historical maritime industrial complex in Blackwattle Bay. The impact of demolition is discussed in Section 5.2.5.</p> <p>Building D is proposed to be adaptively re-used in the park for amenities and storage. Proposed alterations to the building include demolition and rebuilding of its south elevation, replacing the roof, removing internal fabric and installing new partitions.</p> <p>Building D has been assessed as being of moderate significance. It is the most intact building at 1–3 Bank Street externally, though it contains little original or early fabric in the interior. Works will remove all remaining internal fabric and partitions. This includes the likely original timber floor, which was assessed as having termite damage and needing replacement. Original fabric of the south elevation, including doors, sash windows, wall vents and brickwork will similarly be removed.</p>

Heritage item	Discussion
	<p>A structural condition assessment identified that the roof may need replacement due to the poor condition of visible timber structural members in the building. Further investigation into the viability of the present roof structure is recommended, as retaining the original roof would assist in mitigating the impact of modifications to Building D. Similarly, options to retain the south elevation of Building D could be explored.</p> <p>The proposed new south elevation and roof of 1–3 Bank Street will adopt a contemporary design idiom like that of the new building to be constructed south of Building D. All other elevations are proposed to be retained in their current states.</p> <p>Works to Building D will result in some loss in the integrity of the building but will result in its adaptive re-use, which is positive. Proposed works will retain the most significant and architecturally detailed façade of the building facing Bank Street. The modifications to Building D will have a moderate adverse impact to the building but are considered acceptable as they will enable its re-use.</p> <p>It is recommended that expert heritage and structural advice is sought during detailed design development and construction to ensure impacts to Building D are minimised. Options to fully or partially retain the south elevation should be explored. Building D should also be conserved during works.</p> <p><b>The proposal will have a moderate adverse impact to the fabric and spatial arrangements of Building D, 1–3 Bank Street.</b></p> <p><b>The impact is partially mitigated by the re-use of Building D.</b></p>

## 5.2.2 Setting, views and vistas

The impacts to the setting, views and vistas of heritage items within the site are discussed in Table 5.3 below.

Refer to Figure 4.7, Figure 4.8, Figure 4.9, Figure 4.10, Figure 4.11 and Figure 4.12 below for a photomontage of the proposal from key locations within Blackwattle Bay.

Table 5.3 Assessment of the impacts of the proposal on setting, views and vistas.

Heritage item	Discussion
<p><b>Anzac Bridge</b> (Transport for NSW [Roads and Maritime] S170 register)</p>	<p>The proposal will introduce new structures into the setting of the eastern pylon of Anzac Bridge, including the dragon boat storage, kayak kiosk, and the new community and marina office/storage building. Existing buildings of 1–3 Bank Street and the Blackwattle Bay Marina nearby the pylon will be removed.</p> <p>The open space around the bridge will otherwise be maintained and converted into a park. This includes landscaping works, playgrounds, a waterfront promenade and park lighting.</p> <p>These changes will result in localised changes to the setting and views of the bridge structure but will have an entirely negligible effect on the appreciation of the bridge structure. There is no anticipated impact arising from the proposed works.</p>



Heritage item	Discussion
	<b>The proposal will have a neutral impact on the setting, views and vistas of Anzac Bridge.</b>
<b>Glebe Island Bridge</b> (SHR)	<p>Removal of Buildings A, B and C at 1–3 Bank Street will result in the minor loss of the industrial setting of Glebe Island Bridge to the southeast. Retention of Building D along Bank Street will assist in preserving this industrial setting, as seen in the render of View 4 (Figure 4.10). This will result in a minor adverse impact to the bridge.</p> <p><b>The proposal will have a minor adverse impact on the setting, views and vistas of Glebe Island Bridge that is acceptable.</b></p>
<b>1–3 Bank Street, Pyrmont</b> (Unlisted)	<p>The proposal will alter views to and from 1–3 Bank Street by demolition of three of the four buildings on the site. This will alter views of the site from Bank Street, though retention of Building D will retain a key street-facing element of the site. Views from the opposite shore of Blackwattle Bay will be affected, though these views are currently obscured by the boats stored at the marina.</p> <p>Internal views of the site will be lost by the demolition of Buildings A, B and C, resulting in impacts to the ability to interpret the site as a former maritime industrial complex. Refer to view 6 (Figure 4.12).</p> <p>This is partially mitigated by the retention of Building D and the use of the truncated walls of Building A’s northernmost rooms to create an Interpretation Garden. While views are contributory to the interpretability of the site as a maritime industrial complex, the significance of the site is primarily derived from its historical use, association and rarity.</p> <p>Alteration to the setting of 1–3 Bank Street will have a neutral impact.</p> <p><b>The proposal will have a moderate adverse impact on the setting, views and vistas of 1–3 Bank Street.</b></p> <p><b>The impact is partially mitigated by the re-use of Building D and integration of Building A elements into an Interpretation Garden.</b></p>

## 5.2.3 Landscape

The impacts of landscape works to heritage items within the site are discussed in Table 5.4 below.

Table 5.4 Assessment of impacts of the landscape works in the proposal.

Heritage item	Discussion
<b>Anzac Bridge</b> (Transport for NSW [Roads and Maritime] S170 register)	<p>The ground level around the eastern pylon of Anzac Bridge will be raised by 0.5m. The surface finish will be like the existing surface. This will have a negligible impact on the bridge, affecting a miniscule part of the overall structure with no impact to its heritage values.</p> <p>Similarly, landscape changes around the piers on the southeast boundary will alter their setting, though it will not affect the heritage values of the bridge.</p> <p><b>The proposal will have a neutral impact to Anzac Bridge.</b></p>
<b>Glebe Island Bridge</b>	Landscape works to 1A Bank Street and 1–3 Bank Street will open views to and from the foreshore from Glebe Island Bridge. Removed trees will be

Heritage item	Discussion
(SHR)	replaced, maintaining the present vegetated character to this edge of the bridge. The removed trees are not of heritage significance. <b>The proposal will have a neutral impact to Anzac Bridge.</b>
<b>1–3 Bank Street, Pyrmont</b> (Unlisted)	The proposal will significantly alter the landscape of 1–3 Bank Street through the removal of Buildings A, B and C and the construction of a new building. The ground level will be lowered and ramps introduced beside the water to provide access to the marina, including a section over the water.  The significance of 1–3 Bank Street is primarily derived from the historical use of the site. While the present internal courtyard will be removed, it will be replaced by a landscape courtyard between Building D, the Interpretation Garden and the new building. The impacts of the proposal on the landscape of 1–3 Bank Street is therefore lessened.  <b>The proposal will have a minor adverse impact to the landscape of 1–3 Bank Street.</b>

## 5.2.4 Use

The impacts of change of use to heritage items within the site are discussed in Table 5.5 below.

Table 5.5 Assessment of impacts of change of use in the proposal.

Heritage item	Discussion
<b>Anzac Bridge</b> (Transport for NSW [Roads and Maritime] S170 register)	No change
<b>Glebe Island Bridge</b> (SHR)	No change
<b>1–3 Bank Street, Pyrmont</b> (Unlisted)	Building D of 1–3 Bank Street will be adaptively re-used as an amenities and storage building. Historically the building was likely used as an office for the site and was most recently converted to residential use. The existing use of the building does not contribute to its significance. Use for amenities and bathrooms is compatible with its significance and will enable the retention of the building.  The change of use will require modification of the building. The impacts of this are discussed in Section 5.2.1. Impacts will also be mitigated by interpretation of the past significant uses of the building at the site.  <b>The proposed change of use will have a neutral impact to 1–3 Bank Street.</b>

## 5.2.5 Demolition

The impact of demolition to heritage items within the site is discussed in Table 5.6 below.

Table 5.6 Assessment of impacts of demolition in the proposal.

Heritage item	Discussion
<p><b>Anzac Bridge</b> (Transport for NSW [Roads and Maritime] S170 register)</p>	<p>The proposal will not demolish Anzac Bridge.</p>
<p><b>Glebe Island Bridge</b> (SHR)</p>	<p>The proposal will not demolish Glebe Island Bridge.</p>
<p><b>1–3 Bank Street, Pyrmont</b> (Unlisted)</p>	<p>The proposal will demolish three of the four buildings of 1–3 Bank Street. This site is unlisted but has been identified as having local significance as a rare historical maritime industrial complex in Blackwattle Bay associated with the Cam &amp; Sons fishing trawler business. The significance of the site is primarily associated with its historical use by Cam &amp; Sons. While the buildings have some significance as rare maritime industrial remnants, they have been modified, affecting their integrity.</p> <p>Demolition of three of the four buildings is part of the broader scope of works for the Bank Street Park and will assist in achieving the objectives of the proposal. Demolition of the buildings will support the creation of the park, the primary objective of the proposal, by integrating 1–3 Bank Street into the wider site. Demolition will allow the construction of a new purpose-built building that will meet the objective to provide community facilities, marina offices and storage in the park. Demolition of Building A will also open up the part of the site with the best solar access, meeting a key requirement of the proposal.</p> <p>During design development, GML assessed the heritage significance of 1–3 Bank Street and analysed the suitability of the buildings for re-use. Refer to Section 4.3.2 for this discussion.</p> <p>GML’s analysis concluded that Buildings B and C are not suitable for retention and re-use. Building A was considered suitable for retention. It is proposed for demolition to maximise solar access to the park and allow the integration of the 1–3 Bank Street site into the wider Bank Street Park. The south elevation of Building A, a double-height brick wall, presents a significant constraint on these objectives and would effectively sever 1–3 Bank Street from the rest of the park, if retained.</p> <p>Building D is proposed to be adaptively re-used in the park for amenities and storage. It is proposed to demolish and rebuild the south elevation of the building to accommodate its new use. The roof is also proposed to be demolished and replaced. A structural condition assessment identified that the roof may need replacement due to the poor condition of visible timber structural members in the building.</p> <p>Further investigation into the viability of the present roof structure is recommended as retaining the roof may assist in mitigating the impact of its partial demolition. Demolition of the south elevation of Building D may make replacing the roof unavoidable.</p>

Heritage item	Discussion
	<p>Demolition of Buildings A, B and C and the south elevation of Building D at the site will have a substantial impact on the integrity and interpretability of the site as a historical maritime industrial complex. This will result in a major adverse impact to its heritage significance. However, considering that Building C is of little significance and Building B has diminished usability, the demolition of these buildings is considered acceptable.</p> <p>To partially mitigate the impact of the demolition of Building A, an Interpretation Garden which incorporates the truncated walls of the northernmost rooms of the building is proposed. This will include garden beds, seating and interpretation which will allow park users to appreciate a part of the structure which was once there. GML considers this an acceptable solution to achieve the objectives of the proposal and acknowledge the significance of the buildings.</p> <p>Further interpretation of the significance of 1–3 Bank Street and Cam &amp; Sons is proposed in the park.</p> <p>It is recommended that expert heritage and structural advice is sought during detailed design development and construction. This is to ensure impacts to Building D are minimised and that the retention of the truncated walls of Building A is successful.</p> <p>Limited internal significant fabric was identified in Buildings A, B and C. It is recommended that a salvage schedule is prepared to salvage and re-use remaining significant fabric in the design and interpretation. GML recommends the following elements be salvaged at a minimum, though others may be identified*:</p> <ul style="list-style-type: none"> <li>• External sliding door to Room A2 (Figure 2.24)</li> <li>• Original electrical conduit box in room A5 (Figure 2.34)</li> <li>• ‘Workshop’ sign and timber rack over the roller door on the south elevation of Building B (Figure 2.30)</li> </ul> <p>*GML understands that the coal loader crane and bucket stored in the courtyard (Figure 2.26 and Figure 2.31) are related to the New Sydney Fish Market development and are being stored temporarily at 1–3 Bank Street. It will be reused in a public art commission for that project.</p> <p><b>The proposed demolition will have a major adverse impact to the heritage values of 1–3 Bank Street without mitigation.</b></p> <p><b>The impact is partially mitigated by the re-use of Building D and integration of Building A elements into an Interpretation Garden. It is considered acceptable for items of moderate significance in the context of the changing use of the site.</b></p>



## 5.2.6 Curtilage

The impacts of the proposal to the curtilage of heritage items within the site are discussed in Table 5.7 below.

Table 5.7 Assessment of the impacts of the proposal on curtilage.

Heritage item	Discussion
<b>Anzac Bridge</b> (Transport for NSW [Roads and Maritime] S170 register)	The proposal will introduce new structures around the eastern pylon of the bridge. A sufficient curtilage of approximately 5m will be maintained around the pylon at ground level to ensure access for maintenance is maintained. No impact is anticipated as a result.  <b>The proposal will have a neutral impact to the curtilage of Anzac Bridge.</b>
<b>Glebe Island Bridge</b> (SHR)	The proposal will not affect the curtilage of Glebe Island Bridge.
<b>1–3 Bank Street, Pyrmont</b> (Unlisted)	The proposal will integrate the 1–3 Bank Street site into the adjacent land to create the park landscape. The curtilage of the site as defined by the building forms will be altered through their removal. This will impact the legibility of the place as a compact maritime industrial complex. This will have a moderate adverse impact to the place but is partially mitigated by retention of Building D and interpretation of the northernmost rooms of Building A. These will retain some semblance of the former curtilage of the site and may be enhanced by interpretation of the form of Buildings B and C in the ground plane.  <b>The proposal will have a moderate adverse impact to the curtilage of 1–3 Bank Street.</b>  <b>The impact is partially mitigated by the re-use of Building D and integration of Building A elements into an Interpretation Garden.</b>

## 5.2.7 Moveable heritage

The proposal will not impact moveable heritage as none was identified at the subject site.

It is recommended that significant fabric of the demolished buildings is salvaged and re-used. See Section 6.1 Recommendations.

## 5.2.8 Aboriginal cultural heritage

Heritage items in the site do not have Aboriginal cultural heritage values identified in their listings.

GML has prepared a separate Aboriginal Cultural Heritage Assessment Report (ACHAR) to identify and understand the significance of the site to First Nations people. Refer to this report for a discussion of these values.

## 5.2.9 Historical archaeology

GML has prepared a separate Historical Archaeological Assessment (HAA) to accompany the SSDA for Bank Street Park. The assessment found an overall moderate–high potential for historical archaeological remains associated with the development of the site from the c1840s. There is a high potential for remnants of the late nineteenth/early twentieth-century seawall.

The proposal requires excavation and ground disturbance, including the demolition of three of the extant buildings at 1–3 Bank Street (Buildings A, B and C). Some of the footings of Building A (fronting Bank Street) are to be retained in a new planting/seating area. **The proposal has potential to impact on archaeology of local significance.**

### Maritime

Comber Consultants (Comber) has prepared a separate Maritime Archaeological Assessment (MAA) to accompany the SSDA for Bank Street Park. The assessment found a low potential for maritime archaeological remains associated with nineteenth-century wharves, which would be of local significance.

Comber assessed the risk of significant heritage impacts as low but noted that, in the absence of seabed survey data for the site, the presence of remains or degree of impact cannot be established. Comber recommends a survey of the Harbour bed prior to works to provide information which could inform this assessment.

## 5.2.10 Natural heritage

The proposal will not impact natural heritage values as there are no such identified values in or near the subject site.

## 5.2.11 Conservation areas

The subject site is not part of a conservation area. Refer to Section 5.2.13 for the impact assessment of the project on conservation areas in the vicinity.

## 5.2.12 Cumulative impacts

The Bank Street Park proposal is part of the wider redevelopment of the Blackwattle Bay State Significant Precinct. This will result in the transformation of the area from its present industrial character into a residential and mixed-use precinct.

The impacts to 1–3 Bank Street arising from the proposal will result in the further loss of the historical industrial character of Blackwattle Bay. This has been an ongoing historical

process since the deindustrialisation of Sydney’s Inner Harbour in the late twentieth century.

GML considers the cumulative impact of the proposal to be acceptable, with the alternative of retention ‘as-is’ likely resulting in the site’s continued disuse and deterioration. Adaptation of the buildings to new uses would require extensive repairs and conservation as significant structural, access and termite issues have been identified in all buildings.

The current proposal of a public park and new mixed community facilities represents a significant ongoing public benefit.

While the loss is a heritage impact, examples throughout Pyrmont demonstrate that the redevelopment and adaptive re-use of former industrial sites can produce interesting and high-quality spaces of public benefit. Interpretation of the history of the site can successfully communicate its values to the wider community in a way which respects its history and adapts the site for contemporary use.

The design will re-use and adapt elements of the buildings at 1–3 Bank Street to a high-quality public space that is similar to other examples of former Harbour industrial sites. A Preliminary Heritage Interpretation Framework (PHIF) (GML 2023) has also been developed for the Bank Street Park project to interpret the site’s history and significance. Both will assist in mitigating the impact of the proposal, while enabling the maritime industrial character of the site to be communicated.

### 5.2.13 The Conservation Management Plan

The consistency of the proposal against the relevant policies of the Glebe Island Bridge CMP (NSW Public Works Authority, 2022) is assessed in Table 5.8 below. No CMP was identified for Anzac Bridge or 1–3 Bank Street.

Table 5.8 Consistency with the Glebe Island Bridge CMP policies.

Policy No.	CMP policy	Consistency assessment
7.1	<p><b>Setting, views and curtilage</b></p> <p>The setting and key views of Glebe Island Bridge should be maintained through:</p> <ul style="list-style-type: none"> <li>retention of significant visual and physical links to, from and across the site and the surrounding water spaces;</li> <li>retention of significant views to and from Glebe Island Bridge;</li> <li>retention of Glebe Island Bridge as a land crossing and maritime thoroughfare with clear and legible historic circulation pathways; and</li> </ul>	<p>View from the foreshore to the bridge will be enhanced by the removal of buildings at 1–3 Bank Street. These buildings currently impede views to the bridge from the southeast. Their removal will provide more close-up views from the public domain in the southeast.</p> <p>Proposed roadworks will maintain the historical</p>

Policy No.	CMP policy	Consistency assessment
	<ul style="list-style-type: none"> <li>retention and enhancement of the distinctive industrial maritime character and setting of Glebe Island Bridge, as part of the working harbour.</li> </ul>	<p>circulation pathways of the bridge.</p> <p>Removal of three of the buildings at 1–3 Bank Street will result in the minor loss of the industrial maritime character of the bridge’s setting. However, retention of the seawall and Building D will enable this historical setting to be interpreted.</p>
12.1	<p><b>New work</b></p> <p>It is recognised that changes to the place are required to facilitate the long-term use of the bridge and its embankments. New work to existing heritage places must be sympathetic to the appearance and character of the extant significant fabric. Where it is necessary to reconstruct fabric that is missing or damaged beyond repair, the new work must be distinguishable as such on close inspection. Where other changes are proposed that do not relate to the restoration or reconstruction of historic fabric, a contemporary aesthetic that is sympathetic to the historic detailing is acceptable.</p>	<p>The proposal will make minor changes to footpaths, kerbs and the road network on Bank Street as it joins Glebe Island Bridge.</p> <p>Works will occur on the eastern abutment of the bridge, affecting fabric of little significance.</p> <p>Works are minor and consistent with the existing road surface. No impact is anticipated.</p>

## 5.2.14 Other heritage items in the vicinity

The impacts of the proposal to the views, vistas and setting of heritage items in the vicinity is discussed in Table 5.9 below.

Table 5.9 Impacts to the views, vistas and setting of heritage items in the vicinity of the site.

Heritage Item	Listing details	Heritage impact assessment
<b>Local heritage conservation area (HCA)</b>		
<i>Glebe Point</i>	<i>Sydney Local Environmental Plan (Sydney LEP) C28</i>	<p><b>Neutral</b></p> <p>The proposal will result in some loss of the industrial setting of Glebe Point HCA. However, the design has selected materials which complement the existing conditions of Blackwattle Bay and will result in additional open green space being introduced (View 1, Figure 4.7). Views to the site are also constrained through much of</p>



Heritage Item	Listing details	Heritage impact assessment
		<p>the area due to the existing built form. This will make the modifications barely noticeable; they will have a negligible impact on the setting.</p> <p>No impact is anticipated.</p>
<i>Glebe Point Road</i>	<i>Sydney LEP C29</i>	<p><b>Neutral</b></p> <p>This HCA has oblique and filtered views of the site. Conversion of the site into a park will have no impact on its significance.</p>
<i>Lyndhurst</i>	<i>Sydney LEP C31</i>	<p><b>Neutral</b></p> <p>The site is a significant distance from the Lyndhurst HCA with modifications being barely noticeable and having a negligible impact on its setting.</p> <p>Visibility may also be affected by the New Sydney Fish Market development.</p> <p>No impact is anticipated.</p>
<i>Annandale Heritage Conservation Area</i>	<i>Inner West LEP C1</i>	<p><b>Neutral</b></p> <p>The site is a significant distance from the Annandale HCA with modifications being barely noticeable and having a negligible impact on its setting.</p> <p>No impact is anticipated.</p>
<b>Local heritage item</b>		
<i>Escarpment Face from former "Saunders' Quarry"</i>	<i>Sydney LEP I1199</i>	<p><b>Neutral</b></p> <p>Modifications to the site will not reduce the visibility of the escarpment rock face. While there will be some loss of its industrial setting, this will not impact the significance of this item.</p>
<i>Blackwattle Bay Park</i>	<i>Sydney LEP I649</i>	<p><b>Neutral</b></p> <p>The proposal will result in some loss of the industrial setting of the item. However, the design has selected materials which complement</p>

Heritage Item	Listing details	Heritage impact assessment
		<p>the palette of Blackwattle Bay Park (View 1, Figure 4.7). The park will result in the same re-use of former industrial land for parkland as Blackwattle Bay Park, being highly compatible with its significance.</p> <p>No impact is anticipated.</p>
<p><i>Former incinerator &amp; semi-circular stone structure</i></p>	<p>Sydney LEP I683</p>	<p><b>Neutral</b></p> <p>The proposal will result in some loss of the industrial setting of the item. However, the design has selected materials which complement the existing conditions of Blackwattle Bay and will result in the introduction of additional open green space (View 3, Figure 4.9). The modifications will be barely noticeable and have a negligible impact on its setting.</p> <p>No impact is anticipated.</p>
<p><i>Pope Paul VI Reserve</i></p>	<p>Sydney LEP I647</p>	<p><b>Neutral</b></p> <p>This item has oblique, distant views of the site (View 2, Figure 4.8). Conversion of the site into a park will have no impact on its setting or significance.</p> <p>No impact is anticipated.</p>
<p><i>Kauri Foreshore Hotel</i></p>	<p>Sydney LEP I657</p>	<p><b>Neutral</b></p> <p>No impact is anticipated.</p>
<p><i>Warehouse 'Greens Woolstore'</i></p>	<p>Sydney LEP I658</p>	<p><b>Neutral</b></p> <p>No impact is anticipated.</p>
<p><i>House 'The Retreat'</i></p>	<p>Sydney LEP I791</p>	<p><b>Neutral</b></p> <p>This item has only oblique views of the site which are heavily screened by the vegetation of Blackwattle Bay Park and Bellevue.</p> <p>No impact is anticipated.</p>
<p><i>Iron/sandstone palisade fence</i></p>	<p>Inner West LEP I11</p>	<p><b>Neutral</b></p> <p>No impact is anticipated.</p>

Heritage Item	Listing details	Heritage impact assessment
<i>Street Trees – row of Brush Box</i>	<i>Inner West LEP I10</i>	<b>Neutral</b> No impact is anticipated.
<b>State agency's S170 heritage and conservation register</b>		
<i>White Bay Power Station (outlet) Canal</i>	<i>Sydney Ports Corporation S170 register 4560026</i>	<b>Neutral</b> Views to the site from this item are heavily screened by buildings and marinas on Glebe Island and in Rozelle Bay. No impact is anticipated.
<b>State environmental planning policy</b>		
<i>Glebe Island Wheat Silos A, B, C</i>	<i>SEPP (Precincts—Eastern Harbour City) 2021 1A, 1B, 1C</i>	<b>Minor Adverse</b> The proposal will result in a minor loss of the silos' industrial setting, which will have a negligible impact. No impact is anticipated.
<i>Railway Truss Bridge, Johnston Street</i>	<i>SEPP (Precincts—Eastern Harbour City) 2021 5001333</i>	<b>Neutral</b> No impact is anticipated.
<b>State Heritage Register</b>		
<i>Bellevue</i>	SHR 00470	<b>Neutral</b> The impacts of the proposal to Bellevue are similar to those of Blackwattle Bay Park. The proposal will result in some loss of the industrial setting of the item. However, the design has selected materials which complement the existing conditions of Blackwattle Bay and will result in additional open green space being introduced (View 1, Figure 4.7). This will result in a negligible difference to the setting of Bellevue. No impact is anticipated.
<i>Pyrmont and Glebe Railway Tunnels</i>	SHR 01225	<b>Neutral</b> No impact is anticipated.

Heritage Item	Listing details	Heritage impact assessment
<i>Glebe and Wentworth Park Railway Viaducts</i>	SHR 01034	<b>Neutral</b> No impact is anticipated.
<i>White Bay Power Station</i>	SHR 01015	<b>Neutral</b> Views to the site from this item are heavily screened by Anzac Bridge and structures on Glebe Island. While the proposal will result in a minor loss of the industrial setting of the power station, its visual connection is so constrained that no impact is anticipated.
<b>Unlisted</b>		
<i>Ferry Road, Glebe Boatsheds: University of Sydney Rowing Club and Glebe Rowing Club</i>		<b>Neutral</b> No impact is anticipated. This item has oblique views of the site (View 3, Figure 4.9). Conversion of the site into a park will have no impact on its setting or significance. No impact is anticipated.

### 5.2.15 Commonwealth/National Heritage significance

The proposal does not impact a place listed on the Commonwealth Heritage or National Heritage lists.

### 5.2.16 World Heritage significance

The proposal does not impact a World Heritage property.

## 5.3 Endnotes

<sup>1</sup> Department of Planning and Environment 2023, *Guidelines for preparing a statement of heritage impact*, Environment and Heritage, Department and Planning and Environment.



## 6 Summary and recommendations

This report has identified the impacts of the proposal to listed and unlisted heritage items within the 1A–19 Bank Street, Pyrmont site and in the vicinity. Based on the findings of this report, the following conclusions are made:

- 1–3 Bank Street is an unlisted heritage item. It has been found to have heritage significance at a local level as a rare example of an early twentieth-century maritime industrial complex in Blackwattle Bay.
- Further investigations by GML have identified that 1–3 Bank Street has undergone multiple changes and that the fabric of 1–3 Bank Street is of a moderate or low level of significance.
- Options to retain the buildings were explored but discounted for a range of reasons (refer to section 4.3.2).
- The proposal will demolish Buildings A, B and C of 1–3 Bank Street and adaptively re-use Building D. This will have a major adverse impact on the assessed heritage significance of the complex.
- GML considers the impacts of the proposal to be acceptable from a heritage perspective. This is in light of the constraints on retaining the buildings and the low significance of Building C and Building B’s poor viability for re-use. Retention of Building A would also place significant constraints on the design of the park and impede the achievement of several key project objectives.
- The impact of the proposal is partially mitigated by the adaptive re-use of Building D and the retention of elements of Building A within an Interpretation Garden. The salvage and re-use of significant fabric and interpretation of the site will provide further partial mitigation of this impact.
- The renewal of the site as a public park will also provide greater benefits than retaining it ‘as-is’, with 1–3 Bank Street having poor prospects for adaptive re-use if the proposal does not proceed. The buildings at 1–3 Bank Street are in poor condition and not fit for purpose as they require repairs and upgrades. This would be for limited commercial or public benefit in the context of the newly rezoned Blackwattle Bay Precinct and is not commensurate with the desired future character of the area.
- While the loss is a heritage impact, examples throughout Pyrmont demonstrate that the redevelopment and adaptive re-use of former industrial sites can produce interesting and high-quality spaces of public benefit. The interpretation of the history of the site can successfully communicate its values to the wider community in a way which respects its history and adapts the site for contemporary use.
- The proposed adaptive re-use of Building D involves works that will have an adverse impact on the building that are considered acceptable in the context of the evolution

of the site. Further mitigation of the impact may be possible by following the polices below that are related to the retention of as much original fabric as possible.

- The proposal will result in a neutral impact to Anzac Bridge.
- The proposal will result in a neutral impact to the setting of Glebe Island Bridge.
- The proposal will have a neutral impact to heritage items in the vicinity of the site.

## 6.1 Recommendations

To help minimise or mitigate the identified heritage impacts of the proposal, GML provides the following recommendations:

### **Recommendation 1—Building D, 1–3 Bank Street**

- Technical advice should be sought to identify whether Building D’s existing roof structure is viable and could be repaired and re-used in lieu of replacement.
- Options to fully or partially retain the southern elevation of Building D should be explored at the detailed design stage in conjunction with a suitably qualified and experienced heritage consultant/architect.
- A schedule of conservation works should be prepared for Building D. Original timber sash windows and doors should be retained and repaired wherever possible and replacements with modern windows should be avoided. Brickwork of the street-façade should be repaired and made good.

### **Recommendation 2—Interpretation Garden design development**

- Explore options to retain more fabric of Building A in the Interpretation Garden during design development in conjunction with a suitably qualified and experience heritage consultant/architect. Options to fully retain whole elements, such as doorways, walls and the steel-framed windows, should be prioritised ahead of retaining only some of these elements to improve the experience of the ‘ruins’ while enabling easy movement and use.
- The advice of a structural engineer should be sought to ensure that retention of more elements is viable.

### **Recommendation 3—heritage advice**

- A suitably qualified and experienced heritage consultant / heritage architect should provide ongoing heritage advice during the design development and construction phases of the Bank Street Park project. The objective of this heritage advice will aim to minimise the loss of fabric of Buildings D and A.
- The heritage consultant/heritage architect should also provide advice on the appropriate treatment of the seawalls where they are proposed to be modified.

#### **Recommendation 4—salvage of significant fabric**

- A salvage schedule should be prepared during the detailed design stage identifying early or original fabric at 1–3 Bank Street that should be salvaged for re-use in the design or interpretation in Bank Street Park. This includes the following elements:
  - Brick masonry of demolished buildings
  - External sliding door to Room A2 (Figure 2.24)
  - Original electrical conduit box in room A5 (Figure 2.32)
  - Workshop sign and timber rack over the roller door on the south elevation of Building B (Figure 2.30)
- Opportunities to identify how and where salvaged material might be used should be developed in conjunction with the design team during the preparation of the salvage schedule.
- The salvage schedule should provide clear guidance on the appropriate removal, storage and re-use of salvaged material. This includes how these items will be catalogued and stored during construction.

#### **Recommendation 5—materiality**

- The new community facilities and marina office / storage building should be designed to be sympathetic to the materiality of Building D and Glebe Island Bridge. Its façade should be predominantly masonry brick or sandstone.

#### **Recommendation 6—archival recording**

- An archival recording of 1–3 Bank Street and the areas around Glebe Island Bridge and Anzac Bridge should be undertaken prior to the commencement of works. The recording should be conducted in accordance with the appropriate Heritage NSW guidelines for archival recordings.

#### **Recommendation 7—interpretation**

- Proposed works should include heritage interpretation of the industrial history of the site and specifically 1–3 Bank Street. An interpretation plan should be developed during design development which builds on the work of the PHIF (GML 2023). The plan must consider how interpretation will be implemented in accordance with the appropriate Heritage NSW guidelines for heritage interpretation.
  - Interpretation should consider how and where the demolished buildings at 1–3 Bank Street can be interpreted, especially in the proposed interpretation garden.
  - Interpretation should consider how salvaged material identified in the salvage schedule can be re-used in interpretation.

# 7 Appendices

## Appendix A







Mapping of heritage items in the vicinity

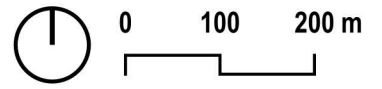


# Bank Street Park

## Identified Heritage Assets

### Key

-  Study Area
-  State Heritage Item
-  Section 170 Heritage Item
-  LEP Heritage Conservation Area - General
-  LEP Heritage Item - General
-  LEP Heritage Item - Landscape



Projection: GDA94 MGA 56

