



Architectural Design Statement

State Significant Development
Compass 2 Warehouse & Distribution Centre
Revision C

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Document Control

Project | Compass 2 Warehouse & Distribution Centre
Project Address | Lot 1, DP1274322, Eastern Creek Drive
Eastern Creek NSW 2766
(Blacktown Local Government Area)
Document | Architectural Design Statement
Client | Charter Hall
Revision | N/A
Date | 26 November 2021
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Introduction

1.0 Introduction

This architectural design statement forms part of the Environmental Impact Statement (EIS) made by Charter Hall to the Department of Planning, Industry and Environment NSW (DPIE) for SSD-30923027.

The design statement has been prepared as part of the above mentioned submission and is to be read alongside Qanstruct's architectural documentation 4387.21 / TP.01-TP.10 inclusive all revision B.

This design statement should be read in conjunction with the Landscape Design Statement, Traffic Report, Civil Engineering details and other consultants reports that also address Urban Design and Visual items of the SEARs.

1.1 Project Description

Construction and 24/7 operation of a warehouse and distribution centre at Lot 1 Eastern Creek Drive, Eastern Creek, comprising:

- Earthworks involving cut and fill works;
- Site preparation works and servicing;
- Warehouse, main office, dock office, loading docks, carparking, forklift charging room;
- External hardstands and landscaping

As per Qanstruct's architectural documentation 4387.21 / TP.01-TP.10 inclusive all revision B.

Introduction

1.2 Compliance Table

The proposal responds to the Planning Secretary's Environmental Assessment Requirement (SEARs) issued by the DPIE specifically addresses the 'Built Form and Urban Design' item, as well as the 'Design Quality' items raised by the following:

Key Issue No. and Description	Issue and Assessment Requirements	How it is Addressed	Section of this Report
Issue 3 Design Quality	Demonstrate how the development will achieve: <ul style="list-style-type: none">• Design excellence in accordance with any applicable EPI provisions• Good design in accordance with the seven objectives for good design in Better Placed	The report makes an assessment on how the design specifically responds the seven objectives in Better Placed. Read in conjunction with architectural documentation.	Section 3.0
Issue 4 Built Form and Urban Design	Explain and illustrate the proposed built for, including a detailed site and context analysis to justify the proposed site planning and design approach.	The report illustrates a site analysis diagram alongside a detailed description to demonstrate the industrial context of the site to justify the use of this type of development.	Section 2.0, 2.1
	Demonstrate how the proposed built form (layout, height, bulk, scale, separation, setbacks, interface and articulation) addresses and responds to the context, site characteristics, streetscape and existing future character of the locality.	The architectural documentation demonstrates the actual heights, setbacks and site characteristics of this proposal. This report details how some potential issues relating to bulk and scale is managed, and its impact on the streetscape.	Section 2.1
	Demonstrate how the building design will deliver a high-quality development, including consideration of the façade design, articulation, materials, finishes, colours, any signage and integration of services.	The architectural documentation shows the articulation of the external face of the building, with detailed descriptions of the materials, finishes and colours used. The report addresses the design thinking behind these decisions	Section 2.2
	Assess how the development complies with the relevant accessibility requirements.	Refer to Accessibility Report.	Refer to Accessibility Report

Project Description & Proposal

2.0 Project Site Context and Proposal

The subject site is located within Western Sydney Employment Area (WSEA) and is approximately 50km west of Sydney Central Business District (CBD). In total the site consists of approximately 4.8ha of land and is irregularly shaped. The subject site is located within Blacktown LGA and is zoned IN1 General Industrial.

The site as a frontage of approximately 48m along Eastern Creek Drive to the south west, and is roughly 240m – 300m wide. The subject site also adjoins the future extensions of Honeycomb Drive extension currently under assessment by Council as part of DA.20-01689.

Via old Wallgrove Road, the site is also connected to the Westlink M7 corridor and Western Motorway M4 interchange. Public transport is serviced by various bus stops along old Wallgrove Rd, connecting the site to the neighbouring suburbs of Mount Druitt, Rooty Hill, St Marys and Wetherill Park.

The surround area is of an industrial nature, with majority of buildings conducting operations relating to freight transport services, as well as warehouse supply and distribution.

The intended use and design continues to provide a high quality general industrial, light industrial; warehouse and distribution and ancillary office land uses which complements the surrounding industrial context of Eastern creek. The proposal consists of 21,850sqm of Warehouse space, (including Forklift Charging Area) and a total of 1,750sqm of ancillary office GFA.

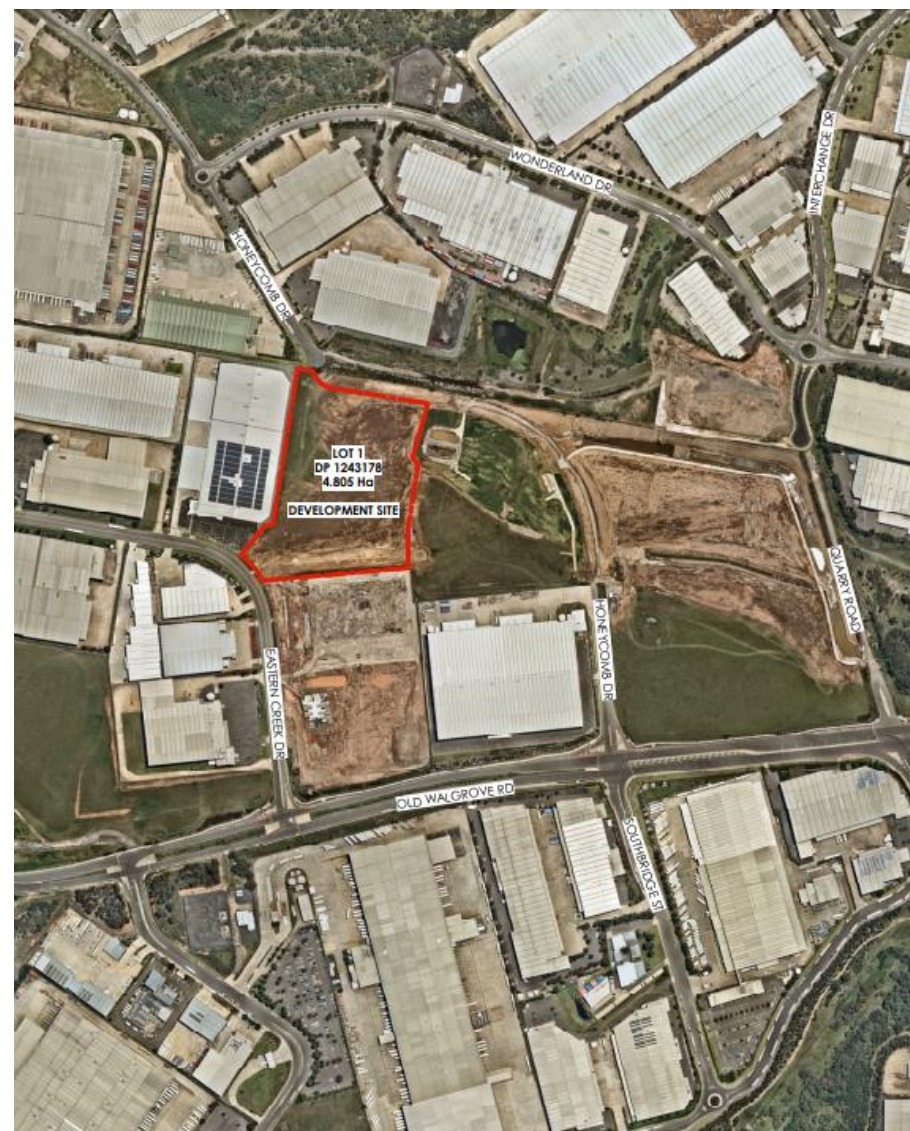


Figure 1 – Locality Plan – Eastern Creek Drive

Site Layout

2.1 Site Layout

The proposal consist of a temperature-controlled warehouse distribution facility 201,850sqm of warehouse space and 1,750sqm of ancillary office space.

There is an existing stormwater drainage easement and overland flow path running along the southern boundary, with a proposed electrical easement running along the western boundary supplying power to the site and the adjacent neighbouring property to the north. The site falls approximately 4m from the western boundary to the south-eastern side of the site to accommodate for overland flow path requirements.

The scheme takes into account building design and storage, staging, site coverage, street setbacks, easements, overland flow paths, landscaping, height, colour, scale, materials and finishes, visual façade treatments, signage and lighting as part of the application.

The building is primarily accessed from Eastern Creek Drive, with the intention of providing access from the Honeycomb Drive extension in the future. A temporary crossover to Honeycomb Drive cul-de-sac facilitates trucks exiting from the site until the extension is built. The proposal separates truck and car traffic, with a one-way thoroughfare for trucks loading onto the hardstand for the increased safety of people and vehicles across the site.

The building has minimal presence from Eastern Creek Drive and Honeycomb Drive. The hardstand location faces west towards the adjoining property, with the two-level main office fronting majority of the view from Eastern Creek Drive. The office is setback 40m from Eastern Creek Drive, providing suitable landscape treatment to lessen the impact of the warehouse façade at street level. Industrial plants, storage and waste storage areas associated with tenant operations are placed away from the street frontages providing a high-quality presentation to the streetscape.

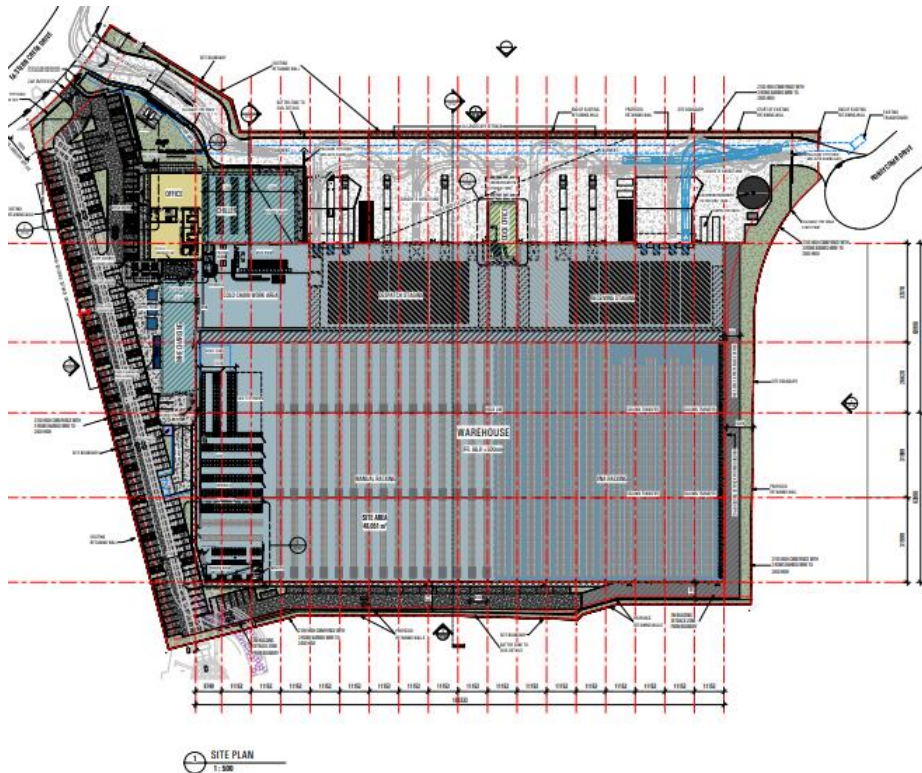


Figure 2 – Proposed Site Plan

Site Layout

2.1 Site Layout

The setbacks along Eastern Creek Drive and Honeycomb Drive integrate dense plants of native endemic species to create a natural setting of varying height and depth. Refer to Landscape Architects plans for details. Street fronting staff and visitor parking (including accessibility) are located. A pedestrian foot path is provided to the main office from Eastern Creek drive to aide pedestrians navigate the site safely.

Each building has been provided with staff outdoor open space areas adjacent to each office facility with landscaped green areas. Street front setbacks have been respected providing landscaped visual buffer zones along Eastern Creek Drive and Honeycomb Drive. The dense natural landscape proposed, screens the buildings facades along with the streetscape. Refer to landscape architecture's details for landscaped zones.

Building Heights / Bulk / Scale

Building height is set at 21.46m top of ridge, with eaves sitting 17-18m high. This allows for high bay racking systems suitable for warehousing storage required by tenant. The facility requires HVAC units to be installed on the roof which sit at approx. 2.0m high.

The perceived bulk of the building is managed by the existing levels on the site as the building sits roughly 2m below the neighbour to the west. The short street frontages and increased building setbacks also allows for the building to sit comfortably amongst the surrounding buildings.

Building Setbacks

Building setbacks follow or exceed the required setback along street frontages. Side and rear setbacks vary and allow for fire trucks access around buildings as required by BCA requirements for Large Isolated buildings and DCP requirements for Development in Industrial Areas.

Landscape setback / buffer zones are as follows:

Eastern Creek Drive – 10m Landscape and Building Setback
Honeycomb Drive – 10m Landscape and Building Setback
Neighbouring Property Boundaries – 3m



Figure 3 – Southern Boundary showing 6m high retaining wall

Access and Carparking

Access for heavy vehicles to loading hardstand and waste collection areas are allocated away from street fronts. All carparking access and heavy vehicle access and separate from each other for safety and minimizing the congestion of traffic flow across the estate. Parking rates are calculated as per council's requirements (refer Traffic Consultants report) and numbers provided for each facility meet requirements.

Refer to Traffic Consultant's report for details, including addressing linkage to surrounding public transport, cycling and pedestrian networks.

Waste Location Area

On-site waste areas are limited to hardstand areas. Ongoing waste management will be controlled by individual building users via private contractor.

Water Reuse and Stormwater Management

All buildings are provided with water recycling and reuse tanks. Refer to Hydraulic / Civil engineer's details for site stormwater collection, onsite detention systems and overland flow paths to mitigate onsite and localized flooding.

Building Design

Lighting

Lighting is to be provided with a combination of light poles and building mounted lighting around the site for on-site security and safety. Lighting is to be positioned to shine inward onto the site to minimize the light spillage onto adjoining properties. The layout of the buildings and loading areas will ensure neighbouring properties will not be affected by light spill.

2.2 Building Design

Building Finishes

The external building façade features a neutral grey palette along a series of PIR panels typically required for temperature controlled facilities. The warehouse utilises alternating colours to visually break up the warehouse length façade components. The colours of the building alongside the adjacent soon-to-be constructed Data Centre to the south will be uniform to create an identifiable consistency across the estate, with feature 'blue' panels used minimally to create interest. The dominance of materiality will be softened over time when the landscape is expected to be at full maturity within the setbacks and carparking areas. The nature of this treatment involves using screening elements that integrate with above mentioned materials and the proposed landscape design to create a visual dialogue that is experienced by both the scale of the buildings and that of the pedestrian level. This neutral approach uses whites, greys and darker highlighting tones. The main office features a combination of precast concrete panels, prefabricated perforated metal cladding and performance glazing in aluminium framing.

All downpipes and services 'blend' into the building façade and talks to the architectural elements and palette the building.

Signage

Signage will be considered on an estate wide basis such that there will be a consistency in materials and finishes of the signs across the estate. Signage will be a combination of building mounted signage, estate and tenant identification signage in landscape setbacks, at access road and driveway entries, and at building entries. The signage design will be considered as part of the landscape and language used in the architecture of the buildings to provide placemaking and wayfinding principles for safety and user experience of the estate.

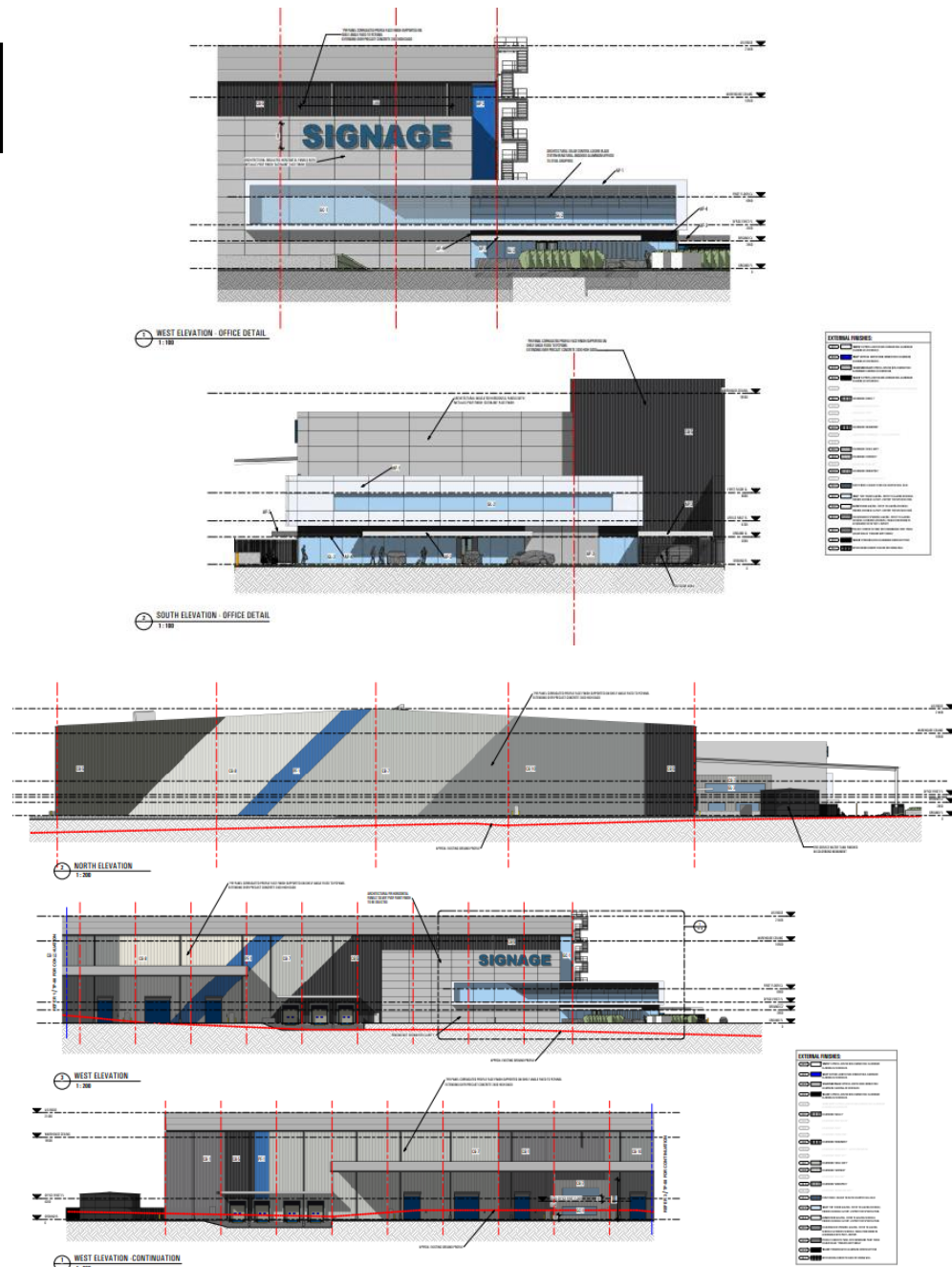


Figure 4 – Proposed Site Elevations

Design Quality

3.0 Better Placed

3.1 Better Fit

Sitting within IN1 General Industrial zone, the proposed development is heavily informed by its local character by taking cues from other buildings of similar bulk and scale. From Eastern Creek Drive, the building is narrowly framed by the newly built Jaycar warehouse to the west, and an approved Data Centre currently under construction by the Applicant (Charter Hall). As part of the same estate, the building façade elements intend to complement each other and create a holistic character to a growing industrial zone.

3.2 Better Performance

The proposed development features 2,915sqm of landscape area including a variety native endemic species which will provide biodiversity back into former greenfield site. Roughly 45% of the roof features a series of solar panels to assist with green energy consumption on-site. The proposal also features a 72kL rainwater tank to be reused for non-drinking uses such as toilet flushing, water heating systems and irrigation.

Sustainability initiative can be reviewed in detail in the ESD report.

3.3 Better for Community

This development aims to assist the ongoing trajectory of Western Sydney as a Business District and embrace the community who will be occupying the site. It is expected to provide approximately 480 construction jobs and 438 ongoing jobs. With its close proximity to the major M7 / M4 interchange, the site can be easily accessed from all parts of Sydney via road or more locally via public transport along Old Wallgrove Road. Bicycle parking has also been provided for cyclists.

3.4 Better for People

With input from traffic consultants, the site layout provides safe access for pedestrians, cars and trucks, which have all been given separate access from Eastern Creek Drive. The office features an abundance of external glazing providing ample amounts of natural light and promoting wellbeing for all users. There is approximately 150sqm of shaded outdoor lunch space for staff to use recreationally. The proposal also addresses all areas of DDA compliance by providing appropriate access and amenity as per AS 1428.1.

3.5 Better Working

The building works efficiently to its intended use as a temperature controlled facility by separating loading, staging and storage operations for the warehouse. One-way traffic has been considered to manage the efficiency of loading and dispatch movement on-site. With input from the user, the fit-for-purpose warehouse still provides flexibility for other future tenants to occupy the space. The layout also allows for future access from the planned Honeycomb Drive extension, while still functioning reliably in the current design.

3.6 Better Value

The development provides ample opportunity for employment and business growth in the Western Sydney region. It provides added value to a former greenfield site by productively using the space for its intended land use zone, as well as reintroducing aspects of landscape and sustainability to remediate the site. Long term, the proposal is expected to not only provide a return on investment financially, but is also expected to contribute to the needs of a growing population via warehouse distribution operations across the state.

3.7 Better Look and Feel

Visually, the proposal generates interest through a series of alternating PIR cladding panels, with prominent facades feature a 'Charter Hall Blue' stripe. The materiality of the warehouse creates a dialogue with the landscaped streetscapes with softens the hardness of the cladding.

The two-storey office together with the adjoining chiller and warehouse creates a layering effect from the main entrance reducing the apparent bulk of the building.

All signage pylons have been designed and placed such that it draws people into the site in a safe and simple way