

Christopher Curtis

Subject: FW: Penrith Council Comment on Open Space Edge Road - Kemps Creek
Attachments: CO14429.01-PR40-A.pdf

From: Gavin Cherry <gavin.cherry@penrith.city>
Date: Monday, 7 August 2023 at 2:42 pm
To: Christian Kublins <christian.kublins@in-dev.com.au>
Cc: Robert Walker <Robert.Walker@penrith.city>, Stephen Masters <stephen.masters@penrith.city>
Subject: Penrith Council Comment on Open Space Edge Road - Kemps Creek

Afternoon Christian,

Thankyou for the email below and the explanation this afternoon over the phone.

I can confirm that Council does not raise any outright concerns or objections to suggested realignment of the open space edge road to within RE1 zoned land instead of wholly or part contained within Industrial / employment zoned land. A similar position has just been formed and reported to the Sydney Western Sydney Planning Panel for their determination of a DA further north of the subject site. The outcome however is pending.

There are however orderly development considerations that DPE as the consent authority must be satisfied of in the assessment of the proposed realignment. This may also require input from Transport for NSW in addition to Council as Local Road Authority as part of the detailed review phase during public exhibition

The realignment must work north and south noting that this calls into question separate land parcels not subject of the current proposal. Of particular note is the timing of northern connections which extend beyond the attached plan. This may be largely outside the scope of the current proposal, but it would need to be demonstrated that the realignment can functionally work with minimal regrading / earthworks implications for adjacent landowners.

The suitability of the realignment also depends on infrastructure requirements of Sydney Water which need to be ascertained as part of the SSD assessment process. In essence it must be demonstrated that the functional nature of the land as open space is not compromised by the realigned and relocated roadway.

Any intended staging of the SSD development proposal would also inform timing of road delivery and temporary turning head arrangements.

Please do not hesitate to refer this email to DPE for their consideration in the SSD application lodgement process.

regards

Gavin Cherry
Development Assessment Coordinator
Development Services

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T [+61247328125](tel:+61247328125) | F +612 4732 7958 | M
PO Box 60, PENRITH NSW 2751
www.visitpenrith.com.au
www.penrithcity.nsw.gov.au



From: Christian Kublins <christian.kublins@in-dev.com.au>
Sent: Friday, July 28, 2023 1:30 PM
To: Kathryn Saunders <kathryn.saunders@penrith.city>; Stephen Masters <stephen.masters@penrith.city>
Cc: Matthew Thiselton <matthew.thiselton@gibbgroup.com.au>; Anthony McLandsborough <anthony.m@atl.net.au>
Subject: Open Space Edge Road - Kemps Creek

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Hi Kathryn & Stephen,

Per our meeting at Council a couple of weeks back, we've taken opportunity to demonstrate the re-alignment of the Open Space Edge Road, in the RE1 land and demonstrated a turning head at each block.

Could you kindly review the sketch and provide feedback, so that we can progress the concept with our neighbours and submit the sketch with our SSDA, so that we can finally pass DPE's test of adequacy, knowing that this matter has been reviewed with Council.


Should you have any queries, please don't hesitate to call me.

Thank you.

Regards,

Christian Kublins



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 www.in-dev.com.au
 [PO Box 858 Willoughby NSW 2068](#)

LEGEND:

LEVELS DATUM IS AHD.

EXISTING SITE LEVELS AND DETAILS BASED ON SURVEY INFORMATION PROVIDED BY BOXAL SURVEYORS DATED 02.09.20.

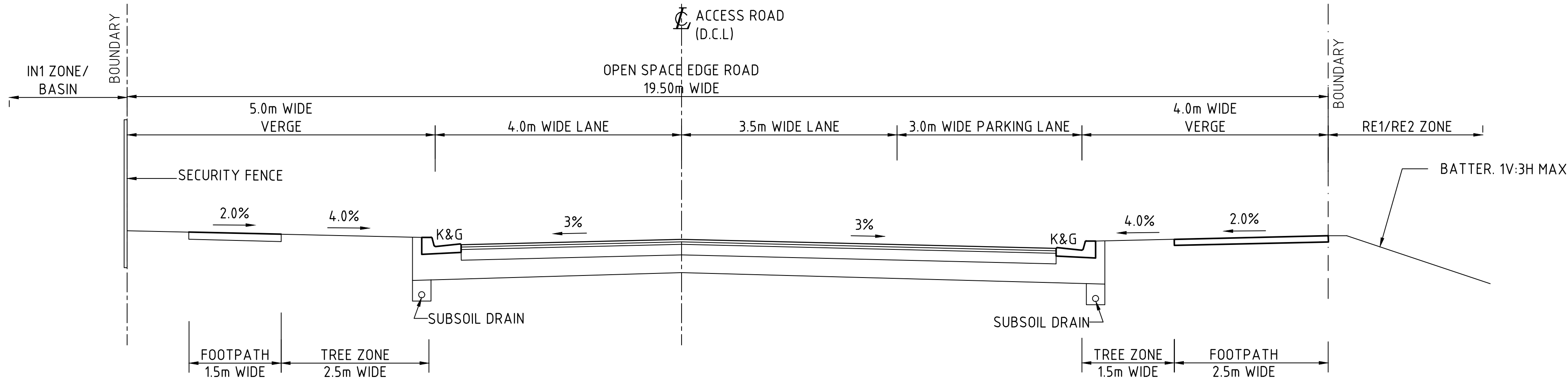
- SJP, SEALED JUNCTION PIT
- KIP, KERB INLET PIT
- SGGP, SINGLE GRATED GULLY PIT
- OVERLAND FLOW DIRECTION
- PROPOSED DRAINAGE LINE
- EXISTING DRAINAGE LINE
- FINISHED PAVEMENT CONTOUR (MAJOR) 0.5m INTERVALS
- FINISHED PAVEMENT CONTOUR (MINOR) 0.1m INTERVALS
- FINISHED PAVEMENT SPOT HEIGHT
- EXISTING SEWER MAIN
- EXISTING WATER MAIN
- EXISTING RECYCLED WATER MAIN
- EXISTING UNDERGROUND ELECTRICAL
- EXISTING TELECOMMUNICATIONS (NBN)

STORMWATER DRAINAGE NOTES:

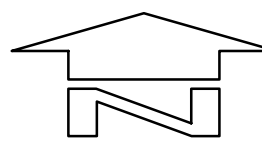
- ALL STORMWATER WORKS TO BE COMPLETED IN ACCORDANCE WITH AUSTRALIAN STANDARD AS3500.3 PLUMBING AND DRAINAGE, PART 3: STORMWATER DRAINAGE.
- THE MINOR (PIPED) SYSTEM HAS BEEN DESIGNED FOR THE 1 IN 20 YEAR ARI STORM EVENT AND THE MAJOR (OVERLAND) SYSTEM HAS BEEN DESIGNED FOR THE 1 IN 100 YEAR ARI STORM EVENT.
- ALL FINISHED PAVEMENT LEVELS SHALL BE AS INDICATED ON FINISHED LEVELS PLANS DA518.DA52.
- PIT SIZES SHALL BE AS INDICATED IN THE SCHEDULE WHILE PIPE SIZES AND DETAILS ARE PROVIDED ON PLAN.
- EXISTING STORMWATER PIT LOCATIONS AND INVERT LEVELS TO BE CONFIRMED BY SURVEY PRIOR TO COMMENCING WORKS ON SITE.
- ALL STORMWATER PIPES Ø375 OR GREATER SHALL BE CLASS 2 (WITH HS2 SUPPORT) REINFORCED CONCRETE WITH RUBBER RING JOINTS UNLESS NOTED OTHERWISE.
- ALL PIPES UP TO AND INCLUDING Ø300 TO BE uPVC GRADE SN8 UNO.
- PIPE CLASS NOMINATED ARE FOR IN-SERVICE LOADING CONDITIONS ONLY. CONTRACTOR IS TO MAKE ANY NECESSARY ADJUSTMENTS REQUIRED FOR CONSTRUCTION CONDITIONS.
- ALL CONCRETE PITS GREATER THAN 1000mm DEEP SHALL BE REINFORCED USING N12-200 EACH WAY CENTERED IN WALL AND BASE. LAP MINIMUM 300mm WHERE REQUIRED. ALL CONCRETE FOR PITS SHALL BE F'c<25 MPa. PRECAST PITS MAY BE USED WITH THE APPROVAL OF THE ENGINEER.
- IN ADDITION TO ITEM 9 ABOVE, ALL CONCRETE PITS GREATER THAN 3000mm DEEP SHALL HAVE WALLS AND BASE THICKNESS INCREASED TO 200mm.
- PIPES SHALL BE LAID AS PER PIPE LAYING DETAILS. PARTICULAR CARE SHALL BE TAKEN TO ENSURE THAT THE PIPE IS FULLY AND EVENLY SUPPORTED. RAM AND PACK FILLING AROUND AND UNDER BACK OF PIPES AND PIPE FAUCETS, WITH NARROW EDGED RAMMERS OR OTHER SUITABLE TAMPING DETAILS.
- CONCRETE PIPES UNDER, OR WITHIN THE ZONE OF INFLUENCE OF PAVED AREAS SHALL BE LAID USING HS2 TYPE SUPPORT, AS A MINIMUM, IN ACCORDANCE WITH AS 3725. AGGREGATE BACKFILL SHALL NOT BE USED FOR PIPE BEDDING AND OR HAUNCH/SIDE SUPPORT.
- WHERE PIPE LINES ENTER PITS, PROVIDE 2m LENGTH OF STOCKING WRAPPED SLOTTED Ø100 uPVC TO EACH SIDE OF PIPE.
- ALL SUBSOIL DRAINAGE LINES SHALL BE Ø100 SLOTTED uPVC WITH APPROVED FILTER WRAP LAID IN 300mm WIDE GRANULAR FILTER UNLESS NOTED OTHERWISE. LAY SUBSOIL LINES TO MATCH FALLS OF LAND AND/OR 1 IN 200 MINIMUM. PROVIDE CAPPED CLEANING EYE (RODDING POINT) AT UPSTREAM END OF LINE AND AT 30m MAX. CTS. PROVIDE SUBSOIL LINES TO ALL PAVEMENT/ LANDSCAPED INTERFACES, TO REAR OF RETAINING WALLS (AS NOMINATED BY STRUCTURAL ENGINEER) AND AS SHOWN ON PLAN.
- WHERE SUBSOIL DRAINAGE PASSES UNDER A PAVEMENT OR A SLAB, UNSLOTTED uPVC ARE TO BE PROVIDED UNLESS NOTED OTHERWISE.
- ALL PIPE GRADES 1 IN 200 MINIMUM UNO.
- PROVIDE STEP IRONS IN PITS DEEPER THAN 1000mm.
- MIN. 600 COVER TO PIPE OBVERT BENEATH ROADS & MIN. 400 COVER BENEATH LANDSCAPED AND PEDESTRIAN AREAS.
- PIT COVERS IN TRAFFICABLE PAVEMENT SHALL BE CLASS D 'HEAVY DUTY'; THOSE LOCATED IN NON-TRAFFICABLE AREAS SHALL BE CLASS B 'MEDIUM DUTY' U.N.O.
- PROVIDE CLEANING EYES (RODDING POINTS) TO PIPES AT ALL CORNERS AND T-JUNCTIONS WHERE NO PITS ARE PRESENT.
- DOWN PIPES (DPI) TO BE AS PER HYDRAULIC ENGINEERS' DETAILS WITH CONNECTOR TO MATCH DPI SIZE U.N.O. ON PLAN. PROVIDE CLEANING EYE AT GROUND LEVEL.
- PIPE LENGTHS NOMINATED ON PLAN OR LONGSECTIONS ARE MEASURED FROM CENTER OF PITS TO THE NEAREST 0.5m AND DO NOT REPRESENT ACTUAL LENGTH. THE CONTRACTOR IS TO ALLOW FOR THIS.
- WHERE CONNECTION TO EXISTING INGROUND DRAINAGE SYSTEMS, OPEN SWALES, CHANNELS OR ANY OTHER EXISTING SYSTEM, IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO VERIFY THE LOCATION AND INVERT ON SITE AT THE BEGINNING OF THE CONSTRUCTION PERIOD. REFER ANY VARIANCE FROM DOCUMENTATION OR SURVEYS TO THE ENGINEER FOR CLARIFICATION.

FINISHED LEVELS PLAN NOTES:

- LEVELS DATUM IS AUSTRALIAN HEIGHT DATUM (A.H.D.).
- GRADING REQUIREMENTS TO BE COMPLETED IN ACCORDANCE WITH AUSTRALIAN STANDARD AS2890.1, AS2890.2 AND AS2890.6.
- ALL CONTOUR LINES & SPOT LEVELS INDICATE FINISHED PAVEMENT LEVELS U.N.O. ON PLAN.
- CONTOUR INTERVALS
 - THE MINOR CONTOUR INTERVAL IS 0.1m.
 - THE MAJOR CONTOUR INTERVAL IS 0.5m.
- HARDSTAND GRADING
 - MINIMUM PAVEMENT GRADE IS TO BE 1:100 (1%).
 - GRADING OF ON-GRADE DOCKS TO BE 1:100 (1%) FALL AWAY FROM THE DOCK FACE FOR A LENGTH OF 15m U.N.O.
 - GRADING OF TRUCK CIRCULATION ZONES TO BE MINIMUM AS NOTED ABOVE, 3-4% NOMINAL AND MAX. 5%.
- CAR PARKING AREA GRADES
 - MINIMUM PAVEMENT GRADE IS TO BE 1:100 (1%), DESIRABLE MINIMUM GRADE 1:50 (2%).
 - MAXIMUM PAVEMENT GRADE IS TO BE 1:20 (5%) N CARPARKING AREAS AND 1:25 (4%) ELSEWHERE.
 - DISABLED ACCESS PARKING ZONES AND SHARED SPACE TO BE MAXIMUM OF 1:33 (3%) IN ASPHALT PAVEMENT AND MAXIMUM OF 1:40 (2.5%) IN CONCRETE PAVEMENT.
 - CARPARK RAMP GRADES TO BE MAX 1:5 WITH 2.5m SMOOTH TRANSITION AT TOP AND BOTTOM U.N.O.
- TRUCK RAMP GRADES
 - MAXIMUM B-DOUBLE OR 19.0m AV RAMP GRADES ARE TO BE 1:8.3 (12%) U.N.O. ON PLAN.
 - PROVIDE MINIMUM 4.0m LONG TRANSITION WHERE CHANGES OF GRADE EXCEED 1:20 (5%) AT A CREST U.N.O.
 - PROVIDE MINIMUM 3.0m LONG TRANSITION WHERE CHANGE OF GRADE EXCEED 1:20 (5%) AT A SAG U.N.O.
 - TRANSITIONS ARE TO PROVIDE A SMOOTH CONTINUOUS CIRCULAR AND TANGENTIAL CHANGE IN GRADE TO ENSURE NO SHARP OR ACUTE CHANGES IN GRADE ARE PRESENT.
- WHERE FIRE BRIGADE ACCESS IS REQUIRED, MAXIMUM RAMP GRADIENTS ARE TO BE 1:6 (16.6%), DESIRABLE RAMP GRADIENTS ARE TO BE 1:8 (12.5%) WITH 7m TRANSITION TOP AND BOTTOM U.N.O. ON PLAN.
- PERMANENT BATTER SLOPES ARE TO HAVE A MAXIMUM GRADE OF 1V:3H U.N.O. BASED ON GEOTECHNICAL ASSESSMENT. PROVIDE MINIMUM 0.5m BERM BETWEEN THE BACK OF KERB OR PAVEMENT EDGES AND THE TOP OR TOE OF A BATTER.
- ALL BATTER SLOPE WITH GRADES AT OR EXCEEDING 1V:6H ARE TO BE TURFED IMMEDIATELY OR APPROPRIATE EROSION CONTROL IS TO BE PROVIDED TO THE SATISFACTION OF THE ENGINEER.
- ALL FOOTPATHS ARE TO FALL AWAY FROM THE BUILDING AT 2.5% NOMINAL GRADE.
- ALL PAVEMENTS ARE TO BE SET AT 30mm BELOW THE FINISHED FLOOR LEVEL OF THE WAREHOUSE AND OFFICE AREAS. PROVIDE LOCAL FEATHERING AT DOORWAYS OR ROLLER SHUTTERS TO PROVIDE FLUSH FINISH AS REQUIRED.
- WHERE NEW AND EXISTING INTERFACING IS REQUIRED, MATCH EXISTING LEVELS AND PROVIDE SMOOTH INTERFACE BETWEEN NEW AND EXISTING GRADIENTS. REFER ANY CONCERNS TO THE ENGINEER.



PROPOSED OPEN SPACE EDGE ROAD-TYPICAL CROSS SECTION
SCALE 1:50



CONCEPT CIVIL WORKS PLAN-OPEN SPACE EDGE ROAD
1:1000 SCALE

**PRELIMINARY
FOR INFORMATION ONLY**

10m 0 10 20 30 40 50 60 70 80 90 100m
SCALE 1:1000 AT A0 SHEET SIZE

			ARCHITECT			CLIENT			PROJECT			CONSULT AUSTRALIA			COSTIN ROE CONSULTING			DRAWING TITLE		
			[watch this] SPACE design			M CONSULTING			CONCEPT ROADWAY 805-817 MAMRE ROAD KEMPS CREEK NSW			PO Box N419 Sydney NSW 1220 Level 4, 4 Woodmill Street, Millers Point NSW 2000 p: +61 2 9251 7699 e: mail@costinroe.com.au			CIVIL & STRUCTURAL ENGINEERS			CONCEPT CIVIL WORKS PLAN OPEN SPACE EDGE ROAD		
ISSUED FOR INFORMATION			27.07.23			A			DESIGNED			CHECKED			CADD REF.			DRAWING No		
AMENDMENTS			DATE			ISSUE			MC			MC			C014429.01-PR4.0			C014429.01-PR4.0		
			AMENDMENTS			DATE			MC			MC						ISSUE		
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Christopher Curtis

From: Christian Kublins <christian.kublins@in-dev.com.au>
Sent: Friday, 16 June 2023 8:13 AM
To: Gregory Ingleton; Daniel Cunningham
Cc: Matthew Thiselton; Anthony McLandsborough
Subject: Re: [External] Re: 805 Mamre Road, Kemps Creek

Hi Greg,

Thank you for your reply. We have issued the information formally to DPE, however without any feedback from SW, they’ve been pushing back on accepting the DA.

Equally, we understand that a feasibility assessment could take a number of months to finalise, and given we’re ready to lodge we thought it more appropriate to lodge the DA and follow that process, than undertake a feasibility and then lodge anyway.





If there’s anything here I’m missing, please let me know.

Thank you again.

Regards,

Christian Kublins



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 www.in-dev.com.au
 [PO Box 858 Willoughby NSW 2068](#)

From: Gregory Ingleton <GREGORY.INGLETON@sydneywater.com.au>
Date: Thursday, 15 June 2023 at 3:09 pm
To: Christian Kublins <christian.kublins@in-dev.com.au>, Daniel Cunningham <DANIEL.CUNNINGHAM@sydneywater.com.au>
Cc: Matthew Thiselton <matthew.thiselton@gibbgroup.com.au>, Anthony McLandsborough <anthony.m@atl.net.au>
Subject: RE: [External] Re: 805 Mamre Road, Kemps Creek

Hi Christian,
Sorry for the slow response. It would be more beneficial if the information could be sent officially either as part of your application to DPE or through a feasibility application to Sydney Water. We have an abundance of applications coming in through official channels which need to take priority.
If you need any guidance on the feasibility process please let me know.

Cheers
Greg

From: Christian Kublins <christian.kublins@in-dev.com.au>
Sent: Wednesday, 14 June 2023 7:18 AM
To: Gregory Ingleton <GREGORY.INGLETON@sydneywater.com.au>; Daniel Cunningham <DANIEL.CUNNINGHAM@sydneywater.com.au>
Cc: Matthew Thiselton <matthew.thiselton@gibbgroup.com.au>; Anthony McLandsborough <anthony.m@atl.net.au>
Subject: [External] Re: 805 Mamre Road, Kemps Creek

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Hi Greg and Daniel,

I’d just like to follow up my earlier email of 5 June to confirm that you had received it and arrange a meeting with you to discuss our site.





Could you kindly confirm your availability for a brief discussion over the next fortnight?

Thank you.

Regards,

Christian Kublins



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 [PO Box 858 Willoughby NSW 2068](#)

From: Christian Kublins <christian.kublins@in-dev.com.au>
Date: Monday, 5 June 2023 at 11:50 am

To: Greg Ingleton <GREGORY.INGLETON@sydneywater.com.au>, Daniel Cunningham <daniel.cunningham@sydneywater.com.au>
Cc: Matthew Thiselton <matthew.thiselton@gibbgroup.com.au>, Anthony McLandsborough <anthony.m@atl.net.au>
Subject: 805 Mamre Road, Kemps Creek

Gents,

Please excuse my direct email, however I’ve been instructed by DPE’s David Schwebel to reach out to Sydney Water re an application we’ve made for a development at 805 Mamre Road, Kemps Creek on behalf of Gibb Group.

The rejection letter from DPE is attached for your records, which notes there is a lack of engagement with Sydney Water, however this is not the case. The site has been presented previously, in a meeting with AT&L on 1 September 2022, at which Greg was present, however we never received any follow up correspondence from the meeting – which DPE is pushing us to attain.

Could I trouble you to either review the attached development and water strategy, or simply reply to this email noting you will review the application once lodged?

Should you require any additional information or wish to meet to discuss the proposal, please don’t hesitate to contact me.

Thank you in advance and again, my apologies for the direct email.

Regards,

Christian Kublins





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christian.kublins@in-dev.com.au



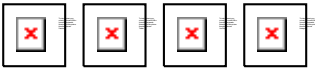
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MINUTES

PreDA discussion – 805 Mamre Road, Kemps Creek

Date	11 October 2021	
Time	1:30pm	
Venue	Microsoft Teams	
Chairperson	Laura Van Putten	
Invitees	Laura Van Putten Pahee Rathan Matthew Thiselton Anthony McLandsborough Rebecca Butler-Maden Tim Lewis Andrew Johnson Christian Kublins Tracey Page	TfNSW TfNSW Gibb Group AT&L Ason Group Ason Group Ason Group TfNSW
Apologies:	Robert Rutledge	

1.	Welcome
2.	Content
	<p>MT – Development application overview</p> <ul style="list-style-type: none"> • Site approximately 26000sqm GFA • approximately Trip rates 60 vehicles per hour in the AM/PM peak periods • interim access to Mamre Road is required – ultimate internal north south road connection to Mirvac AIP access intersection. • Attempts have been made to adjoining property owners to support the north/south link – at this stage this is not achievable. <p><u>Interim Access</u></p> <ul style="list-style-type: none"> • PR – A deceleration lane would be required for the temporary access to Mamre Road. • PR – the current access location would result in the need for the Deceleration lane to be located in front of the adjoining property. Has there been any discussion with this land owner? There would need to be an agreement with the adjoin property owner. <i>In addition it should be noted that there would likely be the need for internal connections to be discussed and agreed to, in order to have 1 access from Mamre Road to both properties. TfNSW would not support multiple accesses from a deceleration lane.</i> • PR – Should a signed agreement not be supported by the adjoining property owner the deceleration lane would need to be provided for along the development property boundary only. The current design would need to be shifted as far north on the property (noting the Reparian Buffer restrictions in the northeast corner of lot). • PR – explained that a 3.5m width strip of land within the property boundary will be required to be dedicated as road. This is required due to the need to future proof the Mamre Road corridor. Should the Mamre Road be upgraded prior to the internal road

	<p>local road network the deceleration lane would need to be relocated within the property boundary.</p> <p><u>Additional discussion – 1080 Mamre Road</u></p> <ul style="list-style-type: none"> • Brief overview of the location • PR – confirmed the same approach as above would be required should there be need for temporary access to Mamre Road. this would result in the need to rethink the preliminary design provided.
3.	Suggestions and Solutions
	<ul style="list-style-type: none"> • The deceleration is to be located within development site frontage. Whilst this is a 'temporary' access, there is no timeline for the adjoin land owners to construct the internal road connections. In this regard the access is to be considered as a long term 'temporary' arrangement. • If the adjoining land owner agrees to let the land be used for a deceleration lane then a letter of intent or agreement will need to be provided to show that a deceleration lane along their frontage would be support and how. The letter will need to outline agreement to land dedication for future road widening should TfNSW need to relocate the deceleration lane within the private property. In addition internal access arrangement with the adjoining property owner will need to be considered. (The land dedication is required for the deceleration including - batters, underground services, footpaths etc). • Pedestrian paths need to be consistent with TfNSW Strategic plans on both sides. • Building setback from the adjusted road boundary is to be consistent with the DCP
4.	Actions
	<ul style="list-style-type: none"> • the design is to be redeveloped consistent with the discussion above prior to submitting to DPIE
5.	Close

Subject: RE: 805 Mamre Rd, Access Strategy
Date: Monday, 22 November 2021 at 1:55:13 pm Australian Eastern Daylight Time
From: Laura Van putten <Laura.VAN.PUTTEN@transport.nsw.gov.au>
To: Rebecca Butler-Madden <Rebecca.BMadden@asongroup.com.au>
Attachments: GG MAM MP07 - P3.pdf, image001.jpg

Hi Rebecca

That's a great outcome with regards to the access. Would you like me to circulate the plan and provide preliminary comments on the proposed access?

Kind regards,

Laura van Putten

T 02 8849 2480 | M 0429 505 961

From: Rebecca Butler-Madden [mailto:Rebecca.BMadden@asongroup.com.au]
Sent: Monday, 22 November 2021 12:30 PM
To: Laura Van putten <Laura.VAN.PUTTEN@transport.nsw.gov.au>
Cc: Robert Rutledge <Robert.Rutledge@transport.nsw.gov.au>; Pahee Rathan <Pahee.RATHAN@transport.nsw.gov.au>; matthew.thiselton@gibbgroup.com.au; Anthony McLandsborough <anthony.m@atl.net.au>; Tim Lewis <tim.lewis@asongroup.com.au>; Andrew Johnson <andrew.johnson@asongroup.com.au>; christian.kublins@in-dev.com.au
Subject: RE: 805 Mamre Rd, Access Strategy

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Hi Laura,

Thanks for issuing through the meeting minutes, we have no requested changes.

In regards to the discussion held, please see an updated site plan with the updated access strategy.

Discussions with the southern neighbour to date have not been forth coming. As such, discussions have been held with the neighbour to the north, and a temporary, shared access arrangement has been agreed. The neighbour has confirmed their acceptance and willingness to provide a letter of support as a part of the application.

We note that roads are a permissible use under the RE1 land zoning and the arrangement would only be until such time the final access arrangement is completed.

Kind Regards,

Rebecca Butler-Madden

Senior Transport Planner | Ason Group

M: +61 406 421 154 | T: +61 2 9083 6601 | E: rebecca.bmadden@asongroup.com.au

A: Suite 17.02, Level 7, 1 Castlereagh Street, Sydney NSW 2000

From: Laura Van putten <Laura.VAN.PUTTEN@transport.nsw.gov.au>
Sent: Thursday, 4 November 2021 5:19 PM
To: Rebecca Butler-Madden <Rebecca.BMadden@asongroup.com.au>

Cc: Robert Rutledge <Robert.Rutledge@transport.nsw.gov.au>; Pahee Rathan <Pahee.RATHAN@transport.nsw.gov.au>; matthew.thiselton@gibbgroup.com.au; Anthony McLandsborough <anthony.m@atl.net.au>; Tim Lewis <tim.lewis@asongroup.com.au>; Andrew Johnson <andrew.johnson@asongroup.com.au>; christian.kublins@in-dev.com.au
Subject: RE: 805 Mamre Rd, Meeting Request

Hi Rebecca

Please see attached minutes to the meeting held on 11 October 2021 fro the subject development site. Please let me know if you would like to suggest any amendments to the minutes, thanks.

Kind regards,

Laura van Putten

T 02 8849 2480 | M 0429 505 961

From: Rebecca Butler-Madden [<mailto:Rebecca.BMadden@asongroup.com.au>]
Sent: Wednesday, 6 October 2021 11:58 AM
To: Laura Van putten <Laura.VAN.PUTTEN@transport.nsw.gov.au>
Cc: Development Sydney <Development.Sydney@transport.nsw.gov.au>; Robert Rutledge <Robert.Rutledge@transport.nsw.gov.au>; Pahee Rathan <Pahee.RATHAN@transport.nsw.gov.au>
Subject: 805 Mamre Rd, Meeting Request

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Hi Laura,

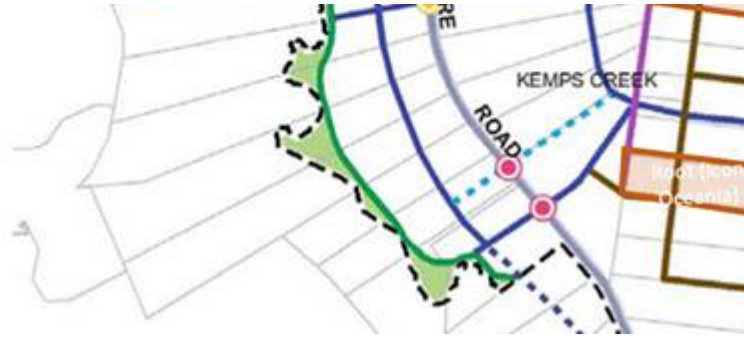
Trust that you are well an enjoyed a nice long weekend.

We have another Mamre Road project to discuss and would like to arrange a meeting. The client on this is Matt Thiselton from Gibb Group (who also have a site to the north of Aldington Road) and this will be submitted as DA to Council.

Please see attached plan. As you can see, the site is reliant on internal connections to the north and south to align with the requirements of access in regards to the DCP. However, just on the basis of our understanding of the landowners, around the Site there is no intention to develop these sites in the immediate future (note that we have asked the client to liaise properly with the landowners to explore all possibilities). As such, to progress development of the site, it is likely that an interim access via Mamre Road will be required. This is a key aspect for progressing the Site, and as such would like to discuss the requirements with yourselves.

Can you please arrange for a consultation with your team?





Kind Regards,

Rebecca Butler-Madden

Senior Transport Planner | Ason Group

M: +61 406 421 154 | **T:** +61 2 9083 6601 | **E:** rebecca.bmadden@asongroup.com.au

A: Suite 17.02, Level 7, 1 Castlereagh Street, Sydney NSW 2000

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Our Ref: PL22/0003
Contact: Jacqueline Klincke
Telephone: (02) 4732 8391

28 February 2022

Ethos Urban
173-185 Sussex Street
SYDNEY NSW 2000

Dear Ethos Urban

**Pre-lodgement Advice
Proposed Development Two Warehouse Buildings & Attached Offices
At 805-817 Mamre Road KEMPS CREEK NSW 2178 (Lot 26 DP 258414)**

Thank you for taking part in Council's pre-lodgement meeting on 20 January 2022. The meeting was a useful opportunity for Council to gain an understanding of your proposal and in relation to the recently issued industry-specific SEARs for SSD-30871587.

Council has undertaken a review of the submitted information provides the following advice for your consideration and action, prior to the preparation and formalisation of your Environmental Impact Statement (EIS).

Information given by the pre-lodgement panel does not constitute a formal assessment of your proposal and at no time should comments of the officers be taken as a guarantee of Council's endorsement of your proposal.

If we can help you any further regarding the attached advice, please feel free to contact me on (02) 4732 8391.

Yours sincerely

Jacqueline Klincke
Development Assessment Planner

Penrith City Council
PO Box 60, Penrith
NSW 2751 Australia
T 4732 7777
F 4732 7958
penrithcity.nsw.gov.au

PENRITH
CITY COUNCIL

PRE-LODGE MENT ADVICE

Proposal Two Warehouse Buildings, Attached Offices, Internal Roads and Ancillary Works, and subdivision.

Address Lot 26 DP 258414
805-817 Mamre Road KEMPS CREEK NSW 2178

Attendees:

Proponent

Christopher Curtis
Christian Kublins
Matthew Thistleton
Emma Chee
Nikita Gleeson

Penrith City Council

Jacqueline Klincke – Development Assessment Planner
Abby Younan – Planning Administration Officer
Stephen Masters – Senior Development Engineer
Daniel Davidson – Senior Traffic Engineer
Kirk Ryan – Team Leader Environmental Health
Aisha Poole – Resource Recovery Coordinator
Craig Squires – Building Certification and Fire Safety Coordinator

Zoning: IN1 General Industrial, SP2 Infrastructure, RE1 Public Recreation, ENZ Environment and Recreation

Site Constraints: Bushfire prone land, affected by 4.5m wide and variable access easement, 6m wide and variable easement for right of way, 70m wide transmission easement, 2m wide easement for water pumping, 4m wide easement for water pump purposes, affected by Australian Noise Exposure Forecast (ANEF) contours of 20-25. The site contains vegetation and its rear western interfaces with a mapped waterway. The site has frontage to Mamre Road and in its current design is not designed to accommodate industrial traffic movements. Land topography.

Development Type: Torrens Subdivision, 2 Warehouse and Distribution Facilities with Ancillary Offices and Internal Road

The pre-lodgement panel will endeavour to provide information which will enable you to identify issues that must be addressed in any application. The onus remains on the applicant to ensure that all relevant controls and issues are considered prior to the submission of an application.

RELEVANT EPIs POLICIES AND GUIDELINES

- Environmental Planning and Assessment Act 1979
 - Environmental Planning and Assessment Regulation 2000
 - Biodiversity Conservation Act 2016
 - Sydney Regional Environmental Plan No. 20 – Hawkesbury Nepean River (No. 2 - 1997)
 - State Environmental Planning Policy No. 55 – Remediation of Land
 - State Environmental Planning Policy (Infrastructure) 2007
 - State Environmental Planning Policy (Western Sydney Employment Area) 2009
 - State Environmental Planning Policy (Western Sydney Aerotropolis) 2020
 - State Environmental Planning Policy (Vegetation in Non-Rural Areas) 2017
 - State Environmental Planning Policy (Stage and Regional Development) 2011
 - State Environmental Planning Policy No. 33 – Hazardous and Offensive Development.
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- Mamre Road Precinct Development Control Plan 2021
 - Council's Stormwater Drainage Specifications for Building Developments
 - Council's Engineering Design Specifications for Civil Works
 - Council's Engineering Construction Specifications for Civil Works
 - Council's Cooling the City Strategy
 - NSW Government Architects suite of documents (Greener Places, Better Placed)
 - Planning Guidelines for Walking and Cycling (NSW Government 2004)

Draft Plans and Policies

- Draft Environmental SEPP
- Draft Remediation of Land SEPP
- Draft Cumberland Plain Conservation Plan
- EIS Design and Place SEPP
- Draft Connecting with Country Framework

KEY ISSUES AND OUTCOMES

The proposal is to address the following issues:

1. PLANNING:

(a) Setbacks

Sections 4.2.2 and 4.2.3 of the Mamre Road Precinct DCP require a 10m building setback from the edge of RE1 zoned land, with a 5m landscaped setback. In certain parts, the northern elevation of Warehouse 1 does not appear to be setback 10m from the edge of the RE1 zoned land. An extensive hardstand area also directly abuts the RE1 zone, with no landscaping that is at least 5m wide provided. Further design amendments are required to ensure Warehouse 1 is appropriately setback from the RE1 zone portion and riparian corridor on the site, with adequate landscaping.

Furthermore, the prescribed 7.5m building setback from local estate roads (i.e. Open Space Edge Road) does not apply to Warehouse 2 in this instance as the RE1 zone located at the rear of the newly created Lot 2 extends over the road. In this regard, Warehouse 2 is required to comply with the setback requirements pertaining to portions of land zoned RE1. Therefore, the western elevation of Warehouse 2 is not provided with a compliant 10m building setback, with a 5m landscape setback to the RE1 zoned land. The scale and size of Warehouse 2 should therefore be reduced in order to provide for compliant setbacks with additional landscaping in accordance with the DCP.

In addition, the development shall ensure a 12m setback from the collector road is provided to each warehouse, with a 6m landscape setback.

(b) Interface of Warehouse 2 to Open Space Edge Road Frontage

Particular attention should be made to Section 4.2.5 of the Mamre Road Precinct DCP which requires facades along primary street frontages (i.e. Collector Industrial Roads and Open Space Edge Roads) to strengthen passive surveillance and streetscape character. Furthermore, elevations visible from the public domain (which includes all road frontages) must be finished with materials and colours and articulation that enhances the appearance of that façade and provide an attractive and varied streetscape.

Concern is raised regarding the current arrangement and interface of the western elevation Warehouse 2 to the Open Space Edge Road, which includes a minimal setback to the built form with a non-compliant landscaped setback, in conjunction with a large 110m wide expansive façade with no visual relief. Further consideration is required to enhance the articulation of that façade and activate this street frontage along the Open Space Edge Road through architectural design elements, glazing, the potential relocation of administration components of the building and additional communal spaces.

(c) Levels and Interfaces

Interfaces between the Riparian Buffer zone to the north-east, the Open Space at the western edge, and the development shall be practical and not result in lengths

of high retaining walls and inaccessible landscaped areas or finished levels which result in poor amenity and inaccessibility.

Details of owner's consent where required and details of in principal support from TfNSW for the delivery and operation of roads and intersections including the proposed deceleration land along Mamre Road and the proposed 8m wide access road intersection are to be provided.

In relation to the 8m wide access road, it is unclear from the submitted plans what the intent of the 8m wide access road will be, whether this is proposed to be dedicated to Council and what the levels and interfaces will be and what the impacts will be from its location within the riparian zone. This aspect of the proposal is unsupportable without sufficient reasoning and detail, noting that the RE1 zoned land is identified for acquisition under SEPP WSEA.

In relation to roads and access points, fully detailed civil, architectural and landscape plans are required to explain the design and delivery timing of all interim and ultimate roads and access points. Interfaces with northern and southern neighbours are to be detailed with sections.

The open space edge road must not to be located within the land zoned RE1.

(d) Overall Design and Height of Warehouses

Section 4.2.1 of the Mamre Road Precinct DCP requires warehouses to have lower elements towards the street, environmental and open space areas, and riparian corridors. Particular concern is raised in regard to the height (i.e. 14.6m) of the western wall of Warehouse 2 and its interface with the Open Space Edge Road. As previously noted above, greater consideration is required to the overall presentation of Warehouse 2 to the Open Space Edge Road in order to minimise bulk and scale, provide visual relief from the road, and activate the streetscape frontage. Height is to be measured from Natural Ground Level.

Section 4.2.5 of the Mamre Road Precinct DCP requires offices to be designed as an integral part of the building, rather than an unintegrated addition. The current location of both offices appears not to be sufficiently integrated with the built forms of the warehouses, this is particularly evident on the east elevation of Warehouse 2.

Further consideration is required in the location, design and materiality of these office spaces, in order to comply with the design controls outlined in the DCP.

Section 4.2.4 of the Mamre Road Precinct DCP requires communal areas must receive a minimum of 2 hours of direct sunlight between 11am and 3pm on 21st June. The proposed location of the communal area for Warehouse 2 is located on the southern side of the warehouse which may impact solar access. Further consideration is required in this regard. Communal office areas are to be high amenity and be colocated with landscaped elements and are to comply with the relevant section of the DCP.

(e) Landscaping, Car Parking & Driveways

Section 4.2.5 of the Mamre Road Precinct DCP requires large areas of car parking (i.e. greater than 20 spaces) to be located at the rear or side of the site, and not directly visible from the public domain. It is noted that all loading and services bays of each warehouse are located to the side of the site. However, the proposal includes extensive car parking (greater than 20 spaces) at the front of each warehouse which will be directly visible from the street frontage.

Further consideration must be given to providing extensive landscaping and planting. Landscaping is to provide for a variety of trees and shrubs of varying heights and densities within the landscaped setbacks to reduce the visual impacts associated with the car park to the public domain. The DCP is clear in requiring 1 canopy tree for each 10 spaces as a minimum. Council's preference is for planting to occur at a rate of 1 in 6 spaces (as per PDCP 2014).

Concern is also raised in relation to the number of vehicle crossings (i.e. 3 crossings/driveways) to each warehouse which limits the opportunity for landscaping within the front setback, in conjunction with limiting the locations available for the planting of street trees.

The proposal must ensure the development complies with the 10% tree canopy and a minimum 15% of pervious surfaces controls outlined in Section 4.2.3 of the Mamre Road Precinct DCP.

(f) Development Contributions

The applicant is advised to consider the timing of the lodgement of a development application and the effect of Clause 270 of the Regulations. Clause 270 of the *Environmental Planning and Assessment Regulations 2000*, states that *'pursuant to section 4.16(11) of the Act, a development application in relation to any land zoned IN1 General Industrial under State Environmental Planning Policy (Western Sydney Employment Area) 2009 must not be determined by the consent authority unless a contributions plan under Section 7.18 of the Act has been approved for the land to which the application relates.'*

Council's Draft Mamre Road Precinct Development Contributions Plan was publicly exhibited between 19 November 2020 to 17 December 2020. However, due to a number of changes, the draft plan was re-exhibited from 9 December 2021 to 27 January 2022. The draft plan can be viewed online at yoursaypenrith.com.au.

The re-exhibition has now concluded, and all submissions received are currently being reviewed and will be considered by Council at a future meeting. It is envisioned that the Draft Contributions Plan will be reported to the Ordinary Council Meeting scheduled in March 2022.

(g) Additional Matters

The design of the development shall include Environmentally Sustainable Design (ESD) principles and take into consideration the hot temperatures experienced in Greater Western Sydney and the Penrith LGA. The proposal must also demonstrate compliance with the relevant ESD controls outlined in Section 4 of the Mamre Road

DCP and within the SEPP WSEA. An ESD Report with recommendations is to accompany the application.

All roads shall comply with the Mamre Road DCP specifications for the identified road typology. Furthermore, landscaping and setbacks are to be designed to comply with the Mamre Road Precinct DCP, including in relation to canopy targets, road hierarchy, and the requirement of one canopy tree per 10 car parking spaces within minimum 1.5m wide landscaped blisters. Any design of retaining walls must also be compliant with the DCP.

The design must ensure domestic and heavy vehicle access points are separated, and that any substations and supporting infrastructure be integrated within the built form of the development and/or suitably located and landscaped.

Signage is to be addressed in any future application and a signage plan and strategy is required.

The application is to be accompanied by the list of documents and reports required by the Mamre Road Precinct DCP – Lodgement Requirements.

2. ENVIRONMENTAL MANAGEMENT:

(a) Noise Impacts

An acoustic assessment shall be undertaken to demonstrate that the proposed development will not have any impact on nearby sensitive receivers. This report is to be prepared by an appropriately qualified acoustic consultant, and is to consider noise impacts including, but not limited to:

- Demolition/Earthworks
- Construction
- Mechanical plant
- Vehicular movements
- Road traffic noise

Should mitigation measures be necessary, recommendations should be included to this effect. The recommendations and mitigation measures must be shown on all architectural plans.

(b) Air Quality

An air quality assessment is required to be undertaken to demonstrate that the proposed development will not have any impact on the health of nearby sensitive receivers or the environment. This report is to be prepared by an appropriately qualified consultant.

(c) Land Contamination

Any application is to address all relevant requirements under State Environmental Planning Policy No. 55 - Remediation of Land (SEPP 55). Consent cannot be granted to any development unless the relevant requirements of SEPP 55 have been satisfied. Any application is to demonstrate that the land is suitable for the

proposed purpose through a Detailed Site Investigation. This report must be completed by a suitably qualified person(s) or company.

(d) Offensive/Hazardous Development

Should it be required, a Preliminary Hazard Analysis will need to be prepared by an appropriately qualified consultant which assesses the suitability of the site and the associated risks and hazards with the site operations in accordance with the State Environmental Planning Policy No. 33 – Hazardous and Offensive Development (SEPP 33) and the relevant associated assessment guideline and advisory papers published by the NSW Department of Planning.

(e) Waste Management

Any application shall be accompanied with a Waste Management Plan addressing waste produced during the demolition, earthworks and construction phases of the development. It should address waste quantities, storage locations and removal.

A separate operational phase Waste Management Plan should be provided for each of the separate developments proposed on the subject property. It should address waste quantities, storage locations and removal. Vehicular access for collection also needs to be addressed. The waste storage areas should also be set off the boundary and covered to minimise nuisance from noise, odour and vermin.

(f) Dam De-Watering

Any application shall provide further details on the removal of the existing dam located on site and ecological impacts, in conjunction with a Dam De-Watering Plan. This Plan is to address, at a minimum is to detail safeguards and measures to mitigate impacts on adjacent residences and the environment in the form of an attached De-Watering Contingency Plan.

The Dam De-Watering Plan is to include a water quality assessment. The water quality assessment is to include (but not be limited to) the following parameters and contaminants:

- Temperate
- Turbidity (ntu)
- Dissolved oxygen (mg/L and % saturation)
- Biochemical Oxygen Demand (BOD)
- pH
- Salinity (ppt)
- Nutrients (Total Nitrogen and Phosphorus)
- Escherichia coli
- Faecal coliforms
- Copper
- Arsenic
- Cadmium
- Lead
- Mercury
- Zinc, and

- Any additional parameters as recommended by the environmental consultant.

The quality of the dam water is to be assessed against appropriate Australian and New Zealand Guidelines for Fresh and Marine Water Quality. The water quality parameters should be measured and reported in the same units expressed in the Australian and New Zealand Guidelines for Fresh and Marine Water Quality in a summary table with any exceedances highlighted. The number of water quality samples for testing is to be determined by the environmental consultant and should be representative of the volume and size of the dams.

For the Dam De-Watering Contingency Plan, the Plan is to outline any contingencies that may be required during the de-watering process, such as if the water quality results are to come back and are triggering Australian and New Zealand Guidelines for Fresh and Marine Water Quality threshold, then a sediment and assessment may also be required, as well as removal of both water and sediments to an offsite licensed waste facility.

Any Reports need to be completed by an appropriately qualified person(s) or company and be prepared in accordance with NEPM 2013 and the relevant EPA guidelines. An appropriately qualified person(s) is defined as “a person who, in the opinion of the Council, has a demonstrated experience or access to experience in hydrology, environmental chemistry, soil science, eco-toxicology, sampling and analytical procedures, risk evaluation and remediation technologies.

(g) Sewerage Infrastructure

Any application shall confirm that the proposed development will be connected to Sydney Water’s reticulated sewer and that capacity exists for the development.

(h) Biodiversity

An assessment shall be undertaken in accordance with the Biodiversity Offset Scheme for potential impacts on mapped vegetation. Further, a Test of Significance for vegetation on remaining parts of the site is to be completed. Consideration is to be given to the Biodiversity Values Map.

Furthermore, given that there is a riparian corridor traversing through the site, the objectives and controls in section 2.2 – Biodiversity of the Mamre Road Precinct Development Control Plan must be adhered to and implemented where appropriate.

(i) General Environmental Health Impacts

In any application, the environmental impacts associated with the construction phase of the development will need to be addressed, such as water quality, noise, dust/air quality and erosion and sediment control. This can be included in the Statement of Environmental Effects and plans.

3. ENGINEERING:

(a) Stormwater

- Stormwater drainage for the site must be in accordance with the Mamre Road Precinct DCP and shall meet the objectives of Section 2.4 Integrated Water Cycle Management.
- A stormwater concept plan, accompanied by a supporting report and calculations, shall be submitted any the application. The application shall include concept stormwater plans for both any interim and ultimate developments.
- The stormwater concept plan shall demonstrate how the development complies with the Waterway Health and Water Sensitive Urban Design Controls (No 1 – 9) from Section 2.4 Integrated Water Cycle Management of the Mamre Road Precinct DCP.
- Any application is to demonstrate how stormwater discharge from the proposed development complies with each of the Trunk Drainage Infrastructure Controls (No 10 – 22) from Section 2.4 Integrated Water Cycle Management of the Mamre Road Precinct DCP. As the contributing catchment is greater than 15ha a naturalised trunk drainage system is to be provided in accordance with the DCP. In relation to Control 16), Council will not accept trunk drainage pipes or culverts within the public road. Reference is also made to Section 2.2 Biodiversity and Section 2.3 Riparian Land of the DCP for the land identified as Environmental Conservation in Figure 2 of the DCP.
- A Water Management Strategy prepared by a suitably qualified person is to be provided for the site. The strategy shall address water conservation, water quality, water quantity, and operation and maintenance.
- The application shall include MUSIC modelling (*.sqz file) demonstrating compliance with water quality controls of the Mamre Road Precinct DCP.
- Penrith City Council will not accept the dedication of any estate water quantity or water quality basins. Any estate drainage basins are to be maintained in perpetuity by the estate. It is Council's preference that all water quantity and water quality treatment be provided on the individual lots. Any on-site detention system or water quality system must be within common property and accessible from the street.
- Any drainage basin shall be located clear of the 1% AEP flood extents from South Creek.

(b) Mainstream and Local Overland Flow Flooding

- The site has been identified as being subject to flood related development controls.
- The site is affected by mainstream flooding from South Creek and local overland flow flooding from the local catchment.
- Any application must demonstrate that the development proposal is consistent with the Objectives and Control No's 10 -15 of the Mamre Road Precinct DCP Section 2.5 Flood Prone Land.
- Any application must be accompanied by a Flood Report prepared by a suitably qualified person to assess the developments impacts upon regional flooding and local overland flow flooding.
- Overland flows shall be managed through the site in a safe manner.

- Any fencing of the area impacted by overland flows shall be of an open style fence to permit the passage of floodwaters.
- Further information regarding Council's Flood Studies is available from Council's website at the following address:
<https://www.penrithcity.nsw.gov.au/services/other-services/floodplain-management>

(c) Earthworks

- Any application shall demonstrate compliance with the Controls of Section 4.4 Earthworks and Retaining Walls of the Mamre Road Precinct DCP.
- A site cut / fill plan is to be submitted that includes all retaining walls, retaining wall heights and batter extents. The plan shall include any batters or retaining walls along the boundaries. The potential impact of any retaining walls upon future development of adjoining lands is also to be addressed.
- All retaining walls are to be located on private lands, all top of wall heights and locations are to be nominated with levels to AHD.
- No retaining walls or filling is permitted for this development which will impede, divert or concentrate stormwater runoff passing through the site.
- Any application is to be supported by a geotechnical report prepared by a suitably qualified person and shall address, but not be limited to ground water movement, salinity, contamination and potential damage to adjoining public and private infrastructure during construction.

(d) Subdivision Works

- Any application shall demonstrate compliance with the Controls of Section 3 Precinct and Subdivision Design of the Mamre Road Precinct DCP.
- Any application is to be accompanied by a subdivision concept plan.
- A Stage 2 Road Safety Audit is to be submitted with any application.

4. ROADS AND TRAFFIC:

- The proposed shared access road along the northern boundary will potentially conflict with the land identified as Environmental Conservation in Figure 2 of the DCP. It is recommended that consultation occur with the Department of Planning, Natural Resource Access Regulator (NRAR) and TfNSW for a suitable access location.
- If a shared access road is proposed along the common northern boundary, then the application shall include the adjoining landowners consent along with detailed plans of any required left turn slip / deceleration lanes and left turn acceleration lanes on Mamre Road. Correspondence from TfNSW for any proposed access arrangements off Mamre Road are to be provided with any application.
- The internal road network shall be designed in accordance with Section 3.4 of the Mamre Road Precinct DCP. Road widths and typologies shall be in accordance with Table 9 and Figures 14 and 16 of the DCP.
- The development shall deliver both the internal north-south Collector Industrial Road and Open Space Edge Road as per Figure 12 of the Mamre Road Precinct DCP.

- Long sections of the north-south Collector Industrial Road and Open Space Edge Road, extending into the adjoining properties, shall be provided with any application demonstrating the road levels will not hinder the future development of the adjoining lots or hinder extension of the road by way of excessive cut or fill.
- It is acknowledged that the internal north-south Collector Industrial Road will ultimately service adjoining properties to the north and south as per the DCP. As timing for the development of the lands to the north and south are unknown, details of any temporary turn-around facility at each end of the internal road are to be provided.
- Separate driveway access shall be provided for heavy vehicles and passenger vehicles from a public road. Driveway widths into the industrial lots shall comply with AS2890.2
- The road network is to be designed for 30m Performance Based Standards (PBS) Level 2 Type B vehicles and tested for a 36.5m PBS Level 3 Type A vehicles.
- Any application shall be supported by a detailed Traffic Impact Assessment (TIA) prepared by a suitably qualified person. The TIA shall include, but not limited to, the following:
 - Compliance with new Mamre Road Precinct DCP.
 - Demonstrated compliance of access, parking and manoeuvring with regard to AS2890.1, AS2890.2 and AS2890.6 with swept paths to be provided.
 - Adequate separation of heavy vehicle access and loading areas from car parking areas (including any internal pedestrian paths of travel).
 - Safe access of B-Double without conflicting with other vehicles/travel lanes, in accordance with Austroads with regard to requisite turn treatments at the intersection.
 - Frontage treatments must take into account future proposed shared/cycleway path and bus infrastructure (where relevant) on Mamre Road
 - Detailed traffic generation analysis.
 - Addressing all other items with regard to the issued SEARs.
- The development shall also include electric vehicle charging stations in car parking areas at a minimum of 5% of all spaces – charge station shall be located nearest the entry points to office buildings.

It is requested that the following documentation be submitted with any Development

Application:

- Detailed Survey Drawing
- Fully detailed architectural plan set
- Statement of Environmental Effects
- Long Sections of the Collector Road and Open Space Edge Road
- Large-scale Sections and Details through Site Boundaries and other Major Interfaces
- Hazardous Building Materials Survey
- Preliminary Hazard Analysis
- Phase 1 Preliminary Site Investigation or Phase 2 Detailed Site Investigation
- Remedial Action Plan (if required)
- Traffic and Parking Assessment Report
- Stage 2 Road Safety Audit
- Schedule of External Materials and Finishes
- Signage Details (if proposed)
- Flood Impact Risk Assessment
- Current Certificate of Title
- Site Plan
- Shadow Diagrams
- Air Quality Assessment
- Geotechnical Report
- Arborist Report
- Flora and Fauna Assessment and Management Report
- Detail and Evidence of Liaisons with TfNSW
- Context Plan demonstrating how development fits within the Structure Plan
- Landscape Plan Package
- Subdivision Concept Plan
- Stormwater Concept Plan
- Stormwater Management Strategy
- Dam De-Watering Plan and Dam De-Watering Contingency Plan – Accompanying details addressing ecology
- Bulk Excavation and Civil Works Plan Package
- Waste Management Plan(s)
- WSUD Strategy Plan with MUSIC modelling (*.sqz file)
- Services Report and Utilities Plan
- Operational Plan(s) of Management
- Noise Impact Assessment
- Building Code of Australia Report
- Access Report
- Environmentally Sustainable Development Report
- 3D electronic model in SketchUp Pro .SKP format
- CIV Estimate prepared by a registered Quantity Surveyor

Furthermore, please refer to Appendix B of the Mamre Road Precinct DCP for a list of documents and reports that are to be submitted with any application.

Sydney Water Services

It is recommended that Sydney Water is contacted to ascertain servicing availability. Please contact Sydney Water's Growth Planning and Development Team on 8849 4649 or email urbangrowth@sydneywater.com.au for this information.

Key Land Based Considerations

Bushfire Prone Land will likely require lodgement of a Bushfire Assessment Report.

Flood Affected Land will require floor levels to Australian Height Datum (AHD).

Impacts to native vegetation (including grassland) will require an assessment under the NSW Biodiversity Offset Scheme and may require a Biodiversity Assessment Report or a Test of Significance.

Christopher Curtis

From: Tim Smith <tsmith@wsaco.com.au>
Sent: Wednesday, 23 March 2022 6:07 PM
To: Christopher Curtis
Cc: Gordon Kirkby; Planning and Safeguarding
Subject: RE: 805-817 Mamre Road, Kemps Creek

Hi Chris,

Thanks for getting in touch – likewise hope you are well. We understand that your development comprises the provision of a warehouse and distribution centre at 805 Mamre Road.

WSA requests that the following matters be addressed in the SSD environmental assessment:

Trigger	General Comments
State Environmental Planning Policy (Precincts – Western Parkland City) 2021	The future Development Application needs to have regard for relevant objectives and provisions of <i>State Environmental Planning Policy (Precincts – Western Parkland City) 2021</i> , with particular focus on the provisions of Part 4.3 of Chapter 4 of the SEPP.
Wildlife Hazards	Given that the site is within the 3-8km wildlife buffer, consideration needs to be given to the landscape design and species selected, to ensure that wildlife attraction risk is adequately addressed. This includes in relation to potential wildlife risk of the riparian buffer and RE1 Public Recreation zoned areas
	Any proposed fill should be detailed, noting the fill needs to be non-putrescible.
	Measures to mitigate wildlife risk should be detailed, including measures to mitigate wildlife attraction (e.g. storage of waste indoors, use of fixed garbage lids).
Prescribed Airspace	<p>The proposal will need to assess the development's potential impacts on the Obstacle Limitation Surface (OLS), during construction and operation. It should be noted that the <i>Airports Act 1996</i> covers any intrusions into prescribed airspace, which could include:</p> <ol style="list-style-type: none">constructing permanent structures, such as buildings, into the protected airspace;temporary structures such as cranes protruding into the protected airspace; oractivities causing non-structural intrusions into the protected airspace such as air turbulence from stacks or vents, smoke, dust, steam or other gases or particulate matter. <p>If it is likely that any of the above components would result in an impact on protected airspace, approval may need to be obtained in accordance with the <i>Airports Act 1996</i> and the <i>Airports (Protection of Airspace) Regulations 1996</i>.</p>
Reflectivity	Solar panels proposed on the site should be designed and sited to avoid glare impacts to pilots.

Kind regards,

Tim

Tim Smith
Planning Manager
Airport Planning and Design

+61 429 008 963
tsmith@wsaco.com.au



From: Christopher Curtis
Sent: Friday, 18 March 2022 9:34 PM
To: tsmith@wsaco.com.au
Cc: Gordon Kirkby <GKirkby@ethosurban.com>
Subject: 805-817 Mamre Road, Kemps Creek

Hi Tim,

Hope you've been well.

We're currently in the final stages of preparing an EIS for 805-817 Mamre Road, Kemps Creek, within the Mamre Road Precinct, and wanted to see if you had any comments from an airport perspective on the proposal, which is for two industrial sheds. We've received Industry Specific SEARs from DPE for this, as the proposal is for State Significant Development.

If you're able to advise if the airport wishes to make comment, or alternatively if you wish to meet for a quick meeting the week commencing 21 March, that would be great.

I've attached the final plans for your perusal to assist.

Kind regards,

Chris

Christopher Curtis

Associate Director
Planning

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173 Sussex Street
Sydney NSW 2000
(Gadigal Land)



Ethos Urban acknowledges and pays respect to the past, present and emerging Traditional Custodians and Elders of this nation and the continuation of cultural, spiritual and educational practices of Aboriginal and Torres Strait Islander peoples.

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responsibility for virus checking. If the content of this email is personal or unconnected with our business, we accept no liability or responsibility for it.

Christopher Curtis

From: Jessica Braden <jessica.braden@nrar.nsw.gov.au>
Sent: Tuesday, 12 April 2022 9:49 AM
To: Christopher Curtis
Subject: 805-817 Mamre Road, Kemps Creek (SSD-30871587)

Hi Chris,

I am emailing in regards to 805-817 Mamre Road, Kemps Creek. The road on the northern side of the property should be in accordance with the Guidelines for Controlled Activities - Watercourse crossings. The design should maintain existing or natural hydraulic, hydrologic, geomorphic and ecological functions of the watercourse.

In the EIS submission please also address the setbacks provided for the riparian corridor and any works within waterfront land. The guidelines for Controlled Activities should be taken into consideration for any works on waterfront land. These include setbacks dependent on stream order, vegetation management and stormwater outlets.

A full assessment will be conducted at EIS stage.

Guidelines for Controlled Activity Approvals: <https://www.nrar.nsw.gov.au/how-to-apply/controlled-activities/guidelines-for-controlled-activities>

Kind regards,
Jessica

Jessica Braden
Water Regulation Officer
Licensing and Approvals
Department of Planning and Environment—Water

E jessica.braden@nrar.nsw.gov.au **W** www.nrar.nsw.gov.au/how-to-apply

PO Box 53, Wollongong NSW 2520



From 1 April 2022, all licensing and approvals functions undertaken by NRAR under the Water Management Act 2000 and Water Act 1912, have been transferred to the NSW Department of Planning and Environment—Water. From now on, please contact NSW Department of Planning and Environment—Water for your water licensing and controlled activity approval needs, at waterlicensing.servicedesk@dpie.nsw.gov.au

NRAR continues to be responsible for compliance and enforcement of water laws. You can contact NRAR in relation to compliance and enforcement matters at nrar.servicedesk@dpie.nsw.gov.au

I acknowledge the traditional custodians of the land and pay respects to Elders past and present.
I also acknowledge all the Aboriginal and Torres Strait Islander staff working with NSW Government at this time.

Christopher Curtis

From: Christopher Curtis
Sent: Wednesday, 6 April 2022 9:12 AM
To: NRAR Service Desk Mailbox
Subject: RE: FIN0652164 : NRAR - 805-817 Mamre Road, Kemps Creek - SSD Application trk:00430000823

Hi Ilse,

Following up on this to see if the below was received, and if NRAR have any comments or would prefer to wait till the SSD is formally referred by DPE?

Kind regards,

Chris

Christopher Curtis

Associate Director
Planning



T. +612 9956 6962
M. +61 419 660 592
W. ethosurban.com

173 Sussex Street
Sydney NSW 2000
(Gadigal Land)



Ethos Urban acknowledges and pays respect to the past, present and emerging Traditional Custodians and Elders of this nation and the continuation of cultural, spiritual and educational practices of Aboriginal and Torres Strait Islander peoples.

This email is confidential and may contain information that is confidential and privileged.

If you are not the intended recipient please notify us by return email or phone, and delete the original message.

From: Christopher Curtis
Sent: Thursday, 24 March 2022 9:34 AM
To: NRAR Service Desk Mailbox <nrar.servicedesk@dpie.nsw.gov.au>
Subject: RE: FIN0652164 : NRAR - 805-817 Mamre Road, Kemps Creek - SSD Application trk:00430000823

Hi Ilse,

See attached – hopefully these come through.

Kind regards,

Chris

Christopher Curtis

Associate Director
Planning

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Sydney NSW 2000
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If you are not the intended recipient please notify us by return email or phone, and delete the original message.

From: NRAR Service Desk Mailbox <nrar.servicedesk@dpie.nsw.gov.au>
Sent: Thursday, 24 March 2022 9:33 AM
To: Christopher Curtis <CCurtis@ethosurban.com>
Subject: RE: FIN0652164 : NRAR - 805-817 Mamre Road, Kemps Creek - SSD Application trk:00430000823

Hi Christopher,

Thank you for contacting Natural Resources Access Regulator.

Please note, there was no attachment in your email.

You can forward missing attachments in a reply email.

Kind regards,

Ilse

Reg Coord - Service Support Team

Natural Resources Access Regulator | Lands & Water Division | Department of Planning & Environment
Locked Bag 5022, Parramatta NSW 2124

T: 1800 633 362

E: nrar.servicedesk@dpie.nsw.gov.au

W: www.industry.nsw.gov.au/nrar

To contact the NRAR Hotline and make a suspicious activity report call: 1800 633 362



N
A

Read the NRAR Progress Report 2019-20

The Department of Planning, Industry and Environment acknowledges that it stands on Aboriginal land. We acknowledge the Traditional Custodians of the land and we show our respect for Elders past, present and emerging through thoughtful and collaborative approaches to our work, seeking to demonstrate our ongoing commitment to providing places in which Aboriginal people are included socially, culturally and economically.

----- Original Message -----

From: CS Connect Service Centre <cspconnect@service-now.com>;

Received: Thu Mar 24 2022 09:19:56 GMT+1100 (Australian Eastern Daylight Time)

To: nrar.servicedesk@dpie.nsw.gov.au <nrar.servicedesk@dpie.nsw.gov.au>; nrar.servicedesk@dpie.nsw.gov.au <nrar.servicedesk@dpie.nsw.gov.au>; nrar.servicedesk@dpie.nsw.gov.au <nrar.servicedesk@dpie.nsw.gov.au>; NRAR Service Desk <nrar.servicedesk@dpie.nsw.gov.au>;

Subject: FIN0652164 : NRAR - 805-817 Mamre Road, Kemps Creek - SSD Application

received from: CCurtis@ethosurban.com

Good morning,

We're acting on behalf of Gibb Group, the proponents for a warehouse and distribution centre development at 805-817 Mamre Road, Kemps Creek. We have received Industry Specific SEARs from DPE, and are currently preparing the SSD application for submission.

As part of the SEARs (attached), NRAR is to be consulted. I have attached a copy of the plan set for your information.

Ecologique are the biodiversity consultant on the project who are currently finalising their assessments – once complete, I will also pass this across.

If you would be able to provide comment, either via email or alternatively we are happy to have a Teams meeting, that would be greatly appreciated. I do note that the SSDA, when lodged, will be referred to NRAR so understand if commentary will be provided at that stage.

Kind regards,

Chris

Christopher Curtis

[cid:image001.png@01D83F5A.CADF0690] <<https://ethosurban.com/>>

Associate Director

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[cid:image002.png@01D83F5A.CADF0690] <https://twitter.com/Ethos_Urban>

[cid:image003.png@01D83F5A.CADF0690] <<https://www.linkedin.com/company/ethosurban/>>

[cid:image004.png@01D83F5A.CADF0690] <<https://www.facebook.com/Ethos-Urban-539964869726570/>>

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Ref:MSG5346188_lkWmRblibfwFDpmYkfxj

Christopher Curtis

From: Christopher Curtis
Sent: Tuesday, 12 April 2022 4:10 PM
To: Christopher Curtis
Subject: FW: Mamre Road Precinct - 200 Aldington Road

Christopher Curtis

Associate Director
Planning



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W. ethosurban.com

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Sydney NSW 2000
(Gadigal Land)



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If you are not the intended recipient please notify us by return email or phone, and delete the original message.

From: David Schwebel <David.Schwebel@planning.nsw.gov.au>
Date: Wednesday, 6 April 2022 at 9:26 am
To: Anthony McLandsborough <anthony.m@atl.net.au>, Marnie Stewart <Marnie.Stewart@environment.nsw.gov.au>, Susan Harrison <Susan.Harrison@environment.nsw.gov.au>
Cc: Jocelyn Dela-Cruz <Jocelyn.Dela-Cruz@environment.nsw.gov.au>, Christian Kublins <christian.kublins@in-dev.com.au>, Matthew Thiselton (<matthew.thiselton@gibbgroup.com.au>)
<matthew.thiselton@gibbgroup.com.au>, Gordon Kirkby <gkirkby@ethosurban.com>
Subject: RE: Mamre Road Precinct - 200 Aldington Road

Hi Anthony

I have discussed with Marnie, and the Environment and Heritage Group (formerly EES) waterway health requirements to be addressed as part of the EIS are contained in the Integrated Water Cycle Management section of the Mamre Road DCP and the MUSIC modelling toolkit. At this point, they have nothing additional to advise.

As you know, Sydney Water has been appointed as the stormwater management authority for the precinct. In case you don't already have it, a contact at Sydney Water is Greg Ingleton - gregory.ingleton@sydneywater.com.au.

If you have a preliminary design you want feedback on, or any particular questions, feel free to send them through to me and I can pass them on to the appropriate person for a response.

Please contact me if you would like to discuss.

David Schwebel

Planning Officer, Industry Assessments
Development Assessment

Department of Planning and Environment

T 02 9274 6400 E david.schwebel@planning.nsw.gov.au

dpie.nsw.gov.au

4 Parramatta Square
12 Darcy Street
Parramatta NSW 2150



I acknowledge the traditional custodians of the land and pay respects to Elders past and present. I also acknowledge all the Aboriginal and Torres Strait Islander staff working with NSW Government at this time.

From: Anthony McLandsborough <anthony.m@atl.net.au>

Sent: Tuesday, 5 April 2022 7:27 AM

To: Marnie Stewart <Marnie.Stewart@environment.nsw.gov.au>; Susan Harrison

<Susan.Harrison@environment.nsw.gov.au>; David Schwebel <David.Schwebel@planning.nsw.gov.au>

Cc: Jocelyn Dela-Cruz <Jocelyn.Dela-Cruz@environment.nsw.gov.au>; Christian Kublins <christian.kublins@in-dev.com.au>; Matthew Thiselton (matthew.thiselton@gibbgroup.com.au)

<matthew.thiselton@gibbgroup.com.au>; Gordon Kirkby <gkirkby@ethosurban.com>

Subject: RE: Mamre Road Precinct - 200 Aldington Road

Marnie further to your email below, we are engaged, as you are probably aware, across a number of SSD development application within both Mamre Road and the wider Aerotropolis and require direction on the following matter from you.

Specifically in relation to 805 Mamre Road Kemps Creek Logistics (SSD-30871587), we have been requested by the department to consult with you in relation to the project.

Are you available for a meeting or do you wish for the DPIE to arrange a meeting?

If you could come back as soon as possible please.

Employment) 2021 and/or any other contributions policy or plan including details of any plan agreements proposed to deliver infrastructure and services. During preparation of the EIS consultation must be undertaken with the relevant parties regarding any VPA required, and proposed, the EIS is to include evidence of an agreed Terms of Offer or agreed draft agreement executed with the relevant party.

Waterway health

- You are reminded that the Department strongly encourages you to consult with Environment Energy and Science Group and Sydney Water with regards to waterway health targets and drainage requirements for the precinct and include evidence of this consultation as part of EIS.

Regards,

Anthony McLandsborough | **National Managing Director**

t: 02 9439 1777 | 0433 973 423

e: anthony.m@atl.net.au

w: www.atl.net.au 

a: Level 7 153 Walker Street North Sydney NSW 2060

**CIVIL ENGINEERS | PROJECT MANAGERS | WATER SERVICING COORDINATORS |
INFRASTRUCTURE PLANNERS AND ADVISERS**



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