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ABN: 81 168 423 872

**TO** Stephanie Maxwell; Development Manager (Aliro Group)

**FROM** Mario Dizon Jr.; Traffic Engineer

**CC** Dr. Ali Rasouli, Principal Traffic Engineer

**SUBJECT** P1960 Prospect Logistics Estate, Pemulwuy (SSD-10399) – Modification 3

Dear Stephanie,

I refer to the State Significant Development (SSD-10399) of the Prospect Logistics Estate (PLE), situated between Foundation Place and Clunies Ross Street, Pemulwuy.

The project team is in the process of preparing the lodgement of a Modification 3 (the MOD 3) for the site, capturing changes to Lots 3, 4, 5, and 6 (herein referred to as the Collective Lot). In response, this Transport Statement has been prepared to support the transport and accessibility elements of the MOD 3 scope of works.

## Introduction & Project History

It is acknowledged that Ason Group have previously undertaken traffic studies and transport assessment for the PLE State Significant Development (SSD-10399), and Modification 2 (SSD10399-Mod-2). It is noted that SSD-10399 was approved on 2 July 2021<sup>1</sup>, while Modification 2 has received approval on 2 March 2022<sup>2</sup>.

The PLE State Significant Development (SSD-10399) relates to the construction and operation of a large format industrial warehouse, and distribution estate with associated ancillary office, and on-site car parking facilities. The PLE SSD-10399 included approved SSD yield as summarised in **Table 1**.

TABLE 1: APPROVED SSD YIELD				
Lot No.	Warehouse m <sup>2</sup> GFA	Office m <sup>2</sup> GFA	Café m <sup>2</sup> GFA	Car Parking provided
1	18,912	1,396		125
2	21,782	1,640		112
3	12,556	1,318		95
4	5,349	476		101
5	10,401	1,109		
6	8,441	1,013		51
7	8,927	893		50
Café Area			146	15
<b>Total</b>	<b>86,368</b>	<b>7,845</b>	<b>146</b>	<b>549</b>
<b>Total Building GFA</b>	<b>94,359</b>			NA

<sup>1</sup> <https://pp.planningportal.nsw.gov.au/major-projects/projects/prospect-logistics-estate>

<sup>2</sup> <https://pp.planningportal.nsw.gov.au/major-projects/projects/prospect-logistics-estate-mod-2-warehouse-7>

The approved Modification 1 involved the amendment of the internal storage limit of dangerous goods of Warehouse 2 (approved as a warehouse or distribution centre under PLE SSD-10399) only to allow tenants to effectively operate within the warehouse in accordance with the provisions of SEPP 33. The approved Modification 1 to SSD 10399 involves an increase to the approved quantity of dangerous goods stored within Warehouse 2. It is noted that Modification 1 did not require any transport assessment/ statement and has received approval on 21 December 2021.

The approved Modification 2 involved the amendment of Lot 7 Warehouse of the PLE to reflect minor updates to the proposed warehouse configuration, with changes to site circulation, parking provision and warehouse size. **Table 2** highlights the key changes, with the full scope of modification works detailed in the project submission:

TABLE 2: MODIFICATION 2 (LOT 7)			
Element	Approved Original Masterplan	Approved Mod 2 Yield	Difference
Lot 7 Warehouse GFA	8,927 m <sup>2</sup>	9,309 m <sup>2</sup>	+ 382 m <sup>2</sup>
Lot 7 Office GFA	893 m <sup>2</sup>	604 m <sup>2</sup>	- 289 m <sup>2</sup>
Lot 7 Catwalk GFA	n/a	407 m <sup>2</sup>	+ 407 m <sup>2</sup>
Lot 7 Workshop Mezzanine GFA	n/a	40 m <sup>2</sup>	+ 40 m <sup>2</sup>
Lot 7 GFA	9,820 m <sup>2</sup>	9,913 m <sup>2</sup> (assessable area <sup>1</sup> ) 10,360 m <sup>2</sup> (total area)	+ 93 m <sup>2</sup> (1)
Parking Provision	50 spaces	99 spaces	+ 49 spaces

Notes) 1. The assessable area does not include the Catwalk and Workshop Mezzanine areas as part of the assessable GFA for the warehouse.

**Figure 1** shows the approved Lot 7 site plan under the Modification 2. In addition to the abovementioned yield changes, an access crossover has been proposed to the west of the warehouse 7, as well as a dedicated access crossover to the parking area.

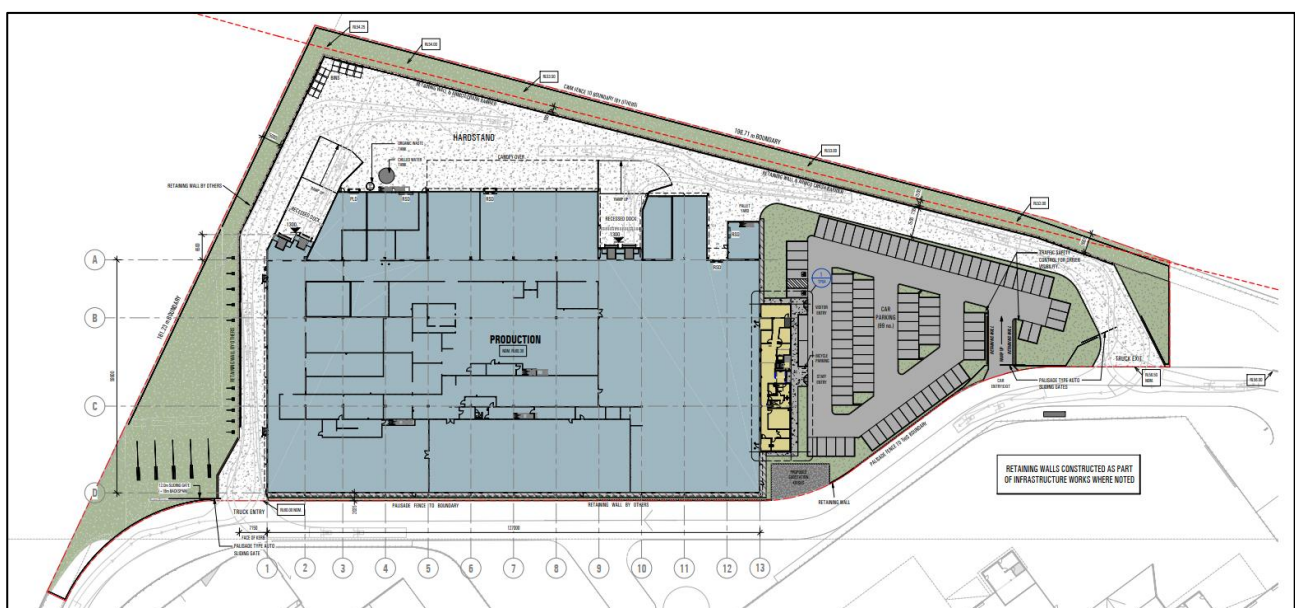


Figure 1: Modified Lot 7 Warehouse (MOD 2)

As discussed above, the MOD 3 (subject to this Transport Statement) only applies to the Collective Lots which is further described in below.

## Approved Site Plan for the Collective Lots

Below figure shows the latest approved site plan for the Collective Lots.

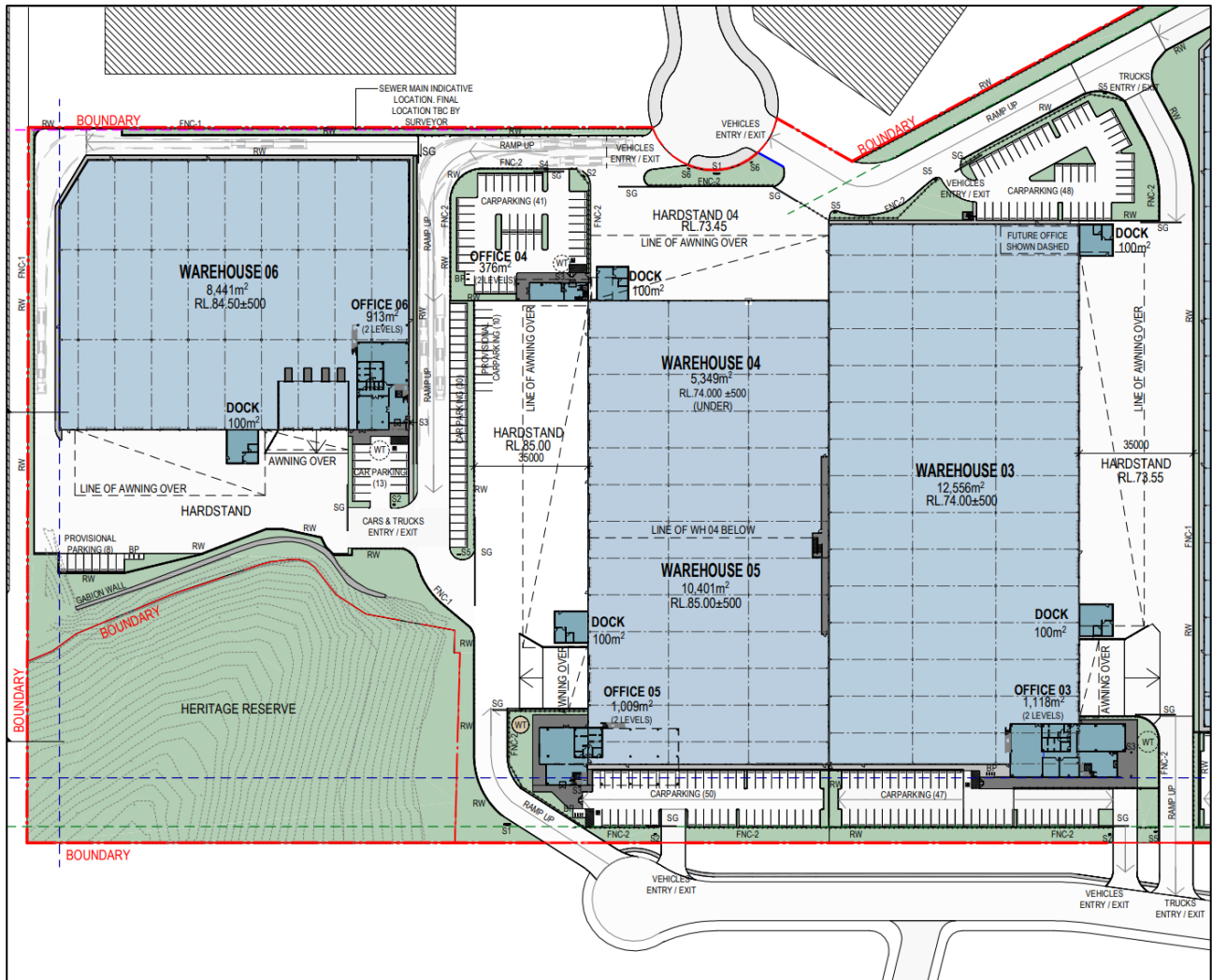


Figure 2: Approved Lot 3,4,5 and 6 Warehouse – Under Original SSD-10399 (approved 2 July 2021)

It is important to note that the previously approved modifications 1 and 2 did not impact the Collective Lots.

## MOD 3 Description

MOD 3 comprises:

- Changes to the functional layout and amalgamation of Warehouses 3, 4, and 5 into a single warehouse (herein referred to as Amalgamated Warehouse 3) which translates to:
  - minor decrease to the warehouse GFA from 28,306 m<sup>2</sup> to 27,041 m<sup>2</sup> (decrease of 1,265 m<sup>2</sup> GFA)
  - decrease to the office GFA from 2,903 m<sup>2</sup> to 1,377 m<sup>2</sup> (decrease of 1,526 m<sup>2</sup> GFA)
- Changes to the functional layout of Warehouse 6 (change from Ambient Warehouse to a Freezer and associated changes). These yield changes to the warehouse and office GFAs are as follows:
  - minor increase to the total warehouse 6 GFA from 8,441 m<sup>2</sup> to 8,885 m<sup>2</sup> (increase of 444 m<sup>2</sup> GFA)
  - minor decrease to the office GFA from 1,013 m<sup>2</sup> to 515 m<sup>2</sup> (decrease of 498 m<sup>2</sup> GFA)
- Changes to the parking numbers provided.

**Table 3** highlights the key changes, with the full scope of modification works detailed in the project submission:

<b>TABLE 3: DEVELOPMENT YIELD</b>								
Lot No.	APPROVED ORIGINAL MASTERPLAN			PROPOSED YIELD			DIFFERENCE	
	Warehouse m <sup>2</sup> GFA	Office m <sup>2</sup> GFA	Car Parking Provided	Warehouse m <sup>2</sup> GFA	Office m <sup>2</sup> GFA	Car Parking Provided	Warehouse/ (Office) m <sup>2</sup> GFA	Car Parking Provided
3,4,5	28,306 <sup>a</sup>	2,903 <sup>a</sup>	196 <sup>b</sup>	27,041 <sup>c</sup>	1,377	134	-1,265/ (-1526)	-62
6	8,441	1,013	51	8,885	515	40	+444/ (-498)	-11

Notes) a. This presents the total of the individual GFAs of warehouses and offices in Lots 3, 4, and 5 as seen in Table 1.  
 b. This presents the total of the car parking spaces provided in Lots 3, 4, and 5 as seen in Table 1.  
 c. Amalgamation of Warehouse 3, 4, 5 into a single warehouse.

**Figure 33** shows the revised masterplan for the Collective Lot within the PLE under the MOD.

In addition to the abovementioned yield changes, modifications to the access crossover design of Lots 3, 4, and 5 on Clunies Ross Street (east of the Amalgamated Warehouse 3) have been propositioned which highlights a single two-way dedicated crossover to the parking area.

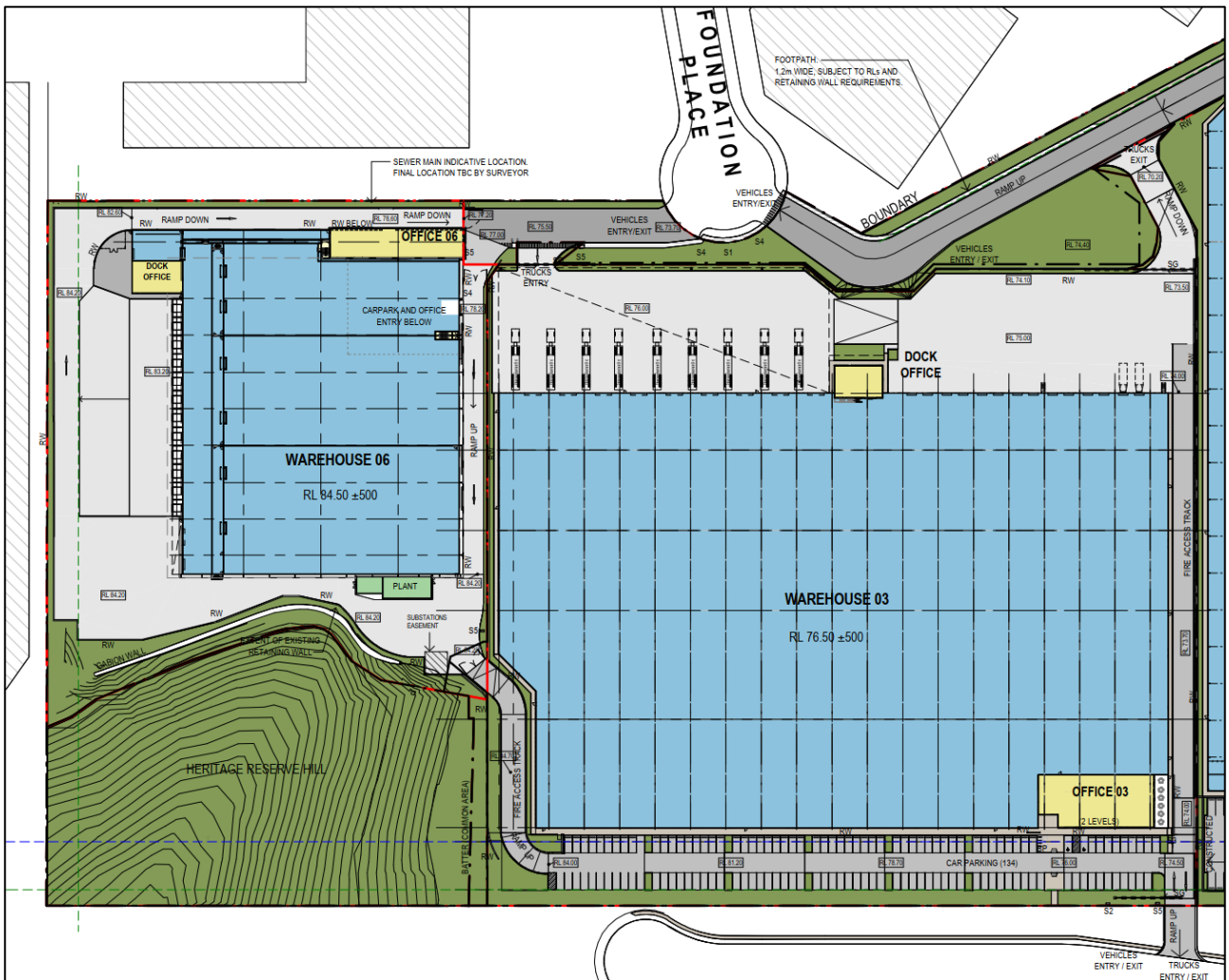


Figure 3: MOD3 Lot 3, 4, 5, and 6 Warehouse

## Traffic Generation

The traffic assessment accompanying the original SSD submission has been undertaken based on rates from the RMS Guide to Traffic Generation (2002) and RMS Technical Direction (2013/04a). The following rates were adopted for the approved assessment:

- 0.265 vehicle trips per 100m<sup>2</sup> of GFA per peak hour period; and
- 1.892 vehicle trips per 100m<sup>2</sup> of GFA per day

**Table 4** provides a comparative assessment of traffic generation between the approved masterplan and MOD 3.

TABLE 4: TRAFFIC GENERATION									
Element	Approved Masterplan <sup>a</sup>			Modification 3			Difference		
Lot	Lot 3, 4, and 5	Lot 6	Sub-total	Amalgamated Warehouse 3	Lot 6	Sub-total	Amalgamated Warehouse 3	Lot 6	Sub-total
AM Traffic	83 (37 + 15 + 31)	25	108	72	25	97	-11	0	-11
PM Traffic	83 (37 + 15 + 31)	25	108	72	25	97	-11	0	-11
Daily Traffic	582 (254 + 110 + 218)	179	761	538	178	716	-44	-1	-45

Notes) a. Traffic generation for Lots 3, 4, 5, and 6 are reproduced from the Transport Assessment prepared by Ason Group (AG ref: P1227r01v2\_SSD TIA\_Prospect Logistics Estate (SSD-10399))  
<https://majorprojects.planningportal.nsw.gov.au/prweb/PRRestService/mp/01/getContent?AttachRef=SSD-10399%2120200819T015354.735%20GMT>

The above table demonstrates that the changes in GFA over the approved site plan for Warehouses 3, 4, 5, and 6 are anticipated to reduce the net traffic generation of the PLE to the site during the road network AM and PM peak periods as well as the daily vehicle trips.

**Therefore, there will be minor decrease in traffic generated compared to what was previously approved.**

Accordingly, it has been considered that the traffic generation associated with the MOD 3 translates to a better traffic outcome from the original SSD (SSD-10399) when adopting approved traffic generation rates. Therefore, no additional modelling/ detailed traffic assessment, (including an assessment of the intersection of Foundation Place and Prospect Highway) is warranted, and the MOD 3 is deemed to be supportable.

## Parking Provision

### Car Parking

As part of the SSD approval, the parking demand vs. supply assessment was undertaken against several controls due to the sites' location between the Blacktown City Council DCP and Cumberland City Council DCP 2021 (formerly Holroyd DCP 2013), as well as the RMS Guide. In this instance, the applicable **approved** parking rates to the MOD 3 are provided in **Table 5**.

TABLE 5: APPROVED PARKING RATES		
Land Use	Cumberland City Council DCP 2021	RMS Guide
Warehouse / Industrial	1 space per 300 m <sup>2</sup> GFA	1 space per 300 m <sup>2</sup> GFA
Office	1 space per 40 m <sup>2</sup> GFA	1 space per 40 m <sup>2</sup> GFA



The Development Consent issued for the original SSD (SSD-10399) on 2 July 2021 stipulated development controls in relation to car parking which also shows the above parking rates.

Based on this and proposed development yield shown in Table 3, the Collective Lot is required to provide a sum of 167 spaces as shown in more detail in **Table 66**. Accordingly, the Collective Lot was subsequently approved for provision of 247 spaces.

An assessment of the revised Collective Lot yields for the MOD 3 has demonstrated a provision of 174 spaces. It is also noted in Table 3 that the Amalgamated Warehouse 3 and Proposed Warehouse 6 present a decrease in parking spaces by 62 and 11 spaces respectively compared to the original approval.

As indicated in Table 66, the Amalgamated Warehouse 3 meets and exceeds the required on-site parking as per the Council parking rates whilst Warehouse 6 presents a shortfall of 3 spaces. A provisional parking of 4 spaces will be provided on the hardstand area to compensate this shortfall.

As such the proposed parking is considered acceptable and readily satisfies the requirements of the Development Consent and accommodates the anticipated operational requirements of the site and ensures all parking demand including ad-hoc demands are kept within the site.

<b>TABLE 6: PARKING PROVISION</b>							
<b>Lot</b>	<b>Use</b>	<b>Parking Rate</b>	<b>Yield m<sup>2</sup> GFA</b>	<b>Parking Required</b>	<b>Total Parking Required</b>	<b>Parking Provided</b>	<b>Difference</b>
<b>Amalgamated Warehouse 3 (formerly WH 3,4,5)</b>	Warehouse	1 space per 300 m <sup>2</sup> GFA	27,041	90	124	134	+10
	Office	1 space per 40 m <sup>2</sup> GFA	1,377	34			
<b>Warehouse 6</b>	Warehouse	1 space per 300 m <sup>2</sup> GFA	8,885	30	43	40	-3
	Office	1 space per 40 m <sup>2</sup> GFA	515	13			
<b>Total</b>					167	174	+7

### Accessible Parking

The Development Consent issued for the original SSD (SSD-10399) on 2 July 2021 stipulated development controls in relation to accessible parking. It is stated in the Development Consent that for accessible parking, the rate of 2 spaces for every 100 car parking spaces must be utilised.

Based on the parking provision of 174 spaces, the Collective Lot is required to provide 3 accessible spaces (2 spaces in the Amalgamated Warehouse 3; 1 in Proposed Warehouse 6).

MOD 3 provides 2 accessible spaces in the Amalgamated Warehouse 3. As for Warehouse 6, the number of accessible spaces is not specified. As such it is recommended for Warehouse 6 to provide at least 1 accessible parking space. The total of which equates to 3 accessible spaces for the Collective Lot, satisfying the requirements of the Development Consent.

It is noted that a total of 8 accessible parking spaces have been provided as part of the original SSD (SSD-10399) which are distributed throughout parking areas of the development.

### Bicycle Parking

The Cumberland City Council DCP 2021 and the Development Consent do not specify parking requirements for the provision of on-site bicycle parking. With that, reference has been made to the Planning Guidelines for Walking and Cycling 2004. Based on Table 3 of the aforesaid guide, it assumed that the Amalgamated

Warehouse 3 falls under the staff bracket of 50-149 staff, while Warehouse 6 is within the 13-49 staff bracket. In this instance, the higher end of each bracket is considered for a conservative assessment.

Additionally, the above-mentioned guide provides the following bicycle parking rates

- Staff parking of 3-5% of staff,
- Visitor parking: 5-10% of staff,

Based on the assumed brackets and the higher end of the bicycle parking rates, the warehouses present the following bicycle parking requirements:

- Amalgamated Warehouse 3
  - 8 spaces for staff
  - 15 spaces for visitors,
- Warehouse 6
  - 3 spaces for staff
  - 5 spaces for visitors

Consideration of end-of-trip facilities is however important with respect to staff with the development plans indicating adequate storage within the office and hardstand areas for the storage of the required number of spaces for staff.

Notwithstanding, the need or otherwise of bicycle parking for staff and/ or visitors can be requested as a suitable Condition of Consent (CoC) which can readily be provided at the time of Construction Certificate (CC).

## Design Commentary

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The hardstand area, accesses and parking areas have generally been assessed against compliance with the following relevant Australian Standards:

- AS2890.1:2004 for car parking areas,
- AS2890.2:2018 for commercial vehicle loading areas; and
- AS2890.6:2009 for accessible (disabled) parking.

It is noted that the ramp gradient review for the proposed MOD site plan was excluded from our scope of work noting that the Project Civil Engineer is expected to review the ramp designs.

It is expected that any detailed construction drawings relating to the hardstand area, accesses and parking lot shall comply with these Standards. Furthermore, Australian Standards compliance is expected to form a standard condition of consent to any development approval.

Detailed Design will be undertaken based on relevant Australian Standards (AS2890 series) as part of the Construction Certificate (CC). The review undertaken as part of this MOD suggest that a detailed Operational Traffic Management Plan (OTMP), which is outside of the scope of this transport statement, is deemed necessary for both warehouses and car parking facilities

Reference should be made to the design commentary provided in **Attachment A**.

This design commentary can be summarised into the below key points.



- For both warehouses, the typical service vehicle used were 20 metres Articulated Vehicles (AV), 12.5m Heavy Rigid Vehicle (HRV), and 8.8m Medium Rigid Vehicle (MRV). The largest vehicle to enter the site is a 26m B-Double (considered as the design vehicle).
- Our assessment assumes vehicles smaller than 26 metre B-Doubles are able to undertake reverse movement for loading / unloading at Roller Shutter Doors (RSDs).
- 26 metres B-Doubles are assumed for side loading only (for both warehouses)
- In relation to the Amalgamated Warehouse 3:
  - It is assumed the crossovers on the western end of the site (from Foundation Place) to be one-way with truck movements to be anti-clockwise for both speculative sheds.
  - The RSDs nearest to the site entry are to be used by the following vehicles:
    - 6 RSDs for 20m AV (or smaller) use only
    - 2 RSDs for 12.5m HRV (or smaller) use only
    - 1 RSD for 8.8m MRV (or smaller) use only
    - 12 RSDs on the Flush Docks are to be used by 20m AV (or smaller).
    - 2 RSDs on the Flush Docks are to be used by 12.5m HRV (or smaller).
    - 26m B-Doubles are to be allowed for side loading only on any of the docks.
  - Two options are assessed for Fire Truck route access to the site.
    - Option 1 includes Fire Truck entry at Foundation Place access and traverse through the Fire Access Track towards the carpark area and exiting at Clunies Ross Street.
    - Option 2 highlights Fire Truck entry at the egress point of the site and traverse through the Fire Access Track (north of the warehouse) and then existing at Clunies Ross Street.
    - Both options to be reviewed and agreed by the fire consultants. Both options require modifications to the site plan.
- In relation to Warehouse 6:
  - The recessed docks are to be used by the following vehicles
    - 10 Docks for 20m AV use only
    - 2 Docks for 12.5m HRV use only
  - 26m B-doubles are to be allowed for side loading on any of the docks. (Noting that a 26m B-Double will not be able to straighten its vehicle body while loading to allow for egress from the site).
  - The under croft parking shall form compliance with AS2890.1:2004 and AS2890.2:2018.

## Summary

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Based on the above assessments undertaken for the works captured as part of the MOD 3, Ason Group has concluded the following:

- The GFA changes included in this MOD 3 are anticipated to reduce the net traffic generated by the PLE to the site during AM and PM peaks as well as daily vehicle trips where there will be minor decrease in traffic generated compared to what was previously approved. Therefore, this MOD 3 translates to a better traffic outcome compared to the original SSD (SSD-10399).
- The car parking provision of this MOD 3 is in line with the approved car parking provision and readily satisfies DCP requirements.

The proposed modifications to the internal site design are expected to provide compliance with relevant Australian Standards. It is expected that any detailed construction drawings relating to the hardstand area, accesses and parking lot shall comply with these Standards. Furthermore, Australian Standards compliance is expected to form a standard condition of consent to any development approval.

As such, Ason Group has determined that the proposed changes can be considered acceptable on traffic and parking impact assessment grounds.

Yours sincerely,



**Mario Dizon Jr.**

*Traffic Engineer*

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# Attachment A:

## Swept Paths & Design Review

The lot, hardstand area and parking space have been assessed in accordance with

- *AS2890.1:2009 Off-street car parking*
- *AS2890.2:2018 Off-street commercial vehicle facilities*
- *AS2890.6:2009 Off-street parking for people with disabilities*

NOTE: Assessment of the ramp has not been undertaken and the design of the ramp is to be done by the civil engineer as per Client's advice.

Parking spaces have been assessed against User Class 1A. Parking spaces shall be kept clear of pipes, conduit and retain unobstructed headroom space.

Largest Vehicle to enter Site:

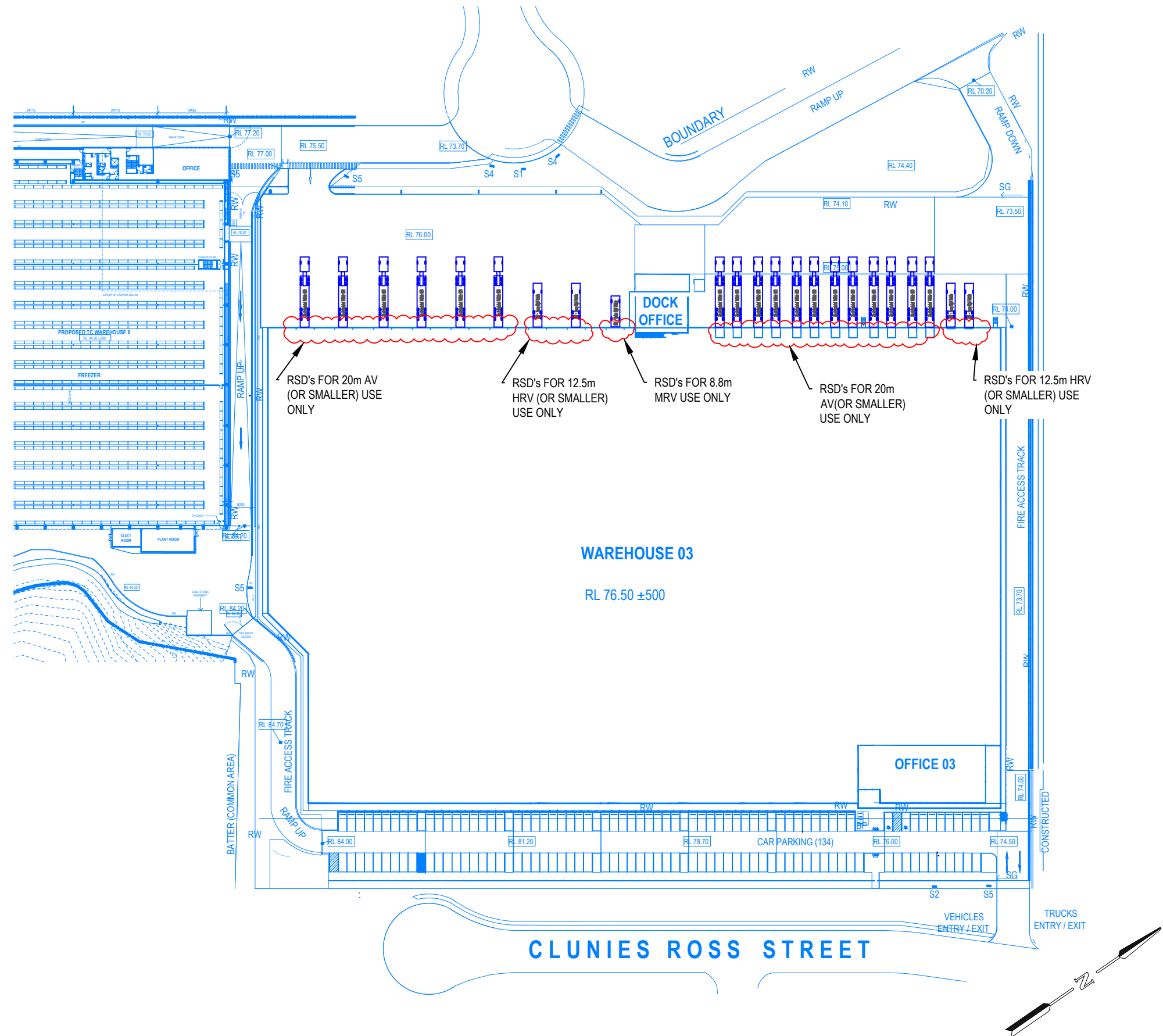
- 26m B-Double


Typical Service Vehicle:

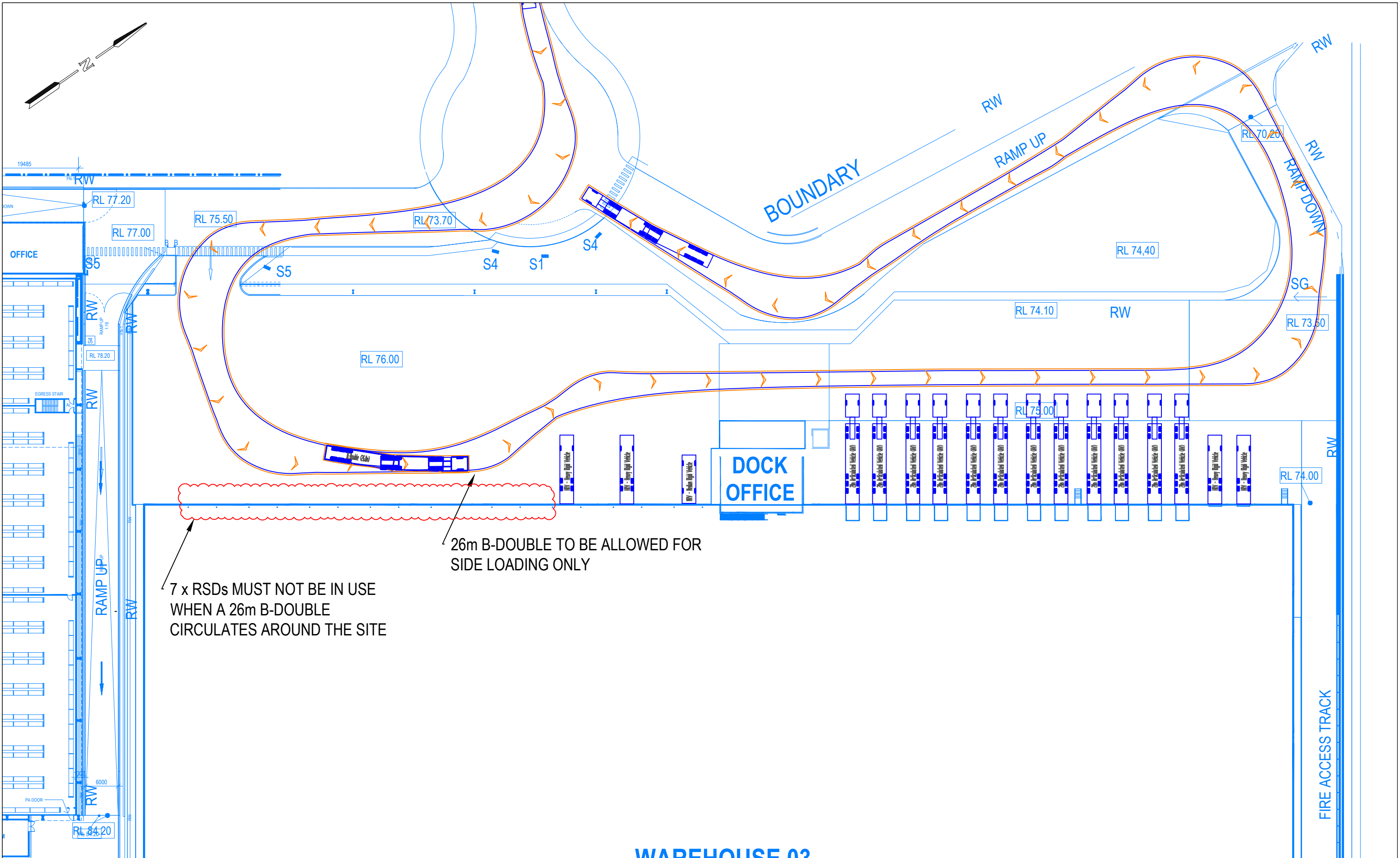
- 20m Articulated Vehicle
- 12.5m Heavy Rigid Vehicle
- 8.8m Medium Rigid Vehicle

Fire Service Vehicle:

- 12.5m Heavy Rigid Vehicle (Aerial Appliance)



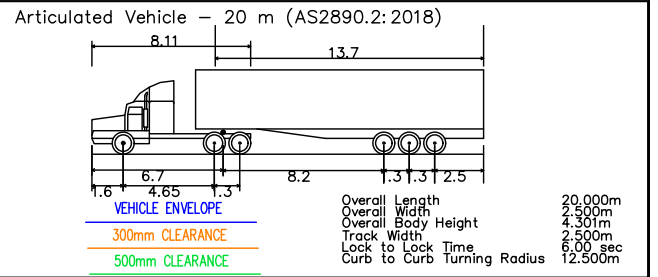
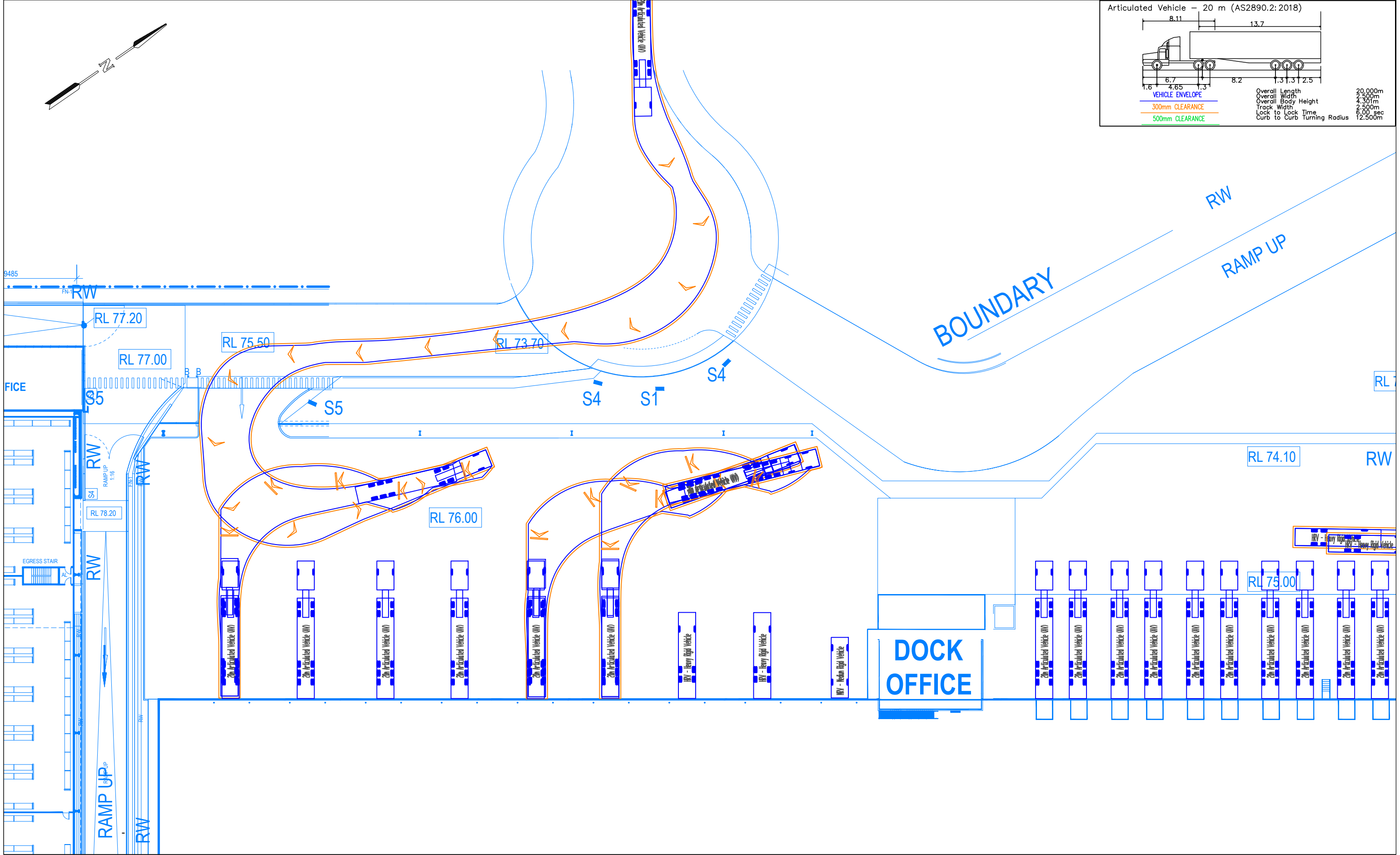
<div>GENERAL NOTES</div> <div>This drawing is provided for information purposes only and should not be used for construction. Base Plan prepared by Aliro Group, received 22 July 2022 (WH3) and 18 July 2022 (WH6). Swept path assessments completed at 10 km/h and 300mm clearance.</div>	DESIGNED Mario Dizon	PAPER SIZE A3	CLIENT ALIRO GROUP	DOCUMENT INFORMATION DESIGN REVIEW		<div></div> <div>Suite 17.02, Level 17, 1 Castlereagh St Sydney NSW 2000 info@asongroup.com.au</div>
	APPROVED BY X.XXXX	DATE 26.07.2022	PROJECT P1960  FOUNDATION PLACE AND CLUNIES ROSS STREET	OVERVIEW - PROPOSED WAREHOUSE 3 (AMALGAMATED)		
	SCALE Custom	NTS		FILE NAME AG1960-01-V03.dwg	SHEET AG01	



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	APPROVED BY X.XXXX	DATE 26.07.2022	PROJECT P1960	DESIGN REVIEW	
	SCALE Custom	NTS	FOUNDATION PLACE AND CLUNIES ROSS STREET	ACCESS AND EGRESS 36m B-DOUBLE	
				FILE NAME AG1960-01-V03.dwg	SHEET AG02

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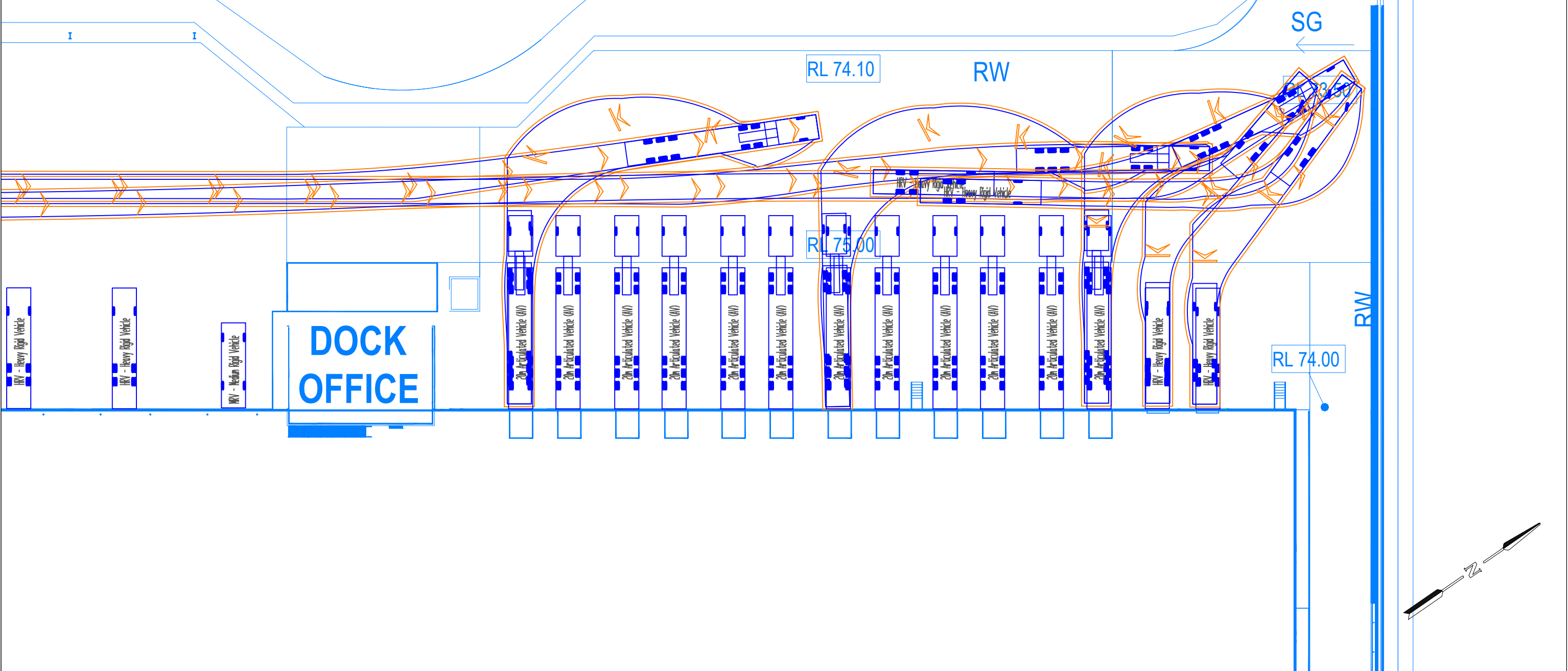
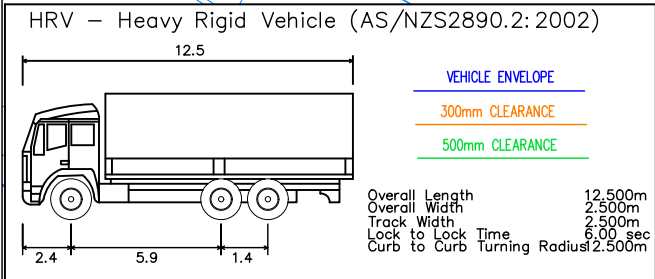
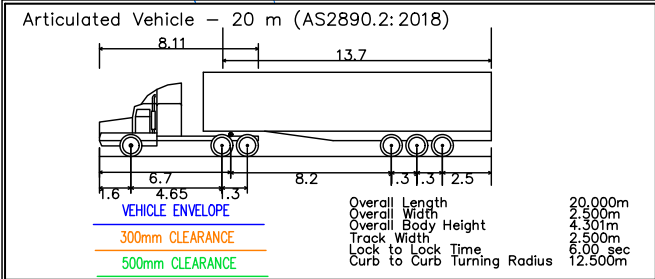


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	APPROVED BY X.XXXX	DATE 26.07.2022	PROJECT P1960			
	SCALE 1:500	0 5 10	FOUNDATION PLACE AND CLUNIES ROSS STREET	FILE NAME AG1960-01-V03.dwg	SHEET AG03	

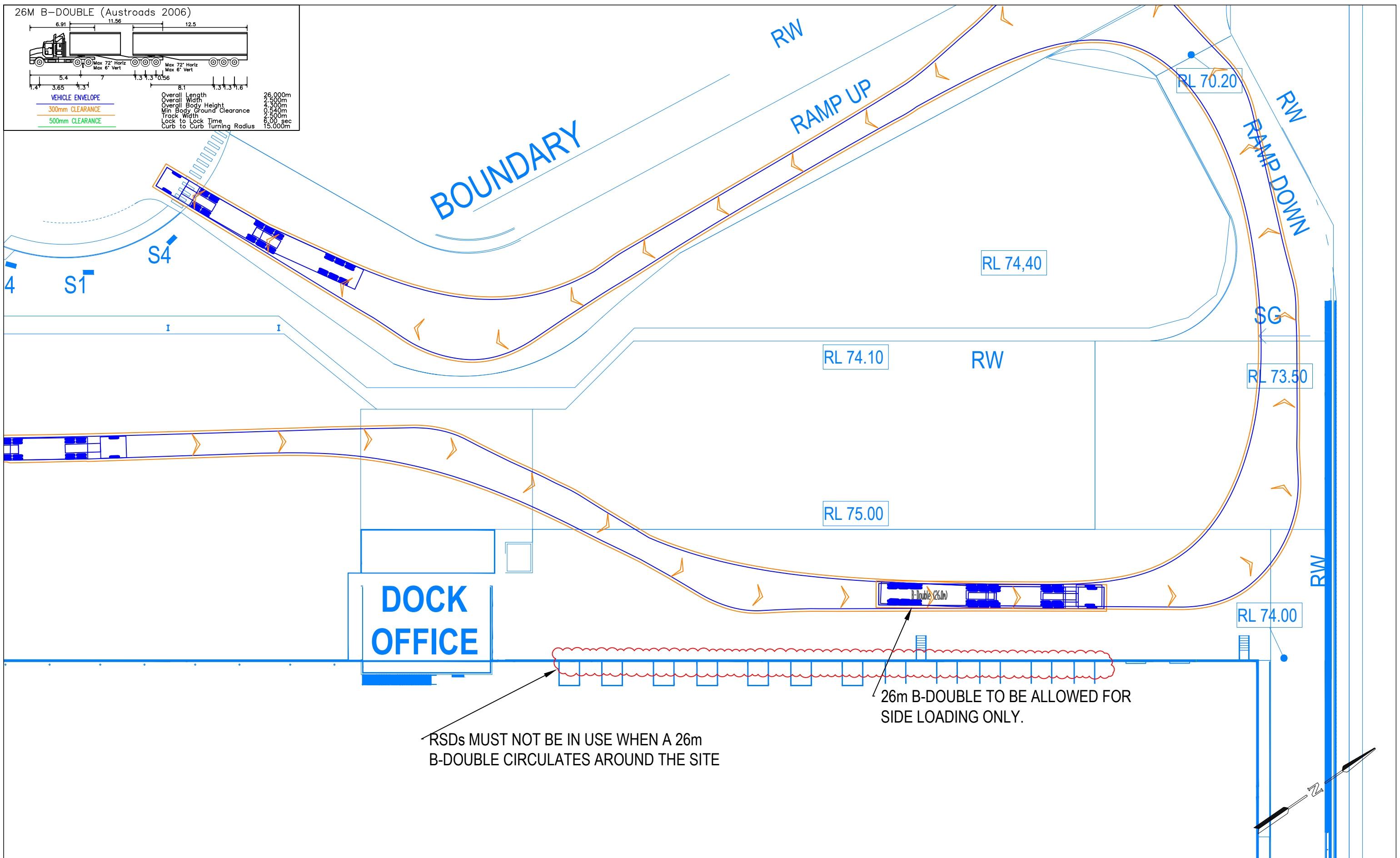










GENERAL NOTES	DESIGNED	PAPER SIZE	CLIENT	DOCUMENT INFORMATION	
	Mario Dizon	A3	ALIRO GROUP		
	APPROVED BY	DATE	PROJECT	DESIGN REVIEW	
	X.XXXX	26.07.2022	P1960	20m AV and 12.5m HRV ON FLUSH DOCKS	
This drawing is provided for information purposes only and should not be used for construction. Base Plan prepared by Aliro Group, received 22 July 2022 (WH3) and 18 July 2022 (WH6).. Swept path assessments completed at 10 km/h and 300mm clearance.	SCALE	0 4 8		FILE NAME	SHEET
	1:400			AG1960-01-V03.dwg	AG06
			FOUNDATION PLACE AND CLUNIES ROSS STREET		
				Suite 17.02, Level 17, 1 Castlereagh St Sydney NSW 2000 info@asongroup.com.au	



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	<b>APPROVED BY</b> X.XXXX	<b>DATE</b> 26.07.2022	<b>PROJECT</b>  P1960	26m B-DOUBLE ON FLUSH DOCKS		
	<b>SCALE</b> 1:400		FOUNDATION PLACE AND CLUNIES ROSS STREET	<b>FILE NAME</b> AG1960-01-V03.dwg	<b>SHEET</b> AG07	

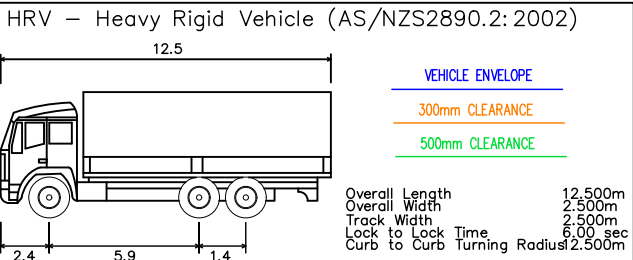
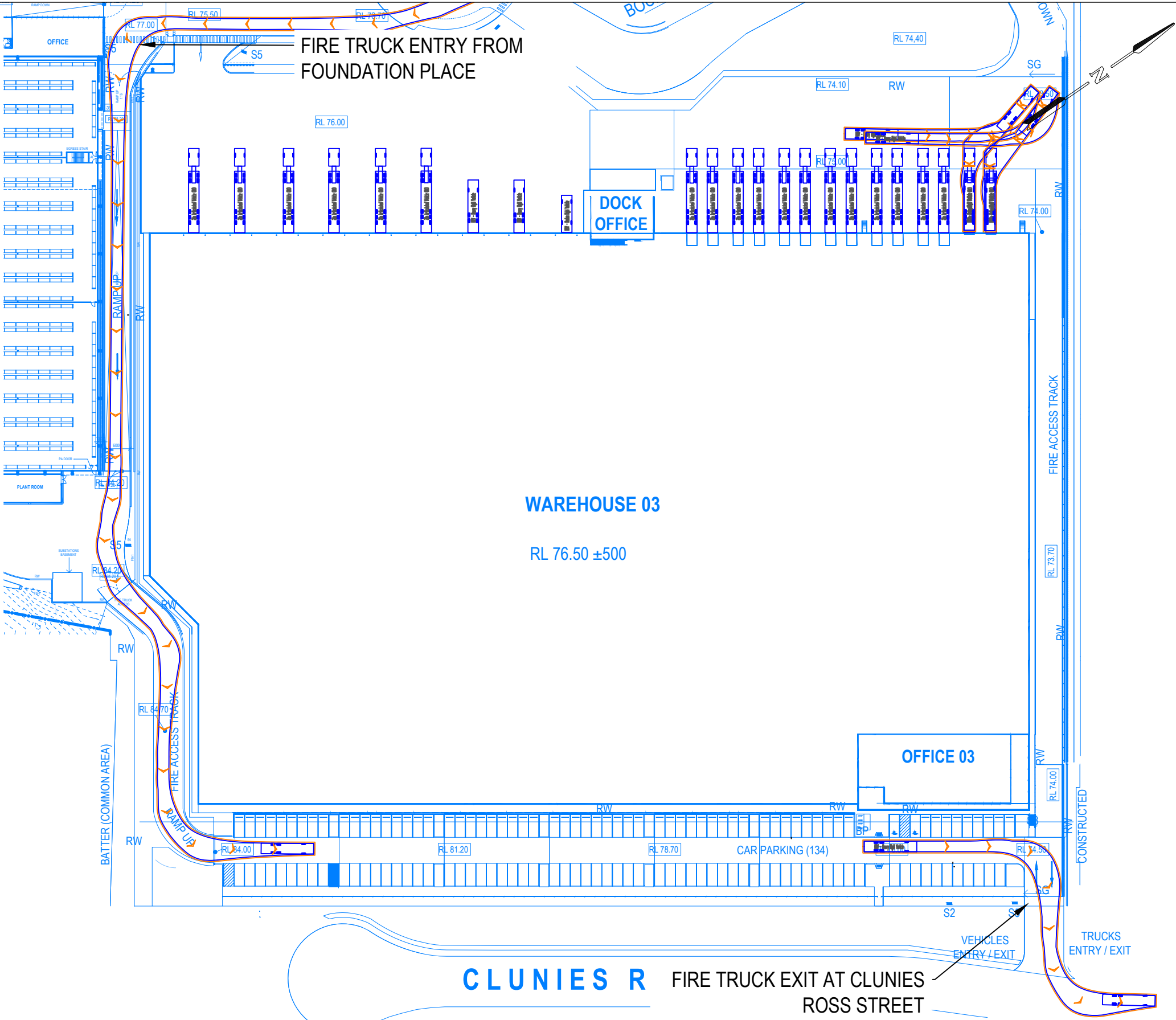


- Notes:
- The lot, hardstand area and parking space have been assessed in accordance with
- AS2890.1:2009 Off-street car parking
  - AS2890.2:2018 Off-street commercial vehicle facilities
  - AS2890.6:2009 Off-street parking for people with disabilities

NOTE: Assessment of the ramp has not been undertaken and the design of the ramp is to be done by the civil engineer as per Client's advice.

Parking spaces have been assessed against User Class 1A. Parking spaces shall be kept clear of pipes, conduit and retain unobstructed headroom space.

- Largest Vehicle to enter Site:
- 26m B-Double
- Typical Service Vehicle:
- 20m Articulated Vehicle
  - 12.5m Heavy Rigid Vehicle
  - 8.8m Medium Rigid Vehicle
- Fire Service Vehicle:
- 12.5m Heavy Rigid Vehicle (Aerial Appliance)



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	Mario Dizon	A3	ALIRO GROUP	DESIGN REVIEW		
	APPROVED BY	DATE	PROJECT	FIRE TRUCK ACCESS ROUTE OPTION 1 - OVERVIEW		
	X.XXXX	26.07.2022	P1960			
SCALE		NTS	FOUNDATION PLACE AND CLUNIES ROSS STREET	FILE NAME	SHEET	
Custom				AG1960-01-V03.dwg	AG08	

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HRV – Heavy Rigid Vehicle (AS/NZS2890.2:2002)

12.5

VEHICLE ENVELOPE

300mm CLEARANCE

500mm CLEARANCE

Overall Length

Overall Width

Track Width

Lock to Lock Time

Curb to Curb Turning Radius

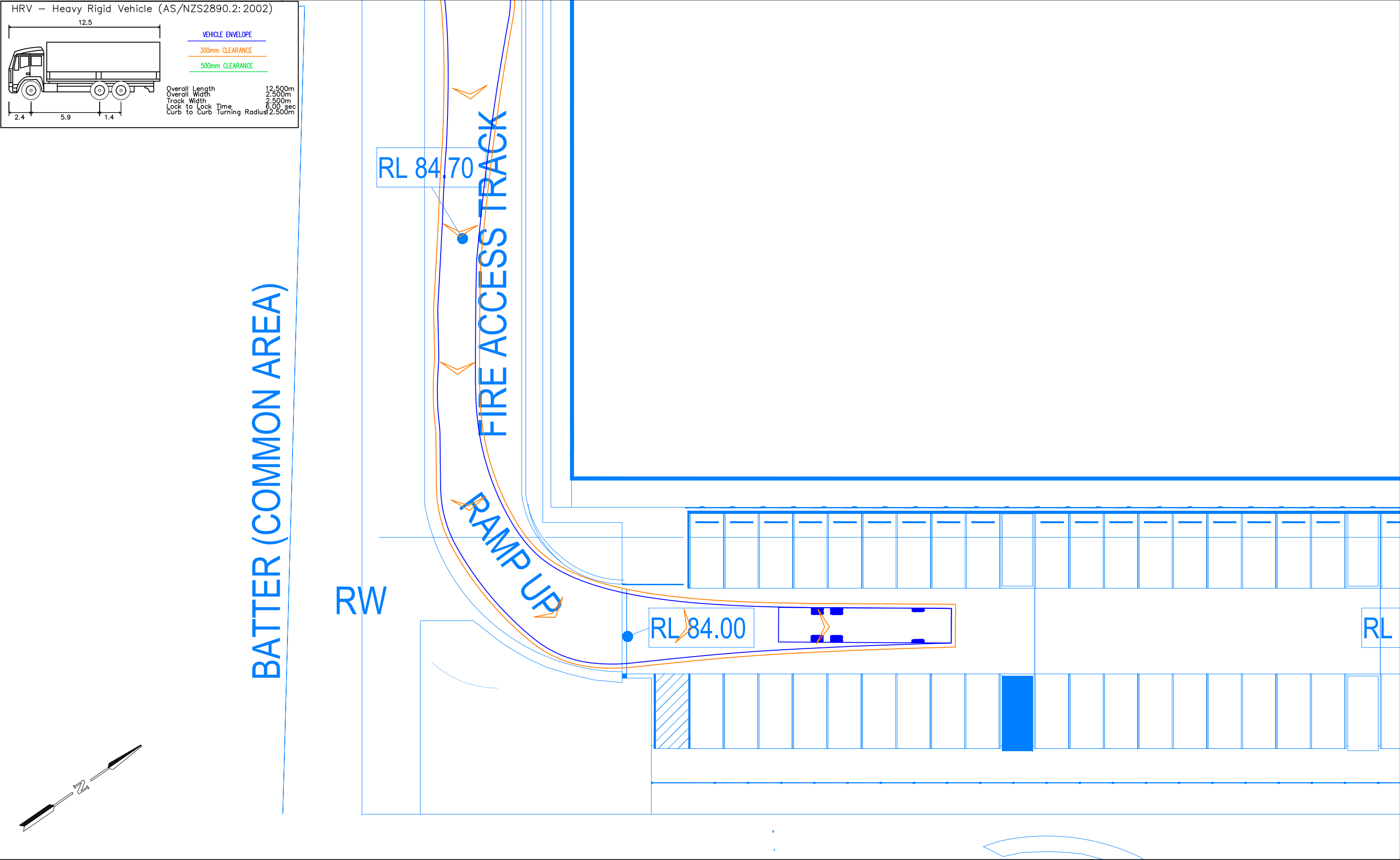
12,500m



2,500mm

2,500mm

6.00 sec

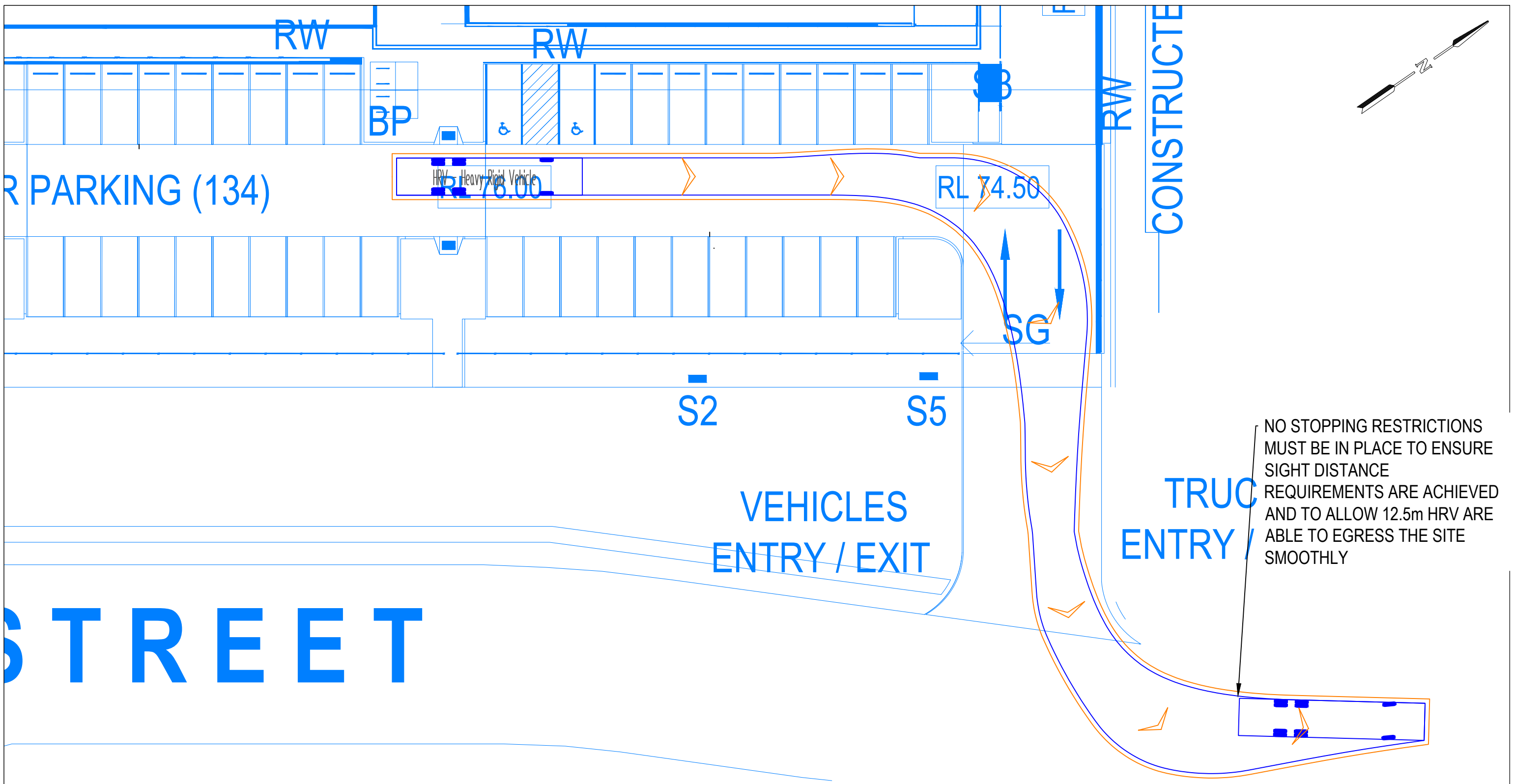
12,500m



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	<div>APPROVED BY</div> <div>X.XXXX</div>	<div>DATE</div> <div>26.07.2022</div>	<div>PROJECT</div> <div>P1960</div>	<div>FIRE TRUCK ACCESS ROUTE OPTION 1 - 12.5m HRV</div>		
	<div>SCALE</div> <div>1:250</div>	<div></div>	<div>FOUNDATION PLACE AND CLUNIES ROSS STREET</div>	<div>FILE NAME</div> <div>AG1960-01-V03.dwg</div>	<div>SHEET</div> <div>AG09</div>	



ASON GROUP LTD. 10/01/2022 12:54:35 PM | CAD REFERENCE: C:\Users\Mario Dizon\Desktop\CAD\P1960 Prospect Logistic Estate WH3,4,5,6\AG1960-01-V03.dwg | Mario Dizon |



NO STOPPING RESTRICTIONS  
MUST BE IN PLACE TO ENSURE  
SIGHT DISTANCE  
REQUIREMENTS ARE ACHIEVED  
AND TO ALLOW 12.5m HRV ARE  
ABLE TO EGRESS THE SITE  
SMOOTHLY

HRV – Heavy Rigid Vehicle (AS/NZS2890.2: 2002)

VEHICLE ENVELOPE

300mm CLEARANCE

500mm CLEARANCE

Overall Length

Overall Width

Track Width

Lock to Lock Time

Curb to Curb Turning Radius



12.500m

2.500m

2.500m

6.00 sec

2.500m

<div>GENERAL NOTES</div> <div>This drawing is provided for information purposes only and should not be used for construction. Base Plan prepared by Aliro Group, received 22 July 2022 (WH3) and 18 July 2022 (WH6). Swept path assessments completed at 10 km/h and 300mm clearance.</div>	<div>DESIGNED</div> <div>Mario Dizon</div>	<div>PAPER SIZE</div> <div>A3 OR A1</div>	<div>CLIENT</div> <div>ALIRO GROUP</div>	<div>DOCUMENT INFORMATION</div> <div>DESIGN REVIEW</div>		<div></div> <div>Suite 17.02, Level 17, 1 Castlereagh St Sydney NSW 2000 info@asongroup.com.au</div>
	<div>APPROVED BY</div> <div>X.XXXX</div>	<div>DATE</div> <div>26.07.2022</div>	<div>PROJECT</div> <div>P1960</div>	<div>FIRE TRUCK ACCESS ROUTE OPTION 1 - 12.5m HRV</div>		
	<div>SCALE</div> <div>1:250</div>	<div></div>		<div>FILE NAME</div> <div>AG1960-01-V03.dwg</div>	<div>SHEET</div> <div>AG10</div>	
			<div>FOUNDATION PLACE AND CLUNIES ROSS STREET</div>			

HRV – Heavy Rigid Vehicle (AS/NZS2890.2:2002)

12.5

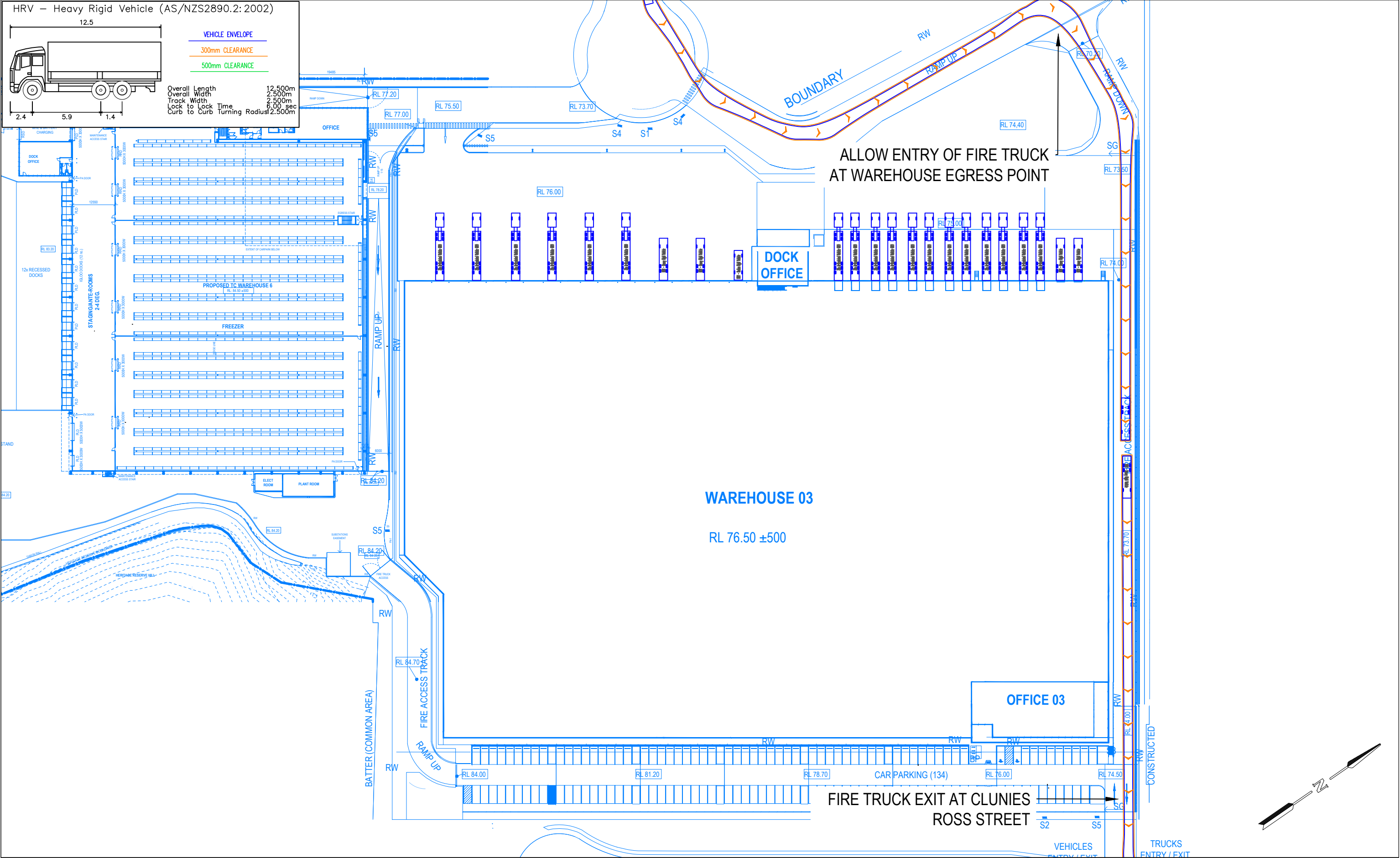
2.4 5.9 1.4

VEHICLE ENVELOPE

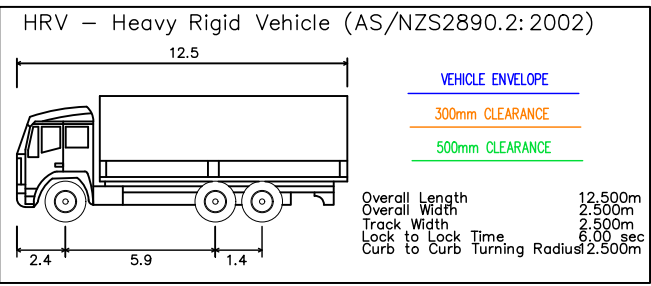
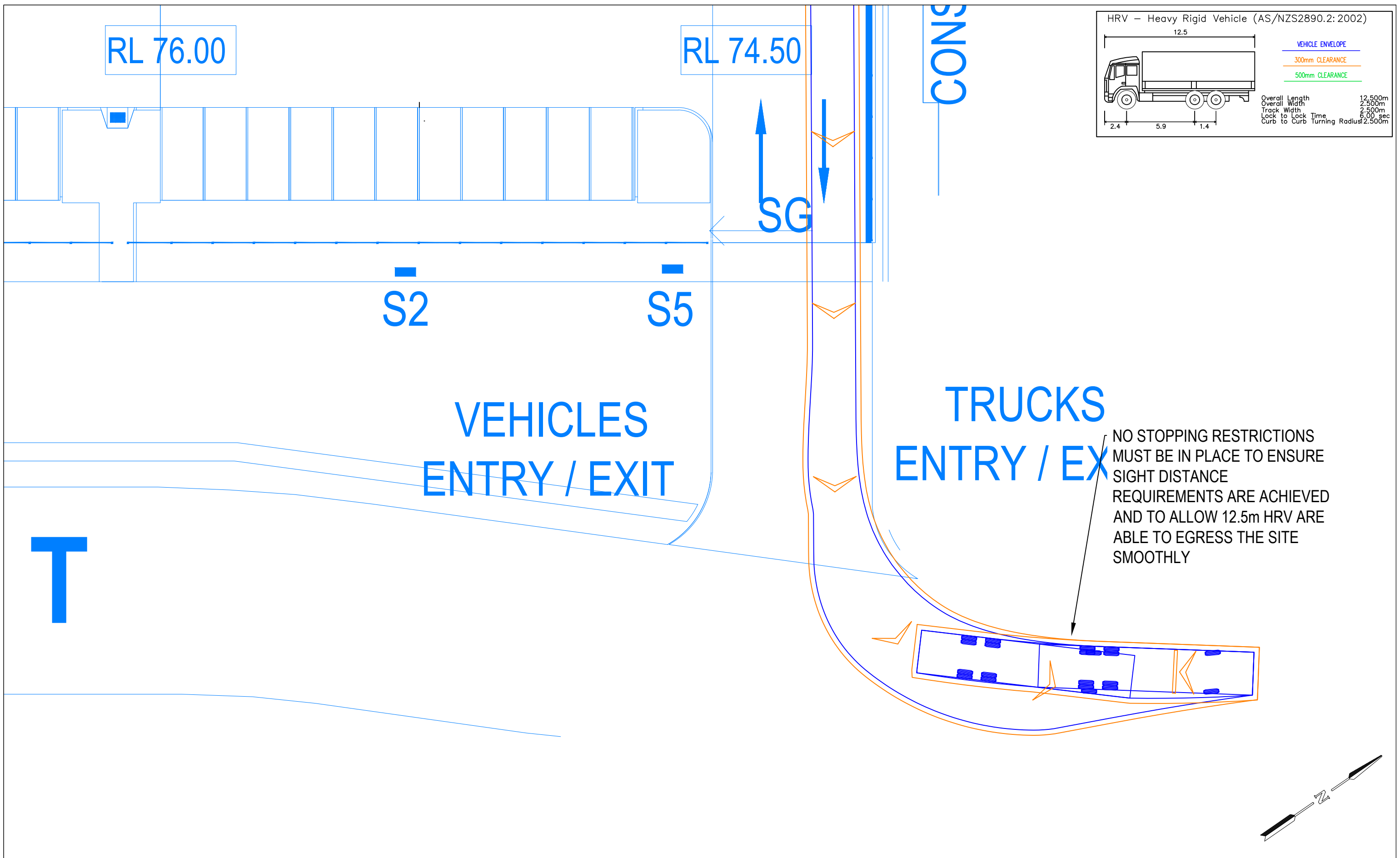
300mm CLEARANCE



500mm CLEARANCE

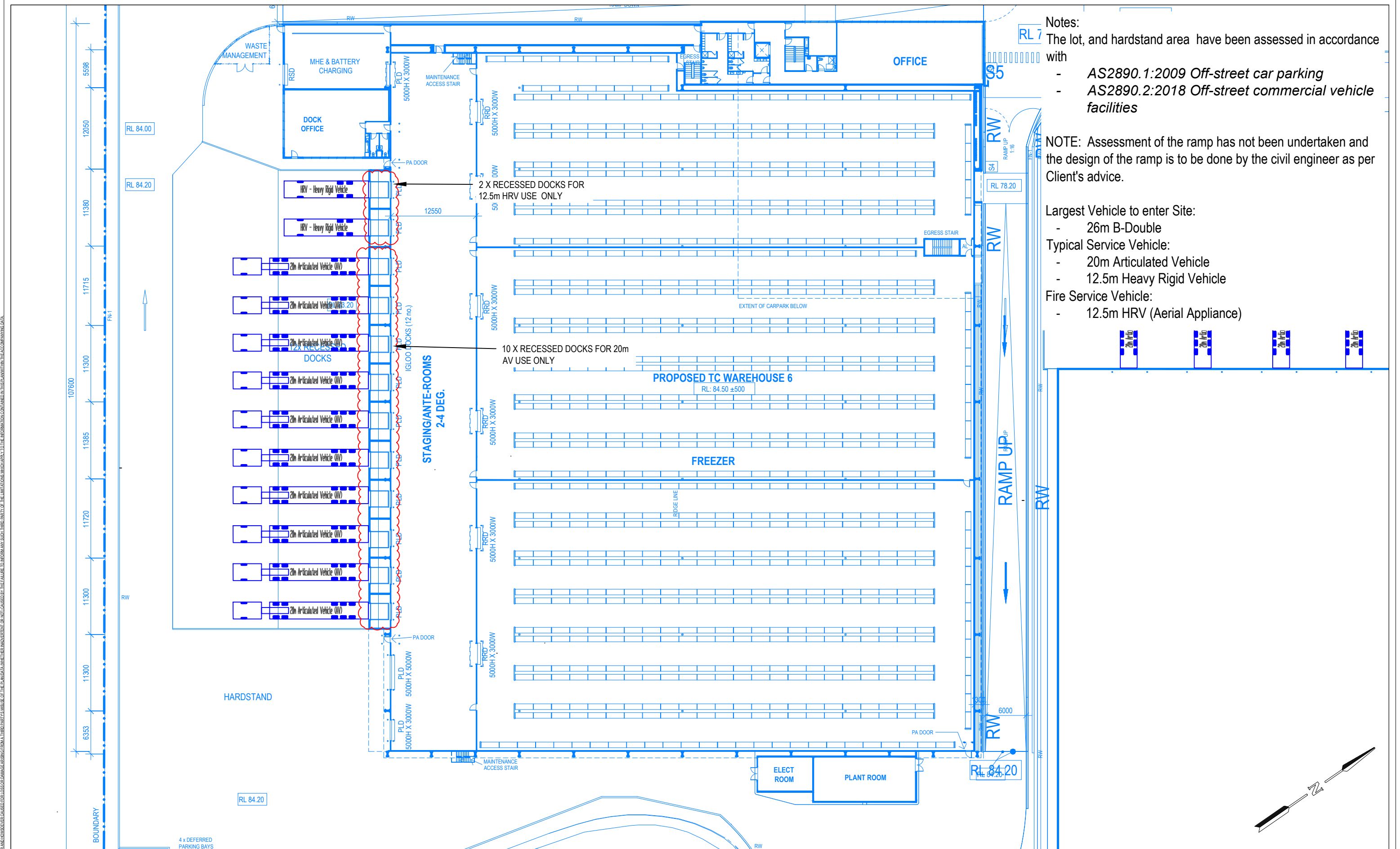
Overall Length 12,500m  
Overall Width 2,500m  
Track Width 2,500m  
Lock to Lock Time 6.00 sec  
Curb to Curb Turning Radius 12,500m



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	<div>APPROVED BY</div> <div>X.XXXX</div>	<div>DATE</div> <div>26.07.2022</div>	<div>PROJECT</div> <div>P1960</div>	<div>FIRE TRUCK ACCESS ROUTE OPTION 2 - OVERVIEW</div>		
	<div>SCALE</div> <div>1:200</div>	<div></div>	<div>FOUNDATION PLACE AND CLUNIES ROSS STREET</div>	<div>FILE NAME</div> <div>AG1960-01-V03.dwg</div>	<div>SHEET</div> <div>AG12</div>	



Notes:  
The lot, and hardstand area have been assessed in accordance with

- AS2890.1:2009 Off-street car parking
- AS2890.2:2018 Off-street commercial vehicle facilities

NOTE: Assessment of the ramp has not been undertaken and the design of the ramp is to be done by the civil engineer as per Client's advice.

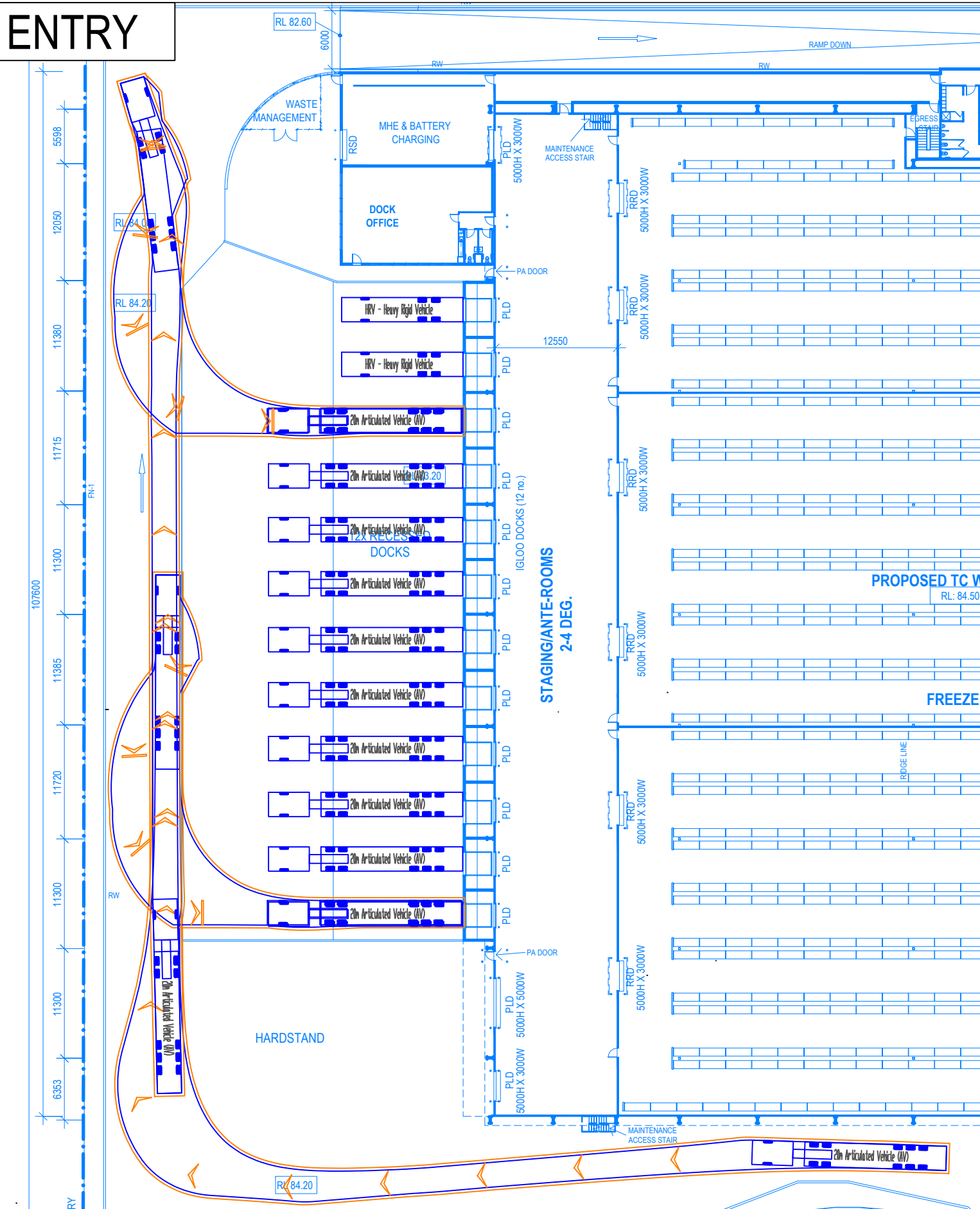
- Largest Vehicle to enter Site:
- 26m B-Double
- Typical Service Vehicle:
- 20m Articulated Vehicle
  - 12.5m Heavy Rigid Vehicle
- Fire Service Vehicle:
- 12.5m HRV (Aerial Appliance)

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	APPROVED BY X.XXXX	DATE 26.07.2022	PROJECT P1960	DESIGN REVIEW		
	SCALE Custom	NTS	FOUNDATION PLACE AND CLUNIES ROSS STREET	OVERVIEW - PROPOSED WAREHOUSE 6		
				FILE NAME AG1960-01-V03.dwg	SHEET AG13	

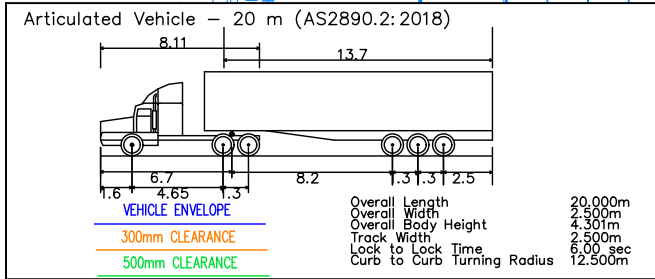
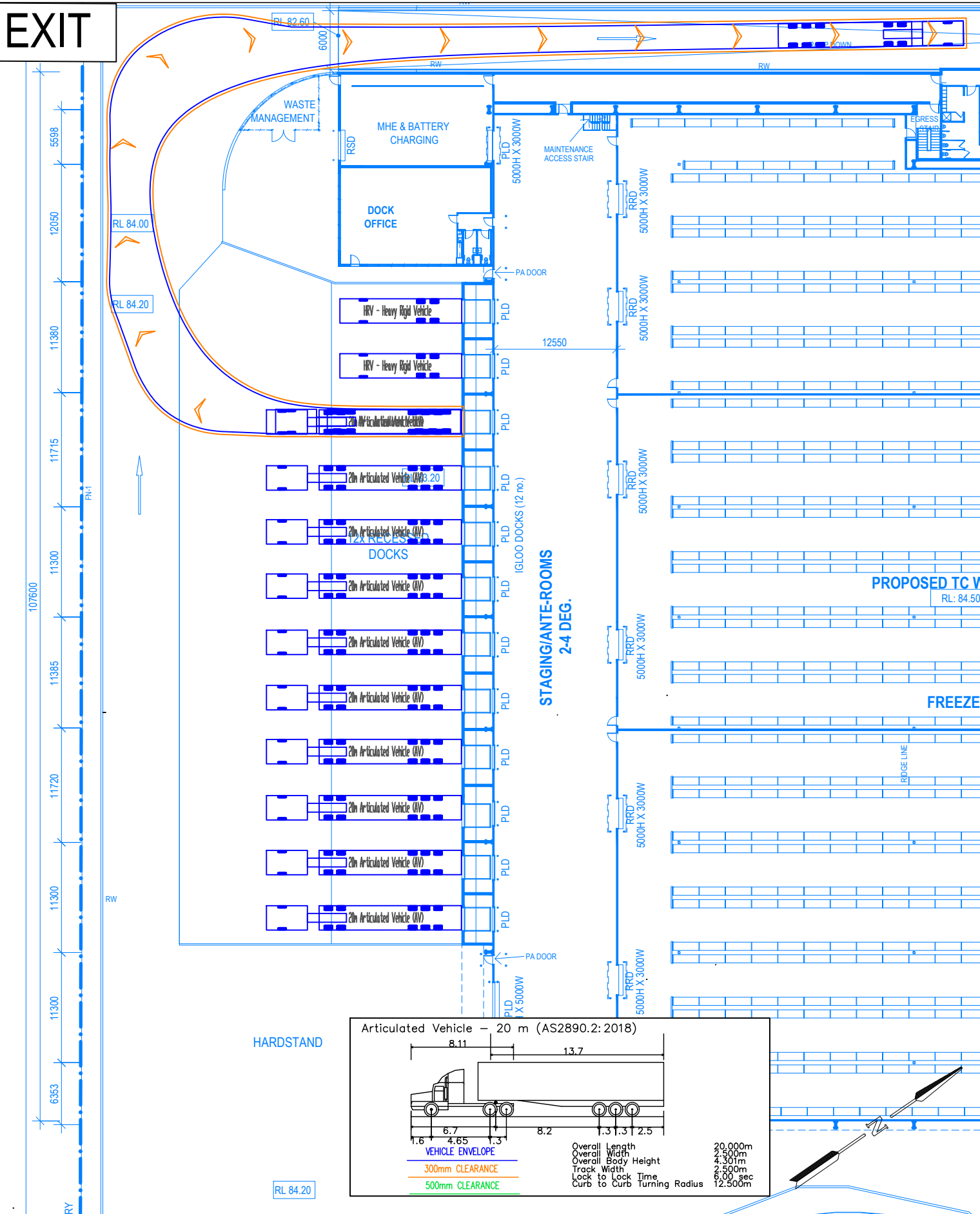




ENTRY



EXIT



GENERAL NOTES

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Base Plan prepared by Aliro Group, received 22 July 2022 (WH3) and 18 July 2022 (WH6).  
Swept path assessments completed at 10 km/h and 300mm clearance.

DESIGNED	PAPER SIZE	CLIENT
Mario Dizon	A3	ALIRO GROUP
APPROVED BY	DATE	PROJECT
X.XXXX	26.07.2022	P1960
SCALE		FOUNDATION PLACE AND CLUNIES ROSS STREET
1:500	0 5 10	

DOCUMENT INFORMATION

DESIGN REVIEW
20m ARTICULATED VEHICLE SWEEP PATH
FILE NAME
AG1960-01-V03.dwg

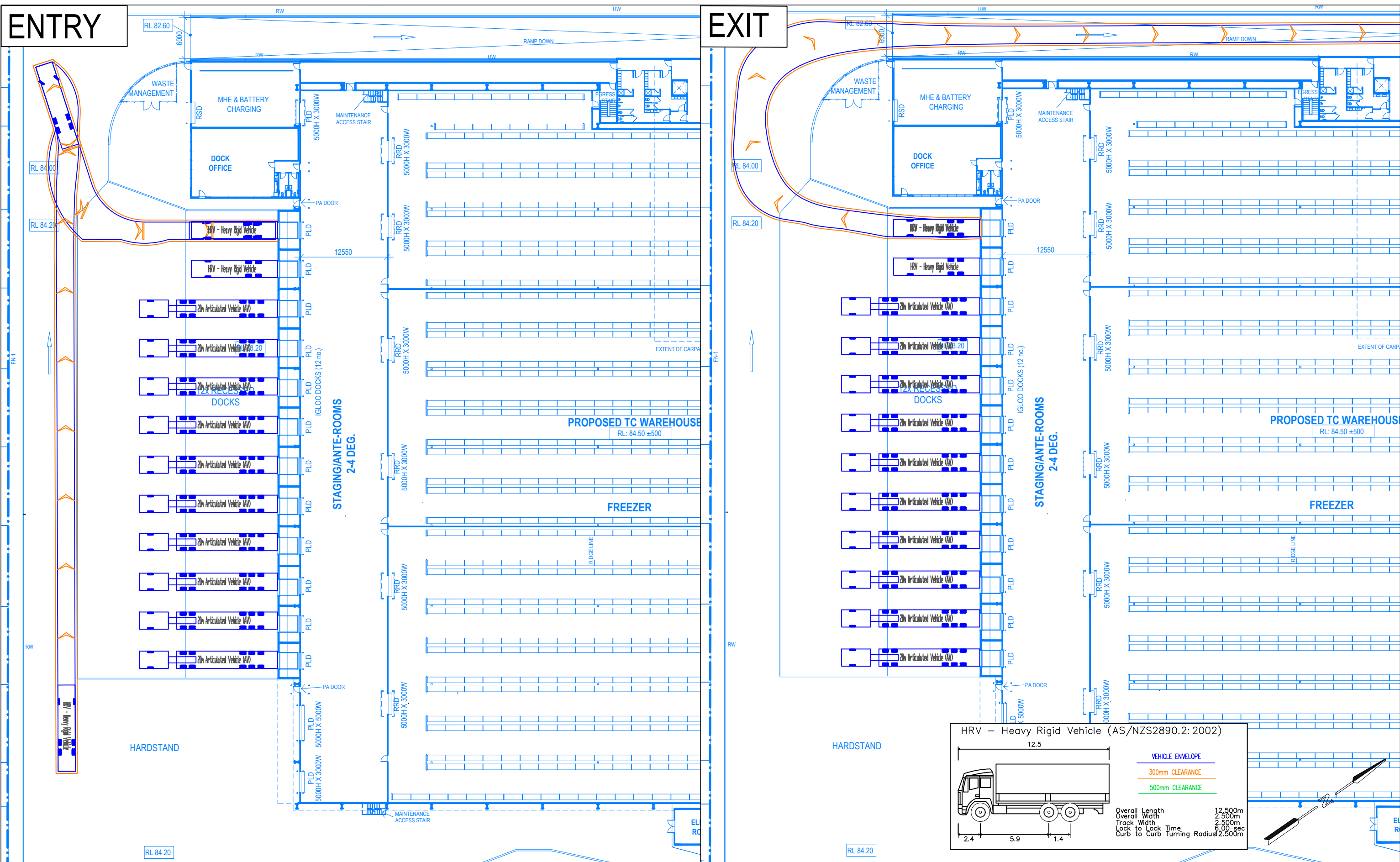
**asongroup**

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# ENTRY

EXIT



## GENERAL NOTES

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Swept path assessments completed at 10 km/h and 300mm clearance.

DESIGNED Mario Dizon	PAPER SIZE A3	CLIENT ALIRO GROUP
APPROVED BY X.XXXX	DATE 26.07.2022	PROJECT P1960
SCALE 1:500		FOUNDATION PLACE AND CLUNIES ROSS STREET

## DOCUMENT INFORMATION

DESIGN REVIEW

12.5m HEAVY RIGID VEHICLE SWEEP PATH

FILE NAME  
AG1960-01-V03.dwg

SHEET  
AG16

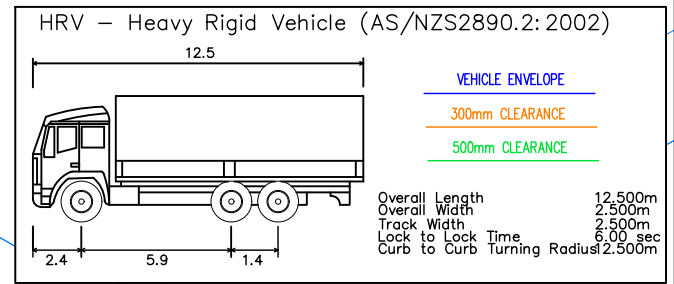
**asongroup**

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HRV – Heavy Rigid Vehicle (AS/NZS2890.2:2002)

The diagram illustrates the dimensions and vehicle envelope for a Heavy Rigid Vehicle (HRV) according to AS/NZS2890.2:2002. The vehicle is shown in profile, consisting of a cab and a long rectangular trailer. The overall length is 12.5m. The trailer has a width of 300mm and a height of 500mm. The cab has a width of 2.4m. The wheelbase is 5.9m, and the distance from the front of the cab to the first axle is 1.4m. The vehicle envelope is shown as a blue rectangle with a width of 300mm and a height of 500mm. The overall length is 12.5m. The overall width is 2.500m, the overall height is 2.500m, the track width is 2.500m, the lock to lock time is 6.00 sec, and the curb to curb turning radius is 2.500m.

Overall Length	12.500m
Overall Width	2.500m
Track Width	2.500m
Lock to Lock Time	6.00 sec
Curb to Curb Turning Radius	2.500m

RL 76.50  $\pm$ 500

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Base Plan prepared by Aliro Group, received 22 July 2022 (WH3) and 18 July 2022 (WH6).  
Swept path assessments completed at 10 km/h and 300mm clearance.

DESIGNED Mario Dizon	PAPER SIZE A3	CLIENT ALIRO GROUP
APPROVED BY X.XXXX	DATE 26.07.2022	PROJECT  P1960  FOUNDATION PLACE AND CLUNIES ROSS STREET
SCALE Custom	NTS	

## DESIGN REVIEW

FIRE TRUCK ACCESS ROUTE TO WH6

FILE NAME
AG1960-01-V03.dwg

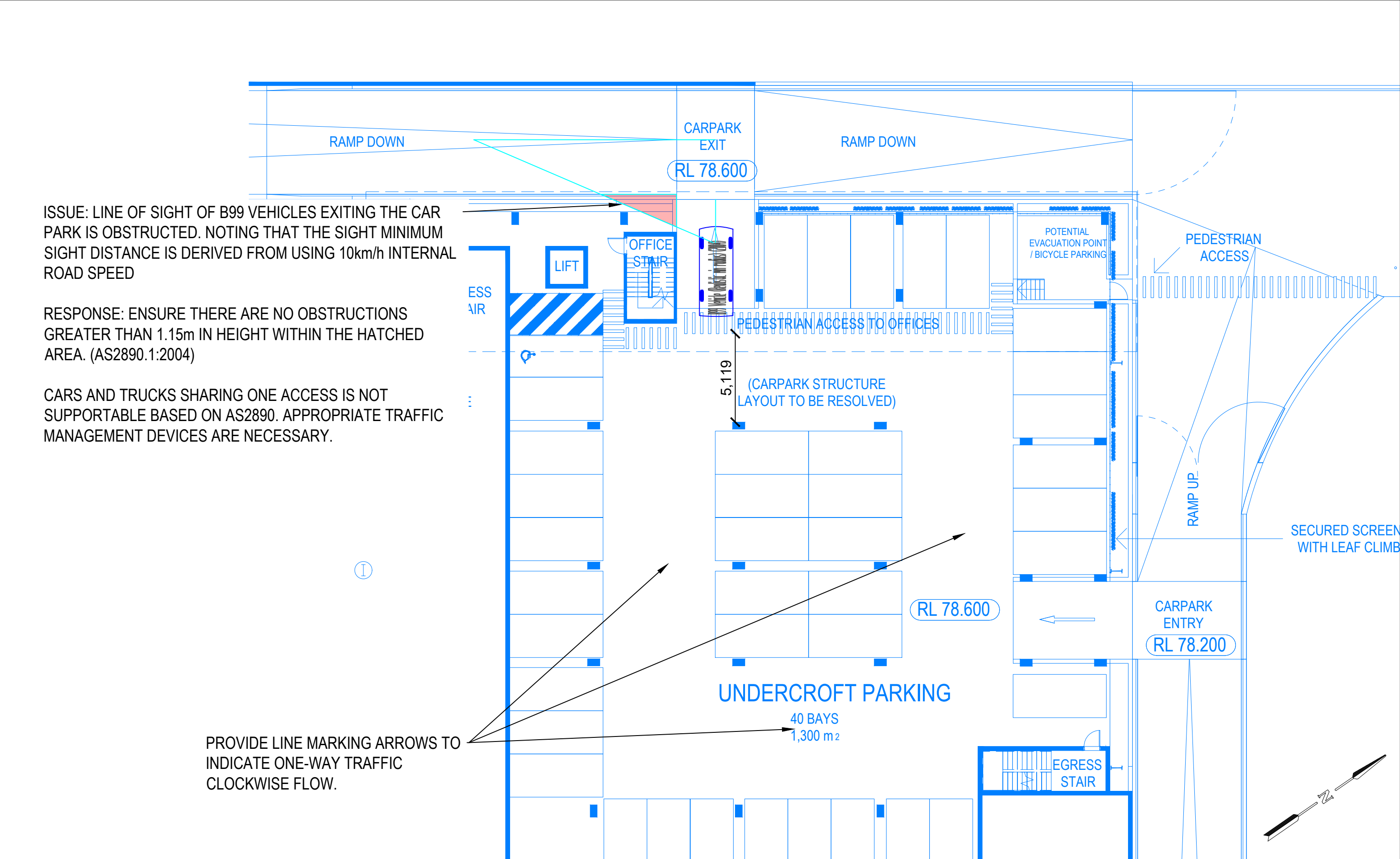
SHEET
AG17


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	<div>APPROVED BY</div> <div>X.XXXX</div>	<div>DATE</div> <div>26.07.2022</div>	<div>PROJECT</div> <div>P1960</div>	<div>Warehouse 6 - Undercroft parking</div>		
	<div>SCALE</div> <div>1:200</div>	<div><div>0</div><div>2</div><div>4</div></div>	<div>FOUNDATION PLACE AND CLUNIES ROSS STREET</div>	<div>FILE NAME</div> <div>AG1960-01-V03.dwg</div>	<div>SHEET</div> <div>AG19</div>	