

Mr Adam Koutsamanis
Sydney Metro
Level 43, 680 George Street
PO BOX K659
SYDNEY NSW 2000

Dear Mr Koutsamanis,

Thank you for your email dated 19 July 2019 and attached documentation requesting to satisfy Condition A23 (Design Guidelines) to the Concept Approval the Pitt Street North Over Station Development (SSD 8875).

The Department has reviewed the documentation provided and considers it address the requirements of Condition A23. Accordingly, as the Secretary's nominee, I am satisfied that the requirements of Condition A23 have been met.

We note the Sydney Metro Design Review Panel (SMDRP) reviewed the amended Design Guidelines on 16 July 2019. The SMDRP considered the revisions to Design Guidelines raised potential for ambiguity in implementation and delivery. It is noted the Department is responsible for future assessment against the design guidelines. Notwithstanding this, the Department seeks clarification on the SMDRP's comments with respect to Condition A24, which relates to the design approach on the integration of the over station development and the approved station (under CSSI 7400).

If you have any more questions, please contact Mr Marcus Jennejohn, Senior Planning Officer, Key Sites Assessments, at the Department on (02) 8289 6798.

Yours sincerely,



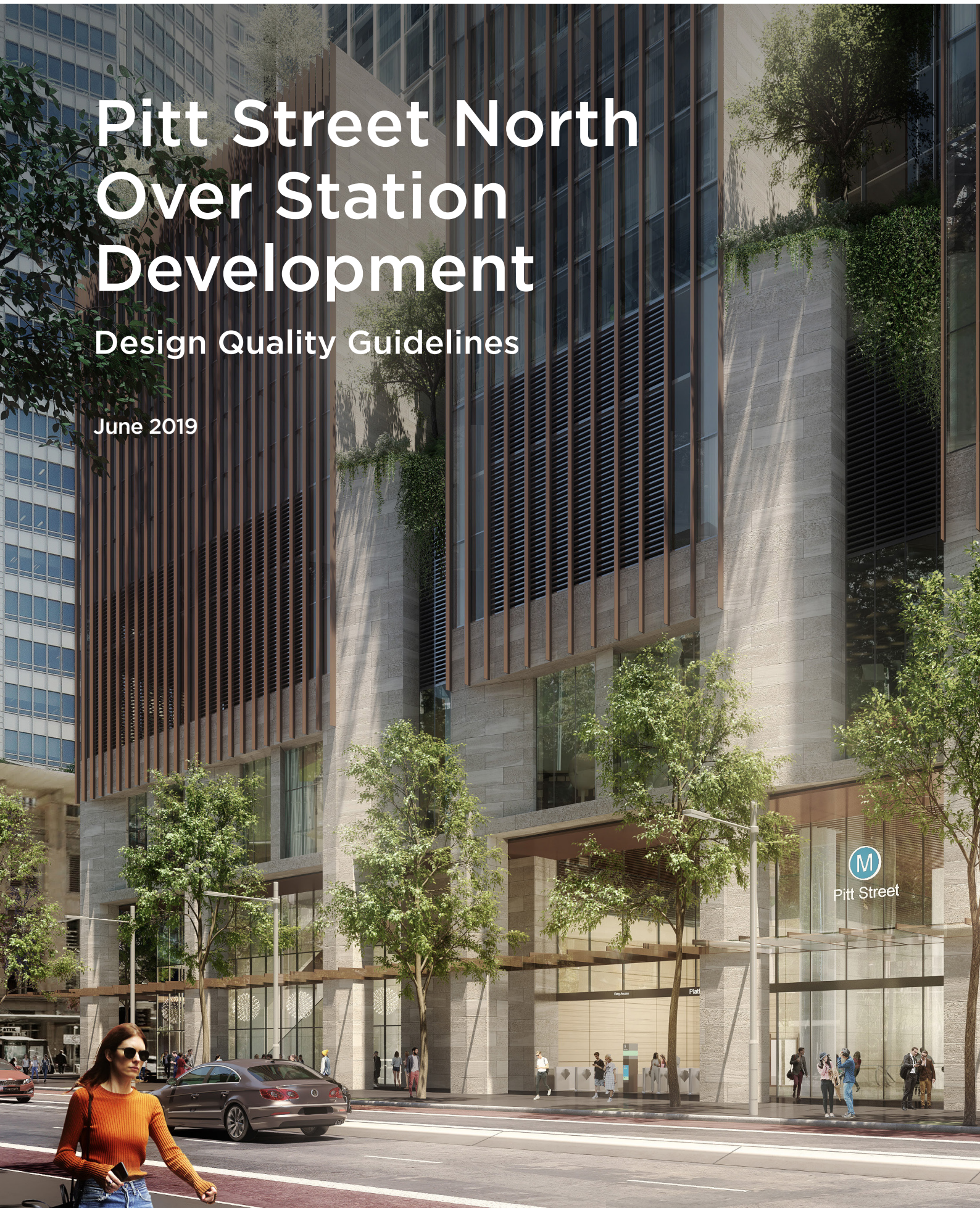
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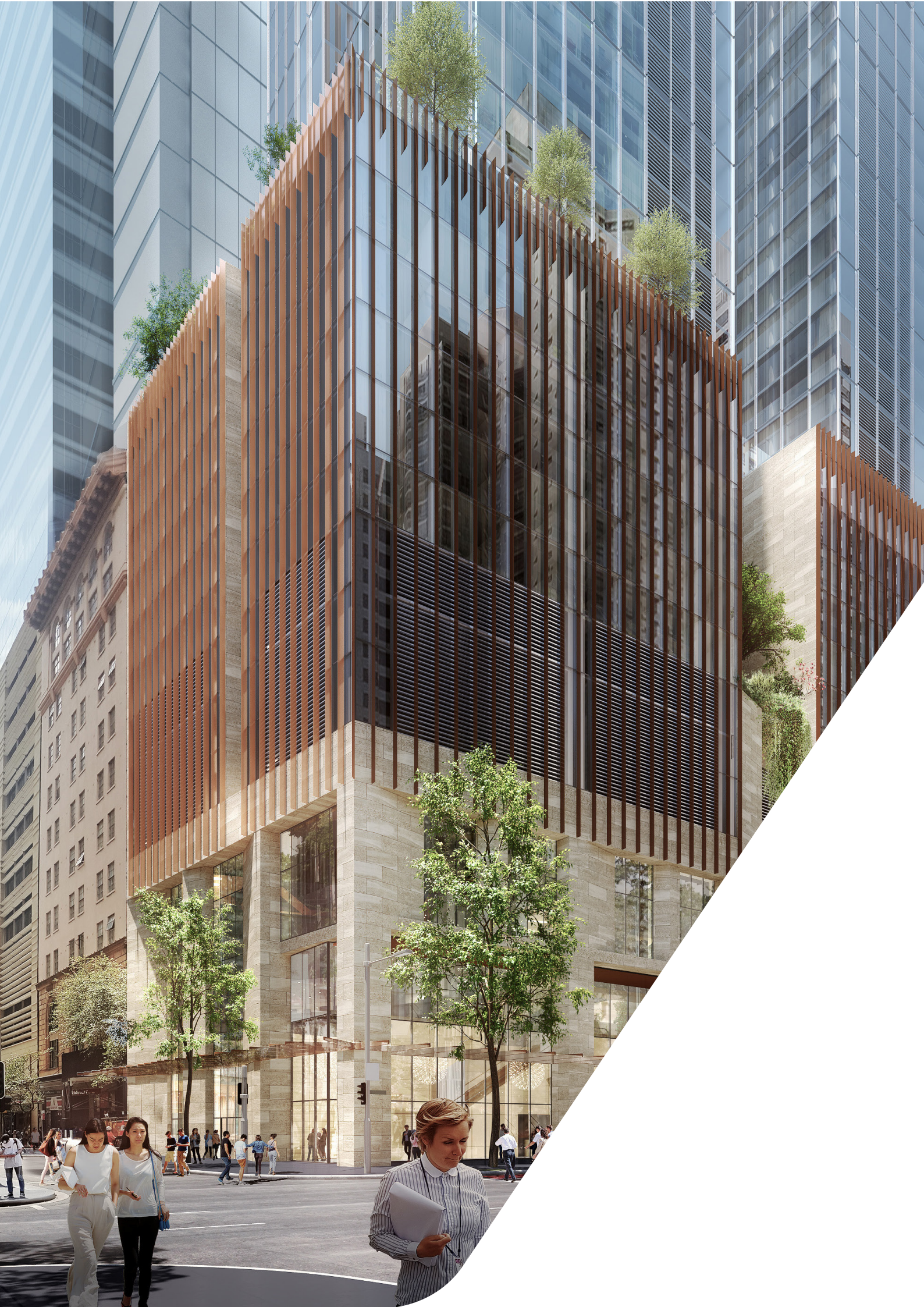
David McNamara
Director
Key Sites Assessments

Pitt Street North Over Station Development

Design Quality Guidelines

June 2019





Purpose of the Document

The purpose of this document is to guide the design of the Sydney Metro Pitt Street North over station development (OSD) and provide a reference document for the assessment of design outcomes.

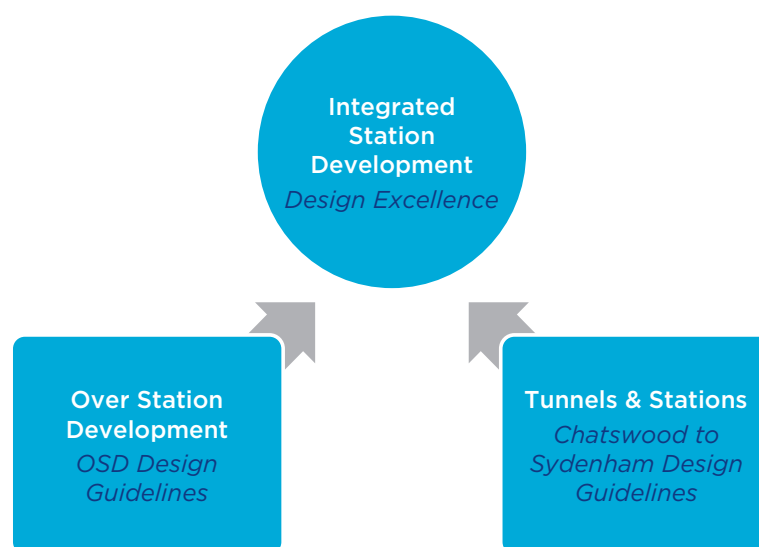
These design guidelines provide a set of overarching principles and outcomes capable of interpretation irrespective of final land use. They have been formulated to ensure delivery of design excellence across all responses to the site.

Design parameters are included for built form, heritage, integration with the public domain and Sydney Metro station, movement and connectivity and legacy outcomes of the development. These have been prepared with reference to:

1. Concept State Significant Development Application Design Report for Pitt Street North Over-Station development, March 2018
2. 'Sydney Metro City & Southwest: Chatswood to Sydenham Design Guidelines' (Sydney Metro CSW Design Guidelines), June 2017
3. 'Sydney Metro City & South West Pitt Street North over-station development: Heritage Impact Statement'.

Separate approval pathways for the station and the over station development have required the preparation of separate design guidelines for each component. These guidelines build on those accompanying the 'Sydney Metro City & Southwest: Chatswood to Sydenham Design Guidelines' and as such should be read in conjunction with them.

A key focus of these guidelines is to set clear design objectives for the OSD elements that integrate with the station, and to ensure consistency in the design approach for both elements. It is intended that the guidelines also inform and complement the future Station Design and Precinct Plan and Interchange Access Plan and are to be considered as part of the Sydney Metro Design Excellence Strategy for the site.



Vision

The Pitt Street North OSD will redefine the local urban fabric, contributing to the renewal of an evolving CBD precinct. The design will deliver a high amenity public domain, reflect the local context and make a significant contribution to an important heritage locality.

The development will provide an exceptional and distinctive built form to mark both the Metro station location and the site's central position on Park Street, one of Sydney's significant east-west avenues connecting Central Sydney to the East. The OSD will seamlessly integrate with all building elements across the site, including the public domain, station and entrances and transfer elements.

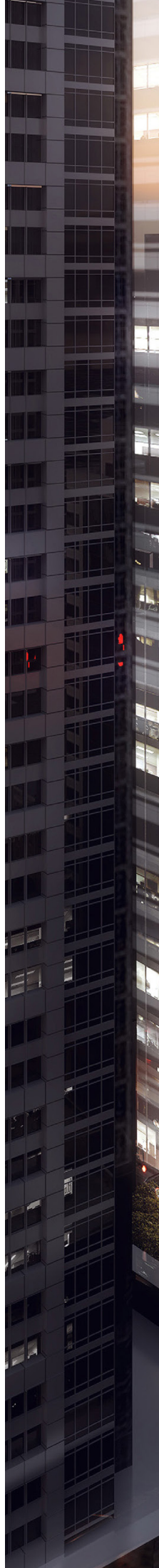




Figure 1 – Artists Impression of Indicative Design

Site Overview



Figure 2 - Site Map

The proposed Pitt Street North OSD site is located on the corner of Pitt, Park & Castlereagh Streets, as shown in Figure 2, and is one of two entries to the Pitt Street Metro Station. The site has a site area of 3,150sqm and features a total street frontage of approximately 157m.

Site Context

Significant features of the site and its context include:

1. A central location on Park Street, one of Sydney's significant east-west avenues connecting Central Sydney to the East, and forming part of the significant visual and transport link between Hyde Park the Town Hall Civic Precinct.
2. A local character that is defined by a mix of civic, residential, retail, commercial & hotel uses, with a varied built form tailored to the constraints of the location. This includes reduced building heights from those found in the north of Central Sydney.
3. Building heights largely determined by age of building, heritage considerations within the precinct and requirements for solar access to Hyde Park.
4. A number of significant heritage items to the north, south and east of the site, with close proximity to Sydney Town Hall, the QVB, and Hyde Park.
5. A streetscape characterised by its width, including footpaths, and the short length of Park Street with views to both Hyde Park to the east and Town Hall and QVB to the west.
6. A strong ground level activation due to significant retail and commercial activity.
7. Significant pedestrian movement between existing public transport infrastructure, surrounding land uses and Hyde Park/public open space
8. Significant future pedestrian desire lines between the site and the proposed Town Hall Square. Varied street wall and podium heights between the northern and southern sides of Park Street.
9. A variety of opportunities to enhance the public domain & the broader urban context through an improved and activated pedestrian environment, contextual and human scale design, integrated landscaping and quality finishes.

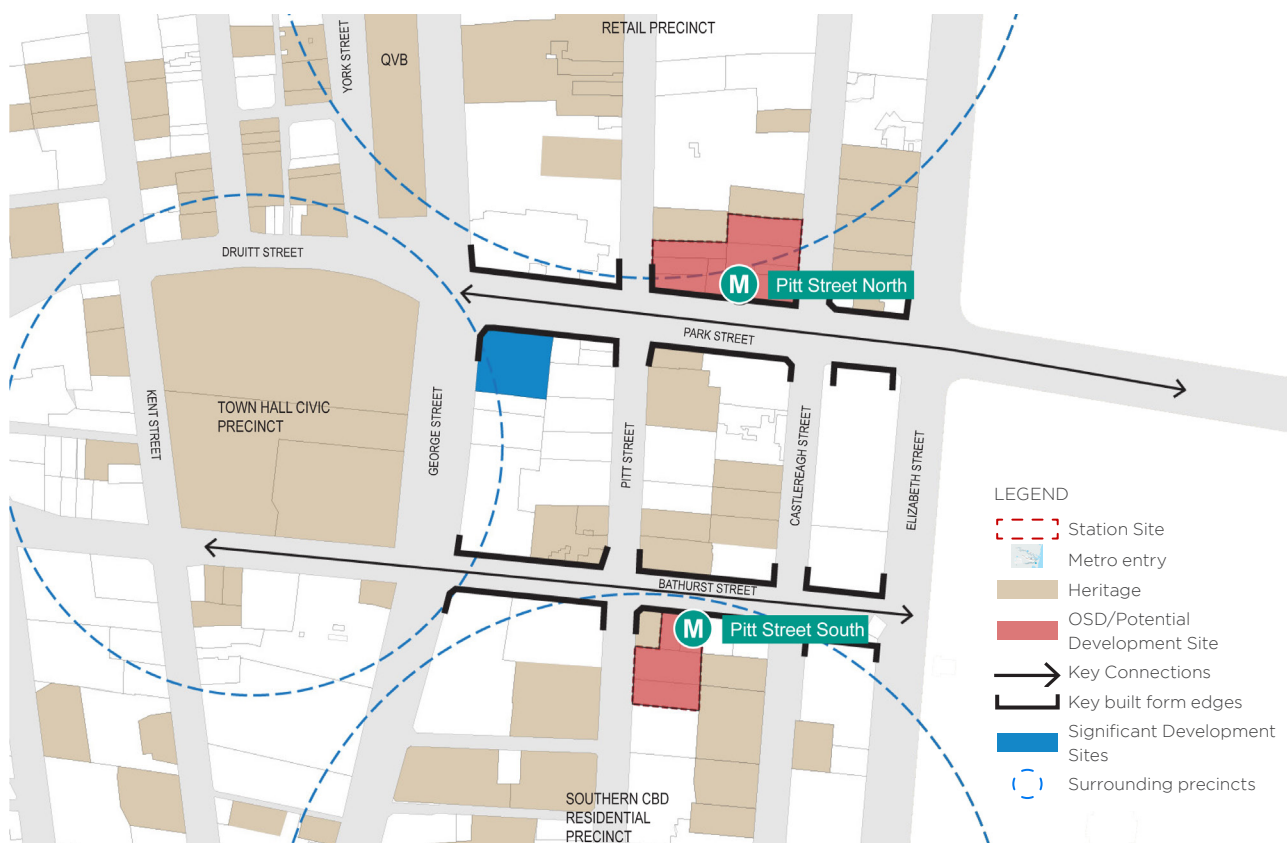


Figure 3 - Site Context

Sydney Metro Design Objectives

The design outcomes for the Pitt Street North OSD are underpinned by the design objectives for all Sydney Metro projects.

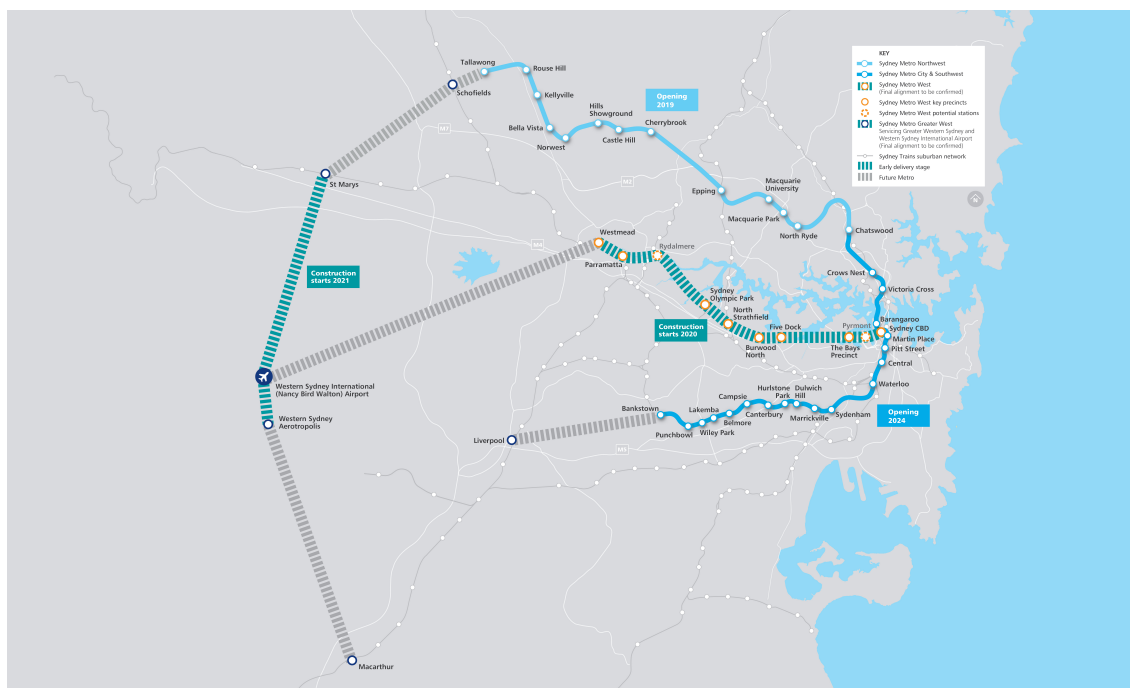


Figure 4 – Sydney Metro project

Designs for the station, station precinct and the over station development must deliver on the following:

Ensuring an easy customer experience.

Sydney Metro places the customer first. Stations are welcoming and intuitive with simple, uncluttered spaces that ensure a comfortable, enjoyable and safe experience for a diverse range of customers.

Being part of a fully integrated transport system.

Sydney Metro is a transit-oriented project that prioritises clear and legible connections with other public and active transport modes within the wider metropolitan travel network that intersect with this new spine.

Being a catalyst for positive change.

Sydney Metro is a landmark opportunity to regenerate and invigorate the city with new stations and associated development that engage with their precincts, raise the urban quality and enhance the overall experience of the city.

Being responsive to distinct contexts and communities.

Sydney Metro's identity is stronger for the unique conditions of centres and communities through which it passes. This local character is to be embraced through internationally benchmarked high quality station architecture and public domain that is well integrated with the valuable inherited urban fabric of existing places.

Delivering an enduring and sustainable legacy for Sydney.

Sydney Metro is a positive legacy for future generations. A high standard of design across the corridor, stations and station precincts, that sets a new benchmark, is vital to ensuring the longevity of the Metro system, its enduring contribution to civic life and an ability to adapt to a changing city over time.

Sydney Metro City And South West Chatswood To Sydenham Design Guidelines

The Chatswood to Sydenham (C2S) design guidelines form part of the environmental impact statement (EIS) for the C2S Critical State Significant Infrastructure approval and establish design standards to guide the interface outcomes between stations and their surrounding locality.

The C2S design guidelines provide the following key design drivers and urban design strategies for Pitt Street station. As a component of the approved C2S EIS, these drivers and strategies must inform the design response for the Pitt Street North site.

Key design drivers

1. Provide space for customers in a busy pedestrian environment by extending the public domain into the station entries.
2. Integrate with the Sydney City Centre Access Strategy and other CBD planning strategies.
3. Anticipate connections to a future Town Hall Square and other nearby developments.
4. Extend the transport focus along Park Street, near Pitt Street.

Urban design strategies

Linking Hyde Park to the Civic Precinct

As increasingly important pedestrian streets, Park Street and Bathurst Street will require public domain improvements.

A Street-grid of Interchange

The entrances to the new Metro station address Park and Bathurst Streets. These two streets will be key to interchange movements, especially to the bus and light rail services that run along the north-south streets of the city.

Frontages to east-west streets

The primary address of both Metro entries will be to the east-west connectors, reinforcing the importance of these streets and facilitating interchange between transport modes.

Extending the materiality and character of the surrounding public domain into the station entries creates the opportunity for a seamless experience.

Optimising development over stations

The entrances to the station provide an opportunity to facilitate renewal. Future development above these spaces should reflect the context of the locality and positively contribute to the built form and character of the area.

DESIGN GUIDELINES

Built form

Respond to the existing urban fabric and built form context of this mid-town location through a finer, textured-grain and human scale podium design through articulation of the structure, levels and details and a simple, refined over station design. This will reflect both the significant heritage architecture of the locality and the evolving commercial nature of this area of the city. Ensure the design responds appropriately to final land use choice and directly integrates transitions between the station, podium and over station elements of the development.





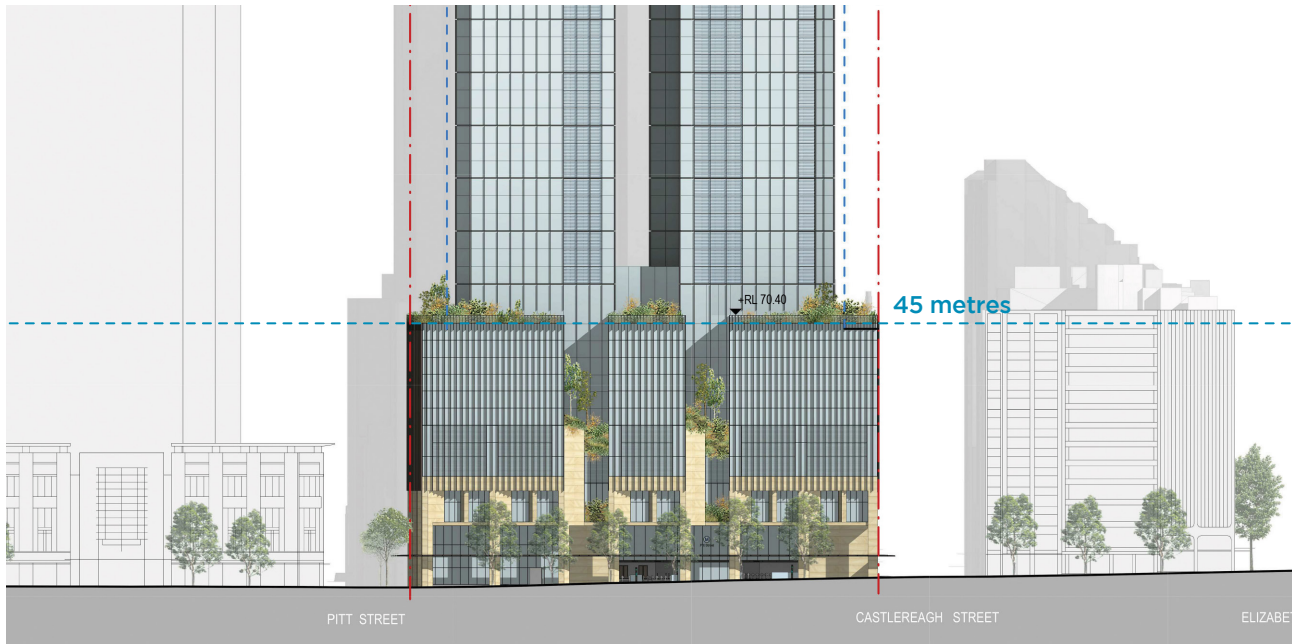
Figure 5 - Artists impression of Pitt Street North Station entrance

Podium and Street Wall

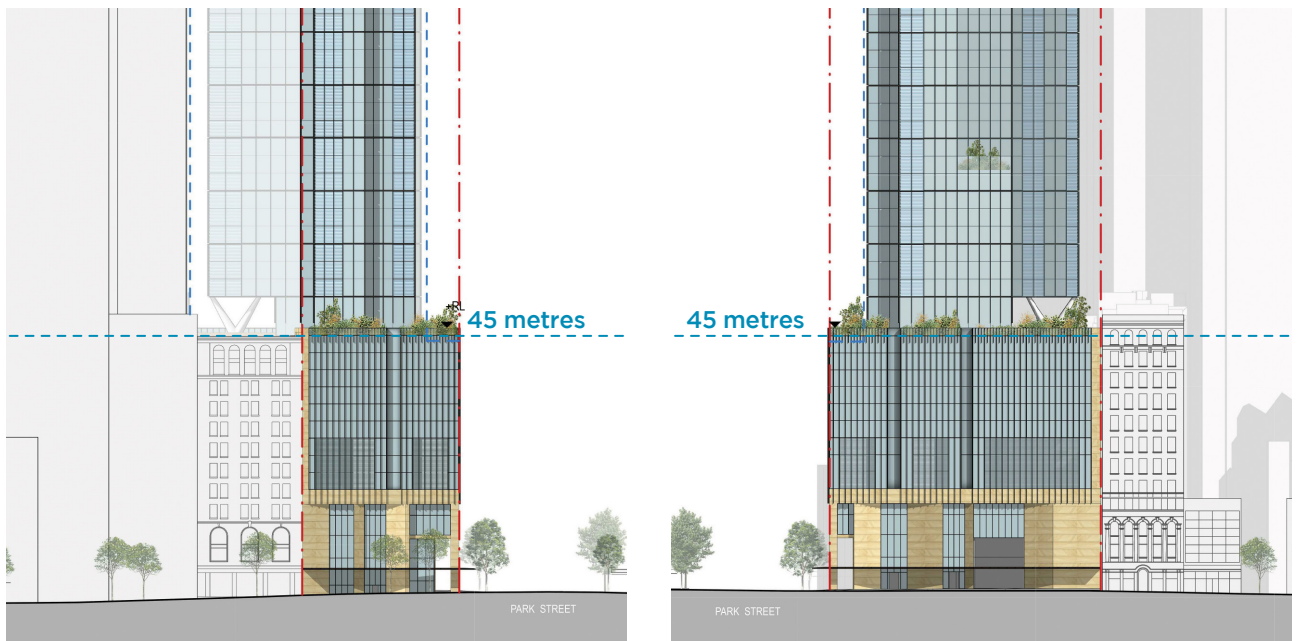
Podium form & articulation should aim to re-establish architectural order within the immediate precinct, demonstrating heritage & contextual sensitivity, with scale and massing that relates well at the human scale. Design excellence, articulation and finish are delivered irrespective of end use and capture opportunities for varied responses accordingly.

This is to be achieved through:

1. Recognising the surrounding streetscape scale and providing an enhanced interface with adjacent heritage buildings, with direct reference to the height and articulation of these buildings, including:
 - a) Treatment of the podium/street wall to incorporate a high proportion of masonry compared to window glazing, strong visual depth, a high degree of architectural modelling, articulation and detail, and high-quality materials that reflect the building composition of heritage items in the vicinity. Window glazing to be deeply recessed.
 - b) The Park Street frontage of the podium responding to the scale of Sydney Town Hall, ensuring that the out of scale podium of the Galleries Victoria is not used as a direct scale reference. (See Figure 6: Podium and Street Wall - Indicative Elevation - Park Street)
 - c) The Pitt Street frontage of the podium responding to major horizontal and vertical elements of the National Building and the Criterion Hotel, including the second-floor and upper cornices of the National Building. (See Figure 6: Podium and Street Wall - Indicative Elevation - Pitt Street)
 - d) The Castlereagh Street frontage of the podium responding to major horizontal and vertical elements of the Masonic Club, including the second and third floor cornices of the former Masonic Club as well as upper cornices. (See Figure 6: Podium and Street Wall - Indicative Elevation - Castlereagh Street)
 - e) The form of the podium interpreting the subdivision pattern established during the late-nineteenth and early twentieth century through the modulation and articulation of the street frontages, noting the particular significance of the National Building and the Masonic Club.
 - f) A 45m street wall podium height, referencing Ashington Place (National Building) (284A-250 Pitt Street) and NSW Masonic Club (169-173 Castlereagh Street). (See Figure 6: Street Wall Height and Streetscape References)
 - g) Dividing the podium into distinct forms along Park Street, with further articulation through the introduction of vertical reliefs along Pitt and Castlereagh Street. (See Figure 6: Podium and Street Wall - Indicative Elevation)
 - h) Om setbacks to the rear boundary in response to the adjoining sites heritage significance and inability to develop any higher.
2. Alignment of over station development with established building alignments at lower levels, with lobbies provided from secondary street frontages.
3. Provision of landscaping throughout the podium design, laying spaces of relief & activation and referencing landscaping carried through from Hyde Park.



Park Street Elevation



Pitt Street Elevation

Castlereagh Street Elevation

Figure 6 – Podium and Street Wall - Indicative Elevations

Source: Concept SSDA Design Report – Pitt Street North OSD, Architectus

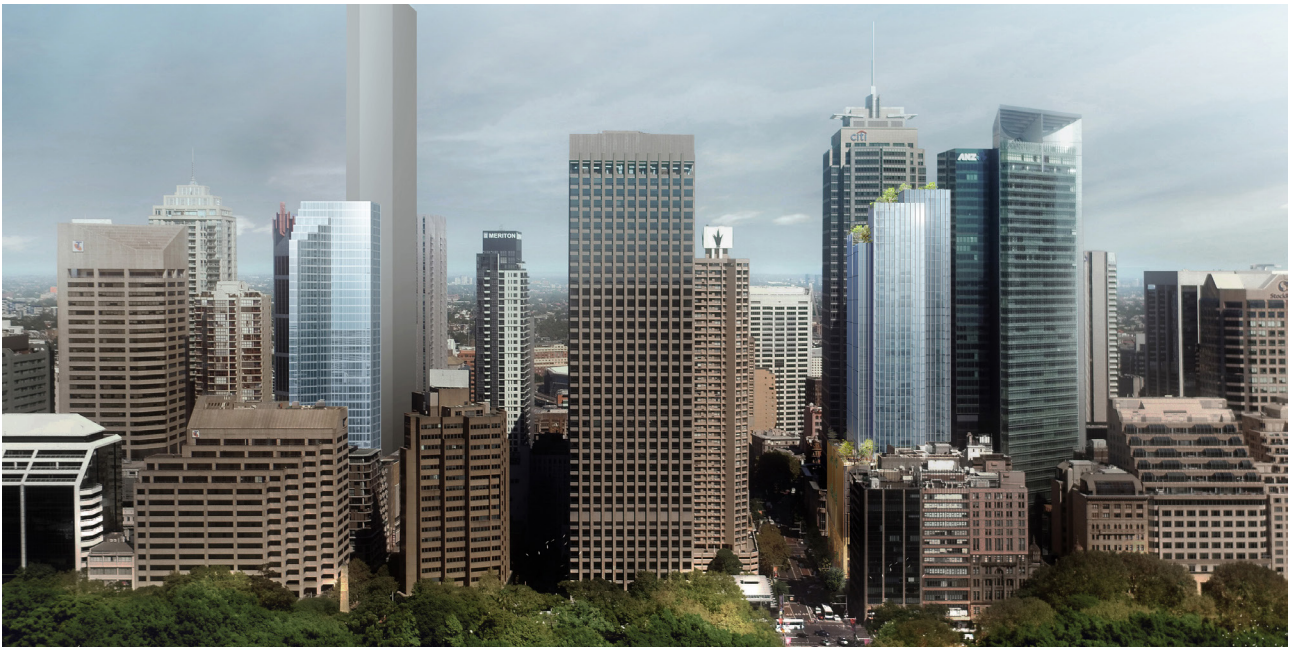


Figure 7 – Artists impression of Pitt Street North, alongside Pitt Street South, within the Sydney CBD skyline.

Built Form above the Podium

Provide an exceptional and distinctive built form above the podium that responds to the evolving height, scale and character of the area. The built form will respond to the sites proximity to ANZ/ Liberty Place & Citigroup and impacts on solar access to Hyde Park, the proposed Town Hall Square and the wider public domain. Design excellence, articulation and finish are delivered irrespective of end use and capture opportunities for varied responses accordingly.

This is to be achieved through:

1. Recognition of the contextual relationship with the surrounding heritage listed items.
2. Compliance with City of Sydney LEP 2012 street setbacks of 8m to Pitt, Castlereagh and Park Street, with potential to provide an averaged setback along Park Street to align with the station structure.
3. Appropriate setbacks to protect light access to adjoining light wells of Ashington Place (National Building) (284A-250 Pitt Street) and NSW Masonic Club (169-173 Castlereagh Street) and use of reflective or light coloured materials to encourage light penetration.
4. Modulation of the design to minimise the overall scale of the development relative to ANZ/Liberty Place & CitiGroup, considering tower crowding as perceived particularly from Hyde Park & Town Hall. (See Figure 8: Indicative Built Form above the Podium)
5. Avoiding the continuation of the diagonal NW plane façade alignment otherwise established by the proposed 201 Elizabeth Street & ANZ/ Liberty Place.
6. Maximise solar access to the public domain, through:
 - a) Design and articulation to ensure no additional overshadowing to Hyde Park on June 21st, between 12pm and 2pm (required by SLEP 2012 Sun Access Plane controls)
 - b) Responding to the reduced shadow cast by the redevelopment of 201 Elizabeth Street on Hyde Park on June 21st, between 12pm and 2pm - Sydney Metro preliminary design work propose an angled offset of the north eastern corner of 4.1m to achieve this outcome.
 - c) Creation of opportunities to increase solar access to the proposed Town Hall Square.
 - d) The design and articulation of roof forms to minimise additional shadow impacts to Hyde Park between 12 noon and 2pm throughout the year.
7. Use of materials that reflect the function of the over station development, distinguishing them from the surrounding context and providing a simpler design resolution within the city skyline.

8. Provision of landscaping throughout the design, laying spaces of relief and referencing landscaping carried through from Hyde Park.
9. Achievement of SEPP65 & ADG requirements and must:
 - a) Provide appropriate building separation to maintain a reasonable level of residential privacy
 - b) Maximise solar access to residential apartments within the development with consideration to:
 - (i) the number of apartments or development density
 - (ii) limiting the number of single aspect/south facing apartments
 - c) Minimise overshadowing impacts to surrounding residences, including private residences at 27 Park Street (Park Regis).
10. Provide articulation of the tower to present as multiple forms, when viewed from both Town Hall and Hyde Park, with vertical expression along Park Street incorporating continuous elements of relief for the full height of the building above the podium to reduce the mass and scale of the future built form and ensure the built form better responds to the massing and scale of surrounding buildings.
11. Incorporate building articulations, building modulations and facade treatments to provide distinctive visual breaks along the Park Street frontage of the site, respecting the surrounding subdivision and built forms patterns. The distinctive visual breaks shall be proportional to the overall building height and length of the street frontage.

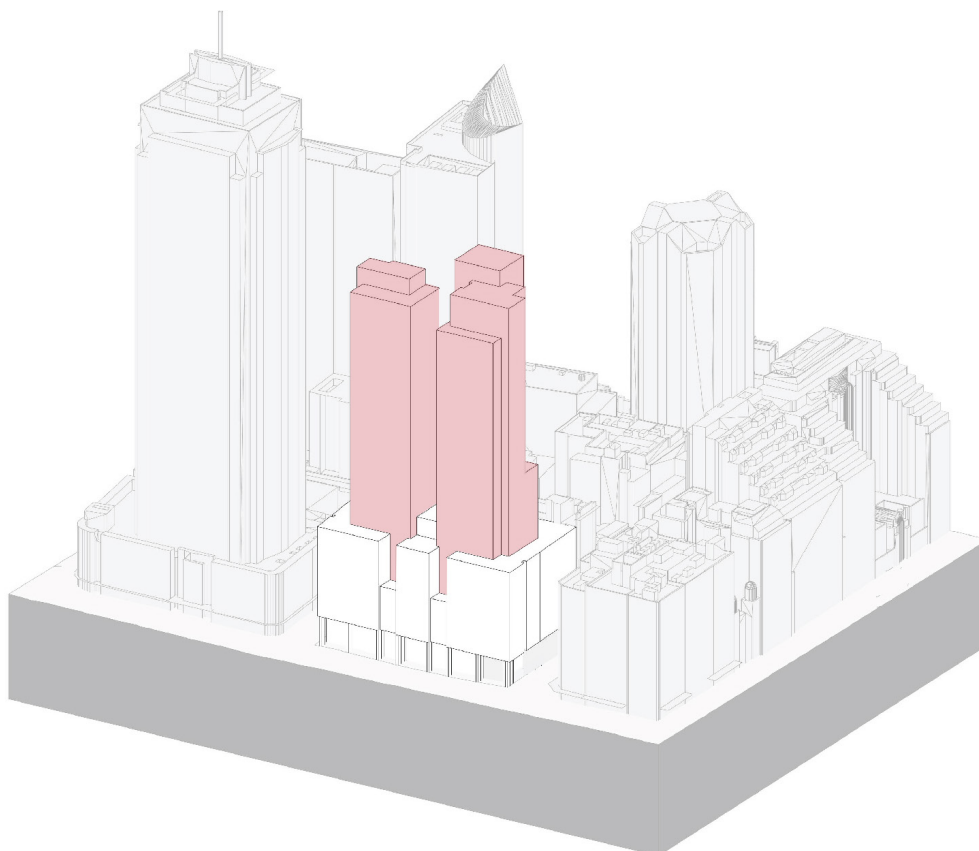


Figure 8 – Indicative Built Form above the Podium

Source: Concept SSDA Design Report – Pitt Street North OSD, Architectus

Public Domain and Place



Figure 9 – Artists impression of public domain

Contribute to a high amenity, well-considered and articulated public domain that addresses the significance of the site and the complexity of high pedestrian activity in a relatively constrained location. Provide a strong relationship between Pitt Street Station North and South and pursue innovative opportunities to maximise activation of the spaces within the site and fronting the street network.

This is to be achieved through:

1. Enhancing the quality of the public domain, including provision of widened footpaths, new street trees, paving upgrades and public art.
2. Providing space for customers in a busy pedestrian environment by recessing station entries to widen the pavement and provision of uncluttered movement corridors, including minimum footpath width requirements from the building line to the back of kerb line of:
 - a) 3.3m on Pitt Street and Castlereagh Street
 - b) 10.5m on Park Street.
3. Reinforcing the importance of Park Street as a primary City avenue and east-west connection by locating the main entry points to Sydney Metro stations on this street.
4. Providing a strong, well demarcated street address to each building through strong form modulation and well activated ground floors.
5. Innovative design solutions to maximise activation within a constrained street frontage, including capturing opportunities along Castlereagh and Pitt Street. Activation opportunities should investigate a range of offerings that attract users to the place and includes a mix of building entrances and retail uses.
6. Promoting a safe & user-friendly environment including weather protection, security measures & wayfinding etc. This should include as a minimum:
 - a) Minimising opportunities for criminal and anti-social behaviour.
 - b) Incorporating awning cover that relates to surrounding buildings to create a continuous weather protection edge to all street frontages.
 - c) Seamless integration of all signage with the architectural character of the scheme and surrounding context, providing an elegant and uncluttered approach and coordinated with Metro and City of Sydney signage. Signage location and placement must integrate with City of Sydney DCP 2005 - Signage and Advertising Structures.
7. Reinforcing the east west connection between Hyde Park and the Town Hall Civic precinct, including maintaining existing views.
8. Considering the future evolution of the broader precinct, including pedestrian and visual connections with the proposed Town Hall Square.

9. Provision of public art, integrated and cohesive with the design of the built form which potentially recognises former uses and is coordinated with nearby public art, including the future 'Cloud Arch' and Metro public art.
10. The design and location of fire stairs, services, plants and other similar building elements must minimise their visual impacts at street level.
11. The design and dimensions of any colonnade and awning along the street frontages of the site must integrate and support capacity for pedestrian access and connection to and from the station entrance and the over station development.

Movement and Connectivity



Figure 10 – Pedestrian activity around Pitt Street North site

Acknowledge the important movement and interchange function of Park Street and integrate the site's role as an entry point into the precinct. Prioritise pedestrian access, permeability and amenity within the development and across the precinct and facilitate legible, safe and convenient interchange opportunities across transport modes.

This is to be achieved through:

1. Mitigating pedestrian overcrowding through the use of additional footpath width along Park Street.
 2. Managing pedestrian flow at ground level through separation of lobbies and Metro entries to different street frontages.
 3. Clustering support services at ground level, including egress points, to simplify the articulation of the ground plane and ensure clarity between the various functions and lobbies.
 4. Integrating with the Sydney City Centre Access Strategy.
 5. Facilitating safe and adequate pedestrian space at adjoining road crossings and driveways, including provision of traffic management infrastructure as required.
 6. Designing to minimise cyclist conflict with vehicles and pedestrians.
 7. Providing clear and legible interchange with all transport modes, including:
 - a) Town Hall Station
 - b) City and South East Light Rail on George Street
 - c) Bus stops on Park Street, Bathurst Street, Castlereagh Street, and Elizabeth Street.
 - d) Bicycle parking facilities and the future cycle connection on Castlereagh Street.
 - e) Vehicle drop off and pick-up from Bathurst Street entry and taxi bays on Pitt Street and Park Street.
- (See Figure 11: Interchange Opportunities)
8. Consideration of views to the Park Street façade and station entry, particularly in relation to bus queuing along the frontage.
 9. Anticipating connections to the proposed Town Hall Square and other nearby developments.
 10. Strengthening East West connections, including as a connection to green space.
 11. Retaining existing and incorporating new street trees to reduce the heat island effect and supplement existing avenue planting.



Figure 11 – Interchange Opportunities

LEGEND

- | | | | |
|--|------------------------|--|--|
| | Station site | | Existing cycle route |
| | Metro entry | | Proposed cycle route |
| | Existing Train Station | | Proposed taxi bay |
| | Existing Bus Access | | Proposed Kiss & Ride (existing short stay parking) |
| | Existing Bus Stop | | Interchange movement |
| | Future Light Rail Stop | | |
| | Proposed bike parking | | |

Integration and Legacy

Provide an OSD that seamlessly integrates all components of the development and is a positive legacy for future generations.

This will be achieved through:

1. Delivering a high standard of design and finish that promotes longevity and adaptability over time.
2. Functional integration of the various permissible uses with the Sydney Metro component should be seamless, simplifying the vertical division and coordination of services wherever possible.
 - a) Permissible uses should be functionally separated as much as possible at ground level to assist in pedestrian circulation and serviceability
(See Figure 12: Indicative separation of uses).
 - b) Back of house operations and services should be consolidated wherever possible while maintaining any required separation between the OSD and Sydney Metro (See Figure 13: Indicative Service Division).
 - c) Consider and allow for flexible future use of functional spaces & services coordination.

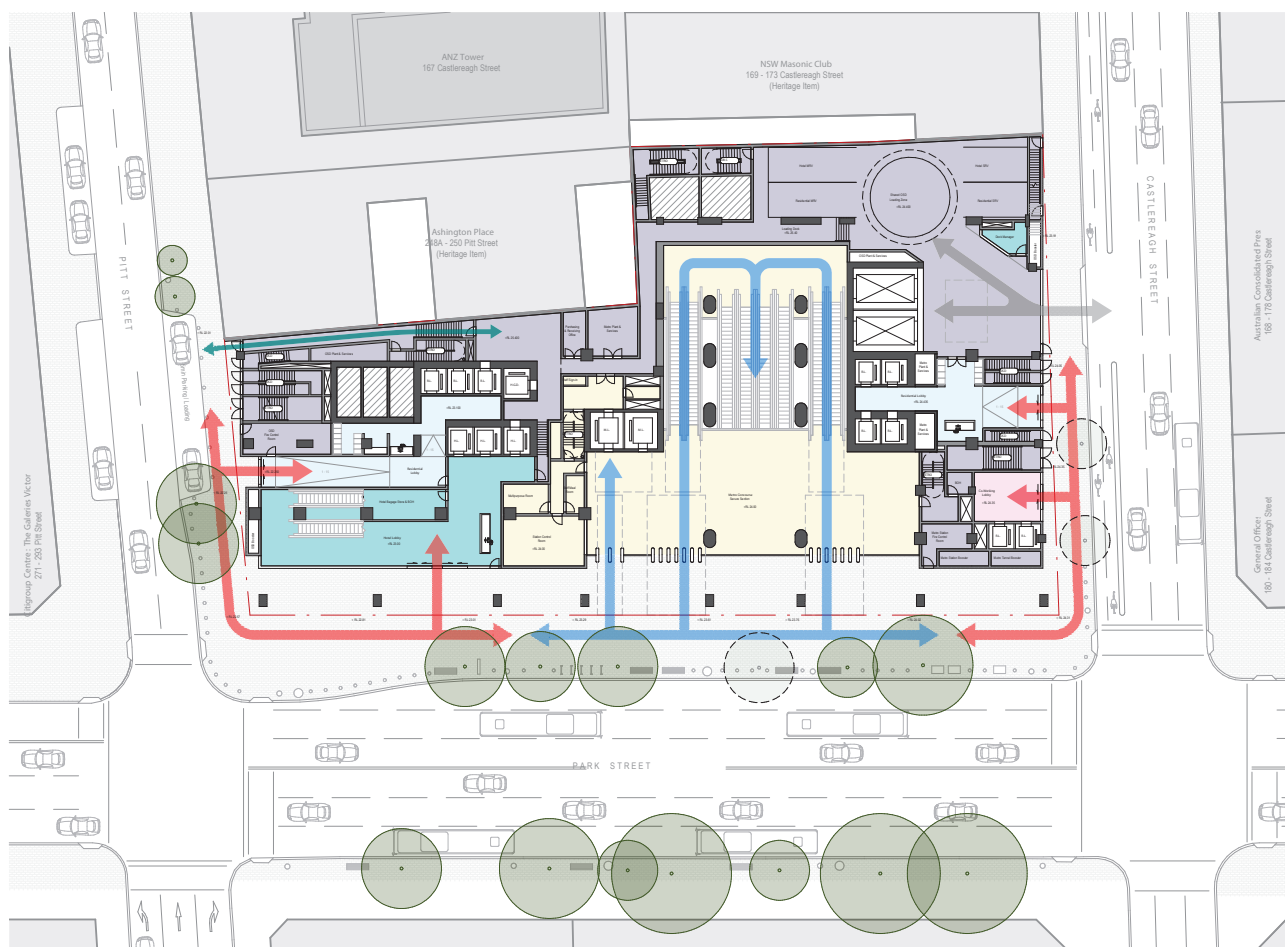


Figure 12 – Indicative separation of uses.

Source: Concept SSDA Design Report – Pitt Street North OSD, Architectus

3. Delivering an over-station development that:

- a) Does not have any adverse impact on the design and/or operation of the Sydney Metro Station;
- b) Is capable of complete demolition and reconstruction, or major maintenance or modification, without significant interference to the operation of the Sydney Metro Station;
- c) Will allow independent access, servicing and maintenance from normal station activities and operation;

d) Integrates efficiently with the station structure;

e) Achieves unity in design through connecting the station entry, podium and built form above the podium, as a single readable piece of architecture including to provide continuity and well considered transitions of bulk and scale between the station box and the over station development design;

f) Provides visual connectivity between the OSD lobby and the public domain.

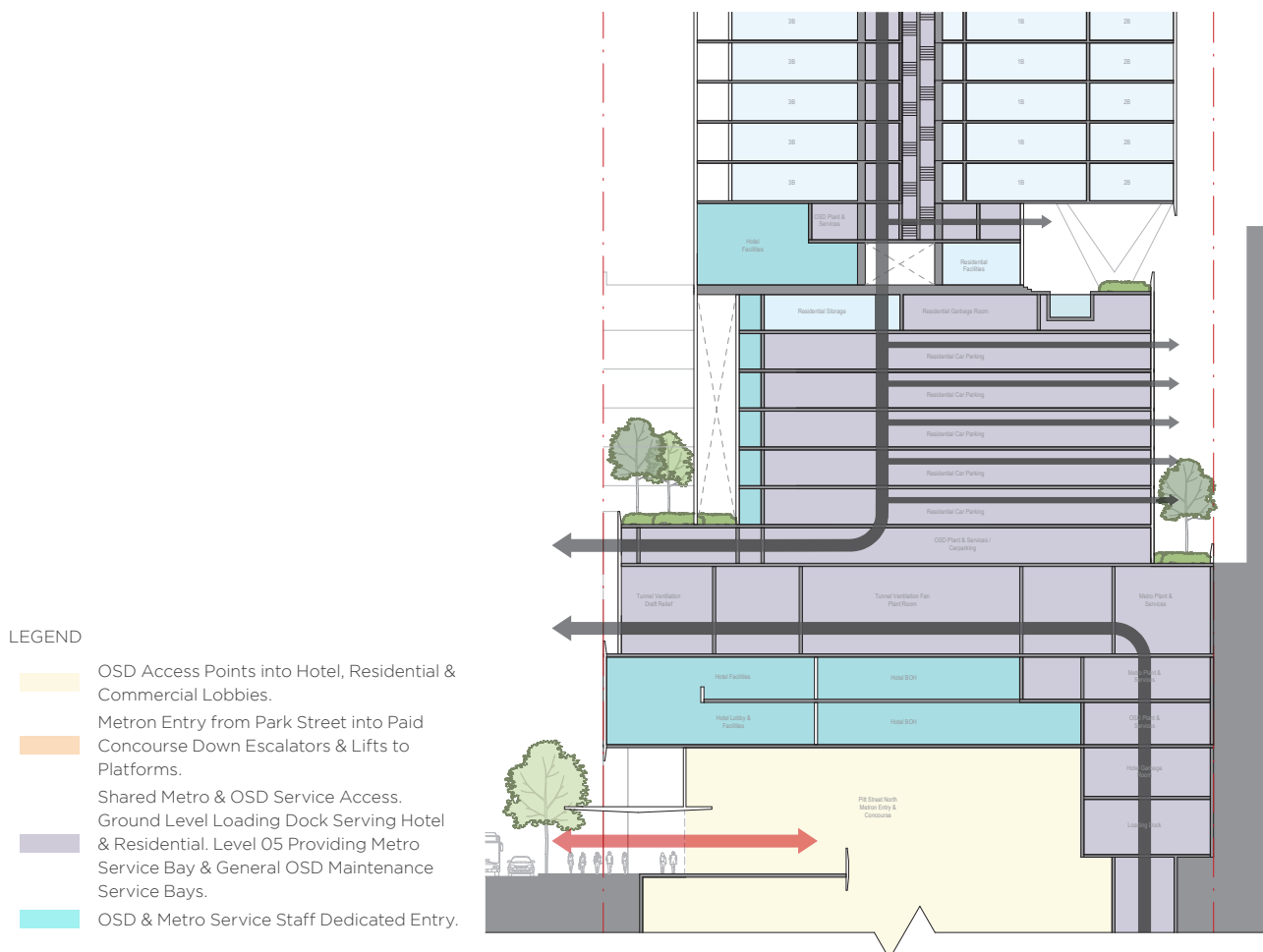


Figure 13 - Indicative Service Division

BENCHMARKS

Sydney Metro has identified benchmark projects that demonstrate the design quality aspirations for the two Pitt Street integrated station development sites.

These benchmarks have been selected to showcase the minimum quality expected in relation to:

1. Integrated design outcomes.
2. Built Form above the podium that showcases high quality design and contribute positively to the city skyline.
3. Architecture that responds to adjoining buildings and streetscape character and scale.
4. A design that provides a high quality public space that is integrated, connected, active, safe and comfortable for customers and pedestrians.
5. A design that fulfils the needs of a civic station entry and a high quality OSD entry with associated servicing.
6. Materials and finishes that are high quality and appropriate to the context.
7. Integration of joyful public art and public domain elements that contribute to a good experience of the place.
8. Well considered strategies in façade and services integration that contribute towards best practice sustainable outcomes.


Each benchmark has been chosen to endorse a variety of design outcomes as outlined in the table below. Further details of these projects are provided in the Pitt Street Design Quality Benchmarks and are to be used to guide design outcomes for the over-station development.


Benchmark	Wynyard Place 10 Carrington St, Sydney	Lumiere' Bathurst St, Sydney	AHL Headquarters + Hilton hotel 478 & 488 George St, Sydney	5 Martin Place	Upper House 520 Swanston St, Carlton, Melbourne	Nishi building 25 Edinburgh Ave, Canberra, ACT
Integrated design	✓	✓	✓	✓	✓	✓
Positive contribution to skyline	✓	✓	✓	✓	✓	
Streetscape character and scale	✓	✓	✓	✓	✓	
High quality public space	✓	✓		✓		
Civic station entry and high quality OSD entry	✓	✓		✓		✓
High quality materials and finishes	✓	✓	✓	✓		✓
Public art and public domain elements	✓	✓				✓
Best practice sustainability	✓			✓		✓



Contact us

For more information visit our website sydneymetro.info or contact us via:

 **1800 171 386** 24-hour community information line

 **sydneymetro@transport.nsw.gov.au**

 **Sydney Metro**
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Haymarket, NSW 1240


 If you need an interpreter, call Translating and Interpreting Services National on **131 450** and ask them to call **1800 171 386**



Figure 14 – Artists impression of Park Street frontage

