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**URBIS**

# **SUPPLEMENTARY RESPONSE TO SUBMISSIONS REPORT**

Atlassian Central  
8-10 Lee Street, Haymarket

Prepared for  
**VERTICAL FIRST PTY LTD**  
5 August 2021



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**We acknowledge, in each of our offices, the Traditional Owners on whose land we stand.**

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# 1. INTRODUCTION

This Supplementary Response to Submissions Report (**SRtS**) has been prepared by Urbis on behalf of Vertical First Pty Ltd to address the matters raised by government agencies following the notification of the Response to Submissions (RtS) to relevant stakeholders from 1 July to 15 July 2021. The SRtS relates to the proposed Atlassian Central State Significant Development (**SSD**) application (SSD-10405) in relation to the site at 8-10 Lee Street, Haymarket (the site).

The Department of Planning, Industry and Environment (**DPIE**) issued a Request for Additional Information letter to the applicant on 23 July 2021 requesting a response to the submissions received and a response to a number of items within Schedule 1 of that request.

As with the original exhibition of the SSD, no agency has formally 'objected' to the project. This provides a strong level of support for the project, albeit that further information is sought with respect to some areas of the project.

This document provides a summary of the submissions received, outlines further engagement with stakeholders which has occurred following receipt of submissions and a formal response to the submissions.

The design integrity of the project continues to be retained and as a result of the responses provided we are of the view that the assessment of the application can be finalised.

## 1.1. OVERVIEW

The RtS was notified to relevant stakeholders from 1 July 2021 to 15 July 2021. During this period, submissions were received from NSW government agencies, the City of Sydney Council (**Council**) and other key public authorities. Submissions from the following public authorities were received requesting further information:

- Department of Planning, Industry and Environment (**DPIE**)
- City of Sydney (**Council**)
- Transport for NSW (**TfNSW**)
- Heritage NSW – Heritage Council of NSW, Historical Archaeology and Aboriginal Cultural Heritage
- DPIE Water and the Natural Resources Access Regulator (**NRAR**)

The following two submissions were received however provided no further comment:

- NSW Environment Protection Authority (**EPA**)
- Sydney Water

This SRtS provides a response to the DPIE Request for Information as well as the public authority submissions. Supporting specialist documentation accompanies this SRtS and are described in **Section 1.3** below.

## 1.2. SUPPORTING DOCUMENTATION

This RtS is supported by the following amended documentation:

Table 1 Supporting Documentation

Document	Prepared by	Appendix
RFI Wind Memo	RWDI	Appendix A
Reflective Glare Memo	Inhabit	Appendix B
Revised Construction Staging	BOJV	Appendix C

<b>Document</b>	<b>Prepared by</b>	<b>Appendix</b>
Draft Aboriginal & Historical Archaeological Test Excavation Methodology and Research Design	Urbis Heritage	<b>Appendix D</b>
Road Safety Audit	JMT Consulting and DC Traffic Engineering	<b>Appendix E</b>
Landscape Drawings – Public Domain	Aspect Studio	<b>Appendix F</b>

## 2. RESPONSE TO DPIE REQUEST FOR INFORMATION

The following table sets out a response to each issue raised by DPIE in their Request for Information in response to the RtS.

### 2.1. HERITAGE

Table 2 Response to DPIE Heritage Matters

DPIE Comments		Response
Heritage		
1	Further consideration should be given to addressing the ongoing concerns of Heritage NSW and Council regarding the Inwards Parcels Shed roof and Council's ongoing concerns about the proposed reconstruction of the existing arches fronting Ambulance Avenue with a single wide arch.	<p>A response to each of the two key items is provided in the following sections:</p> <ul style="list-style-type: none"><li>▪ <b>Section 2.1.1</b> – Former Inwards Parcels Shed Roof and</li><li>▪ <b>Section 2.1.2</b> – Ambulance Avenue Heritage Wall</li></ul>

#### 2.1.1. Inwards Parcels Shed Roof

##### Issue

The design of the western component of the Former Inwards Parcels Shed Roof, including the proposed bleacher seating has been the subject of ongoing feedback from various stakeholders including Heritage NSW, Council as well as the Design Integrity Panel (DIP).

The importance of a sensitive design response to this space has been highlighted and is acknowledged.

##### Response

While this element is a minor design item relative to the overall SSDA, its importance in ensuring a balanced approach to European Heritage, Designing with Country and environmental wind conditions is critical. To enable further consultation on design development on this aspect of the development, we recommend this is be managed by a Design Modification Condition.

Further design review and amendments will be made in consultation with DPIE, Heritage NSW, Council and the DIP. This will allow all stakeholders to understand and evaluate the detailed aspects of this space in a collaborative manner. This includes a review and refinement of key design aspects relating to:

- **Heritage** – Ensuring a balanced approach to the articulation of the shed roof form and the future adaptive reuse of the shed with respect to European Heritage.
- **Wind** – Should the western component of the roof retain a trafficable component that includes seating, further design review is to be undertaken to ensure this area is capable of achieving adequate wind comfort criteria.
- **Designing with Country** – The inclusion and aspiration of the design team to deliver upon the GANSW Connecting with Country framework is an important aspect of the proposal. It is our firm intention to ensure this remains integral to future design development of the shed roof.

Nothing within the above consultation will limit the dismantling of the existing roof to enable site works to commence. The reason for this is that the dismantling of the shed is required to facilitate excavation of the site. Given the construction staging of the project, the above consultation and detailed design will be required ahead of CC6.



## Proposed Solution

It is strongly requested that any condition of consent, must not limit the ability for the temporary dismantling of the inwards parcels shed and should require consultation with various stakeholders to the satisfaction of and approval by the Secretary of the Department of Planning, Industry and Environment as the Consent Authority.

Should the above approach be supported, a plan will be provided that identifies the areas that will be the subject of the condition. Figure 1 below identifies the area relating to the western component of the shed roof.

### Condition of Consent:

#### Design Modification

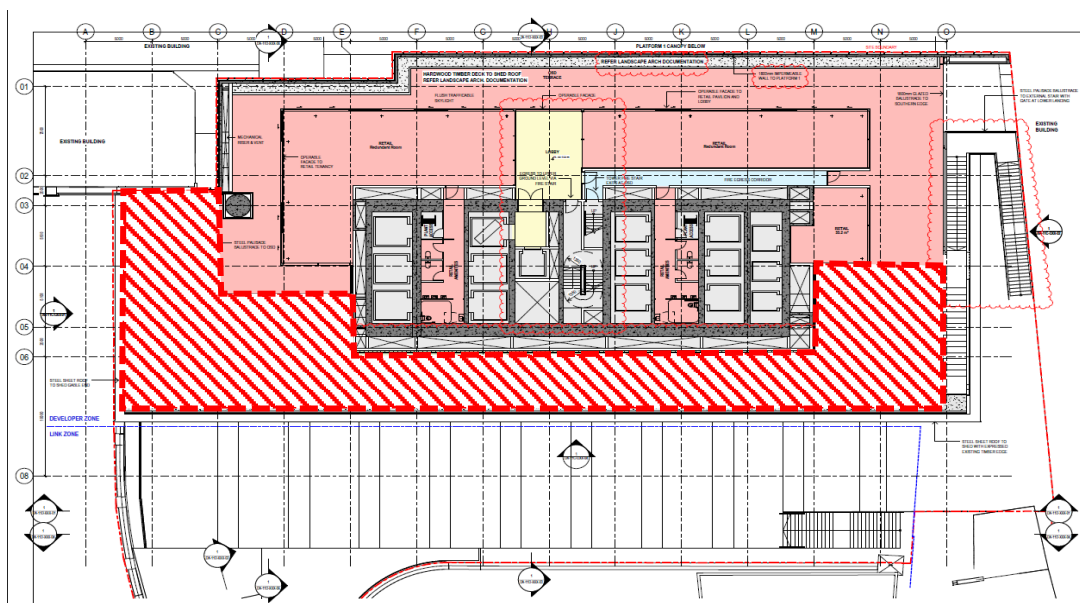
Prior to the release of the Construction Certificate for Stage 6 the design of the western component of the former inwards parcels shed roof (as identified on DA-10B-G03-01) is to be modified. The following matters are to be taken into consideration.

- (a) Design modification must, where possible, preference the retention of heritage fabric and the ability to perceive the inwards parcels shed roof form.
- (b) Evidence of consultation and endorsement of design modifications is to be provided. Consultation is to occur with Heritage NSW, the City of Sydney and the Design Integrity Panel.
- (c) Design modifications are to retain the Designing with Country principles that underpin the current design.

Documentation confirming the endorsed design modification and consultation is to be submitted for approval by the Secretary of the Department of Planning, Industry and Environment.

**Note** – This condition does not preclude the temporary dismantling of the former inwards parcels shed.

Figure 1 Proposed area relating to design modification condition (DA-10B-G03-01)



Picture 1 Inwards Parcels Shed Roof

Source: SHoP + BVN

 Subject to further consultation  
and design development

## 2.1.2. Ambulance Avenue Heritage Wall

### Issue

The Department are seeking further consideration to be given to addressing the concerns of Heritage NSW and City of Sydney Council with respect to the proposed reconstruction of the existing arches fronting Ambulance Avenue with a single wide arch.

We note specifically that Heritage NSW are seeking further detail on the “rationale” for these works, and how the demolition and re-use of fabric and structures will be minimised. In addition, the City of Sydney is concerned that the “scale and construction are poorly proportioned, and inconsistent with the fine classical language, fabric and detail of the retaining wall” and they “don’t support prioritising assumed pedestrian levels at 2056 +15% to the detriment of significant heritage fabric”.

### Response

The applicant has had considerable (and ongoing) engagement with a variety of stakeholders in relation to the detailed design of the pedestrian link zone and interface with the existing archways adjacent to Ambulance Avenue.

This has been critical in ensuring that there is oversight, coordination and integration between all parties – all of which are reflected in both the *Western Gateway Sub-Precinct Design Guidelines* and the *Western Gateway Sub-Precinct Publicly Accessible Space Strategy*. This includes recent endorsement from TfNSW to lodge the SSDA as well as detailed reviewed of the Response to Submissions documentation.

TfNSW has mandated technical design guidelines and requirements for all proponents in the Western Gateway Sub-Precinct to manage the delivery of future transport infrastructure requirements critical to success of this precinct.

The Western Gateway Design Guide which has been through extensive negotiations between all relevant government stakeholders (including the City of Sydney and Heritage NSW) note the following objectives specifically related to the pedestrian connections:

- (d) *Ensure that the **design and width of the pedestrian connections through the sub-precinct are capable of comfortably accommodating the volumes of pedestrian flows and desire lines,** anticipated under a future fully developed scenario for the Central Precinct.*
- (e) *Ensure that the publicly accessible managed space **facilitates the effective future integration of the sub-precinct** with the city and the adjacent sub-precincts.*
- (f) *Ensure the publicly accessible managed space is **comfortable and safe to use for the intended purpose.***

Indeed, there is a very strong and overt focus on:

- The importance of the design and width of the connections
- The requirements to achieve appropriate comfort and the volumes of pedestrian flows and desire lines
- The critical need for safety for ‘the intended purpose’

However, we also acknowledge that this needs to be balanced with the careful design, planning and a careful consideration of the existing heritage fabric to ensure that heritage values are preserved where possible.

Accordingly, both the technical inputs on pedestrian connections and heritage analysis have been undertaken concurrently.

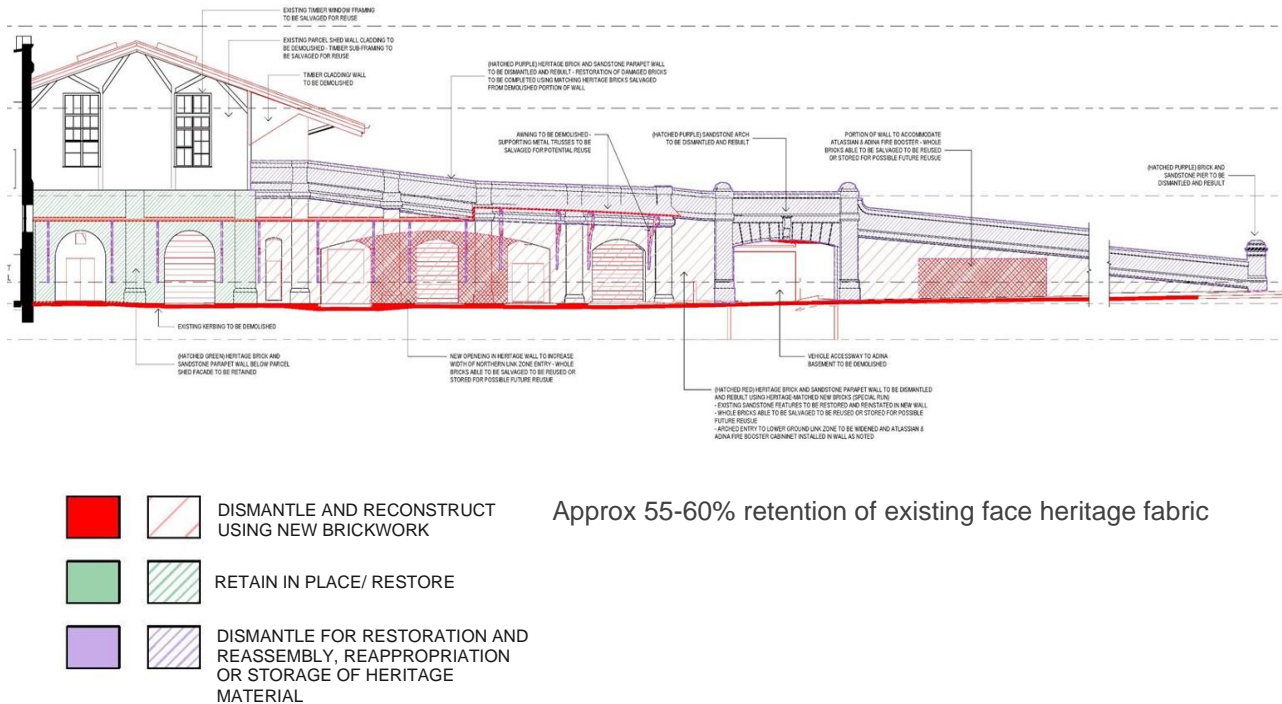
There has been detailed options analysis undertaken to explore alternative solutions for the design of the heritage arches (as put forward by the City of Sydney) and previously submitted with the RtS. However, the very clear outcomes of these investigations were:

- Detailed investigations by Traditional Stonemasonry Consulting have been undertaken and has been previously provided within the Heritage Building Fabric Deconstruction and Storage Report submitted alongside the RtS Heritage Response Report. In line with Figure 2 below, the report notes the difficulty that exists in dismantling the brick component of the heritage wall due to the very strong mortar that was used in its construction. The report states that it could be very difficult to remove the mortar without

damaging the majority of the bricks. As a result of this the methodology seeks the complete salvage of all sandstone embellishments and the demolition of the existing brickwork where salvaging of any bricks during this process is highly encouraged to minimise the extent of new brick manufacture.

- The Heritage Building Fabric Deconstruction and Storage Report provides a detailed explanation of the Ambulance Avenue Retaining wall deconstruction and storage methodologies and procedures.

Figure 2 Proposed Dismantling Methodology for the Ambulance Avenue Heritage Wall



Approx 55-60% retention of existing face heritage fabric

Source: SHoP+ BVN

- Urbis Heritage are satisfied that all attempts to avoid or minimise the extent of fabric to be removed or demolished have been undertaken. Heritage impacts are being mitigated through the careful salvage, conservation and reinstatement of the brick and sandstone parapet wall, as well as the sandstone embellishments along the wall. Urbis Heritage is comfortable that the original confirmation of the openings can be understood through careful interpretation and a refined approach to the reconstruction of this wall to clearly delineate between old and new.
- Detailed pedestrian modelling analysis by ARUP confirms that the width of the pedestrian link to comfortably accommodate the volume of pedestrian flows and desire lines must strictly be a minimum of 13.1m to achieve the required safety levels required from TNSW. There is no flexibility available to reduce this width to achieve TNSW's strict requirements. The options analysis by ARUP are documented in the EIS and RTS provided.
- While the assumed pedestrian levels and modelling are catering for (and future proofing) for future demands, ultimately the significant investment in infrastructure needs to be delivered immediately by Atlasian as part of this development application and be in place to align with the opening of the Sydney Metro. The timing is therefore of critical importance to the State.
- Shop/BVN Architects, Urbis Heritage, TTW, Freeman Ryan and Traditional Restorations Consulting have worked closely together to explore the best heritage and design response which manages the technical pedestrian modelling requirements, but also the best way to acknowledge and interpret both the European and Indigenous heritage response in the lower pedestrian link zone.
- As detailed in the EIS and RtS, and of critical importance to the early site enabling works, the Inwards Parcels Shed and archway will need to be dismantled at the very early stage of construction of the project (anticipated in early 2022). This matter has been very carefully examined given the heritage significance of the site, and a detailed dismantling strategy has been prepared by Traditional Restorations Company (James Ginter) to guide this process in accordance with Heritage NSW requirements.

- TTW Engineers have also prepared a Draft Structural Deconstruction Methodology to identify a safe method of methodically deconstructing and reconstructing the existing shed and arches in order to maintain structural stability, retain critical heritage fabric and provide efficiency.

## Conclusion & Recommendations

Based on the above analysis, it is proposed to maintain the existing design for Ambulance Avenue as per the RtS. Whilst Atlassian recognise the importance of sensitively managing the heritage impacts associated with the arches on Ambulance Avenue, this needs to be balanced with the public infrastructure requirements to ensure the Western Gateway Precinct can meet the needs of the public both on completion and into the future.

Additionally, a number of appropriate safeguards will be in place to manage this matter carefully, including (but not limited to) conditions related to:

- Adherence to the recommendations of the *Heritage Conservation Management Plan* and *Heritage Impact Statements* by Urbis Heritage
- Adherence to the heritage interpretation principles established in accordance with the *Heritage Interpretation Strategy* by Freeman Ryan)
- Adherence to the *Heritage Building Fabric Deconstruction and Storage* report and recommendations by Traditional Stonemasonry Consulting
- Adherence to the *Draft Structural Deconstruction Methodology* by TTW Engineers to maintain structural stability, retain critical heritage fabric and provide efficiency.

## 2.2. WIND IMPACTS

Table 3 Response to DPIE Wind Matters

DPIE Comments		Response
<b>Wind Impacts</b>		
2	<p>Provide an options analysis of any additional mitigation measures which could be implemented within the proposal (rather than Blocks B and C) to</p> <ul style="list-style-type: none"> <li>▪ meet comfort levels for sitting in the future 'Central square' and a minimum 200 m2 area within Railway Square as required by the Western Gateway Sub-precinct Design Guide</li> </ul> <p>address areas which do not meet the safety criteria established by the Western Gateway Sub-precinct Design Guide in day 1, 2 and 3 scenarios.</p>	<p>With respect to both Railway Square and Central Square, the proposal complies with the comfort criteria in the required areas under the WGDG in all scenarios – refer to Image 2a, 2b and 2c of the Wind Memo prepared by RWDI at <b>Appendix A</b>.</p> <p><b>Section 2.2.2</b> below addresses the proposal in relation to the established safety criteria within the WGDG.</p> <p>Temporary Mitigation Strategies are proposed in relation to the safety criteria and are address in detail at <b>Section 2.2.3</b>.</p>
3	<p>In addition to the built form/heritage concerns raised by Heritage NSW and Council about the bleachers on the Inwards Parcels Shed roof, further consideration should be given to:</p>	<p>The response at <b>Section 2.1.1</b> of this report is considered to satisfy this item. A design modification condition is proposed to enable further consultation and review of the design of the western component of the Former Inwards Parcels Shed Roof. Part of this review will focus on the wind environment of</p>

DPIE Comments	Response
<ul style="list-style-type: none"> <li>the appropriateness of the wind mitigation measures from a design and visual impact perspective</li> <li>the ability to provide wind comfort appropriate for the intended use of the bleachers.</li> </ul> <p>If an appropriate heritage, design and wind comfort/safety level cannot be achieved consideration should be given to removing the bleachers from the proposal.</p>	<p>this space should the trafficable component be retained.</p>

## Overview

The importance of a safe and comfortable wind environment throughout the public domain is acknowledged and all parties within the western gateway sub-precinct are working collaboratively to achieve the outcomes required by the Western Gateway Design Guide (WGDG). It is however noted that with a continually changing wind environment within the Western Gateway sub-precinct, that we are not able to solve every outcome with one solution.

The below sections seek to respond to the items raised by DPIE and will similarly respond to the matters raised by the City of Sydney. It is noted that the Atlassian team are currently working closely with the Central Place Sydney team (Block B) to ensure alignment with respect to precinct wind environment conditions. Our wind consultant, RWDI, is also now engaged by the owners of Block B and Block C which will assist in ensuring a holistic wind approach and outcome for the sub-precinct.

In summary, the Wind Memo prepared by RWDI confirms that:

- There are no exceedances of the comfort criteria in a Day 3 (completed Western Gateway Sub-Precinct) scenario nor are there any safety criteria exceedances that can be addressed by the Block A proposal.
- The proposal complies with the requirements of the WGDG, in particular achievement of the required comfort criteria, in all Day 1, Day 2 and Day 3 scenarios. Subsequent to the RtS submission, the design of Block B has evolved resulting in significant improvements to the wind environment condition on Day 2. These design refinements within Block B eliminate any local exceedance of safety criteria within Henry Deane Plaza in a Day 2 scenario.
- Wind conditions at test locations to the south are currently being reviewed in the Block B design development. The Block A development does not impact these locations and accordingly mitigation measures (if required) for a Day 2 scenario will be addressed through the assessment of Block B.
- With the temporary localised mitigation measures proposed below, the development will not result in any exceedance of safety criteria in a Day 1 or Day 2 scenario.

We further wish to note that due to the current development program of Block A and Block B, the outcome of a Day 1 scenario is unlikely to ever eventuate. Should there be any alignment with the Day 1 scenario, this will only occur for a short period of time within which temporary localised mitigation measures can be employed to achieve compliance with the relevant wind safety criteria.

Noting that above considerations, a Wind Memo prepared by RWDI has been prepared and is provided at **Appendix A**. The modelling of the above scenarios was based on the existing surrounding context and does not include any future proposed nearby developments. Further, modelling was undertaken without any mitigation measures (such as landscaping or screening) on any of the blocks within the sub-precinct.

Table 5 below, summarises the wind environment considerations with respect to the proposal.

Table 4 Summary of Wind Comfort and Safety Criteria – Day 1, Day 2 and Day 3

Day	Comfort Criteria	Safety Criteria
<b>Day 1 – Block A only*</b>	Achieved	<ul style="list-style-type: none"> <li>Minor exceedance of &lt;1m/s at Location 52.</li> <li>Safety criteria achieved with inclusion of temporary localised mitigation as per below for further detail.</li> </ul>
<b>Day 2 – Block A and B</b>	Achieved	<ul style="list-style-type: none"> <li>Minor exceedance of &lt;1m/s at Location 42, 44 and 76.</li> <li>Safety criteria achieved with inclusion of temporary localised mitigation.</li> <li>It is noted that the inclusion of landscaping in the wind modelling will further mitigate wind impacts.</li> <li>The concept plan for Central Square notes extensive landscaping and tree planting. Pending the final design of Central Square, the designated area will be able to achieve sitting conditions in Day 1 and 2 scenarios.</li> <li>We note that it is unlikely Central Square will completed prior to Day 2 in which case there will not be an exceedance.</li> </ul>
<b>Day 3 – Block A, B and C</b>	Achieved	<ul style="list-style-type: none"> <li>The proposal for Block A complies with the wind safety criteria in a Day 3 scenario.</li> <li>The exception being locations to the south of Block A which require further solutions from the CPS (Block B) site. Due to wind directions, Block A is unable to influence a Day 3 scenario.</li> </ul>

\*Due to the current development program of Block A and Block B, it is expected that a Day 1 scenario will not eventuate.

### Proposed Solution – Temporary Localised Mitigation Measures

Minor exceedances with regard to the wind safety criteria in certain locations outside of the Block A site have been identified. It is proposed that these areas can be made safe through the installation of temporary localised mitigation measures.

We propose that relevant conditions of consent be provided that require the implementation of temporary local mitigation measures in the event that the scenario under which non-compliant wind outcomes will be realised.

The wind environment of the areas identified in Table 6 can be improved through the use of localised wind mitigation strategies, noting that these would need to be installed outside of the site. Examples of possible temporary mitigation measures have been identified in the Wind Memo prepared by RWDI. Given the potential for localised wind mitigation measures within the public domain, exploration of public artworks that also mitigate wind are proposed. These temporary mitigation measures will therefore result in a positive contribution to the public domain, whilst also delivering a safe wind environment for pedestrian. Refer to Figure 3 below for examples of similar installations.

We proposed the following suggested conditions of consent to require implementation of localised mitigation strategies should the wind outcomes nominated under a Day 1 or Day 2 scenario eventuate.

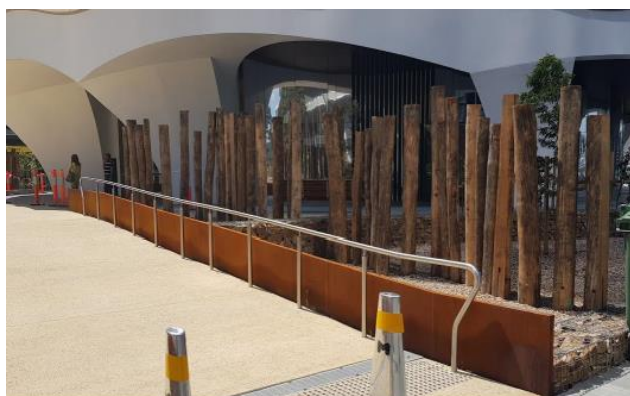
Table 5 Wind Mitigation Strategy

Day	Temporary Mitigation Strategy	Suggest Condition of Consent
<b>Day 1</b>	<p>Location 52 was found to marginally exceed the safety criteria during the Day 1 scenario due to the southerly winds (&lt;1m/s).</p> <p>This could be addressed by localised screening near this area to help break the gust effect, noting that this location satisfies the standing comfort criteria.</p>	<p><b>Temporary Wind Mitigation – Day 1</b></p> <p>Prior to the release of the Occupation Certificate, if Block B and Block C are not completed, documentation identifying temporary localised wind mitigation measures in relation to Railway Square (Location 52) are to be submitted for approval by relevant landowner. Documentation is to include appropriate plans, relevant approvals and evidence of consultation with relevant stakeholders.</p> <p><b>Note</b> – This condition is considered to be satisfied if a Day 1 scenario as described in the approved SSDA does not eventuate.</p>
<b>Day 2</b>	<p>Location 42 and 44 were found to marginally exceed the safety limit criteria(&lt;1m/s), noting that the area has been modelled as a flat open area, while dense landscaping is noted in this area as part of the concept plan.</p> <p>Hardscaping solutions could be included in this area in the form of artwork, similar to the timber screening adjacent to Banksia, Sky Park on Melbourne Quarter and the proposed 2021 Mpavilion design, which are shown at Figure 3.</p>	<p><b>Wind Mitigation – Day 2</b></p> <p>Prior to the release of the Occupation Certificate, if Block C is not completed, documentation identifying temporary localised wind mitigation measures in relation to Central Square (Location 42 and Location 44) are to be submitted for approval by relevant landowner.</p> <p>Documentation is to be prepared, in consultation with the public domain design for Central Square and is to include appropriate plans, relevant approvals and evidence of consultation with relevant stakeholders.</p> <p><b>Note</b> – This condition is only applicable, if Central Square is completed prior to realisation of the Day 2 scenario.</p>
<b>Day 2</b>	<p>Location 76 is noted to be influenced by the westerly winds during the Day 1 and 2 scenarios. This could be addressed through screening along the station platform, similar to mitigation measures used on other station platforms (Chatswood, Glenfield, Wolli Creek etc).</p> <p>The non-compliance at Locations 72 is noted to be due to the current pavilion design between Block A and B which is being revised to address this flow affect.</p>	<p><b>Wind Mitigation – Day 2 – Central Station Platforms</b></p> <p>Prior to the release of the Occupation Certificate, if Block C is not completed, documentation identifying temporary localised wind mitigation measures in relation to Central Square (Location 76) are to be submitted for approval by relevant landowner. Documentation is to include appropriate plans, relevant approvals and</p>



Day	Temporary Mitigation Strategy	Suggest Condition of Consent
		evidence of consultation with relevant stakeholders.  <b>Note</b> – This condition is considered to be satisfied if a Day 2 scenario as described in the approved SSDA does not eventuate

Figure 3 Examples of Wind Mitigation Measures



Picture 2 Banksia, Docklands Melbourne



Picture 3 Melbourne Quarter Sky Park



Picture 4 2021 M Pavilion by Map Studio

Source: RWDI Wind Memo – August 2021

## 2.3. TRAFFIC

Table 6 Response to DPIE Traffic Matters

DPIE Comments	Response
<b>Traffic</b>	
4 Noting TfNSW recommends the Lee Street pick-up/drop-off facility (PUDO) must not be used between 7am and 7pm, confirm how Adina Hotel PUDO would be appropriately provided for / managed between the hours of 7am-7pm. Consideration should be given to	Further consultation with TfNSW has occurred following review of their recommended conditions of consent. In particular, concern was raised by Atlassian with regard to the recommended condition restricting the hours of operation of the PUDO area to outside of 7am-7pm.



DPIE Comments	Response
<p>whether the PUDO facility could be relocated to an alternative suitable location.</p>	<p>Following a review of the Road Safety Audit prepared for the Lee Street driveway access, TfNSW provided the below correspondence dated 30 July 2021, which states:</p> <p><i>Based on the advice from TfNSW Customer Journey Planning team, TfNSW agrees to remove the following suggested Condition of Consent included in the TfNSW submission letter:</i></p> <p><i>“Vehicle stopping shall not be permitted on the proposed Lee Street pick up and drop off zone between 7am and 7pm.”</i></p>
<p>5 Provide a copy of the ARUP traffic report relied on (EIS TIA page 33) relating to future intersection modelling. In addition, clarify the Day 1 proposal impact on intersection performance (i.e. excluding Blocks B and C) of the loading/unloading servicing arrangement including existing and proposed LoS for intersection performance.</p>	<ul style="list-style-type: none"> <li>▪ This has not been raised as an issue by TfNSW nor any other stakeholder to date.</li> <li>▪ The ARUP traffic report can found at <a href="https://www.planningportal.nsw.gov.au/draftplans/exhibition/central-precinct-western-gateway-rezoning-proposal">https://www.planningportal.nsw.gov.au/draftplans/exhibition/central-precinct-western-gateway-rezoning-proposal</a>) – Refer to exhibited documents for Block B – Annex M - Traffic</li> <li>▪ Future traffic generation is approximately half of that currently generated by the site, no further traffic modelling is considered warranted.</li> </ul>
<p>6 The EIS TIA (page 33) states traffic modelling considered the operation of the Lee Street / Regent Street intersection during AM/PM peaks – clarify whether this is referring to the small intersection south of the site or the large intersection with George/Pitt streets opposite the site. If referring to the small intersection, provide an assessment of the operational traffic impact on the Lee/George/Pitt Street intersection.</p>	<ul style="list-style-type: none"> <li>▪ This has not been raised as an issue by TfNSW nor any other stakeholder to date.</li> <li>▪ The modelling refers to the intersection south of the site which has been considered separately by TfNSW as part of the planning for the precinct.</li> <li>▪ As future traffic generation is approximately half of that currently generated by the site, no further traffic modelling is considered warranted.</li> </ul>
<p>7 Confirm the as-existing vehicle trip generation for the site during the peaks to allow for a comparison with the proposal.</p>	<ul style="list-style-type: none"> <li>▪ There are a 46 car parking spaces on the Site as well as in the adjacent Ambulance Avenue which will be removed.</li> <li>▪ These 46 cars would generate approximately 20 car movements under current conditions during peak hours.</li> <li>▪ The Atlassian proposal does not include any private car parking and instead only service vehicles will be generated – equating to approximately 10 vehicles.</li> </ul>

DPIE Comments		Response
		<ul style="list-style-type: none"> <li>This is half of the existing traffic generation of the site.</li> </ul>
8	Confirm why vehicle trip generation during the PM peak has not been provided.	<ul style="list-style-type: none"> <li>The trip generation during the PM peak hour is consistent with that in the AM peak hour.</li> </ul>

## 2.4. OTHER MATTERS/ ADDITIONAL INFORMATION REQUESTED

Table 7 Response to all other DPIE matters

DPIE Comments		Response
<b>Other matters/Additional information requested</b>		
9	Provide further information in relation to the findings of the EIS Reflectivity Assessment (Table 9) which indicates an instance where reflective glare would be acceptable but disturbing.	<p>A clarification has been prepared by Inhabit that seeks to clarify the criteria applied with regard to reflective glare.</p> <p>The original report submitted with the EIS, identifies the Daylight Glare Probability (<b>DGP</b>) which is the index commonly used to evaluate glare from daylight dependent on the usage of a space.</p> <p>&lt;0.40 is the limit set for 'discomfort' and</p> <p>&lt;0.45 is the limit set for 'disability'</p> <ul style="list-style-type: none"> <li>Transport (road, tram &amp; rail drivers) is set to &lt;0.40 to mitigate 'discomfort'.</li> <li>General public spaces are set to &lt;0.45 to mitigate 'disability' however allowing some discomfort as non-safety critical tasks are carried out in these areas.</li> </ul> <p>With regard to areas referenced in Table 9 of the report, calculated values fall between the discomfort and disability criteria, which is acceptable for public spaces.</p> <p>The purpose of annotating 'disturbing glare' in parenthesis is to highlight that some discomfort may be present but not to a level set as disabling.</p> <p>Refer to <b>Appendix B</b>.</p>
10	Provide assessment/justification for the internal amendments to the YHA accommodation, in particular the removal of the YHA Head Office floorspace and operational / reception area. Further, clarify whether staff / security would be present at the facility 24/7 and how guests would be	<p>YHA had planned to locate a new Corporate Head office in the new facility, this office was not related to the Hostel Office or management. This office will no longer be located at the new facility and will be replaced with guest facilities.</p>

DPIE Comments	Response
<p>received and the use appropriately managed, in the absence of a reception area.</p>	<p>Travel to level one is by the three dedicated YHA lifts, similar to the arrival experience at Sydney Harbour YHA. On arrival at level one, new guests will be met by YHA hosts who will greet them and organise check in with a more casual environment. Streamlining and automation of administrative tasks has reduced the requirement for formal multiple workstations allowing the removal of a static reception desk and large back-office areas. Clearly identifiable YHA team members will be on site and available 24/7 to manage arrivals, departures and in-house guests.</p> <p>The YHA team at level one and within the hostel will include a property manager and sufficient staff to manage the administrative, housekeeping, food and beverage and guest services functions of the property. There will be team members on site 24/7, with the total number at any time dependant on the occupancy of the property.</p> <p>External building signage will announce the location of the Railway Square YHA and direct guests to the YHA lift lobbies on the lower and upper ground floors. At these lift lobbies YHA guests will be directed by clear signage to the reception on level one.</p> <p>Access to all rooms and after hours entry to the property will be controlled by RFID - radio frequency identification, technology which enable the exchange of data between key cards and readers.</p>
<p>11 Provide a response to the EIS public submission made by Frasers Property Australia and Dexus Funds Management (owners of Block B).</p> <ol style="list-style-type: none"> <li>1. <i>Consideration of the titling arrangements in the Precinct insofar as they may impact on the delivery and staging of the proposal.</i></li> <li>2. <i>Incorporation of the latest Block B design competition winning scheme and endorsed public realm strategy within revised wind modelling and assessment.</i></li> <li>3. <i>Note that the Atlassian proposed 'Day 2' servicing scenario remains subject to the proponent reaching agreement with the Consortium to facilitate this access via</i></li> </ol>	<p>The following responses are provided to the key items nominated in the submission made by the owners of Block B.</p> <ol style="list-style-type: none"> <li>1. Whilst progressing the SSDA, the Block A proponent continues to work closely with all precinct landowners to ensure that titling and land tenure arrangements are suitably resolved to enable the delivery and staging of the project.</li> </ol> <p>The registration of the stratum subdivision lots and extinguishment of easements will be progressed through the delivery phase of the project and registered prior to Practical Completion.</p> <ol style="list-style-type: none"> <li>2. The incorporation of the Block B design competition winning scheme within the wind model occurred prior to submission of the RtS.</li> </ol>

DPIE Comments	Response
<p><i>Block B. Whilst this is supported in principle by the Consortium, any other use of Block B (such as pedestrian diversions under Pedestrian Access Option 2) place a significant cost burden on the Consortium and will be subject to separate commercial negotiations.</i></p> <p>4. <i>Note that the proposed structural connection and associated engineering detail (including impacts on Sydney Water assets) remain subject to agreement with the Consortium.</i></p> <p>5. <i>Acoustic measures should be implemented within Block A to ensure that the ground plane retailers of Blocks A, B and C can operate harmoniously with the occupants of the proposed Block A hotel and office.</i></p>	<p>We continue to work closely with CPS to achieve a precinct-wide approach to wind matters. It is further noted that Atlassian's wind consultant, RWDI is now also engaged by the owners of Block B.</p> <p>3. Comments from the Consortium in relation to the Pedestrian Access Option 2 as proposed in the Construction Management Plan (CMP) are acknowledged. Vertical First can confirm that the Pedestrian Access Option 2 is not required as part of the project construction staging and delivery options and was noted in the CMP as a potential alternative to Option 1.</p> <p>It is acknowledged that the proposed Day 2 scenario requires access to be granted via the Block B. Vertical First confirms that the SSDA proposes that approval is only being sought for a Day 1 access scenario. In the event that Block B development proceeds then a future Day 2 scenario will be subject to further planning approvals.</p> <p>4. The proposed structure connection and engineering detail has been developed based on the existing land rights or of the Vertical First or the Block A proponent. While the parties will continue to coordinate on future Day 2 connections there is no further agreement required with the consortium to enable the Day 1 project to progress.</p> <p>5. This comment is acknowledged and agreed. Design of the YHA component of the development has consideration for the proposed existing and future ground plane outcomes including proposed retail and activation opportunities.</p>
<p>12 Provide landscaping drawings (Day 1 only) including scale, date, reference and issue number for inclusion as part of the determination.</p>	<p>Landscape Drawings have been prepared by Aspect Studio and are provided at <b>Appendix F</b>.</p>
<p>13 Update the construction staging (EIS Table 2) to include basement and driveway works, provide information within blank 'Associated Works' boxes, clarify what '4b podium façade' relates to noting the IP Shed is located at Stage 6 and provide a clearer</p>	<p>The construction staging table provided at Table 2 of the EIS has been amended in response to the comments from DPIE. The amended Construction Staging table is provided at <b>Appendix C</b>.</p>

DPIE Comments	Response
summary description of heritage works for Stage 6.	
<p>14 Confirm whether the TfNSW's recommended conditions (RtS submission) and Council's recommended conditions (EIS, RtS and separate emails regarding public art and subdivision) are agreed to.</p>	<p><b>City of Sydney Recommended Conditions</b></p> <p>The following response was provided within the RtS Report in response to the proposed conditions of consent provided by Council. It is copied here as the response has not changed.</p> <p>The City has provided a number of recommended conditions of consent as part of their submission and in separate correspondence through DPIE (public art and subdivision). While these conditions are agreed in principle we seek to reiterate the importance of the proposed construction staging provided in the original EIS and updated as part of this SRtS. Drafting of conditions that make reference to "submission and approval prior to issue of any Construction Certificate" should be changed to "the relevant construction certificate" to align with the staged CC process proposed in our EIS. The above is noted in relation to the drafting of all conditions of consent.</p> <p>In the case of heritage and public domain related conditions, this should be changed to "issue of a construction certificate for structure above the slab-on-ground".</p> <p>Clarification is required with regard to Council's original SSDA submission in relation to Item 7 as to whether Service Vehicle Size Limit is a final OC (operational) requirement and will not be reflected in any construction activity restrictions.</p> <p>We assume that the City is aware of the Atlassian project lease agreement with TfNSW for Ambulance Avenue therefore "loading &amp; unloading during construction" is permitted to occur from this area without restriction.</p> <p><b>TfNSW Recommended Conditions</b></p> <p>Following consultation with TfNSW, it is understood that the following suggested condition will be removed and this will be confirmed to DPIE in writing by TfNSW.</p> <p>"Vehicle stopping shall not be permitted on the proposed Lee Street pick up and drop off zone</p>

DPIE Comments		Response
		<p>between 7am and 7pm” – Page 3 TfNSW RtS Submission</p> <p>Subject to deletion of the above suggested condition, the recommended conditions provided by TfNSW are agreed.</p>

## 3. RESPONSE TO AGENCY SUBMISSIONS

### 3.1. CITY OF SYDNEY

Table 8 Response to City of Sydney Comments

City of Sydney Comment		Response
<b>Process</b>		
	The City maintains its concerns regarding the assessment process as raised in previous correspondence. It is imperative that the Western Gateway Design Guide is finalised as soon as possible and to ensure that the subject development demonstrates adherence to this guide.	The Western Gateway Design Guide was approved on 15 July 2021. As a result we believe this matter is now resolved.
<b>Minor overshadowing of Prince Alfred Park</b>		
1	The matters raised previously have been resolved and we note that the development now complies with the Sun Access Plane requirements for Prince Alfred Park.	Noted
<b>Heritage</b>		
2	While City staff remain concerned with the extent of demolition proposed, the reincorporation and interpretation of significant fabric is supportable. Conditions of consent regarding any Heritage Interpretation Strategy should require consultation with the City.	Noted and agreed.
3	<p>The bleachers continue to have an unacceptable impact on what will remain of the former Inwards Parcels Shed as follows:</p> <ul style="list-style-type: none"> <li>a. The bleachers and pavilion obscure most of the original roof form, reducing the legibility of the item. The realignment of the stairs on the southern elevation exacerbates this issue.</li> <li>b. The mitigation measures recommended to improve the wind conditions on the bleachers (see page 15 of the wind report) add visual clutter and reinforce the unsuitability of this space for respite and recreation. The applicant proposes that this space may be used for outdoor meetings and presentations, however, the space does not meet the wind comfort criteria for this purpose.</li> <li>c. Given its heritage status, the overlaid space is unnecessary with regards to the provision of open space (public access will be restricted) and connectivity throughout the precinct. The applicant</li> </ul>	<p>In response to continued feedback from agencies regarding the design of the western component of the Former Inwards Parcels Shed, it is proposed that this area be the subject of a design modification condition. This condition will require further design review and development in consultation with Council, Heritage NSW and the Design Integrity Panel. We note that this element has received strong support from Aboriginal stakeholders in relation to Designing with Country and Connecting with Country, and it is the applicants strong desire that these considerations are maintained through the further design resolution of this space.</p> <p>The proposed single archway to the Ambulance Avenue retaining wall is being driven by Transport for NSW</p>

City of Sydney Comment	Response
<p>foreshadows future connection to the Over Station Development (OSD), however, this is uncertain at this stage. The City staff during consultation with the applicant recommended that the bleachers be removed and a physical connection to the OSD explored at later date following the exhibition of the draft controls for the OSD.</p> <p>d. The City strongly encourages the inclusion of First Nations' knowledge and representation within development through a genuine engagement with the Gadigal people. However, the provision of a small green roof as an example of "Designing with Country" appears shoehorned into the application and a means to justify the much more prominent remainder of the bleachers. While the First Nations community feedback was positive regarding the bleachers, it is disappointing that this homage to the original landscape and First Nations' culture would be located on a privately controlled roof with limited visibility from the public domain and subject to uncomfortable wind conditions.</p> <p>The Government Architect's Designing with Country discussion paper and Draft Connecting with Country Framework reference the Australian Indigenous Design Charter as a pathway to successfully and genuinely design for Country. While there has been meaningful consultation with First Nations people as documented in Appendix D of the RtS, the design for the Shed does not appear led by First Nations people. An alternative approach that could protect the Shed roof and provide greater connection to the public domain could be the incorporation of First Nations' artwork on the soffit.</p>	<p>infrastructure requirements and pedestrian safety analysis. The design team is committed to developing a design resolution for the truncated pilasters and remainder of the brick and stone wall which is appropriate from a heritage perspective while also delivering the necessary width for future pedestrian travel.</p>
<p>4 While the City does not object to the reduced separation between the Shed and tower above, the pavilion structure continues to add unnecessary bulk and thereby undermine the void between the Shed and the soffit.</p>	<p>No changes are proposed to the pavilion structure. The pavilion structure was previously presented to various stakeholders including the DIP who supported the introduction of this element in particular how it resolves wind impacts in this area. This has been document in the Design Integrity Endorsement submitted with the original EIS as well as the RtS.</p>
<p>5 City staff acknowledge the changes made to the eastern wall adjoining Platform 1, however, this continues to be a missed opportunity to incorporate artwork or heritage</p>	<p>The full Heritage Interpretation Plan has not yet been prepared. Please refer to other comments below from Freeman</p>



City of Sydney Comment		Response
	interpretation as previously suggested. The proposal should be drafted prior to determination and finalised prior to the issue of any Construction Certificate.	Ryan Design regarding Heritage Interpretation.
6	City staff raise no objections to the form and materiality of the northern elevation of the Shed, particularly the use of reeded glass.	Noted.
7	The new wide arch fronting Ambulance Avenue is not supported as its scale and construction are poorly proportioned, is inconsistent with the fine classical language, fabric and detail of the retaining wall and will have a negative impact on the heritage significance of the place. The City does not support prioritising assumed pedestrian levels at 2056 +15% to the detriment of significant heritage fabric. A review of the assumptions within the pedestrian modelling must be undertaken in light of the lack of access through Block B to the future redevelopment of the bus layover along with the remote work from home trend, including Atlassian's directive for staff to work at home permanently.	<p>The proposed single wider archway to the Ambulance Avenue retaining wall is being driven by Transport for NSW infrastructure requirements and pedestrian safety analysis. The design team is committed to developing a design resolution for the truncated pilasters and remainder of the brick and stone wall which is appropriate from a heritage perspective while also delivering the necessary width for future pedestrian travel.</p> <p>Refer to <b>Section 2.1.1</b> of this report for further information in response to this item.</p>
8	It is recommended to amend the design to three arches based on the proportions, arch centring, and construction of the existing decorated arch immediately to the west. The three arches are to be placed between the re-constructed, full-height engaged brick piers.	Refer to <b>Section 2.1.1</b> of this report for further information in response to this item.
9	Conditions of consent are recommended requiring consultation with the City and Design Integrity Panel to review the construction design and materiality of the Shed, eastern boundary wall and design of the arches prior to the issue of any Construction Certificate. City staff will provide draft conditions on request.	<p>The proponent does not agree with this recommendation. As the City are not the Consent Authority they should be removed as a party to any condition.</p> <p>Should a condition be imposed, this should be in consultation with the DIP (upon which the City has representation) and to the satisfaction of the Secretary of the Department of Planning, Industry and Environment as the Consent Authority. Further, should a condition be imposed, timing should be associated with respect to the relevant Construction Certificate given the stages of construction to which these items relate.</p>
<b>Wind</b>		

City of Sydney Comment		Response
10	The City maintains its concerns regarding wind impacts from the proposed development and the cumulative impacts from all towers within the Western Gateway.	Responses addressing this item are provided in <b>Section 2.2</b> of this report. Reference is also made to the Wind Memo prepared by RWDI at <b>Appendix A</b> which provides a thorough assessment of the cumulative impacts of all towers within the sub-precinct.
11	City staff note that the proposed wind mitigation measures in the 'Day 1' scenario will create unsafe wind conditions in Railway Square. The Day 2 and 3 scenarios create additional unsafe wind conditions within Henry Deane Plaza and the public domain. The design must be amended such that wind speeds do not exceed the safety criteria and endanger the public.	Responses addressing this item are provided in <b>Section 2.2</b> of this report.  It is noted that there are no available design amendments for the Block A design that will resolve the wind in external public domain areas.  As such, it is proposed that conditions of consent be applied to require temporary localised wind mitigation measures be put in place to manage these concerns.
12	The proposed wind mitigation measures on the bleachers and to the pavilion over the OSD connection (see page 18 of the wind report) are excessive, do not achieve design excellence and are contrary to the draft Western Gateway Design Guide. The applicant's wind report burdens Blocks B and C with resolving uncomfortable and unsafe wind conditions, rather than addressing these issues holistically. The applicant must resolve these issues with their neighbours prior to determination.	It is noted that the wind consultant for Atlassian Central, RWDI is now acting for all parties within the sub-precinct. This will mean a holistic and coordination response to the wind environment is achieved for the precinct.  With reference to <b>Section 2.1.1</b> of this report, it is proposed that the western component of the Former Inwards Parcels Shed will be the subject of a design modification condition. This condition, if supported by DPIE, will require further design refinement through consultation with Council, Heritage NSW and the Design Integrity Panel to achieve a positive and agreed outcome for the Inwards Parcels Shed Roof, including related wind conditions.
<b>Noise</b>		
13	The City maintains its recommendation for naturally ventilated spaces within the hostel to comply with the City's draft Alternative natural ventilation of apartments in noisy environments performance pathway guideline.  <a href="https://www.cityofsydney.nsw.gov.au/development-guidelines-policies/alternative-natural-ventilation-">https://www.cityofsydney.nsw.gov.au/development-guidelines-policies/alternative-natural-ventilation-</a>	We note that the YHA is a hostel and is defined as tourist and visitor accommodation pursuant to Sydney LEP 2012. Further SEPP 65 and the Apartment Design Guide do not apply to tourist and visitor accommodation. While the recommendation of the City is noted, it is not considered appropriate to require

City of Sydney Comment	Response
<a href="#">apartments-noisy-environments-performance-pathway-guideline-dr</a>	<p>a draft policy that is intended for residential apartments to apply in these circumstances.</p> <p>The amenity and experience of visitors to the YHA is important and the optimisation of the YHA facade as proposed by the RtS provides opportunity for each guest to have direct access to natural ventilation through a manually operate window located in each room. All rooms are provided air conditioning for situations where external noise or weather requires windows to be closed, or for guest preference.</p> <p>To require a higher level of natural ventilation and acoustic attenuation is considered unnecessary and would burden the project with considerable costs and redesign of the YHA component of the development.</p>
14 We request that the Demolition and Construction Noise and Vibration Management Plan be prepared in consultation with City staff prior to the issue of any Construction Certificate.	Noted. We understand a condition of consent will be provided accordingly.
<b>Tree Management</b>	
15 City staff maintain our suggestions regarding the provision of trees throughout the public domain to mitigate urban heat island and contribute to the visual amenity of the precinct. The wind report recommends a row of trees within the upper link zone to mitigate wind impacts. The City reinforces its preference for a row of trees to be provided, similar to what has been achieved at the boardwalk at Barangaroo.	<p>We acknowledge the City's suggestions, however, note that we are faced with several constraints along the upper link zone that limit us with the number and placement of trees. These are as follows:</p> <ul style="list-style-type: none"> <li>▪ Swept path requirements for Franner truck access</li> <li>▪ 5.2m wide pedestrian modelling clearance requirement, TfNSW's required minimum clear width for pedestrians movements for 2056.</li> <li>▪ Requirement from TfNSW for demountable landscape elements. This is allow for potential increases in the width of the upper link zone to meet future needs for pedestrian movement.</li> </ul>

City of Sydney Comment		Response
		<ul style="list-style-type: none"> <li>▪</li> <li>▪ BCA requirements for balustrade offset along heritage wall.</li> <li>▪ CoS Soil volume requirements for small trees.</li> <li>▪ Architectural skylights (needing to remain clear of these).</li> <li>▪ There being no opportunity for setdowns in the architectural slab layout.</li> </ul>
16	The City's previous suggestion was for additional tree planting on the ramp between Lee Street and the subject site. Sufficient space is provided to achieve this and should be conditioned accordingly.	It is noted that the depth of tree pits are limited by the 3.6m clear height to the dive ramp. The dive ramp design cannot accommodate further setdowns to allow for additional trees. We can only provide sufficient space for tree pits where it is possible to do so.
17	Conditions of consent are recommended requiring consultation with the City prior to the issue of any Construction Certificate regarding the provision and infrastructure to support tree planting. City staff will provide draft conditions on request.	Should a condition of consent be considered necessary, it must be worded with reference to "the relevant" Construction Certification given the importance of construction staging. Tree Planting relates to the public domain and will occur in the latter stages of the project delivery.
<b>Landscaping</b>		
18	The City strongly supports the aspirations of the developer to provide a high-quality landscaped environment throughout the public domain and within the tower and reaffirms the previous recommendations made.	Noted
19	Conditions of consent are recommended requiring further landscape details within the public domain and the tower to ensure their success. City staff will provide draft conditions on request.	<p>Should a condition of consent be considered necessary, it must be worded with reference to "the relevant" Construction Certification given the importance of construction staging. Tree Planting relates to the public domain and will occur in the latter stages of the project delivery.</p> <p>Any condition should only relate to landscaping within the public domain. It is not considered necessary or appropriate</p>

City of Sydney Comment		Response
		for any such condition to apply to landscaping associated with the tower (internal or external).
<b>Public Domain and Water Sensitive Urban Design</b>		
20	City staff will review the MUSIC Link report submitted with the RtS. Conditions of consent can be recommended regarding protecting and aligning with the public domain on request.	Noted.

## 3.2. DPIE WATER AND NRAR

Table 9 Response to DPIE Water and Natural Resources Access Regulator

DPIE Water and NRAR Comments		Response
<b>Prior to Approval</b>		
1	The proponent should demonstrate adequate groundwater entitlements can be obtained for the projects expected water take. The NSW Water Register shows 3929ML are held across 183 Water Access Licences (WAL) in the Sydney Basin Central Groundwater Source, which is higher than the licensable share component. Therefore the proponent must trade from existing licences, or through a controlled allocation to account for the predicted groundwater take of approximately 5ML per year.	Vertical First Pty Ltd are currently in the process of purchasing water from the market. A Section 61 approval was provided on 22 July 2021 by NRAR for a new water access licence - zero share component under the <i>Water Management Act 2000</i> (Application: D1020859 and Reference: 10AL124419).  The process of purchasing the water takes several months.
<b>Post Approval</b>		
2	A WAL must be obtained prior to water take unless an exemption under the <i>Water Management (General) Regulation 2018</i> applies.	Noted

## 3.3. TRANSPORT FOR NSW

Table 10 Response to Transport for NSW

TfNSW Comments		Response
<b>Safety Assessment of the Proposed Development</b>		
1	<b><u>Comment</u></b>  The Response to Submissions prepared to support the development application states the following:	A Stage 2 (Concept Plan) Road Safety Audit (RSA) for the proposed Lee Street access arrangement, Lee Street pick and drop off arrangement and the proposed access arrangement was prepared in accordance

TfNSW Comments	Response
<ul style="list-style-type: none"> <li>▪ The request for the preparation of a Road Safety Audit (RSA) by TfNSW is acknowledged however considered to be too early in the design process;</li> <li>▪ This point in the project will present a more suitable time to undertake the RSA in accordance with the TfNSW recommendation, with the findings and recommendations of the RSA to be incorporated in the final drawing set for the project; and</li> <li>▪ A review of crash data on Lee Street near Ambulance Avenue has been undertaken indicates there has only been three crashes in the last five years, none of which resulted in a serious injury or a fatality (see summary in Appendix B of the Traffic and Transport Response Report provided at Appendix E). A number of pedestrian related crashes were recorded at the existing mid-block pedestrian crossing which will be unchanged as part of the proposal.</li> </ul> <p>It is advised that:</p> <ul style="list-style-type: none"> <li>▪ The proposed Lee Street pick up and drop off area is part of the development proposal. Appendix E of the Traffic and Transport Response Report shows a total of six (6) pedestrian crashes including serious injury crashes in the last five years in the vicinity of the pick and drop off area as well as proposed access to the development;</li> <li>▪ The traffic and pedestrian conflict is a concern due to the number of near misses observed along Lee Street and the fact that a significant number of pedestrians cross at midblock locations along Lee Street. The proposed arrangement is likely to increase conflict involving vehicles leaving the kerb and vehicles changing lanes just after the intersection and pedestrians crossing Lee Street at midblock locations. Any incidents on Lee Street would have potential to impact on general traffic and bus operation with the CBD and beyond; and</li> <li>▪ Earlier a project is audited the more likely that the road safety issues or risks identified can be significantly reduced or eliminated. As a result</li> </ul>	<p>with Austroads Guide to Road Safety Part 6: Managing Road Safety Audits and Austroads Guide to Road Safety Part 6A: Implementing Road Safety Audits by an independent TfNSW</p> <p>accredited road safety auditor.</p> <p>The RSA was prepared by Damien Chee of DC Traffic Engineering and is provided at <b>Appendix E</b>.</p> <p>The RSA did not identify any safety issues associated with the future No Parking zone on Lee Street which TfNSW had previously raised concerns about. Discussions with DC Traffic Engineering have confirmed that this No Parking zone was considered in the audit and no safety issues were deemed to be of significance in this regard.</p> <p>JMT Consulting have prepared a covering letter summarising the findings of the RSA in relation to the proposal. This is provided with the RSA at <b>Appendix E</b>.</p> <p>It is also noted that TfNSW have separately recommended a condition of consent requiring the high and medium priority items noted in the RSA to be further considered prior to a Construction Certificate being issued, with mitigation measures to be proposed to the satisfaction of TfNSW. No objection is raised to this proposed condition.</p>

TfNSW Comments	Response
<p>this minimises compromises in road safety and costly treatments at later stages of the project.</p> <p><b><u>Recommendation</u></b></p> <p>It is requested that the applicant undertakes the following prior to the determination of the development application:</p> <ul style="list-style-type: none"> <li>▪ A Stage 2 (Concept Plan) Road Safety Audit for the proposed Lee Street access arrangement, Lee Street pick and drop off arrangement and the proposed access arrangement in accordance with Austroads Guide to Road Safety Part 6: Managing Road Safety Audits and Austroads Guide to Road Safety Part 6A: Implementing Road Safety Audits by an independent TfNSW accredited road safety auditor; and</li> </ul> <p>Based on the results of the road safety audit, the design drawings needs to be reviewed to implement safety measures in consultation with TfNSW.</p>	
Suggested Conditions of Consent	
<p>2 Attachment B provides suggested conditions of consent</p>	<p>As previously noted –</p> <p>Further consultation with TfNSW has occurred following review of their recommended conditions of consent. In particular, concern was raised by Atlassian with regard to the recommended condition restricting the hours of operation of the PUDO area to outside of 7am-7pm.</p> <p>Following a review of the Road Safety Audit prepared for the Lee Street driveway access, TfNSW provided the below correspondence dated 30 July 2021, which states:</p> <p><i>Based on the advice from TfNSW Customer Journey Planning team, TfNSW agrees to remove the following suggested Condition of Consent included in the TfNSW submission letter:</i></p> <p><i>“Vehicle stopping shall not be permitted on the proposed Lee Street pick up and drop off zone between 7am and 7pm.”</i></p>

## 3.4. HERITAGE COUNCIL OF NSW

The below responses have been prepared by Urbis Heritage and Freeman Ryan Design.

Table 11 Response to Heritage NSW Comments

Heritage NSW Comment		Response
<b>Outstanding Matters</b>  <i>The RTS documentation still does not clearly articulate how the proposed heritage impacts to the SHR listed site's significance and values have been assessed and clearly understood, and how it has been used to inform the development.</i>		
1	The Heritage Council notes that the updated <i>Heritage Setting View Analysis report</i> better demonstrates the obvious visual changes to the precinct, including to its predominant low height scale and development density as a result of the proposal. The renderings now clearly show the cumulative impacts of the proposed tower cluster within the Western Gateway sub-precinct and the significant impacts to the site's State heritage significance and values, including the erosion of the readability (legibility) of the heritage cultural landscape and its context.	Noted.
2	Due to the proposed changes to the vernacular <i>Former Inwards Parcels Shed</i> and surrounding elements (including the Upper Carriage Lane and forecourt, retaining wall elements), it is critical that as much significant fabric as possible is retained to ensure visual clues to the former use and function of the place are maintained, as part of the overall SSD redevelopment outcomes. This is in addition to the need for high-quality and engaging interpretation of this building and its function ( <i>see recommendations below</i> ).	Noted.
<b>Interpretation</b>		
3	We generally support the overarching Heritage Interpretation Strategy for interpretive experiences and elements including the key themes. We note that detailed design development is still yet to be undertaken for the further development of stories, and the selection of appropriate experiences, elements and designs. This should be further developed in unison, and work with and complement the Designing with Country Framework, and as part of the overall design development for the building. This includes the development of a heritage interpretation plan.	Atlassian propose that building design is developed in parallel to developing the heritage interpretation and will work with the Designing with Country Framework. This will be included in the Heritage Interpretation Plan that will be undertaken by Freeman Ryan Design.
4	The Heritage Interpretation Strategy states ongoing consultation with several groups. However, this does	<b>Freeman Ryan Design:</b> Noted and agreed. It is expected that the further



Heritage NSW Comment		Response
	not include Heritage NSW or the City of Sydney. We request that the Heritage Council and these key entities continue to be consulted as part of the detailed design development to ensure there is a consistent and coherent approach for this critical component of the project.	development of the Heritage Interpretation Strategy will be the subject of conditions of consent and will involve consultation with Heritage NSW and the City of Sydney.
5	We also reiterate that interpretation should communicate and strengthen the strong visual and historic connection between the former Inwards Parcels Shed and the former Parcels Post Office building, as well as the connection and function between the Shed and Platform 1.	<b>Freeman Ryan Design:</b> Noted and agreed. It is expected that the further development of the Heritage Interpretation Strategy will be the subject of conditions of consent.
6	In addition, that the interpretive experience is considered across the whole SHR site, including integration with the two other blocks within the Western Gateway sub-precinct, as well as the broader Central Railway Station SHR site. As previously advised, the Heritage Council requests that there be a coordinated and consistent heritage interpretation approach and design across the whole precinct. This is critical for a seamless and wholistic interpretive experience.	<b>Freeman Ryan Design:</b> We agree with the approach that a site wide interpretation needs to be made, but as yet no-one has been commissioned to do this. FRD are in the process of developing site wide as well as site specific themes to facilitate this approach in the future and this will be incorporate within the Heritage Interpretation Strategy for Atlassian Central.
7	We note that public art, wayfinding and signage will be integrated into the overall detailed design. Whilst we encourage public art to consider heritage interpretation themes and stories, we recommend that public art is used as a supplement to heritage interpretation and not a substitute. Both heritage interpretation and public art have their own role and purpose though work in unison and complement each other.	<b>Freeman Ryan Design:</b> Noted and agreed. Coordination and consultation will occur with the Public Art Consultant for Atlassian Central, Amanda Sharrad, to ensure a cohesive outcome for public art, wayfinding and signage, but one that supplements heritage interpretation rather than takes its place.
8	<b>Recommendation:</b> <i>The Heritage Council, Heritage NSW and City of Sydney are to be consulted as part of the ongoing detailed design development of the project's heritage interpretation to ensure there is a consistent and coherent approach for this critical component of the project.</i>	<b>Freeman Ryan Design:</b> Noted and agreed. It is anticipated that relevant conditions of consent will be impose with respect to review and approval of the final heritage interpretation strategy.
9	<b>Recommendation:</b> <i>The project's heritage interpretation should communicate and strengthen the strong visual and historic connection between the former Inwards Parcels Shed and the former Parcels Post Office building, as well as the connection and function between the Shed and Platform 1. The heritage Interpretation is to be considered across the whole SHR site, including integration with the two</i>	<b>Freeman Ryan Design:</b> Noted and agreed.

Heritage NSW Comment		Response
	<i>other blocks within the Western Gateway sub-precinct, as well as the broader Central Railway Station SHR site.</i>	
10	<b>Recommendation:</b> <i>The project's heritage interpretation is to be developed to the satisfaction of the Heritage Council or its delegates.</i>	<b>Freeman Ryan Design:</b> Noted and agreed.
<b>Design with Country Framework</b>		
11	We support the inclusion of the Designing with Country Framework that has commenced setting out the opportunities and specific design measures to reinforce Aboriginal Cultural Heritage and placemaking. It is acknowledged that this document is still being developed and will be augmented and updated as the project progresses. To assist with this task, the project team are encouraged to also reference the GANSW 'Connecting with Country' framework released November 2020.	Noted and agreed, the future work will both reference and engage with the Connecting with Country framework set out by the GANSW.
12	We reiterate that this and the project's heritage interpretation are critical parts of the interpretation of the place and should be an integral part of the overall design work in unison and complement each other. We strongly recommend ongoing consultation and input from Heritage NSW.	Noted and agreed.
13	<b>Recommendation:</b> <i>The Heritage Council and Heritage NSW are to be consulted as part of the detailed design development of the Designing with Country framework to ensure there is a consistent and coherent approach for this critical component of the project.</i>	Noted and agreed.
<b>Visual Changes</b>		
14	We acknowledge the <i>Heritage Setting View Analysis</i> report has been updated to include the two other significant multi-storey developments proposed for the Western Gateway sub-precinct. However, we request that a further visual analysis be undertaken to provide a more holistic analysis of all views including looking from the south (from Broadway) and from the east (from Prince Alfred Park) to the proposed development. This will allow a clearer understanding of the visual impacts of the proposed development.	<p>We wish to refer Heritage NSW to review the Visual Impact Assessment (VIA) prepared by Urbis that was provided as Appendix S of the original SSDA submission. The views that are identified in this request were documented in this analysis.</p> <p>A further detailed response to this item is provided at <b>Section 3.4.1</b> of this report.</p>
15	Again, we request that the legibility of the adapted former Inwards Parcels Shed be emphasised, further	As noted in the Heritage Views Analysis Report, the visual prominence of the

Heritage NSW Comment	Response
<p>to our previous comments around the insertion and scale of the new tower and cumulative neighbouring developments. This includes the Heritage Council request to remove or significantly re-scale the proposed green roof, sought by Atlassian as part of the proposed adaptation of the former Inwards Parcels Shed. This was a significant element of previous Heritage Council submissions and sought to achieve a better reflection of the original, unadorned industrial character of the shed post the insertion of a multistorey tower through its innards. It is noted that the green roof component is still proposed as part of the works but with limited additional content to demonstrate how heritage impacts have been minimised.</p>	<p>proposed tower will gradually diminish as other proposed and approved tower forms emerge into the skyline. The towers will be located in close proximity to form a cluster of height and a new visual gateway at the south end of the CBD. Such visual changes are compatible with the desired future character for the areas and are anticipated by the strategic planning framework for the site, sub-precinct and wider Central State Significant Precinct.</p> <p>Notwithstanding the high level of visual effects in two close views the assessment of other relevant factors such as compatibility with the strategic planning context and desired future character for the sub-precinct and wider precinct reduced the overall level of visual impact.</p> <p>As has been noted previously in this report in relation to the design of the western component of the Former Inwards Parcels Shed, it is proposed that this area be the subject of a design modification condition. This condition will require further design review and development in consultation with Council, Heritage NSW and the Design Integrity Panel.</p>
<p>16 The provided renderings emphasise that the ability to read the former Inwards Parcels Shed structure within the industrial landscape setting and its aesthetic is still considerably diminished, particularly when viewed from Railway Square. We stress that this is important in the context of the broader Atlassian development, which will result in the complete demolition and partial adaptation of the former Inward's Parcels Shed, including diminishment of its significance, setting, authenticity and historic use.</p>	<p>The Heritage Views Analysis Report determined that in close views where the foreground included heritage items, the level of proposed tower form was found to be spatially well separated and juxtaposed with the low height form and visual character of heritage items. In the close views modelled, the proposed tower form did not dominate the composition, the open space setting or 'visual curtilage' of items which remained visually distinct and prominent features in views. The architectural detailing, facade treatment, materials and colours proposed for the tower are contemporary and contrast highly with the predominant colours and materials which character the heritage items.</p> <p>Additionally, the legibility of the adapted former Inwards Parcels Shed is</p>

Heritage NSW Comment		Response
		emphasised by the cantilevered built form which extends above the Parcels Shed, creating visual permeability into the site and a 'sense of space' above its low form. This reduces the perception of the bulk and scale of the proposed built form in some views, such as from the intersection of Pitt and George Streets.
17	<b>Recommendation:</b> <i>The green roof component of the works to be substantially modified to minimise impacts to the industrial character of the site, its setting and the shed. This is to be undertaken to the satisfaction of the Heritage Council.</i>	Please refer to <b>Section 2.1.1</b> of this report for a response to this item.
<b>Changes to the Former Inwards Parcels Shed</b>		
18	The former Inwards Parcels Shed is historically important as an integral part of the SHR listed Central Railway Station and contributed to the role of the railway transport system in the delivery of regional mail. As stated above, the proposed works include demolition and a partial rebuild of the Shed which will have obvious impacts to fabric and setting. We previously requested a reduction to the extent of demolition and removal of fabric as much as possible to minimise impact to significant/original fabric.	As previously addressed, the proposal seeks to retain as much of the original and significant fabric as possible whilst still facilitating the development. See below comment regarding retention and integration of fabric.
19	It is not clearly demonstrated from the additional information provided how the reuse of the demolished fabric will be incorporated and used appropriately. It is unclear how it will be determined what significant elements will and will not be salvaged and the reason for their inclusion/exclusion. Elements such as the brick chimneys and sandstone plinths of the shed provide evidence of the former use of the area and association with the former Inward's Parcels Shed and its historic function. As noted previously, these elements provide visual clues as to the former use and function of the place and should be meaningfully used as part of the adaptation and interpretation of the building.	A detailed dismantling and deconstruction methodology has been prepared by Traditional Restorations Company (James Ginter) for the proposal. Urbis Heritage are satisfied that all attempts to avoid or minimise the extent of fabric to be demolished or removed have been undertaken. For instance, Atlassian have committed to reuse all timber elements to be dismantled onsite in the new building, and other elements such as the brick chimneys will be salvaged where possible and integrated into the fabric of the new building for interpretation and continue the lifecycle of the element.
20	<b>Recommendation:</b> <i>Further detail and designs are required to understand what elements will and won't be salvaged, and how this is determined. Then how they will be reused. This should be undertaken in</i>	The design team are preparing a comprehensive plan package in conjunction with the construction methodology to identify each item of fabric and the intended work to it (retention,

Heritage NSW Comment		Response
	<i>consultation with, and to the satisfaction of the Heritage Council or its delegate.</i>	replacement, modification etc). This information is not yet available.  This requirement could form part of a condition of consent in the same way that a schedule of conservation works would.
<b>Other Environmental Matters</b>		
21	<p>In addition to the demolition of the former Inward's Parcels Shed, the proposed works include extensive demolition to elements which have obvious impacts and loss to fabric, context and setting.</p> <ul style="list-style-type: none"> <li>▪ Lower Carriage Lane retaining wall for temporary construction access and a new large arched opening</li> <li>▪ removal of all awnings along Lower Carriage Lane for temporary construction access and a new large arched opening</li> <li>▪ modifications to surface levels to provide a ramp to the Upper Carriage Way</li> <li>▪ sandstone plinths and corbels, iron vent grates</li> </ul> <p>As noted previously, these elements provide visual and physical evidence of the former use and historic function of the Shed. The visual clues to its former use are even more critical to seek to maintain, as part of the overall SSD redevelopment, due to the substantial impacts.</p>	Noted.
22	<p>The RTS documentation states that the scope of this work has been based on the future broader Western Sub-precinct revitalisation being undertaken by others including TfNSW. However, how this will be manifested from an architectural and urban design perspective is yet to be realised, including its relationship with this proposal.</p> <p><b>Recommendation:</b> <i>Further information is to be provided to better demonstrate the rationale for the proposed works and how the demolition and reuse of fabric and structures will be minimised. This is to be undertaken in consultation with, and to the satisfaction of the Heritage Council or its delegate.</i></p>	Noted.
23	The proposed inclusion of large landscaping along the northern side of the former Parcel's Post Office building (facing the proposed future Third Square) will have visual impacts on the building and its significant	The proposed landscaping works will not detrimentally obscure or impact the Former

Heritage NSW Comment		Response
	<p>façade, as well as the setting of the place. This further compromises the industrial aesthetic of the place and should be removed.</p> <p><b>Recommendation:</b> <i>The proposed large landscaping along the northern side of the former Parcel's Post Office building (facing the proposed future Third Square) is to be removed.</i></p>	<p>Parcels Post Office building or the elements within the subject site.</p> <p>The proposed landscaping contributes to the broader precinct public domain outcomes and also provides support for localised wind mitigation to ensure a comfortable pedestrian environment.</p>
24	<p>A Temporary Protection Plan for the site is developed to ensure that significant buildings and fabric are adequately protected during the works. Protection systems are to ensure significant historic fabric is not damaged or removed, potential impacts due to vibration are minimised, and traffic is appropriately managed during the works. In addition, the monitoring and repair of any damage of significant items (including the former Parcels Post Office building) as a result of construction should include inspection before, during and after completion of the works.</p> <p><b>Recommendation:</b> <i>A Temporary Protection Plan for the site is to be developed to the satisfaction of the Heritage Council or its delegate.</i></p>	<p>It is noted that this recommendation could be resolved via an appropriate condition of consent.</p>
<b>Historical Archaeology</b>		
25	<p>We appreciate that previous comments have been responded to in the Urbis RTS report pp.78-80, including advice that further test excavation is anticipated to be undertaken under a separate Section 60 approval pathway. This will be cross-referenced with the surrounding archaeological context.</p>	<p>Noted</p>
26	<p>Heritage NSW advises DPIE that this is an appropriate strategy and that a prior S57 for initial testing in Ambulance Avenue was approved in October 2020. Should testing identify intact archaeological resources then Conditions of Approval should be placed on the development. This must ensure that management of the historical archaeology is in accordance with an Archaeological Research Design, Work Method Statement, and by an appropriately qualified Excavation Director who meets the Heritage Council Criteria.</p>	<p>Noted. It was originally intended to undertake the consolidated historical archaeological and Aboriginal archaeological test excavation program prior to SSD-10405 approval under a Section 60 approval, AHIP and Development Consent.</p> <p>However, due to changes in the program, it is now expected that test excavation will be undertaken post-SSD-10405 approval in accordance with Conditions of Consent.</p> <p>For the purposes of this Response to Submissions, we have appended to this letter a preliminary draft Aboriginal and Historical Archaeological Research Design</p>

Heritage NSW Comment	Response
	<p>(ARD) to outline the proposed test excavation methodology which will be applied post-SSD-10405 approval.</p> <p>This ARD contains critical recommendations in terms of proposed timeframes and program to ensure that the test excavation aligns with the contractor's Construction Certificate program and construction methodology, and also responds appropriately to the site-specific topographical constraints of the property.</p> <p>The ARD also includes recommendations regarding the test excavation Condition of Consent to be included with the SSD-10405 approval, to ensure that the proposed staged test excavation program is achievable with consideration for the staging of construction works.</p> <p>We would welcome and encourage the opportunity to discuss these recommendations with DPIE and Heritage NSW to ensure that the proposed Condition of Consent and test excavation program is satisfactory prior to SSD-10405 approval being issued.</p> <p>It is expected that the ARD will be updated post-SSD-10405 approval in consultation with Heritage NSW / DPC.</p>
<b>Aboriginal Cultural Heritage</b>	
<p>27 If SSD approval is to occur prior to completion of the test excavation program, we would recommend the proposed archaeological research design and methodology be prepared and submitted to your Department to be considered as part of the approval.</p>	<p>A Draft Aboriginal &amp; Historical Archaeological Test Excavation Methodology (EM) and Research Design (ARD) has been prepared by Urbis Heritage (<b>Appendix D</b>).</p> <p>The current ARD and EM have been developed to support the State Significant Development Application and provide a framework to investigate the nature, spatial and vertical extent, and integrity of any Aboriginal archaeological resource that might exist within the subject area, including any original soil profile beneath the imported fill.</p>

Heritage NSW Comment	Response
	<p>The ARD and EM have been designed based on the following:</p> <ul style="list-style-type: none"> <li>▪ The conclusions and recommendations of the ACHAR for the subject area (Urbis 2021).</li> <li>▪ The conclusions, recommendations and excavation methodology of the HAA for the subject area (AMBS 2020).</li> <li>▪ ALT /TfNSW Site Investigations - Atlassian Central Building (Built/Obayashi), Urbis Issue 30th July 2021; and</li> <li>▪ Atlassian Building Central - Project Overview (Built/Obayashi July 2021), Urbis Issue 30th July 2021</li> </ul> <p><u>Objectives</u></p> <p>The objectives of the ARD and EM are to:</p> <ol style="list-style-type: none"> <li>1. Investigate the nature, spatial and stratigraphical extent, condition and integrity of any Aboriginal archaeological deposits that may be present within the subject area.</li> <li>2. If Aboriginal archaeological deposits are identified, apply relevant research questions to interpret the finds and results in context of local and regional archaeological modelling.</li> <li>3. Provide a detailed methodology for the excavation, salvage and management of Aboriginal objects that might be found in various scenarios during the execution of the archaeological investigation.</li> </ol>

### 3.4.1. Heritage Setting View Analysis

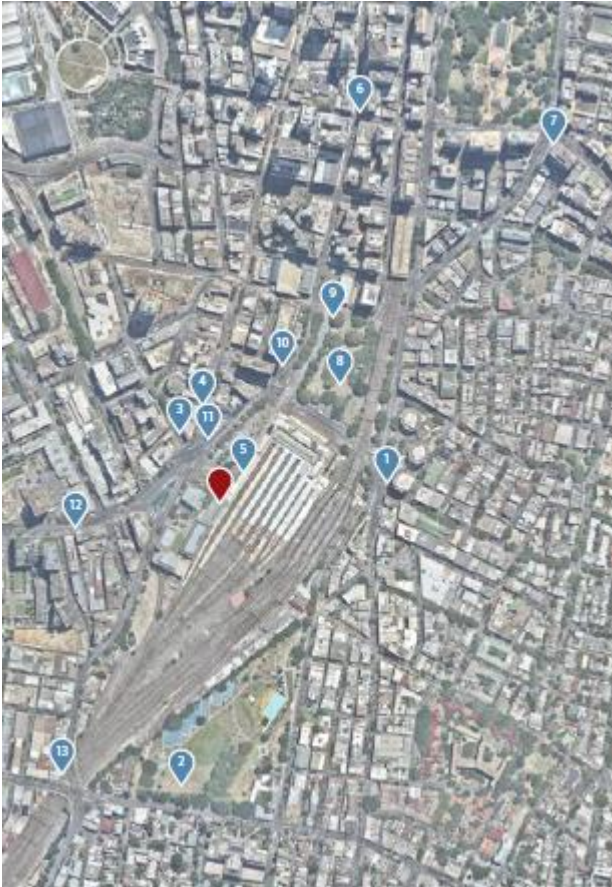
In response to the comment from Heritage NSW at item 14 above, the below response is provided.

***We acknowledge the Heritage Setting View Analysis report has been updated to include the two other significant multi-storey developments proposed for the Western Gateway sub-precinct. However, we request that a further visual analysis be undertaken to provide a more holistic analysis of all views including looking from the south (from Broadway) and from the east (from Prince Alfred Park) to the proposed development. This will allow a clearer understanding of the visual impacts of the proposed development.***



Views looking to the north-east (from Broadway) and views looking to the north (from Prince Alfred Park) are addressed within the Visual Impact Assessment (VIA) submitted with the EIS. Figure 3 below indicates a view point location map from the VIA, where views 12 and 2 represent views that would be available from parts of Broadway and Prince Alfred Park, respectively. The fully rendered views included in the Heritage Setting Views Analysis report should be considered in conjunction with the the full range of views included in the VIA. In our opinion the selection of view places and views modelled provide an adequate representation of the likely changes that would occur subsequent to the approval and construction of the built form proposed.

Figure 4 Selected Views for Photomontages



Source: Urbis

### **Views from Broadway (looking north-east towards the subject site)**

View 12 is towards the subject site from Broadway is an axial view enclosed by built form on either side of Broadway (Great Western Highway). The upper parts of the proposed tower will be visible in upward, oblique views above foreground built form. In this regard the proposed development does not create any significant visual effects in the composition of this view. The construction of the built form proposed will not block views to or between heritage items, access to scenic features and will block only areas of open sky. The proposed tower form sits wholly within the approved building envelope. In this regard the proposed tower is consistent and compatible with the extent of visual effects and level of visual impacts that are contemplated by the controls which relate to the Western Precinct. In our opinion the compliance of the tower form with the permissible building envelope is considered to be a 'down weight' when considering the significance of the overall visual impact. This view is the approximate location of a draft DCP view and as in the VIA to has an overall low visual impact.

### **Views from Prince Alfred Park (looking north towards the subject site)**

The view from Prince Alfred Park is approximately equivalent to the view denoted in the draft DCP from the southern end of the park. The proposal introduces a new built form into the mid-ground composition above the low built form foreground. The slim tower will be seen in isolation against open areas of sky and will predominantly block areas of open space and some background building development. The tower form does not block views to scenic features or heritage items. The proposed development occupies only a narrow part

of a much wider composition and in this regard does not dominate views from Prince Alfred Park. Central Station Clock Tower remains unaffected by the location of the proposed tower. The proposed tower form sits wholly within the approved building envelope. In this regard the proposed tower is consistent and compatible with the extent of visual effects and level of visual impacts that are contemplated by the controls which relate to the Western Precinct. In our opinion the compliance of the tower form with the permissible building envelope is considered to be a 'down weight' when considering the significance of the overall visual impact.

### Additional Documented Views

Figure 4 below identifies other views documented by Urbis which are included in Section 8 of the VIA submitted with the EIS. 25 additional views were captured across the potential visual catchment including others from the south and south-east from Broadway and from Prince Alfred Park. These views were not selected for further analysis or modelling due to low or limited visibility of the subject and the most notable visual landmark – the Central Station Clock Tower. Commentary about view selection is included in sections 3.0 and 4.0 of the VIA.

Figure 5 Additional Documented Views included in the VIA report.



Source: Urbis

Based upon all views analysed, including assessment of photomontages to determine the effects and impacts of the views from both Broadway and Prince Alfred Park, the visual impacts of the proposed development were found to be acceptable.

## 4. CONCLUSION

The Atlassian Central development presents a truly visionary design for the Site. It has taken into consideration the unique attributes of the Site and surrounding context including the broader revitalisation of the Central Precinct and provided a considered design response. The proposal includes a respectful adaptive reuse of the Parcels Shed which is part of the broader Central Station State Heritage Listed item. The heritage significance of the Site has been embraced through the design process to ensure the design respects and celebrates this historic building form and location.

This Supplementary Response to Submissions Report has been prepared to address the matters raised by DPIE and other government agencies following the notification of the Response to Submissions for the Atlassian Central State Significant Development Application (SSD-10405). In particular key responses have been provided with regard to heritage, wind and traffic related matters. Responses are also provided to the agency submissions received.

Detailed environmental assessment of these risks has been undertaken in the preparation of this SSDA Package and subsequent Response to Submissions and the risk levels associated with these aspects of the development have been considered, and on balance the benefits of the project significantly outweigh these risks.

Accordingly, the Atlassian Central development is considered appropriate for the Site and warrants approval. We trust the information provided within this report assist the Department in finalising their assessment of this significant city shaping development.

# DISCLAIMER

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All surveys, forecasts, projections and recommendations contained in or associated with this report are made in good faith and on the basis of information supplied to Urbis at the date of this report, and upon which Urbis relied. Achievement of the projections and budgets set out in this report will depend, among other things, on the actions of others over which Urbis has no control.

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This report has been prepared with due care and diligence by Urbis and the statements and opinions given by Urbis in this report are given in good faith and in the reasonable belief that they are correct and not misleading, subject to the limitations above.

## **APPENDIX A**

## **RFI WIND MEMO**

## **APPENDIX B**

## **REFLECTIVITY CLARIFICATION MEMO**

## **APPENDIX C**

## **REVISED CONSTRUCTION STAGING**

## **APPENDIX D**

# **DRAFT ABORIGINAL & HISTORICAL ARCHAEOLOGICAL TEST EXCAVATION METHODOLOGY AND RESEARCH DESIGN**



## **APPENDIX E**

## **ROAD SAFETY AUDIT**

## **APPENDIX F**

## **LANDSCAPE DRAWINGS – PUBLIC DOMAIN**

