

SEARS SCOPING REPORT

**8 - 10 LEE STREET,
HAYMARKET**

26 NOVEMBER 2019
SA7274
PREPARED FOR ATLISSIAN PTY LTD

URBIS

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1. INTRODUCTION

In accordance with Schedule 2 of the *Environmental Planning and Assessment Regulation 2000 (The Regulation)*, this report has been prepared to request the Secretary's Environmental Assessment Requirements (**SEARs**) to guide the preparation of a State Significant Development Application (**SSDA**) for the development of a mixed-use 'tourist and visitor accommodation' and commercial development at 8 - 10 Lee Street, Haymarket (the **Site**).

This report has been prepared by Urbis Pty Ltd (**Urbis**) on behalf of Atlassian Pty Ltd (the **Applicant**) and has been informed by the preliminary drawings prepared by EC3 with Terroir.

The proposed development is classified as State Significant Development (**SSD**) on the basis that it falls within the requirements of Clause 13(2) of Schedule 1 of the *State Environmental Planning Policy (State and Regional Development) 2011 (SRD SEPP)*, being:

(2) Development for other tourist related purposes (but not including any commercial premises, residential accommodation and serviced apartments whether separate or ancillary to the tourist related component) that—

(a) has a capital investment value of more than \$100 million, or

(b) has a capital investment value of more than \$10 million and is located in an environmentally sensitive area of State significance or a sensitive coastal location. (our emphasis)

The SRD SEPP provides the following definition of 'environmentally sensitive area of State significance':

environmentally sensitive area of State significance means—

...

(h) land, places, buildings or structures listed on the State Heritage Register under the Heritage Act 1977, or (our emphasis)

It is noted that the Capital Investment Value (**CIV**) for the 'tourist and visitor accommodation' (in the form of a 'backpackers') component of the development will be greater than \$10 million and the Site is occupied by the former Inwards Parcels Shed (the **Parcels Shed**) which forms part of the State Heritage listing for Central Station.

The development is proposed to accommodate a new Railway Square YHA and head office for Australian enterprise software company, Atlassian. The establishment of the Atlassian head office at Railway Square is to anchor the creation of a new technology and innovation precinct which will attract and retain global talent in this industry.

The commercial office component of the project is sufficiently, intrinsically and inextricably related to the adaptive re-use of the State Listed Heritage Item and tourist component of the project which is detailed in **Section 5.3**.

The purpose of this report is to provide information to support the declaration of the project as being SSD and the preparation of the SEARs. This report has been prepared with and is accompanied by the following technical documentation:

- QS Statement (**Appendix A**).
- Preliminary Drawings (**Appendix B**).

2. BACKGROUND

2.1. ATLASSIAN AND THE NEW SYDNEY INNOVATION AND TECHNOLOGY PRECINCT

Atlassian is an Australian owned enterprise software company which builds platforms and tools for businesses and Startups, and has grown significantly since their creation in 2002. They are globally recognised as a leader in technology and innovation, employs over 3,500 people (or 'Atlassian's') across 11 offices around the world and has over 125,000 customers.

Atlassian have had a very positive impact on the resurgence of Start-up companies in Australia over the last decade. They have assisted with growing local businesses and tried to find ways to help them evolve and be equipped with the technology to do so.

Atlassian has shown strong commitment to anchor the TechCentral Precinct, from initial ideas through to creation of an industry body to advocate 'Tech Central', through to partnering with the NSW Government as part of the NSW Technology & Innovation Precinct, commitment to a lease arrangement with TNSW for the Site, and ultimately into seeking to amend the current planning controls applicable to the Site to create a unique opportunity to accommodate a significant tech ecosystem in the precinct.

Atlassian are seeking to deliver the first building in the new Sydney Innovation and Technology Precinct, to entrench their presence in the precinct, and also provide space to accommodate tech-Startup companies and entrepreneurs within the establishing precinct.

The development will contribute to achieving the precinct goals, providing the initial anchor tenant for the precinct, delivering approximately 70,000sqm of gross floor space, including space to accommodate Startup and early stage companies, and supporting approximately 4,000 innovation jobs.

By anchoring the Sydney Innovation and Technology Precinct, this project, and the presence of Atlassian on the Site will support the establishment of an

innovation precinct in Sydney which has the potential to deliver the following key support to the technology and innovation sector:

- Physically bring together the technology and innovation industry;
- Accelerate growth in these industries through cross-collaboration;
- Attract, grow and retain talent from both within Australia, and globally; and
- Improve likelihood for Start-ups in the industry to succeed.

2.2. WESTERN GATEWAY SUB-PRECINCT PLANNING PROPOSAL

On 12 July 2019, the Minister for Planning and Public Spaces nominated the Central Precinct a State Significant Precinct (**Central SSP**), which comprises approximately 24 hectares of land in and around Central Station. Within this nomination was the identification of the Western Gateway Sub-precinct which could be considered for early rezoning. The Site is located within the Western Gateway Sub-precinct, as well as the broader Central SSP.

In October 2019, Transport for NSW (**TNSW**) submitted a Planning Proposal to rezone two of the three 'Blocks' within the Western Gateway Sub-precinct. The Site is part of the rezoning proposal, known as Block A. The Planning Proposal was placed on public exhibition on 16th October 2019 and will remain on exhibition until 27th November 2019.

The Planning Proposal seeks to amend the existing planning controls for the Site to enable the following:

- Maximum building height for the Site to RL 200.2.
- Maximum floor space provision for the Site of 70,000sqm of GFA.
- Require future development for new buildings to demonstrate Design Excellence by way of either a:
 - Competitive design process in accordance with the City of Sydney's Competitive Design Policy; or

- Design excellence process that has been agreed with the NSW Government Architect.
- Apply the B8 Metropolitan Centre zone to the entire Western Gateway sub-precinct; including the 281sqm of land currently zoned SP2 Infrastructure which forms part of the Site (air-rights only).
- Remove the Western Gateway sub-precinct area from the Special Character Areas Map.
- Identify the Western Gateway sub-precinct on the Locality and Site Identification Map Foreshore Building Line Map – including the labelling of Blocks A, B & C within the sub-precinct.
- Apply overshadowing controls for Prince Alfred Park from 10am to 2pm.

2.3. COMPETITIVE DESIGN PROCESS

A Design Excellence Strategy (**DES**) was prepared by the Applicant and endorsed by the NSW Government Architect in October 2019. It outlined that:

“accordance with Section 1.2(2) of the City of Sydney Competitive Design Policy 2013, the Proponent will undertake the following Design Excellence Strategy for the redevelopment of the site:

- *Undertake an Invited EOI Architectural Design Competition for the site that will inform the future SSD Development Application (DA).*
- *The Architectural Design Competition Brief for the site is to be developed in accordance with the following principles:*
 - *Following an EOI process the Proponent will invite five (5) Competitors to participate in the proposed competitive process.*
 - *Selection of a range of architectural practices (both international and local) to participate in the Architectural Design Competition; and*

- *Require that each Competitor will be a person, corporation or firm registered as an architect in accordance with the NSW Architects Act 2003 or, in the case of interstate or overseas competitors, eligible for registration with their equivalent association.*

The Architectural Design Competition commenced following the release of the draft planning controls for the Western Gateway Sub-precinct and is being run in accordance with the DES. The Architectural Design Competition is currently underway, with final designs to be presented in mid-December 2019.

As the Architectural Design Competition has been undertaken based on draft planning controls, the outcomes of the Competition will need to respond to the finalised planning controls for the Site and any changes relating to surrounding infrastructure.

3. SITE AND LOCALITY

3.1. SITE LOCATION

The Site is known as 8-10 Lee Street, Haymarket. It is an irregular shaped allotment. The allotment has a small street frontage to Lee Street, however this frontage is limited to the width of the access handle. The Site has an area of 3,486m².

Figure 1 – Site Location and Dimensions



Image Source: EC3 with Terroir

3.2. SITE AND SURROUNDS

The Site is directly adjacent to the Western Wing Extension of Central Station, and forms part of the 'Western Gateway Sub-precinct' of the Central Railway Station lands. It is situated between the existing CountryLink and Intercity railway platforms to the east and the Adina Hotel (former Parcel Post Office) to the west.

Existing vehicle access to the Site is via Lee Street, however the Lee Street frontage of the Site is only the width of the access handle.

Current improvements on the Site include the Parcels Shed, which operated in association with the former Parcels Post Office (now the Adina Hotel). The Site is currently used as the Railway Square YHA. The Site also includes the western entryway to the Devonshire Street Pedestrian, which runs east-west through Central Station under the existing railway lines.

The Site is situated in one of the most well-connected locations in Sydney. It is directly adjacent to Central Station Railway which provides rail connections across metropolitan Sydney, as well as regional and interstate connections and a direct rail link to Sydney Airport. The Site is also within close proximity to several educational institutes and is a city fringe location which provides access to key support services.

Central Railway Station is currently undergoing rapid transformation to allow for integration of rail, metro and light rail transport infrastructure. This will elevate the role of Central Station not only for transport but also enhance opportunities for urban renewal and revitalisation of the surrounding precinct. This is one of the key drivers for the identification of the Central SSP and the Western Gateway Sub-precinct to accommodate a new innovation and technology precinct.

The proximity of the Western Gateway Sub-precinct to the city, while still being located outside the core Sydney CBD, provides opportunity for it to evolve to attract technology and innovation companies. It has access to all required services while being sufficiently separate to the CBD to establish a distinct technology industry ecosystem. Its CBD fringe location will provide affordable commercial rents which will support Startups and entrepreneurs which are a key component of an innovation precinct.

3.3. LEGAL DESCRIPTION

The Site is located within the City of Sydney local government area. The Site comprises multiple parcels of land which exist at various strata levels. The legal description of these allotments and their locations are described below. All the following lots are in the freehold ownership of Transport for NSW, with differing lease arrangements to individual parties within the precinct.

- **Lot 116 in DP 1078271:** YHA is currently the long-term leaseholder of the Site which covers the areas shown in blue below.
- **Lot 117 in DP 1078271:** This is currently in the ownership of TNSW and the applicant is seeking the transfer of the leasehold on this land to provide for an optimise basement and servicing outcome for the Site.
- **Lot 118 in DP 1078271:** This is currently in the ownership of TNSW and the applicant is seeking the transfer of the leasehold for part of the air-rights above part of this allotment to allow for an optimised building envelope for the project.
- **Lot 13 in DP 1062447:** This is currently in the ownership of TNSW but TOGA (who hold the lease for the Adina Hotel) have a long-term lease of this space in the lower ground area.

Figure 2 – Land Ownership and Air Rights



Picture 1 – Upper Ground and Above



Picture 2 – Lower Ground and Below



Picture 3 – Air Rights

- Lot 13
- Lot 116
- Lot 117
- Lot 118
- Site Boundary

4. PROPOSED DEVELOPMENT

4.1. OVERVIEW

The proposed SSDA will facilitate the development of a new mixed-use development comprising ‘*tourist and visitor accommodation*’ (in the form of a ‘*backpackers*’) and commercial office space within the tower form with retail, lobby and food and drink premises at the Lower Ground level and Upper Ground level. The new building will be purpose built to accommodate the new Atlassian Headquarters and new Railway Square YHA backpacker’s accommodation. In addition, there will be additional commercial floorspace to support Tech Start-ups.

As the first project to be realised in the Sydney Innovation and Technology Precinct at Central, the new Atlassian Headquarters will present itself as a catalyst and anchor for future developments by creating and defining a holistic vision for the public realm, reinforce the Site and its adjacent public spaces as the “Gateway” to the new Technology Precinct, (in addition to fulfilling the expectation of future transport and over station development requirements of Transport for New South Wales).

The proposed development is the subject of an Architectural Design Competition which is currently underway, and as such full details of the proposal are yet to be confirmed. The Reference Scheme, which forms part of the Architectural Design Competition Brief identifies the key guiding design principles for the project as illustrated in **Figure 1** below.

Figure 3 – Key Guiding Design Principles of the Architectural Design Competition



4.2. REFERENCE SCHEME

Atlassian has worked closely with an expert project team with both local and international experience in creating very high quality, iconic, innovative buildings and technology/innovation districts which push the boundaries on sustainability, workplace design, placemaking and design excellence.

The project team have collaborated to create a ‘Reference Design’ and building envelope that assists achieve the project objectives and TNSW with the creation of a refined strategic planning and built form framework for the Western Gateway Sub-precinct.

The Reference Design has been prepared to inform the building envelope which is proposed as part of the Planning Proposal for the Western Gateway Sub-precinct and forms part of the Architectural Design Competition Brief. It is the result of significant engagement with various government agencies, neighbouring Proponents, the Central Design Review Panel and ultimately provides the unique opportunity to create Australia’s first global ‘Innovation Precinct’ at Central Station. In addition, it responds to the Draft Design Guideline for the Western Gateway Sub-precinct which has been prepared by TNSW which is also currently on public exhibition.

The Reference Scheme includes indicative details of:

- Land use programming across the building (**Figure 4**).
- Building Height principles (**Figure 5**).
- Tower Setbacks (**Figure 6**).
- Vehicle access, servicing and parking arrangements (**Figure 7**).

Figure 4 – Proposed Land Use programming within the building (indicative)

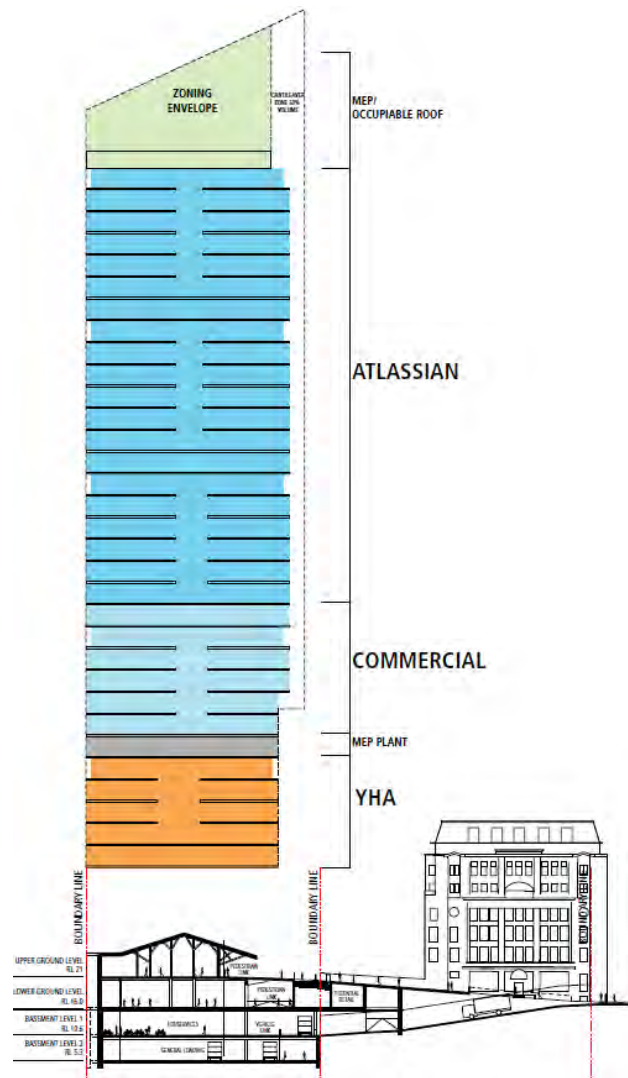


Image Source: EC3 with Terroir

Figure 5 – Proposed Building Envelope Heights

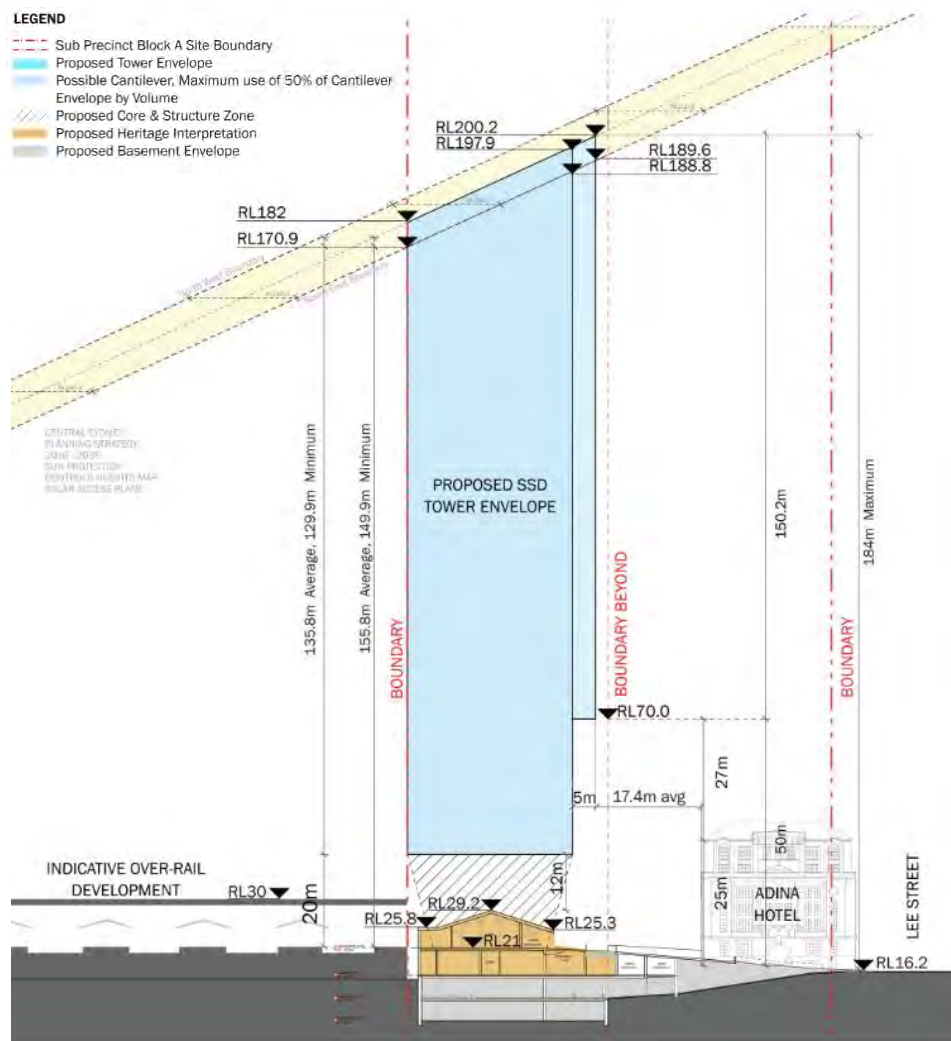


Image Source: EC3 with Terroir

In response to the Central Design Review Panel process which was undertaken in the preparation of the Western Gateway Sub-precinct Planning Proposal, building setbacks and design principles have been adopted into the Western Gateway Design Guideline and the Reference Design which include the tower setback controls illustrated in **Figure 6** below. These controls include cantilever zones on the southern and western sides of the building to allow flexibility within the Architectural Design Competition.

Figure 6 – Tower Setbacks



Image Source: EC3 with Terroir

The proposed development will generally be car free, with no parking provided for the commercial office component of the development. However, vehicle access will still be required to the Site for service and loading vehicles. A temporary solution has been identified to enable the development to operate independent of the broader Western Gateway Sub-precinct development, which also offers flexibility for a more longer-term solution which will support an integrated-basement' for the whole Western Gateway Sub-precinct.

Figure 7 – Potential Vehicle Access Arrangements



Image Source: Arup

5. PLANNING FRAMEWORK

5.1. STATUTORY PLANNING POLICIES

The relevant statutory planning policies that apply to the proposed development are:

- State Environmental Planning Policy (State and Regional Development) 2011 (**SEPP SRD**).
- State Environmental Planning Policy (Infrastructure) 2007 (**SEPP Infrastructure**).
- State Environmental Planning Policy No.55 - Remediation of Land (**SEPP 55**).
- Sydney Regional Environmental Plan (Sydney Harbour Catchment) 2005 (**SREP SH**).
- Sydney Local Environmental Plan 2012 (**Sydney LEP 2012**) including proposed amendments
- Draft Design Guide - Western Gateway Sub-precinct (**Draft Design Guide**).

5.2. APPROVAL PATHWAY OVERVIEW

An amendment to *Sydney Local Environmental Plan 2012* has been prepared and is currently on public exhibition which supports the Reference Design and the building envelope which has been adopted for the Architectural Design Brief. An overview of the planning approval pathway for the SSDA is summarised in **Figure 8** below.

Figure 8 – Overview of Planning Approval Pathway



Source: Urbis

5.3. STATE ENVIRONMENTAL PLANNING POLICY (STATE AND REGIONAL DEVELOPMENT) 2011

Clause 13(2) of Schedule 1 of the SRD SEPP identifies development with a CIV of more than \$10 million for the purpose of 'other tourist related purposes' located 'in an environmentally sensitive area of State significance' is declared SSD.

It is noted that the CIV for the 'tourist and visitor accommodation' (the YHA) component of the development will be greater than \$10 million and the Site is occupied by the Parcels Shed which is forms part of the State Heritage listing for Central Station.

The development is proposed to accommodate a new Railway Square YHA and Headquarters for Atlassian. The establishment of the Atlassian headquarters at

Central Station is to anchor the creation of a new technology and innovation precinct which attract and retail global talent in this industry.

The commercial office component of the project is sufficiently, intrinsically and inextricably related to the adaptive re-use of the State Listed Heritage Item and tourist (backpacker's) component of the project as:

- The project comprises of adaptive re-use works to the Parcels Shed State Heritage Listed item and the built form of the commercial tower will be physically integrated with shared lobbies, interaction spaces and pedestrian linkages at the ground and lower-ground levels. The circulation and services are intertwined with common lifts and lobbies, use of the forecourt and other major services and facilities. The Design Competition Brief for the project promotes the retention and adaptive re-use of the Parcels Shed and encourages innovative design solutions to integrate the YHA and commercial office components of the building in a harmonious manner. Both Atlassian and YHA have been involved in the preparation of the Architectural Design Competition Brief to ensure that appropriate interaction occurs. The successful architect for the project will work on both the YHA and Atlassian components and will not be separated in terms of their design approach for each party.
- YHA are the existing lease holder for the Site, where they currently operate the Railway Square YHA within the Parcels Shed. The delivery of the commercial component of the development would not be achievable without the joint commitment of the YHA and Atlassian to adaptively reuse the Parcels Shed, providing improved facilities for both parties as well as enhanced heritage conservation outcomes.
- Atlassian and YHA are working in collaboration to provide an integrated building 'ecosystem' with the YHA backpackers component providing easy, affordable, on-site tourist accommodation. The nature of technology and Start-up firms is transitioning from a 9-5 workday, to a much more flexible and 24-7 workspace given the connectivity required with other global businesses and offices in different time zones. Atlassian and other Start-ups in the building will therefore benefit from the co-location with YHA which operates beyond the normal working hours of a typical office, and will be activated with other uses in the precinct (i.e. such as the food and

beverage, retail and similar uses that enhance the day and night economies.

- Technology and innovation companies are globally connected, with several international business travellers frequenting the Atlassian headquarters on a daily basis, often in need of low-cost accommodation in close proximity to Atlassian and the innovation precinct.
- The tourist accommodation is an essential component of a 'campus' style technology and innovation precinct, not dissimilar to universities and education facilities which provide on-site accommodation.
- The composition of land uses which the development of a '*tourist and visitor accommodation*' and commercial building with associated retail uses will bring 24/7 activation for the Western Gateway Sub-precinct.
- The development of the mixed-use building will enable the delivery of enhanced heritage conservation and restoration works for the Parcels Shed and increase public access to the building.

It is requested that the Secretary, under Clause 8(2) of SEPP SRD, declares the non-tourist facility component ('*commercial premises*') of the proposal to be sufficiently related to the SSD component of the development and therefore also SSD and subject to the SEARs.

5.4. STATE ENVIRONMENTAL PLANNING POLICY (INFRASTRUCTURE) 2007

SEPP Infrastructure aims to facilitate the effective delivery of infrastructure across NSW by identifying matters to be considered in the assessment of development adjacent to certain types of infrastructure.

In particular, the proposed development will need to consider the application of Clause 86 - *Excavation in, above or adjacent to rail corridors*.

Clause 86 will be addressed as part of the EIS and technical documentation submitted in support of the SSDA. Consultation with the relevant authorities has been ongoing and will continue throughout the SSDA process.

5.5. STATE ENVIRONMENTAL PLANNING POLICY NO 55 - REMEDIATION OF LAND

SEPP 55 promotes the remediation of contaminated land to reduce the risk of harm to human health and the environment.

A Preliminary Site Investigation (**PSI**) was preparation for the Planning Proposal for the Site. The PSI identified that contaminants may be present on the Site due to the land use history, however considered the risk to human and environmental receptors to be low.

Accordingly, the EIS will be supported by a Detailed Site Investigation and Hazardous Materials Assessment.

5.6. SYDNEY LOCAL ENVIRONMENTAL PLAN 2012

The principle environmental plan is the Sydney LEP 2012. The key planning controls and development standards under the current Sydney LEP 2012 which apply to the Site are:

- **Land Use Zoning:** B8 Metropolitan Centre, apart from the part of the Site which enjoys air-rights over Lot 118 DP1078271 which is currently zoned SP2 Railway Infrastructure.
- **Maximum Building Height:** 7.5 metres
- **Maximum Floor Space Ratio:** 3:1

As detailed in **Section 2.2**, a Planning Proposal is currently on public exhibition which proposes to amend the Sydney LEP 2012 controls as they apply to the Western Gateway Sub-precinct, which include the following key proposed controls for the Site:

- Apply the B8 Metropolitan Centre zone to the entire Western Gateway sub-precinct; including the 281sqm of land currently zoned SP2 Infrastructure which forms part of the Site (air-rights only).
- Maximum building height for the Site to RL 200.2.

- Maximum floor space provision for the Site of 70,000sqm of GFA

While these controls are draft and subject to change, they have been adopted in the Architectural Design Competition Brief, which provides flexibility for design development of the winning scheme to respond to the finalised planning controls.

5.7. DRAFT DESIGN GUIDE - WESTERN GATEWAY SUB-PRECINCT

As part of the Western Gateway Sub-precinct Planning Proposal, a draft Design Guide for the Western Gateway Sub-precinct (**Draft Design Guide**) has been prepared by TNSW and forms part of the Planning Proposal exhibited documentation. The Draft Design Guide is to supplement the provisions of the Sydney LEP 2012 by providing more detailed provisions to guide development on land within the Western Gateway Sub-precinct.

The Draft Design Guide forms part of the Architectural Competition Brief to provide built form design guidance for Competitors and will also form part of the assessment of the final design of the development and EIS documentation.

5.8. STRATEGIC PLANNING FRAMEWORK

The following strategic planning documents apply to the Site and will be considered in the preparation of the EIS:

- A Metropolis of Three Cities - The Greater Sydney Regional Plan
- Eastern City District Plan
- Draft Central Sydney Planning Strategy
- Sustainable Sydney 2030 Strategy
- NSW State Infrastructure Strategy 2018-2036
- Camperdown - Ultimo Collaboration Area and Place Strategy
- Central to Eveleigh Urban Transformation Strategy

- Better Placed
- Sydney Green Grid
- City of Sydney Tech Startups Action Plan
- Draft Sydney Local Strategic Planning Statement

6. LIKELY IMPACTS

6.1. LAND USE

The Site is currently zoned B8 Metropolitan Zone, except for a small part of Lot 118 in DP 1078271 which is currently zoned SP2 Railway Infrastructure which is proposed to be rezoned B8 Metropolitan Zone as part of the Western Gateway Sub-precinct Planning Proposal.

The proposed development is permissible within the B8 Metropolitan Zone and is consistent with the zone objectives which will be detailed in the EIS and support documentation.

6.2. BUILT FORM AND URBAN DESIGN

As stated above, the Site is the subject of a Planning Proposal that is currently on public exhibition. The Planning Proposal seeks a maximum height for the Site of RL201.2 and maximum GFA of 70,000sqm.

The built form and urban design outcomes for the Site are the subject of an Architectural Design Competition which is currently underway. While the design has not been resolved, the Reference Design and building envelope presented as part of the Planning Proposal and detailed in **Section 4.2** of this report, have been incorporated into the Architectural Competition Brief.

It is noted that the Urban Design Analysis which informed the Planning Proposal and has been incorporated into the Architectural Design Competition Brief has considered a wide range of matters both within and surrounding the Site, which include:

- The future vision and character of Railway Square espoused within the Central Sydney Planning Strategy;
- The current and planned upgrades of Central Station and how this relates specifically to the Site;
- The strategic context of the Site such as site ownership, topography, street elevations, dominant street grids, precincts adjacent to the Site, character

and heritage context, the broader Western Gateway Sub-precinct, and an examination of the open parks and spaces in the local area.

- A strong examination of the heritage principles that will need to guide the proposed development in close consultation with Heritage Consultants, Weir Phillips Heritage.
- Principles which have guided the development of a building envelope for the Site, and an analysis of shadow and visual impacts on the public domain.
- Provision of a reference design to principally guide site-specific issues such as heritage, vehicular access and the ground plane.

The EIS and supporting documentation will consider the finalised building design, including consideration of the design against each of these issues.

6.3. PUBLIC DOMAIN

The Draft Design Guide includes details of the main challenges of the Western Gateway Sub-precinct to deliver an activated and functional public domain while mitigating the impacts of future tower development.

The Site includes a Link Zone which will support pedestrian movements both east-west along Ambulance Avenue and north-south to connect the Western Concourse to Henry Deane Plaza and beyond.

The public domain design response will also consider how the development can manage the interface with the future 'Third Square' of Sydney at Railway Square to the north of the Site, and contribute to the successful activation of this area.

Details of the public domain works on the Site, and how it will contribute to achieving the public domain objectives of the Western Gateway Sub-precinct and the broader public domain objectives of the Central Station will be provided in the EIS and support documentation.

6.4. PEDESTRIAN ACCESS

The Reference Design prepared by EC3 with Terroir has considered how public domain within the Western Gateway Sub-precinct could be enhanced, including the identification of 'Link Zones' which can support anticipated future pedestrian movement lines. The Reference Design provides opportunity for improved pedestrian connectivity and permeability in the precinct, specifically:

- Connections from lower ground to upper ground to facilitate access to the future building on the Site.
- Through-site connections to the southern adjoining site (Henry Deane Centre) and enhanced public domain at Henry Deane Plaza.

The EIS and support documentation will provide details of pedestrian access through and surrounding the Site, including details of how the development will connect pedestrian flows to the potential future over-rail development and western egress from Central Walk.

6.5. SOLAR ACCESS AND OVERSHADOWING

The proposed height and floor space controls for the Site under the Planning Proposal currently on public exhibition have been guided by the requirement to preserve solar access in Prince Alfred Park and protect the park from additional overshadowing.

Under the Sydney LEP 2012, the current solar access control is:

Clause 6.19 - Overshadowing of certain public places

- (1) *Despite clause 4.3, development consent must not be granted to development that results in any part of a building causing additional overshadowing, at any time between 14 April and 31 August in any year, of any of the following locations (as shown with blue hatching on the Sun Access Protection Map) during the times specified in relation to those locations—*

- (h) *Prince Alfred Park (beyond the shadow that would be cast by a wall with a 20 metre frontage height on the boundary between the park and the railway land)—between 12.00–14.00,*

Under the *draft Central Sydney Planning Strategy (CSPS)*, an amended Sun Access Plane is proposed:

Prince Alfred Park:

Should be replaced with a Sun Access Plane constructed under the same principles, but with an extended period of protection (10am-2pm).

The Architectural Design Brief has included a building envelope which will not breach the existing sun access control under the Sydney LEP 2012 or proposed under the CSPS.

Solar access diagrams will be prepared as part of the EIS documentation.

6.6. AERONAUTICAL

The Obstacle Limitation Surface (**OLS**) across the Site is ~144m AHD, however the Radar Terrain Clearance Chart (**RTCC**) is 244m AHD, and the PANS-OPS Approach Surfaces is 263m+ AHD.

The building height of RL200.2 will not penetrate the RTCC or the PANS-OPS. However, it is noted that the building envelope exceed the OLS. Accordingly, the SSDA will require an 'airspace height' approval from the Commonwealth Department of infrastructure and Regional Development under the *Airports Protection of Airspace Regulations (APAR)*. It is noted that the RTCC surface height is more restrictive than the most limiting PANS-OPS surfaces and therefore the RTCC effectively is the maximum height for a building envelope on the Site.

An Aeronautical Study will form part of the EIS supporting documentation.

6.7. HERITAGE

As discussed above, the Parcels Shed forms part of the Central Railway Station Group which is a State Heritage listed item.

The heritage significance of the Parcels Shed has been a key consideration in the development of the height and GFA controls proposed in the Western Gateway Sub-precinct Planning Proposal. The future redevelopment of the Site seeks to retain the Parcels Shed, including the removal of a contemporary fitout to reinstate its original scale and form of the shed structure.

The Architectural Design Competition Brief requires the adaptive re-use of the Parcels Shed and the EIS will be accompanied by a Heritage Impact Statement which details the impacts and mitigate measures proposed to support the design.

6.8. CULTURAL HERITAGE

An Aboriginal Due Diligence and Non-Aboriginal (Historic) Archaeological Assessment (**Archaeological Assessment**) has been prepared for the Site. As the envelope on Lot 118 in DP 1078271 only applies to the air rights above this land, the Archaeological Assessment does not include Lot 118 in DP 1078271.

The Archaeological Assessment identifies that the Site is located within the traditional lands of the Gadigal clan. Due to their hunter-gatherer culture, it is likely that they moved across the landscape between resources, however it is difficult to ascertain whether any campsites existed at a given location. It is known that following a smallpox epidemic in 1789, only three members of the 60-strong Gadigal clan survived.

The Archaeological Assessment will be submitted as part of EIS documentation.

6.9. DESIGN EXCELLENCE

As detailed in **Section 2.3** above, the Applicant has prepared a Design Excellence Strategy (**DES**) in accordance with the City of Sydney's Design Excellence Guidelines. The DES sets out:

- The location and extent of the competitive design process;

- The type of competitive design process to be undertaken;
- The number of Competitors involved in the process; and
- Method of maintaining design integrity.

In accordance with the DES, an Architectural Design Competition commenced in October 2019 and is currently underway in accordance with the Architectural Design Competition Brief which was endorsed by the Government Architect of NSW.

The final design submitted with the EIS will be the winning design from the Architectural Design Competition and design development post-competition will be undertaken in accordance with the DES and the finalised planning controls.

6.10. VISUAL ANALYSIS

A View Impact Analysis (**VIA**) will be prepared as part of the EIS to assess the impact of the proposed development from key vantage points within the City and surrounding area.

The VIA will adopt the key views identified in the CSPS as well as views across the Site to key surrounding heritage items including:

- Views of the former Parcels Post Office (now the Adina Hotel), through the inclusion of a setback of the tower on the Site by two-times the height of the Former Parcels Post Office building;
- Views to a potential future over-station concourse which will align with the southern face of the former Parcels Post Office building; and
- Views of the former Inwards Parcels Shed from Railway Square and Ambulance Avenue.

6.11. WIND ASSESSMENT

A Wind Assessment will be undertaken as part of the EIS documentation. The Wind Assessment will focus on the following key locations:

- The Ground Level pedestrian accessible areas within the site boundary.

- The Henry Deane Plaza area and the Devonshire Street Tunnel entrance.
- The pedestrian footpath area along Railway Colonnade Drive and Ambulance Avenue.
- Elevated terrace areas and connecting bridges

6.12. TRANSPORT AND ACCESS

As detailed in **Section 4** above, it is proposed that the development generally be car free, with no parking proposed for the commercial office component of the development.

A Transport and Accessibility Impact Assessment Report (**Transport Report**) will be prepared as part of the EIS. The Transport Report will analyse parking requirements, existing and expected traffic impacts on the local road network and the design of proposed vehicular access points including waste and service vehicle access, including consideration of the identified temporary and long-term access arrangement options.

The Transport Report will also consider pedestrian movements through the Site and key pedestrian movement paths through the broader Central Precinct as discussed in **Section 6.4** above.

6.13. SUSTAINABILITY

Atlassian is committed to setting and meeting sustainability aspirations for the development. Atlassian's CEOs are vocal supporters of sustainable energy and development and are true believers in these principles. Atlassian has included sustainability criteria for this building within the Architectural Design Competition Brief and is committed to achieving sustainability excellence.

A Sustainability Assessment will form part of the EIS documentation which will outline the sustainability benefits of the proposed development.

6.14. SITE CONDITIONS

A Preliminary Site Investigation (**PSI**) and a Geotechnical Desktop Study and Risk Assessment (**Geotechnical Study**) have been prepared to determine the suitability of Site for the proposed use.

Both the PSI and the Geotechnical Study conclude that construction of a building within the building envelope contained in the Western Gateway Planning Proposal is feasible and that no suit conditions exist which would prevent future development.

The EIS will be accompanied by an updated PSI and Geotechnical Study which will consider the suitability of the Site to support the final building design.

6.15. SOCIAL AND ECONOMIC EFFECTS

The delivery of a new building for the Atlassian Headquarters will be a catalytic project to support the development of the new Sydney Innovation and Technology Precinct within the Central to Eveleigh Corridor. As the catalytic project for this new precinct, the development will have significant social and economic significance at a local, state and national level. Globally, innovation and technology advancements are occurring at rapid speed, and this is changing the types of jobs required to sustain Australia's economic growth.

An Economic Benefits Report and Office Markets Assessment will be prepared and submitted as part of the EIS documentation, which will detail the key social and economic benefits of the proposed development.

6.16. BUILDING CODE OF AUSTRALIA AND ACCESS

The proposed development will be designed in accordance with the requirements of the Building Code of Australia and will provide legible, safe and inclusive access for all. This will be addressed within a Building Code of Australia and Accessibility Report which will form part of the EIS documentation.

6.17. WASTE MANAGEMENT

A Construction and Demolition Waste Management Plan (**Waste Plan**) will be prepared and accompany the EIS. The Waste Plan will detail all likely waste streams to be generated during demolition and construction and outline proposed measures to dispose of the waste offsite.

A Preliminary Operational Waste Management Plan will also be submitted as part of the EIS and detail proposed waste servicing arrangements, loading zones and ongoing waste management practices to be employed at the site. All demolition, construction and operational waste will be reused or recycled where possible.

6.18. CONSTRUCTION MANAGEMENT

A Preliminary Construction and Environmental Management Plan (**CMP**) will be prepared and provided as part of the EIS. The CMP will detail:

- Timing of construction works to be undertaken;
- Construction hours of operation and programme;
- Materials handling strategy;
- Construction traffic, noise, soil erosion, dust control and stormwater management;
- Environmental management strategies during construction; and
- Waste management.

A Construction Pedestrian and Traffic Management Plan will also be prepared as part of the EIS documentation which will outline proposed traffic control plans and truck routes during construction phase of the works.

6.19. SAFETY AND SECURITY

The EIS will be accompanied by a Crime Prevention Through Environmental Design Report (**CPTED**) that will outline how the principles (surveillance, access

control, territorial re-enforcement and space/activity management) have been integrated into the design of the development to deter crime, manage space and create a safe environment for all who interact with the building, integrated public domain, and its uses.

6.20. CONSULTATION

In addition to the active role in developing the new Sydney Innovation and Technology Precinct and the NSW Industry Taskforce, Atlassian has been closely consulting with a range of government agencies to progress the adaptive re-use of the Parcels Shed and development of their new headquarters on the Site. The proposal and consultation have been informed by a range of technical and supporting studies that accompany this report.

A summary of the key stakeholder consultation undertaken to date is provided below.

Agency	Key Discussion Items
<i>NSW Department of Premier & Cabinet</i>	<ul style="list-style-type: none">• Ongoing discussions on the Innovation Precinct• Partnership on the NSW Tech Taskforce
<i>Transport for NSW</i>	<ul style="list-style-type: none">• Land ownership/leaseholder discussions• Planning approval pathway discussions• Ongoing briefings on project progression• Sharing of information in relation to Central Station Infrastructure upgrades• Partnership on the NSW Tech Industry Taskforce

Agency	Key Discussion Items
NSW Department of Planning Industry & Environment	<ul style="list-style-type: none"> • Planning approval pathway options • State and regional significance and the 'uniqueness' of the proposal • Briefings with the applicant's project team and the NSW Tech Industry Taskforce
NSW Office of Environment & Heritage	<ul style="list-style-type: none"> • Briefing on the heritage aspects of the proposal with the applicant's architect and heritage consultant • Engagement on draft Heritage Impact Statement and Conservation Principles
NSW Office of Government Architect	<ul style="list-style-type: none"> • Briefing on draft Design Excellence Strategy • Early discussions on design competition process
City of Sydney Council	<ul style="list-style-type: none"> • Regular engagement with Council's Chief Executive Officer regarding the Tech Taskforce • Briefing on the proposal with senior planning officers • Early discussions on design competition process
Jobs NSW	<ul style="list-style-type: none"> • Engagement on the curation of the future innovation and technology precinct.
Central Design Review Panel	<ul style="list-style-type: none"> • Three (3) formal presentations by the proponent's project team to the Central Precinct Design Review Panel (CDRP) which included representatives of State and Local government. See discussion below.

Agency	Key Discussion Items
Tech Taskforce	<ul style="list-style-type: none"> • Key Industry representative on the NSW Government's Tech Taskforce which included key innovation and support industry representatives, as well as state and local government agencies and departments.

Further consultation will be undertaken with key government agencies and stakeholders during the preparation of the EIS and support documentation.

7. CONCLUSION

This report provides support to the Request for SEARs for the development of a new mixed-use development comprising the new Railway Square YHA *'tourist and visitor accommodation'* (*'backpackers'*) and Atlassian Headquarters at 8-10 Lee Street, Haymarket. The proposal will be the catalytic development for the new Sydney Innovation and Technology Precinct being developed within the Central to Eveleigh corridor, and will include office space to support technology and innovation Start-ups.

This Request for SEARs seeks a declaration that the proposed development is SSD on the basis that it falls within the requirements of Clause 13(2) of Schedule 1 of the *State Environmental Planning Policy (State and Regional Development) 2011* (SEPP SRD) being *'development for other tourist related purposes...that has a capital investment value of more than \$10 million and is located in an environmentally sensitive area of State significance'*. The Site is occupied by the Parcels Shed which is forms part of the State Heritage listing for Central Station.

It is noted that the CIV for the tourist and visitor accommodation component of the development will be greater than \$10 million and the Site is occupied by the Parcels Shed which is forms part of the State Heritage listing for Central Station, which is land defined as being *'an environmentally sensitive area of State significance'* in Clause 4(1) of the SRD SEPP.

The *'tourist and visitor accommodation'* and commercial office components of the building are sufficiently and inextricably related as will support the operations of one another and are being collaboratively designed and delivered with the adaptive reuse of the Parcels Shed.

On this basis, SEARs are requested for the preparation of an SSDA which will be accompanied by an EIS. The EIS will undertake a comprehensive assessment of the potential environmental impacts of the proposed development and will identify mitigation measures where required. The EIS will also include details of design, construction, fitout and operation of the development within the building envelope proposed in the Western Gateway Sub-precinct Planning Proposal currently on public exhibition.

All relevant impacts of the proposal will be assessed in the EIS, as guided by the SEARs.

DISCLAIMER

This report is dated 13 November 2019 and incorporates information and events up to that date only and excludes any information arising, or event occurring, after that date which may affect the validity of Urbis Pty Ltd's (**Urbis**) opinion in this report. Urbis prepared this report on the instructions, and for the benefit only, of Atlassian Pty Ltd (**Instructing Party**) for the purpose of Request for SEARs (**Purpose**) and not for any other purpose or use. To the extent permitted by applicable law, Urbis expressly disclaims all liability, whether direct or indirect, to the Instructing Party which relies or purports to rely on this report for any purpose other than the Purpose, and to any other person which relies or purports to rely on this report for any purpose whatsoever (including the Purpose).

In preparing this report, Urbis was required to make judgements which may be affected by unforeseen future events, the likelihood and effects of which are not capable of precise assessment.

All surveys, forecasts, projections and recommendations contained in or associated with this report are made in good faith and on the basis of information supplied to Urbis at the date of this report, and upon which Urbis relied. Achievement of the projections and budgets set out in this report will depend, among other things, on the actions of others over which Urbis has no control.

In preparing this report, Urbis may rely on or refer to documents in a language other than English, which Urbis may arrange to be translated. Urbis is not responsible for the accuracy or completeness of such translations and disclaims any liability for any statement or opinion made in this report being inaccurate or incomplete arising from such translations.

Whilst Urbis has made all reasonable inquiries it believes necessary in preparing this report, it is not responsible for determining the completeness or accuracy of information provided to it. Urbis (including its officers and personnel) is not liable for any errors or omissions, including in information provided by the Instructing Party or another person or upon which Urbis relies, provided that such errors or omissions are not made by Urbis recklessly or in bad faith.

This report has been prepared with due care and diligence by Urbis and the statements and opinions given by Urbis in this report are given in good faith and

in the reasonable belief that they are correct and not misleading, subject to the limitations above.

APPENDIX A

QS STATEMENT

Atlassian Pty Ltd
C/-Avenor
Level 17, 9 Castlereagh Street
SYDNEY NSW 2000
Attention: Joseph Ravi

21 November 2019

Dear Joseph,

**ATLASSIAN SYDNEY
RAILWAY SQUARE, SYDNEY
STATE SIGNIFICANT DEVELOPMENT APPLICATION TOURISM CIV LETTER**

We have prepared a detailed Order of Cost Estimate of the Capital Investment Value (CIV) for the proposed Atlassian Development at Central Station, Sydney, in accordance with the Environmental Planning and Assessment Regulation 2000.

Our estimated CIV has been prepared in accordance with the NSW Department of Planning requirements.

WTP confirms that the components of the proposed development attributed to a tourism related use (being the YHA floors within the proposed reference scheme) have a Total Capital Investment Value greater than **\$10,000,000 (Excluding GST)**.

This estimated Capital Investment Value includes all design and construction costs together with all relevant infrastructure works, site services, plant & equipment and all anticipated labour costs.

We note that the above estimate should be considered as indicative only at this stage and that, prior to finalising any design or agreements, detailed estimates will be prepared based on further developed design information.

Should you require any further information please do not hesitate to contact us.

Yours faithfully
WT PARTNERSHIP



ANTONY GEORGE
Associate

WTP REF: 172833 Atlassian YHA_SSDA CIV Letter_2019.11.21

APPENDIX B

PRELIMINARY DRAWINGS



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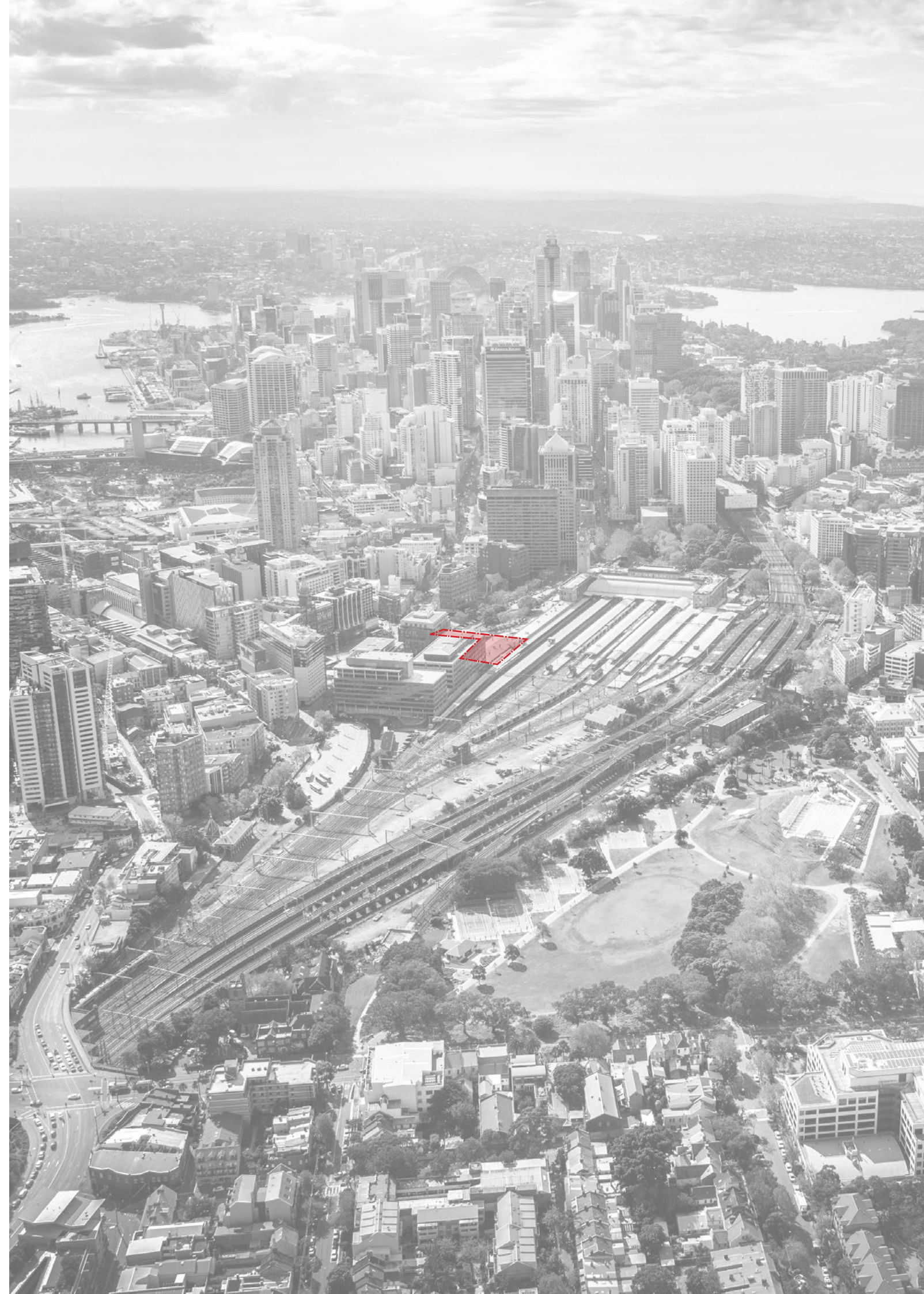
WESTERN GATEWAY SUB-PRECINCT BLOCK A

RAILWAY SQUARE
8-10 LEE STREET

APPENDIX A URBAN DESIGN PLANNING PROPOSAL REPORT

PREPARED FOR
ATLASSIAN, YHA & AVENOR

OCTOBER 2019



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CLIENTS



CONSULTANTS

PLANNING	Urbis
HERITAGE ARCHITECTURE	Weir Phillips
ARCHITECTURE	EC3 with Terroir
	Previous phase undertaken by Bates Smart
LANDSCAPE ARCHITECTURE	Aspect Studios



1.0 INTRODUCTION

VISIONS OF TECH CENTRAL & RAILWAY SQUARE



The Sydney Innovation and Technology Precinct Panel Report 2018

“The Sydney Innovation and Technology Precinct attracts world-leading talent to Sydney’s shores. It is a place where world-class universities, ambitious startups, high-tech giants and the community collaborate to solve problems, socialise and spark ideas that change our world. The Precinct is underpinned by high quality physical and digital infrastructure.

A GLOBALLY CONNECTED PRECINCT ESTABLISHES A BENCHMARK OF AMBITION FOR GENERATIONS TO COME. HELPING MAKE SYDNEY A GLOBAL LEADER IN TECHNOLOGY AND INNOVATION, ENSURING NSW REMAINS A POWERHOUSE IN THE NEW WORLD ECONOMY.

THE PRECINCT IS A LEADER WITHIN THE REGIONAL INNOVATION NETWORK , TAKING A SPIRIT OF COLLABORATION AND ENTREPRENEURIAL NOUS TO TRANSLATE AUSTRALIA’S WORLD-LEADING RESEARCH AND DEVELOPMENT (R&D) INTO GLOBALLY SUCCESSFUL BUSINESSES.

THE PRECINCT DRIVES A SUSTAINABLE AND INCLUSIVE APPROACH TO THE DEVELOPMENT AND GROWTH OF THE PEOPLE, THE INDUSTRIES AND THE COMMUNITIES WHO USE ITS PLACES AND SPACES.

BIG, BOLD AND OUT-OF-THE-BOX THINKING IS SHAPING AND INFLUENCING THE FUTURE. THE PRECINCT WILL EVOLVE AS A LIVING, BREATHING PLACE WHERE PEOPLE AND BUSINESSES LEARN, ADAPT AND GROW WITH EVERY OPPORTUNITY."



Draft Central Sydney Planning Strategy Document

"THE STRATEGY ALSO SUPPORTS THE REVITALISATION OF THE MIDTOWN AND SOUTHERN PRECINCTS. CATALYTIC PROJECTS BY BOTH THE NSW GOVERNMENT AND PRIVATE SECTOR WILL BE INSTRUMENTAL TO IMPROVING THE PROFILE AND MARKET PERCEPTION OF THESE LESS TRADITIONAL EMPLOYMENT AREAS. PROPOSED METRO STATIONS AND THEIR INTEGRATION INTO THE CITY; THE REDEVELOPMENT OF THE SPACE ABOVE THE TRAIN LINES AT CENTRAL SYDNEY RAILWAY STATION; AND PRIVATE SITE REDEVELOPMENTS, MUST ALL CONTRIBUTE TO THE GROWTH OF THIS AREA AS A FUTURE EXTENSION OF THE CBD.

.....
THE CENTRAL TO EVELEIGH PRECINCT WILL BECOME CENTRAL SYDNEY’S NEWEST PRECINCT, YOUTHFUL AND LIVELY, AND SERVING EXISTING EDUCATION, VISITOR ACCOMMODATION AND CREATIVE USES. IT WILL ALSO SERVE THOSE EMERGING AND PERHAPS YET-UNKNOWN BUSINESS MODELS AND WORKPLACES THAT ARE SEEKING A PLAYFUL AND CREATIVE POINT OF DIFFERENCE TO THE TRADITIONAL OFFICE CORE."



Image taken from Draft Central Sydney Strategy Planning Document Supporting Project Idea: Railway Square and Belmore Park

CENTRAL SYDNEY PLANNING STRATEGY

The Draft Central Sydney Planning Strategy (CSPS) by the City of Sydney, unlocks economic opportunities and investment in jobs and supports public improvements that make Sydney an attractive place for business, workers, residents and visitors. The CSPS outlines 10 key moves which prioritize employment growth, increase capacity and ensure infrastructure keeps pace with growth, creating a more sustainable and vibrant public spaces. The CSPS is a 20 year growth strategy that revises previous planning controls and delivers on the City of Sydney’s Sustainable Sydney 2030.

The CSPS, together with the guidelines for amending planning controls, will deliver 2.9 million sqm of new floor space in predominantly in four new tower clusters. The southern most of these relates to the Central Station renewal area created by the NSW government and referenced in the Sydney Innovation and Technology Precinct Panel Report.



Draft Central Sydney Planning Strategy Document



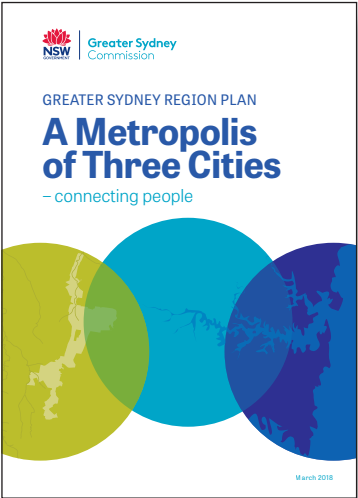
Images taken from Draft Central Sydney Strategy Planning Document

STRATEGIC URBAN CONTEXT

The site adjoins Central Station, the most connected station in NSW serving as an interchange between the new Sydney Metro, new CBD and South East Light Rail, and existing suburban, intercity and interstate train services.

The station sits within the Eastern City District of Sydney’s CBD. This district along with the Harbour CBD are the current engine room of Greater Sydney’s economy. Of the District’s 904,500 jobs, 496,100 are in the Harbour CBD (2016 estimates). Almost half of these are in the finance, professional, business and knowledge sectors.

The Eastern Harbour City has Australia’s global gateway and financial capital, the Harbour CBD, as its metropolitan centre. Well-established, well serviced and highly accessible by its radial rail network, it has half a million jobs. The regional and district plans anticipate the ongoing strategic role and sustained jobs growth within the Eastern Harbour City over the next 20 years.



Greater Sydney Region Plan



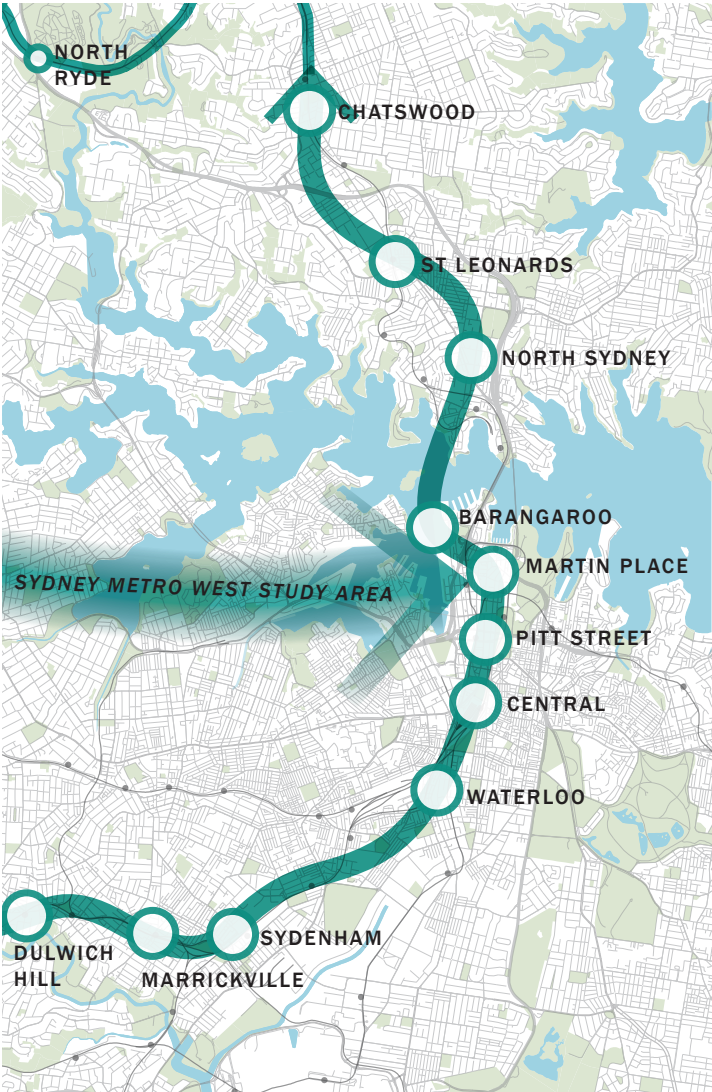
CENTRAL STATION REVITALISATION

METRO

There are significant rail projects underway in the Eastern Harbour City to increase competitiveness, boost business-to-business connections and attract skilled workers with faster commuting times.

The new Sydney Metro link connects the Northwest Rail line from Chatswood to the Sydney Metro South West line at Sydenham in the South. Future connection to the planned Metro West line will also significantly change commute times to and from Greater Parramatta.

The Sydney Metro Line will interchange at Central with the existing Inner West light rail and the new CBD and South East Light Rail line. Revitalisation works on Central Station to improve connectivity between trains, buses, light rail and the new Metro will include a new 19m wide underground concourse (Central Walk). Additional access points to Train Platforms 16 to 23 boost capacity to cater to the projected 66% increase in daily customer demand.



New Sydney Metro Alignment



Artists Impression of New Central Station Concourse

APPENDIX A - URBAN DESIGN PLANNING REPORT

1.0 INTRODUCTION

SYDNEY LIGHT RAIL

The CBD and South East Light Rail is a new light rail network for Sydney, currently under construction. The 12 km route, currently under construction will include 19 stops, extending from Circular Quay in the North to Randwick and Kensington in the South. This new service will connect the major city spine of George Street all the way from Circular Quay along with major open spaces around Moore Park and the Randwick Health & Education Super Precinct with Central Station.



New CBD and South East Light Rail Alignment



Artists Impression of New Central Station Light Rail Platform

APPENDIX A - URBAN DESIGN PLANNING REPORT

1.0 INTRODUCTION

THE CENTRE OF A NEW TECHNOLOGY NETWORK

Central to Eveleigh - an ideal location

"Further development of this attractive and vibrant area begins with the transformation of underutilised heritage assets, creating new public spaces, better transport infrastructure, and improved walking and cycling connections.

THE SYDNEY INNOVATION AND TECHNOLOGY PRECINCT IS LOCATED SOUTH OF THE SYDNEY CBD, SURROUNDED BY THE SUBURBS OF REDFERN, ULTIMO, HAYMARKET, CAMPERDOWN, CHIPPENDALE, DARLINGTON, SURRY HILLS AND EVELEIGH. THESE SUBURBS ARE PLACES OF GREAT HERITAGE, VIBRANCY, CULTURE AND ACTIVITY, SUPPORTED BY EXCELLENT TRANSPORT TO THE GREATER SYDNEY AREA, REGIONAL NSW AND INTO INTERNATIONAL MARKETS.

ALONGSIDE THE AREA IS AUSTRALIAN TECHNOLOGY PARK, TECHNOLOGY AND CREATIVE COMPANIES, CARRIAGEWORKS, ROYAL PRINCE ALFRED HOSPITAL, THREE UNIVERSITY CAMPUSES, SYDNEY SCHOOL OF ENTREPRENEURSHIP, TWO TAFE NSW CENTRES AND NUMEROUS RESEARCH INSTITUTIONS.

AT THE AREA'S HEART, CENTRAL STATION IS THE GATEWAY TO GREATER SYDNEY. WITH REGIONAL AND SUBURBAN TRAIN SERVICES, CONNECTIONS TO LIGHT RAIL, BUS NETWORKS AND TO SYDNEY AIRPORT, THE AREA AROUND CENTRAL STATION IS ONE OF THE BEST CONNECTED DESTINATIONS IN AUSTRALIA –

AND THE BEST IS YET TO COME. A TRANSPORT FOR NSW PROJECT IS UNDERWAY TO RENEW CENTRAL STATION AND THE SURROUNDING AREA. THE VISION IS TO CREATE A NEW DESTINATION FOR SYDNEY - A REVITALISED GATEWAY FOR THE LOCAL, METROPOLITAN AND GLOBAL COMMUNITY.

WITH A RICH MANUFACTURING AND INDUSTRIAL HERITAGE, AN ESTABLISHED CREATIVE CLUSTER AND AN ACTIVE COMMUNITY, THE AREA HAS A STRONG AND ESTABLISHED ENTREPRENEURIAL SPIRIT. REDFERN, IN PARTICULAR, IS A PLACE OF GREAT CULTURAL AND HERITAGE IMPORTANCE TO THE ABORIGINAL COMMUNITY.

THIS IS AN OPPORTUNITY TO FURTHER GROW THIS UNIQUE AREA INTO A CENTRE FOR INNOVATION. IT IS A LONG-TERM, STRATEGIC INVESTMENT THAT RESPONDS TO SHIFTS IN GLOBAL ECONOMIC POWER AND THE CHANGING NATURE OF WORK, INCLUDING TECHNOLOGICAL BREAKTHROUGHS, GLOBAL COMPETITION FOR TALENT, ARTIFICIAL INTELLIGENCE (AI) AND AUTOMATION.

The Greater Sydney Commission recognised the area as the "nation's most mature innovation corridor", noting its unique assets in health, education, research, 'deep tech', business and creative industries."

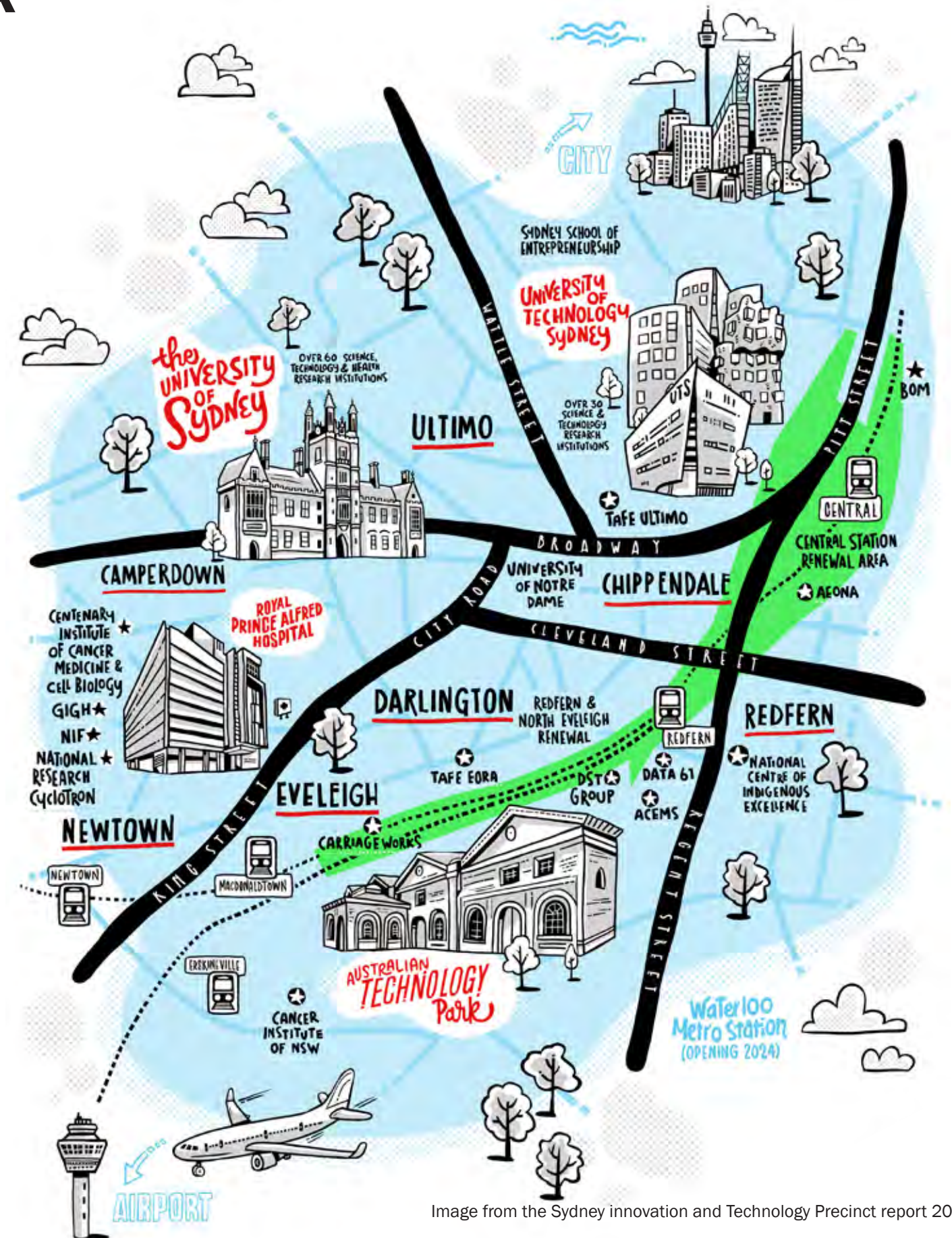


Image from the Sydney innovation and Technology Precinct report 2018



2.0 CONTEXT & SITE ANALYSIS

SITE LOCATION & DIMENSIONS

The Site is located at 8-10 Lee St, Sydney. It is adjacent to Central Station, Railway Square and Henry Deane Plaza. The site is 3768sqm in area. There is a 72m long boundary with the Central Station Platforms to the South East. There is a 43m boundary to the south west to Block B of the Western Gateway sub-precinct which currently includes the commercial building at 14 Lee St.

The existing structure on site was originally the Inwards Parcels Shed for the postal service. Currently the site is occupied by Sydney Railway Square Youth Hostel.

Vehicle access to the site is from Lee St via an upward ramping driveway which also serves as the drop off for the Adina Hotel at 2 Lee St (Western Gateway sub-precinct Block C).



APPENDIX A - URBAN DESIGN PLANNING REPORT

2.0 CONTEXT & SITE ANALYSIS



High resolution photograph from January 2016 clearly shows disused rail siding (not in shadow). Image capture is prior to construction of Sydney Yards Access Bridge.

APPENDIX A - URBAN DESIGN PLANNING REPORT

2.0 CONTEXT & SITE ANALYSIS

LOT OWNERSHIP & AIR RIGHTS

UPPER GROUND & ABOVE
Upper Carriage Lane Level

LEGEND

Lot 13

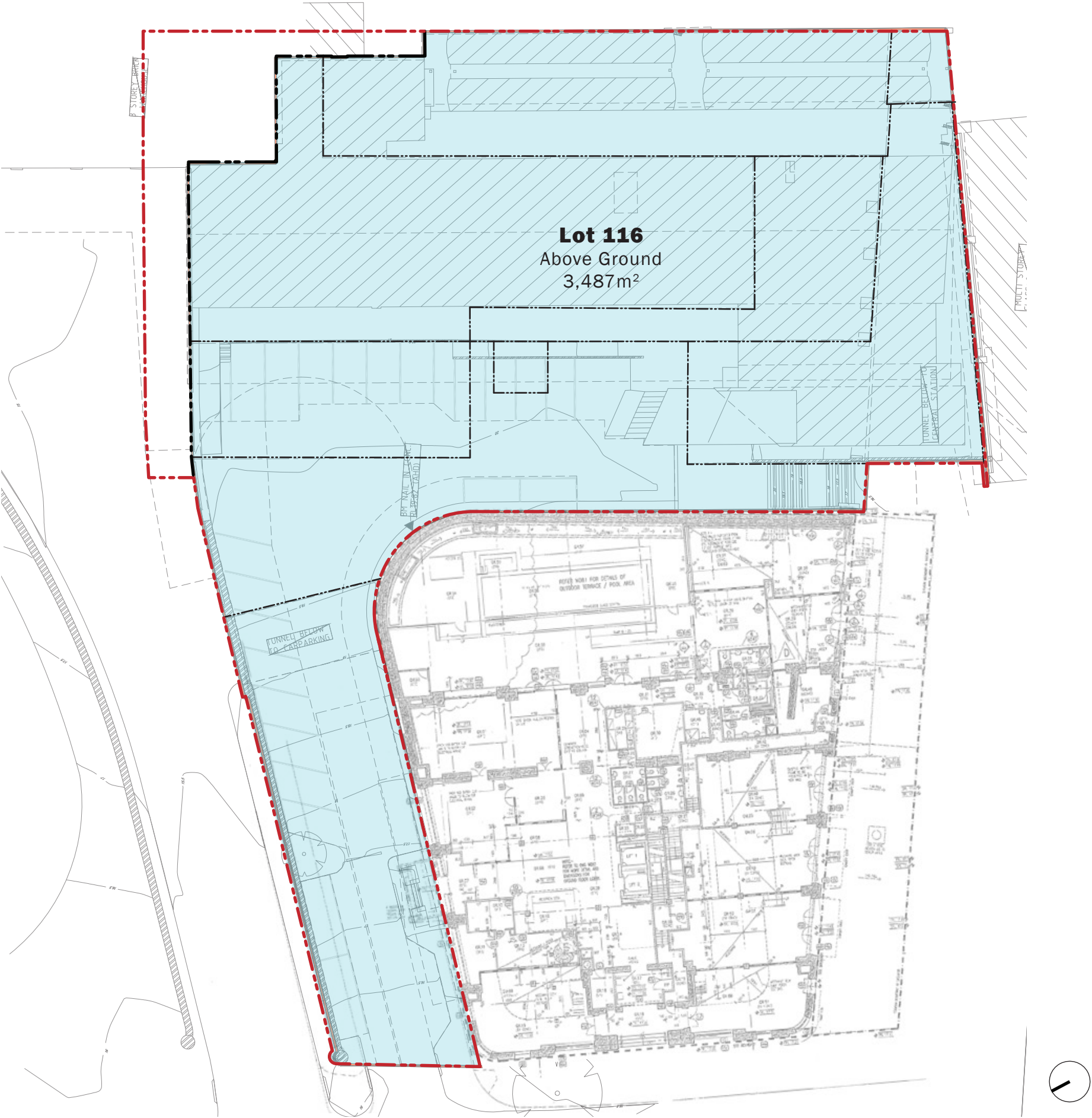
Lot 116

Lot 117

Lot 118

Site Boundary

DIAGRAM FOR INFORMATION PURPOSES ONLY.
The legal definitions of the areas and the accuracy of the surveyed content are outside of our control and should not be relied on in any way.



APPENDIX A - URBAN DESIGN PLANNING REPORT

2.0 CONTEXT & SITE ANALYSIS

LOWER GROUND & BELOW
Lower Carriage Lane Level

LEGEND

- Lot 13
- Lot 116
- Lot 117
- Lot 118
- Site Boundary

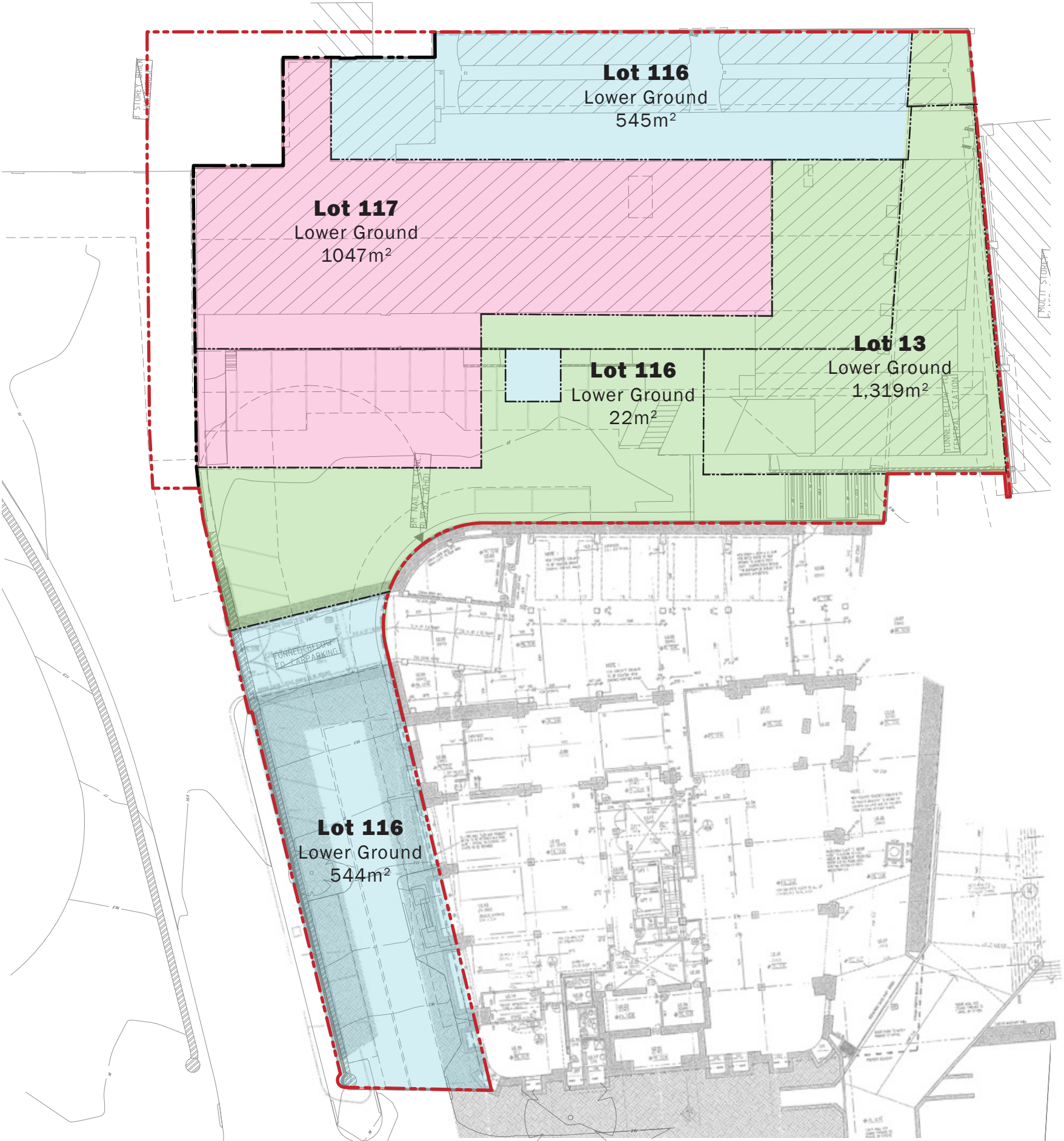


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APPENDIX A - URBAN DESIGN PLANNING REPORT

2.0 CONTEXT & SITE ANALYSIS

AIR RIGHTS

LEGEND

- Lot 13
- Lot 116
- Lot 117
- Lot 118
- Site Boundary

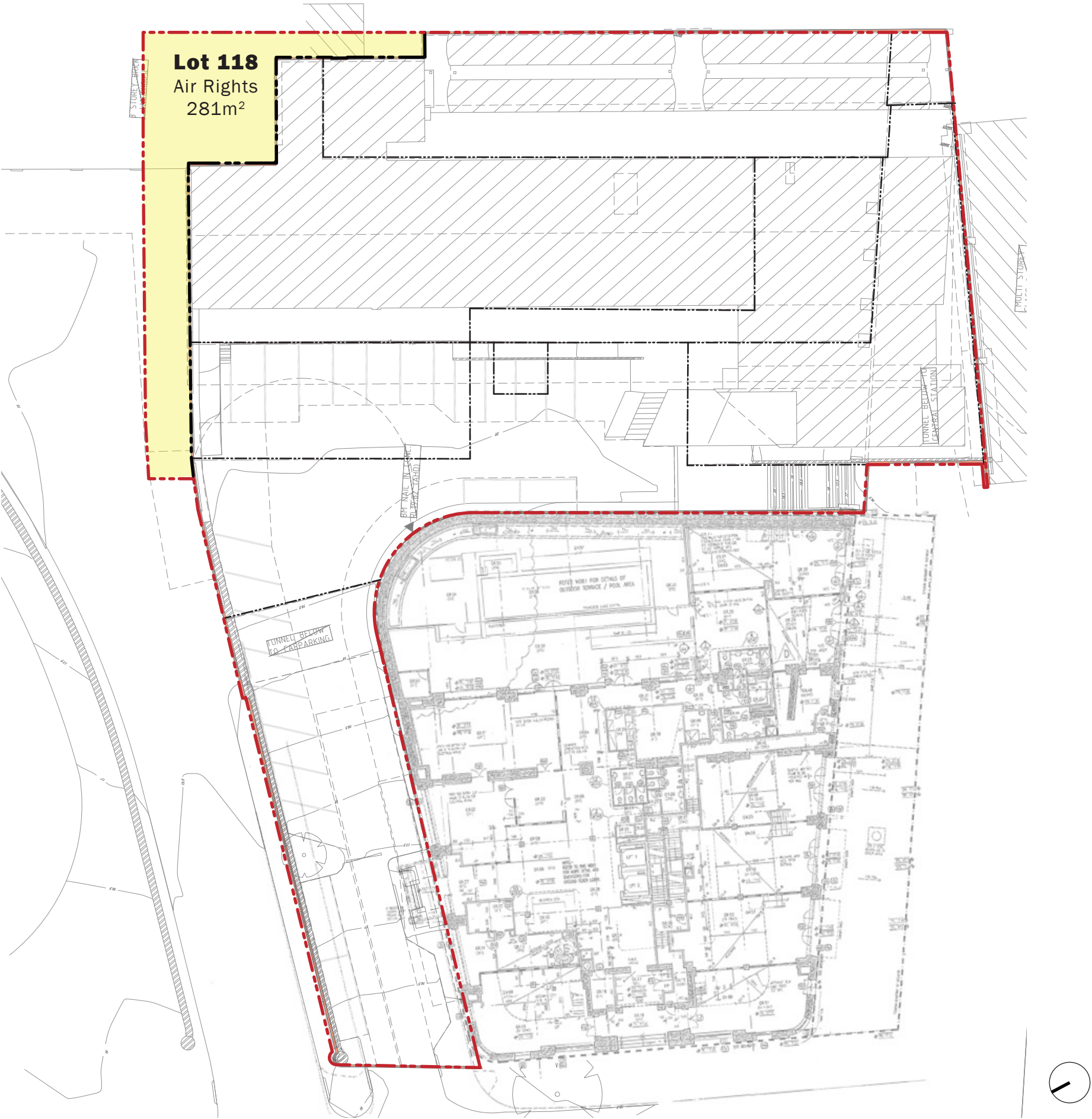
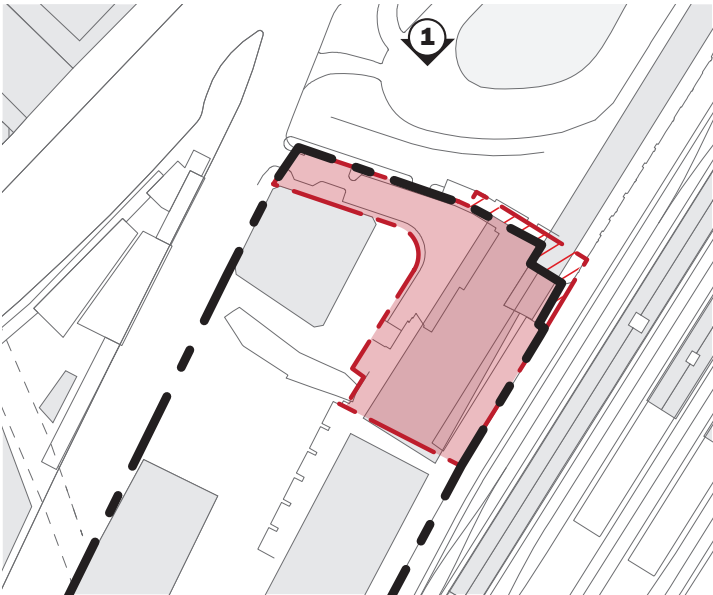


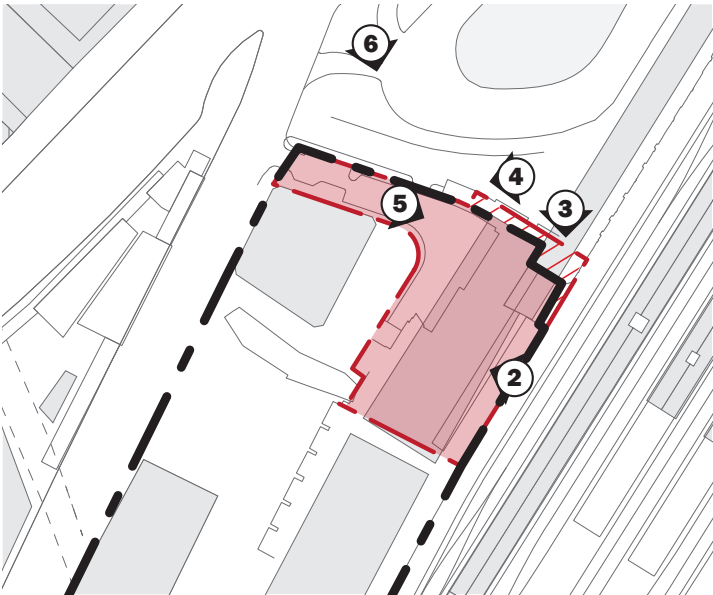
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The legal definitions of the areas and the accuracy of the surveyed content are outside of our control and should not be relied on in any way.

SITE PHOTOGRAPHS



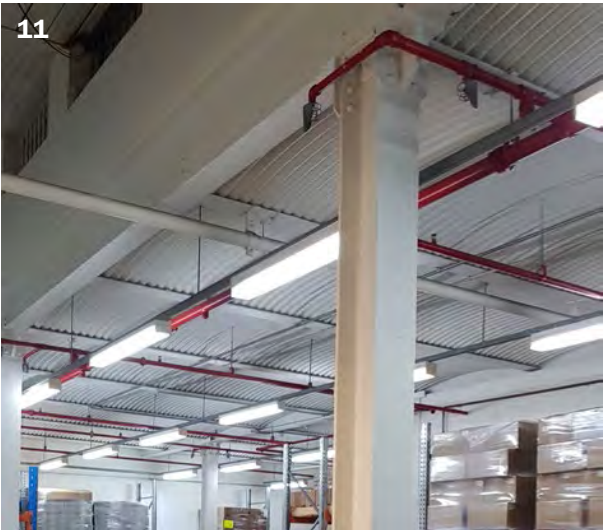
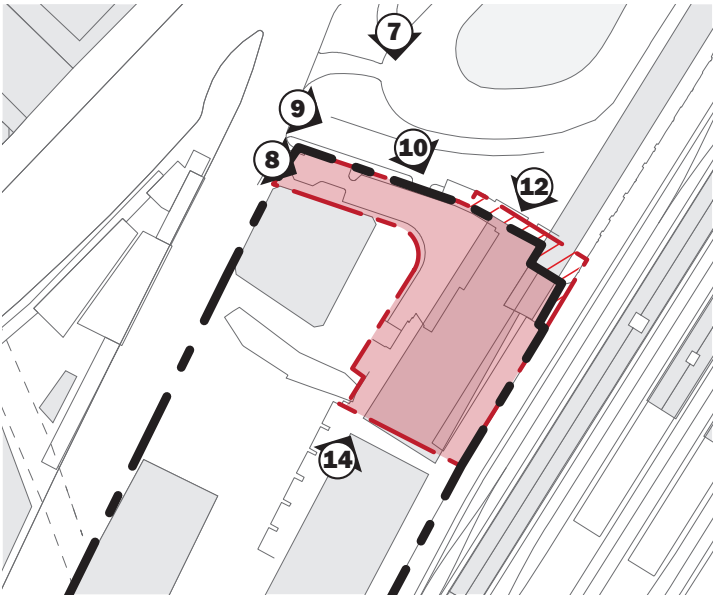
1. Three Quarter View from George Street

SITE PHOTOGRAPHS



2. View from the Roof of the Parcel Shed
3. Lower Carriage Lane looking towards the Parcels Shed
4. Lower Carriage Lane looking towards Lee St
5. Inwards Parcels Shed
6. Aerial View

SITE PHOTOGRAPHS



7. Parcel Shed from Lee St
8. Adina Apartment and parcel shed from the driveway
9. Ambulance Avenue towards Central Station
10. Parcel Shed, Adina CP entry and service zone from Ambulance Ave

11. Lower Ground Ceiling of the Parcels Shed
12. Detail of the wall to Lower Carriage Lane
13. Detail of one of three Parcels Shed Chimneys
14. Parcels Shed Verandah
15. Inside the Parcel Shed

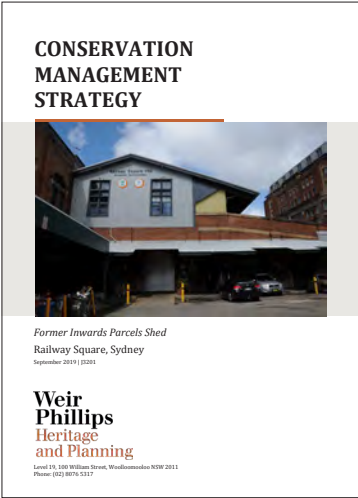
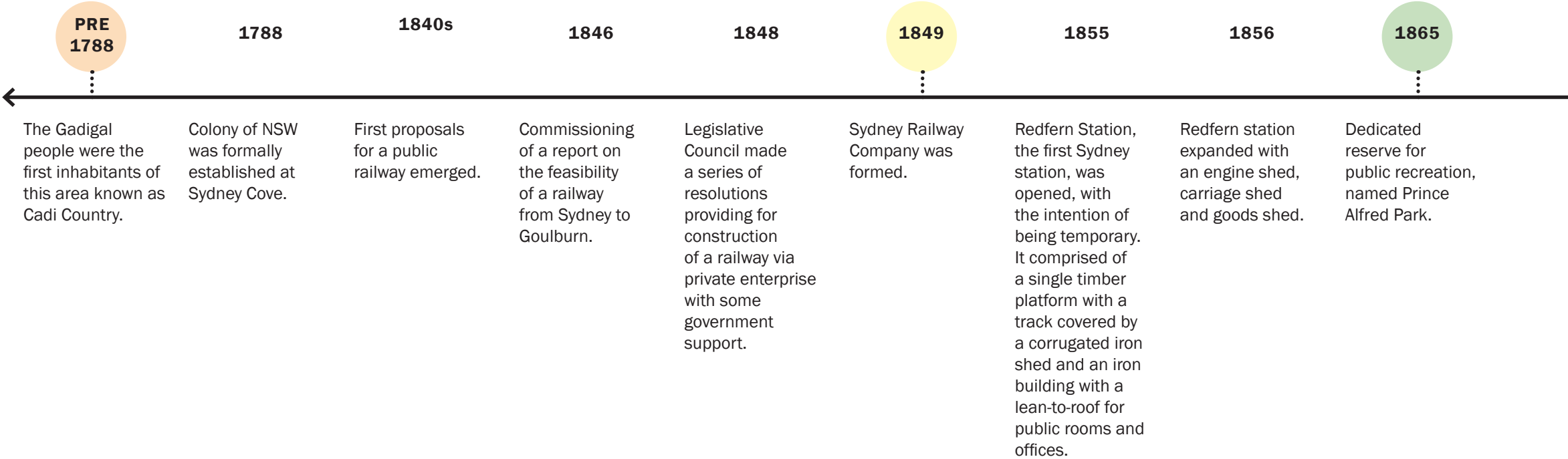


APPENDIX A - URBAN DESIGN PLANNING REPORT

2.0 CONTEXT & SITE ANALYSIS

SITE HISTORY

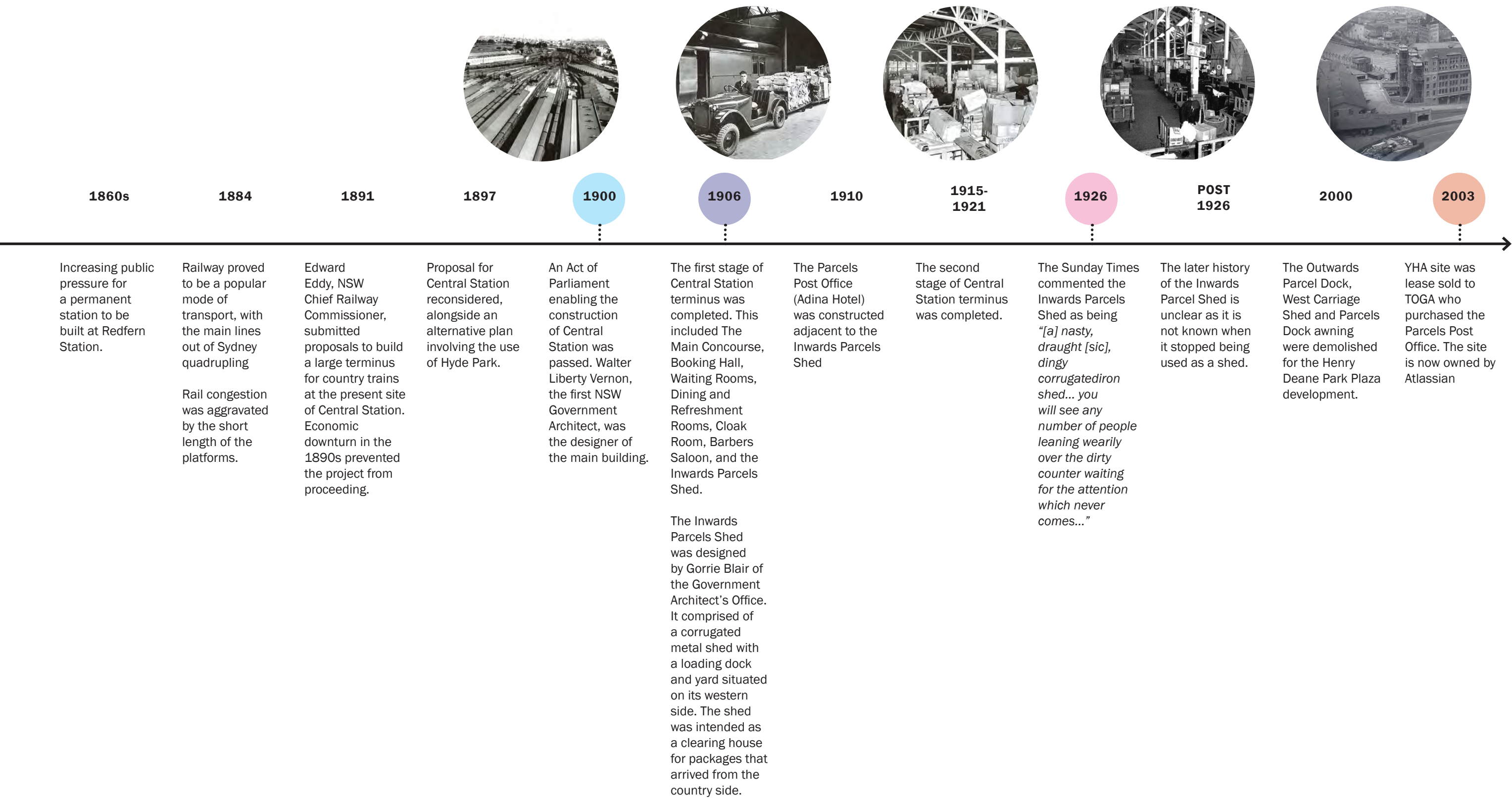
The following timeline are extracts from Section 3 of the CMS Report by Weir Phillips Heritage.



Conservation Management Strategy
Former Inwards Parcel Shed, Railway Square, Sydney,
September 2019, Section 3: Historical Analysis

APPENDIX A - URBAN DESIGN PLANNING REPORT

2.0 CONTEXT & SITE ANALYSIS



APPENDIX A - URBAN DESIGN PLANNING REPORT

2.0 CONTEXT & SITE ANALYSIS

CURRENT LEP PLANNING CONTROLS

The Site is zoned as Metropolitan Centre (B8) according to the Sydney LEP 2012. It is adjacent to the Central Station Infrastructure Zone (SP2).

The currently zoned maximum permissible floor space ratio on the site is 3:1 (V).



SYDNEY LOCAL ENVIRONMENTAL PLAN 2012 - LAND ZONING

Zone	
B1	Neighbourhood Centre
B2	Local Centre
B3	Commercial Core
B4	Mixed Use
B5	Business Development
B6	Enterprise Corridor
B7	Business Park
B8	Metropolitan Centre
IN1	General Industrial
IN2	Light Industrial
R1	General Residential
R2	Low Density Residential
RE1	Public Recreation
SP1	Special Activities
SP2	Infrastructure



SYDNEY LOCAL ENVIRONMENTAL PLAN 2012 - FSR

Maximum Floor Space Ratio (
F 0.6	X 4
H 0.7	Y 4.5
J 0.8	Z 5
L 0.9	AA1 6
N 1	AA2 6.5
P 1.25	AB1 7
S1 1.5	AB2 7.5
S2 1.75	AC 8
T 2	AD 9
U1 2.5	AE 10
U2 2.75	AF 11
V 3	Refer to cl.
W1 3.5	Refer to cl.
W2 3.75	

APPENDIX A - URBAN DESIGN PLANNING REPORT

2.0 CONTEXT & SITE ANALYSIS

The Maximum Building Height on the Site is 7.5m (H) as per the Sydney LEP 2012. It is adjacent to zones with a maximum height of 35m (V.)

The existing Parcel Shed on the site is listed in the Sydney 2012 LEP as a Heritage Item (I824). The Site is also listed on the State Heritage register (5012230) as part of Central Station item.



SYDNEY LOCAL ENVIRONMENTAL PLAN 2012 - HOB

Maximum Building Height (m)		
A 3	T3 28	AB2 85
E 6	T4 29	AC 110
H 7.5	U1 30	AD 130
I 8	U2 33	AE 150
J 9	V 35	AH 235
L 11	W1 40	Area 1
M 12	W2 42	Area 2
O 15	X 45	Area 3
P 18	Y 50	Area 4
R 22	Z 55	Area 5
S1 23	AA1 60	Area 6
S2 24	AA2 65	Area 7
T1 25	AA3 70	Area 8
T2 27	AB1 80	Area 9



SYDNEY LOCAL ENVIRONMENTAL PLAN 2012 - HERITAGE

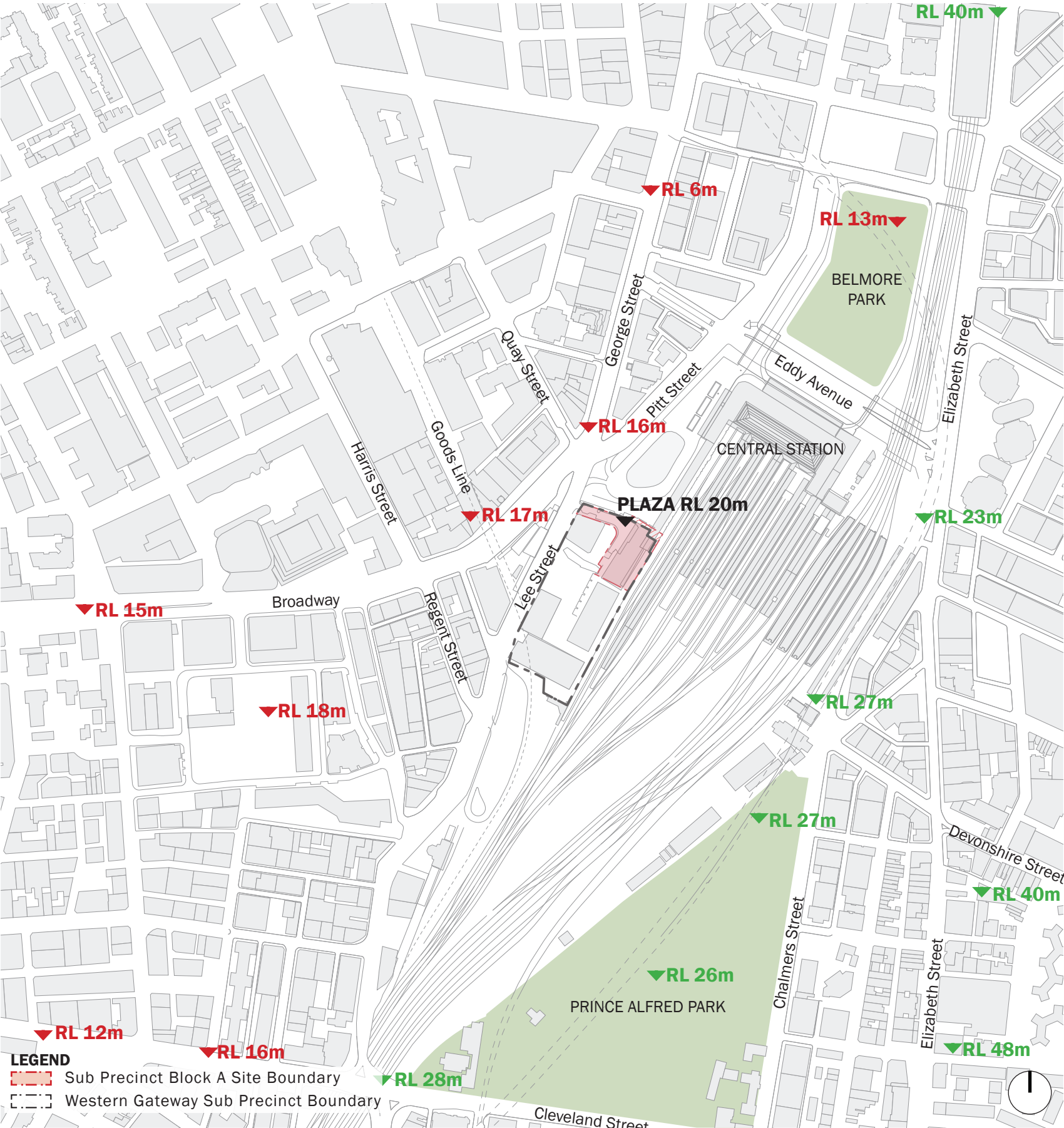
Heritage	
	Conservation Area - General
	Item - General

TOPOGRAPHY

The Site is located on a slope with a generally North-East Aspect. To the South East towards Prince Alfred Park and the Southern Part of Surry Hills, the terrain rises. To the North and North West the terrain drops gradually.

Natural Elements Diagram extracted from Draft Central Sydney Planning Strategy Document

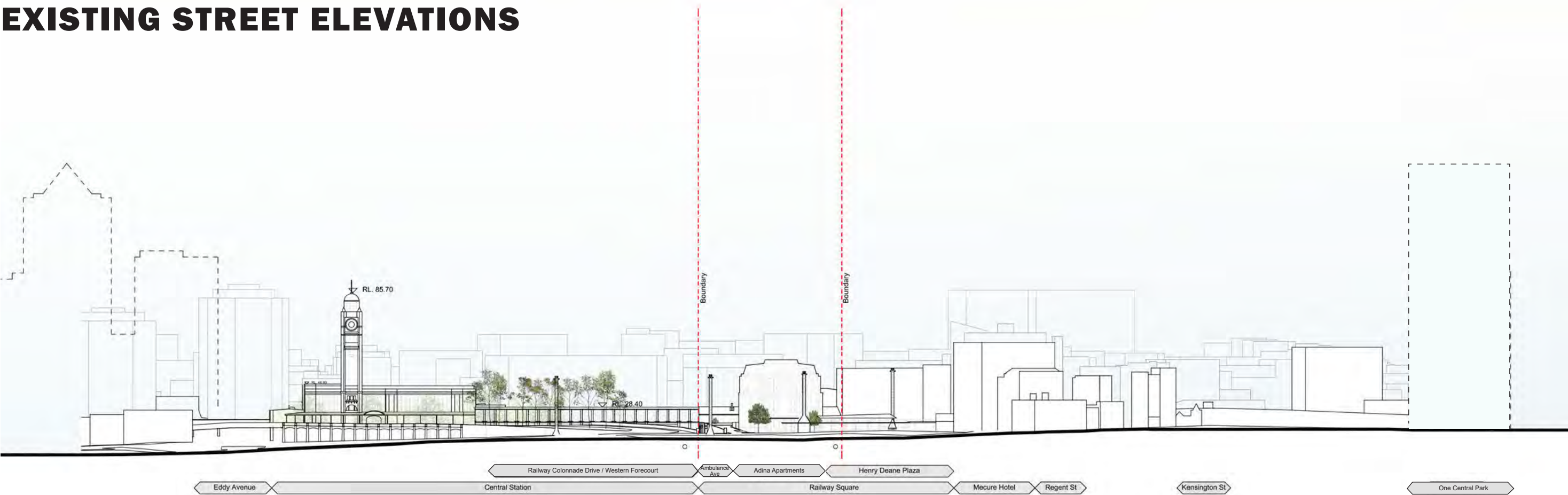
- 2m contours
- Ridgeline



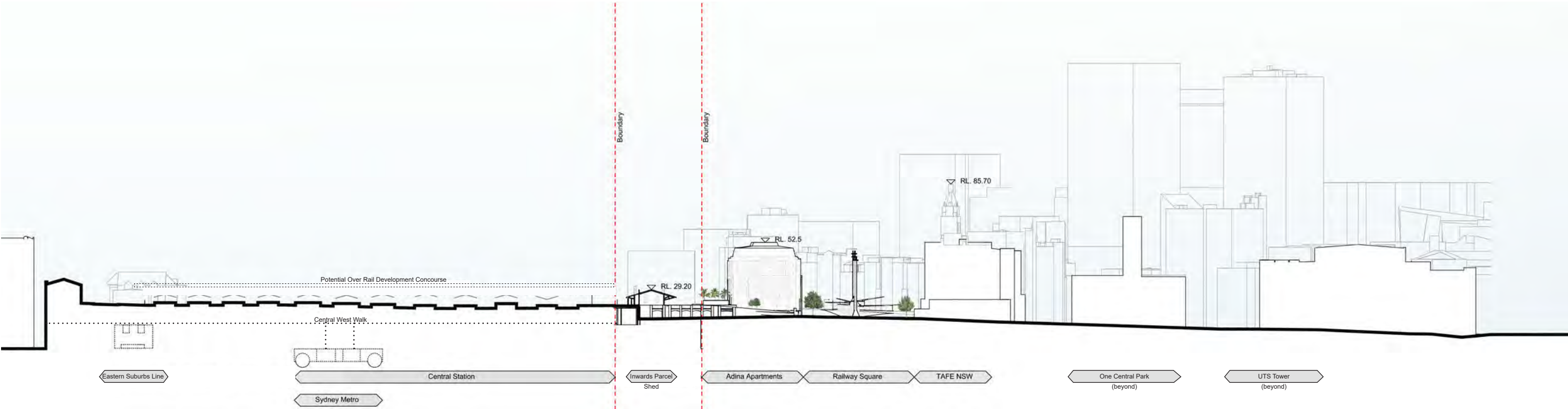
APPENDIX A - URBAN DESIGN PLANNING REPORT

2.0 CONTEXT & SITE ANALYSIS

EXISTING STREET ELEVATIONS



PITT STREET TO GEORGE STREET ELEVATION



AMBULANCE AVENUE / LOWER CARRIAGE LANE ELEVATION

DOMINANT STREET GRIDS

The Site is located in between the dominant urban street grids - the Main CBD, Western CBD and Pymont Grids. Instead the Site aligns to Central Station and the railway corridor.



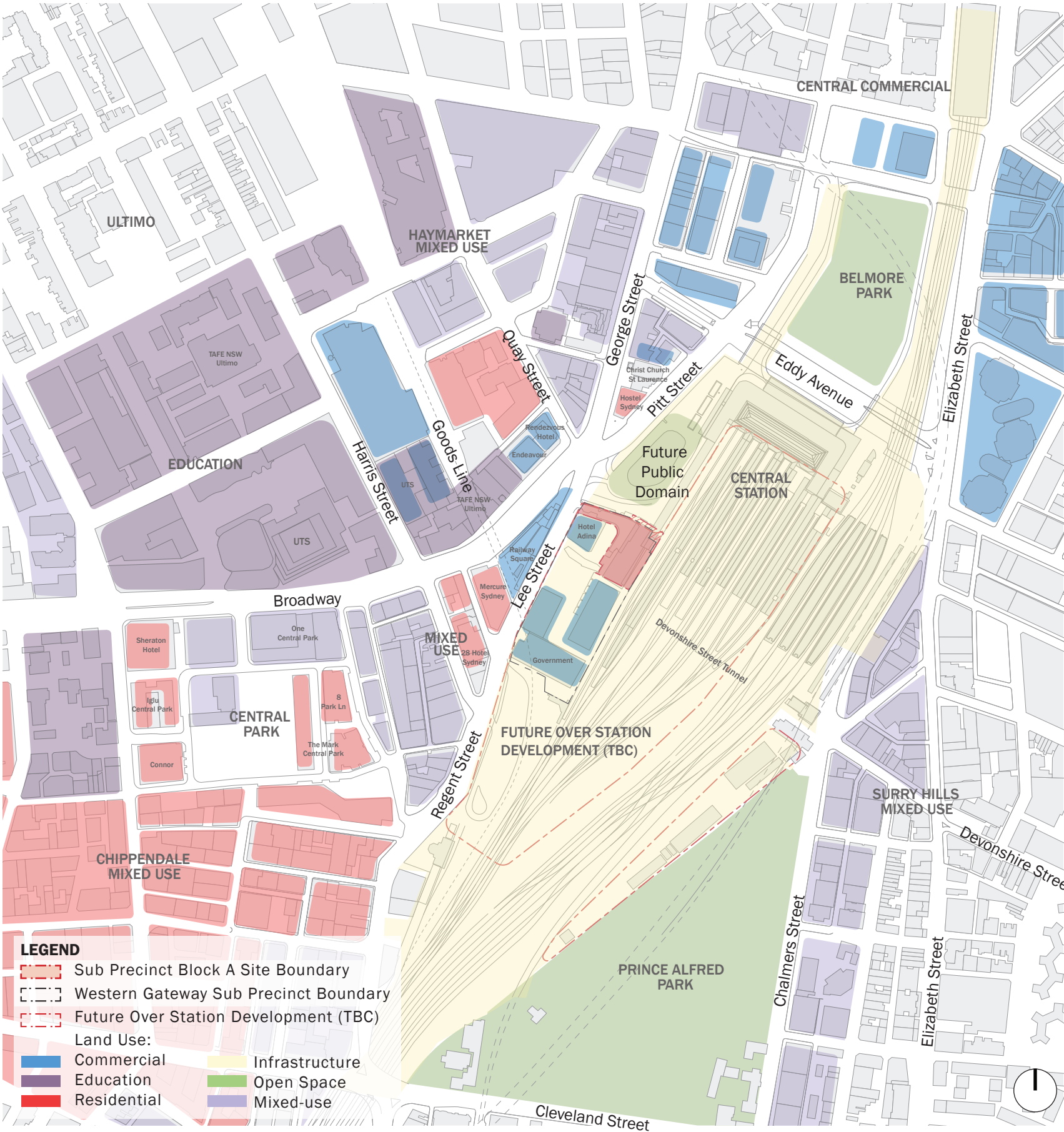
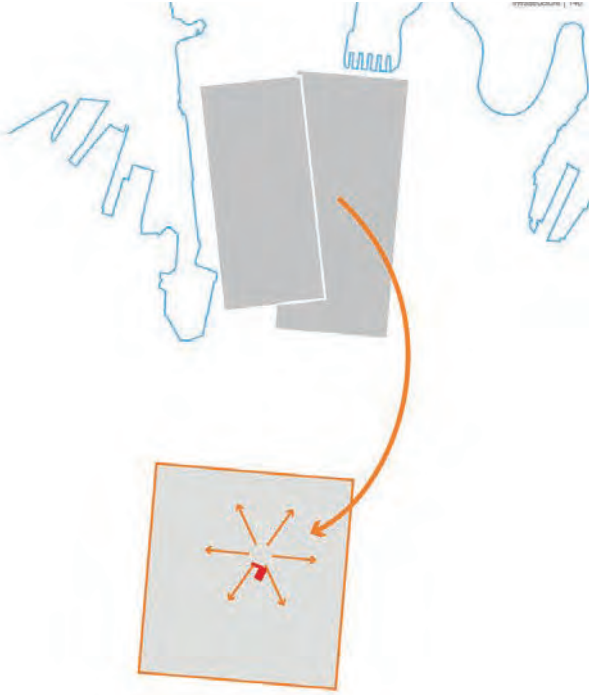
PRECINCTS AND LAND USE

The Site is located within the Central Station Precinct. It is surrounded by Commercial Buildings, namely the Hotel Adina and Government offices to the South West.

The adjacent precinct to the North West, is predominantly made of Education institutions. To the East is the Central Station infrastructure precinct. To the West and South West are the Chippendale and Central Park Mixed Use Precincts.

A future Tech Precinct, Tech Central, is earmarked for the area to the south and east of the site over the existing rail corridor potentially linking to the open space at Prince Alfred Park.

Medium Term Capacity Diagram extracted from Draft Central Sydney Planning Strategy Document



APPENDIX A - URBAN DESIGN PLANNING REPORT

2.0 CONTEXT & SITE ANALYSIS

CHARACTER

The area has a rich heritage context. The Western Gateway sub-precinct Block A is included in the heritage listings for the Central Station precinct and is also on the NSW State Heritage Register. The site includes the Inwards Parcels Shed and a heritage wall along the northern boundary which supports the access ramp. Both are considered heritage items.

The Inwards Parcels Shed has moderate heritage significance and is marked to be retained within the Central Station Conservation Management Strategy.

The Inwards Parcels Shed and the northern wall are integrally linked with the West Wing of Central Station and the Adina Hotel (former Parcels Post Office). Together these elements provide an important historic narrative to the Western Gateway Sub-Precinct.

The building forms part of a narrative that describes the delivery and dispatch of parcels from Central Station during the period up to the early 1980s, when overnight mail trains were run to many parts of NSW delivering all kinds of items to country customers.

The site is also immediately adjacent to the Western Forecourt area of Central Station, and in close proximity to the main station building which are significant heritage features. There are also a series of heritage items in the immediate surroundings of the Western Gateway sub-precinct which contribute to the overall setting and context.

There are opportunities to adapt and express the original character of the site and provide interpretation, in line with heritage principles.

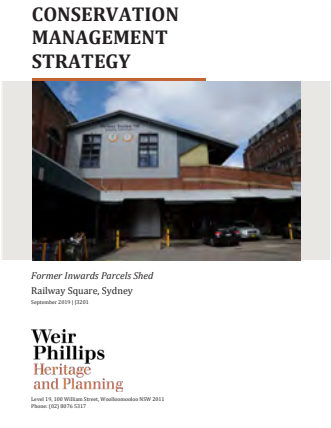
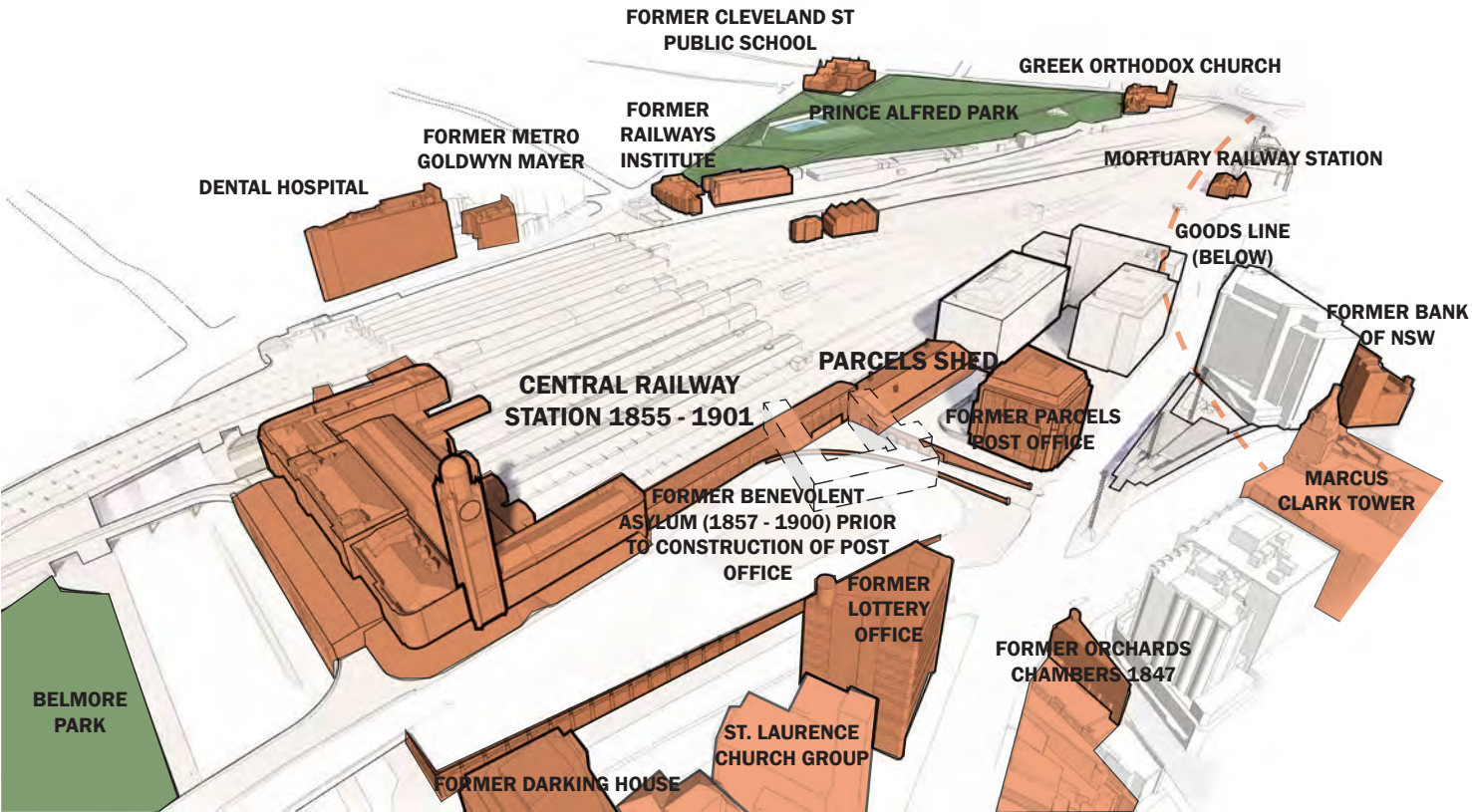
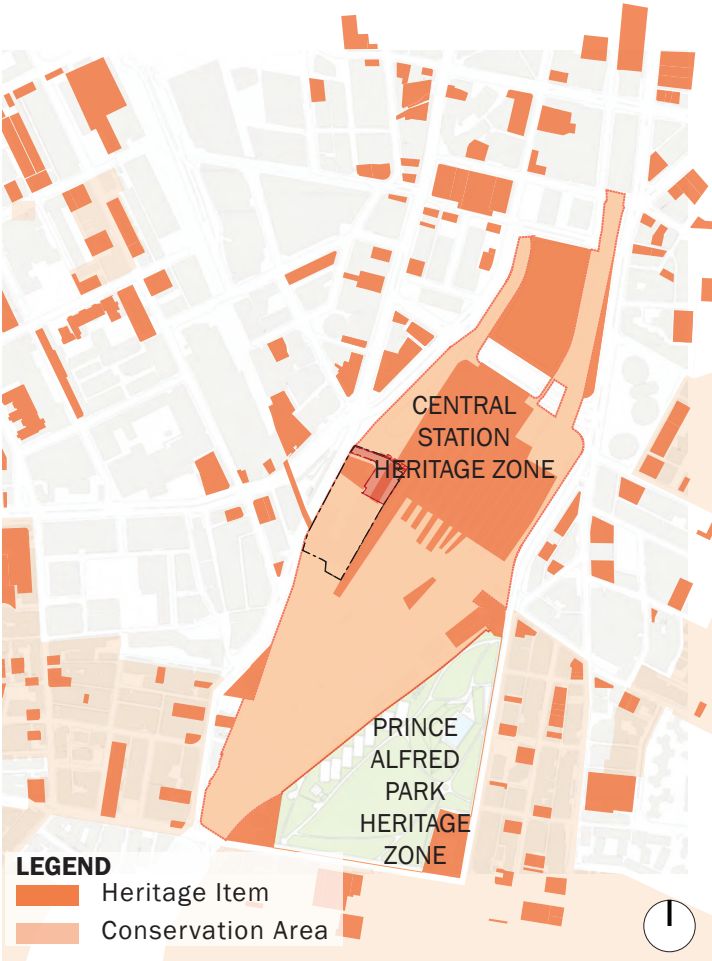
*“Old ideas can sometimes use new buildings. New ideas must use old buildings.”
- Jane Jacobs*



1. View of Railway Square Looking South West
2. View of Railway Square Looking North East
3. Railway Square
4. Parcel Shed
5. Aerial View of Central Station and Internal Parcels Shed on the right

HERITAGE CONTEXT

PARCEL SHED: GRADING & CONDITION		
ELEMENTS	GRADING	CONDITION
YHA Railway Square Overall	moderate	good
Views and Vistas	moderate	n/a
Context and Setting	high	n/a
Parcels Shed Yard	moderate	good
Roof and Awning	moderate	good
Timber Columns & Truss work	moderate	good
Remnant Brick Wall and Sand-stone Plinth	high	fair
Corrugated Iron Cladding	little	very good
Platform	moderate	good
Doors, Windows and Hardware	little	very good
Floors and Paving	little	good
Ceiling and Cornices	little	good
Carriage Accommodation	little	very good
Contemporary Awning and Glazed Barrier	little	very good
Fitout	little	very good
Introduced Services, Mechanical, Electrical, Lighting and Data	intrusive	good
Archaeological Potential	nil	no potential



Conservation Management Strategy
Former Inwards Parcel Shed, Railway Square, Sydney,
September 2019, Section 5: Heritage Values & Significance



Max Dupain photograph looking north towards the Clock Tower

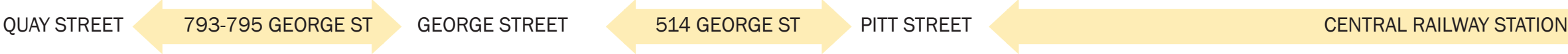
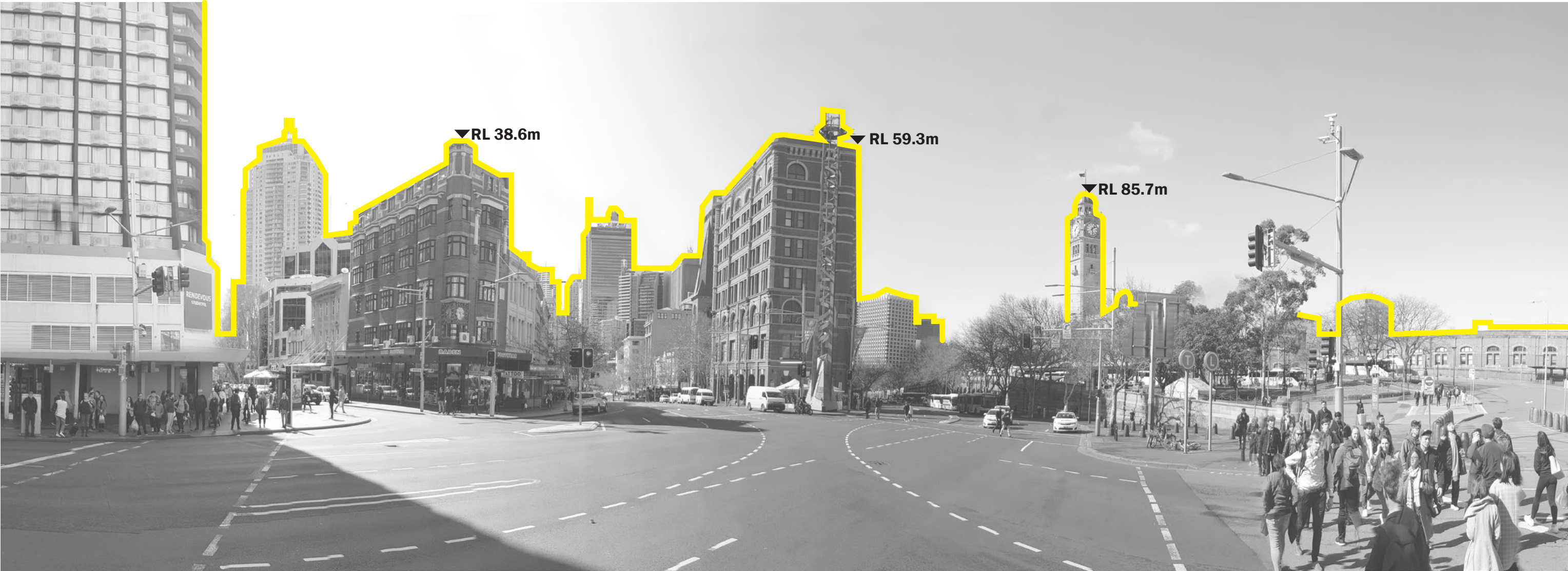
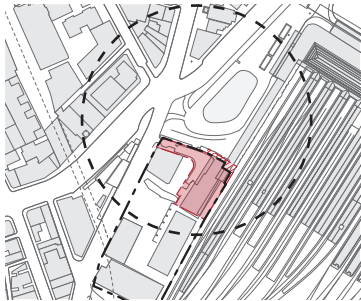


Current panorama looking south to Ambulance Avenue



Historical aerial photograph looking south towards Parcels
Shed and Post Office

NEIGHBOURING BUILDINGS



APPENDIX A - URBAN DESIGN PLANNING REPORT

2.0 CONTEXT & SITE ANALYSIS



VIEW CORRIDORS AND SIGHT LINES

The Site is adjacent to the protected view corridor of Central Station Clock tower from George St and Broadway. The Draft Central Sydney Planning Strategy indicates this a clear to the horizon within the View Protection Planes and Sydney Harbour Views map. A series of other views are designated in the Public Views for Protection map.

Detailed view studies can be found of each of the nominated views in Appendix B View Analysis document. These demonstrate the site is significantly outside the impact zone on the Central Clock Tower protected view corridor and has insignificant impacts on the identified public views.

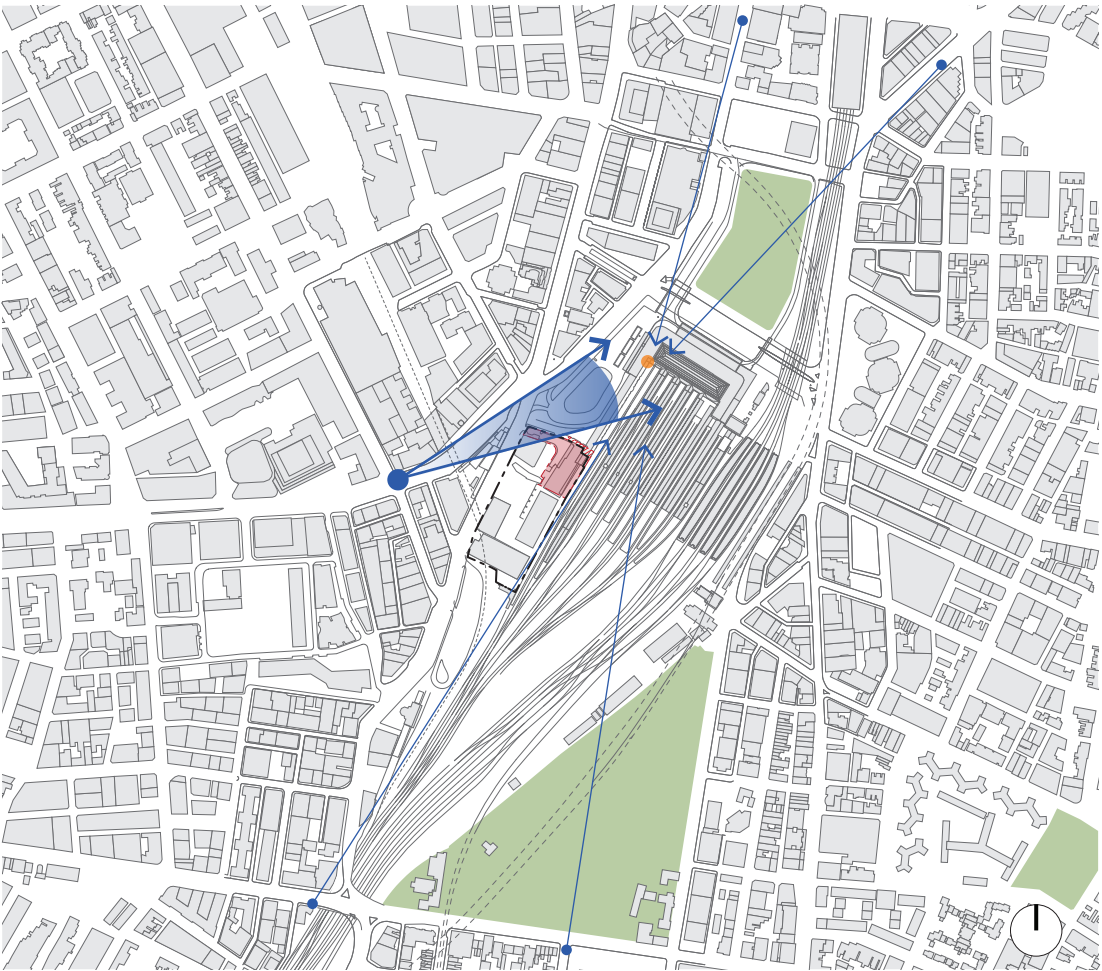
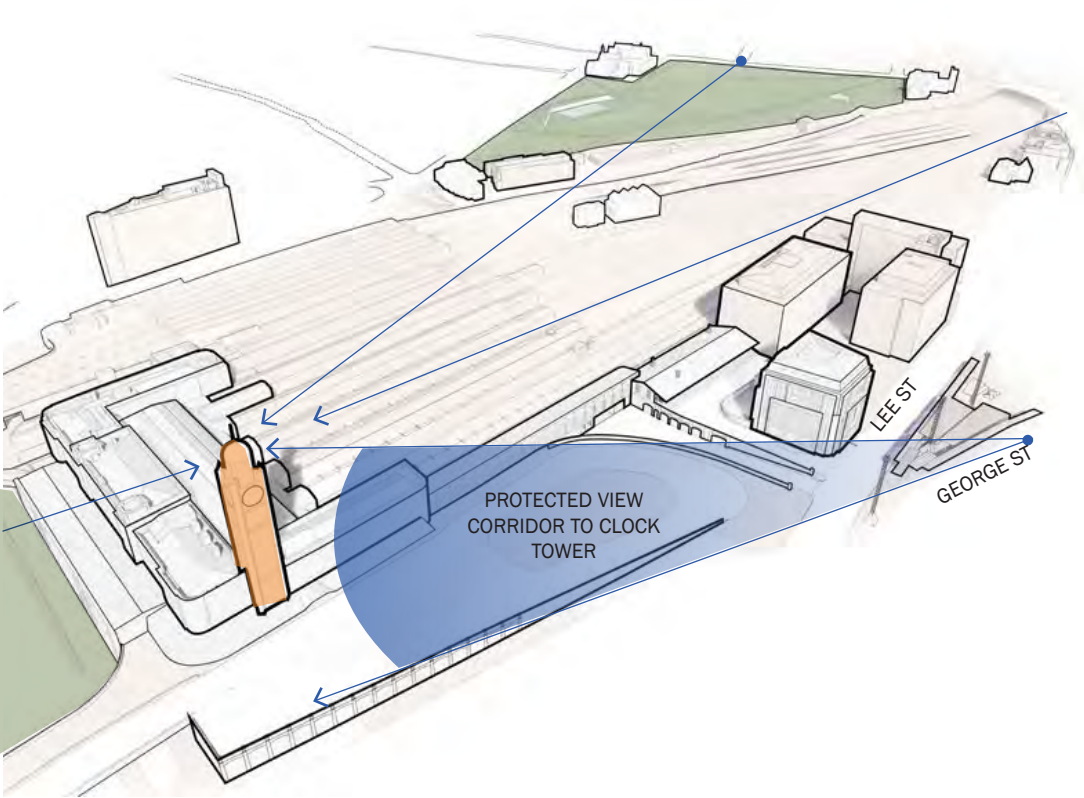


Diagram of View Corridors and Sight Lines



LEGEND

- Sub Precinct Block A Site Boundary
- Western Gateway Sub Precinct Boundary
- Key View - Central Sydney Planning Strategy
- Key View - Altered
- Protected View Corridor
- Clock Tower



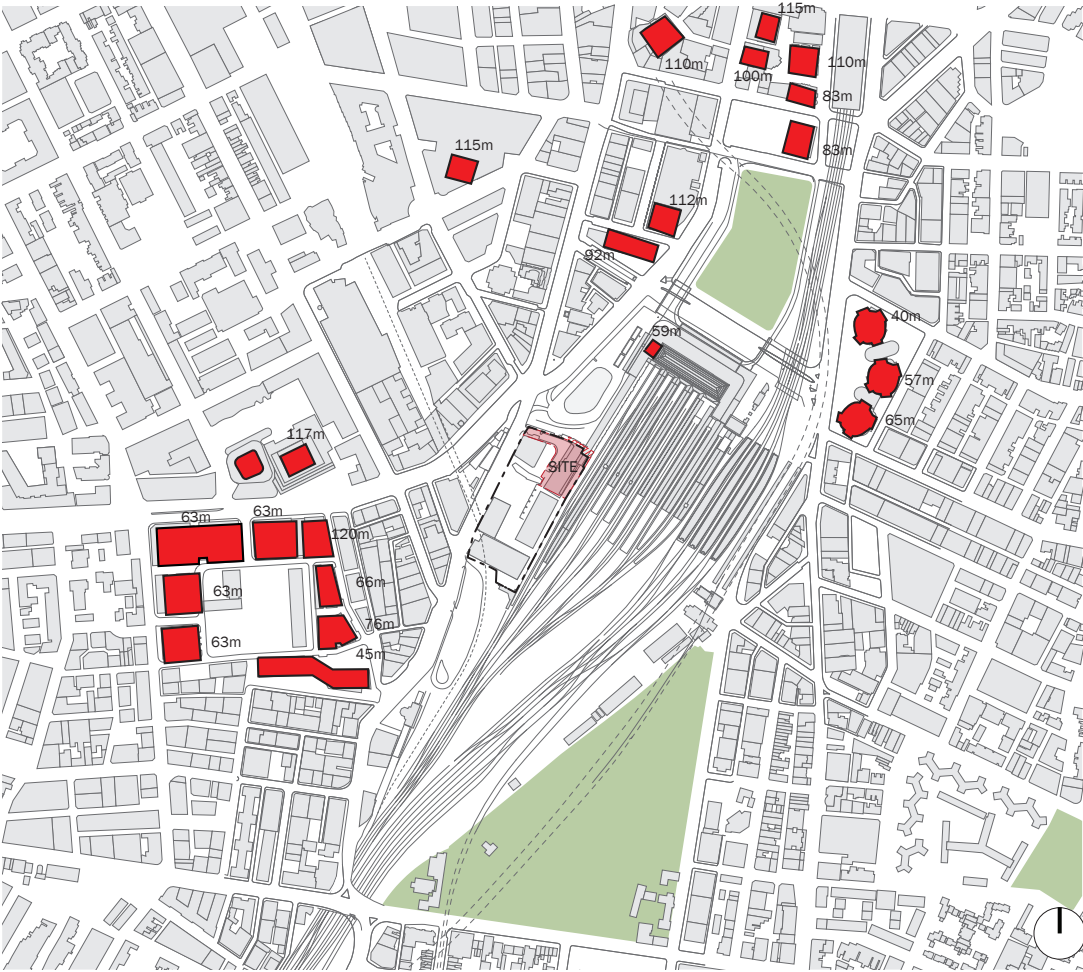
View to Central Station from Prince Alfred Park



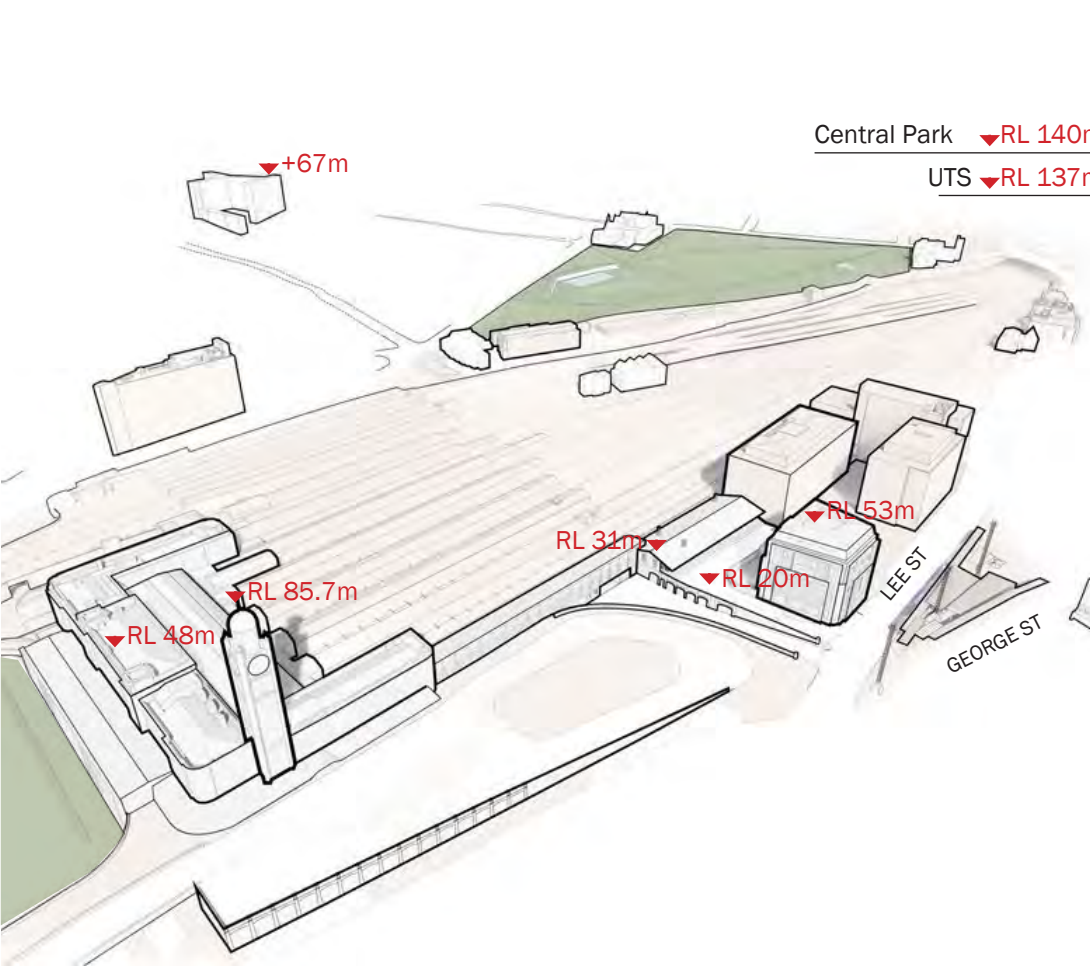
View to Central Station Clock Tower from Corner of Broadway & Pitt Street

TALL BUILDINGS

The Site and adjacent development sites which form the Western Gateway sub-precinct act as a continuation of the Sydney CBD. New buildings in this area will form a joint in the taller built form elements of UTS and Central Park with Haymarket and the Southern end of Sydney CBD.



Tall Buildings



Tall Buildings & Open Spaces / Parks

LEGEND

- Sub Precinct Block A Site Boundary
- Western Gateway Sub Precinct Boundary
- Key Tall Building



One Central Park by Jean Nouvel



Belmore Park and Central Station beyond



UTS Tower by Michael Dysart



The Mark, Central Park

PUBLIC TRANSPORT

Central Station is a key nexus connecting Sydney through multiple modes of transport. The expanded Light-Rail and Sydney Metro are new additions currently under construction. The site is uniquely positioned to add amenity and connectivity between Railway Square and other destinations to the west of the Western Gateway sub-precinct and the Sydney Metro potential egress point.



Draft Central Sydney Planning Strategy Document

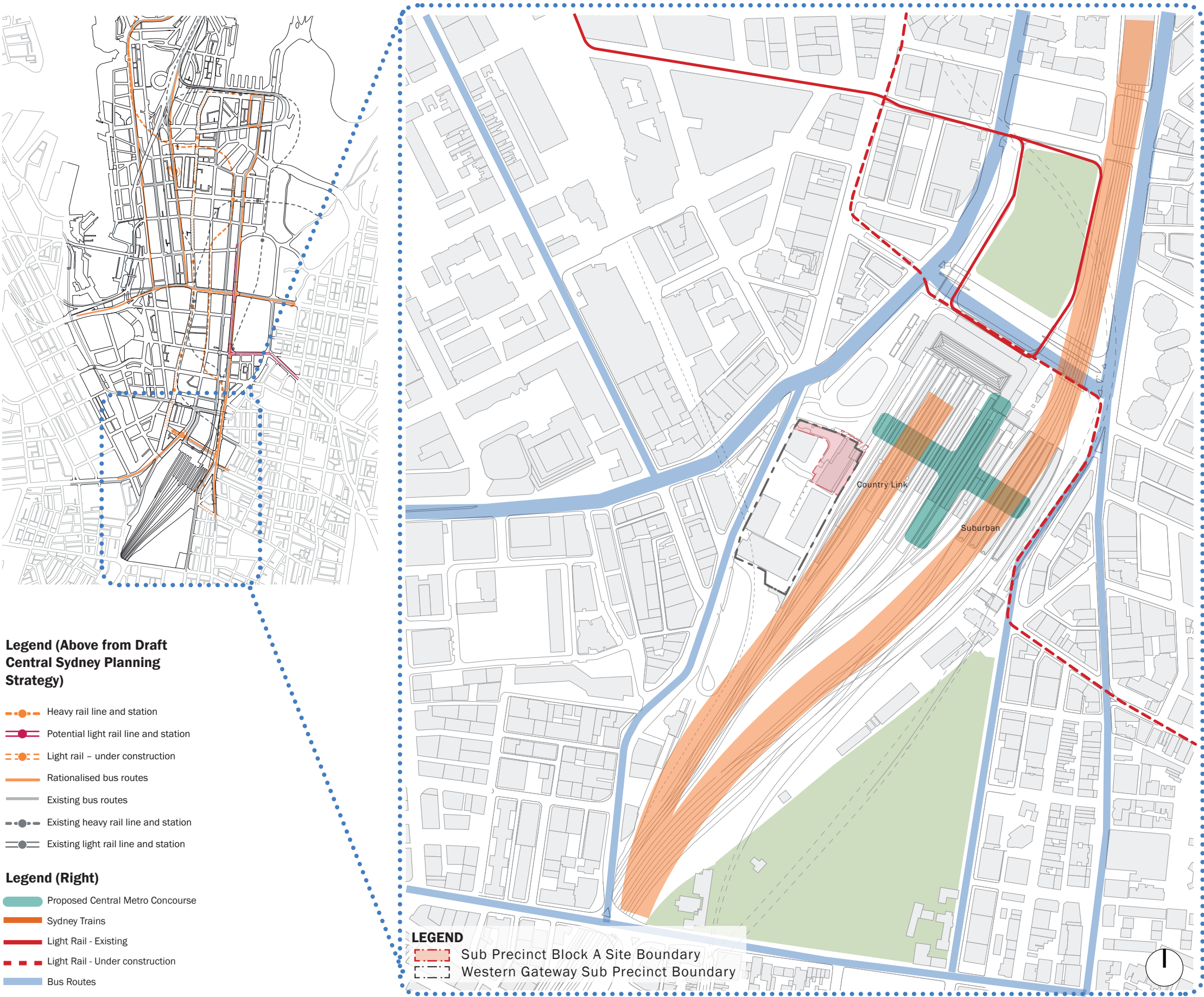


Diagram (left) by City of Sydney.

VEHICULAR MOVEMENT

Abercrombie Street / Regent Street and Cleveland Street form part of a key ring road providing vehicular access around the Sydney CBD. Investigations have flagged the potential for Lee Street to potentially be closed.

The Western Gateway sub-precinct planning has considered the potential for the future closure or more restricted access provisions to Lee Street. Long term access arrangements have been integrated into the precinct planning with all vehicles anticipated to access the precinct via the existing vehicle access point to the south of Block C. All vehicles can then circulate internally within the Western Gateway sub-precinct via a conjoined basement solution.



Draft Central Sydney Planning Strategy Document

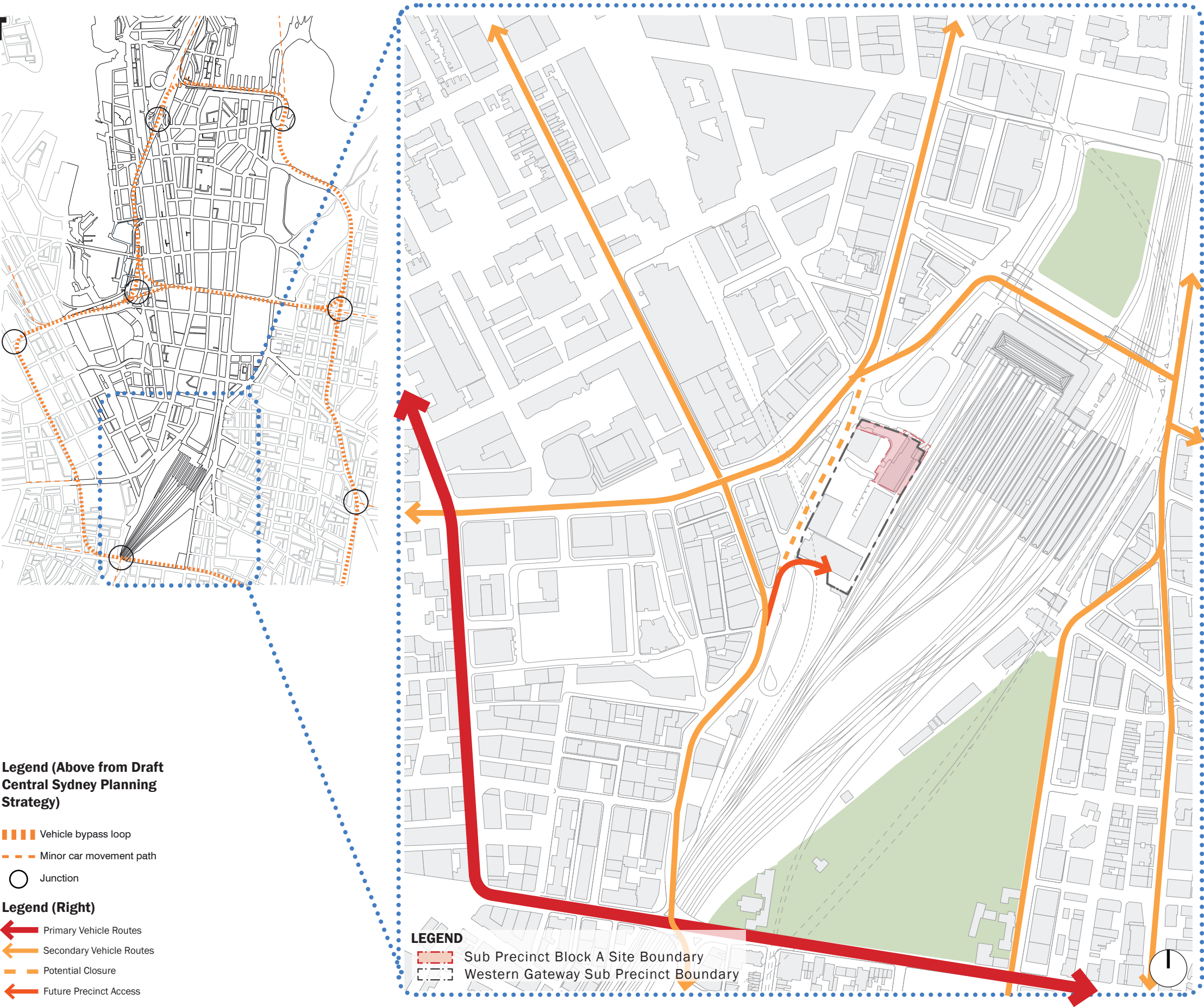


Diagram (left) by City of Sydney.

PEDESTRIAN MOVEMENT

Being at the hub of Sydney’s public transport network there are many pedestrian movements associated with Central Station around the precinct.

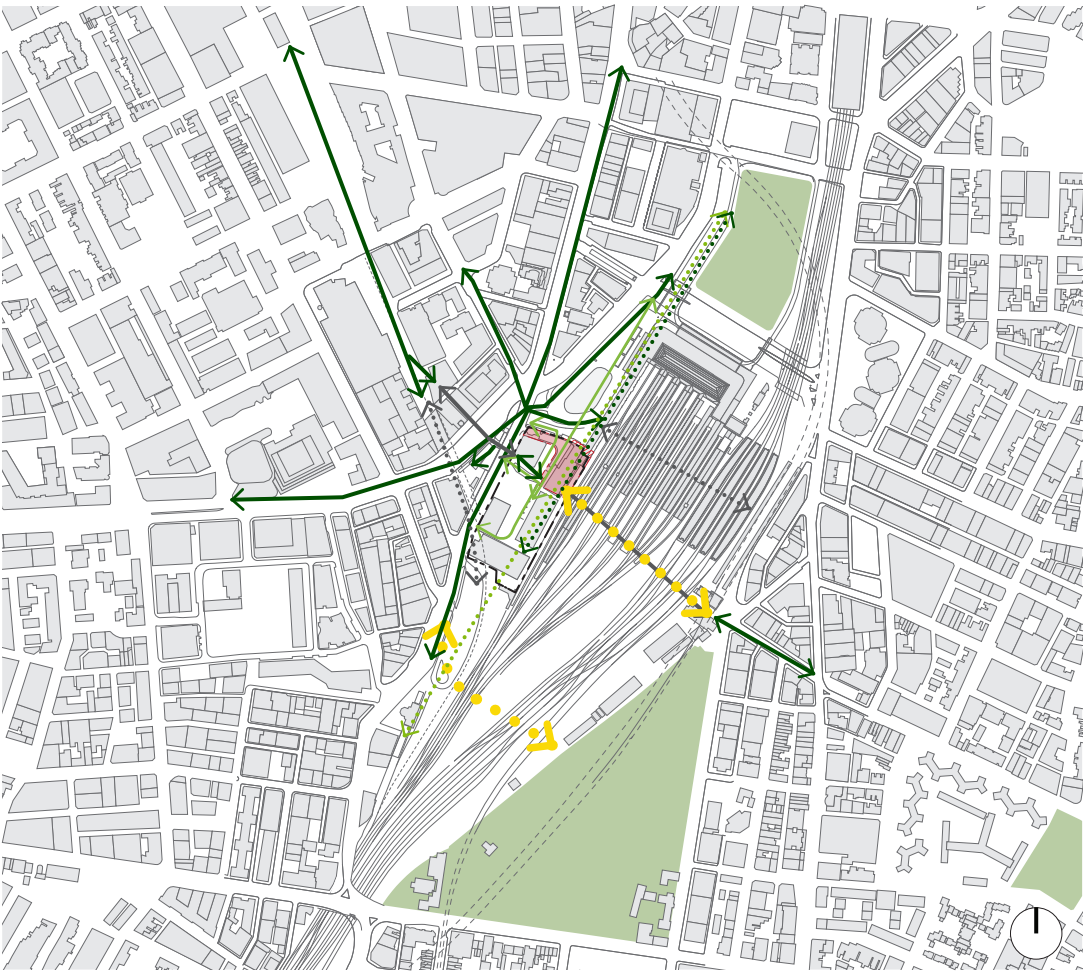
Within walking proximity lie a number of major pedestrian movement attractors. There are a significant number of these currently to the north and west of the precinct. To the south east, lie Prince Alfred Park and southern zones of Surry Hills. Pedestrian movements between the these areas which area currently divided by Central Station are via Devonshire Tunnel. Henry Deane Plaza is the western terminus of the Devonshire Tunnel and provides access into the Lee Street tunnel. This below road tunnel provides pedestrian access to Railway Square and the northern side of George St plus the Goods Line currently serves as a major pedestrian connection.

The Western Gateway sub-precinct anticipates a series of changes to the pedestrian movement patterns:

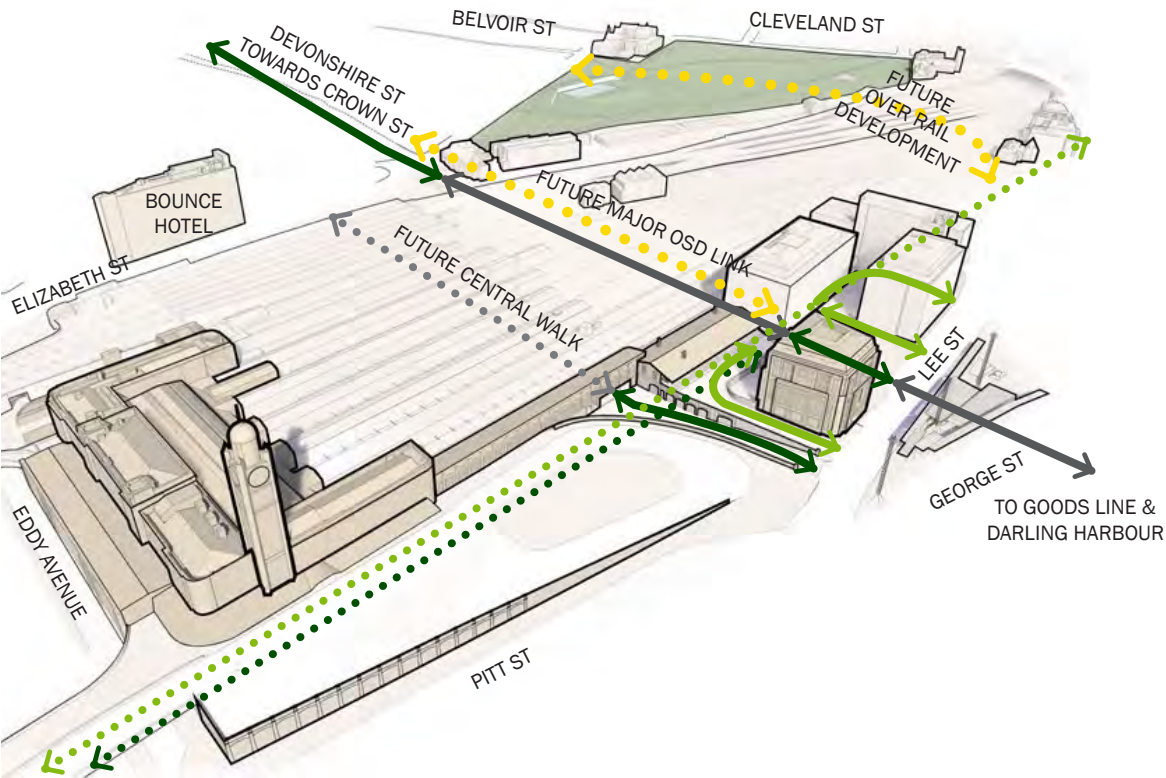
- Opening of Central Walk West - extension of the new major concourse element of Central Station
- North-south links from the Western Forecourt at lower and upper ground levels through the sub-precinct with the opportunity for a future connection onwards to Mortuary Station
- Western interface point of east-west connection across the future over-station development as an extension of Devonshire St

LEGEND

- Sub Precinct Block A Site Boundary
- Western Gateway Sub Precinct Boundary
- Underground Connection - Existing
- Underground Connection - Future
- Lower Level Connection - Existing
- Lower Level Connection - Future
- Upper Level Street Connection - Existing
- Upper Level Street Connection - Future
- Over Station Connection - Future



Pedestrian Movement



Pedestrian Movement & Open Spaces / Parks



Devonshire Street Tunnel



Artist Impression of Future Metro connection



Henry Deane Plaza



Artist Impression of Future Devonshire Street

FUTURE DEVELOPMENT

The diagram adjacent illustrates the opportunities current development sites and potential ones in the immediate vicinity of the site.

A new vertical school is currently under construction at the South Western side of Prince Alfred Park. A new extension has just been completed at UTS, along with the latest phase of development at Central Park. New towers and key public domain sections are now open at Darling Square. There is also potential above the City of Sydney’s Goulburn Street carpark (future development TBC).

The Central Precinct renewal includes proposed future over station development which is currently under investigation. The Western Gateway sub-precinct within which the site falls has been initiated as the first phase of development within the Central Precinct renewal and is adjacent to the new Central Walk West and the planned future open space combining the Western Forecourt and Railway Square.

For more information on the Western Gateway sub-precinct development proposals refer to Section 5 of this report.

LEGEND

Sub Precinct Block A Site Boundary

Western Gateway Sub Precinct Boundary

Future Over Station Development

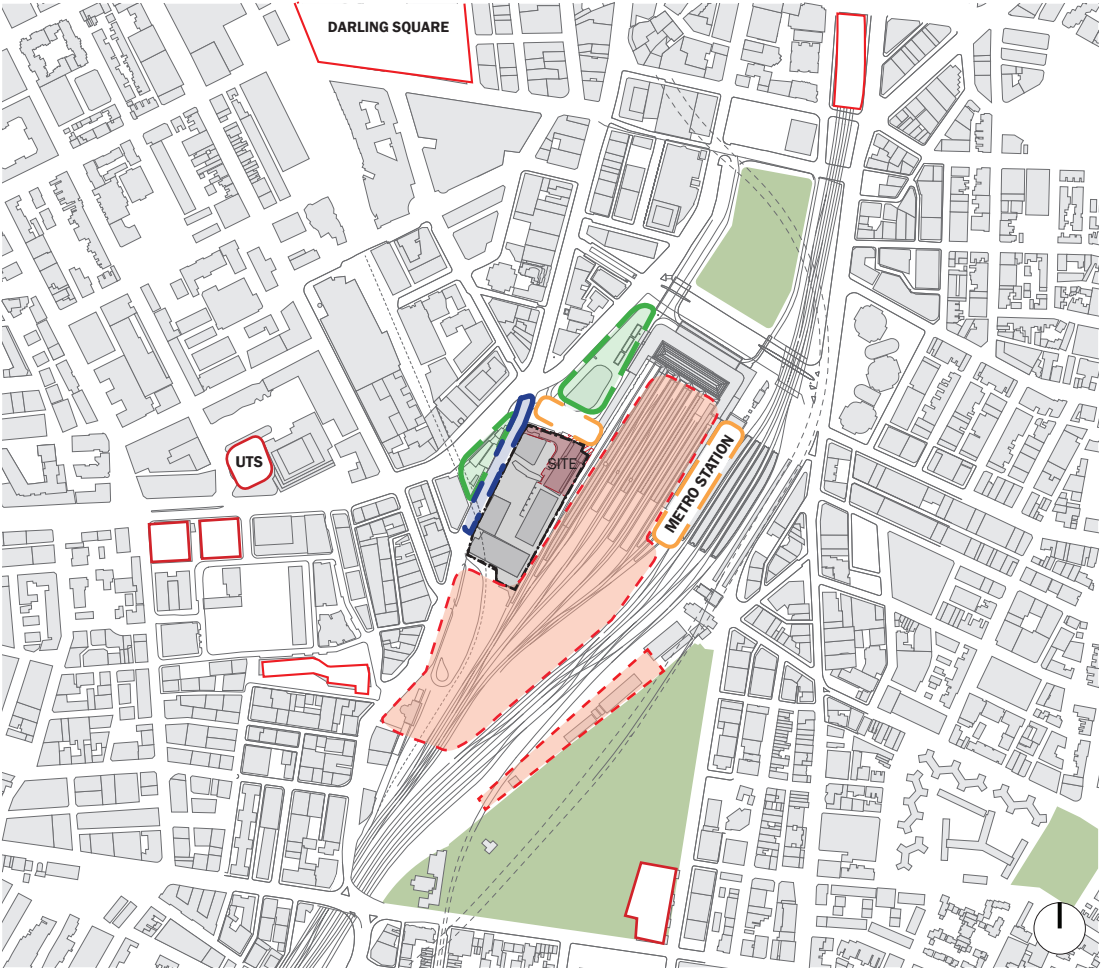
Future Rail Siding Development

Metro Station Development

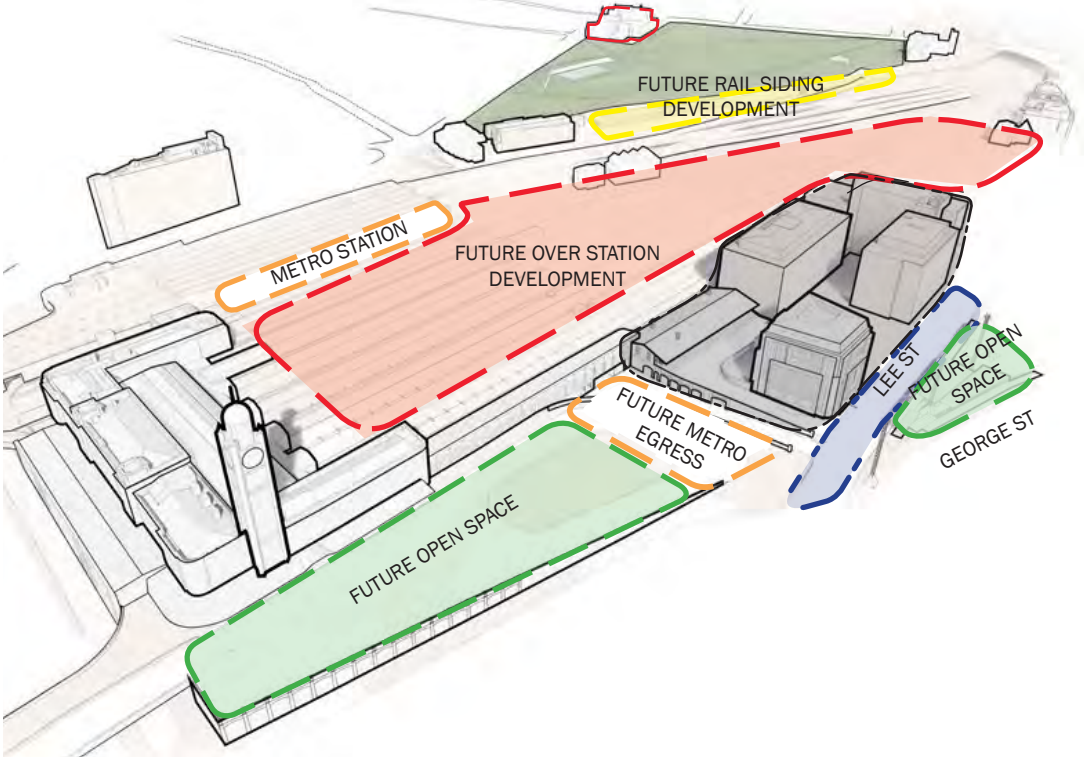
Future Open Space

Surrounding Development

Potential Road Closure



Future Development



Future Development Sites



UTS CB02 building designed by FJMT



Artist impression of Central Station Metro



Artist impression of Chalmers St Light Rail Station



Future Prince Alfred Park High School by FJMT

OPEN SPACES / PARKS

Sydney is characterised by a rich network of green public spaces. These are often within walking distance from each other and provide relief from the everyday intensity of the inner city. These various types of open space including playgrounds, sports & recreational areas, event spaces or ecological corridors, form the backdrop of Sydney life all year round.

The precinct sits at the intersection of several of these green links and lies adjacent to Belmore Park and across the rail, Prince Alfred Park which provides recreational and leisure amenities. This is the most significant of the adjacent areas of open space and solar access protections for this space are designated within planning controls.

To the north of the site is existing open space surrounded by the bus drop-off. This area is earmarked for a future plaza and combined with Railway Square forms part of the ‘Third Square’ at the south end of the George Street civic spine. To the South is Henry Deane Plaza which provides a pedestrian access into Central Station and a connection to Devonshire St and Surry Hills to the East.

Future public open space is anticipated to be delivered as part of any over station development proposal. The position and extent of these elements is currently not known.

LEGEND

Sub Precinct Block A Site Boundary

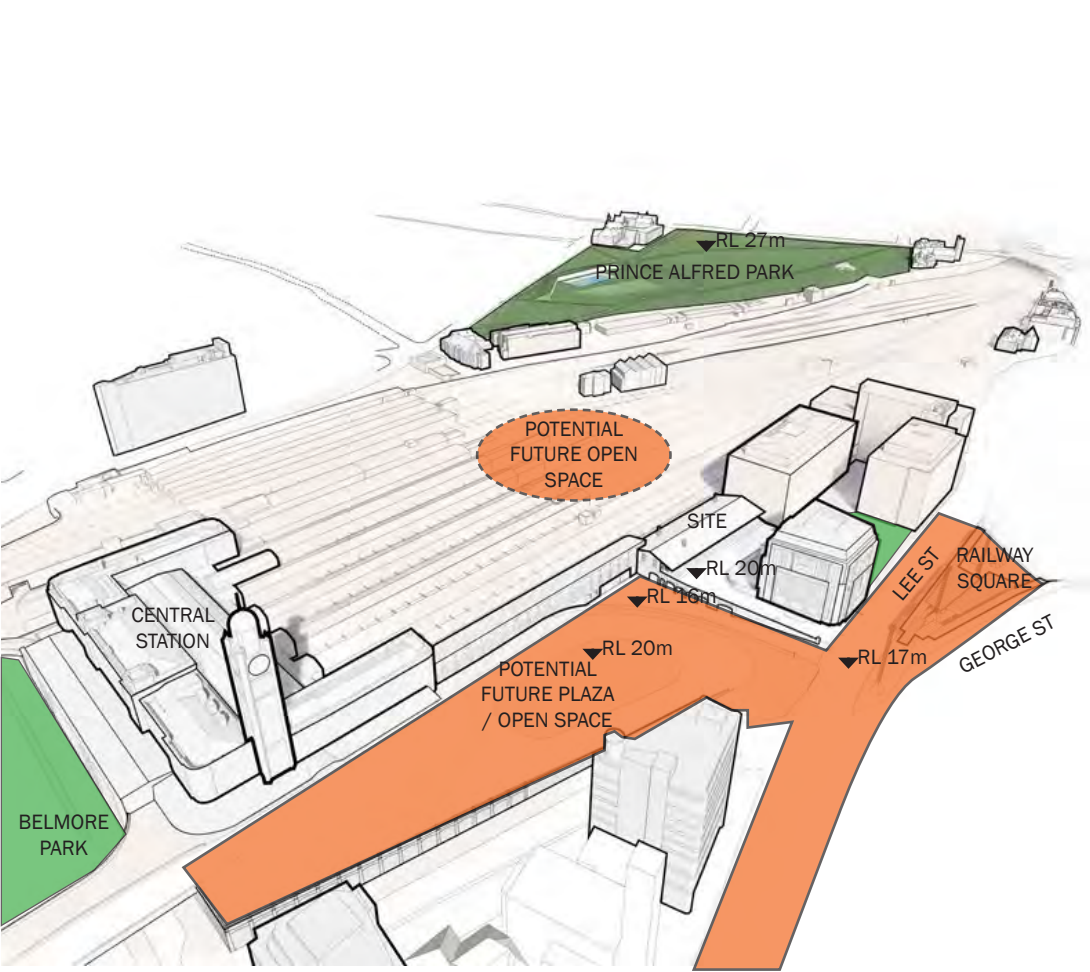
Western Gateway Sub Precinct Boundary

Potential Future Open Space/Parks

Existing Open Space/Parks



Diagram of Open Spaces / Parks



Belmore Park



Henry Deane Plaza



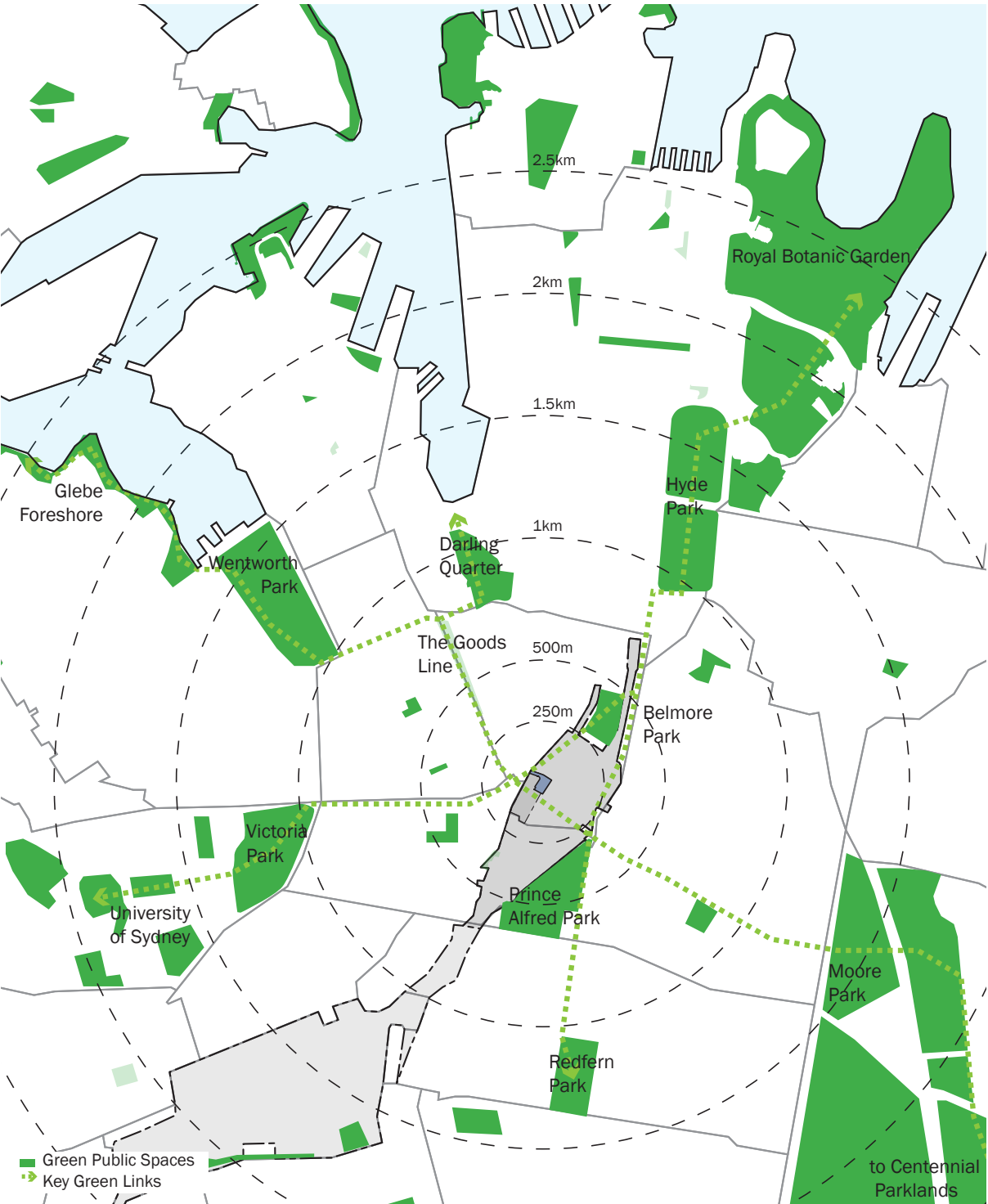
Prince Alfred Park



Chippendale Green

APPENDIX A - URBAN DESIGN PLANNING REPORT

2.0 CONTEXT & SITE ANALYSIS



SUN PROTECTION CONTROLS

The current LEP controls relative to overshadowing of Prince Alfred Park state:

6.19 Overshadowing of certain public places
(1) Despite clause 4.3, development consent must not be granted to development that results in any part of a building causing additional overshadowing, at any time between 14 April and 31 August in any year, of any of the following locations during the times specified in relation to those locations—

(h) Prince Alfred Park (beyond the shadow that would be cast by a wall with a 20 metre frontage height on the boundary between the park and the railway land)—between 12.00–14.00

The City of Sydney’s Draft Central Sydney Planning Strategy provides greater detail creating a Sun Access Plan and expanding the period of protection to 10am - 2pm.



Draft Central Sydney Planning Strategy Document



APPENDIX A - URBAN DESIGN PLANNING REPORT

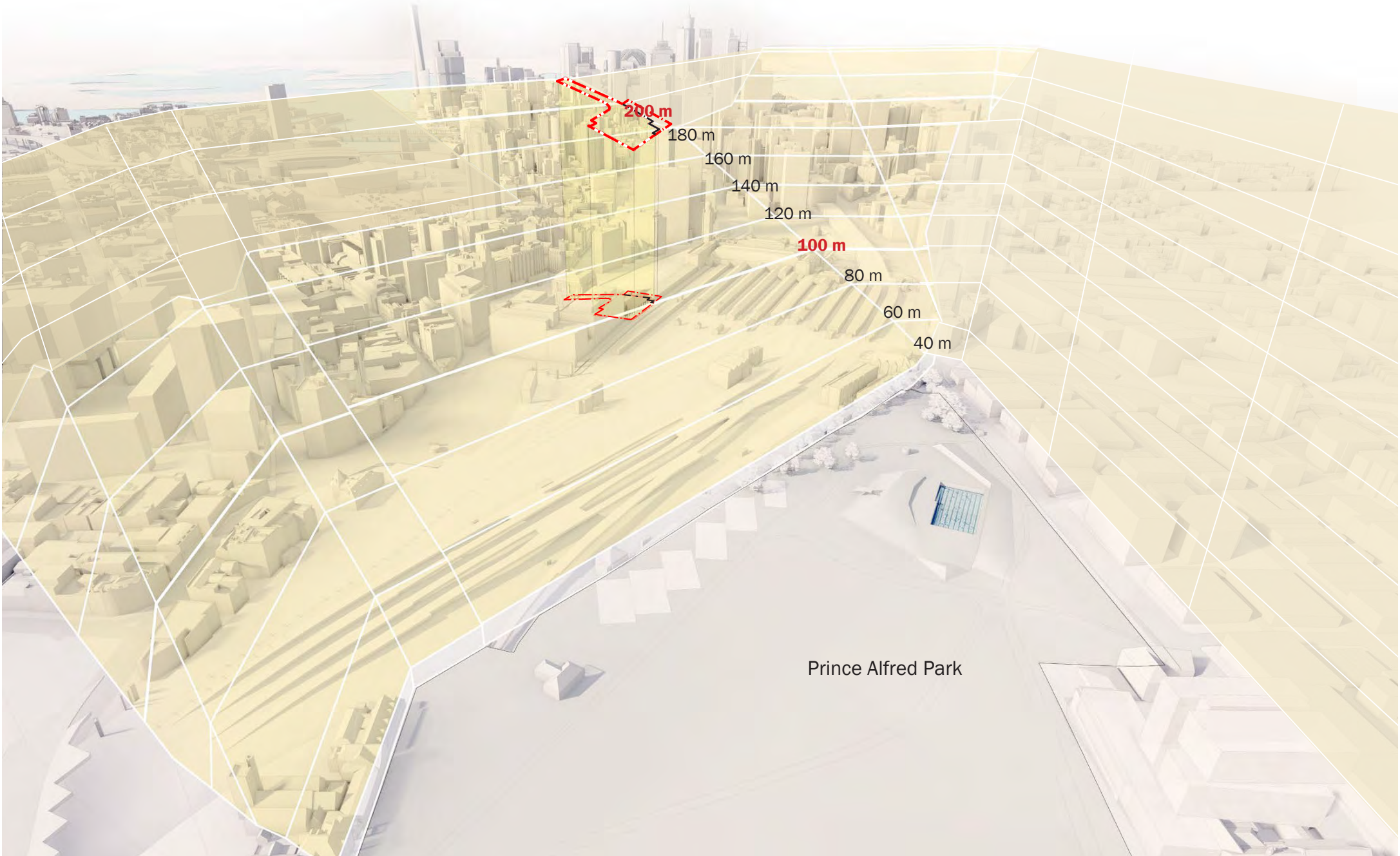
2.0 CONTEXT & SITE ANALYSIS

The City of Sydney’s Draft Central Sydney Planning Strategy Solar Access Contour Map allows for low-scale buildings to be located along the disused rail-siding fronting Prince Alfred Park. This would have the benefit of increasing the safety of Prince Alfred Park at night time. The proposed envelope conforms to these controls.

The mapping of the Sun Access Plane as defined by the City of Sydney’s Draft Central Sydney Planning Strategy in 3D is represented adjacent with the site indicated with the measures in RLs.

The low-scale built form fronting Prince Alfred Park with a 20m high frontage which aligns to the current LEP controls. This height also corresponds with the existing tree line (not shown here).

Detailed solar analysis studies including the Prince Alfred Park can be found in the Appendix C Solar Access Analysis document.



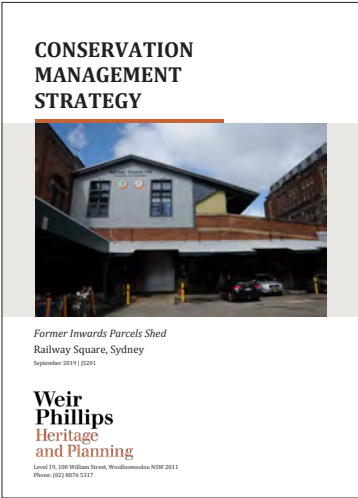
**3.0
HERITAGE
PRINCIPLES**



CONSERVATION PRINCIPLES

The Conservation Strategy embraces the following principles:

- 1. A site-specific Conservation Management Plan (CMP) should form the basis on which to guide future work on the site.
- 2. New development must retain significant fabric and provide for its conservation and maintenance.
- 3. The interface between old and new must respect elements such as original fabric, windows and door openings.
- 4. Promote and communicate the significance of the site through interpretation.
- 5. Integrate new development in a way that respects and responds to the significance of the item and minimises impact on the item.
- 6. Large scale details, sections etc., clearly demonstrating the interface of old and new to be prepared and submitted as part of a future consent.
- 7. Continuation of the heritage listing on the State Heritage Register (SHR) and the RailCorp Heritage & Conservation register (s170) as part of the Sydney Terminal and Central Railway Station Group listing.



Conservation Management Strategy
Former Inwards Parcel Shed, Railway Square, Sydney, September 2019, Section 6: Conservation Management Strategy and Guidelines

GENERAL HERITAGE GUIDELINES

WHAT NEEDS TO BE CONSERVED?

The following elements of the site have been assessed to be of heritage significance and are recommended to be retained in light of the current proposal to achieve best conservation outcomes:

- 1. An understanding of the overall form of the former Parcels Shed.
 - 2. The timber roof trusses and roof structure.
 - 3. The timber columns and struts.
 - 4. The masonry infill panels and chimneys.
- These can be de-constructed and reconstructed for ease of tower construction.
- 5. An understanding of the canopy along the western elevation of the building.
 - 6. Conservation of the Ambulance Avenue wall where possible, in conjunction with future through site links.

WHAT CAN BE CHANGED?

Exteriors

The c. 2004 and later alterations and additions to convert the building to a youth hostel can be changed. The removal of the dining and lounge area on the western side of the shed and the swimming pool can occur without adverse impact. The corrugated metal sheeting for the roof and walls can be replaced, although their replacements should be like for like and have a distinct relationship to the original structure in terms of its use. The aluminium windows can also be changed as these are modern additions. The pattern of openings should remain in similar numbers and proportions to the existing openings.

Any penetrations of the structural core should to maintain an understanding of the simple low-pitched gabled roof of the site.

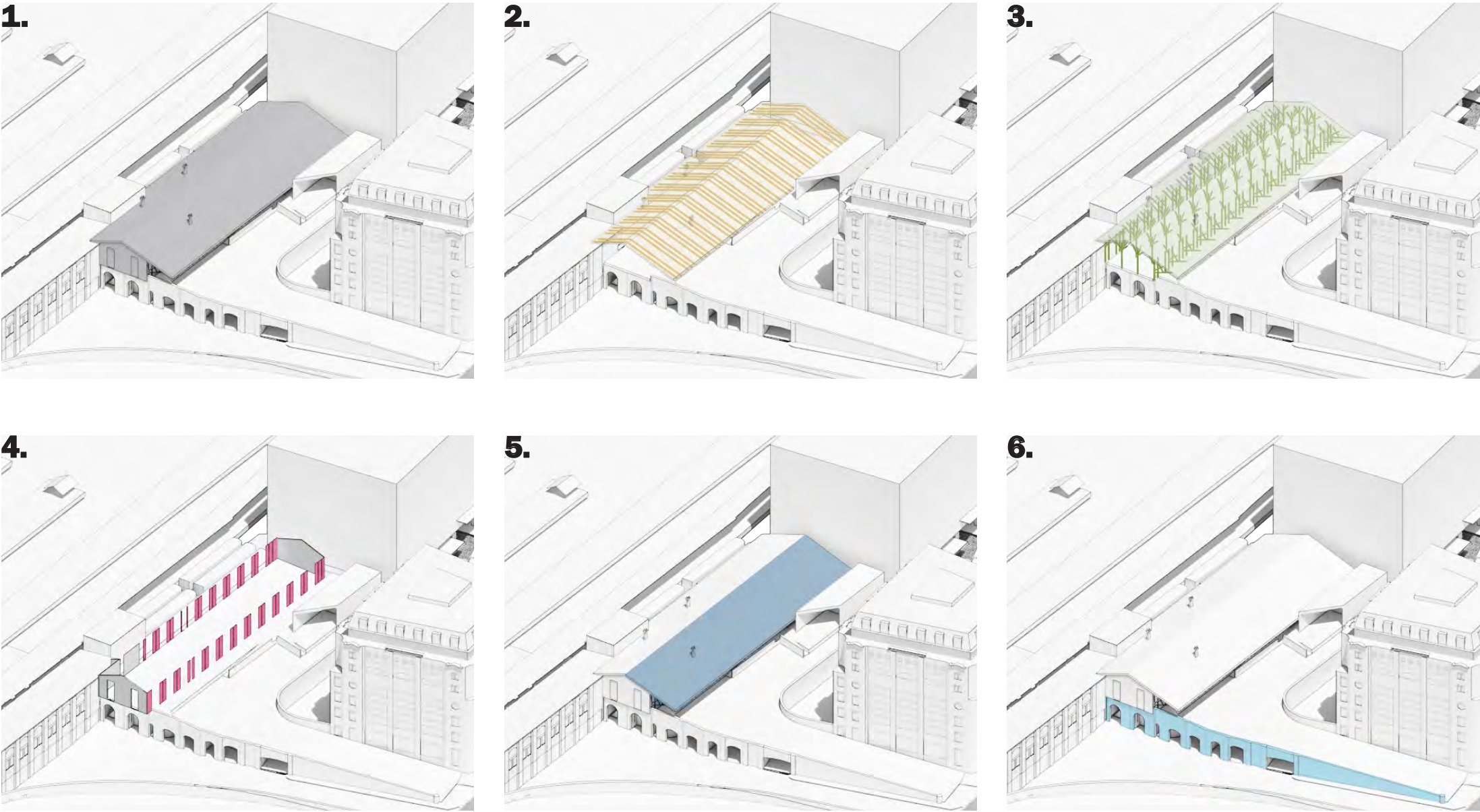


Diagram Series Showing Shed Heritage Preservation Elements

Interiors

The former Inwards Parcels Shed has undergone extensive remodelling of the interior following its conversion into a youth hostel in c. 2004.

There will be no adverse impact to the significance of the building if any of the 21st century additions and alterations are removed. Where possible, the c. 1906 fabric should be retained and conserved, specifically the timber

columns and roof structure and masonry including the fireplaces.

APPENDIX A - URBAN DESIGN PLANNING REPORT

3.0 HERITAGE RESPONSE

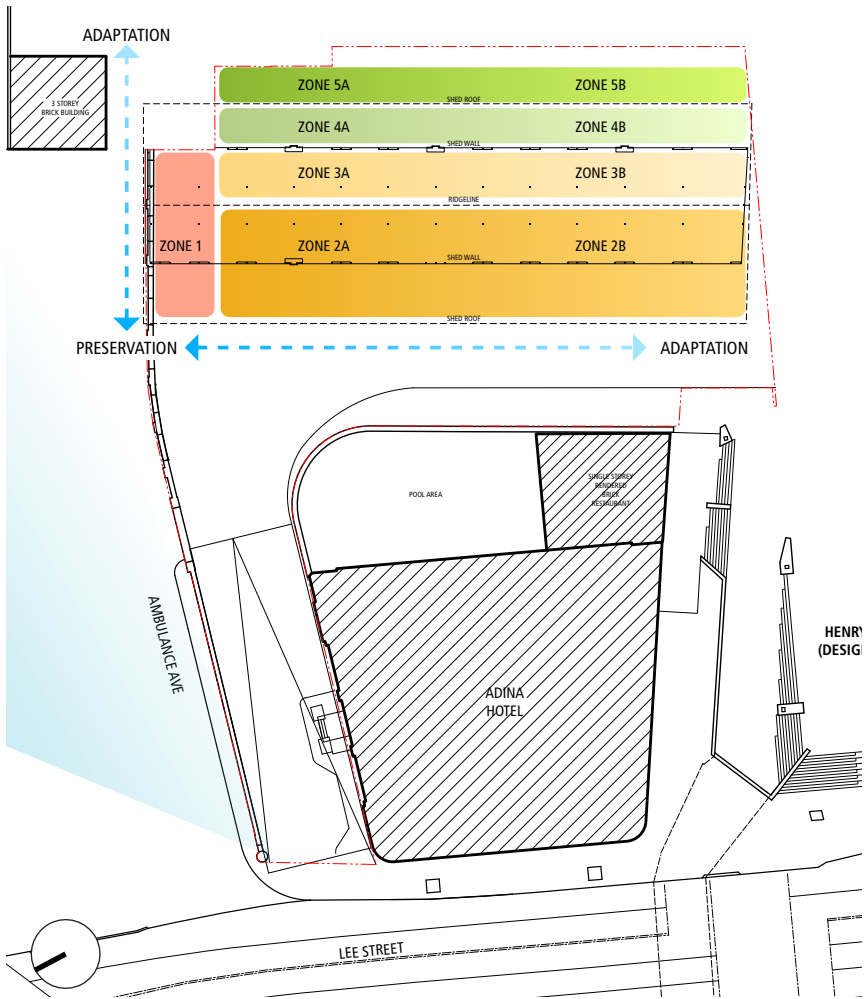
GENERAL GUIDELINES

The following general guidelines are provided:

- The views to and from the former Inwards Parcels Shed, in particular the iconic view from the Western Forecourt.
- An understanding of the overall form and structure of the former Inwards Parcels Shed.
- The degree of separation between the ridge of the existing building and the underside of the proposed tower above as well as the adjoining former Parcels Post Office (Adina).
- The provision of a structure to support a tower above should not interfere with the ability to read the architectural form and original function of the former Inwards Parcels Shed.
- Demonstrate an understanding of the bulk, scale and materiality of the former Inwards Parcels Shed and Basement area on Ambulance Way. There is an opportunity to penetrate Ambulance Avenue to facilitate the introduction of a through site link.
- The opportunity to interpret the history, in particular the intangible heritage of site as part of a comprehensive and coordinated heritage interpretation plan for the wider Central Station site.

HIERARCHY OF ZONES OF PRESERVATION

Informed by the Conservation Management Strategy, the following diagram represents a hierarchy of zones for preservation and adaptation. This can be summarized as a gradient from north-western corner towards interface with 'Platform 0' in the south-eastern corner.



ZONES OF PERMISSIBLE ADAPTATION

- Zone 01 Maintain 3/4 view from Railway Sq; adjacent to public access to over rail development.
- Zone 02 Within the shed, in front of the roof ridge line.
- Zone 03 Within the shed, behind the ridgeline.
- Zone 04 Outside the shed, but with in the roof zone.
- Zone 05 Behind the shed.

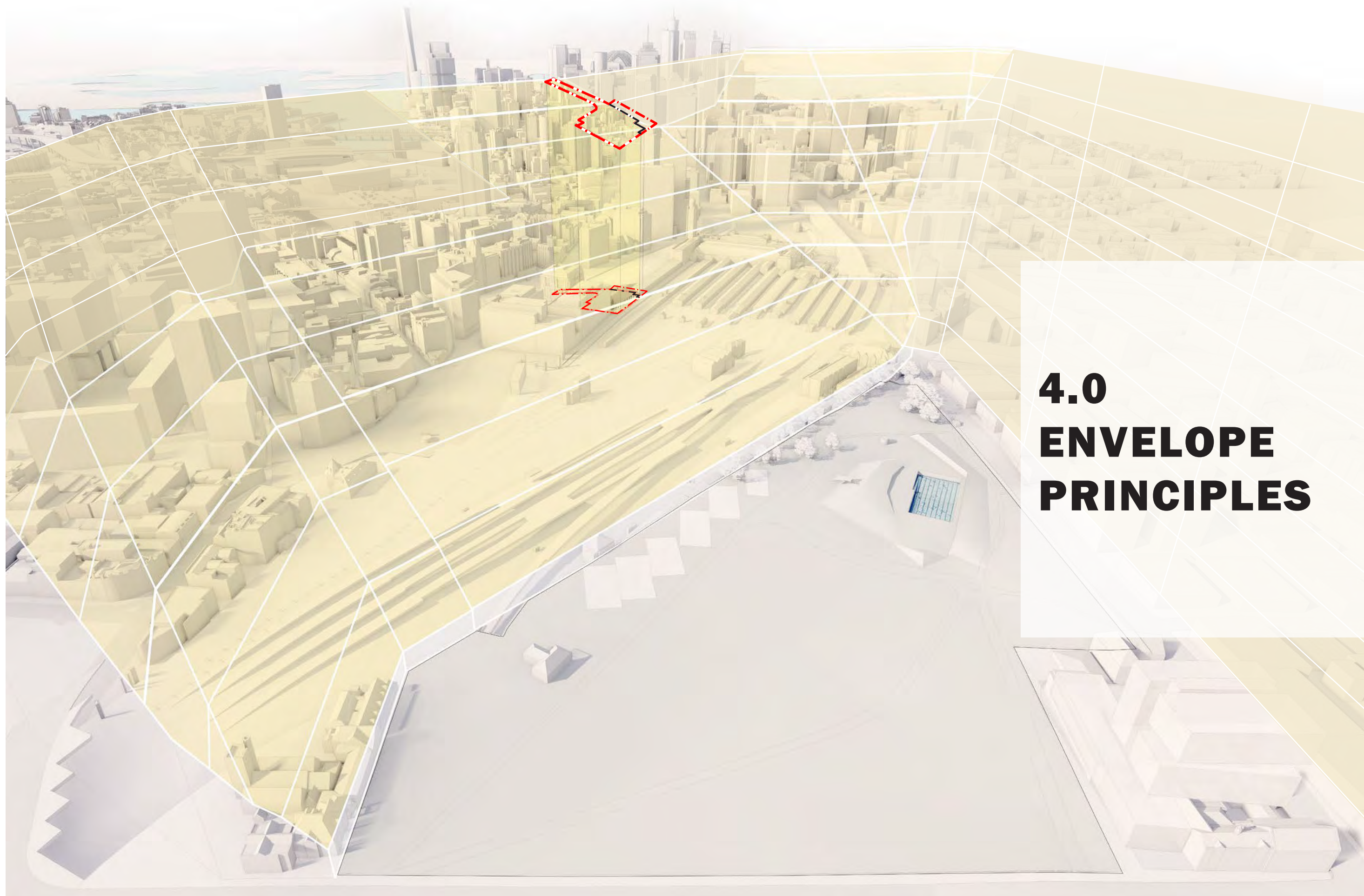
- Zone A Near ambulance avenue, to the north.
- Zone B Away from ambulance avenue, to the south.

Appropriate Outcome

- Preservation
- Adaptation
- Preservation
- Adaptation



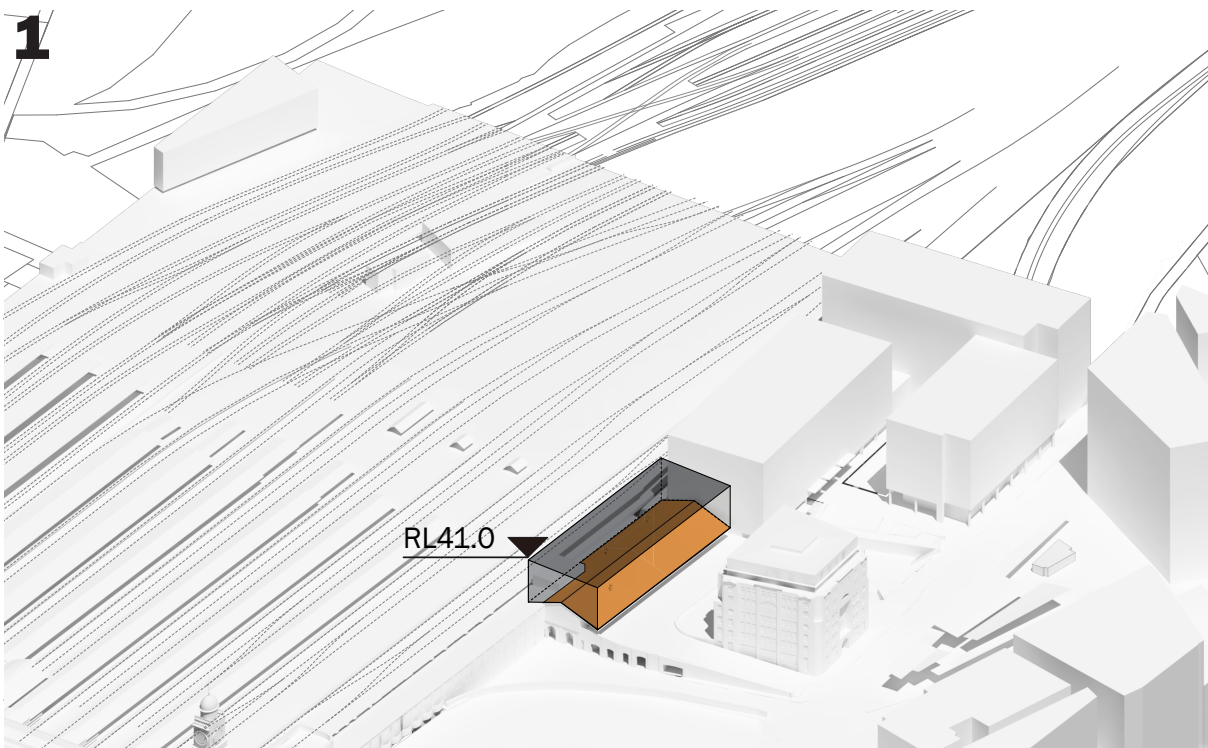




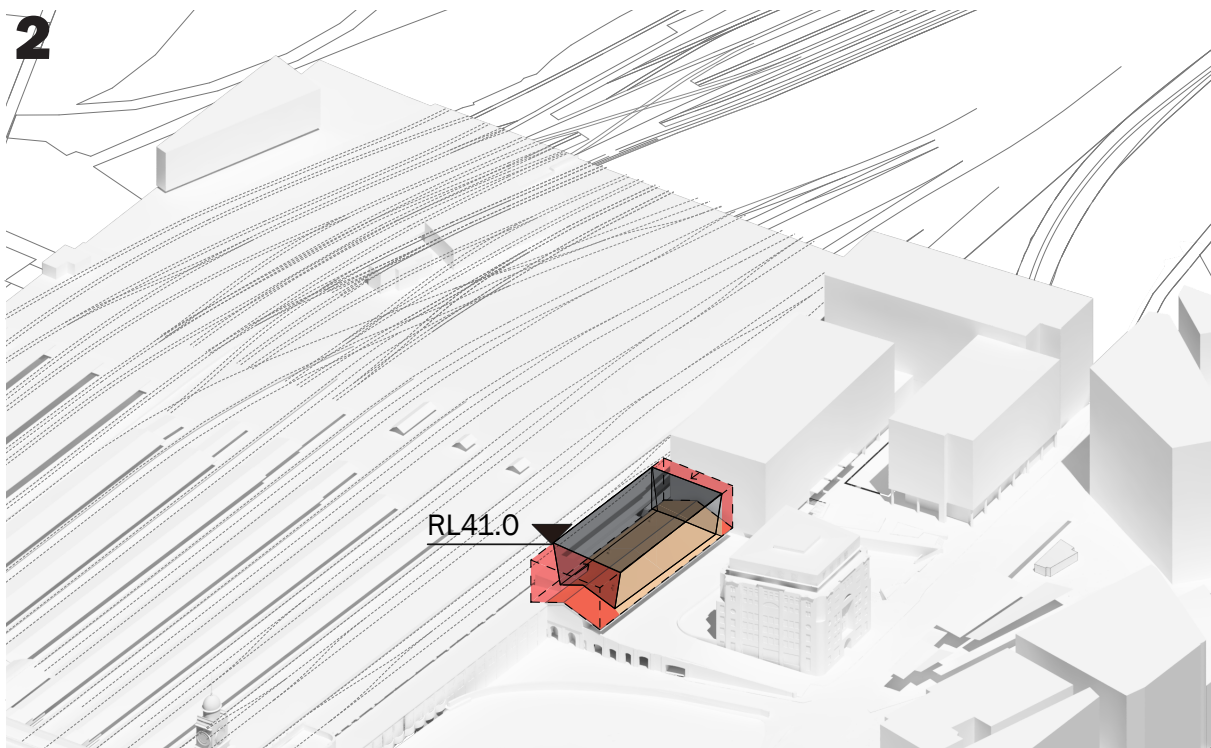
4.0 ENVELOPE PRINCIPLES

ENVELOPE PRINCIPLES

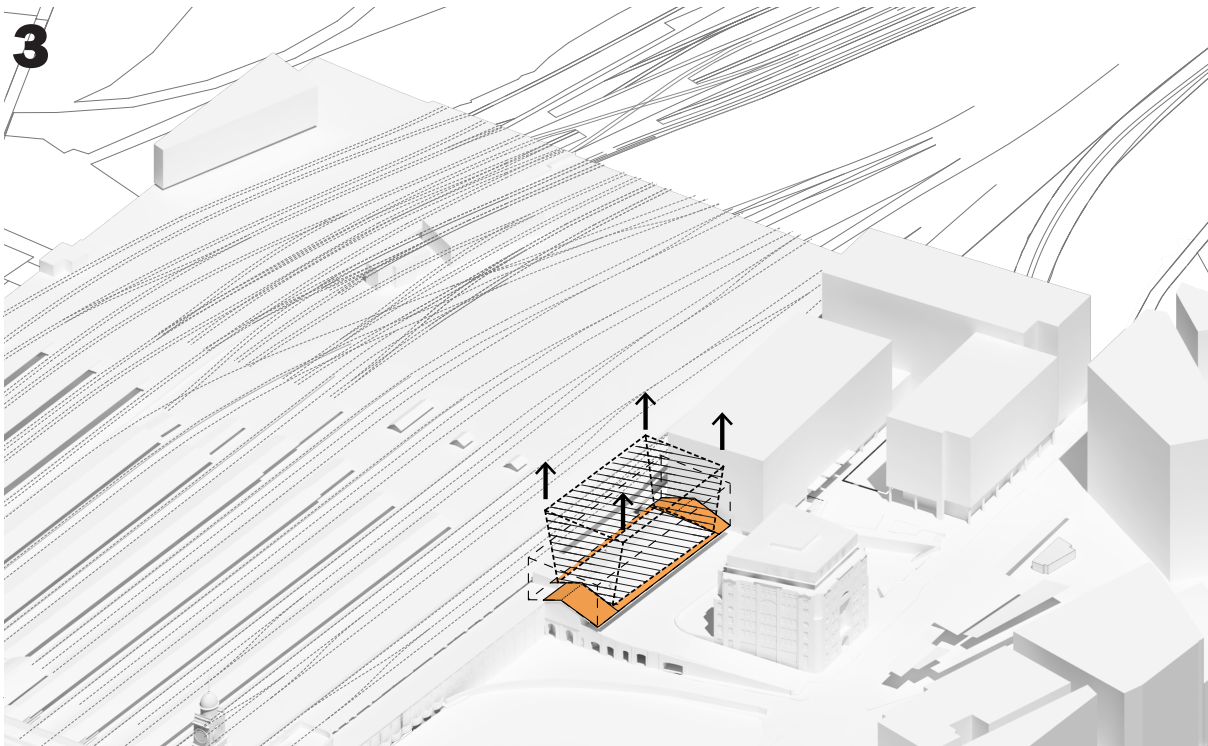
From the context and site analysis and the heritage principles a proposed envelope has been developed. The principles which are the primary drivers of the envelope are:



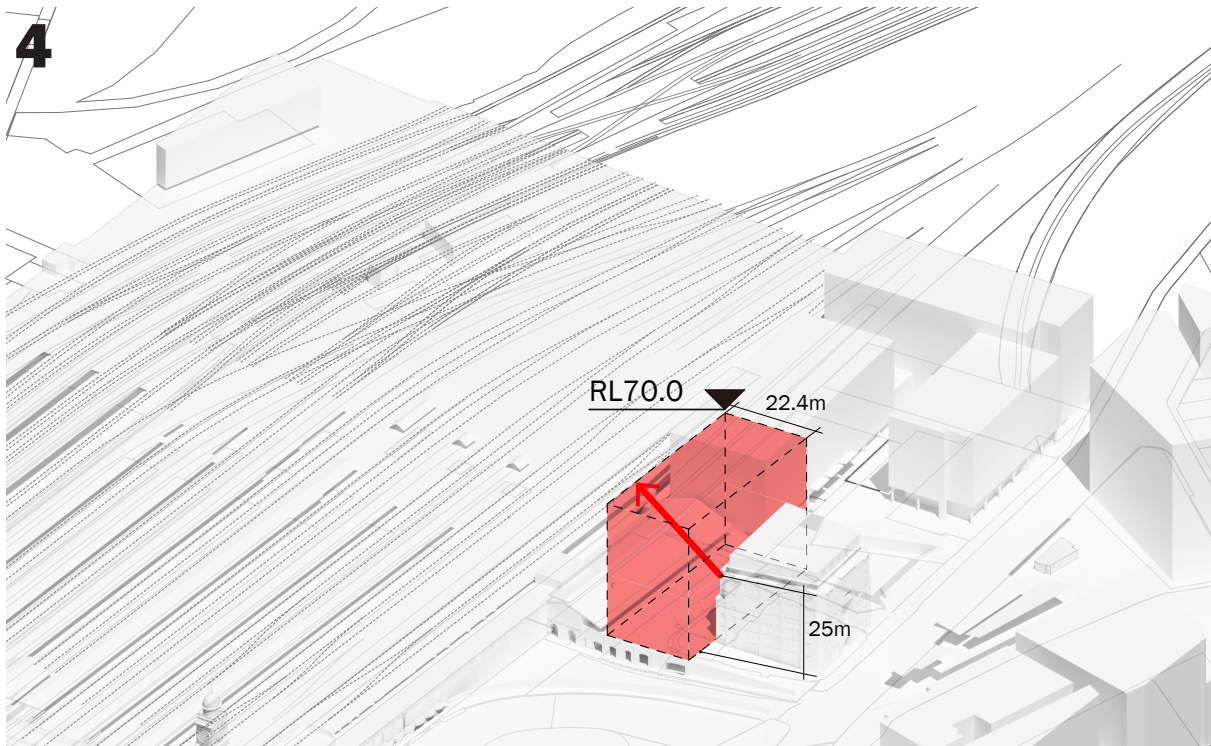
1. ESTABLISH CURTILAGE TO INWARDS PARCELS SHED
Establish “Curtilage” between ridgeline of former Inwards Parcels Shed and underside of main tower form: a minimum height of RL 41.0 is established above the ridgeline of former Inwards Parcels Shed.



2. INSET END ELEVATIONS OF INWARDS PARCELS SHED
Inset any built form elements between main tower and Inwards Parcel shed: at northern end to ensure legibility of 3/4 view maintained and at southern end to keep end of shed clear and avoid structural exclusions zones to Devonshire St tunnel.



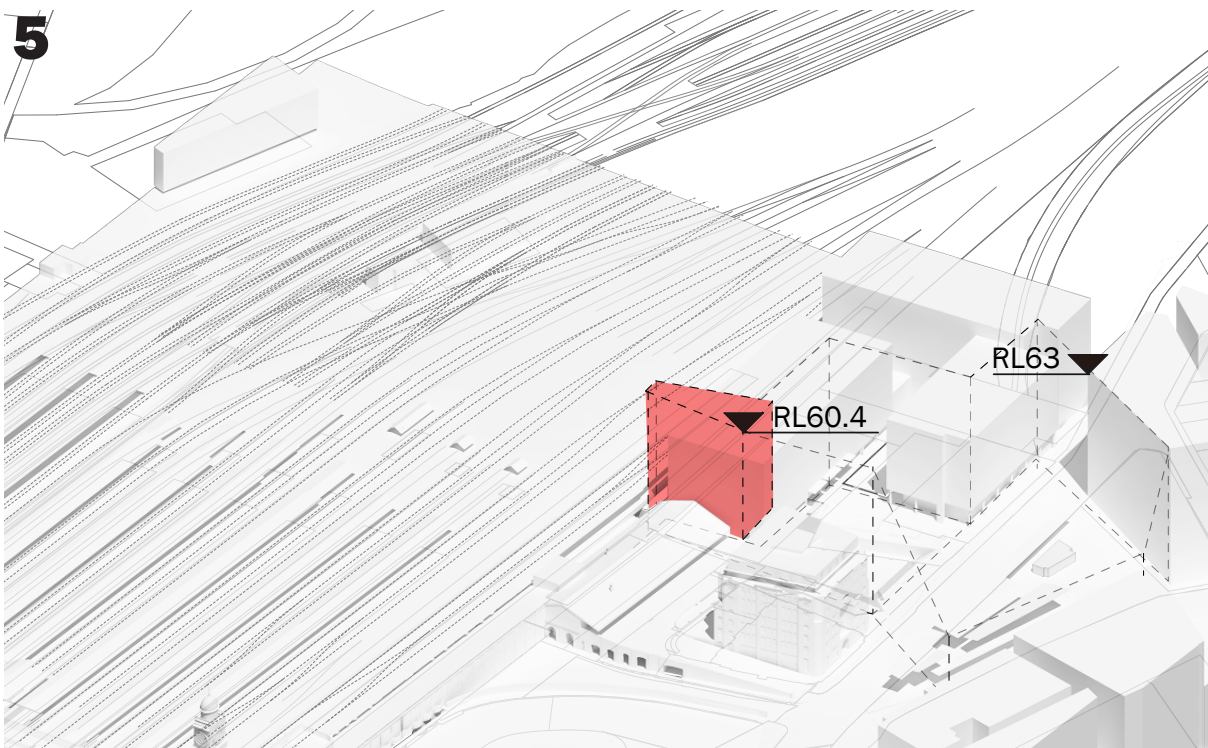
3. DEFINE CORE AND STRUCTURAL ZONE
Core and Structural zone positioned to not impact on public domain zones of north-south link. Core to be positioned to south and east within the remaining volume to maintain the integrity of reading of the north elevation and profile of the Inwards Parcels Shed.



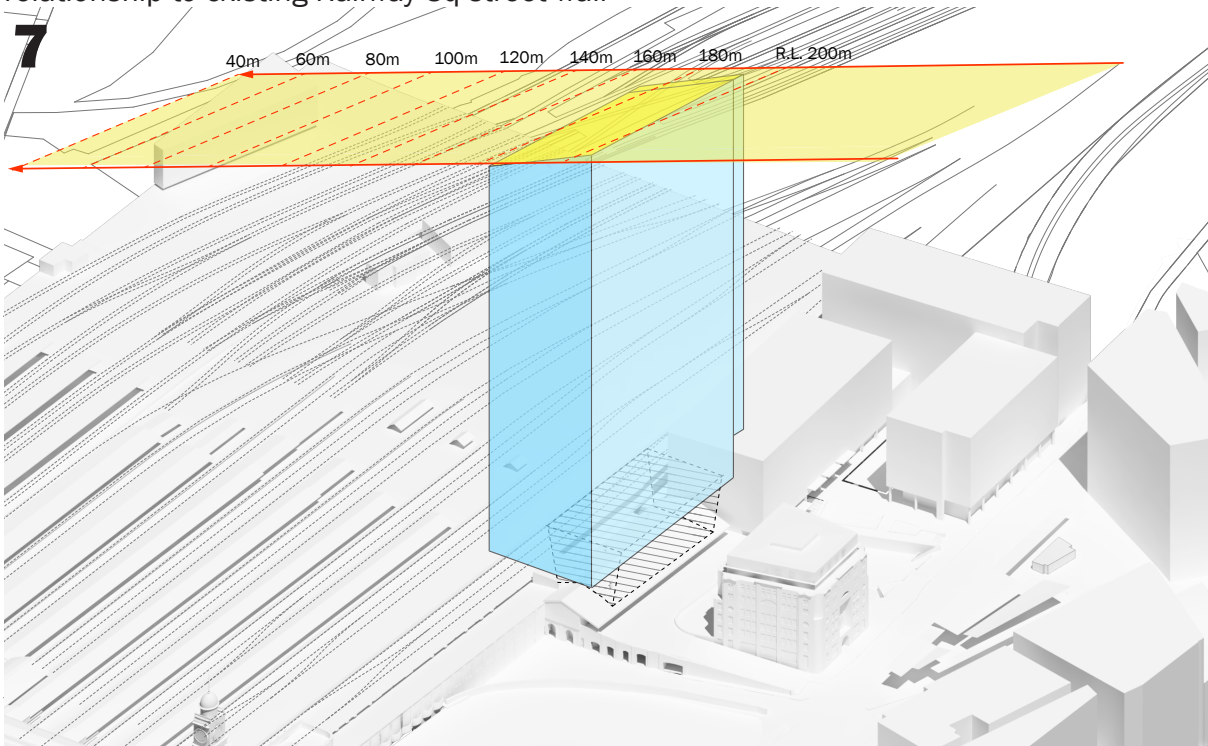
4. ESTABLISH CURTILAGE TO FORMER PARCELS POST OFFICE
Establish “Curtilage” to former Parcels Post Office (Adina): an average distance of 22.4m is established to maintain relationship between former Inwards Parcels Shed and Parcels Post Office (Adina).

APPENDIX A - URBAN DESIGN PLANNING REPORT

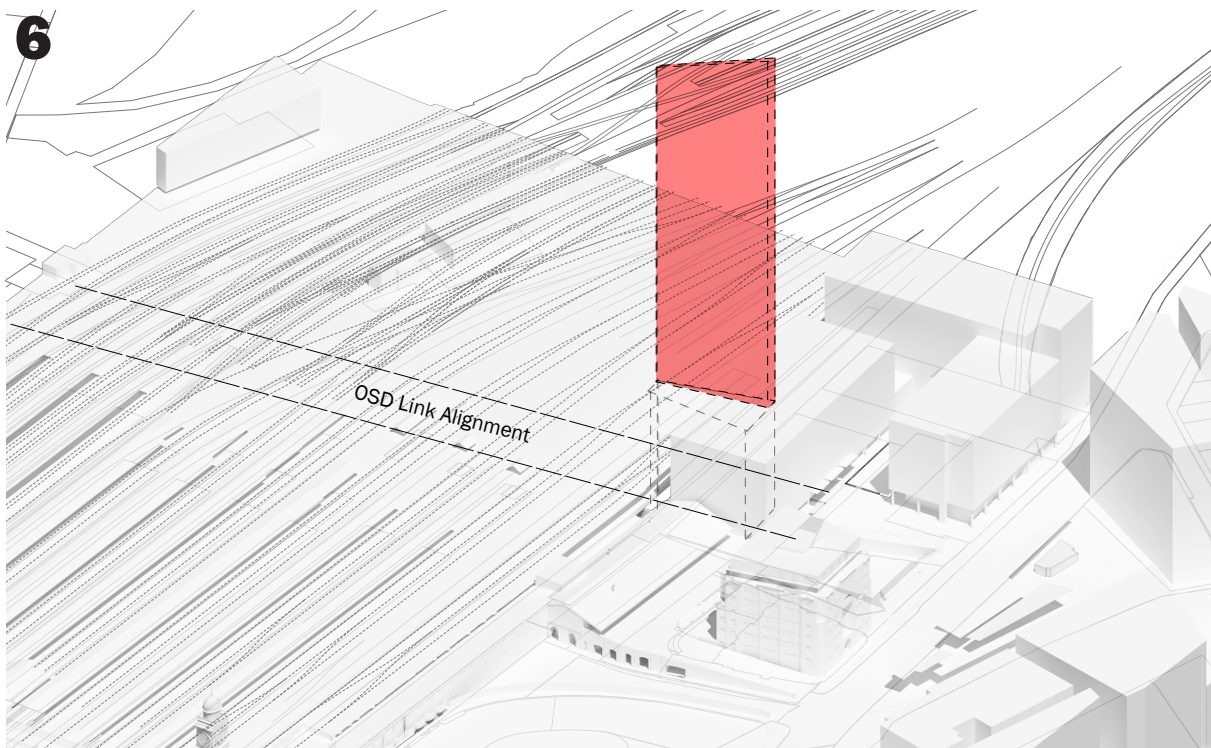
4.0 ENVELOPE DESIGN



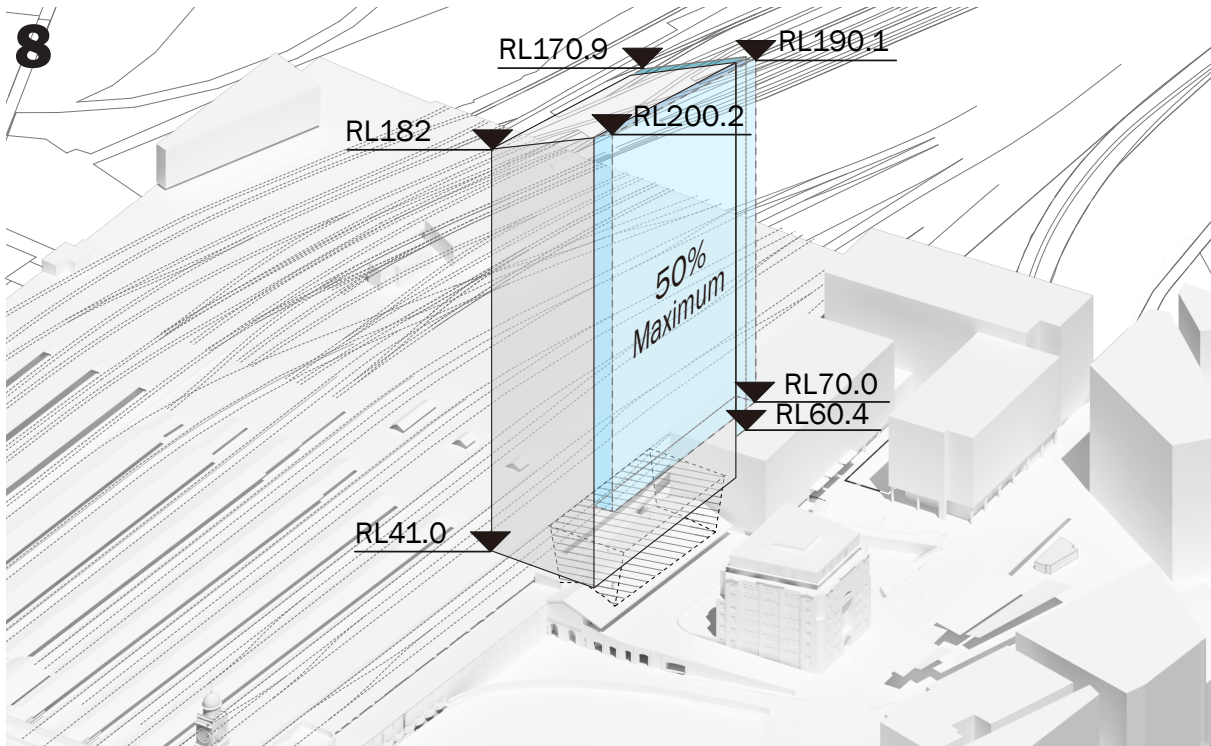
5. RESPOND TO FUTURE PUBLIC SPACE
Form a frontage that responds to the future public spaces of Henry Dean Plaza & Railway Sq at the western terminus of the OSD connection from Devonshire St. Aligned low rise tower to southern face of the former Parcels Posts Office. Low rise zone height defined in relationship to existing Railway Sq street wall



7. MAXIMUM HEIGHT TO SOLAR CONTOUR PLANE
The proposed Tower Envelope is extended in the site area above the other constraints to the maximum height defined by Prince Alfred Park Sun Access Plane.



6. OSD STREET FRONTAGE SEPARATION
Provide separation of high level built form at the western terminus of the OSD connection from Devonshire St on parallel alignment to the southern face of the former Parcels Posts Office.



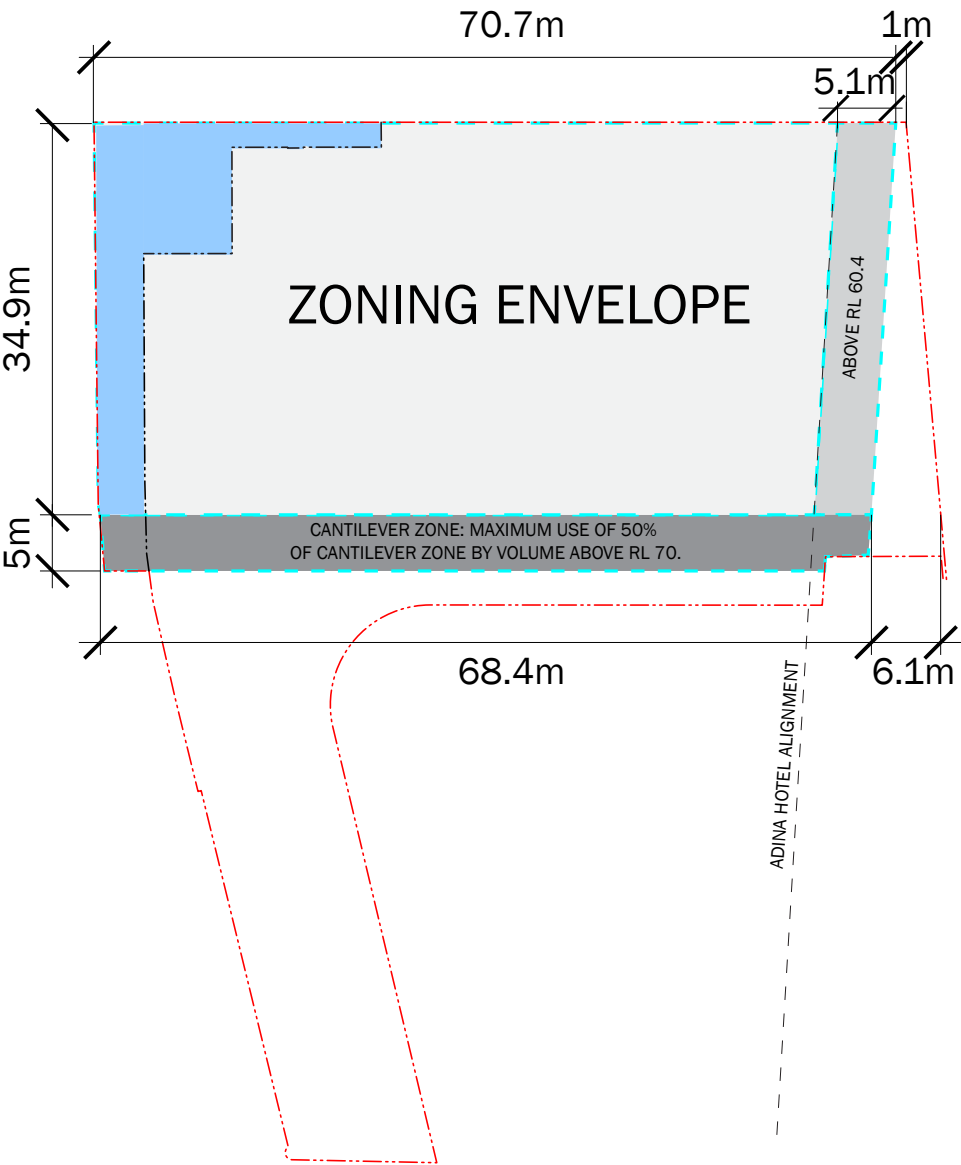
8. POTENTIAL WESTERN CANTILEVER & FUTURE ADINA DEVELOPMENT
A potential cantilever zone has been set back 3m from the western boundary in anticipation of future expansion of the Parcels Post Office on its eastern frontage. The cantilever zone is located 2x height of former Parcels Post Office (Adina Hotel at RL 70) with a maximum of 50% of the volume within the zone

BUILDING ENVELOPE

The building envelope is designed to respect the curtilage of the heritage elements adjacent whilst allowing maximum flexibility during a design competition process.

The envelope’s height conforms to the City of Sydney’s Draft Central Sydney Planning Strategy: Sun Access Plane for Prince Alfred Park.

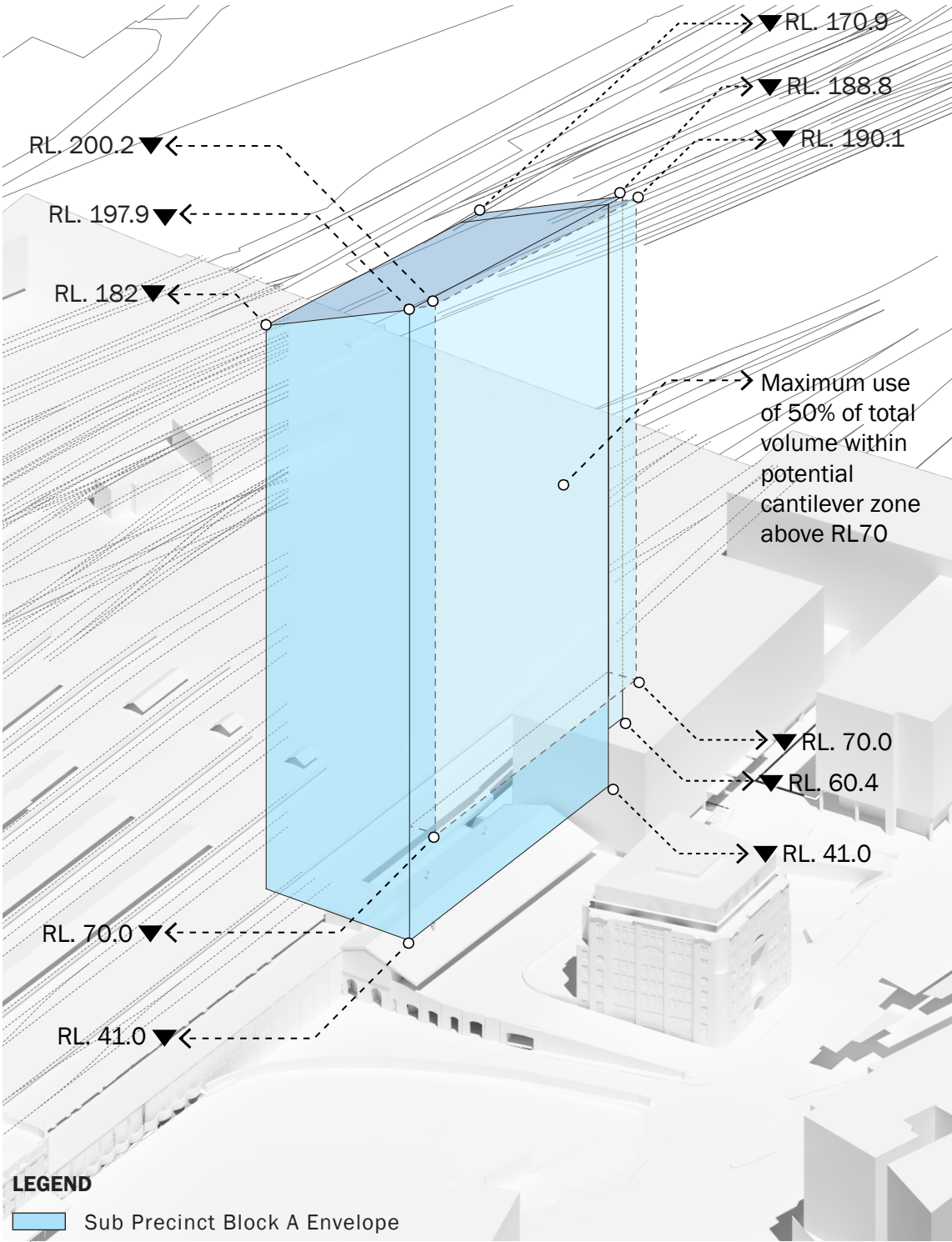
The adjacent image of the following page shows the potential envelope in the context of the existing city, and illustrates that the envelope is 100m below Center Point Tower.



LEGEND

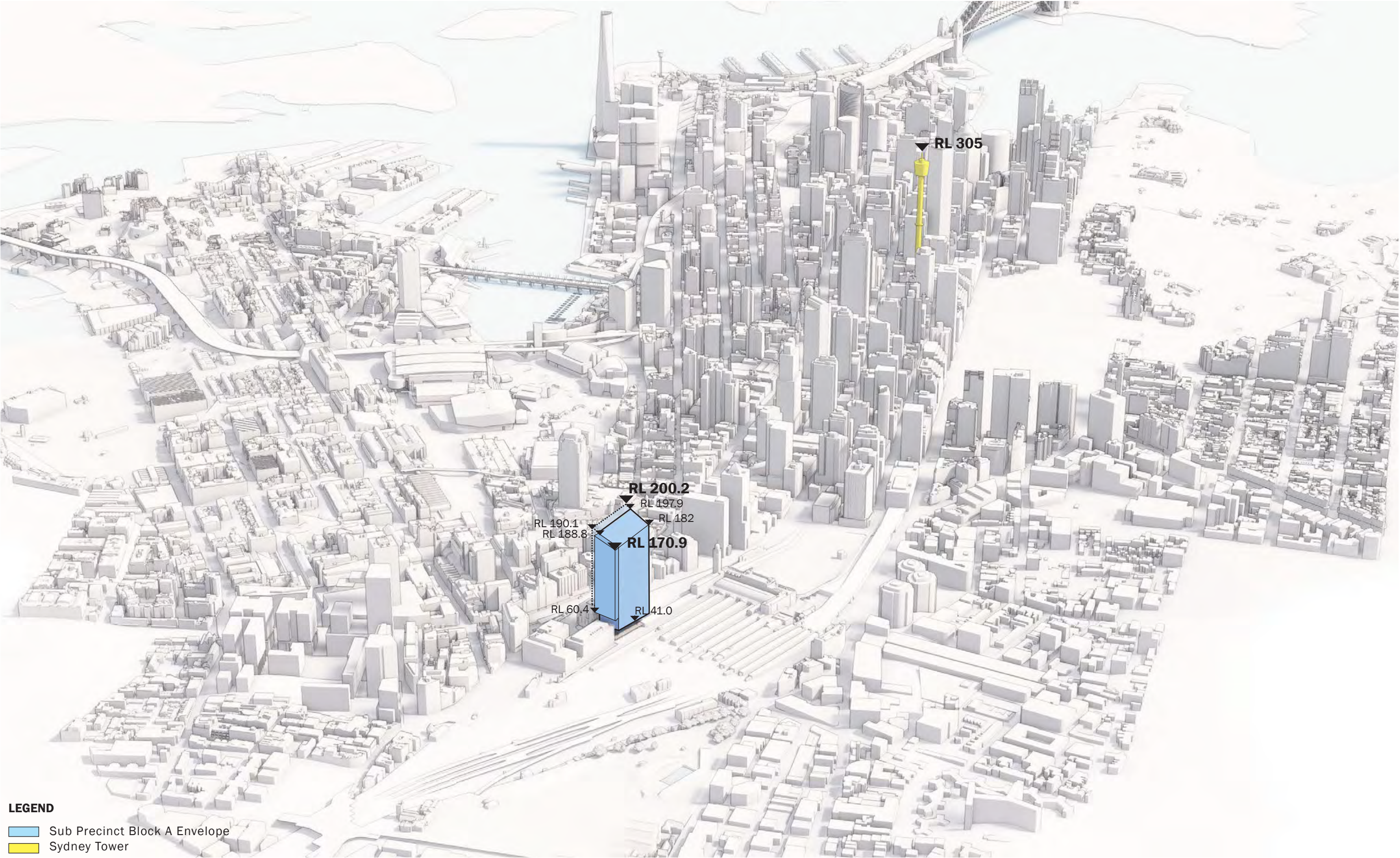
- Sub Precinct Block A Site Boundary
- Proposed Zoning Envelope
- Proposed Southern Cantilever Zone - 100% use
- Proposed Western Cantilever Zone - 50% use
- Proposed Air Rights Zone

The potential cantilever allows for flexibility in the design competition stage, and starts above the Adina Hotel.



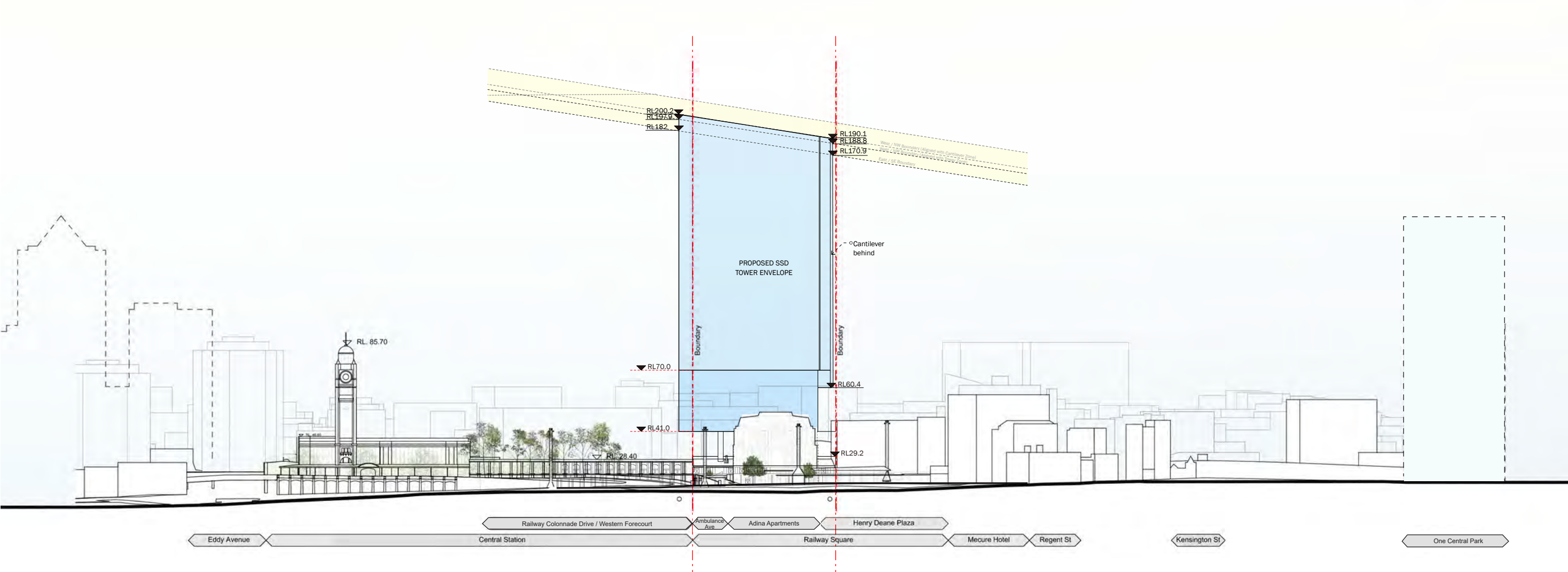
APPENDIX A - URBAN DESIGN PLANNING REPORT

4.0 ENVELOPE DESIGN



BUILDING ENVELOPE

STREET ELEVATIONS



PITT STREET TO GEORGE STREET ELEVATION

LEGEND

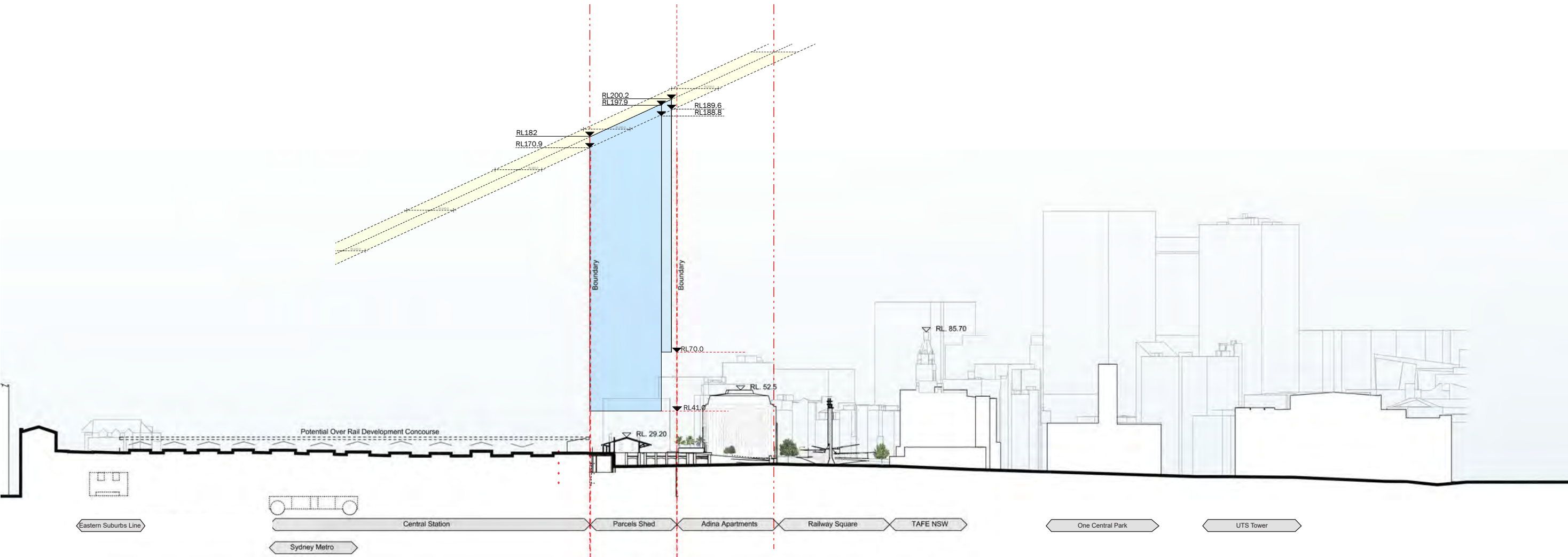
Sub Precinct Block A Site Boundary

Sub Precinct Block A Envelope

Prince Alfred Park Solar Plane

APPENDIX A - URBAN DESIGN PLANNING REPORT

4.0 ENVELOPE DESIGN



CONCOURSE - LOWER CARRIAGE LANE (AMBULANCE AVENUE) ELEVATION

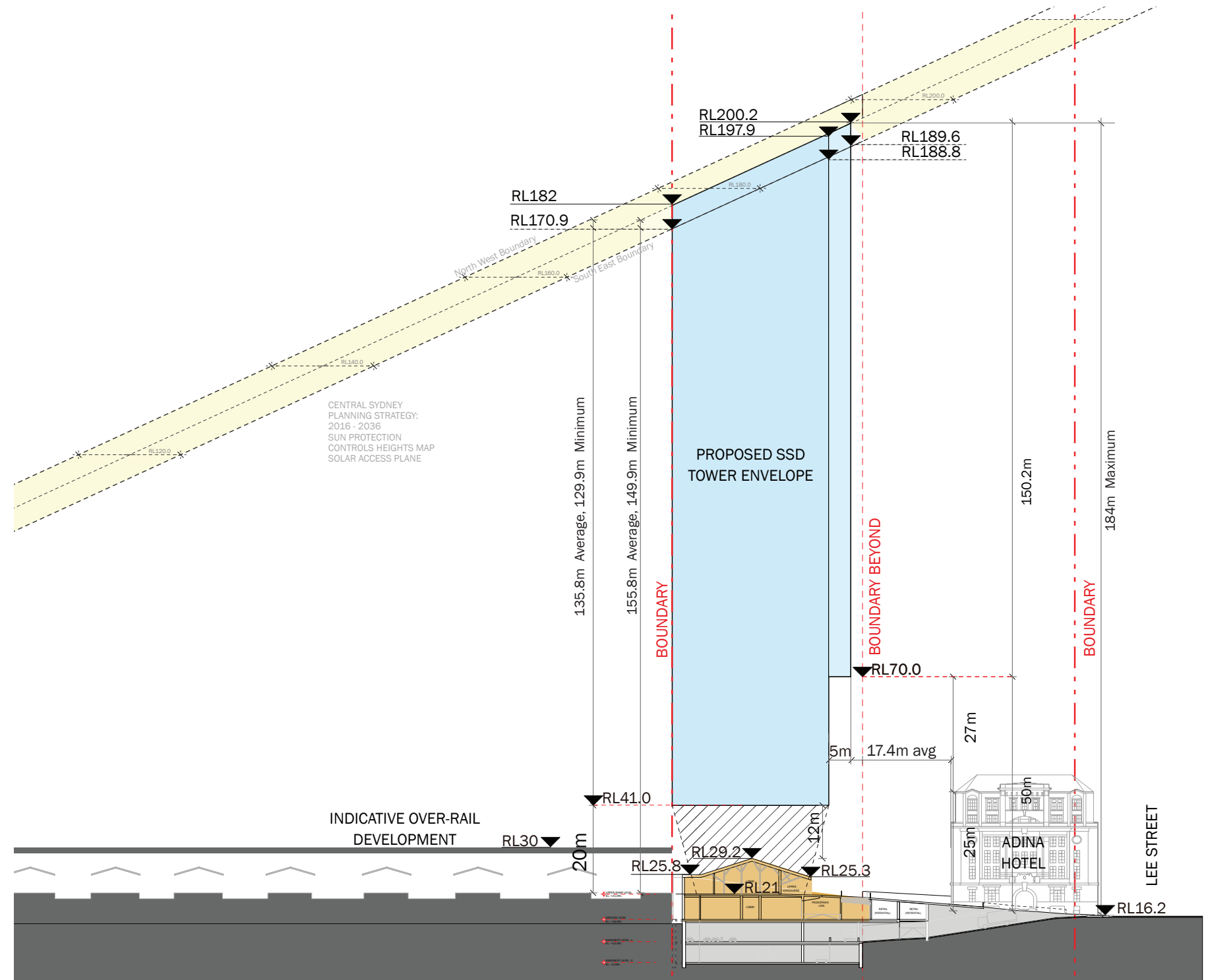
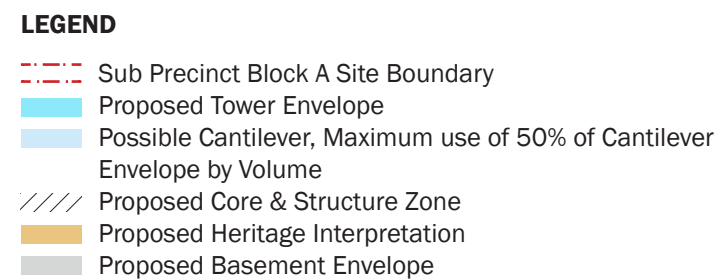
- LEGEND**
- Sub Precinct Block A Site Boundary
 - Sub Precinct Block A Envelope
 - Prince Alfred Park Solar Plane

BUILDING ENVELOPE SECTIONS

SECTION AA

The height of the tower is defined by the City of Sydney's Central Sydney Planning Strategy Sun Access Plane for Prince Alfred Park. This plane is not orthogonal to the Envelope, as such the lowest point is at the South East and the highest point is at the North West.

The section illustrates an indicative zone for a potential cantilever, which is limited to a minor proportion of the total volume of that zone. This allows for dynamic articulation moments whilst respecting the vertical curtilage to the Former Parcel Post Office / Adina Apartments.



APPENDIX A - URBAN DESIGN PLANNING REPORT

4.0 ENVELOPE DESIGN

SECTION BB

As the height of the tower is defined by the City of Sydney's Central Sydney Planning Strategy Solar Access Contour Map, the envelope slopes in both directions. The lowest point is at the South East and the highest point is at the North West.

LEGEND

Sub Precinct Block A Site Boundary

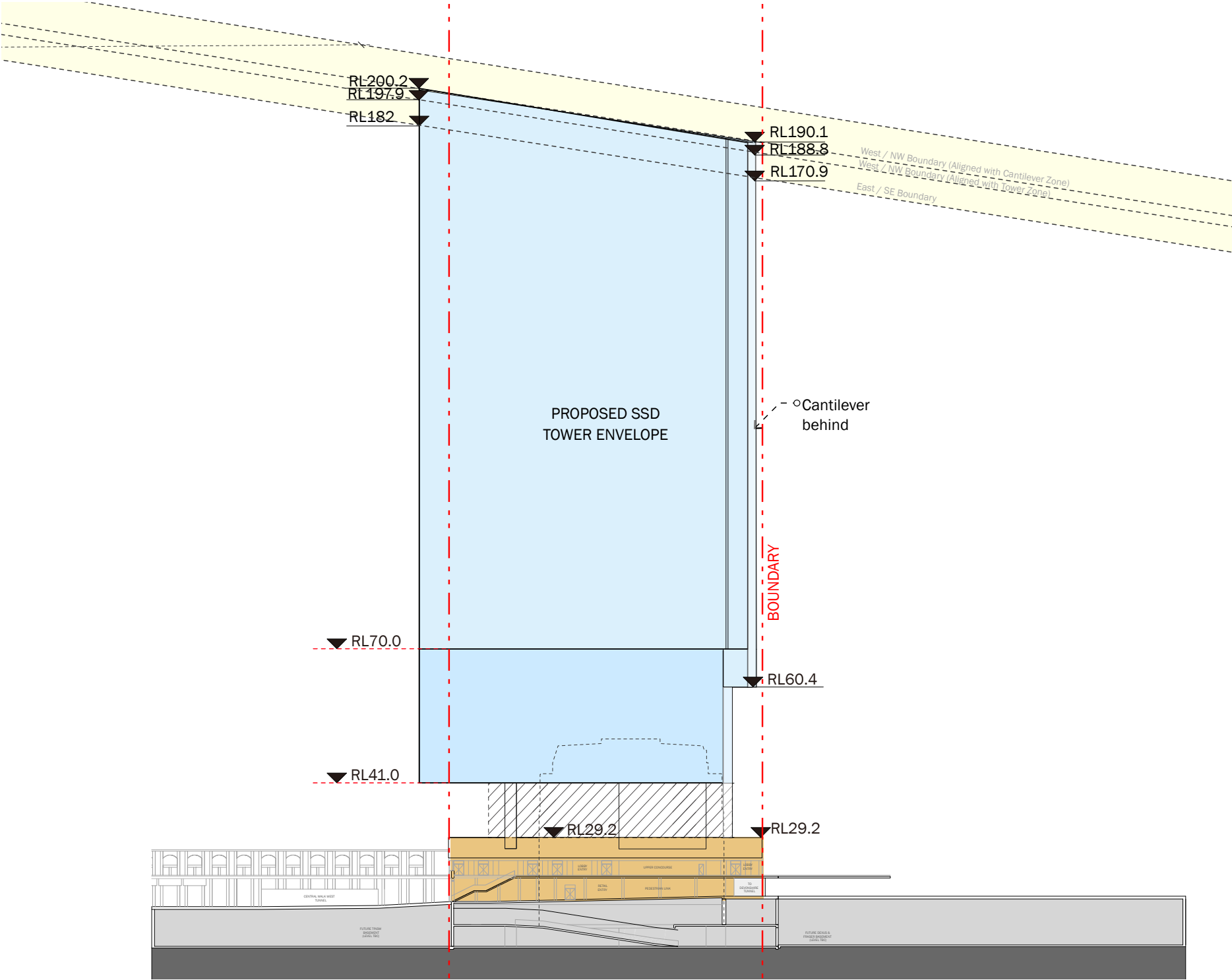
Proposed Tower Envelope

Possible Cantilever, Maximum use of 50% of Cantilever Envelope by Volume

Proposed Core & Structure Zone

Proposed Heritage Interpretation

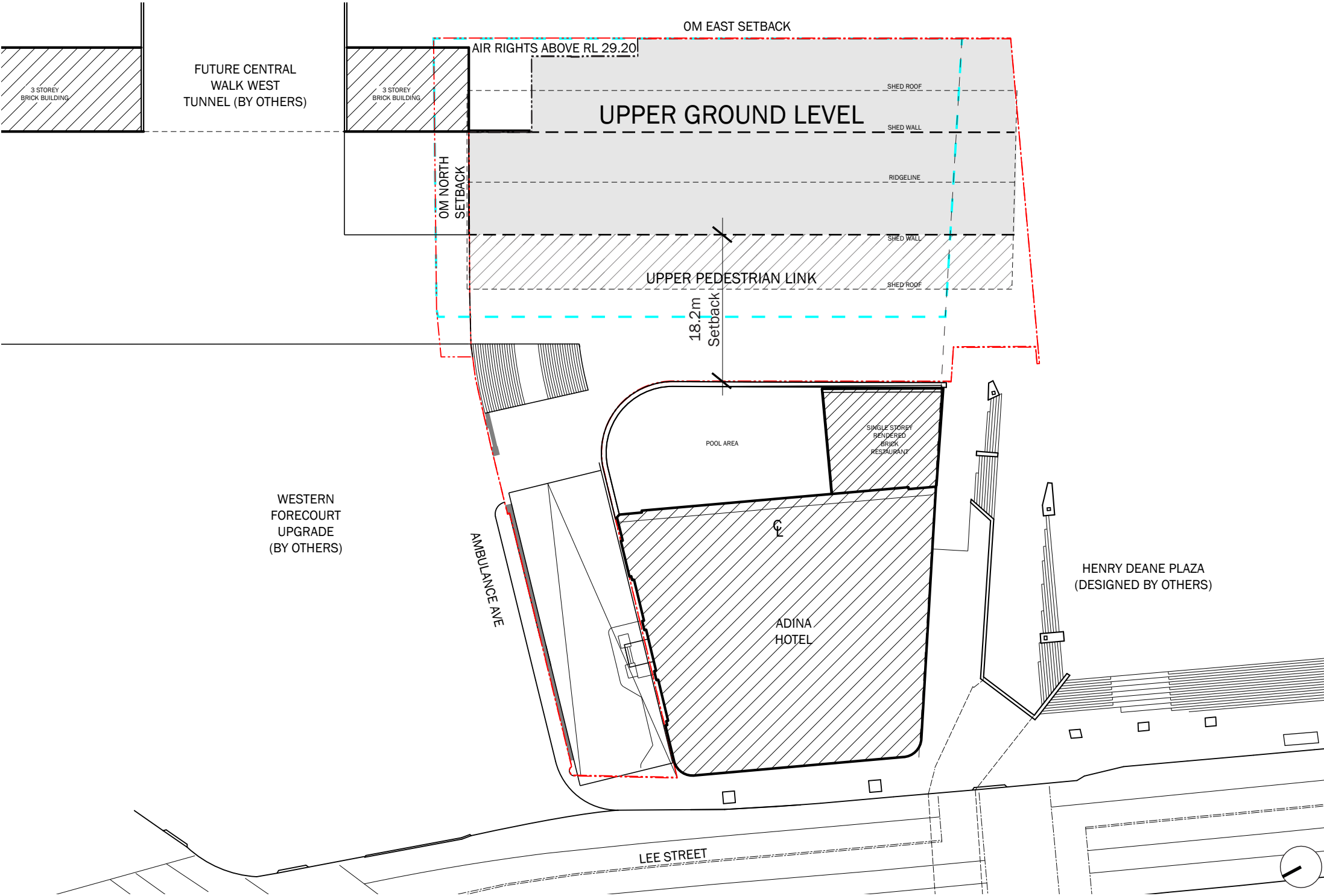
Proposed Basement Envelope



BUILDING ENVELOPE PLANS

UPPER GROUND

The Upper Ground level envelope is defined by the existing northern, western and southern faces of the existing Inwards Parcels Shed and the Lot 116 site boundary to the east.



APPENDIX A - URBAN DESIGN PLANNING REPORT

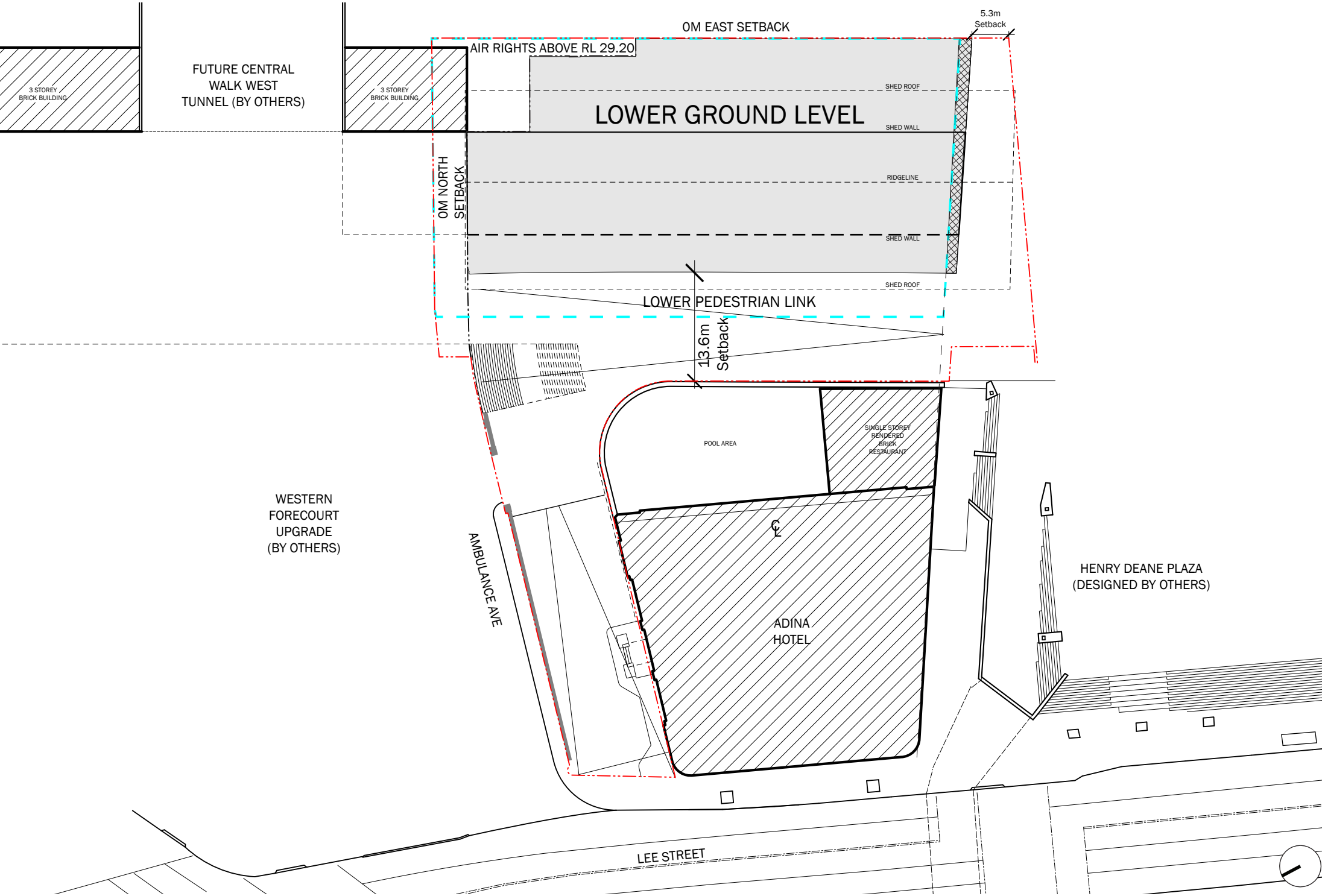
4.0 ENVELOPE DESIGN

LOWER GROUND

The Lower Ground envelope has been defined by the existing northern face, Devonshire Tunnel to the south and the Lot 116 site boundary to the east. The western extent has been determined based on allowances for pedestrian flows to operate north-south connecting:

- to the north the future Central Walk West and public domain zone
- to the south the existing Devonshire St tunnel and Henry Deane Plaza spaces

The set back allowance to the western envelope face to provide the public link zone is 13.6m from the existing boundary line to the Adina Hotel external areas.



APPENDIX A - URBAN DESIGN PLANNING REPORT

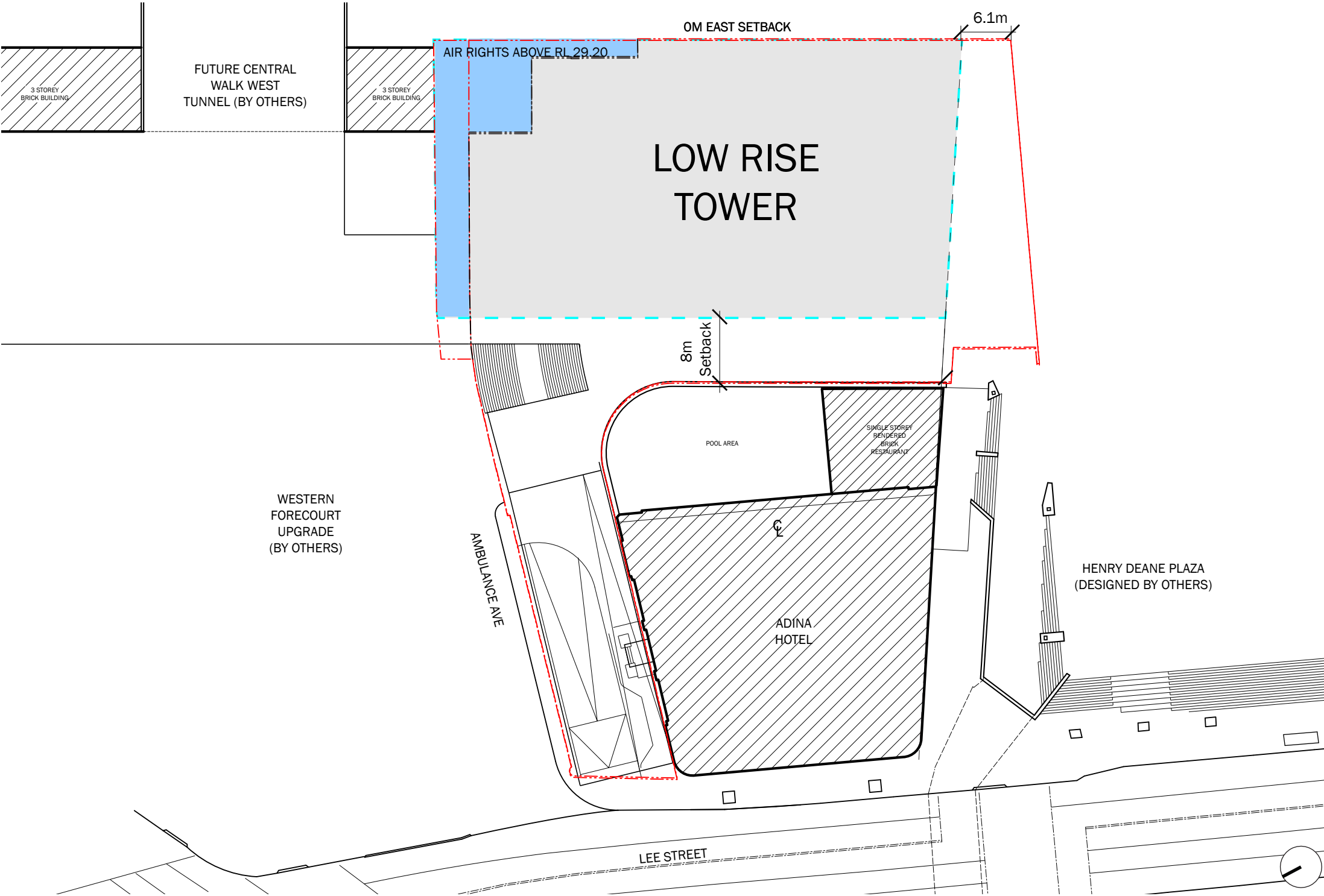
4.0 ENVELOPE DESIGN

LOW RISE

Lot 118 air rights are proposed to be acquired to allow for a potential regular / rectilinear floorplate.

The Low Rise Tower Envelope has an average of 22.4m setback to the former Parcels Post Office (Adina Hotel) eastern facade.

The southern alignment of the low rise tower up to RL60.4 is aligned to the southern face of the former Parcels Post Office (Adina Hotel).



APPENDIX A - URBAN DESIGN PLANNING REPORT

4.0 ENVELOPE DESIGN

HIGH RISE

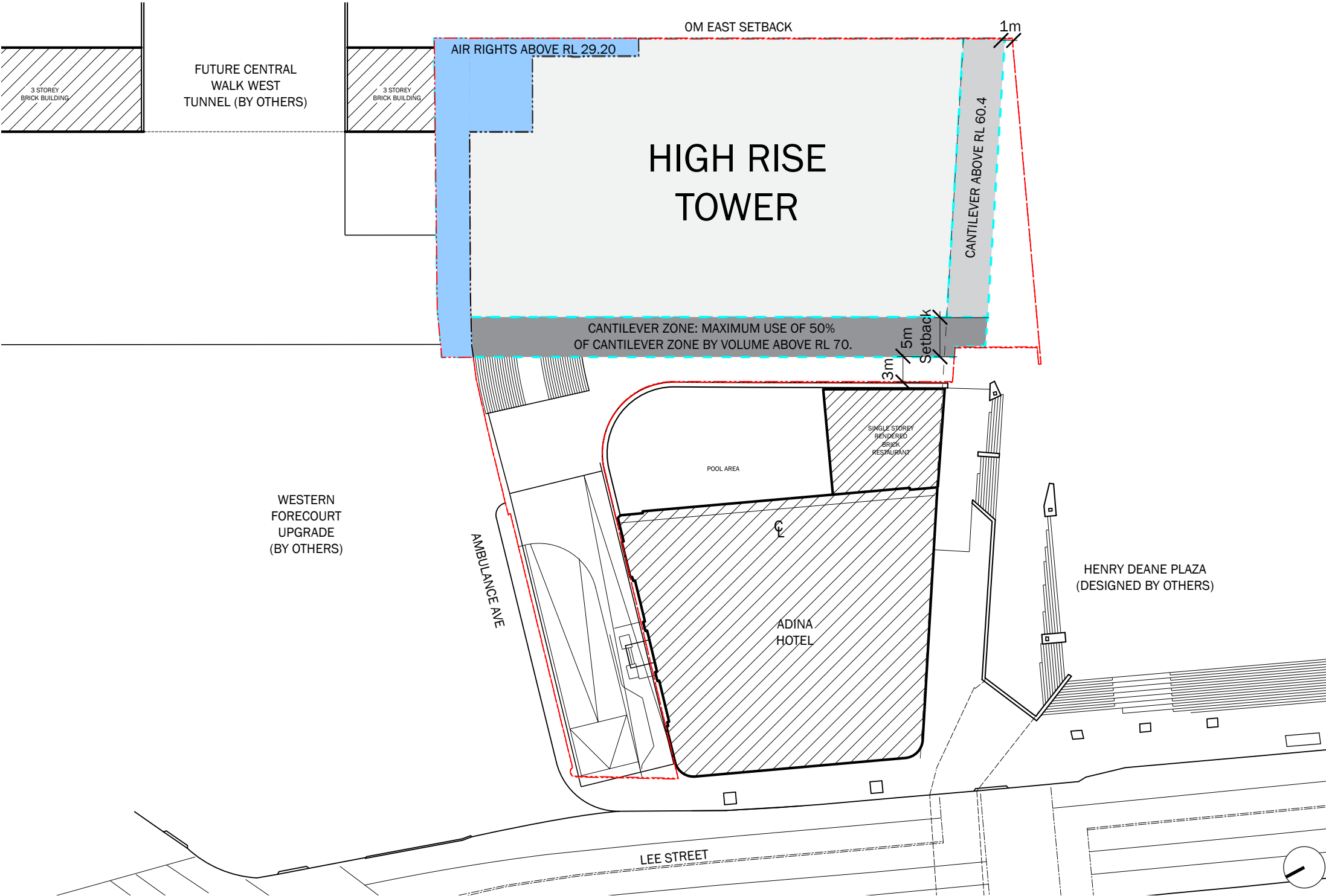
The high rise envelope aligns on the northern and eastern faces to the low rise tower envelope.

The southern alignment above RL60.4 is parallel to the southern face of the former Parcels Post Office (Adina Hotel) but inset in from the southern boundary to provide separation to Block B.

The high rise envelope allows for the potential use of a cantilever zone on the western side, up to a maximum of 50% of that volume. This envelope allows for flexibility in a completion, at an relative level above the curtilage zone desirable for the Former Post Office (Adina Hotel). The cantilever zone has been setback 3m from the western boundary to allow for the potential of an eastern extension to the Adina Hotel.

LEGEND

- Sub Precinct Block A Site Boundary
- Proposed Tower Envelope
- Possible Southern Cantilever
- Possible Western Cantilever, Maximum Use Of 50% Of Cantilever Envelope By Volume
- Air Rights Above RL 29.20



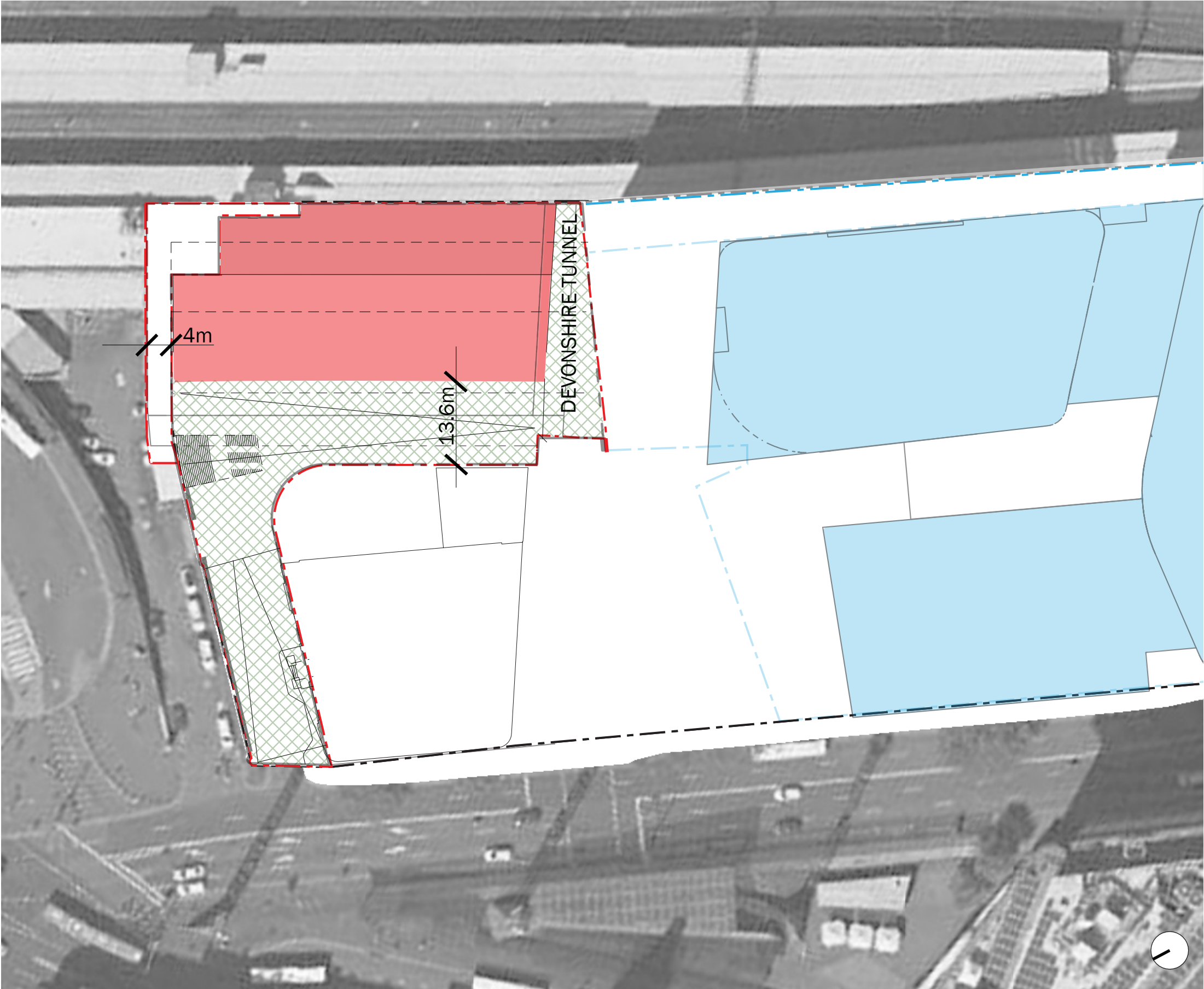


WESTERN GATEWAY PLANS

LOWER GROUND PLAN

The lower ground plan indicates the proposed built form at the street level and that of Devonshire St Tunnel. This indicates the built form of Block A which utilises the lower levels of the Former Inwards Parcels Shed zone in relationship with the envelope information provided by the Block B stakeholders.

- LEGEND**
- Block A Built Form
 - Block B Built Form
 - Block A Public Domain
 - Block A Site Boundary
 - Block B Site Boundary



Lower Ground Context Plan - Scale 1:1000 @ A3

APPENDIX A - URBAN DESIGN PLANNING REPORT

5.0 WESTERN GATEWAY

UPPER GROUND PLAN

The upper ground plan indicates the proposed built form at the RL 21 level which aligns to the existing level of the Former Inwards Parcel Shed. This plan indicates the built form of Block A which utilises the Former Inwards Parcels Shed zone and platform zone to the east in relationship with the envelope information provided by the Block B stakeholders.

LEGEND

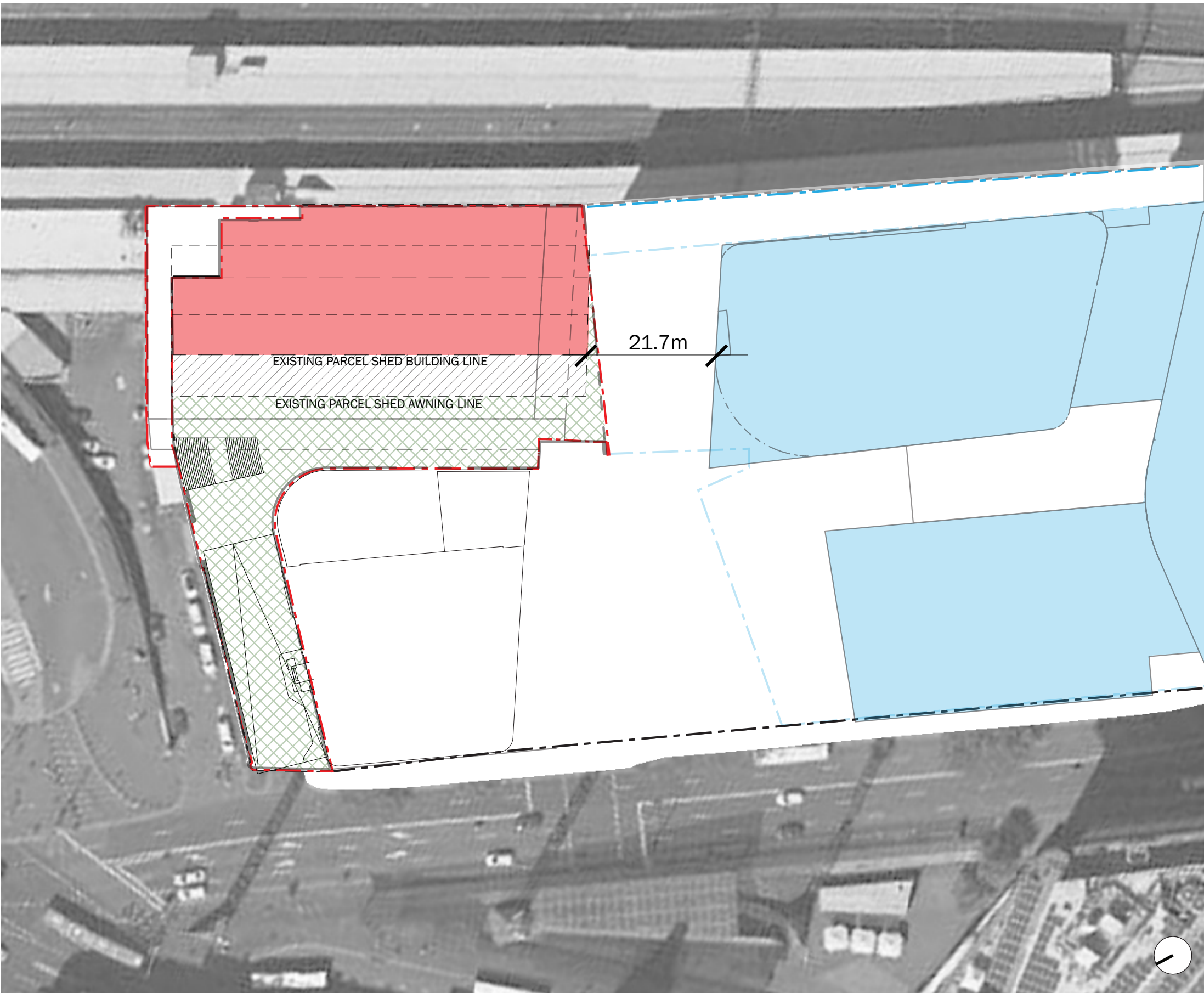
Block A Built Form

Block B Built Form

Block A Public Domain

Block A Site Boundary

Block B Site Boundary



Upper Ground Context Plan - Scale 1:1000 @ A3

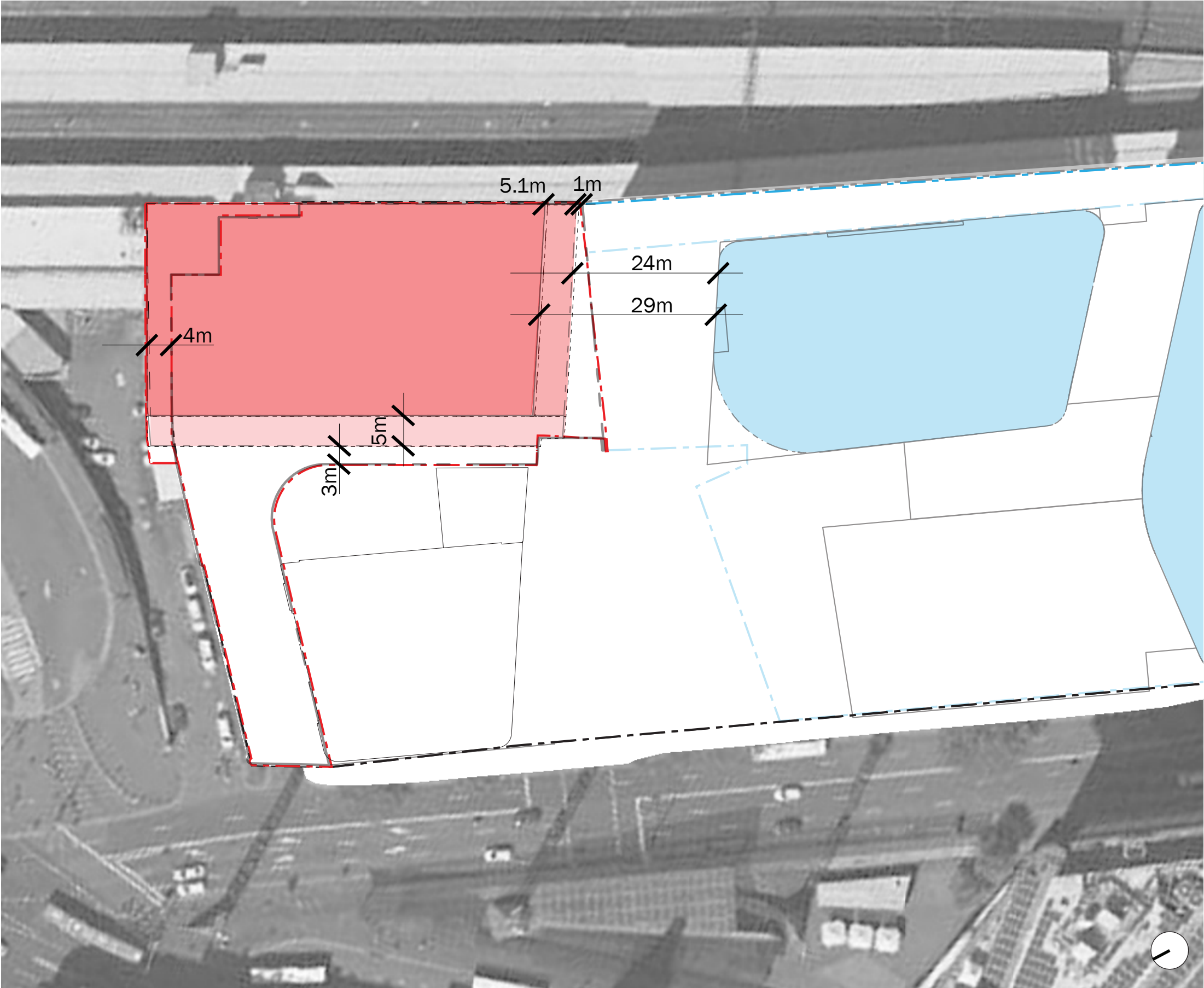
APPENDIX A - URBAN DESIGN PLANNING REPORT

5.0 WESTERN GATEWAY

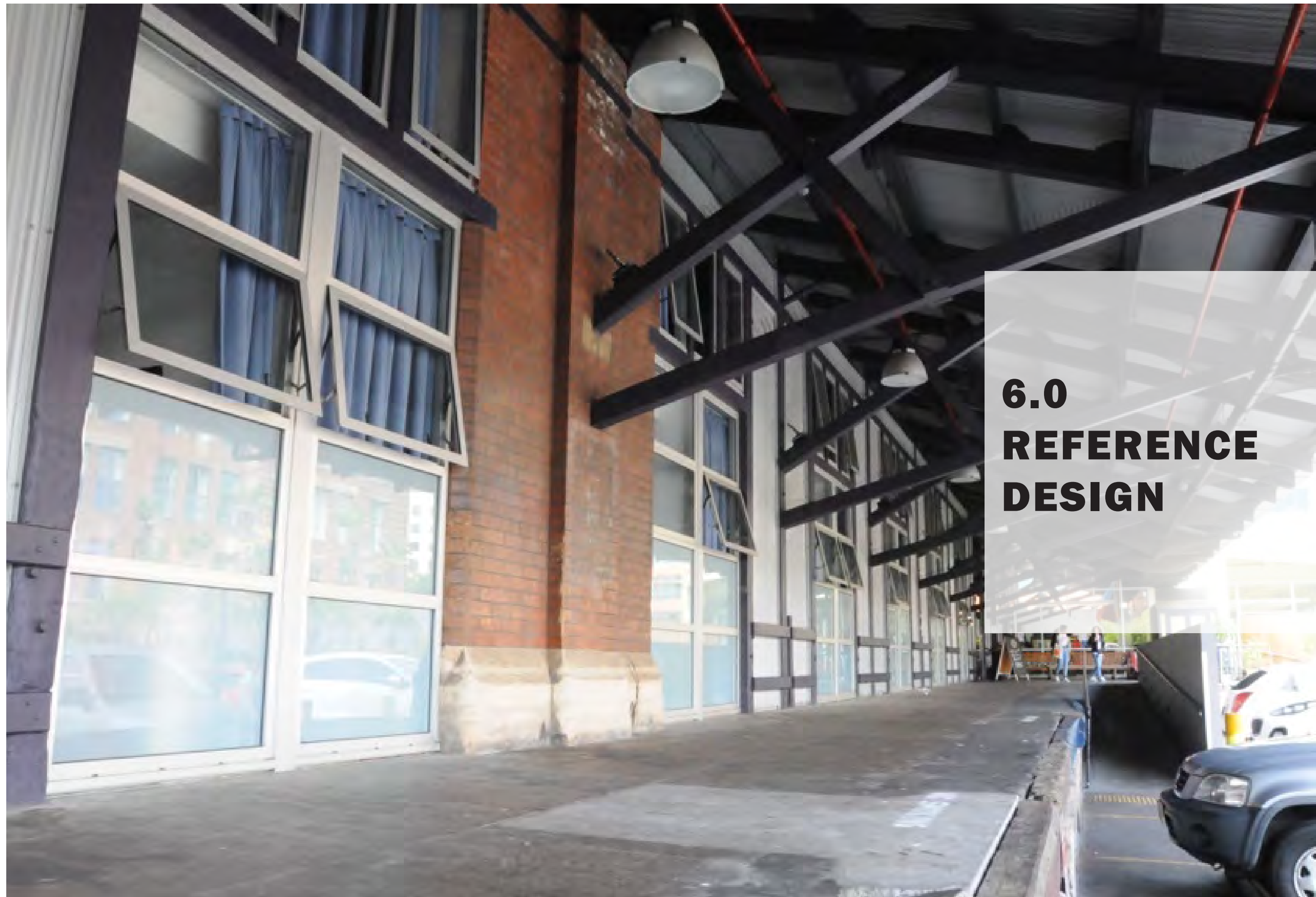
TOWER ENVELOPE PLAN

This plan indicates the tower level built form of Block A with that provided by the Block B stakeholders.

- LEGEND
- Block A Built Form
 - Block B Built Form
 - Block A Public Domain
 - Block A Site Boundary
 - Block B Site Boundary



High Rise Tower Plan - Scale 1:1000 @ A3



6.0 REFERENCE DESIGN

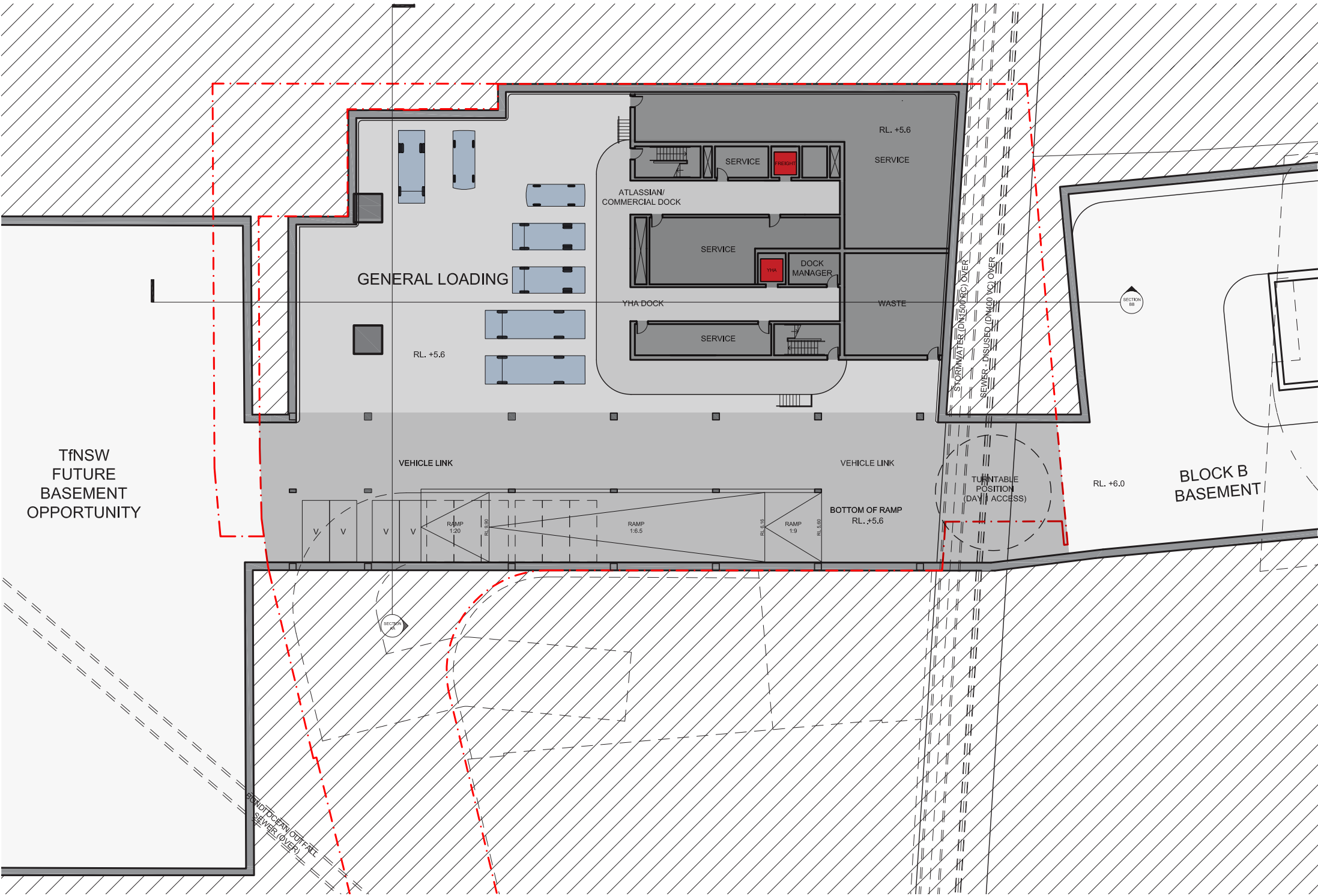
INDICATIVE REFERENCE DESIGN

This reference design is indicative of a potential scheme within the proposed envelope. It is not proscriptive or restrictive - rather it conceptually illustrates one of many potential outcomes based on the envelope proposed

BASEMENT -02 PLAN

The reference design Basement Level -02 Plan includes the loading area servicing the Block A site. It shows both:

- the day one access provisions via ramped access from Lee Street with a turntable at the southern end of the B2 level; and
- the end state arrangement with north - south vehicle access link that enables vehicle access to the site from the Block B site to the south and with through site access to future TfNSW basement area in the north. The ramp from Lee Street down to the Adina Hotel (Block C) basement can be removed once the access from Block B is in place.



1:400 @ A3



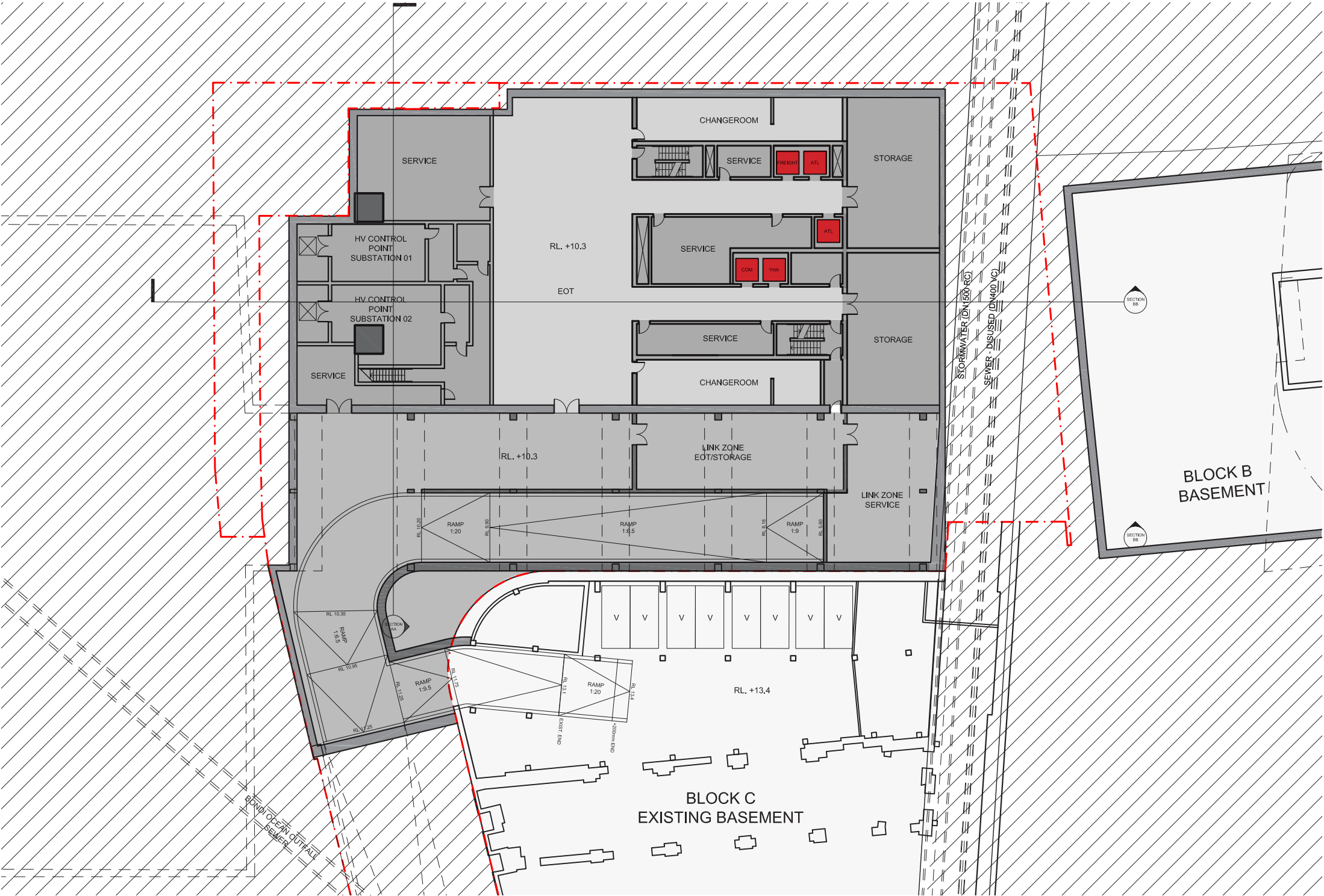
APPENDIX A - URBAN DESIGN PLANNING REPORT

6.0 REFERENCE DESIGN

BASEMENT -01 PLAN

The reference design Basement Level -01 Plan includes plant and services and end of trip facilities.

The ramp between basement level -01 and -02 provides access (including with end state access via basement level -02) between levels. This has the benefit of providing the opportunity for access to the Adina Hotel basement in the long term. Day one access to the Adina Hotel basement is enabled via the ramp from Lee Street. This enables the earlier pedestrianisation of Ambulance Avenue as part of the Central Station renewal.



1:400 @ A3

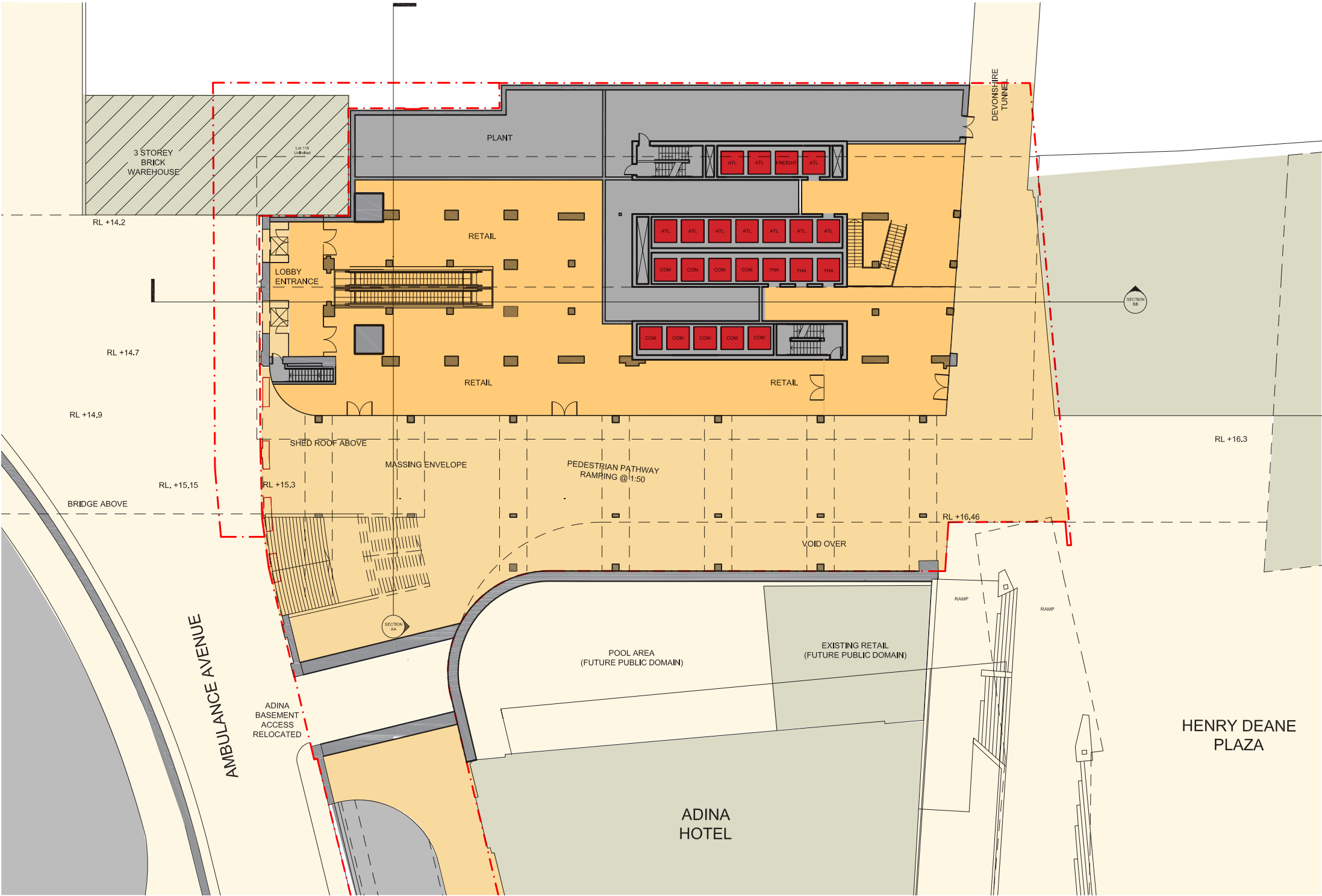
APPENDIX A - URBAN DESIGN PLANNING REPORT

6.0 REFERENCE DESIGN

LOWER GROUND PLAN

The Lower Ground zone is a critical level relative to the public transport commuter and other pedestrian flows. The reference scheme design proposes a north south link within the Block A site which integrates with the proposed Central Walk western extension to the north of the site and links this through to Henry Deane Plaza to the south. Henry Deane Plaza acts as a connection node with Devonshire St Tunnel, Lee St Tunnel to Railway Square on onwards to the northern side of George St and the Goods Line and the proposed Over Station Development (OSD) connection across to Devonshire Street.

Given the significant commuter flows that could use the link space there is low level access points at the northern and southern ends to the lobbies for the Block A building uses to separate these flows. There is also an access stair to the upper ground level. The remainder of the lower ground Block A zone is proposed to be activated with retail.



1:400 @ A3



APPENDIX A - URBAN DESIGN PLANNING REPORT

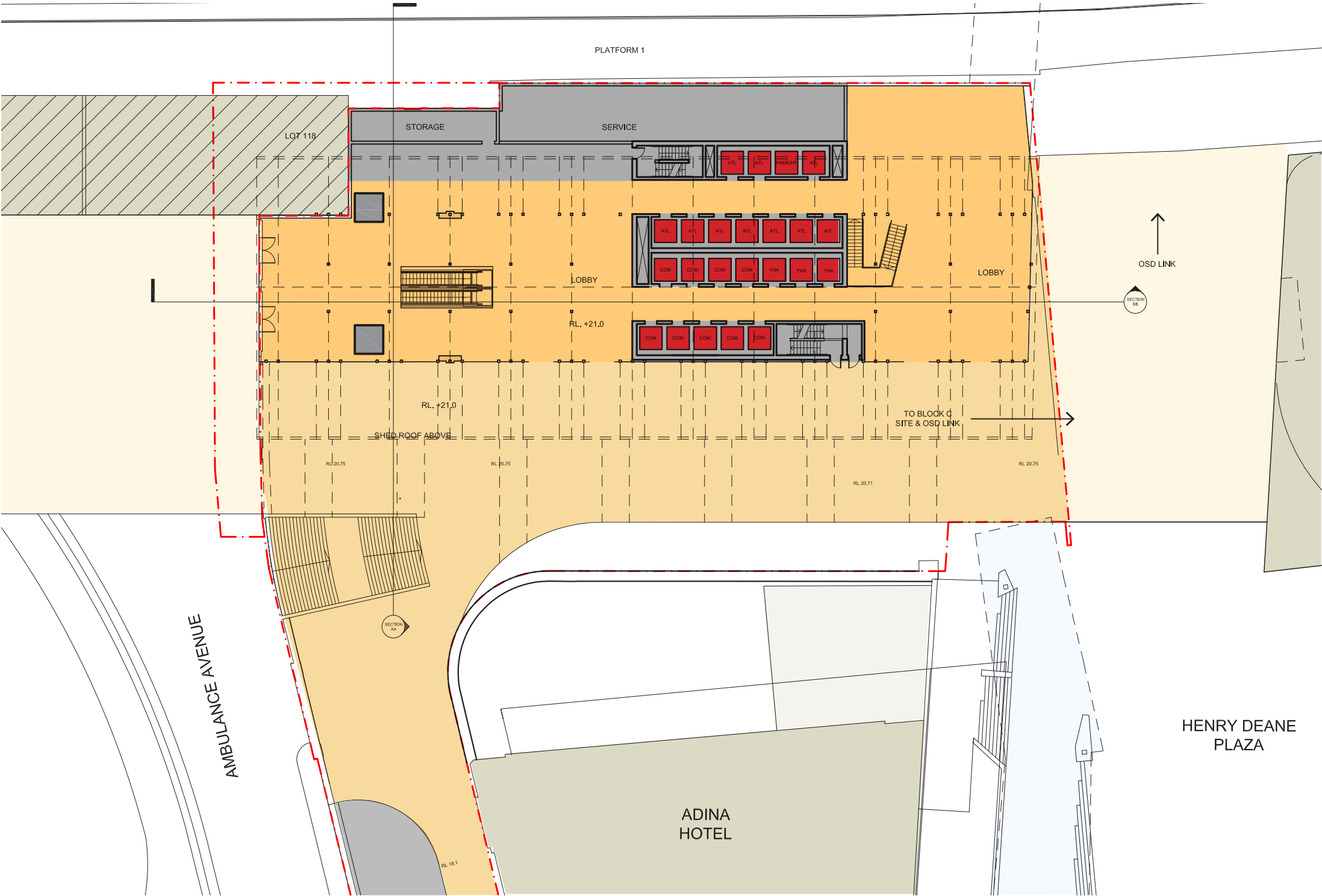
6.0 REFERENCE DESIGN

UPPER GROUND PLAN

The upper ground level is envisioned as the main lobby level for the Block A site. The vision for this space is as a dynamic curated shared space between the YHA Hostel, Young tech start-ups and Atlassian within the former Inwards Parcel Post Shed building.

The upper ground has also been developed to provide a north south public domain link to integrated into public domain zones that other stakeholders are proposing, including the major OSD access point to the south of the shed footprint. Ramped access has been maintained to this level via ramping from Lee Street. Once the end state vehicle access is in place the ramp is envisioned to become a full public domain zone.

A stair connects the upper and lower ground levels at the northern end of the site to take advantage of the aspect to the future Western Forecourt public domain space. Further development with the precinct stakeholders may result in this stair shifting further north beyond the current Block A site boundary.



1:400 @ A3

APPENDIX A - URBAN DESIGN PLANNING REPORT

6.0 REFERENCE DESIGN

TYPICAL HOSTEL LEVEL PLAN

The indicative Hostel level plan shows a rectangular floor plate at the northern end making use of the air rights zone over Lot 118. At the southern end the building has been aligned to the southern face of the Former Parcels Post building (Adina Hotel) to create a sense of the public domain space containment on this alignment.

Within the hostel plan a variety of room types have been provided along with terraces at each level and communal spaces within the northern multiple level spaces.



1:400 @ A3

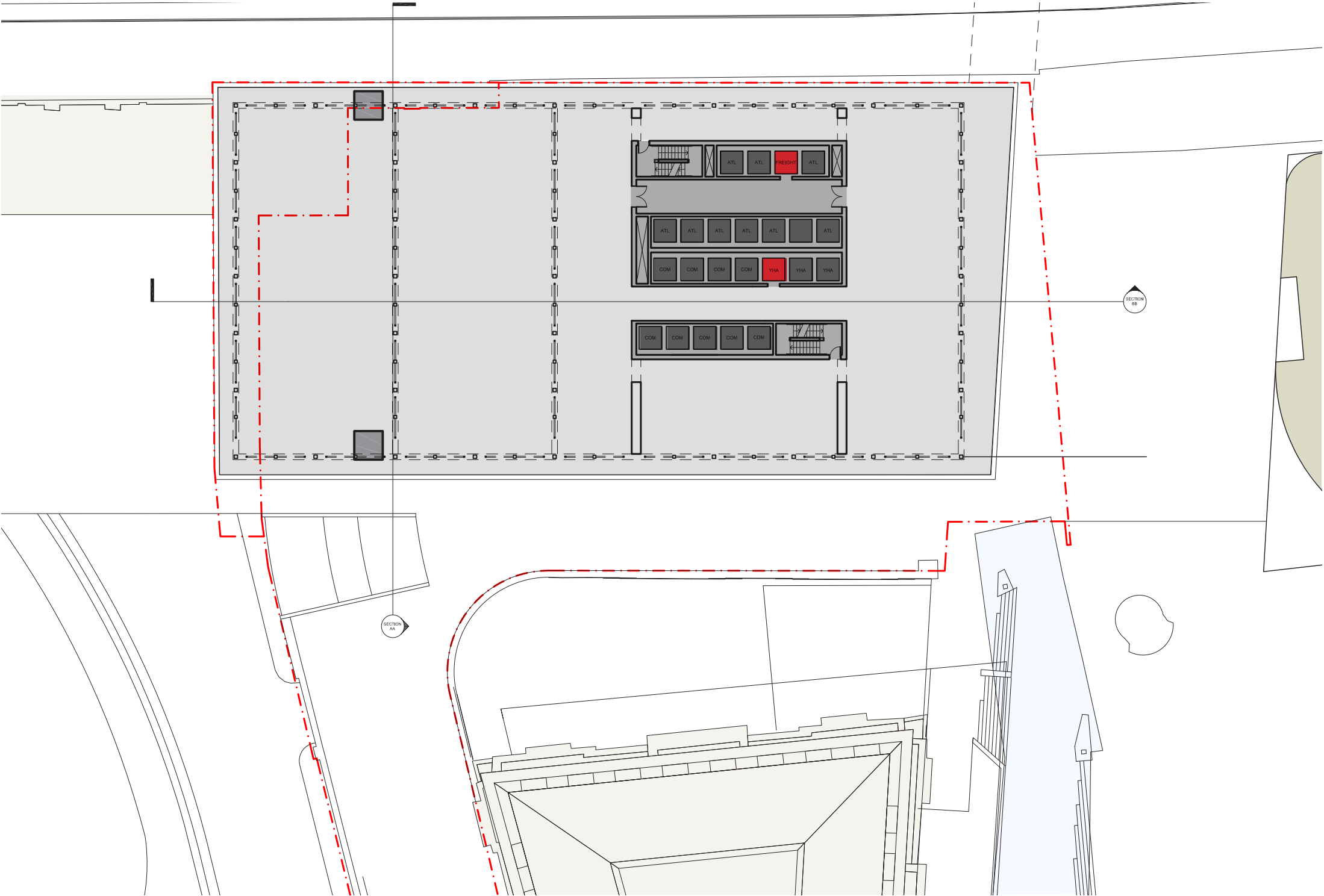


APPENDIX A - URBAN DESIGN PLANNING REPORT

6.0 REFERENCE DESIGN

LOW RISE PLANT PLAN

A lower rise plant level serves both the hotel and workplace component of the tower, and is served by both the hostel and commercial goods lifts.



1:400 @ A3



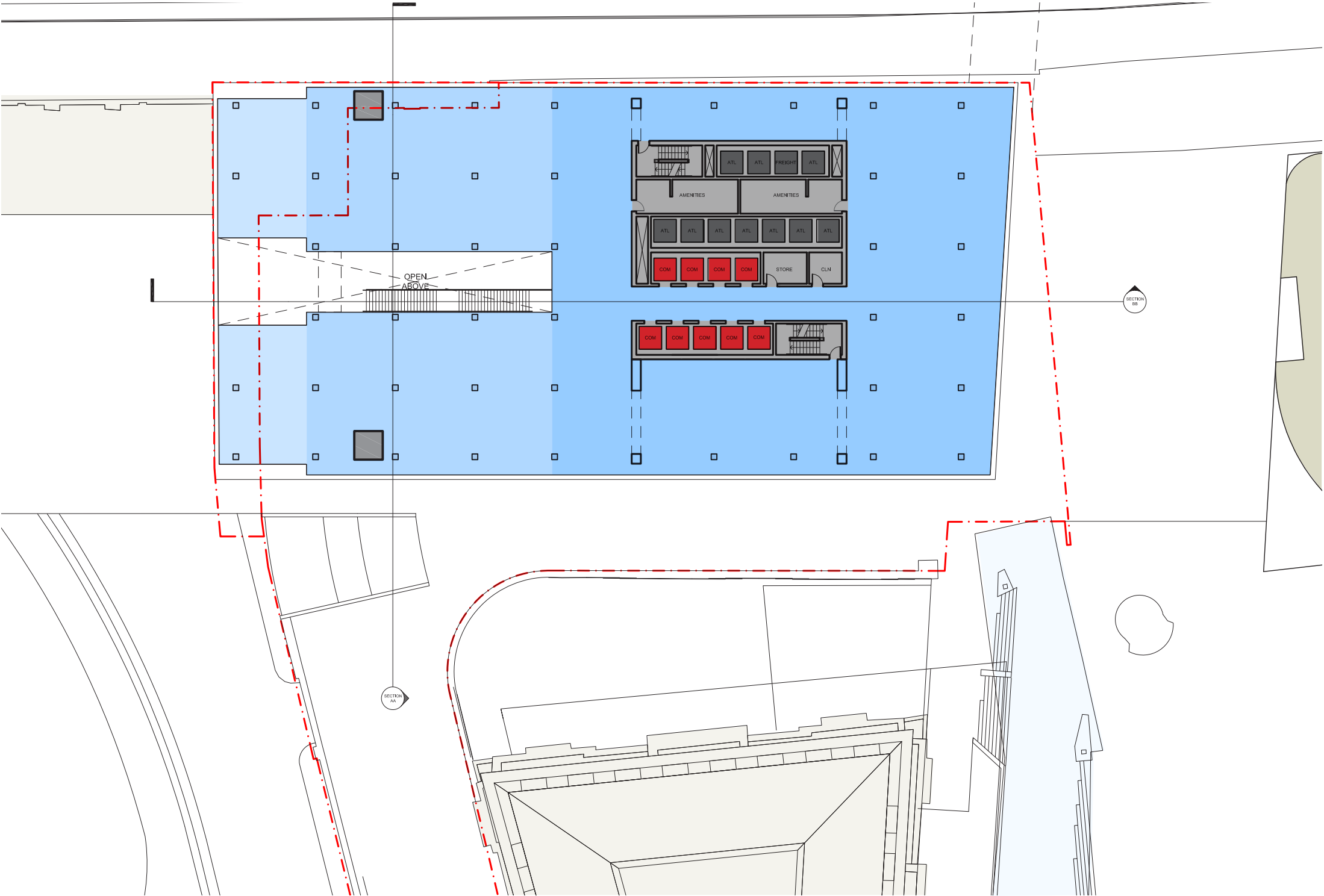
APPENDIX A - URBAN DESIGN PLANNING REPORT

6.0 REFERENCE DESIGN

TYPICAL LOW RISE PLAN

The indicative low rise plan illustrates a simple rectangular floor plate with a shave at the southern end that is parallel to the Former Parcels Post building (Adina Hotel) and to respond to the potential alignment of the OSD link.

The core is positioned to enable the creation of a diversity of neighbourhood clusters around this with an emphasis of space at the northern end to respond to the heritage principles at the low level and optimise the views at the northern end. The northern end of the floor plate has integrated an approach to allow circulation across levels via the void space and allow zones of the floor plate to have natural ventilation and outdoor workspaces.



1:400 @ A3



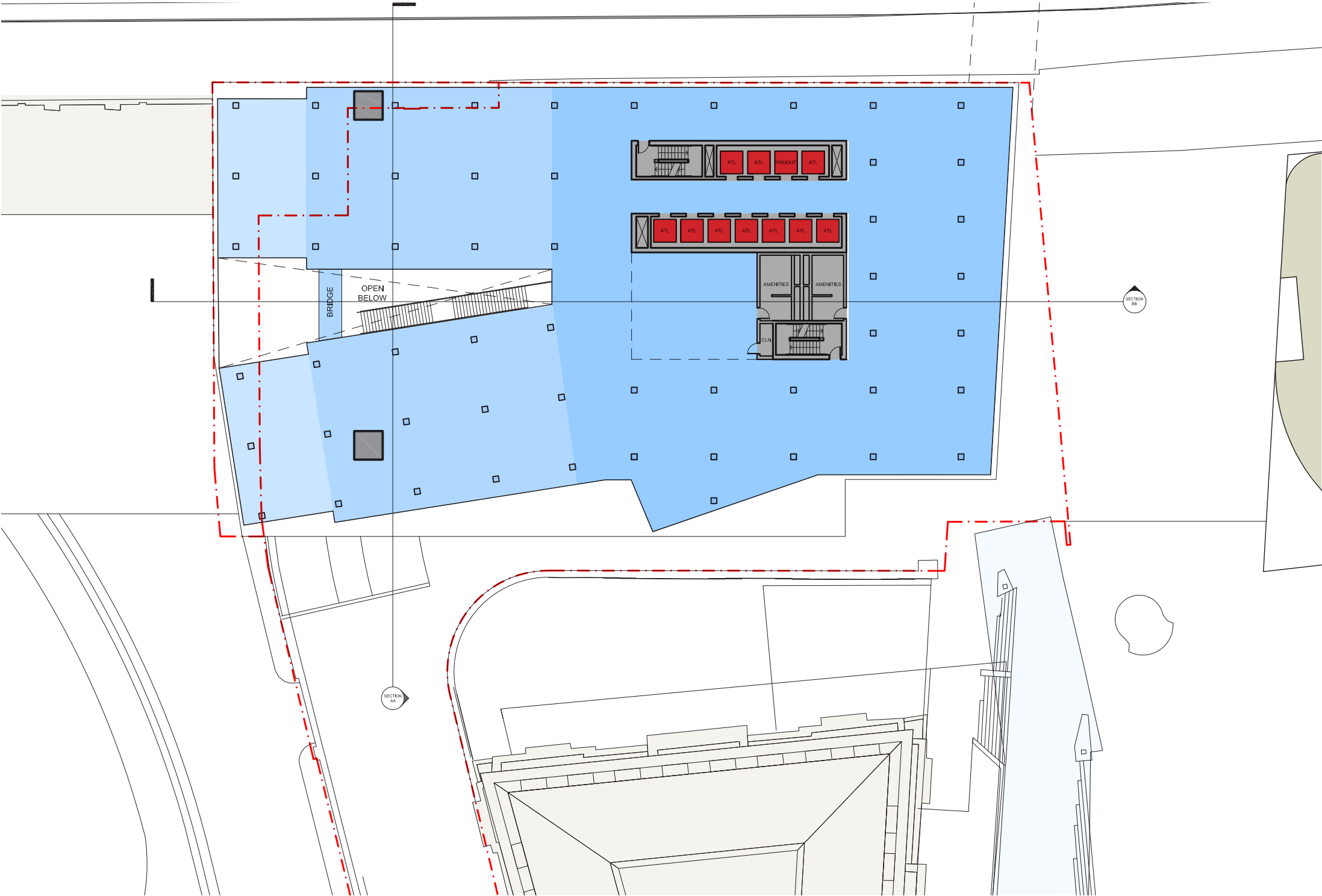
APPENDIX A - URBAN DESIGN PLANNING REPORT

6.0 REFERENCE DESIGN

TYPICAL HIGH RISE PLAN

The indicative high rise plan illustrates a the possible use of the cantilever zone on the west, which begins at a minimum of RL 70, which is 50m above the public domain zone of the upper ground level.

The core is positioned to enable the creation of a diversity of neighbourhood clusters around this with an emphasis of space at the northern end to respond to the heritage principles at the low level and optimise the views at the northern end. The opening of the form at the northern end of the floor plate allows for an improved result relative to natural ventilation and outdoor workspaces and support the strong environmentally sustainable drivers that form a key aspect of the brief. The void it optimised to support the natural ventilations and continues the opportunity for circulation across levels via the void space.



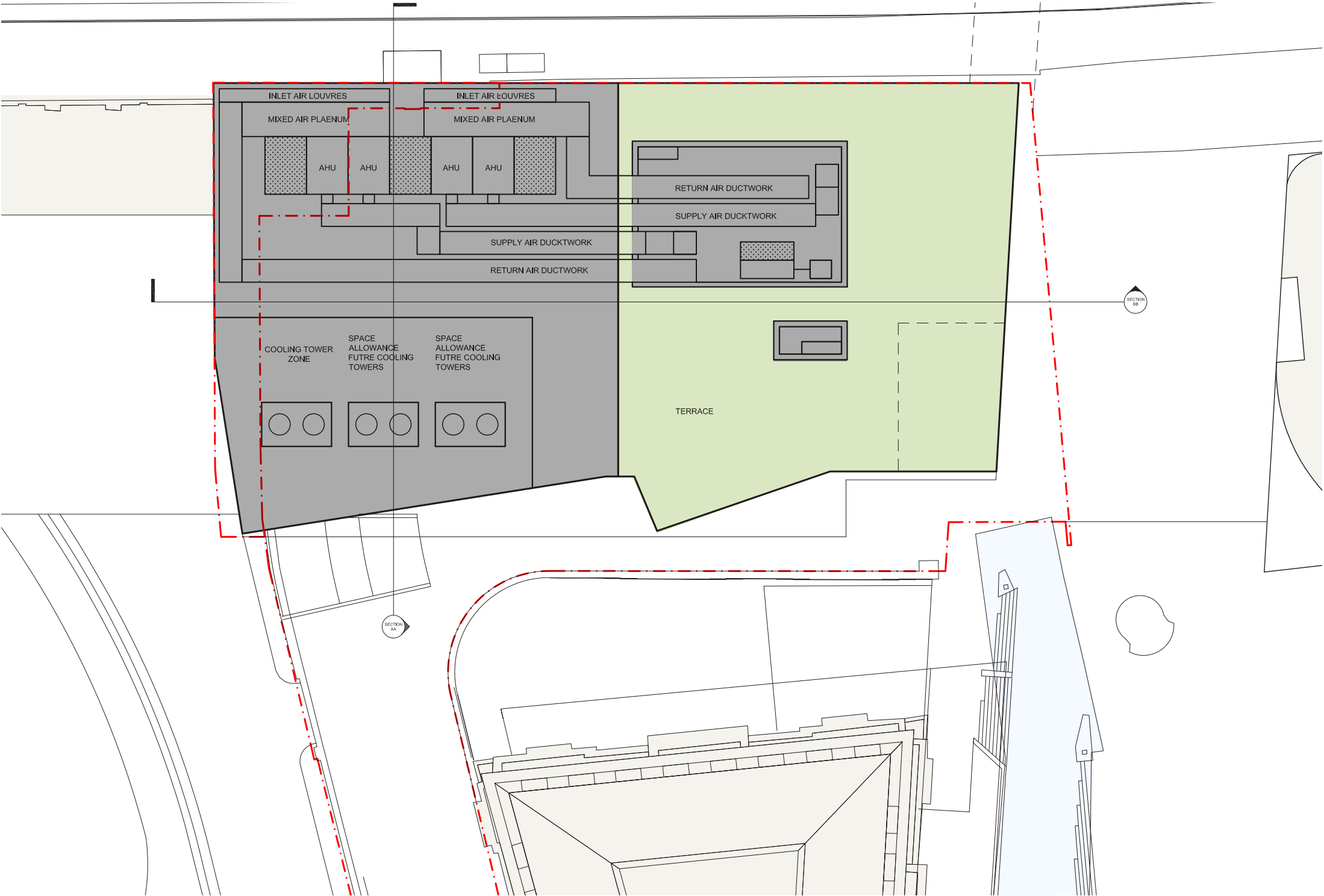
1:400 @ A3



APPENDIX A - URBAN DESIGN PLANNING REPORT

6.0 REFERENCE DESIGN

ROOF PLANT PLAN
A high rise plant level occupies some of the roof space along split level condition with a terrace space to support a roof garden area.



1:400 @ A3



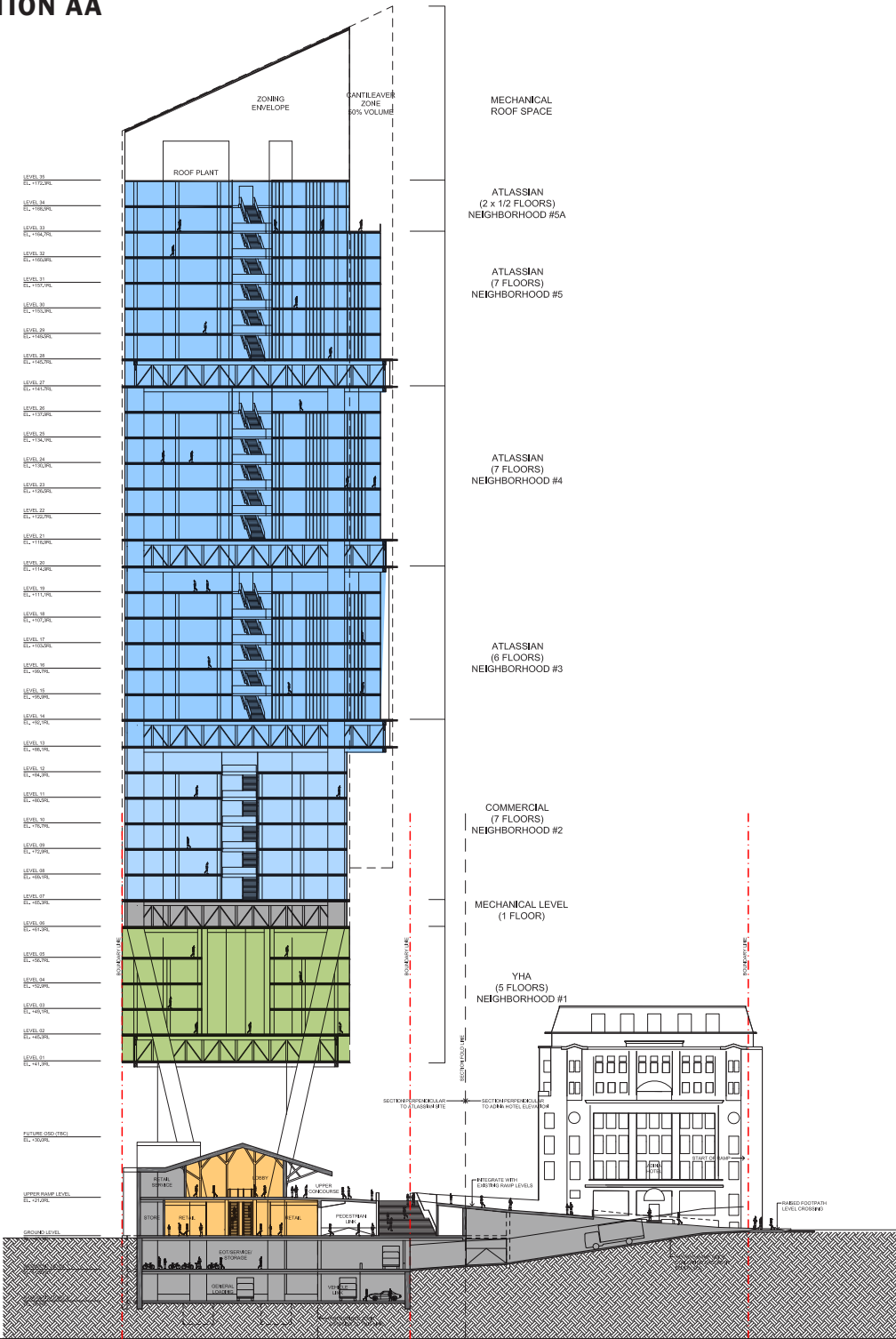
APPENDIX A - URBAN DESIGN PLANNING REPORT

6.0 REFERENCE DESIGN

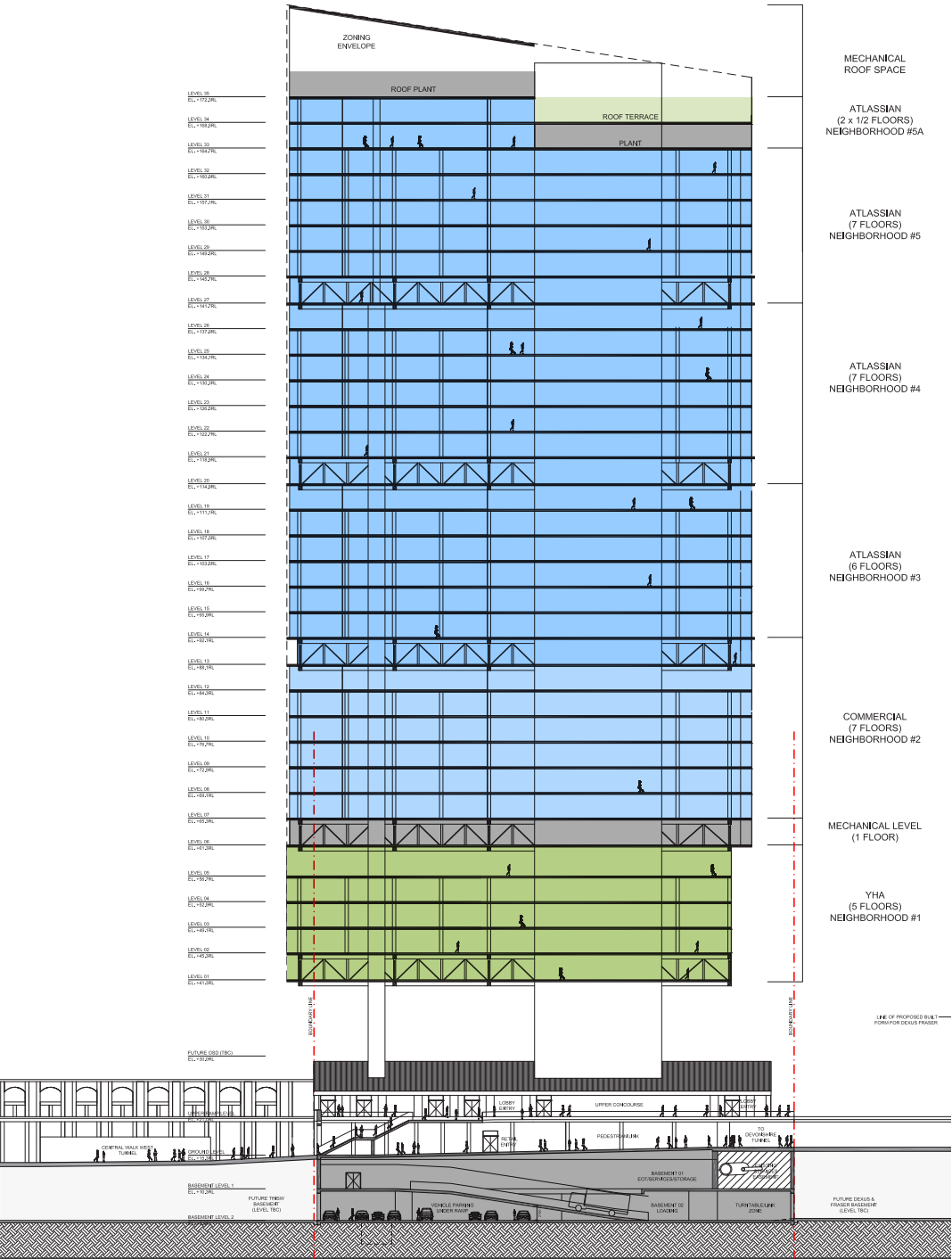
SECTIONS

The indicative reference design sections illustrates the Inwards Parcels Shed lobby fronting Upper Carriage Lane on the upper ground level. The lower ground provides pedestrian movement at the street level with retail activation and access to this upper level. An elevated tower comprised of YHA Hotel and Atlassian / Tech startup workplace sits above the integrated public domain zone.

SECTION AA



SECTION BB



APPENDIX A - URBAN DESIGN PLANNING REPORT

6.0 REFERENCE DESIGN

REFERENCE DESIGN AREA SCHEDULE

The reference design demonstrates that an FSR of 20:1 is achievable within the proposed envelope controls. This schedule is illustrative of one potential stacking arrangement of the proposed hostel, Atlassian/Tech Commercial workplace within the proposed envelope.

Areas are estimates and do not reflect final lift, services or structural requirements.

Number of levels may vary with further services/structural coordination and envelope refinement.



Development Summary

1	Roof Garden	Development Total GFA	69,758 m²
1	Roof Plant		
21	High Rise	Development FSR	20:1
6	Low Rise		
1	Low Rise Plant	Total Commercial NLA	52,642 m²
5	Hostel	Total Development NLA	60,085 m²
	Void		
1	Upper Ground	Total Commercial Levels	28
1	Lower Ground		
2	Basement		
39	Levels		

Floor Level	AHD RL	FLR / FLR	Description	Use	Tenant	GBA	GFA	NLA
L35	AHD + 172.3m	3.8m	1/2 Floor MEP Plant	Services	Plant	1,444 m²	NA	NA
L34	AHD + 168.5m	3.8m	1/2 Tower Floor	Business	Atlassian + Terrace	2,299 m²	1,170 m²	1,101 m²
L33	AHD + 164.7m	3.8m	1/2 Tower Floor/MEP	Business/Services	Atlassian + Plant	2,299 m²	1,170 m²	1,101 m²
L32	AHD + 160.9m	3.8m	Tower Floor	Business	Atlassian	2,299 m²	2,178 m²	1,929 m²
L31	AHD + 157.1m	3.8m	Tower Floor	Business	Atlassian	2,299 m²	2,178 m²	1,929 m²
L30	AHD + 153.3m	3.8m	Tower Floor	Business	Atlassian	2,299 m²	2,178 m²	1,929 m²
L29	AHD + 149.5m	3.8m	Tower Floor	Business	Atlassian	2,299 m²	2,178 m²	1,929 m²
L28	AHD + 145.7m	3.8m	Tower Floor	Business	Atlassian	2,704 m²	2,353 m²	2,065 m²
L27	AHD + 141.7m	4.0m	Tower Floor	Business	Atlassian	2,704 m²	2,499 m²	2,235 m²
L26	AHD + 137.9m	3.8m	Tower Floor	Business	Atlassian	2,299 m²	2,178 m²	1,929 m²
L25	AHD + 134.1m	3.8m	Tower Floor	Business	Atlassian	2,299 m²	2,178 m²	1,929 m²
L24	AHD + 130.3m	3.8m	Tower Floor	Business	Atlassian	2,299 m²	2,178 m²	1,929 m²
L23	AHD + 126.5m	3.8m	Tower Floor	Business	Atlassian	2,299 m²	2,178 m²	1,929 m²
L22	AHD + 122.7m	3.8m	Tower Floor	Business	Atlassian	2,299 m²	2,178 m²	1,929 m²
L21	AHD + 118.9m	3.8m	Tower Floor	Business	Atlassian	2,704 m²	2,353 m²	2,065 m²
L20	AHD + 114.9m	4.0m	Tower Floor	Business	Atlassian	2,704 m²	2,499 m²	2,235 m²
L19	AHD + 111.1m	3.8m	Tower Floor	Business	Atlassian	2,299 m²	2,178 m²	1,929 m²
L18	AHD + 107.3m	3.8m	Tower Floor	Business	Atlassian	2,299 m²	2,178 m²	1,929 m²
L17	AHD + 103.5m	3.8m	Tower Floor	Business	Atlassian	2,299 m²	2,178 m²	1,929 m²
L16	AHD + 99.7m	3.8m	Tower Floor	Business	Atlassian	2,299 m²	2,178 m²	1,929 m²
L15	AHD + 95.9m	3.8m	Tower Floor	Business	Atlassian	2,299 m²	2,178 m²	1,929 m²
L14	AHD + 92.1m	3.8m	Tower Floor	Business	Atlassian	2,704 m²	2,353 m²	2,065 m²
L13	AHD + 88.1m	4.0m	Tower Floor	Business	Atlassian	2,704 m²	2,499 m²	2,235 m²
L12	AHD + 84.3m	3.8m	Tower Floor	Business	Commercial	2,083 m²	2,031 m²	1,714 m²
L11	AHD + 80.5m	3.8m	Tower Floor	Business	Commercial	2,083 m²	2,031 m²	1,714 m²
L10	AHD + 76.7m	3.8m	Tower Floor	Business	Commercial	2,083 m²	2,031 m²	1,714 m²
L09	AHD + 72.9m	3.8m	Tower Floor	Business	Commercial	2,083 m²	2,031 m²	1,714 m²
L08	AHD + 69.1m	3.8m	Tower Floor	Business	Commercial	2,083 m²	2,031 m²	1,714 m²
L07	AHD + 65.3m	3.8m	Tower Floor	Business	Commercial	2,342 m²	2,131 m²	1,964 m²
L06	AHD + 61.3m	4.0m	MEP Plant	Services	NA	2,428 m²	NA	NA
L05	AHD + 56.7m	3.8m	Tower Floor	Hospitality	YHA	2,053 m²	1,520 m²	1,308 m²
L04	AHD + 52.9m	3.8m	Tower Floor	Hospitality	YHA	2,053 m²	1,520 m²	1,356 m²
L03	AHD + 49.1m	3.8m	Tower Floor	Hospitality	YHA	2,053 m²	1,520 m²	1,356 m²
L02	AHD + 45.3m	3.8m	Tower Floor	Hospitality	YHA	2,249 m²	1,520 m²	1,356 m²
L01	AHD + 41.3m	4.0m	Tower Floor	Hospitality	YHA	2,090 m²	1,491 m²	1,325 m²
N/A	AHD + 29.2m	13.8m	Station Over Deck	NA	NA			
UPR	AHD + 21.0m	7.9m	Upper Ramp	Lobby	Atlassian/Comercial/YHA	1,552 m²	1,183 m²	80 m²
GR	AHD + 16.0m	5.0m	Ambulance Way Grade	Lobby & Retail	Public + Lobby Access	1,692 m²	1,330 m²	662 m²
B	AHD + 11.5m	4.5m	Basement	Service	NA	2,571 m²	0 m²	0 m²
LB	AHD + 5.9m	4.5m	Lower Basment	Service	NA	2,426 m²	0 m²	0 m²
					TOTALS	88,376 m²	69,758 m²	60,085 m²
					Site Area	3487	FSR 20:1	

APPENDIX A - URBAN DESIGN PLANNING REPORT

6.0 REFERENCE DESIGN

PRECEDENTS

