

HERITAGE IMPACT STATEMENT & ARCHAEOLOGICAL ASSESSMENT

**BANK STREET
COMMERCIAL WHARF**

**5-11 BANK STREET,
PYRMONT NSW.**

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EXECUTIVE SUMMARY

Urbis has been engaged by UrbanGrowth NSW to prepare the following Heritage Impact Statement and Archaeological Assessment.

The site is not subject to any statutory listings, however is in the vicinity of heritage items, including the Glebe Island Bridge / RTA Bridge No. 61, which is listed on the State Heritage Register and under the SREP (Sydney Harbour Catchment) 2005; and the Escarpment face from the former “Saunders’ Quarry”, which is listed under the City of Sydney LEP 2012. The report also considers seawalls, which although not presently listed, may be of some significance. The Anzac Bridge is also listed as a heritage item under the RMS s170 Register.

The proposal provides for the construction of new temporary facilities on the north-eastern side of Blackwattle Bay in the vicinity of the eastern Anzac Bridge pylon. The proposed works include the following:

- Temporary installation of office accommodation space in portable buildings and associated areas of approx. 70m². This facility, along with a small services area adjacent, will be clad with polycarbonate and zincalume sheet panels.
- Temporary installation of 22 shipping containers (for storage - twelve - 12m and ten - 6m).
- The area containing the temporary facilities will have some landscaping and concreted areas.

The subject site has been assessed and does not meet criteria for heritage listing; however, the extant seawall has been recognised as having aesthetic and historic significance as representative of the development and settlement of Pyrmont around the turn of the 20th century (1900) and of the associated maritime and industrial functions of the area. *It should however be noted that the seawall is located outside the boundary of the works.* The significance of the items in the vicinity has also been acknowledged in section 5.4 of this report.

The site has been assessed as being of low archaeological potential and low significance. Whilst it is acknowledged that there is potential for remains associated with the former early industrial uses of the site (timber yards and skin store), however if present, these remains would be unlikely to be substantial and are unlikely to have the potential to provide new or significant information about the sites history, the uses of which have been documented.

The proposal has been assessed with reference to relevant controls and provisions contained within the Sydney Local Environmental Plan 2012, the Secretary’s Environmental Assessment Requirements (SEARs) issued 12 April 2017, specifically point one - Key Issue 17, and the Heritage Office Guidelines as set out in section 6.3 The works have been recommended for approval, subject to recommendations as set out in section 7.

1. INTRODUCTION

1.1. BACKGROUND

Urbis has been engaged by UrbanGrowth NSW to prepare the following Heritage Impact Statement and Archaeological Assessment. UrbanGrowth NSW is preparing a modification application in relation to the earlier Part 3A approval for Sydney Heritage Fleet facilities at 5 Bank Street, Pyrmont (MP 11_0001). The proposal involves the relocation of a RMS licensed Commercial Operator from Pyrmont Bridge Road, Glebe to the Bank Street, Pyrmont site to enable the development of the New Sydney Fish Market. The proposal is in effect a decanting of water-based operations within Blackwattle Bay and allows a working harbour use to continue in generally the same location with similar operational parameters to its current use.

The site is in the vicinity of heritage items, including the Glebe Island Bridge / RTA Bridge No. 61, which is listed on the State Heritage Register and under the SREP (Sydney Harbour Catchment) 2005; and the Escarpment face from the former “Saunders’ Quarry”, which is listed under the City of Sydney LEP 2012. The report also considers seawalls, which although not presently listed, may be of some significance. The Anzac Bridge is also listed as a heritage item under the RMS s170 Register.

The proposal is required to be considered in relation to potential impacts to heritage items in the vicinity and archaeological potential. This Heritage Impact Statement and Archaeological Assessment is to be submitted with the application.

1.2. SITE LOCATION

The study area is a foreshore strip of predominantly vacant land on the eastern shore of Blackwattle Bay, Pyrmont. (The approximate site area is shown at Figure 1). The location of the actual works referred to in this report lay within the small area, shaded blue, of the outlined broader study area below.

Figure 1 – Site Location



Source – Six Maps

1.3. METHODOLOGY

This Heritage Impact Statement has been prepared in accordance with the NSW Heritage Branch guideline 'Assessing Heritage Significance' (2001). The philosophy and process adopted is that guided by the *Australia ICOMOS Burra Charter* 1999 (revised 2013). The proposal has been assessed with reference to relevant controls and provisions contained within the Sydney Local Environmental Plan 2012, the Heritage Office Guidelines.

This report has been prepared in response to the requirements of the Secretary's Environmental Assessment Requirements (SEARs) issued 12 April 2017.

This Archaeological Assessment has been prepared in accordance with the NSW Heritage Branch guidelines 'Assessing Heritage Significance' (2001) and Assessing Significance for Historical Archaeological Sites and 'Relics' (2009).

1.4. AUTHOR IDENTIFICATION

The following report has been prepared by Fiona Binns (Senior Consultant), Karyn McLeod (Archaeologist) and Gavin Patton (Heritage Research). Stephen Davies (Director) has reviewed and endorsed its content.

Unless otherwise stated, all drawings, illustrations and photographs are the work of Urbis.

1.5. THE PROPOSAL

The proposal provides for the construction of temporary structures on the north-eastern side of Blackwattle Bay in the vicinity of the eastern Anzac Bridge pylon. The proposed works include the following works:

- Temporary installation of office accommodation space in a portable building of approx. 70m². This facility, along with a small services area adjacent, will be clad with polycarbonate and zincalume sheet panels.
- Temporary installation of 22 shipping containers (for storage - twelve - 12m and ten - 6m).
- The area containing the temporary facilities will have some landscaping and concreted areas.

Installation of a portable building to house a small administrative office. The portable building will be approximately 70m² and contain a reception area, 6 workstations and WC facilities. Adjacent to the portable building is a small area for services.

Installation of 22 single stacked shipping containers (12 containers of 12 meters length and 10 containers of 6 meters length).

Circulation space near the portable building and shipping containers will be semi-enclosed and also contain two carparks (Inc. one accessible). Parts of this facility will be concreted and other parts landscaped.

Further details of the proposal are shown in the plans prepared by Bennett and Trimble.

The works would be undertaken in conjunction with a range of other minor / minimal impact works that are exempt from the need for any form of environmental assessment.

2. SITE DESCRIPTION

2.1. THE STUDY AREA

The study area is bounded by the commercial development to the south, Blackwattle Bay to the west, Bank Street to the east and the Old Glebe Island Bridge and Jackson's Landing to the north. The subject site consists of largely vacant land, used in part for storage for dragon boats. Development at 1-3 Bank Street adjoins the site to the north.

A low sea wall consisting of large sandstone blocks constructed as coursed ashlar, is present around the shore line in the study area from the southern embankment of the old Glebe Island Bridge to Poulos Brothers at 21-29 Bank Street. The wall stands two to three courses above high tide (approx. 1m) and has several courses below the low tide water level. It is interrupted by the southern pylon of the Anzac Bridge and the Dragon boat ramp as well as a number of drains. There are additional small remnants of this wall to the south of the study area adjacent to the Bidvest building and adjacent to the carpark of the fish markets.

Figure 2 – Views of the subject site from Blackwattle Bay



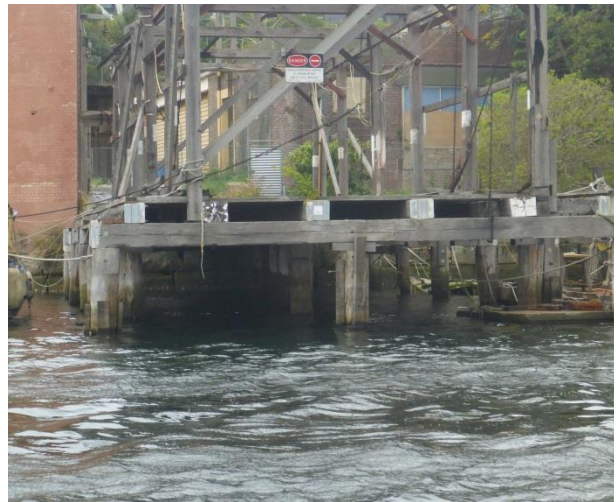
Picture 1 - Bridge embankment and stone sea wall (to the north of the subject site)



Picture 2 - 1 Bank Street and sea wall (view of the later 20th century building on the waterfront)



Picture 3 – Wharf and sea wall at 1 Bank Street



Picture 4 – Detail of the later 20th century wharf and seawall at 1 bank street



Picture 5 – Anzac Bridge and Dragon boat storage



Picture 6 – Anzac Bridge and Dragon boat storage



Picture 7 – Sea wall and Poulos Brothers building adjoining the southern portion of the site



Picture 8 - Detail of the seawall

2.2. LOCAL CONTEXT AND HERITAGE ITEMS IN THE VICINITY

No.1 Bank Street adjoins the site to the north and comprises a complex of three 1-2 storey industrial buildings incorporating a single storey inter-war brick building fronting Bank Street, the southern adjoining extension, constructed in the 1940s and the later 20th century 2 storey building on the waterfront.

Figure 3 - 1 Bank street, Pyrmont



Picture 9 - The northern building is the earliest extant building on the site, with the fabric suggesting an early inter-war construction



Picture 10 - The southern extension of 1 Bank Street was constructed in the mid-1940s however has been variously altered since, including works to the roof form

Industry to the south of the subject site includes the Poulos Bros Seafoods Pty Ltd, which currently operates a seafood processor and distributor; Bidvest Australia Ltd (dry food processing, storage and distribution) from its site; and Hymix Australia Pty Ltd concrete batching plant.

Figure 4 – Commercial and industrial development in the vicinity of the site



Picture 11 – Poulos Brothers and Bidvest



Picture 12 – Bidvest Building



Picture 13 – Hymix concrete batching plant



Picture 14 - Area between Hymix and Bidvest



Picture 15 – View toward the fish markets



Picture 16 - Fish market carpark and sea wall

Development on the eastern side of Bank Street comprises some late 20th century and contemporary multistorey apartment development. The escarpment face from the former “Saunders’ Quarry”, which is heritage listed under the City of Sydney LEP 2012 forms the eastern edge of Bank Street, with development elevated above street level. The escarpment extends westwards from the intersection of Jones and John Street to Bank Street and south to Quarry Masters Drive. Intrinsic to the developmental history of Pyrmont, this is the site of a prominent quarry which supplied sandstone to significant civic buildings in Sydney, including the GPO and Sydney University’s Quadrangle buildings, as well as buildings in Melbourne, Fiji, New Zealand and Canada. Established by Charles Saunders and later directed by his son Robert, the quarries expanded, and are associated with many of Pyrmont’s early buildings.¹

¹ Office of Environment and Heritage: Escarpment Face from Former Saunders Quarry:
<http://www.environment.nsw.gov.au/heritageapp/ViewHeritageItemDetails.aspx?ID=2424701>

Figure 5 - Views of contemporary residential development on the eastern side of Bank Street



Picture 17 - View north along Bank Street with the subject site at left and showing the escarpment face of the former Quarry on the eastern boundary of the street. Apartment development sits above the street level on the peninsula



Picture 18 – Detail of the escarpment face



Picture 19 - View of the escarpment face and contemporary apartment development at the former CSR refinery (right) and Jacksons Landing sites

The site is also in proximity to the state heritage listed former Glebe Island Bridge (RMS Bridge No. 61), which adjoins the site to the north and spans Johnstons Bay.

The Bridge is an electrically-operated, low-level, steel central swing-span road bridge. The central swing-span is supported by a massive pivot pier, founded on a nest of timber piles capped by concrete, on which it can rotate through ninety degrees to allow passage of maritime traffic. The approach spans are two steel deck on stone-faced piers and stone-lined abutments. The bridge includes constructed embankments on both sides of its western approach².

² Office of Environment and Heritage: Glebe Island Bridge:
<http://www.environment.nsw.gov.au/heritageapp/ViewHeritageItemDetails.aspx?ID=5051118>

The bridge has an approach span at each end of 24.7m, two main spans of 29.3m and an overall length of 108m. The roadway is 12.2m wide between kerbs and has a 1.5m wide footway on each side. The central pivot in the waterway is protected by an extensive ring of timber piles. The swing span is mounted on a steel roller track on the cylindrical stone masonry and concrete pivot pier (13.9m high and 12.9m wide) and is swung by means of a 600 volt motor. Traffic was controlled by lights and a pair of timber swing-gates on either end which were electronically interlocked to ensure that the bridge cannot open until the gates are closed. High quality Pymont yellow block sandstone is thought to have been used for dimension stone and Pymont coloured sandstone on the abutment facing and causeway fill.³

Figure 6 – The State Heritage listed former Glebe Island Bridge



Picture 20 – The former Glebe Island Bridge, shown below the Anzac Bridge



Picture 21 – The southern bridge approach (adjoining the subject site)



Picture 22 – Detail of the bridge pylon



Picture 23 – View towards the subject site from the bridge/ Blackwattle Bay

The Anzac Bridge is a listed heritage item under the RMS Section 170 Register. The bridge and approaches dominate the peninsula and the subject site incorporates an easement for one of the bridge pylons. The shoreline of the subject site was also modified in conjunction with the bridge construction. Anzac Bridge is a reinforced concrete cable-stayed bridge built over Johnstons Bay between Glebe Island and the inner Sydney suburbs of Pymont and Darling Harbour. It was designed and built between 1989 and 1995 and when completed the bridge became the longest concrete cable-stayed bridge in Australia, with a central span of 345m. The overall length of the bridge is 805 metres.⁴

³ Ibid

⁴ Office of Environment and Heritage: Anzac Bridge:
<http://www.environment.nsw.gov.au/heritageapp/ViewHeritageItemDetails.aspx?ID=4305018>

Figure 7 - View of the Anzac and Glebe Island Bridges



Picture 24 - View of the bridges spanning Johnstons Bay



Picture 25 - View of the bridge and subject site (with 1 Bank street waterfront development indicated). the approach for the Glebe island bridge is shown at left

3. HISTORICAL OVERVIEW

3.1. AREA HISTORY

The majority of the Pyrmont Peninsula was granted to Surgeon John Harris in 1803. Despite its proximity to the original settlement of Sydney, the development of Pyrmont was slow until the 1840s. Harris's vision for his property was the creation of a country estate. In 1804, Ultimo House was built on the ridge overlooking Blackwattle Bay to take advantage of the water views, and the surrounding land was manicured to resemble an English estate, complete with deer imported from India. The peninsula remained undeveloped until after his death and the land was subdivided by his family from the late 1850s⁵.

For the first half century of European occupation, minimal industrial uses were located around the western shores of the peninsula, while the eastern side was vacant. Ship building and market gardens were located in the area but it was not until the 1850s when the opening of several large quarries along the peninsula in the 1850s saw development and population growth.⁶

From the late 1840s until the end of the nineteenth century, much of the Ultimo / Pyrmont peninsula was quarried to build Sydney's great public buildings which remain the signature of Sydney's nineteenth-century built form. The construction of the first Glebe Island Bridge in 1857 (the present bridge was the second, built c.1901) and the Pyrmont Bridge in 1859 made the peninsula more accessible and extensive rows of terrace housing and pubs were built along the ridge from the 1860s onwards to house the large number of working class families in the area.⁷

Noxious industries also began to locate in the area from the 1830s and included tanneries, boiling down works, copper smelting, pig yards, and tobacco works. The most significant were the Glebe Island Abattoirs in the 1850s which attracted additional industry such as soap factories, and candle makers. The Abattoirs were a source of heavy pollution in the area and local protests led to its closure and relocation in the 1880s.⁸ The development of Blackwattle Bay has been described as... *'essentially the story of (a)... backwater of Sydney to be close enough to be viable for industry and transport and the socially disadvantaged but far enough away for the less desirable aspects of these to make a mark on the growing central district of Sydney'*⁹

The head of Blackwattle Bay was known as Blackwattle Cove or Swamp. Infilling of the swamp occurred in 1876 with the aim of eliminating the noxious smells associated with the removal of the polluting industries. After the land reclamation of the 1880s and the creation of Wentworth Park, the shore line contained a variety of small industry and residences associated with fishing, boat building, wharfage and storage.¹⁰

At the turn of the century the Darling Harbour Goods Yards, with their related cold stores and bulk handling facilities were expanding. The Metropolitan Goods Line was constructed through the newly created Wentworth Park to provide a continuous loop connection from Darling Harbour Goods Yard through to Central Station Yard. The area at this time consisted predominantly of oil, coal and timber storage and they continued to function until the 1960s.¹¹

The fish market site was acquired by NSW fishing industry in 1966 as the permanent site for the fish market. Some buildings of the former oil depot were reused, which have subsequently been demolished. The fish markets have undergone a series of refurbishment and construction phases.

⁵ http://www.powerhousemuseum.com/exhibitions/paradise_early_ultimo.php

⁶ Dictionary of Sydney - Pyrmont

⁷ ibid

⁸ Urbis 2014 Bays Precinct Heritage Constraints

⁹ Godden and Mackay, White Bay to Blackwattle Bay

¹⁰ Maunsell/Aecom, Bank Street Master Plan 2006

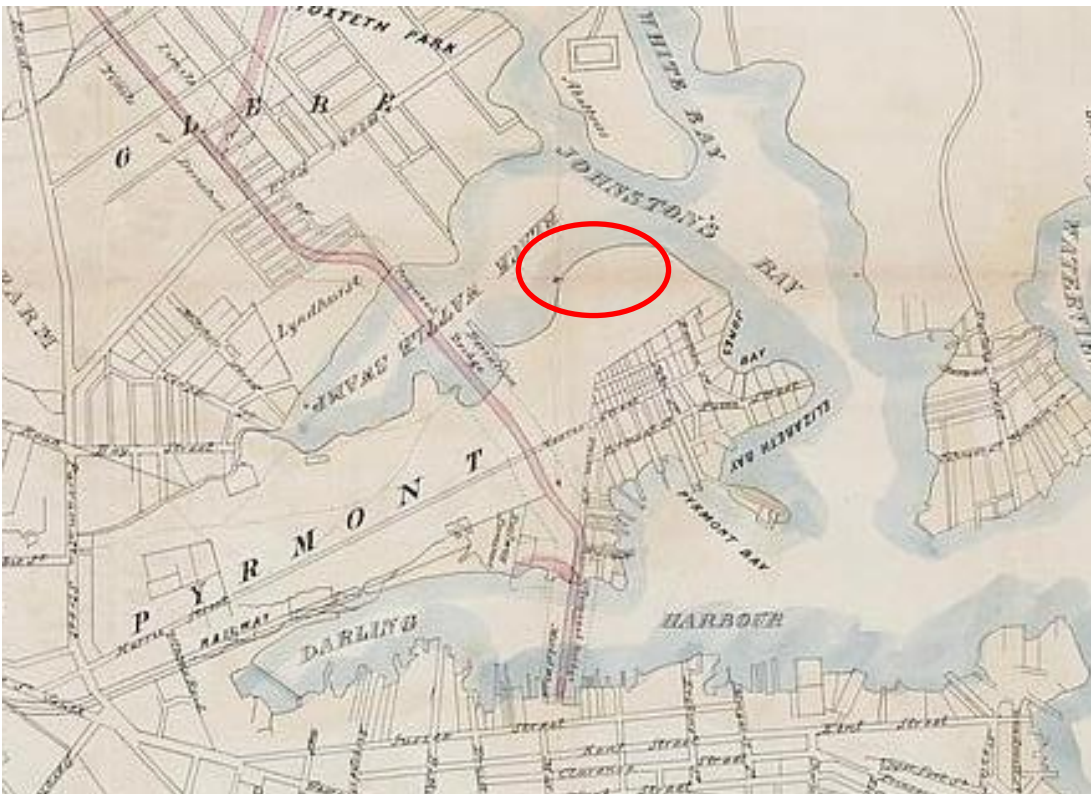
¹¹ ibid

Figure 8 - Plan of Sydney with Pyrmont, NSW 1836



Source - National Library of Australia 1837. MAP T 1551

Figure 9 - A plan of part of Sydney and its environs: showing the bridges and roads to be constructed by the Pyrmont Bridge Company



Source – Sydney Archives 1857. MAP F 310.

As the century progressed and the traffic became more congested, a number of new roads were constructed to alleviate the problem. The Glebe island bridge designed by Percy Allen served the Johnston's Bay crossing adequately until the early 1980s. In the mid-1990s the new Glebe Island Bridge was constructed. The two halves of the bridge were joined on 24 July 1995. On the 11 November 1998 (Remembrance Day), the new Glebe Island Bridge was renamed Anzac Bridge.¹²

3.2. SITE HISTORY

The subject site is located on land granted to Surgeon John Harris in 1803. Harris was granted further land on the peninsula, and by 1818 he owned 233 acres (94 hectares) outright, covering Ultimo, much of adjoining Pyrmont and parts of the Haymarket.¹³ The Harris family leased land at the north western end of the peninsula to Charles Saunders in 1853 for the purpose of quarrying sandstone.¹⁴

Figure 10 - Parish map showing Harris grant over much of the peninsula (undated)



Picture 26 - Plan of the peninsula showing Harris Ultimo land grant (including the eastern portion of Blackwattle Bay and the subject site). Harris's land comprised three grants, and totalled more than 176 acres. the tip of the peninsula was granted to Thomas Jones (55 Acres). to the south there are two smaller land grants to William Mitchell (18 acres) and John Malone (24 acres).

Source: LPI

¹² Urbis 2014 The Bays Precinct Heritage Constraints

¹³ Dictionary of Sydney - Ultimo

¹⁴ http://www.powerhousemuseum.com/exhibitions/paradise_sandstone.php

Figure 11 - Francis Webb Shields map of Sydney 1844



Picture 27 - 1844 plan of the peninsula showing development on the eastern side within the area of Jones' former grant

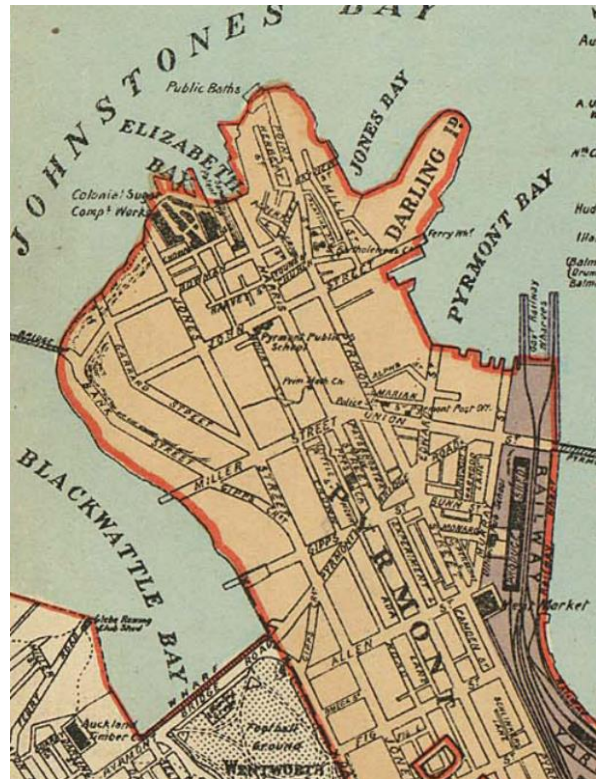
Source: City of Sydney Archives

Figure 12 – Subject site



Picture 28 – Trigonometrical Survey 1865 Source: City of Sydney Archives

Source: City of Sydney Archives



Picture 29 – Atlas of the Suburbs of Sydney 1885-1890 Source: City of Sydney Archives

Source: City of Sydney Archives

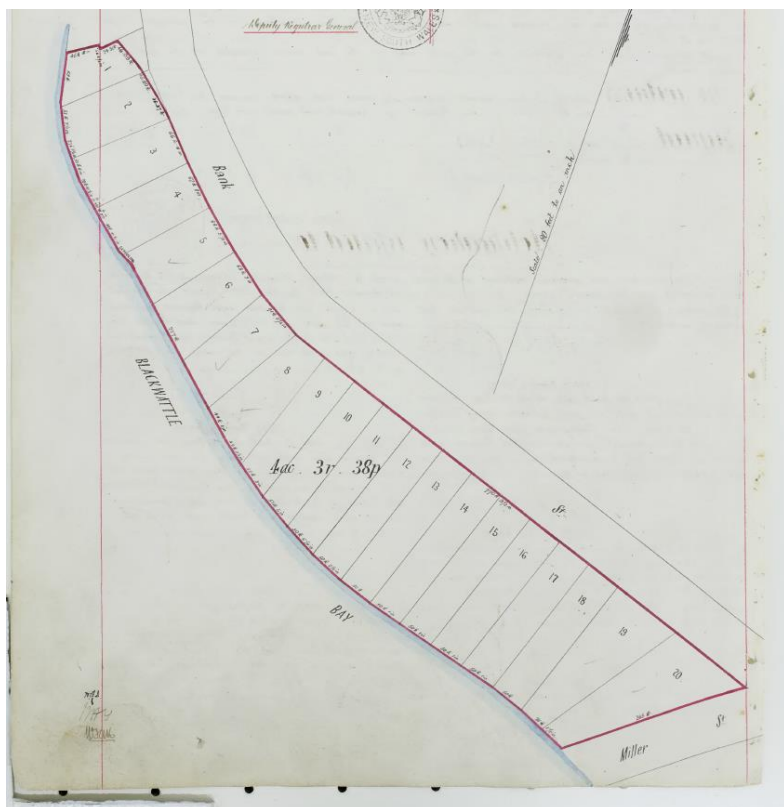
The Saunders' ran three key quarry sites known colloquially as Paradise (Miller and Bank Streets), Purgatory (Wattle Crescent and Allen Street) and Hell Hole (Wattle, Fig and Quarry Streets). Charles lived in Harris Street, not far from 'Paradise', while Robert lived for many years in Abattoirs Road (now Bank Street, also located near 'Paradise').¹⁵ Under his son Robert and grandson Robert Jr, the quarries continued to operate until the 1930s. The operation came to an end just prior to World War II as the demand for sandstone diminished and the use of steel and concrete became the preferred building materials.¹⁶

It is believed this area was quarried below road level following the invention of steam-powered quarrying machines and subsequently backfilled to the road level.¹⁷ Evidence of quarrying activities are located on the eastern side of Bank Street, in the surviving rock face which follows Bank Street.

The earliest Glebe Island Bridge and potentially the road (Bank Street/ Then Abbatoir Road) dates from 1857 and was built to connect Glebe Island and the Abattoirs to Pymont. The connection to Victoria Road was only discussed later in the 1860's. The study area remained virtually unoccupied up until the 1880's, possibly due to the irregular shoreline and proximity to the quarries and abattoir. The 1865 plan shows the area undeveloped apart from indications of two small wharfs and the location of the bridge to Glebe Island. No buildings are indicated in the subject area (Figure 12).

Large scale subdivision of the Ultimo side of the Harris Estate occurred in 1892-3. Long narrow waterfront blocks were established with a number of small wharves around Bank and Miller Street. Around 1900 the Glebe Island Bridge was opened, providing the first indication of a foreshore that closely resembles the shoreline of today.¹⁸ Bank Street was also known as Abattoir Road well into the 20th Century (Figure 14).

Figure 13 - 1899 Certificate of Title plan



Picture 30 - The subject site approximately corresponds to lots 1-11

Source: LPI Volume 1297 Folio 143

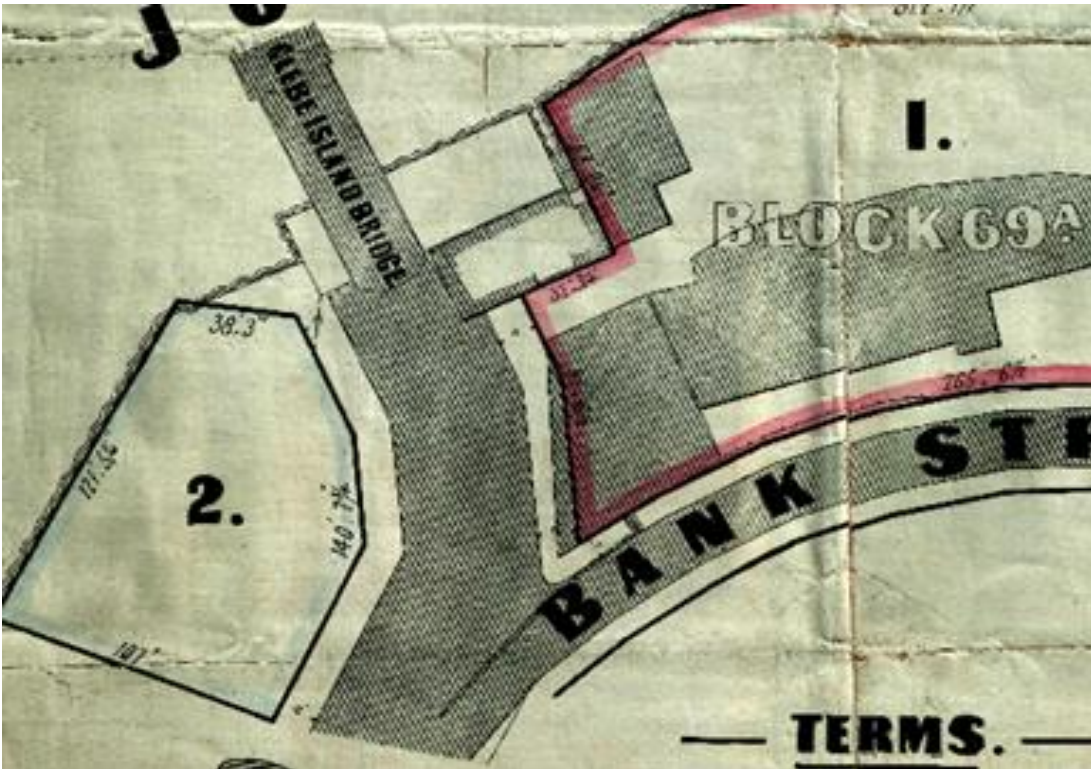
¹⁵ Dictionary of Sydney - Pymont

¹⁶ https://www.engineersaustralia.org.au/sites/default/files/HRP.Saunders_Quarry

¹⁷ JBS&G Bays Precinct Preliminary Site Investigation 2014

¹⁸ Maunsell/Aecom, Bank Street Master Plan 2006

Figure 14 - Subdivision Plans



Picture 31 - Subdivision of the Harris Estate No 14, 1896.

Source: City of Sydney Archives



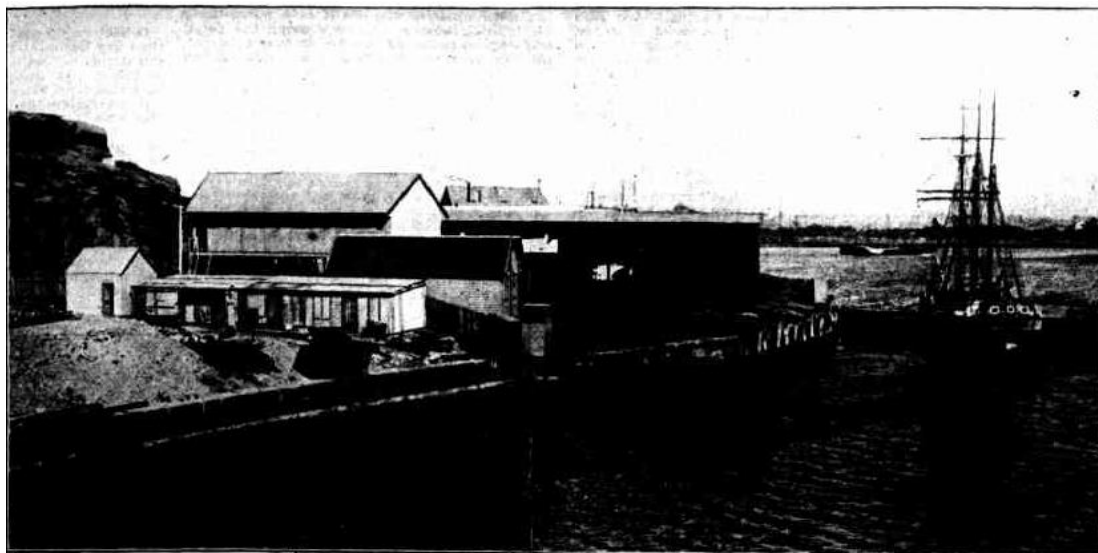
Picture 32 - Blackwattle Bay and Pyrmont land sale 1928.

Source: City of Sydney Archives

Despite the fact that there was limited activity and few wharves in the area, the shoreline suggests that any land reclamation had ceased by the 1920s, providing a potential date for the seawalls. A large jetty was constructed by 1911 and smaller jetties were in place by the late 1920s. In 1913 a tram line was established from the Glebe Island Bridge along Bank Street to the City and land in the area continued to be leased up until 1928 when the occupied allotments were sold at auction.

Whilst Abbatoir Road/ Bank Street was occupied in the latter part of the 19th century, (largely for industrial purposes including timber yards, steel works, skin stores etc) the northern most section on the waterfront including the subject site, appears to have remained largely vacant until around 1900. The subject site remained in the ownership of the Harris family at this time (refer to the Certificate of Title plan at Figure 13, with the subject site comprising approximately lots 4-11) with Sir Matthew Harris recorded as the property owner in 1899. Title documentation also documents a number of leases on the subject site including a lease to Armand Gilhou of lots 9 and 10 and lots 5-8 to Allen Taylor and Robert Murray in 1901.¹⁹ Lots 11-20 were transferred to Robert Sydney Harris in 1913 and lots 1-10 (including the subject site) were transferred to Albert Octavius Harris the following year.²⁰

Figure 15 - 1900 View of Augenson's timber yards (as viewed from the bridge)



Messrs G. Augenson and Company's Timber Yard and Wharf at Pyrmont.

(See letterpress on previous page.)

Picture 33 - Augenson secured a long lease for the site by 1900 and immediately built 'large commodious sheds approximately 130ft long'

Source: AUGENSON AND COMPANY. (1900, October 13). *Australian Town and Country Journal* (Sydney, NSW: 1870 - 1907), p. 33. Retrieved October 26, 2015, from <http://nla.gov.au/nla.news-article71389512>

The Sands Directory records the northern part of the peninsula in 1905 -25 as occupied by various timber merchants including Augenson's Wharf and timber merchants, Taylor Allen and Co (timber store) and Wallis Brothers sawmills, with Paul Puech's skin store located on the southern section of the site. The McEnnally Bros and Co. Ltd lightermen and tugboat proprietors are also recorded at the northern end of the bay, in the vicinity of the bridge, from 1915-1920. Cowlshaws Wharf and timber merchants had replaced Augenson's at the northern end of the bay by 1910. Puech's skin store was also known as Puech and Laeroyd in the early 1900s.

The 1928 land sale plan (above at Figure 14) indicates that the area of the subject site was leased to approximately 4 occupants, Pyrmont Timber Storage at the northern end of the peninsula (replacing Cowlshaws), Allen Taylor and Co, Wallis bros Timber Works and the southernmost lease to Armand Guilhou. The plan also shows that Taylors lease also comprised a large jetty on piles, (which is no longer extant) and a smaller former wharf attached to Wallis's timberyard and a crane. Partial views of the site in the 1930s are shown below at Figure 16.

The 1928 land sale plan (Figure 14) and views of the site (Figure 16) indicates that much of the development associated with this early occupation were utilitarian shed buildings of timber and lightweight construction, with the exception of the northernmost brick building, which is the only surviving early 20th century building (north of the site at 1 Bank Street); the fabric of which suggests early inter-war construction c.1920. 1 Bank Street remained in the ownership of the Harris family until 1930 when lots 1 and 2 were purchased by Charles Caminiti of Sydney, Fish Merchant.

¹⁹ Land and Property Information Certificate of Title Volume 1297 Folio 143

²⁰ Ibid

Figure 16 – Views of the site c.1930s



Picture 34 – View from Saunders street SW into Blackwattle Bay and showing Wallis timber yards (1934)

Source: City of Sydney archives, NSCA CRS 538/398



Picture 35 – View from Blackwattle Bay towards bank street with the subject site in the foreground, and Saunders Quarry and the incinerator behind (1933)

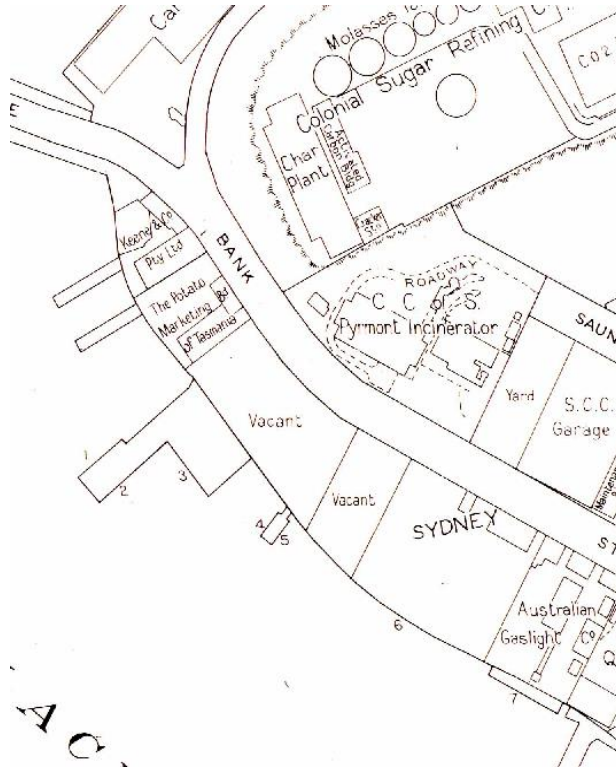
Source: City of Sydney archives, NSCA CRS 538/186a

Figure 17 – 20th Century Development of the site



Picture 36 – Civic Survey 1938-50. This plan shows the location of buildings on the site and the wharfs.

Source: City of Sydney Archives



Picture 37 – City Building Survey 1956. This plan shows that additional wharfs have been constructed in the northern part of the site after 1950.

Source: City of Sydney Archives

During World War II, the wharfs in the area, including the wharf at No.1 Bank Street, were used by the fishing fleet that later undertook mine sweeping operations. The Poulos site to the south of the subject site was also used as a Navy depot.²¹

²¹ Maunsell/Aecom, Bank Street Master Plan 2006

Figure 18 - 1949 aerial of the study area showing that much of the location is vacant storage area



Source: City of Sydney Archives

Figure 19 – The Changing foreshore- 1950s-1970s



Picture 38 – 1955 Aerial

Source: Lands and Property Information



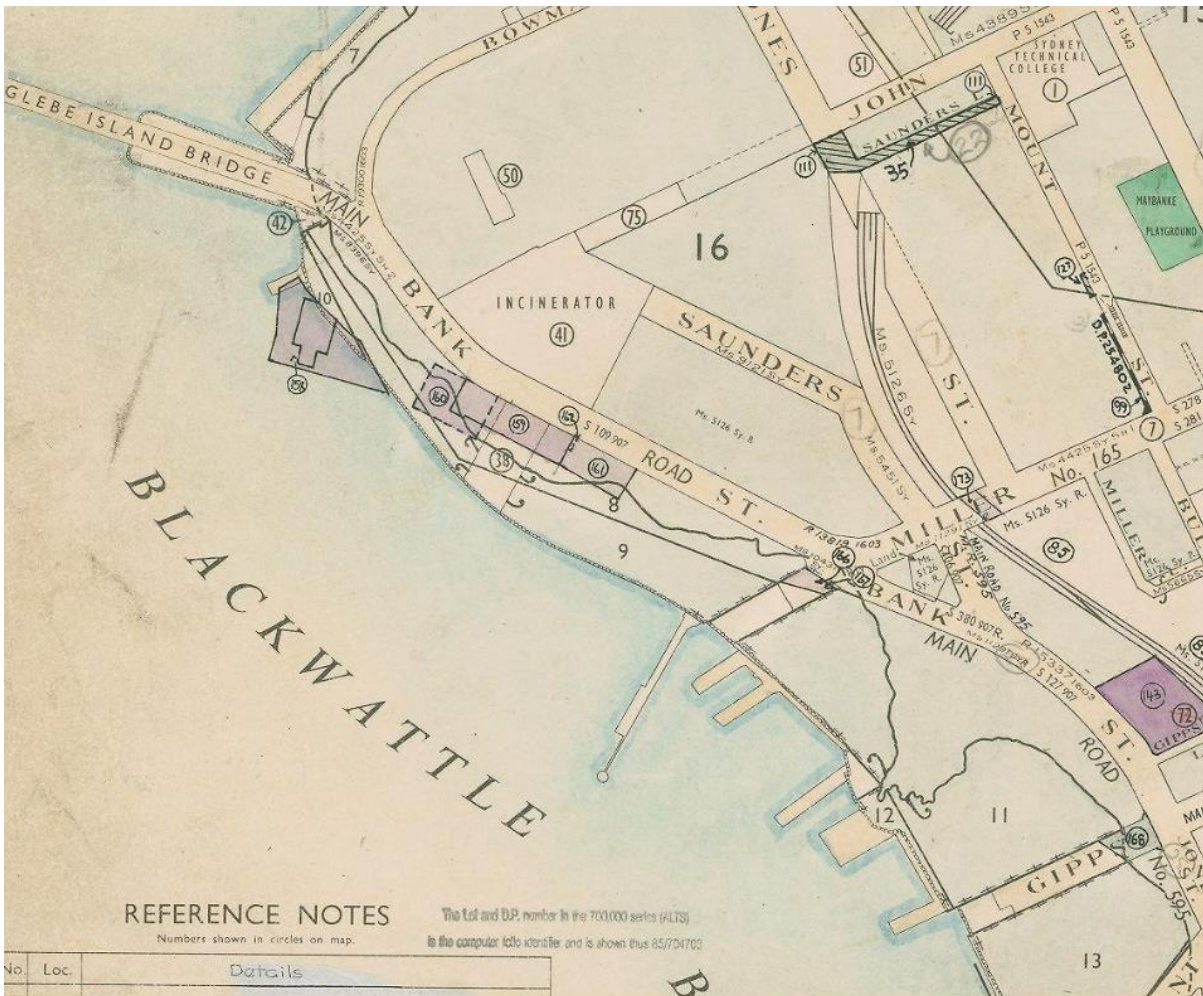
Picture 39 – 1972 Aerial

Source: Lands and Property Information

The southern section of the site is largely vacant by the mid-19th century, and the Taylor and Wallis timber yards buildings have been demolished, although aerials suggest that the land continued to be used for storage.

By 1949 the southernmost building at 1 Banks Street had been constructed and replaced an earlier building on the site (the present street façade likely dates to this period however the building has been variously altered since with numerous applications for alterations in the 1950s, 1960s and 1970s). Another building was constructed on the western edge of the allotment, however much of the land in the study area to the south remained vacant and was used for storage. City of Sydney archives indicate that 1 Bank Street was being used for the storage and grading of potatoes in the mid-1950s. By the 1970s the large jetty had been demolished but the adjacent land held materials possibly associated with the lots to the north. Various uses are documented for the site in the 1980s including boat building and sail making, marine service and maintenance; studio uses are recorded in 1983 including a pottery workshop and glass painting studio. It later partly functioned as an upholstery shop (1986).

Figure 20 - Parish Map (c.1970s)



Picture 40 - The plan shows the original shoreline and the extent of land reclamation on the subject site as well as indicating the stone seawall

The plan notes the following references with regard of the annotated numbers on the site: 10:Resumed for substation gazetted 1910; 38: Resumed by the Municipal Council of Sydney 1929; 42: resumed and vested in the maritime Services Board (MSB) March 1934; 158: acquired by Land acquisition act for state roads(1992); 159: acquired by Land acquisition act for state roads(1992); 160: easements for footings and support (for the Anzac Bridge) 1992

4. ARCHAEOLOGY

4.1. ARCHAEOLOGICAL POTENTIAL

Historical archaeology is the study of the past using physical evidence in conjunction with historical sources. It focuses on the objects used by people in the past and the places where they lived and worked. It can tell us about the way things were made and used and how people lived their daily lives. Archaeology is not just about objects and remains. It is also about landscapes and links between sites. Archaeology is assessed in two ways, the potential for the site to retain an archaeological resource and the significance of that resource.

Archaeological Potential is defined as:

The degree of physical evidence present on an archaeological site, usually assessed on the basis of physical evaluation and historical research. Common units for describing archaeological potential are:

- known archaeological features/sites (high archaeological potential);
- potential archaeological features/sites (medium archaeological potential);
- no archaeological features/sites (low archaeological potential).²²

A significant portion of this precinct has undergone large scale topographic modification including quarrying, infilling and reclamation. The period between 1856-1880 saw the foreshore land reclaimed. This is demonstrated by the presence of a sandstone block sea wall retaining mostly crushed sandstone and igneous gravel fill with occasional clay layers. The degraded sandstone seawall along a small portion of the foreshore provides the interface between the reclaimed land and the edge of the bay. It is the only visible feature that warrants retention as evidence of the previous use of the area. There is some potential for the survival of remnant wharfage, sea walls and structures on the previous shore line; however this is likely to be beneath several meters of fill. *As noted earlier this seawall lies outside the area of the proposed works referred to in this report.*

A number of excavations around the shoreline of the harbour have revealed previous sea walls and remains of wharves. North and South Barangaroo were excavated in 2013 by Austral Archaeology and Casey and Lowe respectively. The excavations at the north of the site have revealed part of shipbuilder James Munn's stone slipway which was constructed on the original shoreline and narrow beach prior to 1830. In addition, a massive sandstone wharf and seawall built by James Cuthbert by 1861, timber jetty piles, thick deposits of wood shavings resulting from boat building, a concrete seawall built between 1900 and 1910 and an extension of Munn Street that connected to the newly constructed Hickson Road and built at the same time on the newly-reclaimed land.²³

Excavation at south Barangaroo found evidence for the original rocky shoreline, sandstone seawalls and reclaimed land that formed wharf facilities for Henry Bass' shipyard in the 1830s, an informal boat ramp and structure made from irregular sandstone 'pavers', brick piers and timber, which was used during the 1830s, but buried by reclamation during the 1840s, a cottage built partially on a rocky outcrop and reclaimed land, remains of a large warehouse structure and a substantial sea wall dating to the 1840s.²⁴

Most of the study area is flat low lying reclaimed land. There is a ground level difference of approximately three to four metres from the road level at Bank Street to the water's edge. This embankment may represent the location of the early shoreline or possibly the edge of the quarry face. A small section of reclaimed land has been added to the shore at the western end of the study area for the Anzac Bridge Eastern Pylon. This portion is contained behind a concrete in-situ sea wall, while the remainder of the study area is retained with older sandstone, concrete or other stone sea walls.²⁵ The study area has been predominantly vacant land used for storage or occupied by mostly temporary buildings since the late 19th century, some of which have been replaced or removed.

²² Department of Urban Affairs and Planning 1996

²³ <http://www.barangaroo.com/media/103397/archaeological%20site%20open%20day%20june%202013.pdf>

²⁴ <http://www.caseyandlowe.com.au/sitebarangaroosouth.htm>

²⁵ Ibid

Presumably the sandstone sea wall in the study area originally lined the bay shore from south of the fish markets to the Glebe Island Bridge abutment, but has been removed or disturbed in sections due to development on the waterfront. The sea wall is likely to date to the early 1900s and it is also possible that it has been modified or rebuilt since the 1980s.²⁶ Sections of sea wall and remains of wharves can be found in many parts of Sydney Harbour.

The potential archaeological resource in the study area might include evidence of the original shore line, evidence of reclamation, earlier sea walls, previous wharf structures, previous buildings, evidence of land use and other maritime activities.

The construction of the Anzac Bridge is likely to have resulted in the disturbance or removal of potential archaeological resource in the northern part of the site. The southern section of the site has been previously occupied by less substantial structures (timber shed buildings that are unlikely to have had substantial footings) and has been left largely vacant since the mid-20th century. Therefore the potential for the study area to contain an archaeological resource related to the development and use of the land is low.

4.2. ARCHAEOLOGICAL SIGNIFICANCE

In NSW the process of finding out whether an item is important is called assessing significance.

Archaeological sites, which contain 'relics' as defined in the NSW Heritage Act, are managed like any other significant item of environmental heritage. They should be treated in the same way with the same level of consideration and assessment process as any other surviving physical evidence of the past such as buildings, works, precincts, landscapes or other places and items with potential or known heritage value. Significance is thus an expression of the cultural value afforded a place, site or item.

The Heritage Council of NSW has developed a set of seven criteria for assessing heritage significance, which can be used to make decisions about the heritage value of a place or item. These include Historic, Social, Associative, Aesthetic, Scientific/Technical, Rarity and Representative.

Archaeological significance has long been accepted as linked directly to archaeological (or scientific) research potential:

A site or resource is said to be scientifically significant when its further study may be expected to help answer questions. That is scientific significance is defined as research potential.²⁷

There are two levels of heritage significance used in NSW: State and local.

The sandstone sea wall is fairly intact in the northern part of the study area but only partially intact in the region of the commercial and industrial buildings. There are also the remains of several wharves in the study area and in the vicinity. The remains of sea walls and wharves are not rare in Sydney Harbour and unless the items are considerably intact, very early or associated with important activities, wharves and sea walls are generally not significant items in their own right.

There is potential for remains associated with the former early industrial uses of the site (timber yards and skin store) however if present, these would be unlikely to be substantial and are unlikely to have the potential to provide new or significant information about the sites history, the uses of which have been documented. Significance is therefore assessed as low.

²⁶ Maunsell/Aecom, Bank Street Master Plan 2006

²⁷ Heritage Branch 2009 Assessing Significance for Historical Archaeological Sites and Relics (Bickford and Sullivan, 1984 pp 23–24).p:8

5. HERITAGE SIGNIFICANCE

5.1. WHAT IS HERITAGE SIGNIFICANCE?

Before making decisions to change a heritage item, an item within a heritage conservation area, or an item located in proximity to a heritage listed item, it is important to understand its values and the values of its context. This leads to decisions that will retain these values in the future. Statements of heritage significance summarise a place's heritage values – why it is important, why a statutory listing was made to protect these values.

5.2. SIGNIFICANCE ASSESSMENT

The Heritage Council of NSW has developed a set of seven criteria for assessing heritage significance, which can be used to make decisions about the heritage value of a place or item. There are two levels of heritage significance used in NSW: state and local.

The following assessment of heritage significance has been prepared in accordance with the 'Assessing Heritage Significance' (2001) guides.

Table 1 – Assessment of heritage significance

Criteria	Significance Assessment
<p>A – Historical Significance</p> <p><i>An item is important in the course or pattern of the local area's cultural or natural history.</i></p>	<p>The subject site reflects the subdivision and development of the western side of the peninsula for maritime and industrial uses in the latter part of the 19th and early 20th centuries. The site is not considered to meet the threshold for listing.</p>
<p>Guidelines for Inclusion</p> <p>shows evidence of a significant human activity <input type="checkbox"/></p> <p>is associated with a significant activity or historical phase <input type="checkbox"/></p> <p>maintains or shows the continuity of a historical process or activity <input type="checkbox"/></p>	<p>Guidelines for Exclusion</p> <p>has incidental or unsubstantiated connections with historically important activities or processes <input type="checkbox"/></p> <p>provides evidence of activities or processes that are of dubious historical importance <input type="checkbox"/></p> <p>has been so altered that it can no longer provide evidence of a particular association <input checked="" type="checkbox"/></p>
<p>B – Associative Significance</p> <p><i>An item has strong or special associations with the life or works of a person, or group of persons, of importance in the local area's cultural or natural history.</i></p>	<p>Does not meet the criterion. Various uses and occupations for the site have been noted however research does not demonstrate significant extended associations.</p>
<p>Guidelines for Inclusion</p> <p>shows evidence of a significant human occupation <input type="checkbox"/></p> <p>is associated with a significant event, person, or group of persons <input type="checkbox"/></p>	<p>Guidelines for Exclusion</p> <p>has incidental or unsubstantiated connections with historically important people or events <input type="checkbox"/></p> <p>provides evidence of people or events that are of dubious historical importance <input type="checkbox"/></p>

Criteria	Significance Assessment
<p>C – Aesthetic Significance</p> <p><i>An item is important in demonstrating aesthetic characteristics and/or a high degree of creative or technical achievement in the local area.</i></p>	<p>has been so altered that it can no longer provide evidence of a particular association <input type="checkbox"/></p> <p>The seawall, located in the broader subject site, demonstrates some aesthetic significance as a c.1900 seawall of masonry block construction (albeit with some modifications and some missing sections). However this is not located within the proposed works referred to in this report.</p> <p>Collectively the site is not considered to meet the threshold for significance.</p>
<p>Guidelines for Inclusion</p> <p>shows or is associated with, creative or technical innovation or achievement <input type="checkbox"/></p> <p>is the inspiration for a creative or technical innovation or achievement <input type="checkbox"/></p> <p>is aesthetically distinctive <input type="checkbox"/></p> <p>has landmark qualities <input type="checkbox"/></p> <p>exemplifies a particular taste, style or technology <input type="checkbox"/></p>	<p>Guidelines for Exclusion</p> <p>is not a major work by an important designer or artist <input checked="" type="checkbox"/></p> <p>has lost its design or technical integrity <input type="checkbox"/></p> <p>its positive visual or sensory appeal or landmark and scenic qualities have been more than temporarily degraded <input checked="" type="checkbox"/></p> <p>has only a loose association with a creative or technical achievement <input type="checkbox"/></p>
<p>D – Social Significance</p> <p><i>An item has strong or special association with a particular community or cultural group in the local area for social, cultural or spiritual reasons.</i></p>	<p>Does not meet the criterion.</p>
<p>Guidelines for Inclusion</p> <p>is important for its associations with an identifiable group <input type="checkbox"/></p> <p>is important to a community's sense of place <input type="checkbox"/></p>	<p>Guidelines for Exclusion</p> <p>is only important to the community for amenity reasons <input type="checkbox"/></p> <p>is retained only in preference to a proposed alternative <input checked="" type="checkbox"/></p>
<p>E – Research Potential</p> <p><i>An item has potential to yield information that will contribute to an understanding of the local area's cultural or natural history.</i></p>	<p>The site has been assessed as being of low archaeological potential and low significance. Whilst it is acknowledged that there is potential for remains associated with the former early industrial uses of the site (timber yards and skin store), however if present, these remains would be unlikely to be substantial and are unlikely to have the potential to provide new or significant information about the sites history, the uses of which have been documented.</p>
<p>Guidelines for Inclusion</p>	<p>Guidelines for Exclusion</p>

Criteria has the potential to yield new or further substantial scientific and/or archaeological information <input type="checkbox"/> is an important benchmark or reference site or type <input type="checkbox"/> provides evidence of past human cultures that is unavailable elsewhere <input type="checkbox"/>	Significance Assessment the knowledge gained would be irrelevant to research on science, human history or culture <input type="checkbox"/> has little archaeological or research potential <input checked="" type="checkbox"/> only contains information that is readily available from other resources or archaeological sites <input checked="" type="checkbox"/>
F – Rarity <i>An item possesses uncommon, rare or endangered aspects of the local area’s cultural or natural history.</i>	Does not meet the criterion.
Guidelines for Inclusion provides evidence of a defunct custom, way of life or process <input type="checkbox"/> demonstrates a process, custom or other human activity that is in danger of being lost <input type="checkbox"/> shows unusually accurate evidence of a significant human activity <input type="checkbox"/> is the only example of its type demonstrates designs or techniques of exceptional interest <input type="checkbox"/> shows rare evidence of a significant human activity important to a community <input type="checkbox"/>	Guidelines for Exclusion is not rare <input checked="" type="checkbox"/> is numerous but under threat <input type="checkbox"/>
G – Representative <i>An item is important in demonstrating the principal characteristics of a class of NSWs (or the local area’s):</i> <i>cultural or natural places; or</i> <i>cultural or natural environments.</i>	Does not meet the criterion.
Guidelines for Inclusion is a fine example of its type <input type="checkbox"/> has the principal characteristics of an important class or group of items <input type="checkbox"/> has attributes typical of a particular way of life, philosophy, custom, significant process, design, technique or activity <input type="checkbox"/>	Guidelines for Exclusion is a poor example of its type <input type="checkbox"/> does not include or has lost the range of characteristics of a type <input checked="" type="checkbox"/> does not represent well the characteristics that make up a significant variation of a type <input type="checkbox"/>

Criteria	Significance Assessment
is a significant variation to a class of items <input type="checkbox"/>	
is part of a group which collectively illustrates a representative type <input type="checkbox"/>	
is outstanding because of its setting, condition or size <input type="checkbox"/>	
is outstanding because of its integrity or the esteem in which it is held <input type="checkbox"/>	

5.3. STATEMENT OF SIGNIFICANCE

The site collectively is not considered to meet the threshold for listing.

The remaining sandstone sea wall in the broader study area has some aesthetic and historic significance as representative of the development and settlement of Pymont in the late 19th / early 20th century and the associated maritime and industrial functions of the area. It is noted that this seawall is not located within the area of the works referred to in this report.

5.4. STATEMENTS OF SIGNIFICANCE FOR ITEMS IN THE VICINITY

The following statements of significance are provided for heritage items in the vicinity of the subject site and have been sourced from the heritage inventory listings for the respective sites.

5.4.1. The Saunders Quarry²⁸

The quarry dates from the key period of Pyrmont as a source of quality construction materials used in prominent Sydney buildings and particularly those designed by Government Architect James Barnet and also internationally.

5.4.2. Statement of Significance – the former Glebe Island Bridge²⁹

The Glebe Island Bridge, across Johnstons Bay, is of state significance as it demonstrates one of the earliest examples of an electric-powered swing bridge in Australia. Technically, it is a complementary structure to the already acclaimed Pyrmont Swing Bridge, and has all the same significant features, including the electrically-driven swing span. Both bridges were designed by Percy Allan, a highly-regarded Australian bridge designer of the late 19th and early 20th century. Both represent the only examples of such types of bridges in New South Wales and are still operable.

²⁸ Office of Environment and Heritage: Escarpment Face from Former Saunders Quarry:
<http://www.environment.nsw.gov.au/heritageapp/ViewHeritageItemDetails.aspx?ID=2424701>

²⁹ Office of Environment and Heritage: Glebe Island Bridge:
<http://www.environment.nsw.gov.au/heritageapp/ViewHeritageItemDetails.aspx?ID=5051118>

6. IMPACT ASSESSMENT

6.1. HERITAGE LISTING

The subject property is not a listed heritage item under any statutory authority. The site is however in the vicinity of heritage items, as set out in Table 2 below and as shown in Figure 21 and Figure 22.

Figure 21 – Sydney LEP2012 Heritage maps (sheet 007) subject site circled.



Source: City of Sydney Council

The following listings apply to the precinct. Where applicable, items have been listed under multiple registers (e.g. the Glebe Island Bridge, which is listed under the SHR, the SREP and various S170 registers).

Table 2 – Relevant Heritage Listings

Item/ address	Listing
State Heritage Register	
Glebe Island Bridge / RTA Bridge No. 61	Heritage Act - State Heritage Register 01914
City of Sydney Local Environment Plan 2012	
Escarpment face from former quarry "Saunders' Quarry"/ Lot 3, DP 839057; Lot 22, DP 1008425; Lot 100, DP 1013159; Lots 602 and 603, DP 1010086; Lot 37, DP 1071670; Lots 59, 61 and 62, DP 270215	City of Sydney Local Environment Plan 2012 / Item #1199
Pymont and Glebe Railway Cuttings and Tunnel	SLEP2012 Item#I1203

Section 170 Register	
Anzac Bridge	Heritage Act - s.170 NSW State agency heritage register - RMS item # 4305018
Glebe Island Bridge / RTA Bridge No. 61	Heritage Act - s.170 NSW State agency heritage register, Roads & maritime Services Heritage and Conservation Register Item # 4301666
Glebe Island Bridge / RTA Bridge No. 61	Sydney Ports s.170 Register Listing No. 4560015 (bridge approaches)
SREP (Sydney Harbour Catchment) 2005	
Glebe Island Bridge / RTA Bridge No. 61	SREP (Sydney Harbour Catchment) 2005 Item #68

Figure 22 - SHR Curtilage for The Glebe Island Bridge



Source: Office of Environment and Heritage

6.2. STATUTORY CONTROLS

6.2.1. Local Environmental Plan

The proposed works are addressed in the table below in relation to the relevant clauses in the LEP.

Table 3 – Relevant LEP Clauses

CLAUSE	DISCUSSION
<p>5.10 Heritage conservation</p> <p>(1) Objectives The objectives of this clause are as follows:</p> <p>(a) to conserve the environmental heritage of the City of Sydney,</p> <p>(b) to conserve the heritage significance of heritage items and heritage conservation areas, including associated fabric, settings and views,</p> <p>(c) to conserve archaeological sites,</p>	<p>The subject site is not subject to any statutory listings however is in the vicinity of heritage items, specifically the former Saunders Quarry escarpment face, the Glebe Island bridge to the north of the site, and the Anzac Bridge which is constructed over and which has an easement over part of the subject site.</p> <p>The proposed works are minor, generally reversible and in keeping with the varied maritime uses and character of the site and Blackwattle Bay more generally. The proposed works provide for improved amenity and public access as well as improved equitable access, with no impact to the heritage items in the vicinity (as detailed below) and no impact to recognised significant site elements, specifically the remnant sandstone seawall.</p> <p>The subject site has been assessed and is considered to be of low archaeological potential and significance.</p>
<p>(4) Effect of proposed development on heritage significance</p> <p>The consent authority must, before granting consent under this clause in respect of a heritage item or heritage conservation area, consider the effect of the proposed development on the heritage significance of the item or area concerned. This subclause applies regardless of whether a heritage management document is prepared under subclause (5) or a heritage conservation management plan is submitted under subclause (6).</p>	<p>The remaining sandstone sea wall on the site has some aesthetic and historic significance as representative of the development and settlement of Pyrmont in the late 19th century/ early 20th century and the associated maritime and industrial functions of the area.</p> <p>The proposed temporary administration and operational facilities are, by their nature, temporary and reversible. They are necessary to ensure the use of the wharf during the implementation of the masterplan. The facilitates are proportionate in terms of scale to the existing development in the vicinity of the subject site (to the west). They would further be horizontally proportioned and would therefore not visually dominate the context of the proximate heritage listed items.</p> <p>No heritage impact will be associated with the installation and use of the temporary facilities. Some minor earthworks associated with the installation are not anticipated to have any physical impact on the</p>

<p>CLAUSE</p>	<p>DISCUSSION</p>
<p>(5) Heritage assessment The consent authority may, before granting consent to any development:</p> <ul style="list-style-type: none"> (a) on land on which a heritage item is located, or (b) on land that is within a heritage conservation area, or (c) on land that is within the vicinity of land referred to in paragraph (a) or (b), <p>require a heritage management document to be prepared that assesses the extent to which the carrying out of the proposed development would affect the heritage significance of the heritage item or heritage conservation area concerned.</p>	<p>significant escarpment opposite at they are separated by an existing two lane road.</p> <p>Proposed landscaping works including provision of the access paths, minor site regrading, and installation of gravel, turf and bollards are minor and do not affect significant fabric or finishes.</p> <p>Archaeological potential has been assessed as low.</p> <p>No works are proposed to the bridge pylon within the site.</p> <p>As identified in section 6.1 above, the site is in the vicinity of heritage items, specifically the Glebe Island Bridge (listed under the SHR, the SREP and s.170 Registers) the former Saunders Quarry escarpment face (listed under the City of Sydney LEP) and the Anzac bridge (listed under the S.170 register). This HIS has been prepared to assist the consent authority/ies in their assessment of the subject proposal.</p> <p>The proposed boating facilities are located within a highly prominent position within the landscape and within the view shed of the significant Anzac Bridge and Glebe Island Bridge. The positioning of the facilities has considered the impact on views to and from these heritage items however, whilst ensuring the facilities meet engineering and operational requirements. It is considered that the proposed works are minimal and will not dominate the respective heritage items in views from Blackwattle Bay. The proposed works are generally reversible and in keeping with the varied maritime uses and character of the site and Blackwattle Bay more generally as well as facilitating greater public interaction with the foreshore and bay. The quarry escarpment presently forms part of a back drop of views to the peninsula and this visual relationship is undiminished by the subject works. Similarly, the modest works will not impact on the Glebe Island or Anzac bridges, which dominate the peninsula, due to their scale.</p>

6.3. HERITAGE OFFICE GUIDELINES

The proposed works are addressed in relation to relevant questions posed in the Heritage Office's 'Statement of Heritage Impact' guidelines

Table 4 – Relevant Questions

QUESTION	DISCUSSION
<p>The following aspects of the proposal respect or enhance the heritage significance of the item or conservation area for the following reasons:</p>	<p>The proposal is considered sympathetic for the following reasons:</p> <p>The proposal has been designed in conjunction with heritage advice and considers the heritage context of the site and heritage items in the vicinity (as outlined above)</p> <p>The proposed works are minor, do not affect significant fabric, finishes or features of the site and are in keeping with the general mixed uses of the area</p>
<p><i>The following aspects of the proposal could detrimentally impact on heritage significance.</i></p> <p>The reasons are explained as well as the measures to be taken to minimise impacts:</p>	<p>There are no works which are regarded as potentially detrimental.</p>
<p>The following sympathetic solutions have been considered and discounted for the following reasons:</p>	<p>N/A</p>
<p><i>New development adjacent to a heritage item</i></p> <p>How does the new development affect views to, and from, the heritage item?</p> <p>What has been done to minimise negative effects?</p> <p>How is the impact of the new development on the heritage significance of the item or area to be minimised?</p> <p>Why is the new development required to be adjacent to a heritage item?</p> <p>How does the curtilage allowed around the heritage item contribute to the retention of its heritage significance?</p> <p>Is the development sited on any known, or potentially significant archaeological deposits?</p> <p>If so, have alternative sites been considered? Why were they rejected?</p> <p>Is the new development sympathetic to the heritage item?</p>	<p>As outlined above in section 6.1 and 6.2.1, the site is in the vicinity of heritage items, specifically the former Saunders Quarry escarpment face, the Glebe Island bridge to the north of the site, and the Anzac Bridge which is constructed over and which has an easement over part of the subject site.</p> <p>The proposed temporary works consist of a facility to support the ongoing operational requirements of the wharf until the new masterplan for the area is implemented. This would be by nature, temporary and reversible. The form and scale of the temporary structure would be proportionate to the scale of the existing items in the vicinity and would not visually dominate the settings of the proximate heritage items.</p> <p>The proposed works will have no significant impact on the setting of the heritage items as the proposal retains those indicative early features that could arguably contribute to the setting of the heritage items and the historic context of the area generally, specifically the sandstone seawall. New works are modestly scaled, incorporating provision of the new</p>

QUESTION	DISCUSSION
<p>In what way (e.g. form, siting, proportions, design)?</p> <p>Will the additions visually dominate the heritage item?</p> <p>How has this been minimised?</p> <p>Will the public, and users of the item, still be able to view and appreciate its significance?</p>	<p>deck, installation of temporary administration and operational facilities, a small section of concrete, site landscaping including minor site regrading, installation of gravel, turf and bollards and installation of a new asphalt universal access ramp. This is characteristic of the general uses of the site and changing uses of the waterfront generally.</p> <p>The quarry escarpment presently forms part of a back drop of views to the peninsula and this visual relationship is undiminished by the subject works. Similarly, the modest works will not impact on the Glebe Island or Anzac bridges, which dominate the peninsula, due to their scale.</p>

7. CONCLUSION AND RECOMMENDATIONS

The proposed works provide for improved amenity and public access as well as improved equitable access, with no impact to the heritage items in the vicinity and negligible impact to recognised significant site elements, specifically the remnant sandstone seawall, which is located outside the area of the works referred to in this report. Works generally affect fabric and finishes of no significance and works over water generally affect previously modified sections of the shoreline.

The proposal has been designed in conjunction with heritage advice and considers the significant context of the site and heritage items in the vicinity. The proposed works are modestly scaled and will have no significant impact on the setting of the heritage items as the proposal retains those indicative early features that could arguably contribute to the setting of the heritage items and the historic context of the area generally. The proposed works are minimal, will be apparent as contemporary insertions and are in keeping with the general character and use of the area. As such, the works will not dominate in views of the peninsula, particularly with consideration for the scale of the heritage listed items in the vicinity, in particular the Glebe Island and Anzac Bridges.

Quarrying was the major industry in the Pyrmont area from the 1850s and the study area remained virtually unoccupied up until the very late 1800s. The site has been assessed as being of low archaeological potential and low significance. Whilst it is acknowledged that there is potential for remains associated with the former early industrial uses of the site (timber yards and skin store), however if present, these remains would be unlikely to be substantial and are unlikely to have the potential to provide new or significant information about the sites history, the uses of which have been documented.

The potential archaeological resource in the study area might include evidence of the original shore line, evidence of reclamation, earlier sea walls, previous wharf structures, previous buildings, evidence of land use and other maritime activities. The construction of the Anzac Bridge is likely to have resulted in the disturbance or removal of potential archaeological resource in the northern part of the site. The site has been previously occupied by less substantial structures (timber shed buildings that are unlikely to have had substantial footings) and has been left largely vacant since the mid-20th century.

Areas of low archaeological sensitivity do not require subsurface archaeological investigations prior to works commencing. However, should artefacts be identified during works, works should stop immediately and the unexpected finds procedure should be followed. An unexpected finds policy follows:

- 1) Stop work in the affected area and protect item.
- 2) Contact a suitably qualified archaeological or heritage consultant to provide advice and assess the item if required.
- 3) Notify the appropriate regulatory authority (such as the Heritage Division) and obtain statutory approvals (if required).
- 4) Implement archaeological or heritage management plan.
- 5) Resume work.

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[Note: Some government departments have changed their names over time and the above publications state the name at the time of publication.]

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