

Application for Modification of Major Project Approval 10_0204

Modification No. 2

Property:

Montefiore Street, Catherine Hill Bay

Applicant:

Coastal Hamlets Pty Ltd

Date:

January 2014

Document Control

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1.0 Introduction

Coastal Hamlets Pty Ltd seeks to amend the approval under Part 3A of the EPA Act 1979 for subdivision at Catherine Hill Bay.

The proposed amendments are:

- Changes to sections of perimeter road with consequent minor lot boundary changes and road pavement width reduction;
- Minor boundary adjustments to the approved subdivision plan to locate electrical kiosks on private land; and
- Revision of the lot layout with a redistribution of lots between stages 1, 2, and 5.

The need for the amendments has arisen from detailed subdivision design and reflects compliance with engineering and road design practice and recent authority policy for electrical infrastructure location.

2.0 Background

Approval for Major Project 10_0204 was granted by the Planning Assessment Commission (PAC) in May 2011.

The approval, condition A7, required a range of changes to be made to the submitted subdivision plan which were then agreed by the PAC.

A change to the subdivision layout as approved by PAC is considered to be a change to the major project approval.

Statutory Situation

The relevant requirements for modification of major project approvals is found in s.75W of the EPA Act as it applied to the approval. Although this section of the Act has been repealed it still applies to major project determinations made prior to the repeal of Part 3A.

Schedule 6A of the EP&A Act sets out the transitional arrangements for the repeal of Part 3A of the Act. Clause 2 – Transitional Part 3A projects defines transitional Part 3A projects as follows:

*2 (1) The following are, subject to this Schedule, **transitional Part 3A projects**:*
(a) an approved project (whether approved before or after the repeal of Part 3A),
(b) a project that is the subject of an approved concept plan (whether approved before or after the repeal of Part 3A),

The project application was approved prior to the repeal of Part 3A and is therefore a transitional Part 3A project. Clause 3 of Schedule 6A provides that "*Part 3A of this Act (as in force immediately before the repeal of that Part and as modified under this Schedule after that repeal) continues to apply to and in respect of a transitional Part 3A project.*" The repealed provisions of Part 3A continue to apply to Project approvals, which includes section 75W dealing with modifications of approvals.

75W Modification of Minister's Approval

(1) In this section:

Minister's approval means an approval to carry out a project under this Part, and includes an approval of a concept plan.

modification of approval means changing the terms of a Minister's approval, including:

- (a) revoking or varying a condition of the approval or imposing an additional condition of the approval, and*
- (b) changing the terms of any determination made by the Minister under Division 3 in connection with the approval.*

(2) The proponent may request the Minister to modify the Minister's approval for a project. The Minister's approval for a modification is not required if the project as modified will be consistent with the existing approval under this Part.

75W(1) is satisfied as Major Project 10_0204 was approved prior to repeal of part 3A.

The current proposal meets the definition of “modification of approval” as the changes requested are to the approved plans for the project, which are specifically part of the terms of approval, see condition A7 of the approval.

The proposed modifications will not result in the development being inconsistent with original approval. However, as the subdivision layout is prescribed as part of the terms of approval any change to the approved subdivision plans requires approval under 75W. This is reinforced by the circumstances of assessment and approval of the major project where the subdivision layout was a highly significant issue.

Background to the subdivision layout modifications sought

There are a number of engineering and practical issues in relation to the approved plan. To resolve these issues a number of amendments to the approved plan are sought.

A set of design principles for the subdivision were set by the Independent Hearing and Assessment Panel that assessed and approved the original subdivision proposal. The principles became one of the assessment criteria for the second and current approval.

The IHAP principles are set out below.

1. *Any development should comply with the principles and intent contained in the NSW Government's Coastal Policy (1997) and the Coastal Design Guidelines for NSW (2003) particularly section 1.6 New Coastal Settlements: Villages and Hamlets.*
2. *The panel considers that the area covered by the Concept Plan at Catherine Hill Bay falls into three distinct precincts:*
 - (i) *Precinct 1 – the area north of the ridgeline including:*
 - a) *The existing established village and surrounding visual catchment; and*
 - b) *Area outside the existing village's visual catchment, including denuded areas and uses/structures associated with former mining activities.*
 - (ii) *Precinct 2 – the area south of the ridgeline in the vicinity of Moonee Beach; and*
 - (iii) *Precinct 3 – the two areas to the north of Montefiore Street and west of the existing village.*
3. *Development should not intrude into the visual catchment of the existing Catherine Hill Bay village.*
4. *There is potential for residential development within Precinct 2 but any such development should be designed having regard to a classic McHarg landscape design analysis of the site and should include the following considerations:*
 - (i) *The environmental attributes of the area and sensitive view sheds including views from the beach and Montefiore Street; and*
 - (ii) *The surrounding context including bush land and the conservation area.*

In particular the development design should:

- a) be low in scale and impact and be nestled within a landscaped setting with significant tree retention;*
- b) Be sensitively sited and not located on headlands, ridgelines or dunes;*
- c) Be sufficiently setback from the Moonee Beach to provide visual separation from the beach;*
- d) Provide for public access Moonee Beach including the provision of discreet public parking areas in locations adjacent to walking paths which provide access to the beach;*
- e) Provide a public road adjacent to the beachfront reserve and other interfaces between the development and the surrounding bush land; and*
- f) Identify individual lots, building footprints, maximum building heights etc, to ensure an appropriate character(s) for any development and provide for a detail and approval process for all dwellings to ensure the proposed designs have regard to the sensitive coastal location.*

A subdivision layout consistent with the principles was lodged with NSW DoPI as part of the Preferred Project Report. The layout as lodged had been fully assessed. The McHarg analysis required under the HIAP principles had identified topographically constrained areas which were accommodated in the subdivision design. There was no perimeter road on the north eastern side of Stage 1 due to physical constraints. Bushfire was addressed by provision of APZ s on the northeast boundary.

The Preferred Project Report was assessed by DoPI and approved by the Planning and Assessment Commission. The approval was however subject to modifications to the subdivision layout in accordance with Condition A7 of the approval. The modification requirements were based on the IHAP principles and achieving full compliance with Planning for Bushfire Protection but had not been subject to full assessment and design. That there might be problems with the particular section of road to the north east of stage 1 was acknowledged in the project assessment report and by the wording of condition A7.

The project assessment report, (Section 7.4 a) Perimeter Road) states that,

"The Commission noted Road 20 may only be extended to lot 1039 as the gradient from thereon north may be too steep to enable a connection to Road 17 or Road 18".

Condition A7 (e) of the approval is,

"Road 20 is to be extended to Lot 1039 and where practical to connect to Road 18 to ensure appropriate bushfire protection for allotments in this area".

The amended Road 20 was included in the subdivision layout as it was possible to achieve an acceptable road gradient. On detailed design of the road it was found that the works needed to achieve the gradient were impractical. Accordingly the deletion of a section of the perimeter road is sought. The deletion is consistent with condition A7(e) in that this section of road is unpractical as foreseen by the condition.

A very minor modification of the layout is sought to accommodate electrical kiosks.

The project approval limits the number of residential lots to 550. The lots are allocated over 7 stages under the approved subdivision design. A reallocation of lots between stages is sought.

3.0 Modification to Remove Part of a Perimeter Road

Since the project approval road and lot numbers have changed. Road 20 of the original approval is now Road 23. Lot 1039 is now

As approved, Road 23 in Stages 1 and 2 of the subdivision provides a perimeter road to the northeast of Stage 1 as shown in **Images 1 and 2** below.

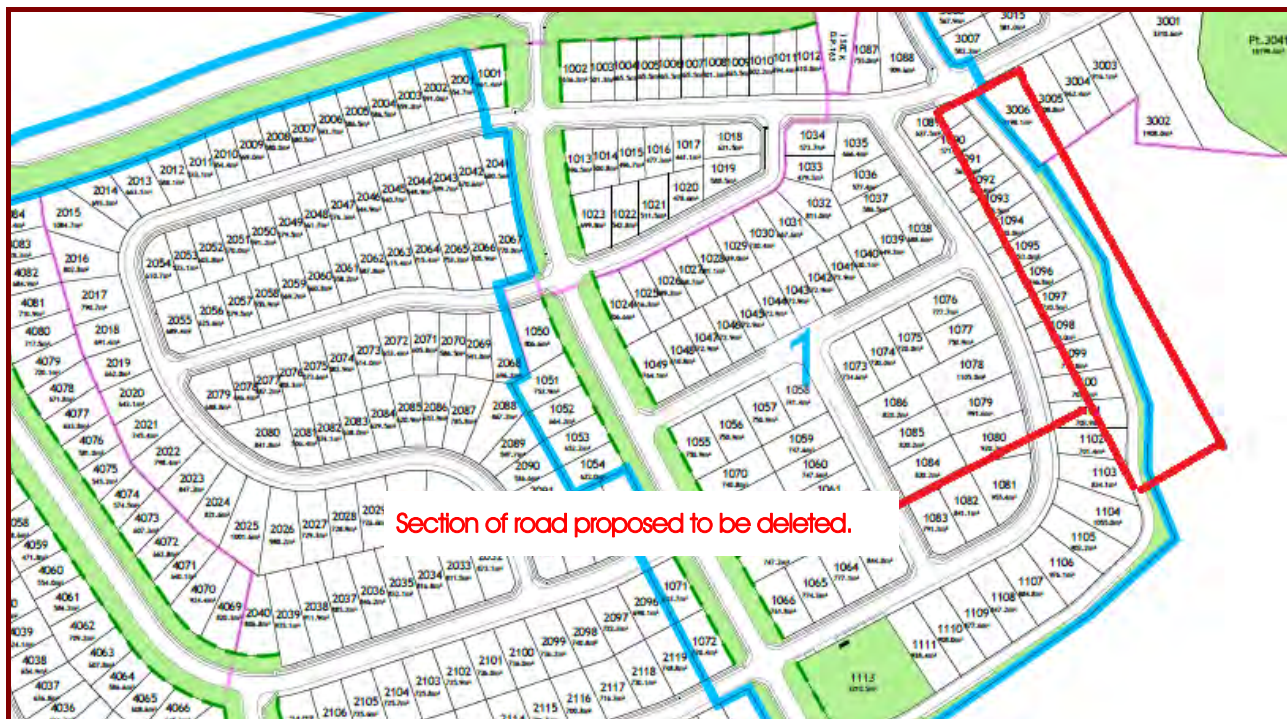


Image 1: Location of section of road 23 proposed to be deleted.

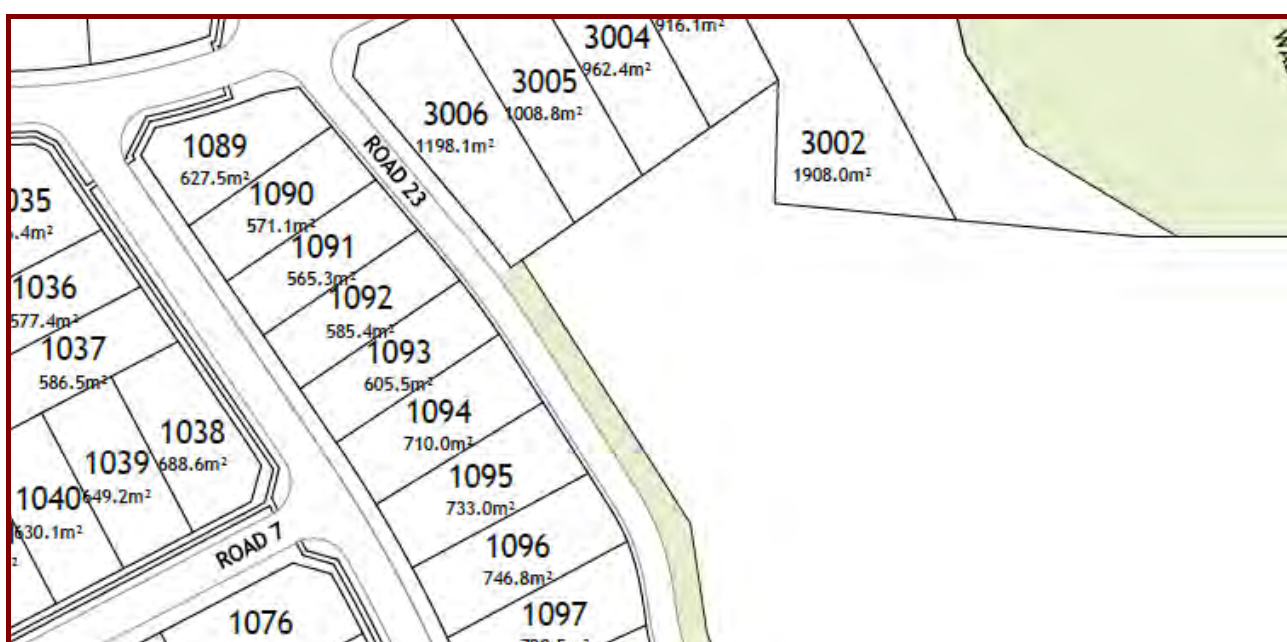


Image 2: The approved subdivision layout at the northern end of Road 23.

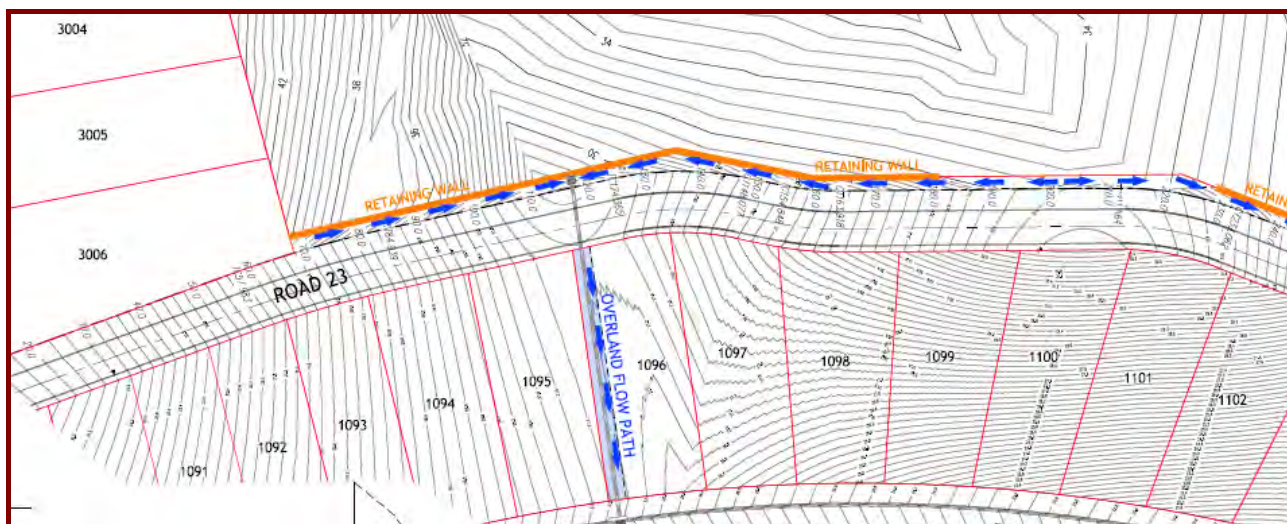


Image 3: The approved road 23 with design contours for the site and adjoining contours.

Due to the significant slopes along the road alignment and allowing for the road to be at maximum grade, to construct Road 23 as approved, either a retaining wall or fill is required on adjoining land. Plans showing the retaining wall and fill options are provided as **Attachment A**. Any such works incorporating a retaining wall create a trap low point for gully drainage, refer to **Image 3** above. A suitable retaining wall would have a height of 9 metres and would be extremely difficult to design and construct without footings on adjoining land.

In order to remove the trap low point, a solution would be fill within the adjoining lands. This option would be to fill some 4000 m² of adjoining conservation land. This option would still result in a retaining wall adjacent to Lot 3006.

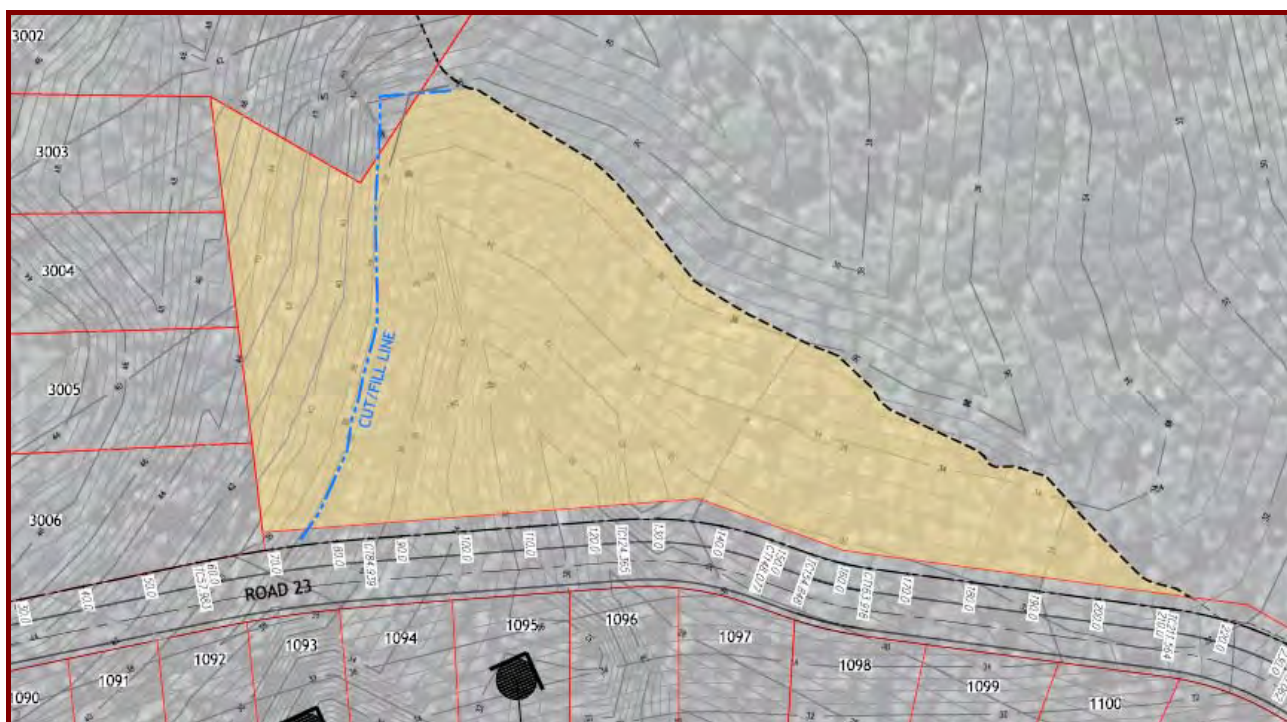


Image 4: Extent of fill on adjoining land required to build Road 23 as approved.

It is now proposed to reduce the length of the perimeter road by deleting all of Road 23 from Lot 1101 north. The need for fill and/or a retaining wall was created by the need to achieve design road grades. Without the need to achieve the grades the rear of the lots can remain mostly at existing ground level and drainage can be achieved without the creation of a trap low point. An overland flow path through Lot 1096 will convey this water into the street drainage system. A design that avoids the problems caused by the perimeter road across the gully is provided as **Attachment B** and includes a comparison with the approved layout.

The design change affects Stage 1. Stage 1 is reduced by one lot from 113 lots to 112 lots including 110 residential lots, 1 lot for the neighbourhood shops and one lot for a drainage basin. One lot has been removed due to the need to link Road 1 to the remaining length of perimeter Road 23. The lot that was approved as 1102 becomes part of Road 23. The lot boundaries are slightly shuffled to incorporate this change and the need for the additional flow path in Lot 1096. The changes in lot boundaries can be seen on the plans at **Attachment B**. Lots 1089 to 1101 are extended by the width of the previous road along their rear boundary. The additions to the lots will not be built on as these areas are required for bushfire asset protection zones for lots 1092 to 1101. This will result in less impact to conservation lands adjoining these lots.

Road 23 is currently approved as a two way, 10.5 metre and variable road reserve with a 6 metre wide pavement. To accommodate road design standards it is proposed that Road 23 be slightly redesigned. There were issues in achieving the required road grades and parking, south west of proposed Lot 1102. In this location retaining walls located on the boundary will be required, but can be minimised if the road width is reduced to 4 metres. At 4 metres width the road is only suitable for one way traffic, proposed to run from south west to north east. This will maintain access as required by the IHAP principles. The 10.5 metre width road reserve will be maintained.

The relevant IHAP considerations, “*to be implemented having regard to a classic McHarg analysis*” are:

Provide for public access Moonee Beach including the provision of discreet public parking areas in locations adjacent to walking paths which provide access to the beach;

Provide a public road adjacent to the beachfront reserve and other interfaces between the development and the surrounding bush land; and

In regard to the IHAP considerations, public access to Moonee Beach has been fully retained and a section of perimeter road has been deleted on the basis of McHarg analysis principles i.e. a road in the proposed location is not justified due to topographic constraints.

The proposed changes have been incorporated into the proposed subdivision plan at **Attachment B**.

4.0 Proposed Layout Change for Electrical Kiosks

Preliminary electrical designs have now been completed and the location of electrical kiosks has been decided.

For security of tenure reasons, kiosks are located on private property and require ready access for maintenance.

The layout of Lots 1051 and 1069 have been modified by addition of a small rectangular area on the street frontage. These areas are to accommodate the kiosks in readily accessible locations.

The rectangular areas take up part of the road reserve and the subdivision green corridors. The road reserve/green corridor is 30 metres wide. The required kiosk easements area is 5.3 metres long by 3.3 metres wide. Given the width of the road reserve / green corridor and its lineal extent there is adequate remaining area for the streets landscaping and the road pavement.

No adverse effects are likely to arise from what are very minor layout changes to accommodate the kiosks.

The proposed changes have been incorporated into the proposed subdivision plan at **Attachment B**.

5.0 Proposed Changes to Lot Layout

A reallocation of lots between stages is proposed. The reallocation will provide for a more efficient subdivision pattern by reducing the area allocated to roads and provide more marketable lots. The changes can be seen on the plans at **Attachment B**.

The removal of perimeter road in Stage 1 will result in loss of 1 lot.

Reconfiguration of stage 2 lots will create an additional 13 lots.

Reconfiguration of Stage 5 will consolidate 14 lots into 3 and remove approximately 150 metres of road and a turning head for a net loss of 11 lots.

Stages 3, 4, 6 and 7 will not be affected. The overall result is 1 additional lot which is within the scope of the approval. The current approval is for "Subdivision of land to create up to 550 residential lots, 1 retail lot, 9 reserves, bulk earthworks infrastructure and including two heritage lots".

The distribution lots as currently approved is as shown in **Table 1**.

Table 1 Lot distribution as currently approved

Stage	Residential Lots	Heritage Lots	Reserves	Retail
1	111		1	1
2	136		1	
3	40		1	
4	84			
5	51		1	
6	79		4	
7	46			
Heritage Precinct		2	2	
Totals	547	2	9	1

Following the proposed reallocation of lots the distribution will be as shown in **Table 2** below.

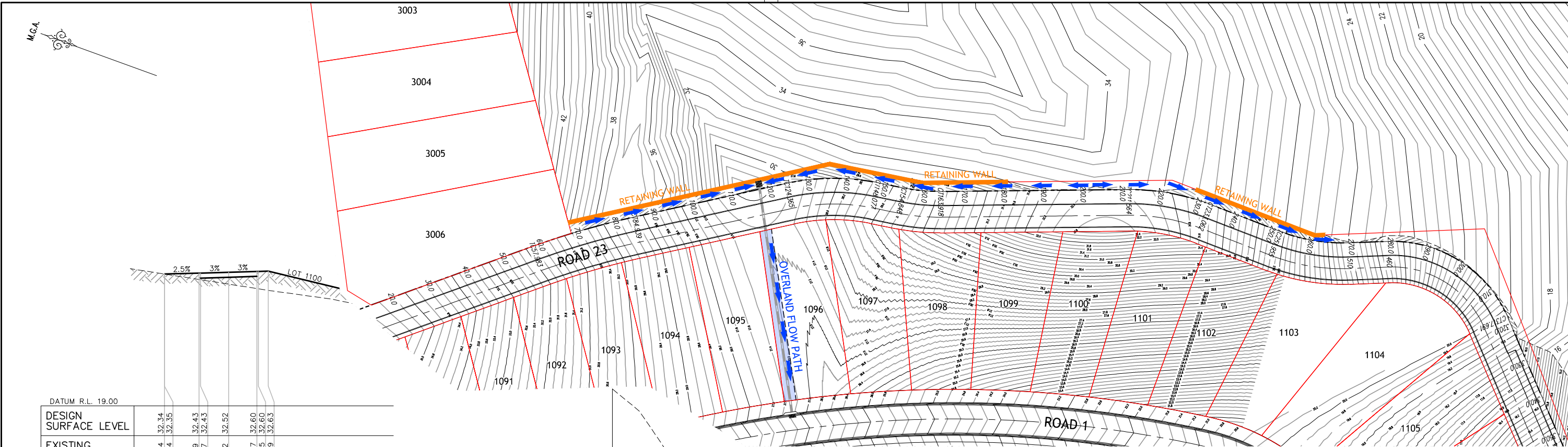
Table 2 Lot Distribution as Proposed

Stage	Residential Lots	Heritage Lots	Reserves	Retail
1	110		1	1
2	149		1	
3	40		1	
4	84			
5	40		1	
6	79		4	
7	46			
Heritage Precinct		2	2	
Totals	548	2	9	1

The lot totals comply with the requirements of the approval. There is capacity under the approval to add a further 2 residential lots. There are no significant impacts arising from the proposed reallocation of lots between stages.

Appendix A

OPTION PLANS FOR ROAD 23 GULLY CROSSING



PLAN — OPTION 2
SCALE 1:500



CRIB WALL

DATUM R.L. 19.00	
DESIGN SURFACE LEVEL	32.34 32.34 32.35 32.43 32.43 32.52 32.60 32.63
EXISTING SURFACE LEVEL	32.34 32.34 32.35 31.79 31.77 31.32 30.85 30.69
OFFSET	-6.51 -6.5 -3 -2.85 0 2.85 3 4

CH 210

DATUM R.L. 25.00	
DESIGN SURFACE LEVEL	28.81 27.80 27.89 27.89 27.97 28.06 28.08
EXISTING SURFACE LEVEL	28.81 28.60 28.42 28.40 28.18 28.55 28.70
OFFSET	-10.53 -6.5 -3 -2.85 0 2.85 4

CH 110

DATUM R.L. 24.00	
DESIGN SURFACE LEVEL	30.75 30.75 30.84 30.93 30.93 30.95
EXISTING SURFACE LEVEL	39.23 38.68 38.65 38.20 37.87 37.86 37.69
OFFSET	-6.5 -3 -2.85 0 2.85 3 4

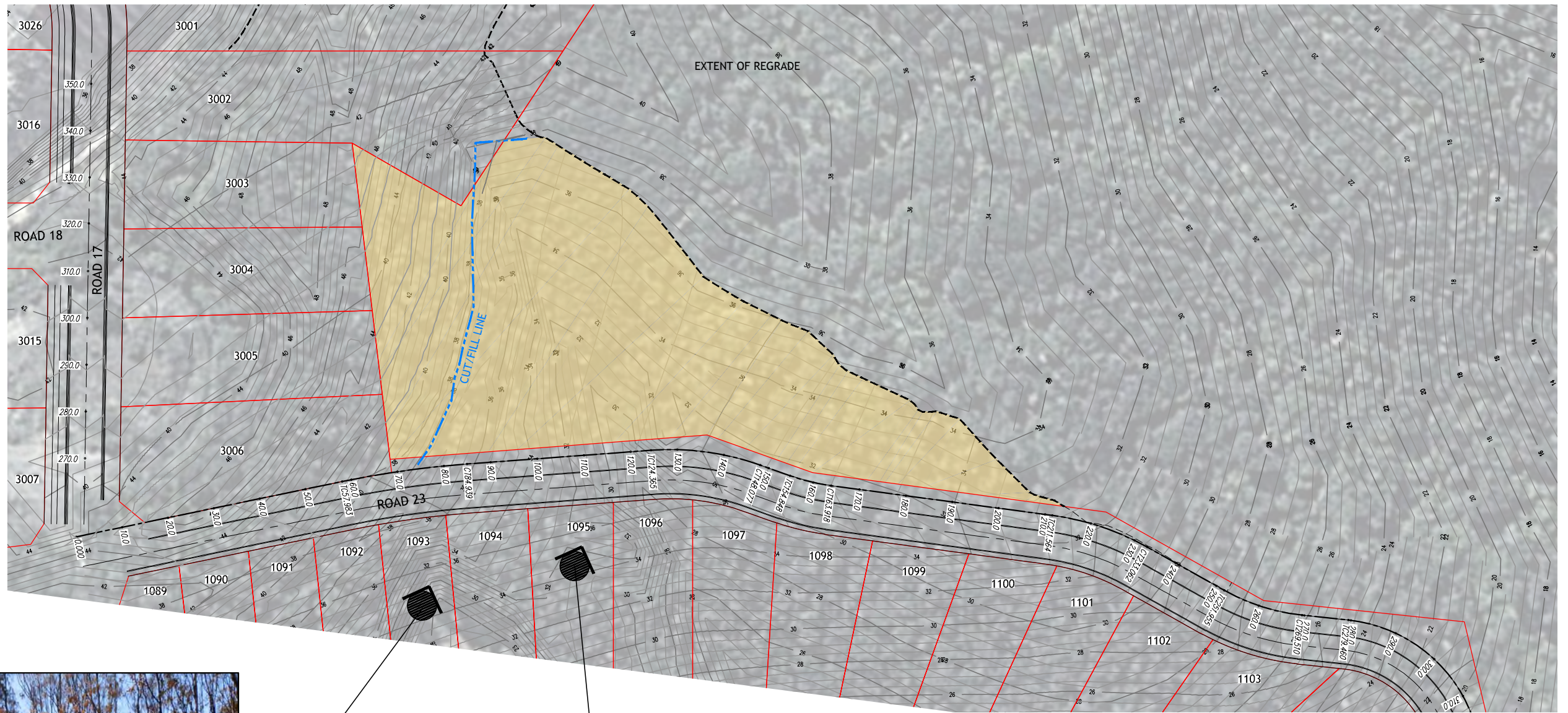
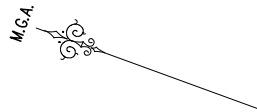
CH 70

DATUM 18.0	
WALL HEIGHT	-9.325 -7.226 -7.287 -6.979 -3.226 -0.910 -1.403 -1.703 -0.594 -0.081 -0.040 0.429 0.473 0.229 0.211 0.354 0.560 0.663 0.521 0.446 -0.066 -0.086 -0.470 -0.689 -0.885
VERGE LEVEL	32.057 30.647 29.713 28.237 27.985 27.818 28.003 28.732 30.142 30.786 30.863 31.929 32.634 32.608 32.963 31.851 31.142 30.363 28.659 28.183 25.912 25.836 24.356 23.489 21.142
EXISTING LEVEL	41.382 37.873 37.000 36.216 31.211 28.729 29.406 30.435 30.736 30.903 30.903 31.500 32.161 32.379 32.353 31.498 30.582 29.700 28.138 27.737 25.979 25.922 24.826 24.178 22.027
CHAINAGE	60.000 75.000 84.939 90.000 105.000 120.000 124.365 135.000 148.077 150.000 154.848 163.918 180.000 195.000 210.000 211.864 225.000 233.062 240.000 251.955 255.000 269.510 270.000 279.460 285.000 300.000

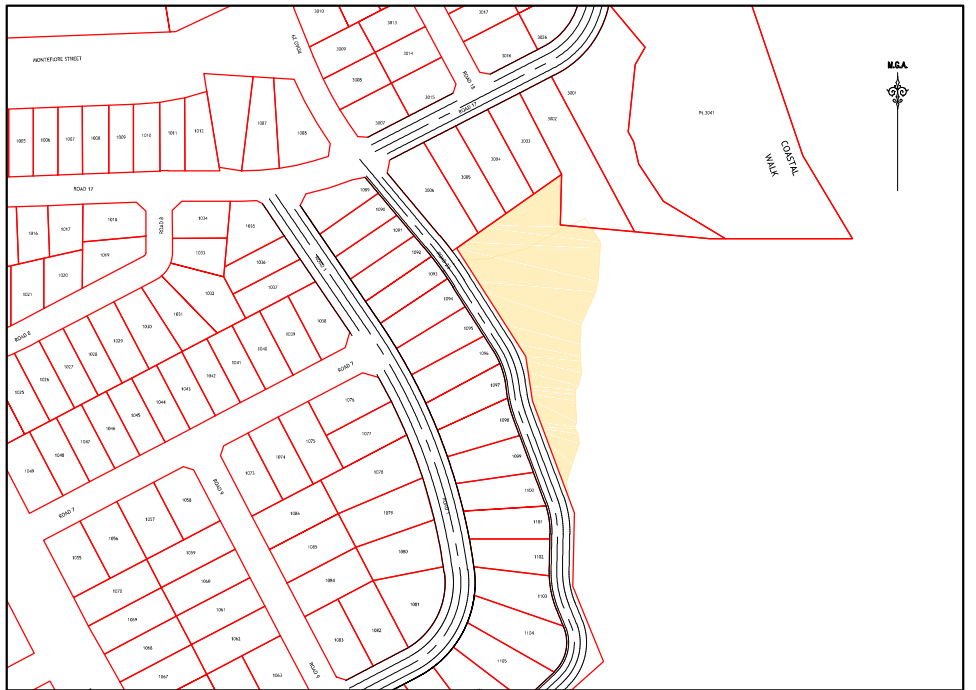
OPTION2
HORIZONTAL SCALE: 500
VERTICAL SCALE: 100

REV. A	DATE 14.11.13	AMENDMENT ISSUED FOR INFORMATION	DESIGN JY	DRAWN JD	CHECKED IB	APPROVED	SCALES 0 10 20 A1 / A3 1:500 / 1:1000 0 5 10 A1 / A3 1:200 / 1:400 0 1 2 3 4 A1 / A3 1:100 / 1:200		Central Coast 2 Bounty Close, P.O. Box 3717, Tuggerah N.S.W. 2259 Phone: (02) 4305 4300 Fax: (02) 4305 4399 email: coast@adwjohnson.com.au www.adwjohnson.com.au ABN 62 129 445 398	CLIENT 	PROPERTY DESCRIPTION CATHERINE HILL BAY LAKE MACQUARIE	PROJECT CATHERINE HILL BAY PLAN TITLE ROAD 23 (PERIMETER ROAD) OPTION 2 OVERLAND FLOW OPTION	SURVEYED ADWJ	DATUM A.H.D.	PROJECT No. 11688(1) -	DISCIPLINE SK	NUMBER 006	REV. A
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DESIGN FILE S:\
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PLAN — OPTION 1
SCALE 1:500



LOCALITY PLAN

REV.	DATE	AMENDMENT	DESIGN	DRAWN	CHECKED	APPROVED	SCALES					CLIENT	PROPERTY DESCRIPTION		PROJECT		
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														PLAN TITLE		ROAD 23 (PERIMETER ROAD) OPTION 1 - FILL OPTION PLAN AND LONG SECTION	
DESIGN FILE S:\								ALL DIMENSIONS ARE IN METRES. DO NOT SCALE				SURVEYED		DATUM	PROJECT No.	DISCIPLINE	NUMBER
Plotted By: jeremyd Plot Date: 21/11/13 - 09:20 Cad File: S:\211688\211688(1)\Drawings\OTHER\SKETCHES\11688(1)-SK-012_A.dwg												ADWJ		A.H.D.	11688(1) -	SK	012
																	REV. A

Appendix B

PROPOSED SUBDIVISION PLANS AND LAYOUT CHANGE COMPARISON PLANS

Version: AI (14/01/2014)
Client: Coastal Hamlets Pty. Ltd.
Survey:
AutoCAD: 11688-SUB-002-AI-C3D

Our Reference: 11688



LOT COUNT				
STAGE	RESIDENTIAL	RESERVE	RETAIL	TOTAL
1	110	1	1	112
2	149	1	-	150
3	40	1	-	41
4	84	-	-	84
5	40	1	-	41
6	79	4	-	83
7	46	-	-	46
HERITAGE	2	2	-	4
TOTAL	550			

<u>CAR SPACES</u>	
ROADS 20, 22 & 23	- 81
ROAD 1	- 11
TOTAL	- 92

AI	06/01/2014	Stage 2 lot layout amended
AH	13/12/2013	Consolidate Stage 5 lots
AG	09/12/2013	Lot numbers amended
AF	04/12/2013	Stage 1 lot layout amended
Ver.	Date	Comment

NOTES

1. PROPOSED EASEMENTS FOR SERVICES & DRAINAGE NOT SHOWN & WILL BE SUBJECT TO FINAL DESIGN.
2. BOUNDARIES HAVE BEEN DETERMINED BY PLAN DIMENSIONS ONLY, AND HAVE NOT BEEN SURVEYED. ALL BEARINGS, DIMENSIONS, AREAS AND EASEMENTS ARE SUBJECT TO FINAL SURVEY



Version: AI (14/01/2014)
Client: Coastal Hamlets Pty. Ltd.
Survey:
AutoCAD: 11688-SUB-002-AI-C3D
Our Reference: 11688

Version: AI (14/01/2014)
Client: Coastal Hamlets Pty. Ltd.
Survey:
AutoCAD: 11688-SUB-002-AI-C3D
Our Reference: 11688



Plan of: STAGING PLAN

Location: Catherine Hill Bay

Council: Lake Macquarie

Datum: MGA Zone 56
Origin: North Direction: Grid
Projection: MGA Zone 56
Contour Interval: N/A

Date: 17th July 2012
Sheet: 3 of 11
Plan By: KT
Project Manager: LB
Office: Coast
Plan Purpose: Subdivision

Version: AI (14/01/2014)
Client: Coastal Hamlets Pty. Ltd.
Survey: AutoCAD: 11688-SUB-002-AI-C3D
Our Reference: 11688

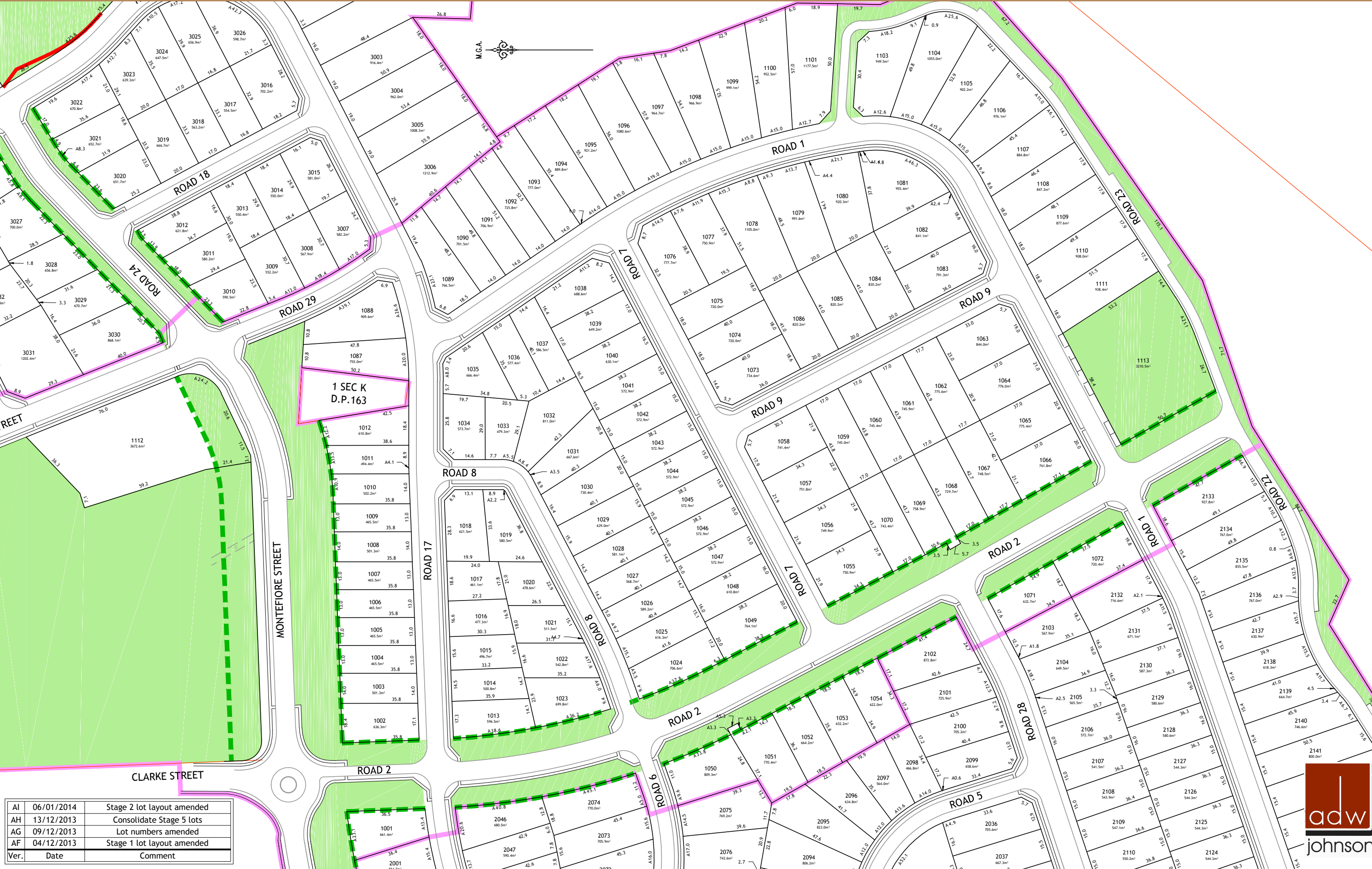


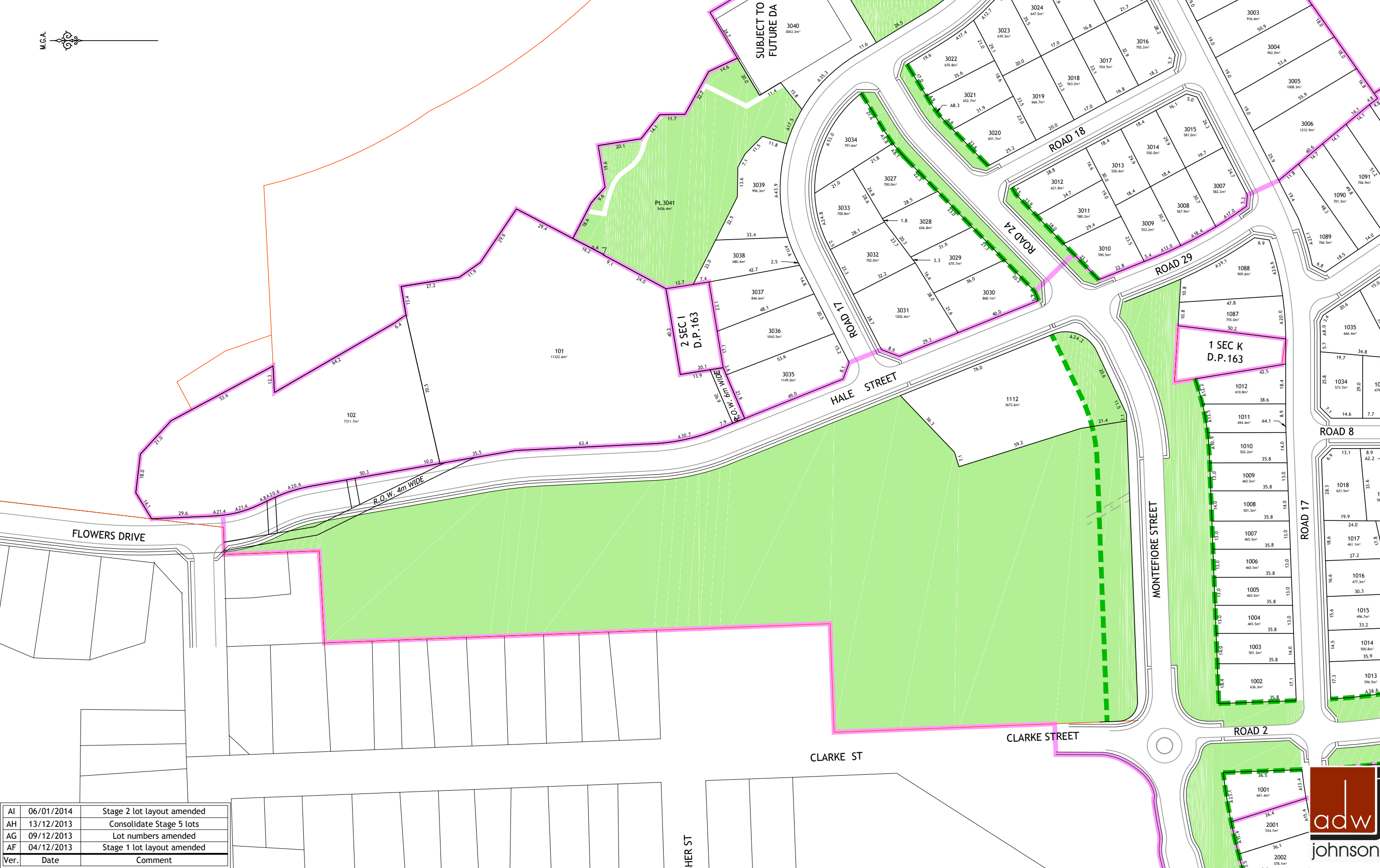
(F) EASEMENT TO DRAIN
WATER 3.05 WIDE (K382897)

- NOTES
- PROPOSED EASEMENTS FOR SERVICES & DRAINAGE NOT SHOWN & WILL BE SUBJECT TO FINAL DESIGN.

AI	06/01/2014	Stage 2 lot layout amended
AH	13/12/2013	Consolidate Stage 5 lots
AG	09/12/2013	Lot numbers amended
AF	04/12/2013	Stage 1 lot layout amended
Ver.	Date	Comment







AI	06/01/2014	Stage 2 lot layout amended
AH	13/12/2013	Consolidate Stage 5 lots
AG	09/12/2013	Lot numbers amended
AF	04/12/2013	Stage 1 lot layout amended
Ver.	Date	Comment

Plan of: STAGE 2

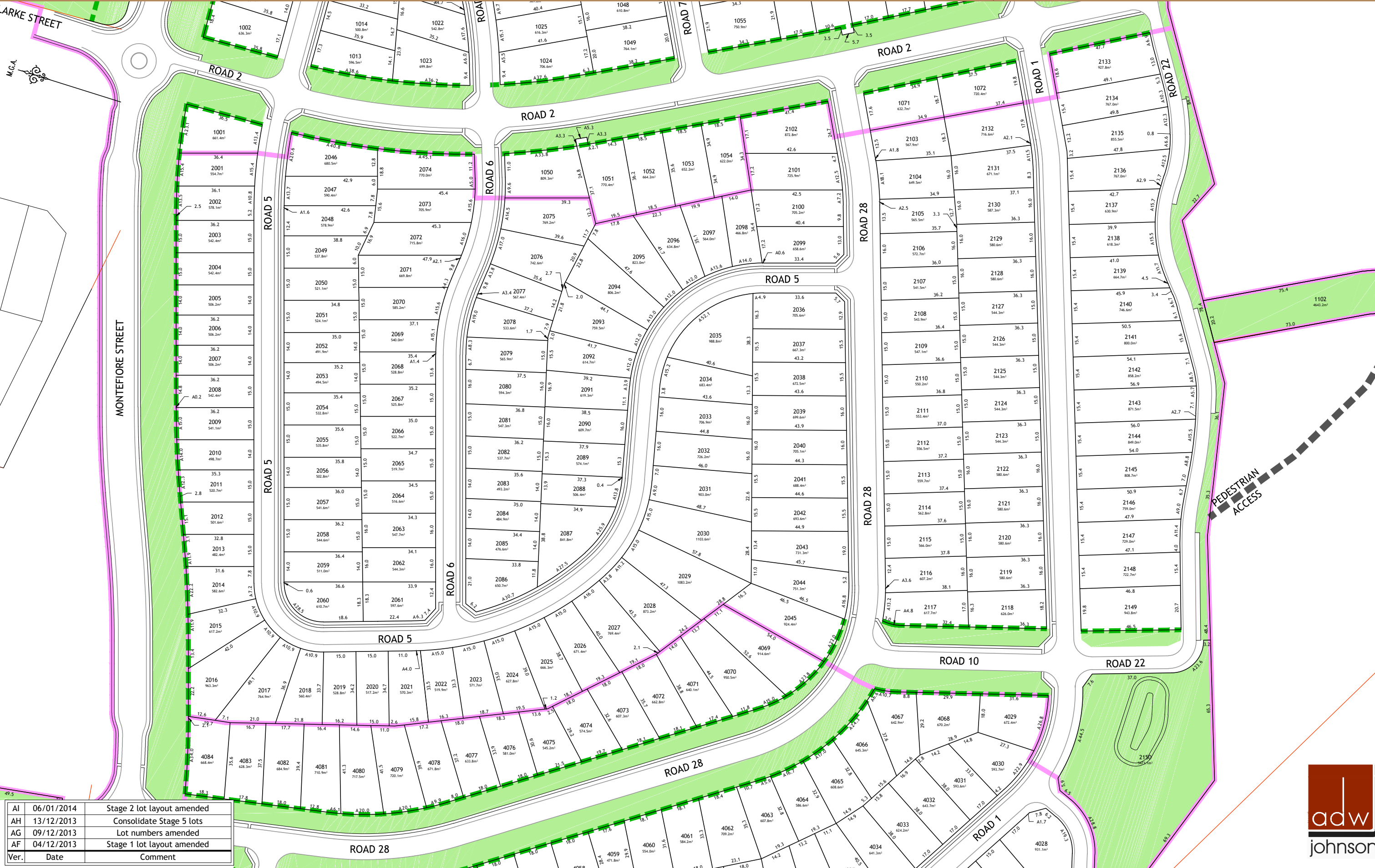
Location: Catherine Hill Bay

Council: Lake Macquarie

Datum: MGA Zone 56
Projection: North Direction: Grid
Contour Interval:

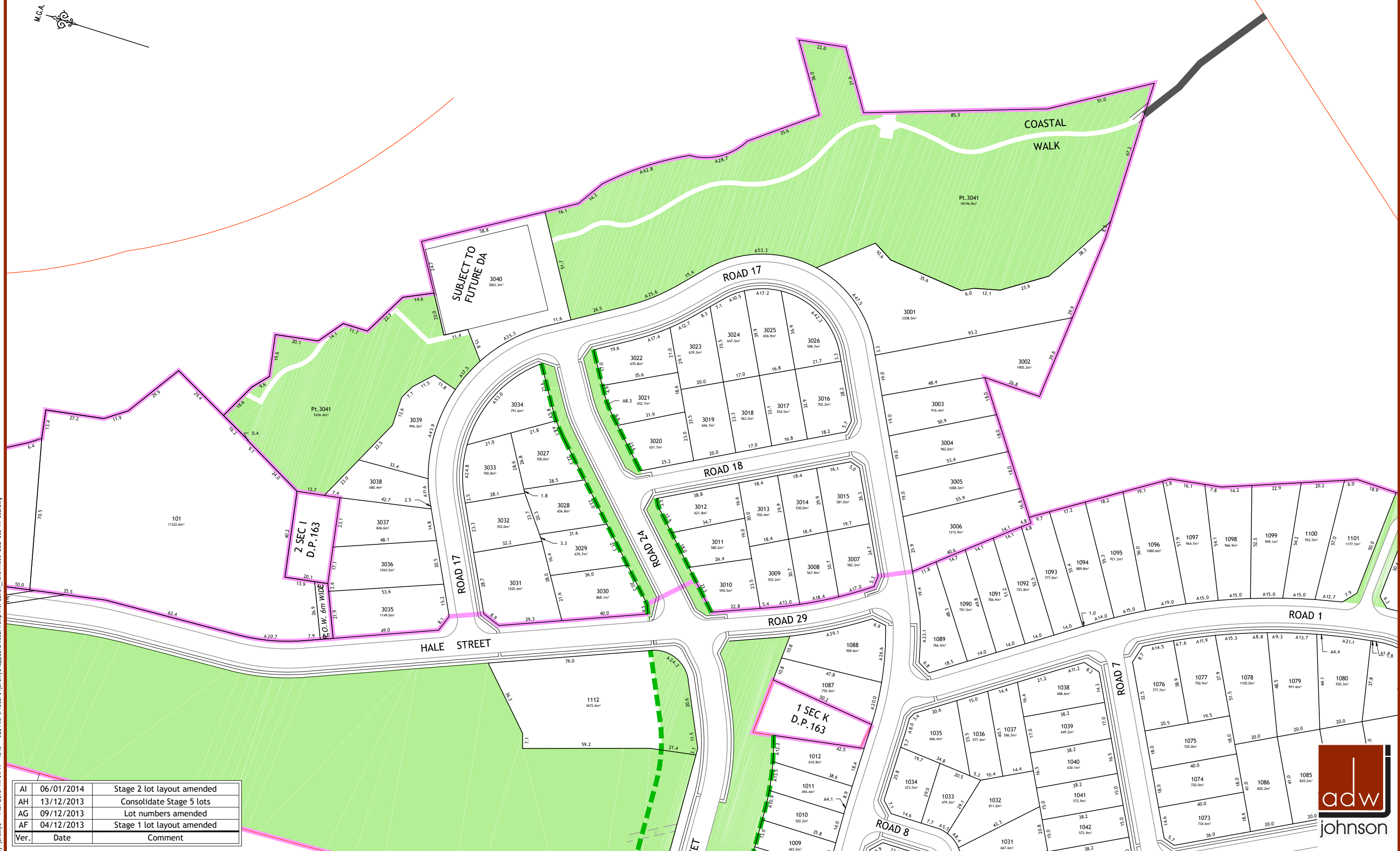
Date: 17th July 2012
Sheet: 6 of 11
Plan By: KT
Project Manager: LB
Office: Coast
Plan Purpose: Subdivision

Version: AI (14/01/2014)
Client: Coastal Hamlets Pty. Ltd.
Survey: AutoCAD: 11688-SUB-002-AI-C3D
Our Reference: 11688



AI	06/01/2014	Stage 2 lot layout amended
AH	13/12/2013	Consolidate Stage 5 lots
AG	09/12/2013	Lot numbers amended
AF	04/12/2013	Stage 1 lot layout amended
Ver.	Date	Comment





Plan of: STAGE 4

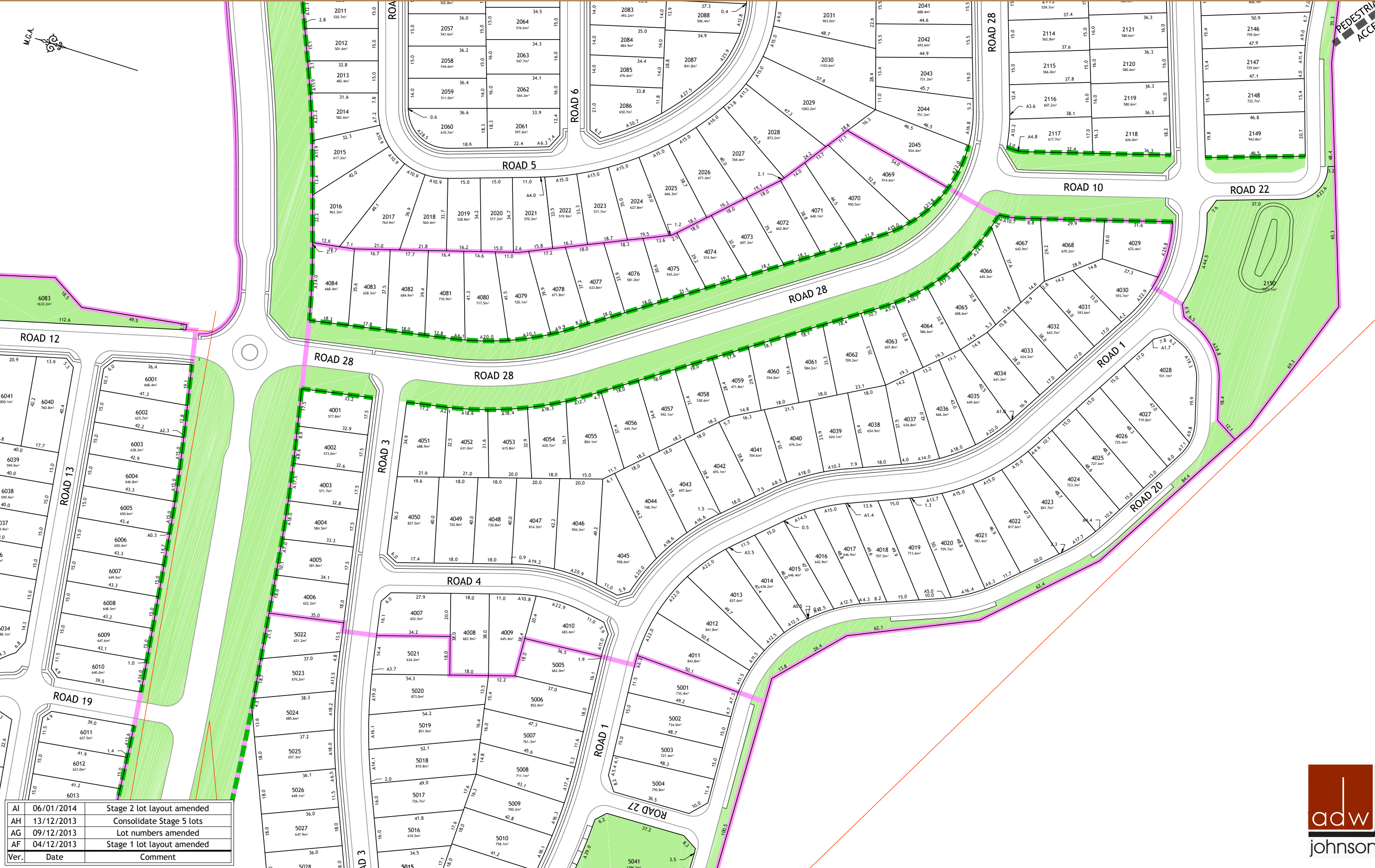
Location: Catherine Hill Bay

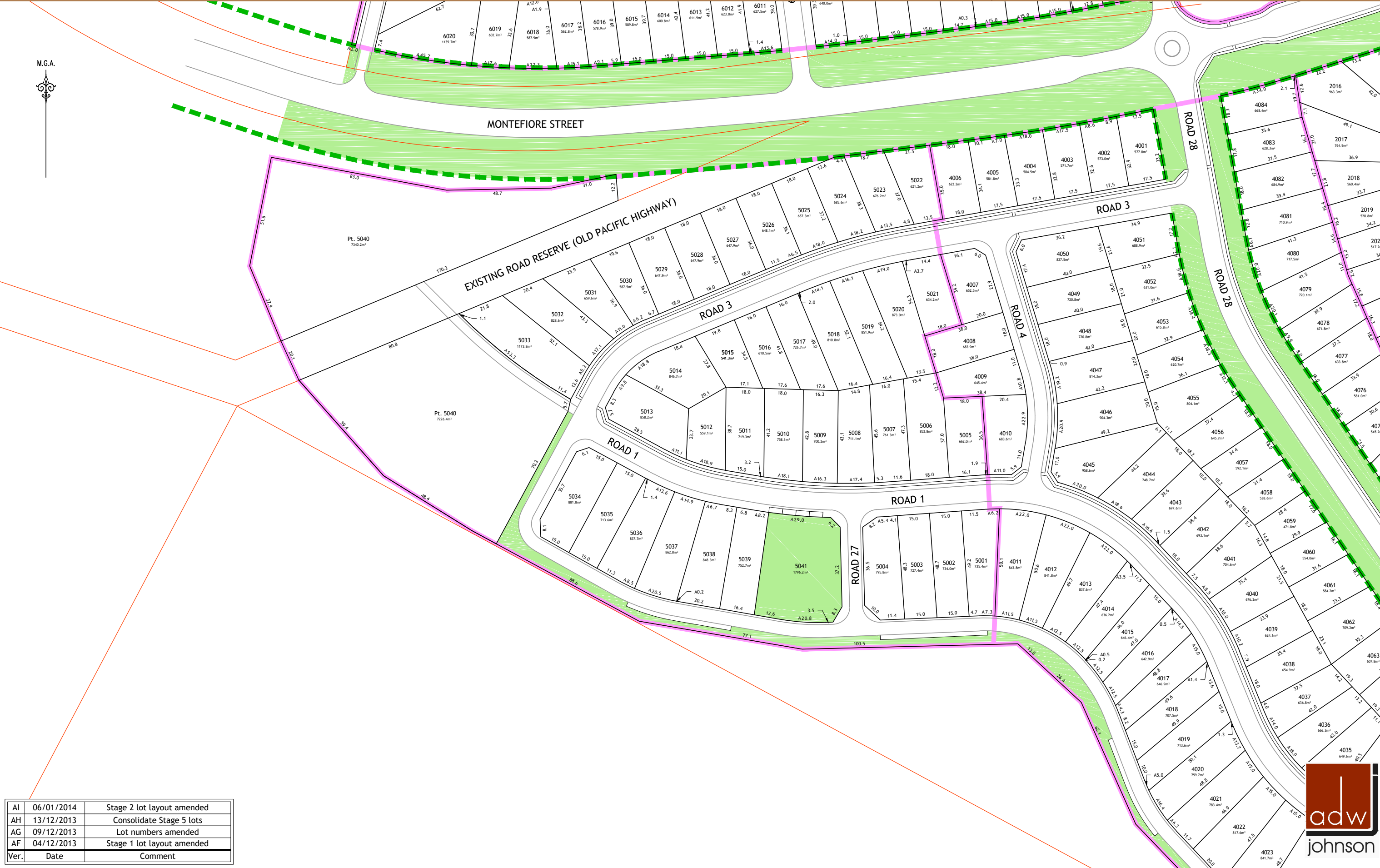
Council: Lake Macquarie

Datum: Origin:
Projection: MGA Zone 56
Origin:
North Direction: Grid
Contour Interval:

Date: 17th July 2012
Sheet: 8 of 11
Plan By: KT
Project Manager: LB
Office: Coast
Plan Purpose: Subdivision

Version: AI (14/01/2014)
Client: Coastal Hamlets Pty. Ltd.
Survey: AutoCAD: 11688-SUB-002-AI-C3D
Our Reference: 11688





AI	06/01/2014	Stage 2 lot layout amended
AH	13/12/2013	Consolidate Stage 5 lots
AG	09/12/2013	Lot numbers amended
AF	04/12/2013	Stage 1 lot layout amended
Ver.	Date	Comment



Plan of: STAGE 6

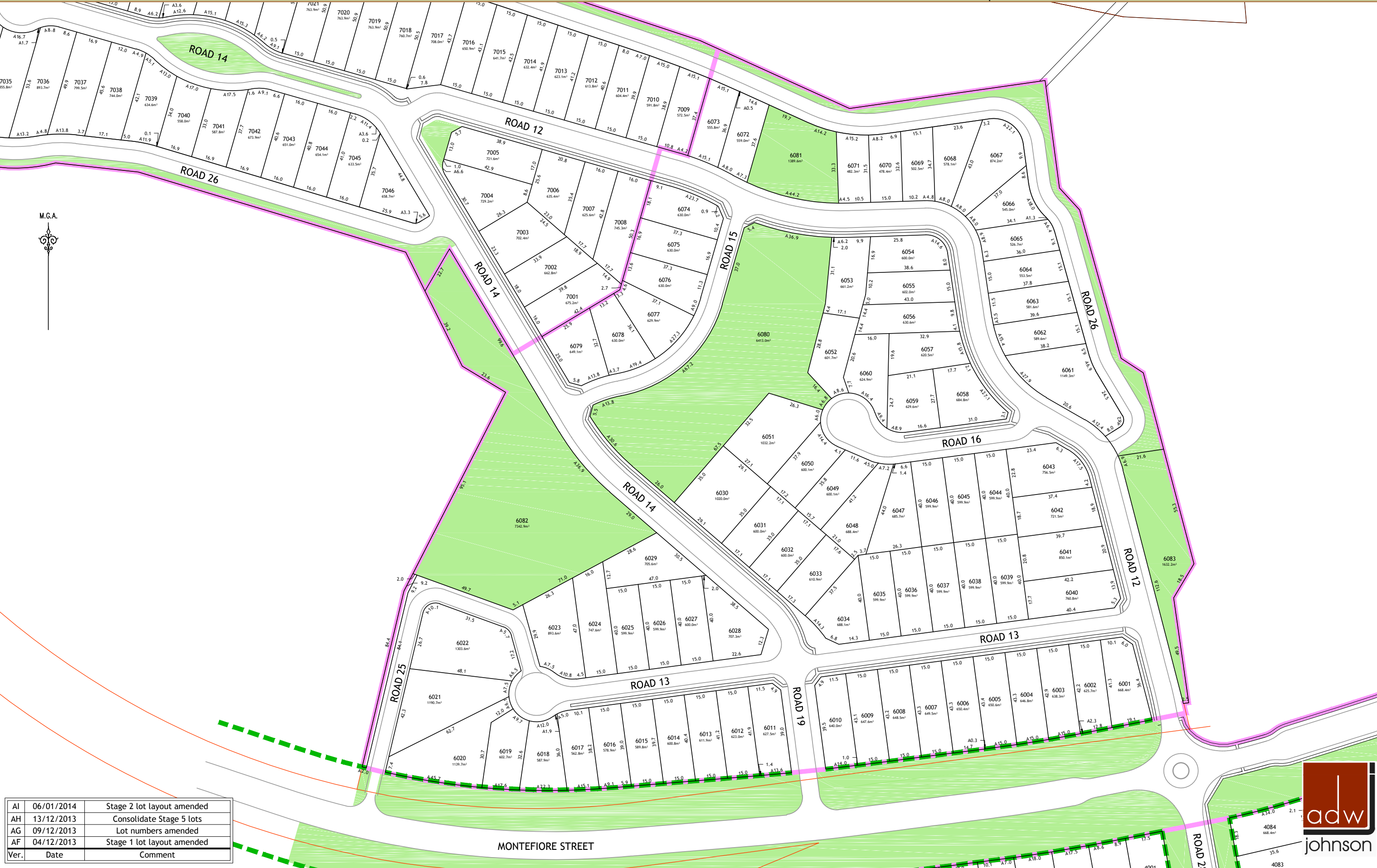
Location: Catherine Hill Bay

Council: Lake Macquarie

Datum: Origin:
Projection: MGA Zone 56
Origin:
North Direction: Grid
Contour Interval:

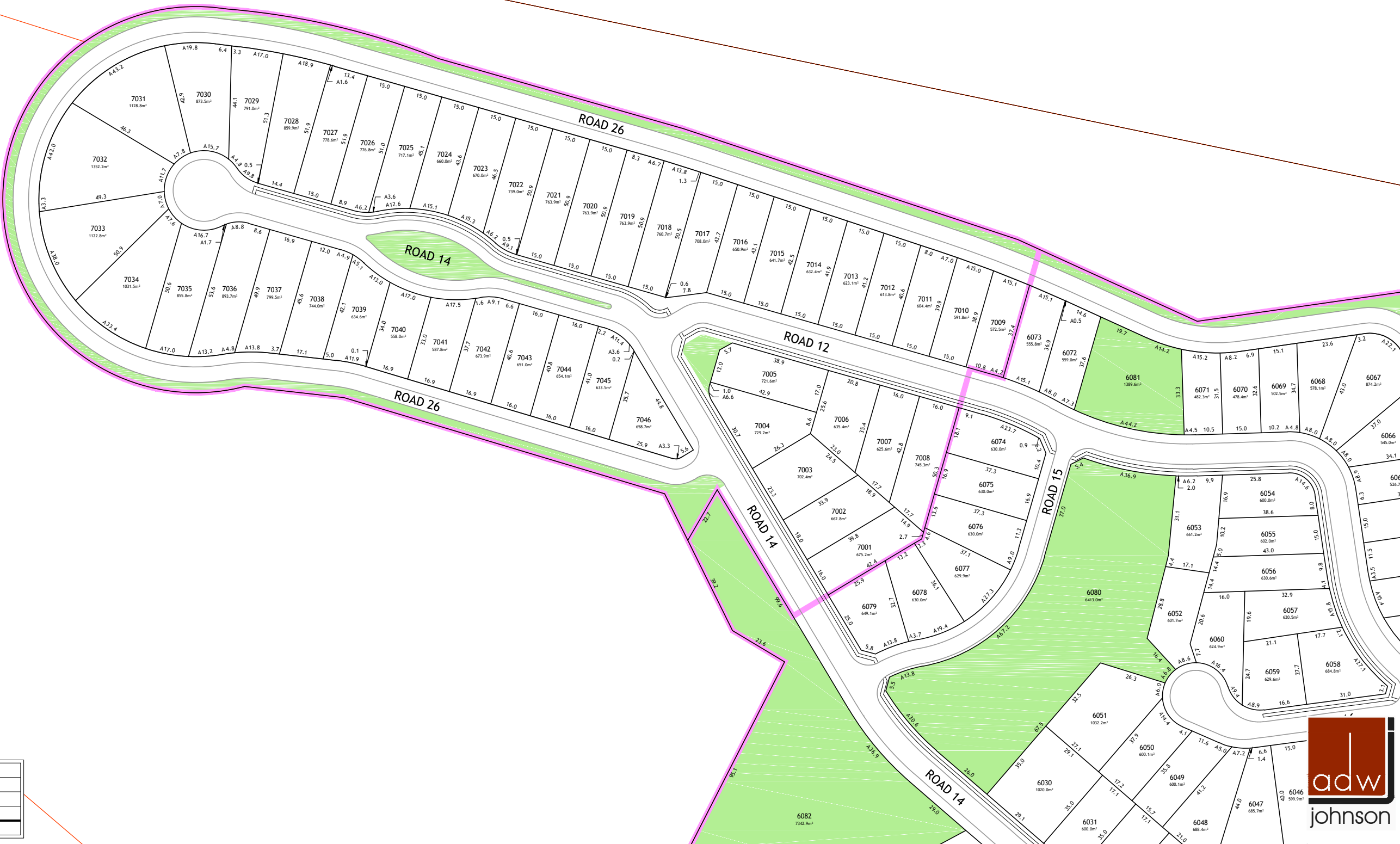
Date: 17th July 2012
Sheet: 10 of 11
Plan By: KT
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Office: Coast
Plan Purpose: Subdivision

Version: AI (14/01/2014)
Client: Coastal Hamlets Pty. Ltd.
Survey: AutoCAD: 11688-SUB-002-AI-C3D
Our Reference: 11688



AI	06/01/2014	Stage 2 lot layout amended
AH	13/12/2013	Consolidate Stage 5 lots
AG	09/12/2013	Lot numbers amended
AF	04/12/2013	Stage 1 lot layout amended
Ver.	Date	Comment





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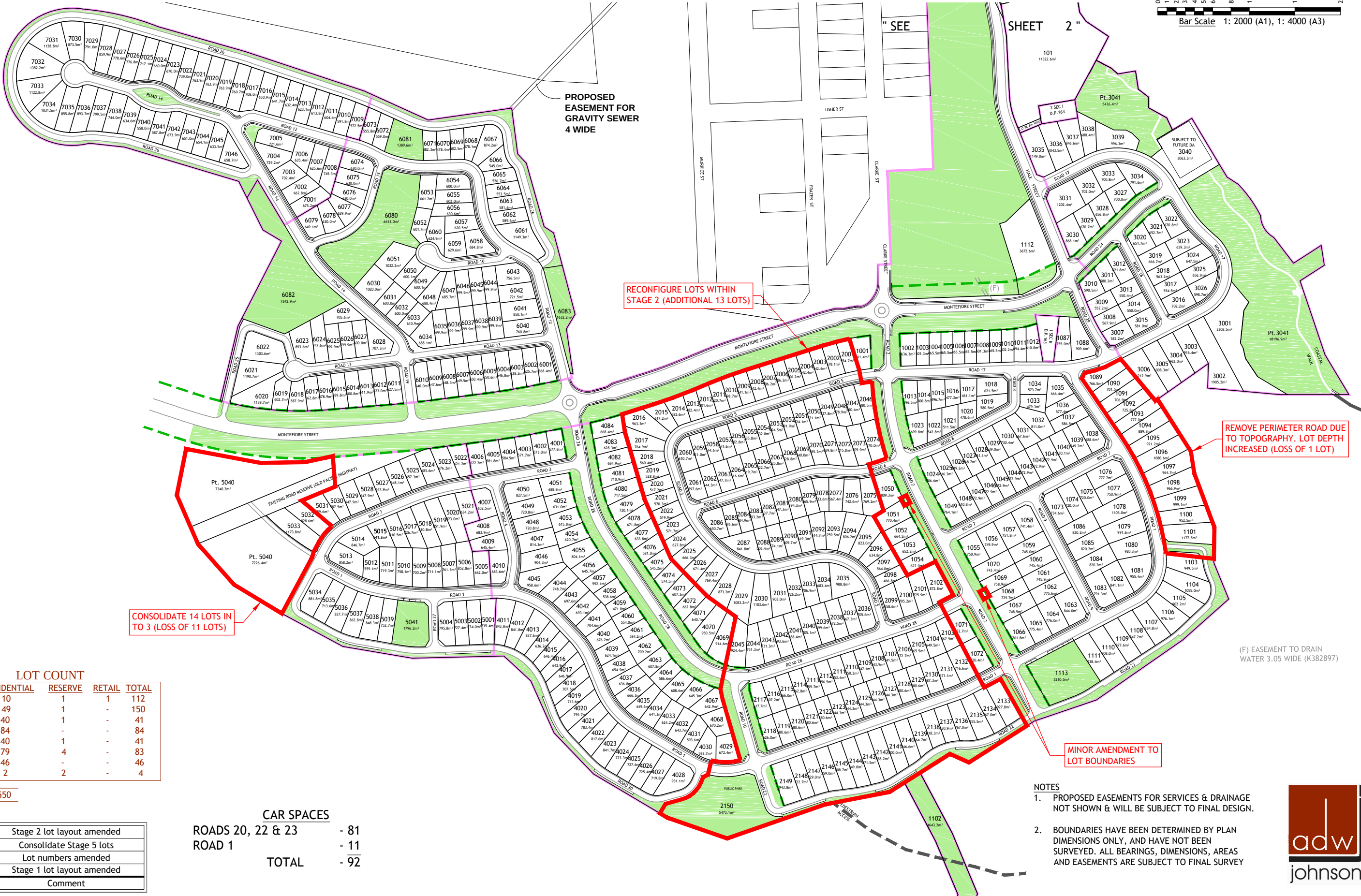
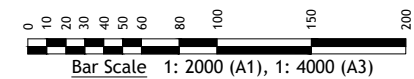
Plan of: SUMMARY OF PROPOSED CHANGES

Location: Catherine Hill Bay
Council: Lake Macquarie

Datum: MGA Zone 56
Projection: MGA Zone 56
Origin: Grid
Contour Interval:

Date: 17th July 2012
Sheet: KT
Plan By: LB
Project Manager: Coast
Office: Subdivision
Plan Purpose:

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LOT COUNT				
STAGE	RESIDENTIAL	RESERVE	RETAIL	TOTAL
1	110	1	1	112
2	149	1	-	150
3	40	1	-	41
4	84	-	-	84
5	40	1	-	41
6	79	4	-	83
7	46	-	-	46
HERITAGE	2	2	-	4
TOTAL 550				

CAR SPACES	
ROADS 20, 22 & 23	- 81
ROAD 1	- 11
TOTAL	- 92

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- NOTES
1. PROPOSED EASEMENTS FOR SERVICES & DRAINAGE NOT SHOWN & WILL BE SUBJECT TO FINAL DESIGN.
 2. BOUNDARIES HAVE BEEN DETERMINED BY PLAN DIMENSIONS ONLY, AND HAVE NOT BEEN SURVEYED. ALL BEARINGS, DIMENSIONS, AREAS AND EASEMENTS ARE SUBJECT TO FINAL SURVEY



Plan of: STAGE 1
COMPARISON OF REVISED ROAD AND LOT LAYOUT

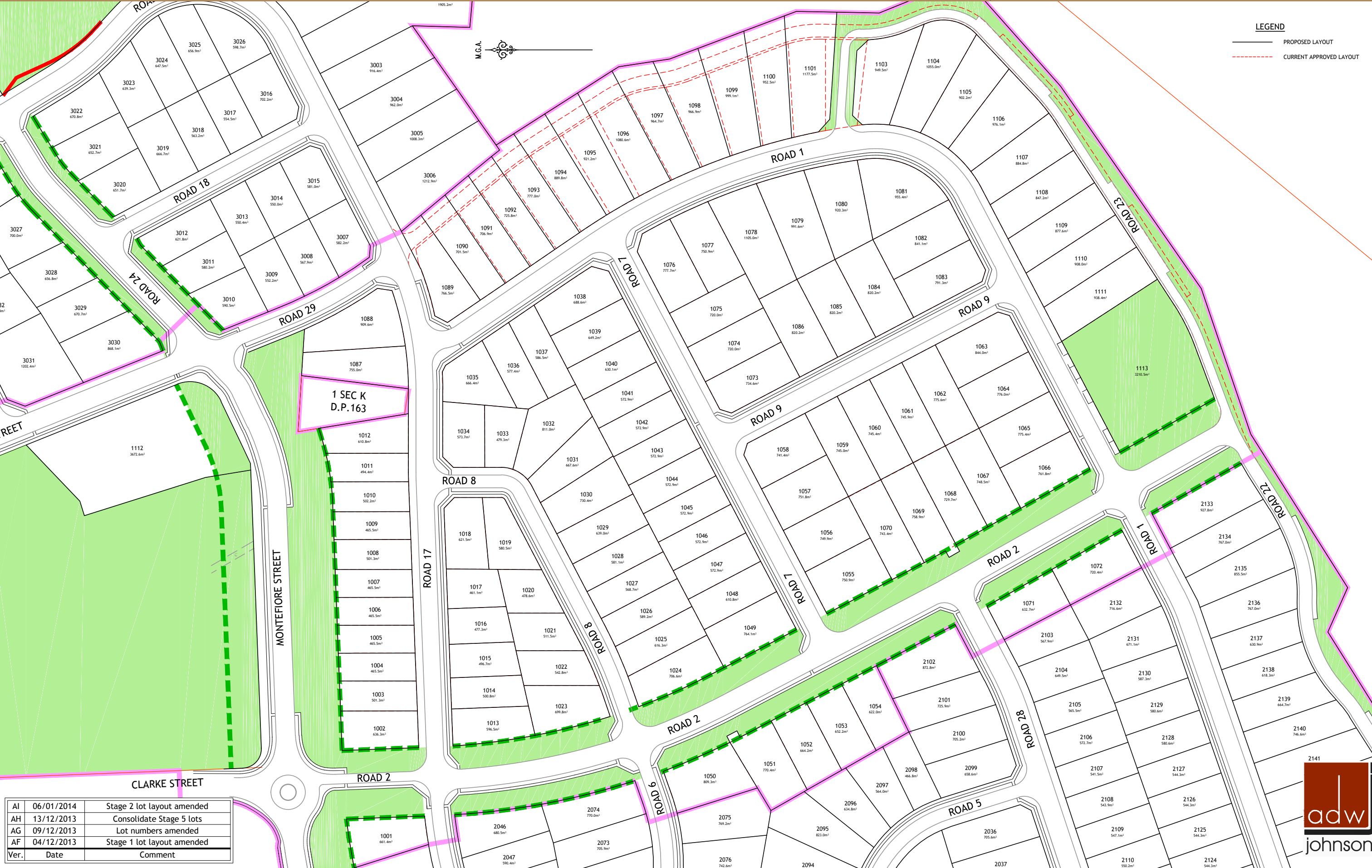
Location: Catherine Hill Bay

Council: Lake Macquarie

Datum: MGA Zone 56
Origin: North Direction: Grid
Projection: MGA Zone 56
Contour Interval:

Date: 17th July 2012
Sheet: KT
Plan By: LB
Project Manager: Coast
Office: Subdivision
Plan Purpose:

Version: AI (14/01/2014)
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Plan of: STAGE 2
COMPARISON OF REVISED LOT LAYOUT

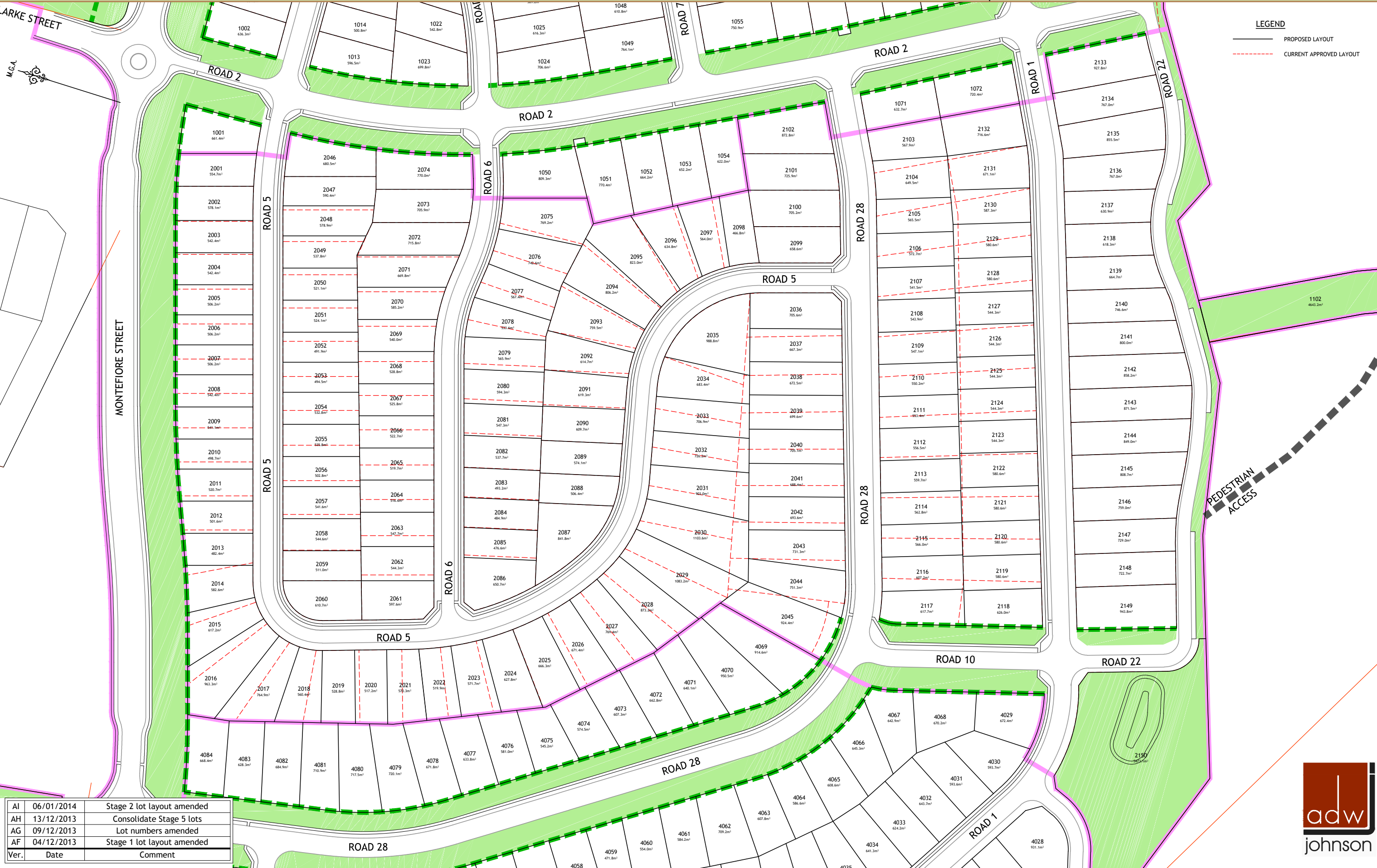
Location: Catherine Hill Bay

Council: Lake Macquarie

Datum: Origin:
Projection: MGA Zone 56
Origin: North Direction: Grid
Contour Interval:

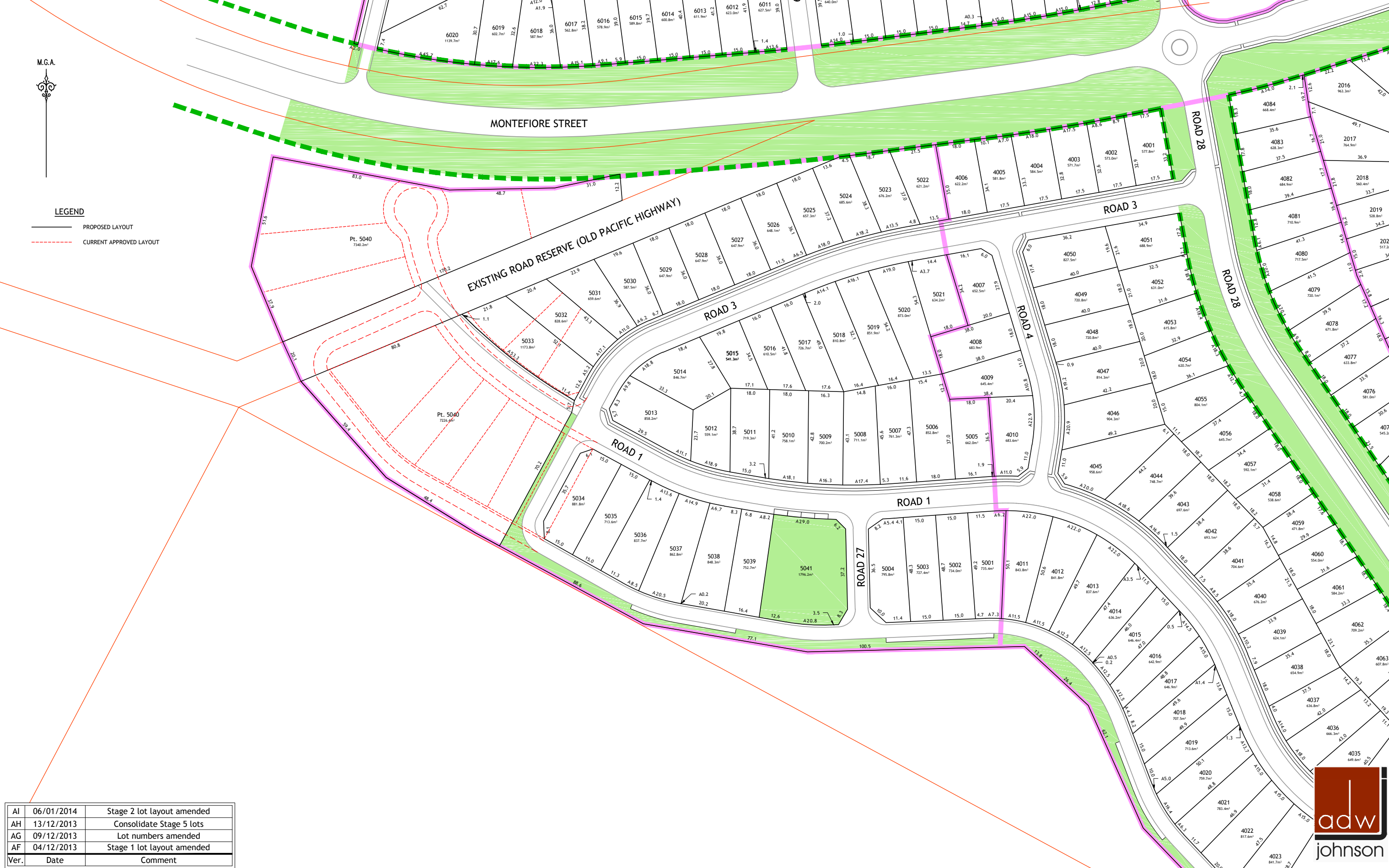
Date: 17th July 2012
Sheet: KT
Plan By: LB
Project Manager: Coast
Office: Subdivision
Plan Purpose:

Version: AI (14/01/2014)
Client: Coastal Hamlets Pty. Ltd.
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