



MERITON

Making Luxury Apartments Affordable

26 June 2009

Mr Andrew Smith
Department of Planning
23-33 Bridge Street
Sydney NSW 2000

Dear Mr Smith,

MP 09_0033: 3-5 PYMBLE AVENUE, PYMBLE

Further to our conversation on the 23 June 2009 relating to the above matter, I provide the following in response to each of the outstanding issues raised.

Plans showing 1A Pymble Avenue

Please find enclosed 3 copies of the Block E floorplans showing the location of 1A Pymble Avenue.

Access into future Block F

Meriton has investigated alternative options of providing internal vehicular access into the Block F envelope. Listed below are three potential options to providing vehicular access into the Block F envelope:

1. The basement of Block A and the basement of the Block F envelope are connected with an underground ramp/aisle;
2. The basement of Block E and the basement of the Block F envelope are connected with an underground ramp/aisle; or
3. A vehicular accessway through the basement of Block D is provided into the common open space area identified for the Block F envelope.

Each of these access options is shown illustrated in **Figure 1**. The merits of each of these options are discussed in the paragraphs that follow.

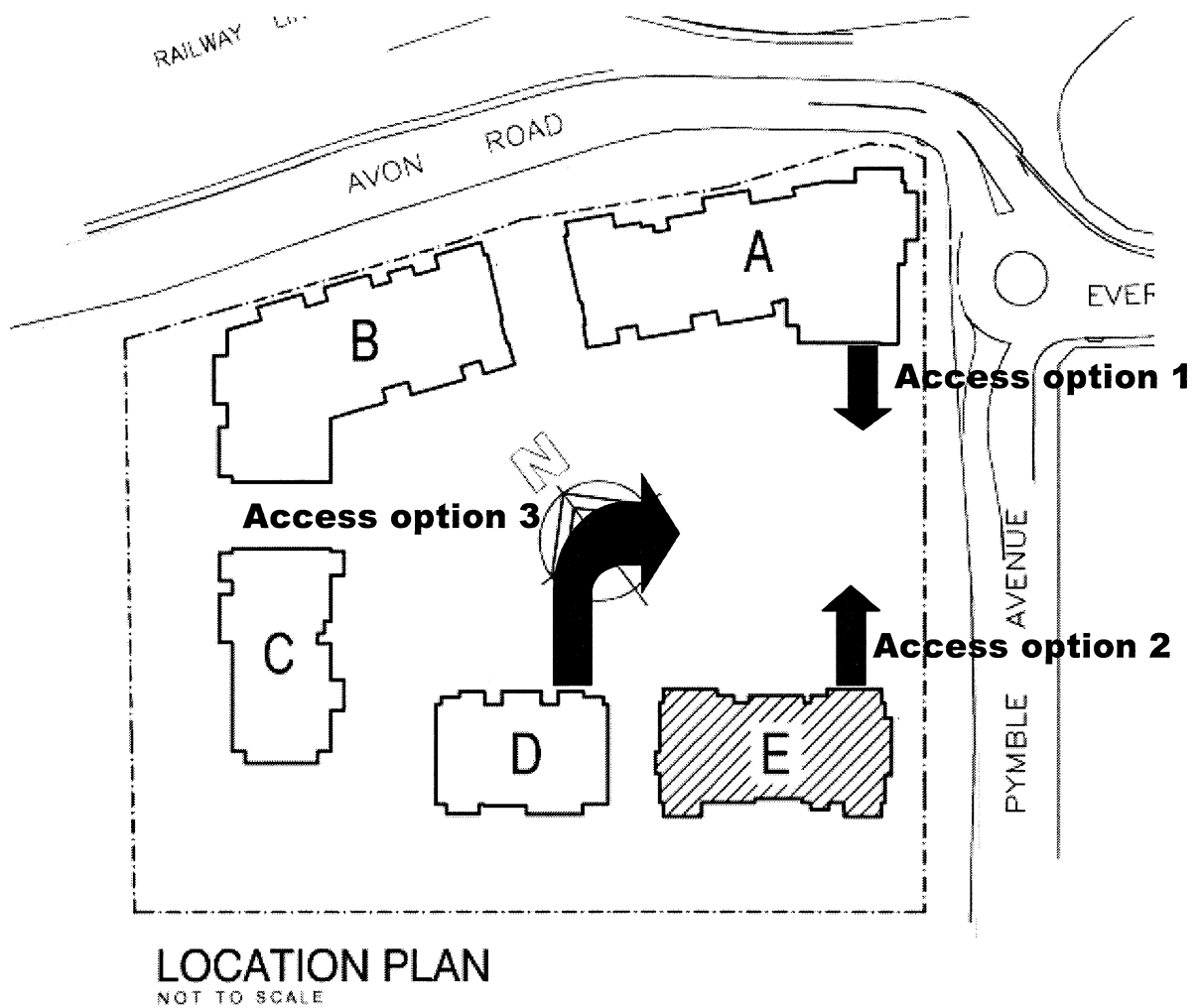


Figure 1: Access options

Access Option 1

This option would involve constructing an underground aisle structure from the southern wall of Block A at parking Level 5 and closed at the boundary of 1A Pymble Avenue. Should the adjoining site be redeveloped with the Block F envelope, a future developer will be required to construct a connecting underground aisle to the boundary.

Access to Block F would be via a travel path through the basement areas of Blocks A and B, which is cumbersome. Complicated easements, right-of-ways, etc, would give rise to even more complicated insurance and maintenance issues for each future property owner and body corporate.

Figure 2 shows the likely access aisle between block A and the Block F envelope.



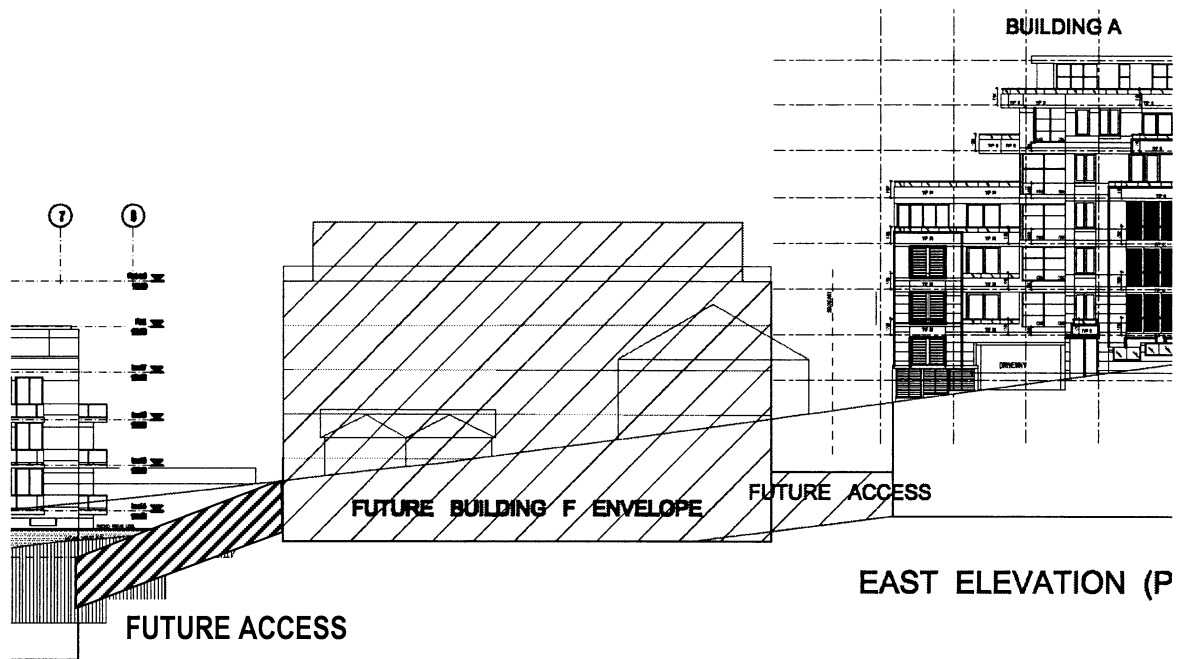


Figure 2: East elevation/section diagram showing potential access from Meriton's Block A or Block E to future Block F

Access Option 2

This option would involve constructing an underground aisle structure from the northern wall of the Block E basement parking area and closed at the boundary of 1A Pymble Avenue. As with Access Option 1, should the adjoining site be redeveloped with the Block F envelope, a future developer will be required to construct a connecting underground aisle to the boundary.

Access to Block F would be via a travel path through the basement areas of Blocks D and E. As with Access Option 1, complicated easements, right-of-ways, etc, would give rise to even more complicated insurance and maintenance issues for each future property owner and body corporate.

Furthermore, the difference in levels of the buildings and the topography of the site will not enable construction of a connecting accessway that complies with gradients in Australian Standard 2890. **Figure 2** shows these level differences.

Access Option 3

This option would involve creating a new opening at the northern wall of the parking basement of Block D and ramping up to Block F (see **Figure 3**). Due to the topography and levels of the approved building, a deep cutting would need to be made well within the dripline of the Sydney Blue Gum which is a critically endangered species. The works proposed in this option would cause significant harm to the tree or require its removal. The environmental impacts of this option are clearly unacceptable.



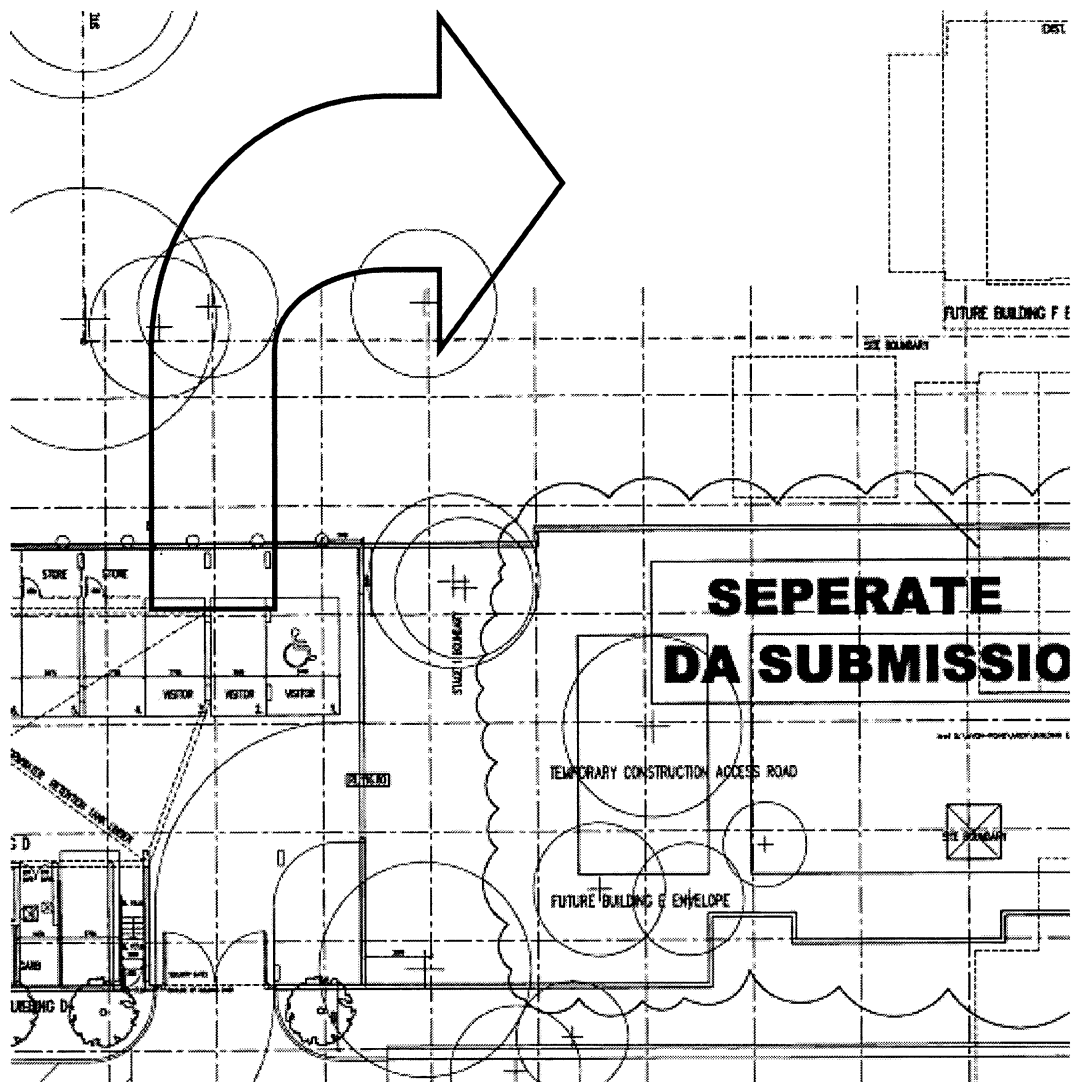
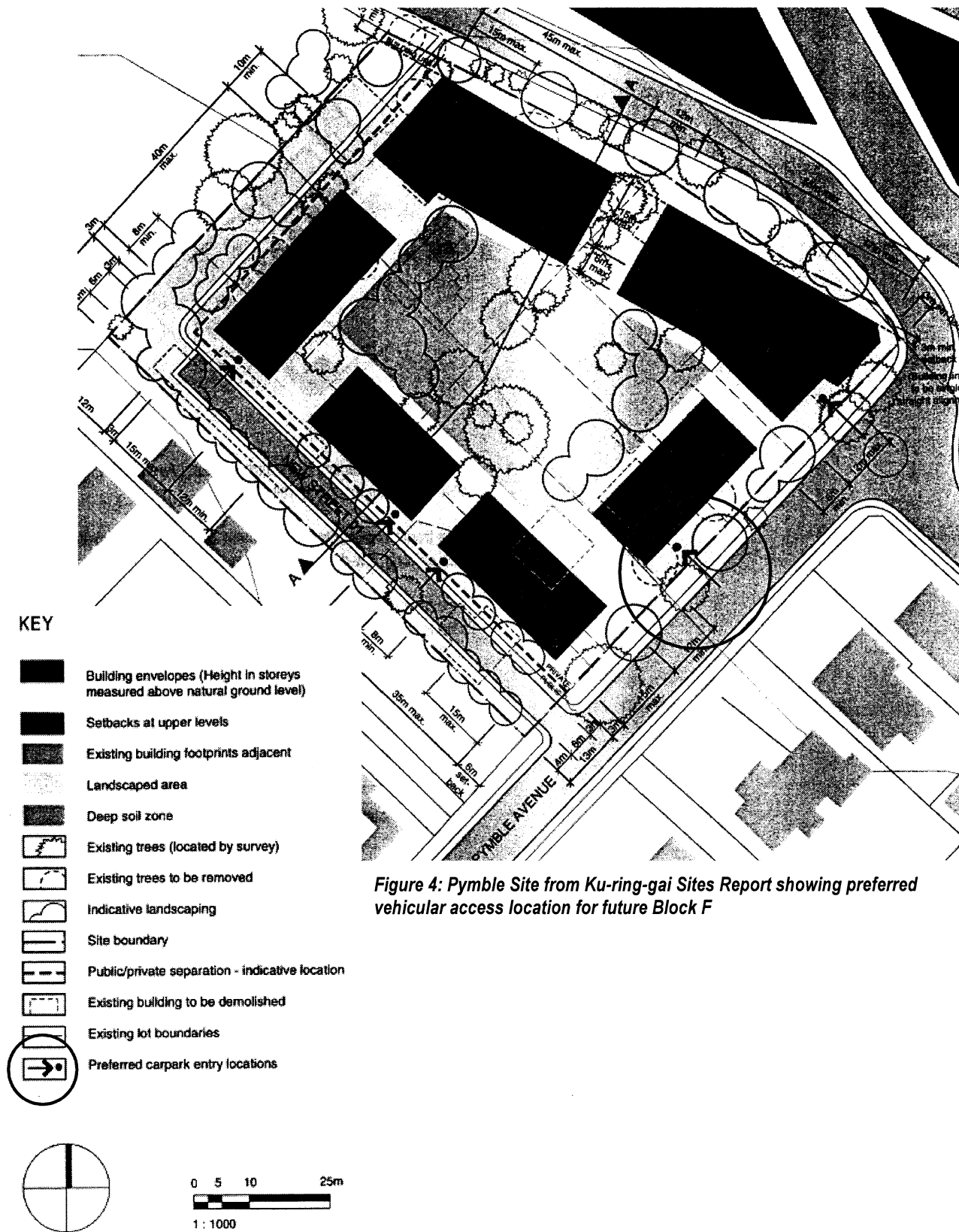


Figure 3: Potential access from Block D basement

Due to the slope of the land and future complicated rights of accessway, easements, insurance matters, maintenance sharing of basements, body corporate issues and damage to the Sydney Blue Gum, it is in our established experience that on this occasion vehicular access from our existing site cannot be suitably provided.

SEPP 53 Compliant Access

The submission prepared by Kerry Gordon Planning Services claims that future Block F will not be able to achieve suitable access arrangements for vehicles. The Building Envelope drawings contained in the Ku-ring-gai Sites Report clearly shows that the preferred carpark entry location is direct from Pymble Avenue into the southern end of Block F (see **Figure 4**).



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Vehicular access into the Block F envelope can be easily achieved by using the access point shown in SEPP 53. A driveway would be constructed with a suitable grade from Pymble Avenue into a carpark level. Vehicles would then ramp down to basement level below street level where units would otherwise be unsuitably located.

As stated in our letter dated 1 June 2009 and as submitted within our Environmental Assessment Report, it has been demonstrated that 1 and 1A Pymble Avenue can be developed for a residential flat building in accordance with SEPP 53 and Council's draft Town Centres LEP.

Meriton has decades of extensive experience in constructing buildings that are typical of the Block F envelope on 1 and 1A Pymble Avenue and the current development site encompassing Buildings A to E. In our opinion, the best solution for vehicular access into Block F is from Pymble Avenue. On this occasion, Meriton is not in a position to provide vehicular access from our property.

Yours sincerely

MERITON APARTMENTS PTY LIMITED



WALTER GORDON

Planning and Development Manager



Meriton Apartments Pty Limited
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