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Museum of Contemporary Art

Museum of Contemporary Art
Redevelopment Project
Preferred Project Report

April 2009



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- B Plans of the Preferred Project
- C Revised application for variation to the Sydney Cove Redevelopment Authority Scheme



1. Introduction

1.1 Overview

The Museum of Contemporary Art (MCA) proposes to redevelop and expand the museum, located in The Rocks.

The MCA redevelopment involves refurbishment to the existing building and an addition to the northern part of the building, and includes a new entrance, a National Centre for Creative Learning, gallery spaces and cultural uses associated with the museum such as lecture theatre, informal café and museum retail shop. The redevelopment would also address the non compliance of the current building in regard to access for all and public safety.

The key objectives for the redevelopment of the MCA are:

- » Equity of access;
- » Coherent circulation throughout the building;
- » Dedicated education spaces;
- » A variety of flexible gallery spaces;
- » Improved revenue optimisation; and
- » A contemporary signifier.

An Environmental Assessment for the redevelopment was prepared in December 2008, to support the MCA's application for approval of the under Part 3A of the *Environmental Planning and Assessment Act 1979* (EP&A Act).

The Environmental Assessment was placed on public exhibition between 10 December 2008 and 2 February 2009. Following exhibition, the Department of Planning provided the MCA with a copy of submissions received.

1.2 Purpose and structure of this report

This report provides a response to the issues raised in submissions and presents the 'preferred project' for which approval is sought. The report is structured into five sections as follows:

- » Introduction;
- » Summary of consultation activities;
- » Response to issues raised in submissions;
- » Description of the preferred project; and
- » Final Statement of Commitments.



2. Consultation activities

2.1 Consultation during the environmental assessment process

Consultation activities undertaken during the environmental assessment process were described in Chapter 5 of the Environmental Assessment.

2.2 Consultation during exhibition

The exhibition of the environmental assessment, including the receipt of submissions, was coordinated and managed by the Department of Planning.

The environmental assessment was exhibited from 10 December 2008 to 2 February 2009 at the following locations:

- » Department of Planning
Information Centre, 22-33 Bridge Street
Sydney
- » City of Sydney
Town Hall House, 456 Kent Street
Sydney
- » Department of Planning website – www.planning.nsw.gov.au

In addition, the MCA has held a number of meetings with government agencies and key stakeholders, including:

- » Department of Planning;
- » Sydney Harbour Foreshore Authority;
- » City of Sydney;
- » NSW Maritime;
- » NSW Police;
- » Energy Australia;
- » The Rocks Chamber of Commerce;
- » Millers Point Resident Action Group; and
- » Local Government Association of NSW.

2.3 Submissions received

In total, 15 submissions were received which comprised:

- » 7 written submissions from government bodies;
- » 2 written submissions from non-government organisations; and
- » 8 written submissions from the public.

The seven government submissions were received from:



- » City of Sydney;
- » Ministry of Transport;
- » NSW Maritime;
- » Roads and Traffic Authority;
- » Sydney Harbour Foreshore Authority;
- » Sydney Ports; and
- » Sydney Water.

Submissions from non-government organisations were received from:

- » Harbour Rocks Hotel; and
- » Millers Point, Dawes Point, The Rocks Resident Action Group.

All submissions were received by the Department of Planning and forwarded to MCA. GHD has reviewed the comments and analysed the issues raised by each government department, non-government organisation or public submission and assisted the MCA in preparing a response to the submissions.



3. Response to issues raised in submissions

This section provides a response to the issues raised in submissions. It is arranged by issue, rather than submission, with similar issues grouped together. For each issue raised, the submission/s that raised the issue have also been identified.

3.1 Design

3.1.1 George Street addition

Issue

Raises concern with George Street elevation in regard to its impact on the existing building, both in terms of the visibility of existing windows and the architectural detailing and form of the (existing) building. The proposed addition will present as a solid four storey element, built to the George Street boundary of the site, and at over 40 metres in length will present as a very dominate element in the streetscape. You are requested to consider options to reduce the scale of the proposed addition to George Street, which should not be significantly above that which presently exists. Any loss of gallery floor space in this area could be replaced within other areas of building where it is proposed to provide for commercial tenants.

The proposal includes changing the facing material from the existing sandstone to a painted material to match the proposed northern extension. Although this has the potential to further obscure the original building detail as viewed from George Street, the works are supporting subject to agreement about the colour selection.

Submission/s

Department of Planning, SHFA

Response

To meet the MCA's needs as a major international museum of contemporary art, one of the objectives of the redevelopment is to provide better gallery space and to increase the amount of space to display the MCA's permanent collection. Galleries within the existing building are constrained by two factors: ceiling height and columns. Existing columns cannot be removed without significant expense and the addition of ceiling beams which further reduce the height. Any additional gallery space therefore has to be a new addition. The MCA also needs gallery space that is naturally top lit.

As well as the height and column restrictions, additional gallery space within the existing building is not feasible because of the need for commercial tenants. Independent financial modelling conducted by Price Waterhouse Coopers confirms that the revenue generated from the commercial tenancies is vital to cover the additional operating costs of the expanded building.

The large gallery (Gallery 2.4) in the north east corner of the new wing replaces and significantly improves upon the existing high-ceilinged gallery on Level 1, which is removed as part of the proposed design to simplify the circulation. This existing gallery has columns and a large stair through it.

To keep the height and bulk of the extension to the north below the top of the existing parapet of Level 7 (i.e. the top edge that generally defines the height of the existing building apart from the central tower), the best solution was to create additional gallery space over the George Street 1991 additions. The



proposal replaces these additions so that there is one extension in the new language. The finishes to the 1990s additions are currently failing. This also improves the existing extension at street level, with a more harmonious canopy and ground level frontage with the likelihood for increased shop activity.

The proposed addition meets the MCA's requirements, providing one gallery floor on Level 3 (Gallery 3.1 and 3.2) with lower ceilings contiguous with the existing building galleries but providing column free space, and providing a new floor above that on Level 4 the majority of which has 5m wall heights with natural top lighting (Gallery 4.2 and 4.3).

As this is an addition to the current galleries on Level 4 of the existing building, it creates a cohesive flow of galleries. The museum experience takes place over 3 floors, rather than 3 plus a mezzanine which currently contributes to the lack of good visitor flow in the exhibition spaces. It also provides the required extra floor space without a large increase in staffing as the new galleries can be patrolled by similar numbers of staff.

The western façade of the existing building has already been compromised by the 1990s additions. The new galleries in this location allow for continued commercial activity at street level and restrict the intervention to the west façade, leaving the dominant east façade without intervention and able to be seen in its original form both close up and at a distance.

The new extension is clearly a dramatic change to the existing. It accepts the large mass and volume of the existing building and responds with a strong architectural statement. The proposal accepts the monolithic nature of the existing building and expresses the new galleries as additional volumes. The language of this intervention is the same as that of the new works to the north so that the 21st century overlay is one clear language and so only two architectural voices speak: the old and the new (with the removal of the 1990s third voice).

The urban context of the MCA is difficult – both a monolithic 1940s office building seen in-the-round and also part of the George Street streetscape characterised on its west by 19th century buildings creating a fine urban grain.

The untouched eastern façade is easily read as the original building, the west however signals the change proposed by this development. It does not try to imitate the materials or modulation of the existing building, but rather imposes a new layer or language.

The cladding is glass reinforced concrete, with a colour palette chosen to reflect the dominant colours of surrounding buildings. These panels are not shiny and the colour is integral to the material – not an applied coat. As a panel system, the façade will be broken up by a recessed jointing system which is shown on the elevations but not easily read on the renderings. This too will add to the texture of the wall and help to modulate the elevation.

In response to the concerns raised, we have re-examined this elevation and modified the height as seen from the street and introduced more detail in the material palette to visually lessen the volume. We have reconsidered the Level 4 gallery and lowered the height of the middle section further revealing the strong central tower of the existing building and acting to further segment and lessen the scale of this elevation from the streetscape.

The northern 'block' of the Level 4 gallery has been reduced in height from RL 22.614 to RL 21.464 (reduction of 1.17 metres), the central 'block' has been reduced in height from RL 21.662 to RL 19.233 (reduction of 2.429 metres), and the southern 'block' has been reduced in height from RL 21.889 to RL 21.870 (reduction of 0.019 metres).

This amendment does limit the operation of this area as it will divide the space into two higher galleries with a lower central section. The design does not however try to disguise the bulk of the additions which are themselves important symbols of change for a new direction for the MCA.

These modifications are shown in Figure 1, Figure 2 and Figure 3.

Figure 1 George Street addition as originally proposed and as amended, viewed front-on

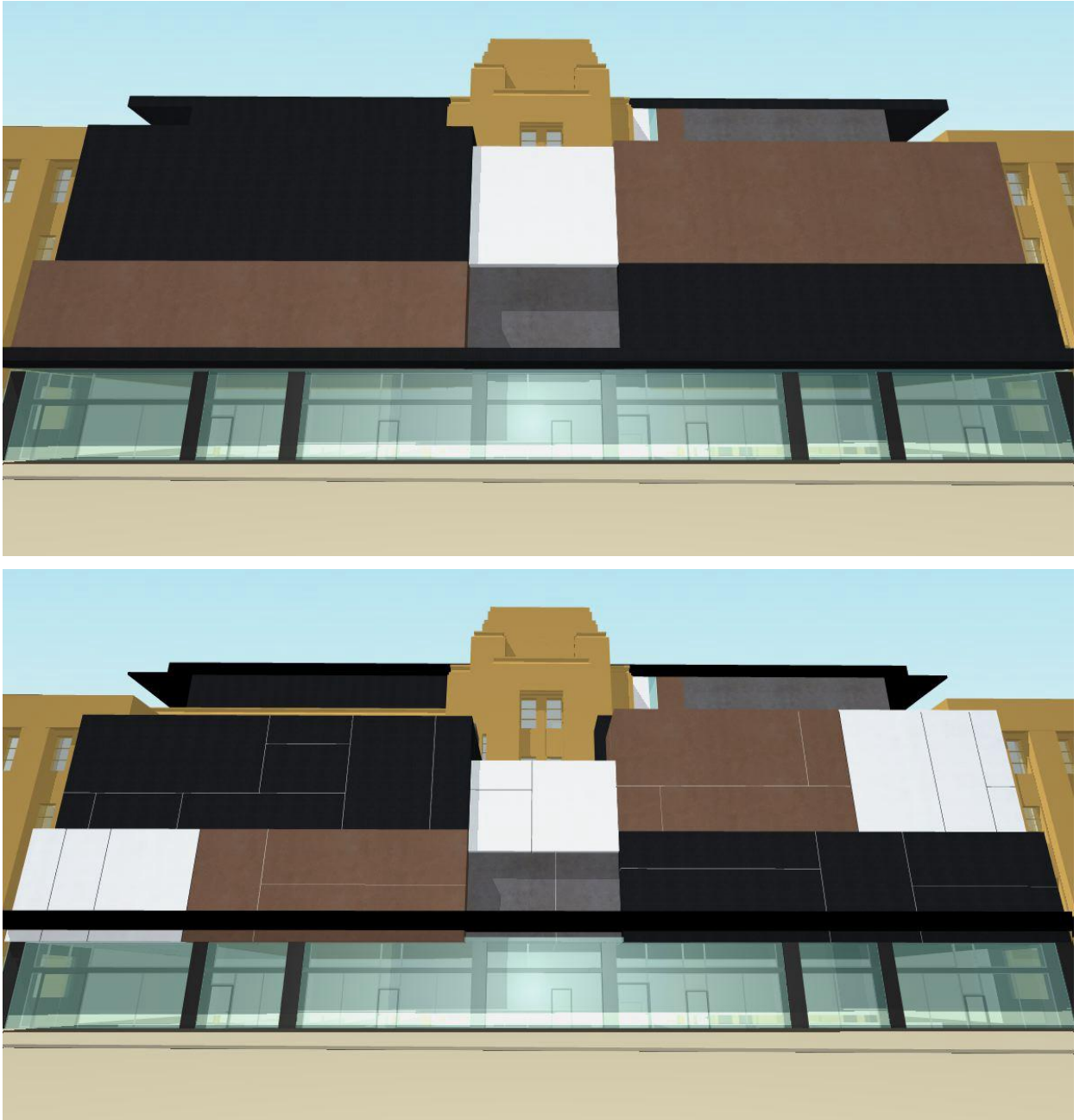


Figure 2 George Street addition as originally proposed and as amended, viewed from the south



Figure 3 George Street addition as originally proposed and as amended, viewed from above



The volumes of the new extension will be seen as you approach the building from both the north and south along George Street, but their dominance will lessen with distance and a more acute angle of vision. Travelling further away the in-the-round nature of the building again becomes evident due to this line of vision. This is also true for views above ground level from the west.

The ideas outlined above were discussed with and supported through independent review by the MCA Design Advisory Panel.



As outlined in Section 5 of the Environmental Assessment, the MCA consulted with the NSW Heritage Council, the National Trust and the Art Deco Society during design development. In these consultations, no concerns with the proposed George Street addition and its impacts on the heritage significance of the existing building were raised.

The design of the George Street addition has been arrived at with considerable thought, review and debate. For the reasons stated we believe that it is the right approach, we believe it both responds to and challenges the existing building and will introduce a new and contemporary life to the language and future of George Street.

3.1.2 Connection between the two buildings

Issue

The connection of the proposed addition to the northern end of the existing building is of a width and positioning that does not relate well to the existing building. A lighter more transparent connection could be provided to enable the separation of the new and existing buildings, to be clearly read from both George Street and Circular Quay. This could also be achieved by some simple relocation or reoriented of the internal stair and/or lift areas. The form and detailing of this connection requires further justification, and consideration should be given to modifying this area of the building.

The design of the lobby could be further improved through investigations into the placement of the lifts and stairs to reveal the northern wall of the existing façade.

Submission/s

Department of Planning, City of Sydney

Response

The 2.3m wide five level glass facade to the western side of Stair 6 is designed to be transparent. The perspectives in Figure 4 and Figure 5 show this as well as demonstrating the opportunity for both those visiting the Museum and passers by on George Street to see right through to Circular Quay through the lift core. The sandstone facade of the existing building will be visible through this glass, further showing its in-the-round nature and the MCA's respect for the existing building. Careful detailing will be undertaken to achieve a minimal glazed connection to both the existing and new, and where the new stair meets the fabric of the existing building.

Figure 4 Connection between existing and proposed building, looking from George Street

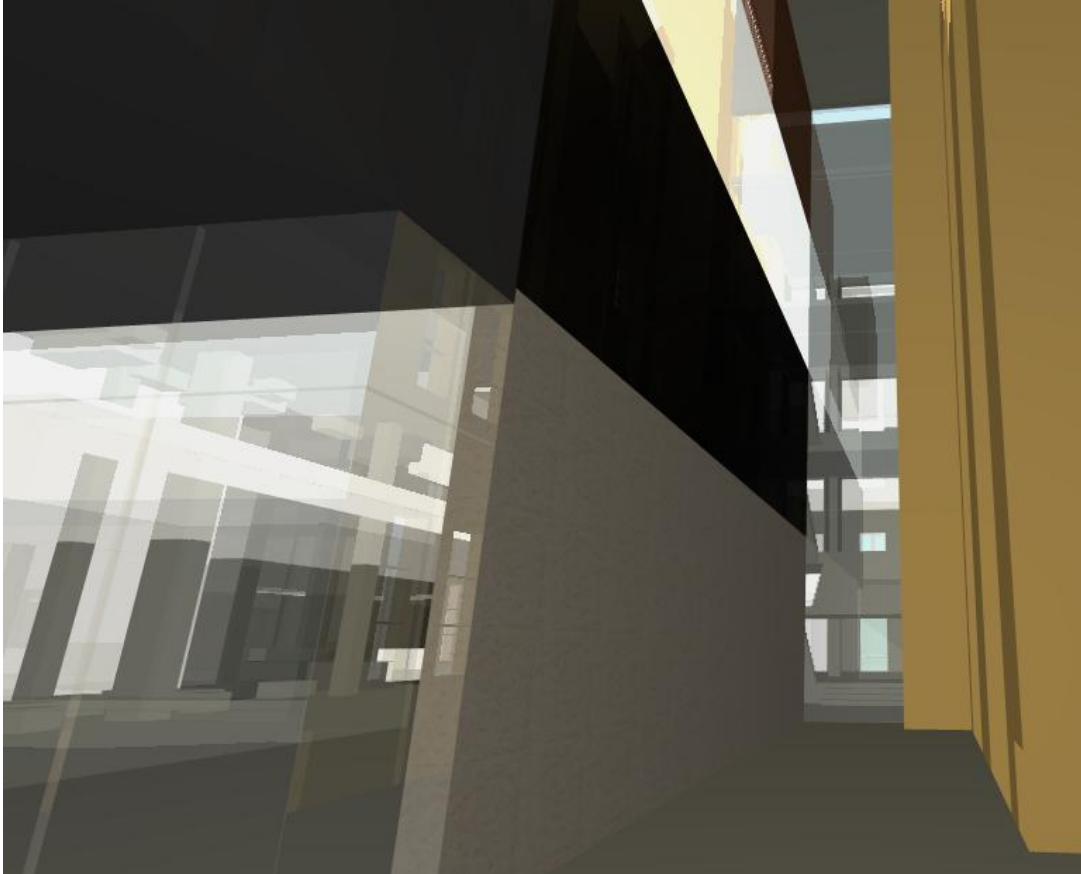
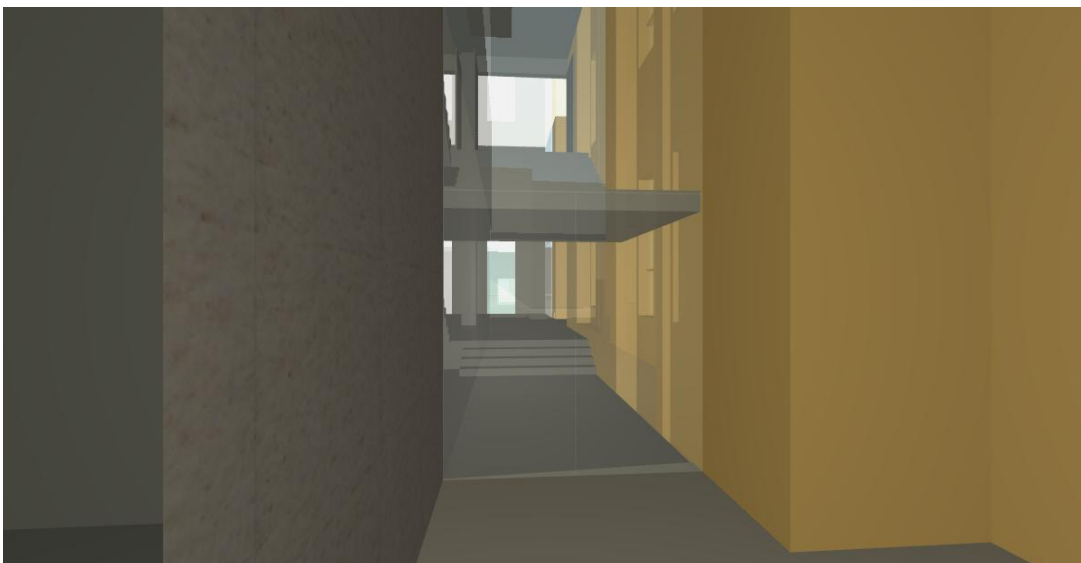


Figure 5 Detail of connection between existing and proposed building, looking from George Street



As the MCA is a public building with high attendances, it was considered important that the transparency between the buildings is accomplished from both the outside and from the inside. This has been achieved by intentionally positioning the main circulation stair up against the existing sandstone façade, by means of which visitors will be constantly interacting with the façade, and views over the Rocks to the west.

The landings between the lifts and Stair 6 will be contiguous with the levels of the existing building. The stair itself and its immediate landings will be carefully detailed to allow a clear reading of the existing building.

On the eastern side, the main new addition has been kept away from the existing building by 5.8m. Occupying part of that space is the lift core which has a glazed eastern facade to Lift 3 allowing views out from the lift cars and from the internal lift lobbies. In response to the concerns raised, we have improved the lightness and transparency of the connection between the existing and new buildings through redesigning the southern lift and its core, to remove the concrete blade on the south side of the core and replace it with glass. This will provide benefit in a greater appreciation of the sandstone facade and greater transparency from Circular Quay. The perspectives in Figure 6, Figure 7 and Figure 8 illustrate this.

Figure 6 Main entry and connection to existing building, viewed from Circular Quay

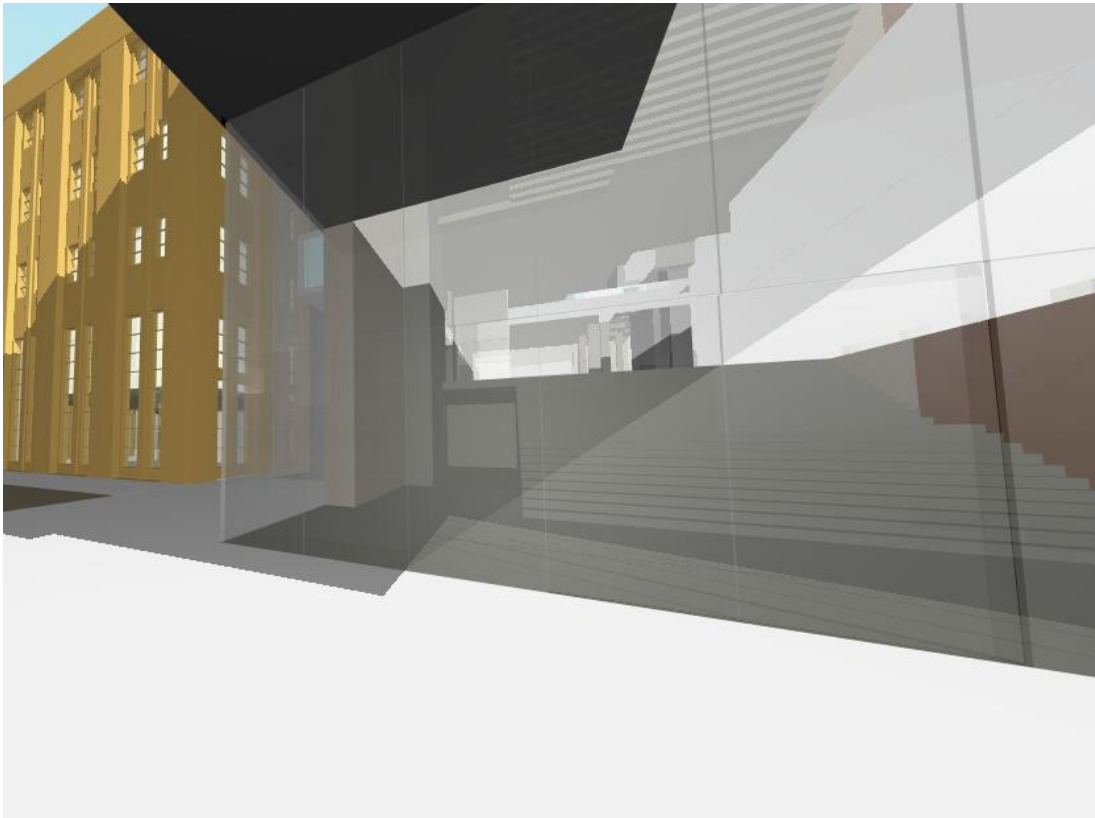


Figure 7 Glass lift and connection to existing building, viewed from Circular Quay

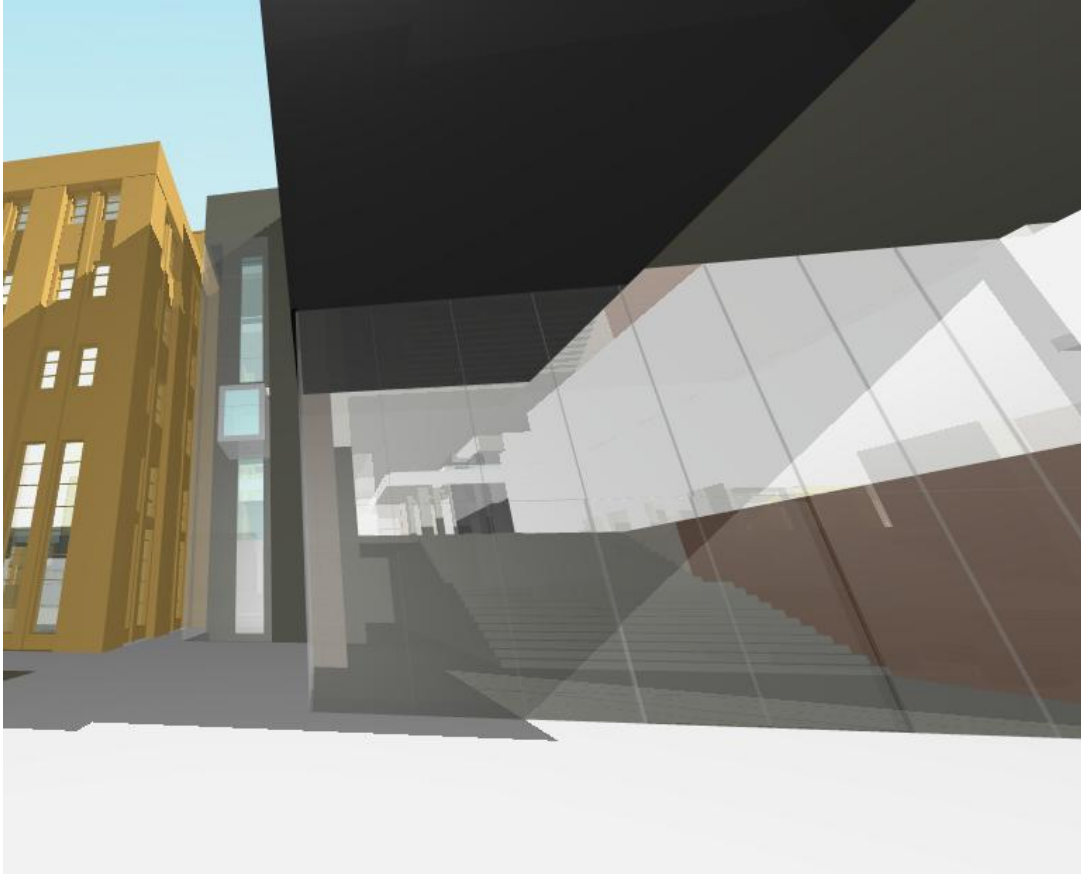


Figure 8 Detail of glass lift and connection to existing building, viewed from Circular Quay





Placement of the new building lifts and stairs has been arrived at to resolve the competing constraints of existing building levels, archaeology and MCA functional brief requirements. It is not considered feasible to relocate or reorientate the internal lifts and stairs nor move these elements further from the existing building. If Stair 6 and the Lifts 3 and 4 were pushed to the north, precious space in the foyer would be lost and the strong ordering gesture of the internal 'street' connecting George Street to Circular Quay would be compromised. Given the MCA's high attendances and projected continued trend for further increases in attendances, it is important that the 'street' foyer be able to handle large numbers such as school groups.

Reorientation of the lifts so that they open in a northerly direction would lead to confusing circulation with visitors exiting the lifts to find that the galleries are actually behind them. This would work against a key objective of MCA brief to provide clear and coherent circulation. It would also result in an unworkable layout for the MCA Shop just north of Stair 6, with further impacts on the space planning of the levels above.

3.1.3 Level 7 rooftop addition

Issue

Scale and form not compatible with existing building, presents a heavy addition, which compromises the integrity of the existing building (including when viewed from Circular Quay). Solid materials and lack of set back from George St are of concern. Could support if reduced height and minimising the depth of roof structure as per discussion in meeting.

Preference is for the walls and roof of the level 7 extension to be set back further from the existing parapets. If the walls were to be uniformly a glass material, it would increase the transparency of the vertical elements and thus reduce the overall visual impact of the extension when viewed from the west and east

Submission/s

Department of Planning, SHFA

Response

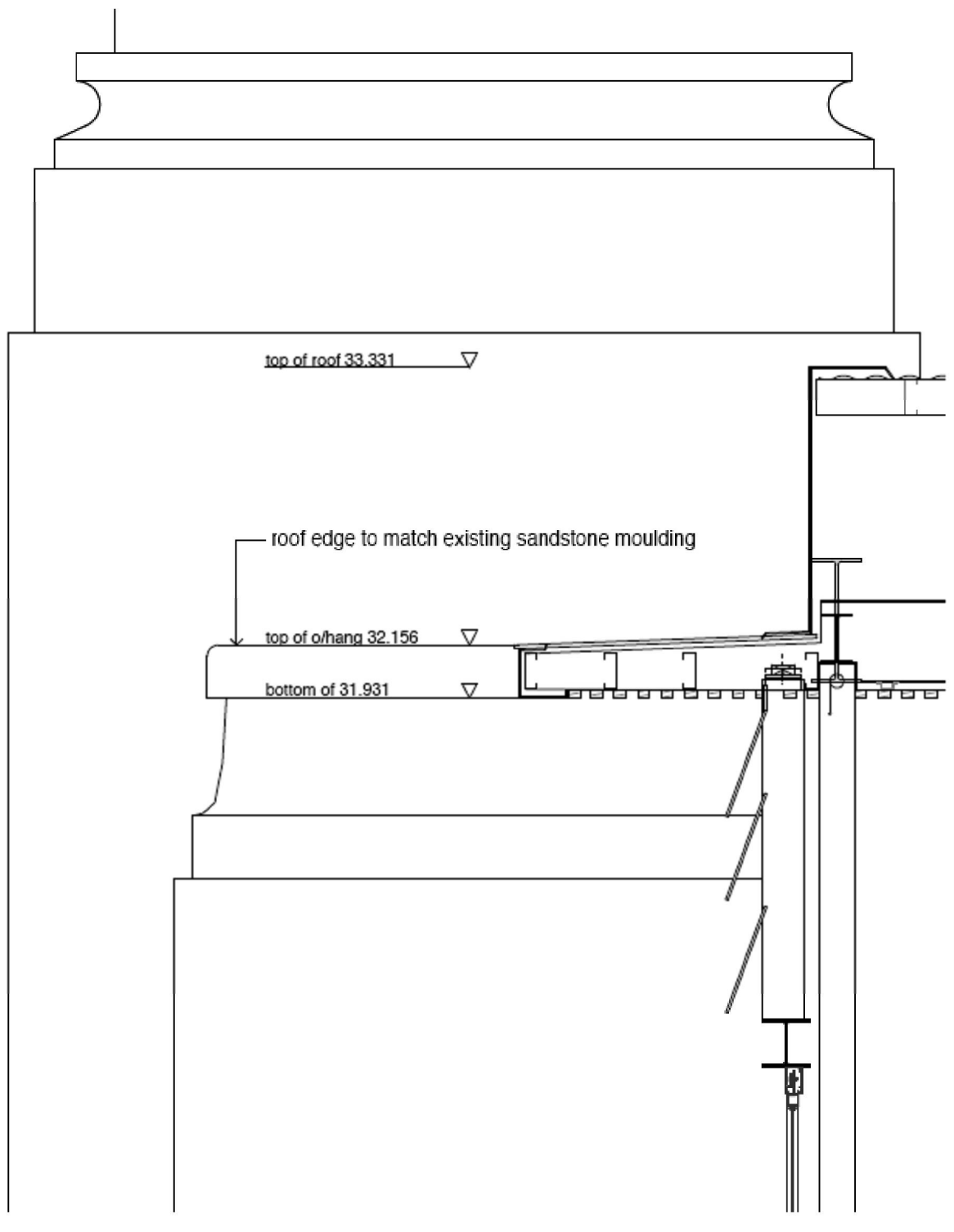
The new addition has been designed to be respectful and subservient to the existing building as the western wall of the addition is set wholly inside the existing parapet.

Setting the western wall further back from the parapet is not considered to be a good idea. It would create a gap and a likely collection point for litter and pigeons etc., and the collection of debris would be unsightly when viewed from the north or south from Decks 7.1 and 7.2.

In response to the comments raised, we have developed the detailing of the roof edge to present a finer more feathered edge to lessen its visual weight and will adjust this edge to more sympathetically meet the existing fabric.

The connection to the existing building has been reconsidered in the light of the submissions and the roof at the George Street edge has been reduced from RL 32.877 to RL 32.156 to achieve alignment with the adjacent existing sandstone moulding. A detailed section is shown in Figure 9.

Figure 9 Detailed section of Level 7 roof





3.1.4 Level 6 plant and northern stair

Issue

The new northern stair providing access to the existing building and new addition, including up to the new level 7 function areas and terraces, presents as a prominent building element. Combined with the plant to the roof top of the addition (level 6) these building elements of differing heights, materials and offsets do not appear to sit comfortably with the existing building or each other. It is requested to consider options for modifying or reorientate the form, scale and siting of these elements to provide a more ordered top level to the proposed building addition. This should also be done with regard to potential view impacts.

The massing of the rooftop extension and any new stairs needs to be resolved in relation to the roof and parapet lines of the existing MCA building. The stairs should be redesigned away from the parapet so they are not highly visible.

The plant area and stairway at Level 6 will result in view loss to the Harbour Rocks Hotel. This includes loss of views from 12 rooms within the hotel of the following elements (to various degrees, as supported by photomontages provided with submission):

- » *Part of the Opera House;*
- » *Opera House harbour setting;*
- » *Fort Dennison;*
- » *Harbour at East Circular Quay; and*
- » *Foreshore at Bradley's Head.*

These views are considered to be significant as they are of iconic Sydney Harbour elements, and their loss will have impacts on revenue.

It is noted that the view loss occurs as a result of ancillary, service floor space, and suggestions have been made for alternative design options to reduce the impact.

Submission/s

Department of Planning, Harbour Rocks Hotel

Response

The Level 6 plant room is tight on size. This plant room houses the chillers, fans, heat exchanges and reverse osmosis water purification plant providing efficient control of the critical and non critical conditions of the MCA and ensure control of the gallery collection. It is not possible to accommodate this plant anywhere else in the existing building and the size of the plant room has already been considerably reduced due to the adoption of a seawater exchange system (part of the MCA's environmental design approach which brings environmental benefit and eliminates the need for cooling towers). We have investigated reordering the plant however an east-west orientation is not achievable as it would compromise the structural integrity and services distribution dictated by the planning of the building levels below. Further to this we believe the current design balances the building form and arrangement and refers to the new Level 7 roof. We believe the proposed plant room location and design is both optimum and logical.

However, we have reviewed the top of Stair 6 in the light of the concerns raised regarding the bulk and view impacts. In this regard, the plans have been amended to remove approximately three quarters of this stair from above the old building parapet. Figure 10 through to Figure 15 shows this and the associated beneficial impact on the view loss from buildings to the west.

Figure 10 Level 6 stair as originally proposed and as amended, looking north-west

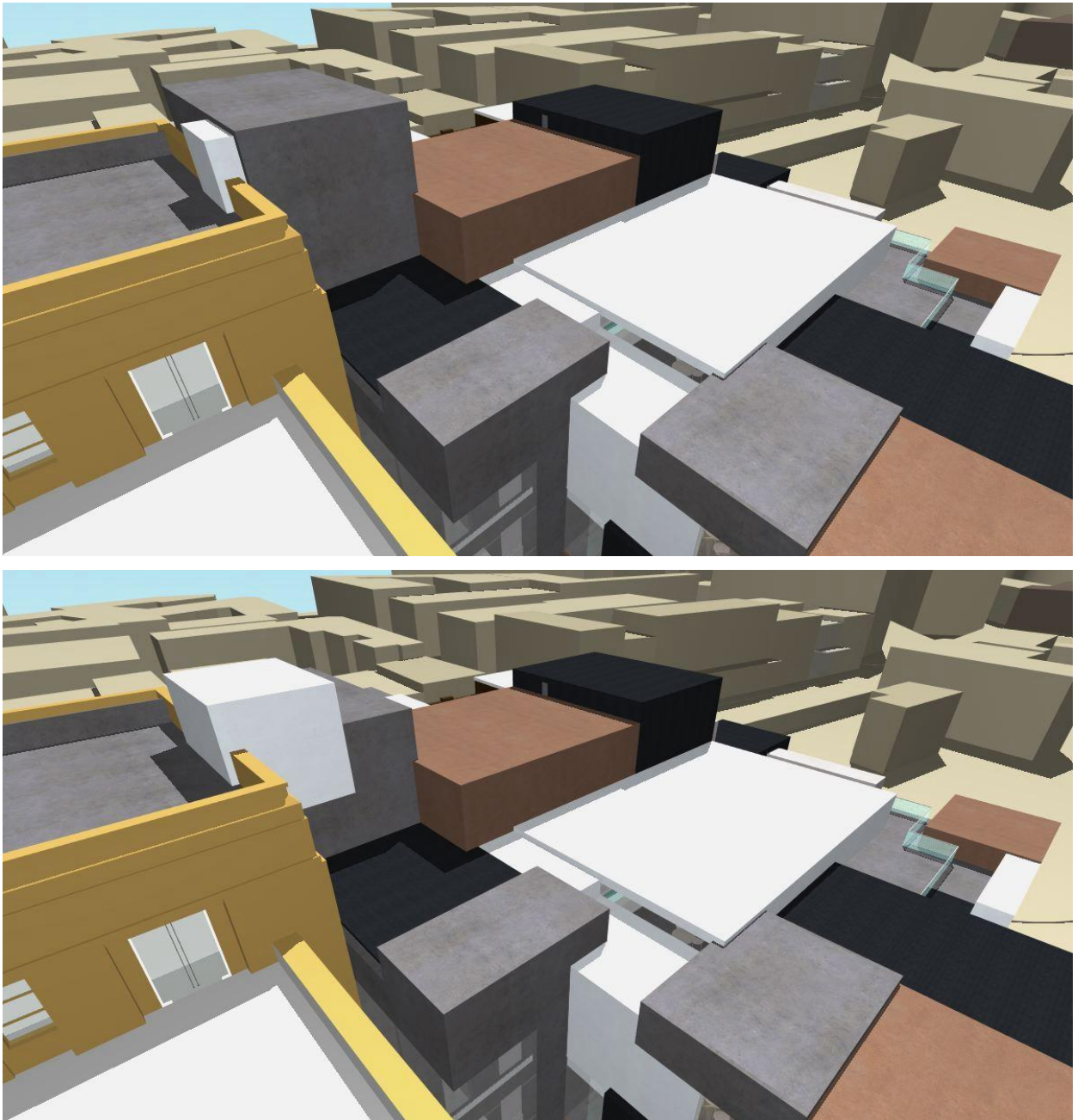


Figure 11 Level 6 stair as originally proposed and as amended, looking west

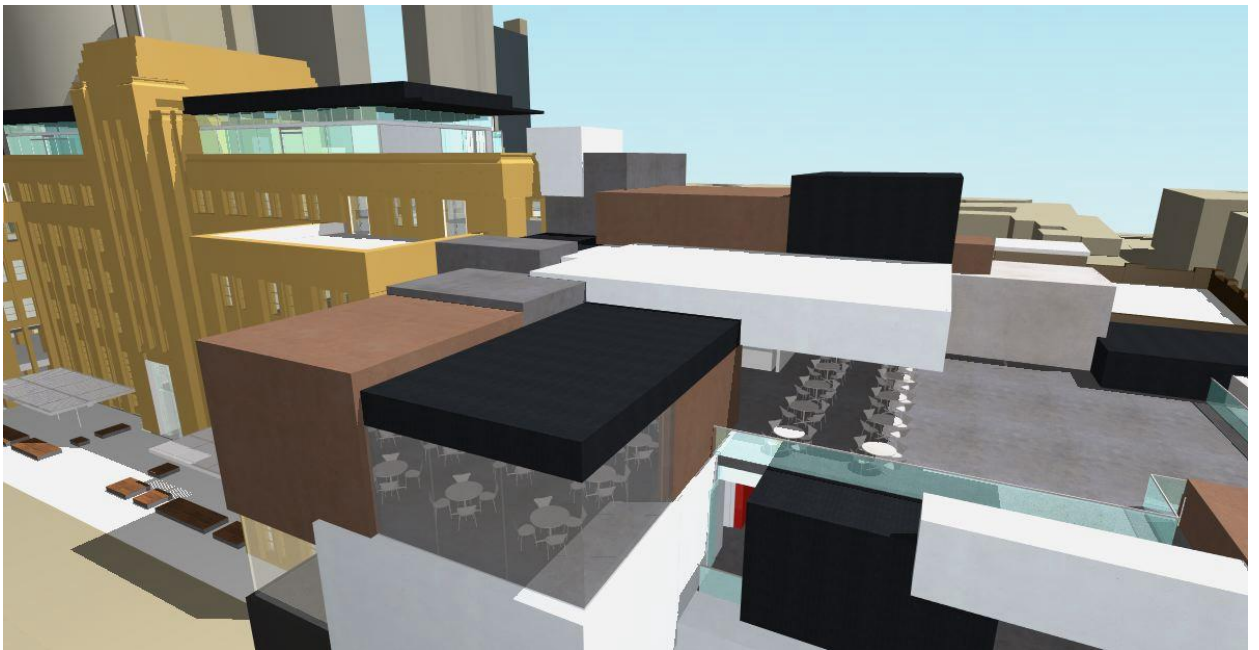
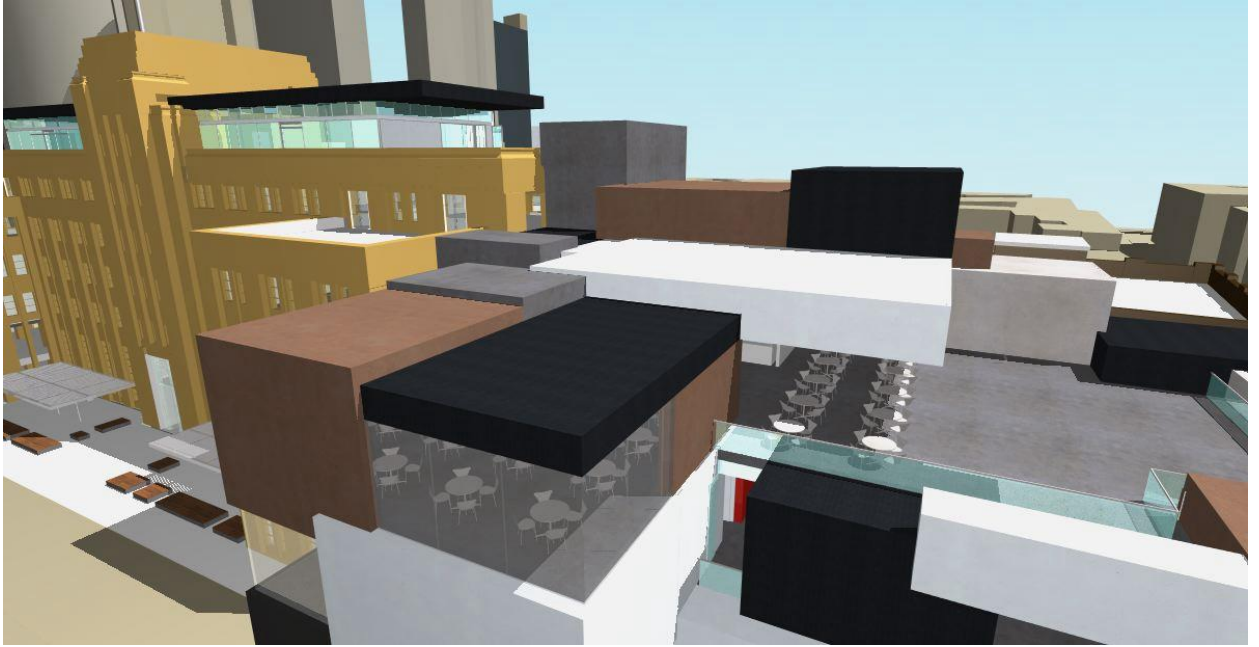


Figure 12 Level 6 stair as originally proposed and as amended, looking south

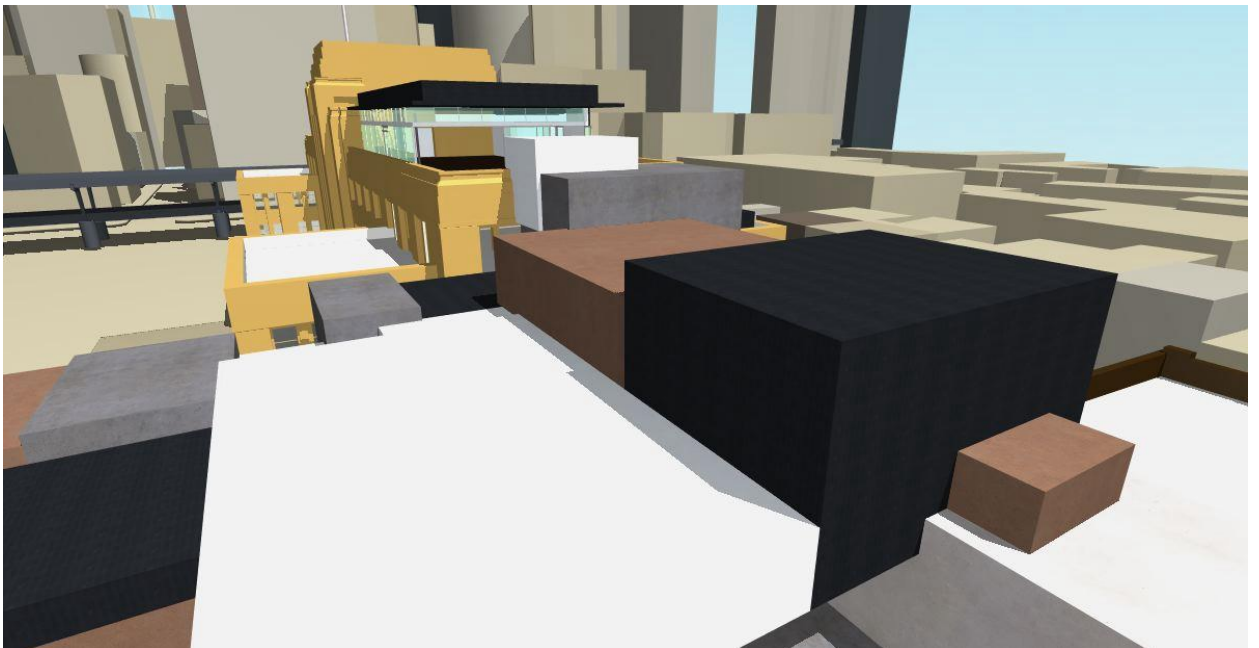
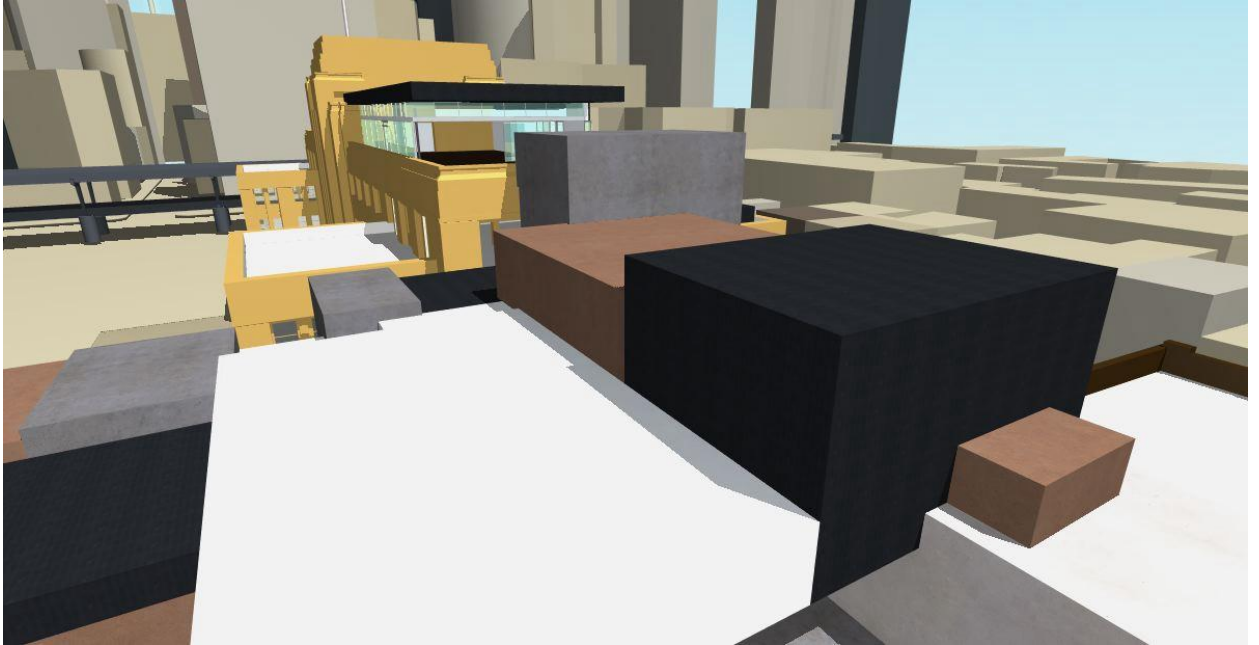


Figure 13 Level 6 stair as originally proposed and as amended, looking east

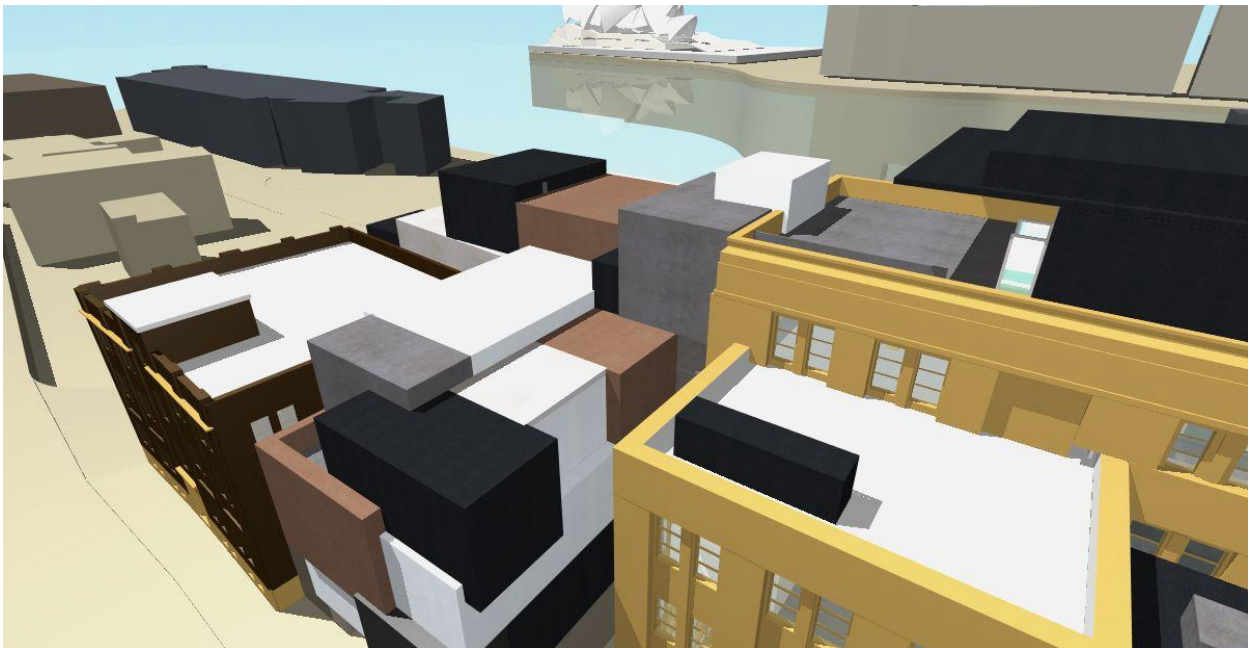


Figure 14 Level 6 stair as originally proposed and as amended, viewed from George Street



Figure 15 Level 6 stair as originally proposed and as amended, viewed from Circular Quay





3.1.5 Overhang of building/building encroachment on Circular Quay West

Issue

The proposal includes a significant projection of approximately 5.5 metres over the property boundary to Circular Quay. It is noted that photomontages have been provided and that these indicate some public view impacts as a result of this projection. These view impacts need to be justified in your response.

The scheme will overhang the site boundary along the Circular Quay frontage. The overhang will also have an impact by reducing views to the Sydney Harbour Bridge from the promenade to the south and affect views to the former MSB Building from the north. This impact is considered less given the width of the eastern promenade.

Submission/s

Department of Planning, SHFA

Response

New views have been generated to show existing views of the Harbour Bridge, and the impact of the proposal (based on the plans submitted with the Project Application), as shown in Figure 16, Figure 17, Figure 18 and Figure 19.

These figures show that the key view from the main Circular Quay West promenade retains complete views to the Bridge (pylons and main arch) as well as the facades of the key buildings on the west side of Circular Quay West. Similarly, although the vista of one pylon of the Bridge is lost when viewed from the Pitt Street extension footpath, the above facades of the key buildings are retained. The resultant vistas are typical to this area, i.e. views beyond are seen through layers of twisted streets and buildings.

Figure 16 Before and after view of the Harbour Bridge, looking from the south-eastern corner of the Circular Quay West promenade



Figure 17 Before and after view of the Harbour Bridge, looking from the Circular Quay West promenade directly in front of the existing building



Figure 18 Before and after view of the Harbour Bridge, looking from the southern end of the Pitt Street extension footpath



Figure 19 Before and after view of the Harbour Bridge, looking from the Pitt Street extension footpath, in front of the existing building





3.1.6 Circular Quay/Level 1 terraces

Issue

The design detail of the covered areas does not appear to clearly pick up the strong articulation of the existing building. This may be due to the scale of the drawings provided. As requested at our recent meeting a more detailed larger scale drawing of this area should be provided, and consideration may need to be given to design and/or material changes to provide structure(s) that relate appropriately to the existing building.

Insufficient details are provided to assess the potential impacts of the proposed new canopy to the Circular Quay elevation.

The issue of whether the proposed canopies facing Circular Quay should be permanent or temporary needs further investigation and details.

Submission/s

Department of Planning, SHFA, City of Sydney

Response

The MCA's functional requirement is for weather protection to the restaurant dining (Terrace 1.1) and Ambassadors Terrace (Terrace 1.2). To overcome the possibility of messy canvas umbrellas being used, a permanent but lightweight and finely detailed awning was adopted. Detailed plans, elevations and section of these awnings are shown in Figure 20, Figure 21 and Figure 22.

A detailed investigation has shown that the thickness of the awning edges can be cut down by 175 mm (from approximately 470 mm to 300 mm thick). The awning's edges are universal beam sections whereby the top edge will cast a shadow on the remainder of the edge, giving an edge largely in shadow.

The break up of the awnings respects the existing building by providing two awnings symmetrical about and fully showing the old entrance doors and vertically proportioned central tower block. The awning design picks up on the strong articulation of the existing building. The awning structure is aligned with the stone mullions behind and the horizontal element of the awning aligns with the horizontal spandrel panel which to the north covers the level 2 slab.

As minimal as possible structure is considered to be the most appropriate design. Given the importance of the location and the institution, a temporary canopy was not adopted as it was felt that a membrane structure would be out of place and would require additional height to be able to stretch the fabric to give the required falls for rainwater.

The MCA's functional requirement for weather protection means that a temporary structure would likely be put up once and would stay up as such. It is also not possible to design an effective temporary awning that can instantly be changed from allowing sun onto patrons on the terraces, to provide sun shading, then to providing full undercover rain protection.

To overcome the possibility of messy canvas umbrellas being used if no awning was provided, a permanent but lightweight and finely detailed awning was adopted. This is considered to be the logical and practical design solution.

Figure 20 Detailed plan of proposed awnings to Level 1 terraces

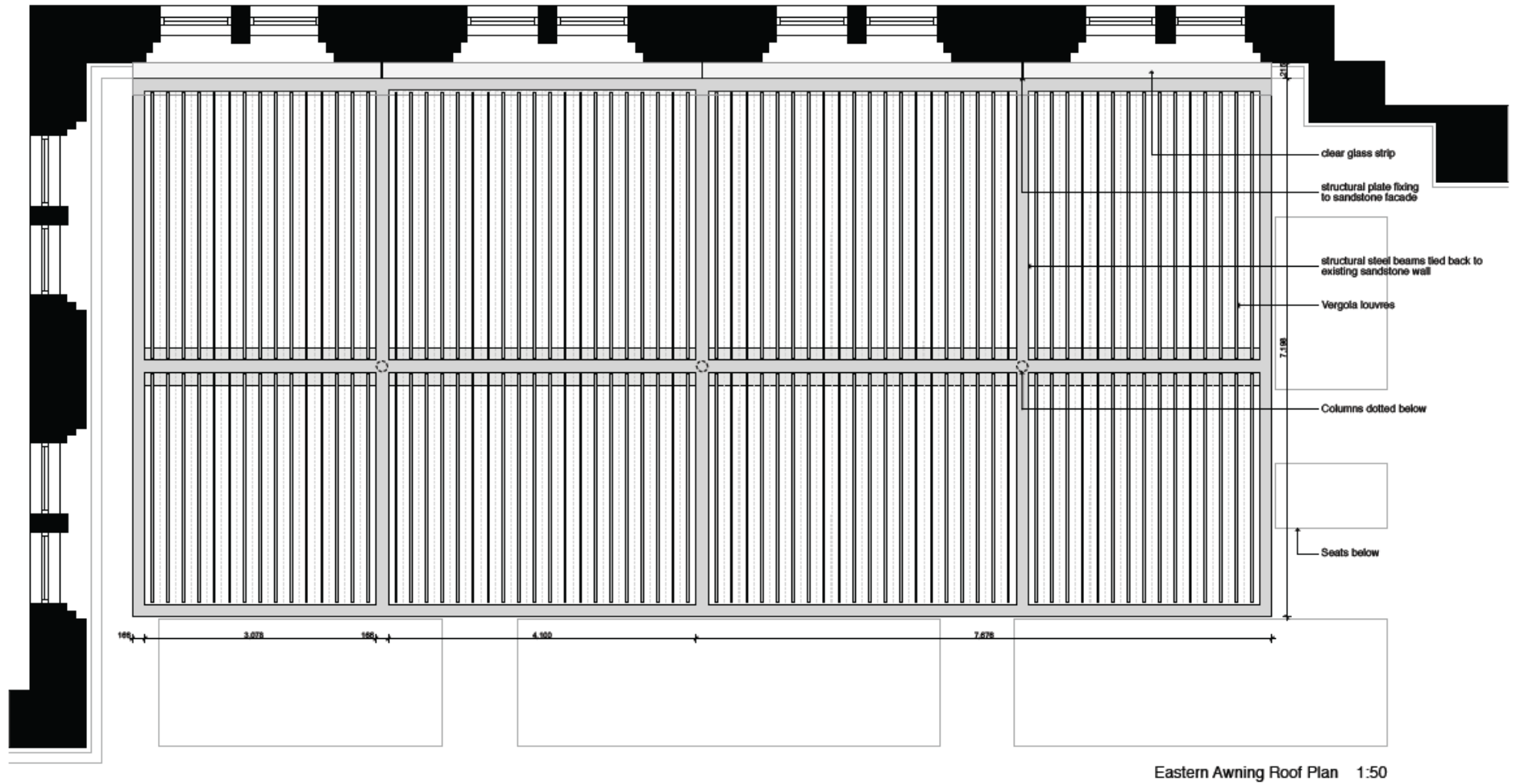


Figure 21 Detailed elevation of proposed awnings to Level 1 terraces

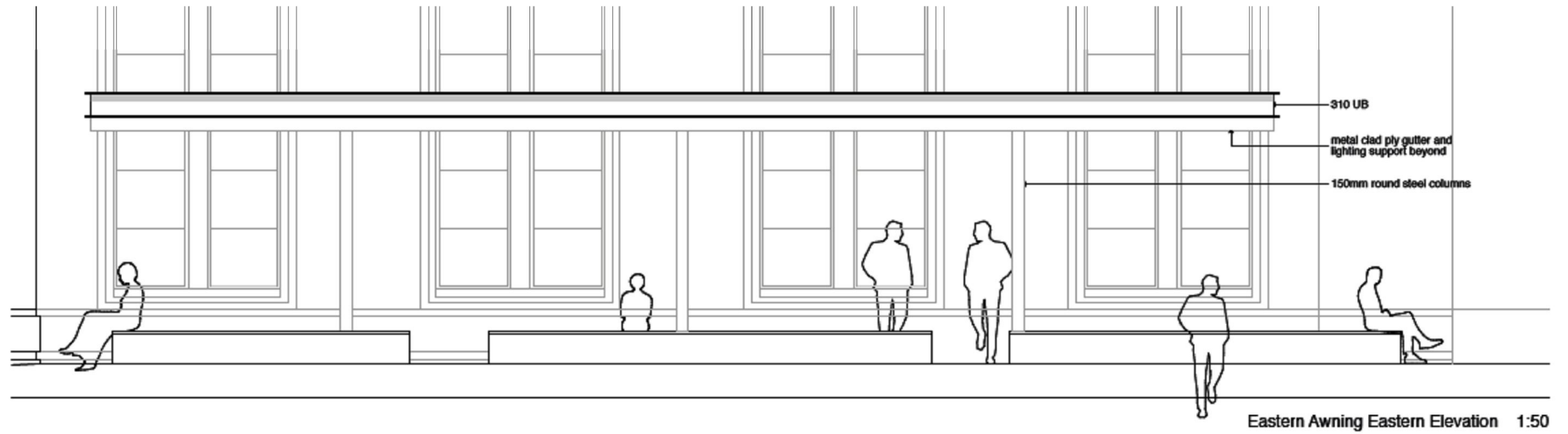
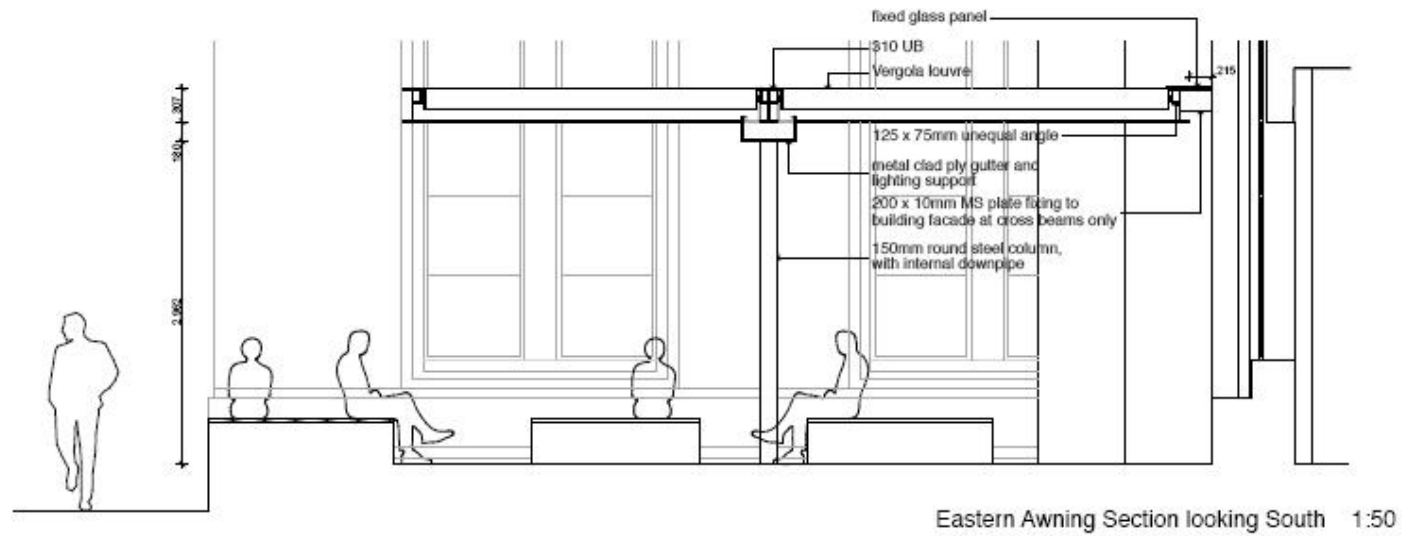


Figure 22 Detailed section of proposed awnings to Level 1 terraces





3.1.7 Building encroachment on George Street

Issue

Building encroachment on George Street is minimal and potentially acceptable. Though there is no justification for encroachment over George Street besides for architectural effect, without further justification the proposal should be kept within site boundaries.

The scheme will overhang the site boundary along the George Street frontage. Issues to be considered include the vista along George Street, views to 132 George Street and the formation of a significant modern element from the south direction.

It should be noted that the George Street encroachment is a departure from the Master Plan which showed the building to align with the site boundary.

Submission/s

SHFA

Response

Careful consideration has been given to the projection over George Street and comments from SHFA have been accommodated in the proposal.

Prior to the project application being submitted, the projection cantilevered beyond the alignment of The Rocks NSW Police building into George Street. Consideration of SHFA's concerns at the time resulted in a redesign of this element and the internal planning to bring the projection back to the current alignment, with the street level being on the boundary and the building above in line with The Rocks NSW Police building. As a result of design changes made in response to SHFA concerns prior to submission of the project application, the capacity of the Lecture Theatre is below the MCA's original functional requirements.

It should be noted that in subsequent meetings with SHFA, they have indicated that they would consider providing a lease or boundary adjustment for the part that cantilevers over other SHFA land (George Street).

Moving the entry doors further to the east would compromise the amenity of a rain protected entry. It would also impact on the entrance foyer, increasing the ramp gradient by reducing the length over which to achieve the change in level to get up to the existing building Level 2. Similarly, reducing this cantilever would severely impact on the capacity of the Lecture Theatre (which is already below the MCA's original functional requirements) and compromise MCA use of one of the key education provisions.

We believe that the projecting façade of the new building strengthens the streetscape by focussing the view corridor towards the Harbour Bridge.

We believe that the concerns raised by SHFA on this issue have already been successfully addressed. To the north there is a generous gap to The Rocks NSW Police building and to the south a generous gap to the old building. Both give ample space to contemplate these adjacent facades. As the cantilever is aligned with The Rocks NSW Police building, no existing views to the north beyond that building are lost and the northern gap reveals the southern facade of The Rocks NSW Police building. The proposed cantilevered facade has been faceted whereby that the lower northern panel is set back the most to reveal more of The Rocks NSW Police building.



3.1.8 Blank wall on Circular Quay façade

Issue

The blank 'grey' wall on the Circular Quay elevation of the building at the ground floor needs to address the public domain and could be further articulated (opportunity for artwork/multimedia).

Submission/s

City of Sydney

Response

It is accepted that this wall should be animated by a commissioned work of art incorporating the archaeological interpretation.

3.1.9 Parapet

Issue

The proposed break in the parapet of the building is satisfactory as long there is further resolution of the design details.

Submission/s

City of Sydney

Response

Modifications have been made to the Level 6 stair, as described in Section 3.1.4.

We are otherwise in agreement with the City of Sydney that the cut through the parapet and how Stair 6 abuts the existing building are very important details. A clear separation between the old and new will be articulated when finely detailed.

3.1.10 Sculpture terrace

Issue

There is a proposed sculpture terrace on level 5 of the extension. This will be located on the northeast corner of the building and will display sculptures from time to time. Request that a condition limiting the height of sculptures be implemented in order to minimise the visual impacts on the hotel.

Submission/s

Harbour Rocks Hotel

Response

It is proposed that sculpture installations on the Sculpture Terrace be temporary and regularly changed. Their inclusion will add points of interest to the view from the hotel. Any condition that restricts the height of the sculpture will restrict the artists' creativity and is not considered appropriate or in keeping with MCA's objective for the Sculpture Terrace.



3.1.11 Building height

Issue

The project exceeds the building height requirements by 18m, and creates visual impacts by its 'bulky' appearance.

The proposal does not comply with the height requirements of the SCRA Scheme.

The Authority notes that the proposed development exceeds the height restriction set out in the Sydney Cove Redevelopment (SCRA) Scheme Building Site Control Drawing XXXIX and will require consideration by the Minister to vary the scheme.

Submission/s

NSW Maritime, SHFA, Harbour Rocks Hotel

Response

The building height prescribed by the SCRA Scheme was based on a proposal to demolish the existing MCA building. It can not be considered to provide an appropriate height limit for the site, having regard to the existing building.

As outlined in Section 4.4.1 of the Environmental Assessment, the proposed height of the northern extension is considered to be consistent with the height of surrounding buildings and lower than the highest points of the existing building.

An application to vary the SCRA Scheme was submitted with the Project Application (Appendix C of the Environmental Assessment).

3.1.12 Views from Suez Canal and George Street to Circular Quay

Issue

The proposal delivers an effective pedestrian link but affects the visual link by sloping the foyer floor up from the George Street entry to the top of the stairs leading down to the Circular Quay entrance.

The existing car park provides a narrow viewing slot to and from The Rocks precinct and Sydney Cove, which will be deleted.

Submission/s

SHFA, NSW Maritime

Response

A wide 'street' through the new MCA addition satisfies the intent of the SCRA scheme as a pedestrian and visual link. This is further reinforced as a visual link through the open gaps between the existing building and new addition on the east and west sides as well as the glass transparency through Stair 6 and the lifts as covered elsewhere in this report.

As demonstrated in Figure 23 and Figure 24, a view of the water of Sydney Cove is retained from the George Street entrance.

Figure 23 View of water of Sydney Cove from the George Street entrance

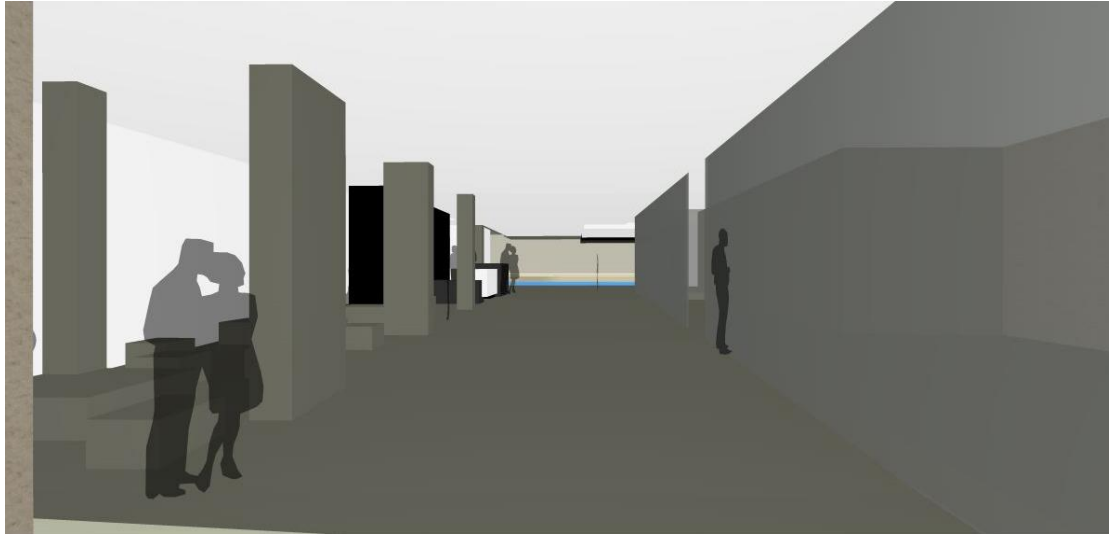
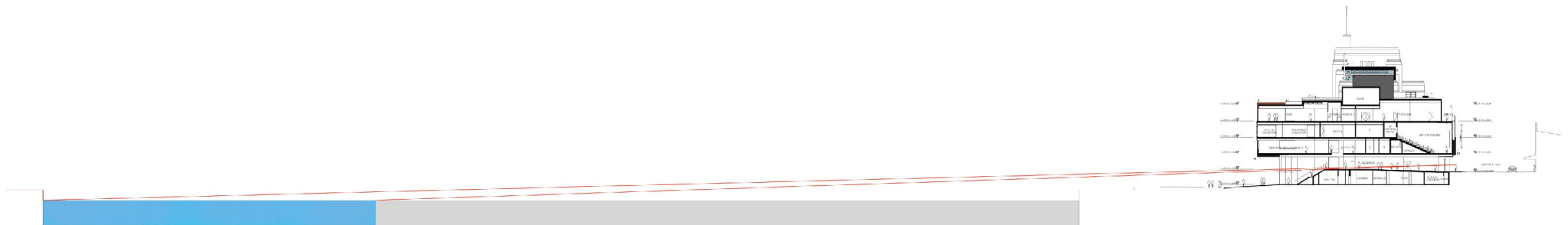


Figure 24 Section showing sight lines to the water of Sydney Cove from the George Street entrance





3.1.13 Design Advisory Panel

Issue

The Authority considers that comments and recommendations arising from the DAP should be considered in the submissions.

Submission/s

SHFA

Response

As outlined in Section 3.2 of the Environmental Assessment, the Design Advisory Panel played a significant role in the development of the proposed design. A letter from the DAP, confirming their support for the proposal, is included in Appendix A.

3.1.14 Proposed new George Street canopy

Issue

Insufficient details are provided to assess the potential impacts of the proposed new canopy to the George Street elevation.

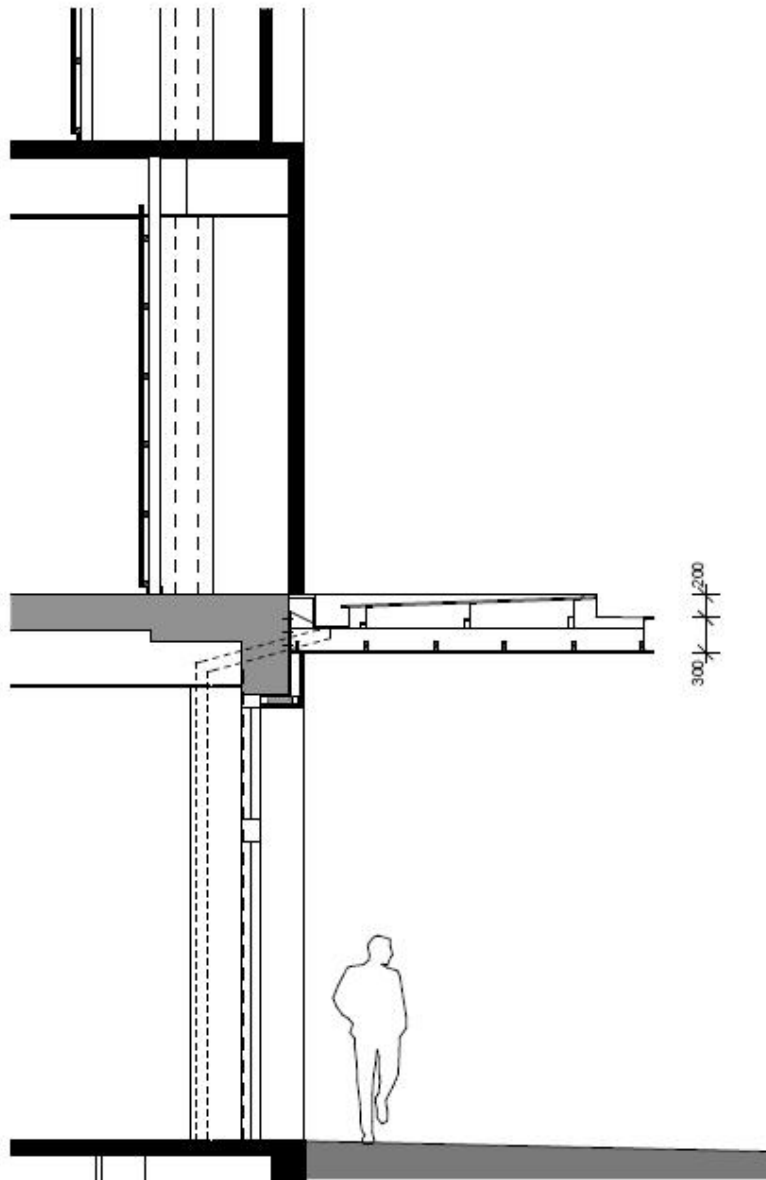
Submission/s

SHFA

Response

The detailed section shown in Figure 25 shows that the design of the edge of the George Street awning is detailed in the same manner as the roof edge to the Level 7 roof and the Level 1 eastern awnings.

Figure 25 Detailed section of the George Street awning



3.1.15 Level 3 deck

Issue

The floor plan indicates a deck proposed to abut the southern wall of the Police Station. The proponent should seek confirmation that his arrangement is satisfactory to the NSW Police Service.



Submission/s

SHFA

Response

NSW Police has agreed to the proposed design and signed a Memorandum of Understanding with SHFA and the MCA in regard to the proposed redevelopment.

A letter from NSW Police, confirming their support for the proposal and acceptance of Deck 3.1 abutting The Rocks NSW Police building, is anticipated shortly.

3.1.16 Unsecured area

Issue

There is an indented area at the George Street level directly south of the entrance that is not utilised and which may attract litter and potential anti social behaviour. The Authority would seek to secure and deny access to the area by the general public.

Submission/s

SHFA

Response

The MCA understands the potential for litter and anti-social behaviour in these areas and it is in their best interests to manage these through the use of CCTV cameras, lighting and security patrols.

3.2 Public domain

3.2.1 Public domain improvements

Issue

Requested to provide further detail of the scope of works, which should include all of the area to the west of the existing path, which is located at the front of the existing MCA and proposed addition. Objective of the public domain should be to create an area of high quality public space, appropriate & compatible with location /surrounding environment and should include appropriate heritage / archaeological elements.

The City of Sydney Design Advisory Panel suggested that the project could be a timely opportunity for the City and the State Government to form a joint vision on the area.

The entry and interface of the building should be well resolved and relate to the public domain.

The proposed ramps, grassed embankments and paved forecourt will have an impact on the functionality of the West Circular Quay public domain. While the Authority supports the need for a reconfiguration of the promenade to accommodate the MCA extension, it also seeks further consultation with the proponent and other stakeholders.

Submission/s

Department of Planning, City of Sydney, SHFA



Response

It is understood that due to funding constraints the previously planned SHFA Circular Quay Public Domain Revitalisation project will not proceed. In spite of this, MCA are committed to working with the SHFA and the City of Sydney to establish strategic urban design principles for West Circular Quay and First Fleet Park public domain. This is reflected in the Statement of Commitments (refer to Section 5).

The proposed MCA development aims to provide a welcoming and inclusive facility. This aspiration is supported and enhanced through the extension of the design ethos beyond the building to the public domain.

The landscape plan submitted with the Project Application shows the new landscaped forecourt to the east of the MCA building on West Circular Quay. This plan, as included in the plans of the Preferred Project in Appendix B, has been amended to further clarify the boundary for the MCA landscaping works.

The proposed landscape design reconfigures the area east of the MCA to respond to the new building and improve the public amenity, signifying the MCA's new Circular Quay entrance and providing a mix of active and passive areas to support public engagement.

The proposed landscaping design will reorganise and simplify the circulation paths to and from the MCA building promoting the principles of legibility and accessibility for all people using the space. It will in turn help to promote the easy movement of pedestrians through the building via the new "public street" linking the Circular Quay promenade to George Street.

It will generate new activity zones for public meeting and recreation and provide a quality, welcoming and inclusive environment for the community; irrespective of whether they enter the building or not.

It will provide an opportunity for public education through both interpretation of the heritage and archaeological of the site, and the potential use for public events and gathering.

The landscaping design incorporated a mix of hard paving and grassed areas regraded to achieve equitable access for all and incorporating seating zones, new appropriate planting, and shading. The shading will be provided by trees, selected to have thin trunks and high canopies to achieve the dual objectives of providing shade, and maintaining views.

The design will resolve and interface with the existing pathways and site levels of the surrounding public domain and reflect the original topography sloping east away from the building towards the Quay.

The proposal falls within broader strategic principles for the West Circular Quay and First Fleet Park and it is hoped that this proposal will assist in prompting further consideration of improvement to the landscaping of the wider area by the project stakeholders.

Considerations in this broader context include:

- » Organisation and zoning to provide opportunity for active and passive recreation – individual and large scale events;
- » Improved circulation and identified through routes north /south and east / west;
- » Integration of areas (First Fleet Park, Circular Quay promenade, MCA forecourt, etc) within an overall design philosophy;
- » Connection of the MCA building to the public domain;



- » Reflection of the history and archaeology of the precinct – consideration of the original shoreline and topography of the site sloping down from the west to the water;
- » Accessibility for all;
- » Appropriate planting, seating and shading;
- » Environmentally Sustainable Design principles;
- » Public safety and security; and
- » Views.

3.2.2 Removal of landscape features and trees

Issue

Design removes a large proportion of established landscape features and trees along Circular Quay West but little detail is supplied in regard to the future landscape design. There are a number of features such as the 1950s Arthur Phillip monument that are removed without justification.

A majority of trees to be removed are in a good condition and should be retained as it is generally very difficult to establish trees in such a location close to the harbour with high pedestrian traffic. These trees include:

- » *Jacaranda trees (trees 1,8 -13)*
- » *Livistona palms (trees 2-7)*
- » *The Southern Hackberry trees on George St (trees 26 & 27). Addition planting to George St should also be considered.*

A revised arborist report should be submitted to ensure the above trees are retained and protected adequately throughout the development. Suitable new planting should be undertaken as part of the development. A Landscape / Public Domain plan should be submitted including measures to ensure retention of the above trees, transplanting and replanting.

Acknowledge the need to remove trees that are within the building footprint but request that trees identified for removal as part of the broader public domain works are not removed before the general arrangement of the reconfigured public domain is resolved.

Submission/s

City of Sydney, SHFA

Response

The proposal requires removal of the existing trees which fall within the footprint of the new building envelope and construction zone. As noted in the Environmental Assessment the assessment by Australis Tree Management identified the trees for removal to have no major environmental or habitat value.

Many of the trees have been confirmed by the arborist as being in a poor state of condition and inappropriate for the location.

It is proposed that the development of the landscaping strategy for Circular Quay and First Fleet Park public domain will consider the opportunity to transplant the existing Cabbage Palms which are in



adequate health. This strategy will take into account the requirements for public amenity to the area, including shading and the provision of appropriate trees, selected to have thin trunks and high canopies to achieve the dual objectives of providing shade, and maintaining views.

3.3 Sydney Regional Environmental Plan (Sydney Harbour Catchment) 2005

Issue

The proposed built form is considered to detract from the existing visual and environmental qualities of the area (particular Circular Quay West). Clauses 13, 22, 25 and 26 of the SREP are of particular relevance.

Consideration of the proposal needs to reference the Sydney Regional Environmental Plan (Sydney Harbour Catchment) 2005 - SREP 2005. One of the required matters of consideration is in relation to the maintenance, protection and enhancement of views and the scenic quality of the foreshore area (Clauses 25 and 26 of the SREP 2005).

In regard to Clauses 25 and 26 of the SREP, the project deletes the existing views to and from the Rocks precinct.

Submission/s

City of Sydney, NSW Maritime

Response

The proposal is considered to be consistent with the relevant planning principles for land within the Sydney Harbour Catchment, as outlined in Clause 13 of the REP.

Clause 20 of the REP states:

The matters referred to in this Division (together with any other relevant matters):

- (a) are to be taken into consideration by consent authorities before granting consent to development under Part 4 of the Act, and*
- (b) are to be taken into consideration by public authorities and others before they carry out activities to which Part 5 of the Act applies.*

Accordingly, as the project is subject to Part 3A of the *Environmental Planning and Assessment Act 1979*, Clauses 22, 25 and 26 of the REP do not apply to the proposed redevelopment.

Notwithstanding this, an assessment of the proposed redevelopment generally in accordance with the intent of these clauses was undertaken in Section 6.1.8 of the Environmental Assessment, which is further supplemented by the analysis provided in Section 3.1.5 above.



3.4 Archaeology

3.4.1 Impact on archaeology

Issue

The proposal to include a sea water heat exchange system for the air conditioning is supported. However, the proposal has potential impacts regarding archaeology, existing services, the seawall and the public domain. The Authority seeks further details and impacts prior to supporting this initiative.

Submission/s

SHFA

Response

As part of the redevelopment design process the MCA have commissioned Casey and Lowe to undertake an Archaeological Management Plan (AMP).

This plan will be guided by the site's historical background and the archaeological knowledge obtained to date from the various archaeological testing programs since 1997. The AMP will be a summation of all that is known about the site and the dockyard / Commissariat in general. It will look at what is proposed in terms of development impact, and look at where these impacts will be in terms of the historic remains. It will specify what will be carried out in the archaeological early works to record those areas of the site affected by where the development may lead to loss or disturbance of dockyard fabric.

There have been a number of archaeological testing investigations undertaken at the MCA site since 1997 including two specifically associated with the current scheme. Information arising from site testing to date is included in the AMP.

It is proposed that the construction program for the redevelopment of the MCA will incorporate separate early archaeological works with the objective to investigate and record the potential impacts of this project prior to the construction process.

3.4.2 Archaeological interpretation

Issue

The public domain should include appropriate heritage/archaeological elements.

The Authority seeks a strategy that addresses the management and display of archaeological finds, relocation of existing historical elements, interpretation of the Colonial Docks and ways of integrating site history into the overall design.

No attempt to interpret or reveal existing substrate of convict era docks in the archaeological program. This is a lost opportunity.

The entry and interface of the building with the public domain should relate to the heritage and archaeological interpretation.

Submission/s

Department of Planning, SHFA, City of Sydney



Response

An Interpretation Zone was shown on proposed Level 1 floor plan submitted as part of the Project Application. This zone is external to the proposed addition, in the vicinity of the Circular Quay entrance.

The Statement of Commitments has been modified to reflect the MCA's commitment to developing an Interpretation Strategy in consultation with SHFA and the City of Sydney (refer to Section 5). As the MCA is a contemporary art institution, it proposes to take a contemporary approach to interpretation, which will involve artists working with the archaeologist, using site and archaeological findings and the AMP for inspiration.

The AMP will act as a brief whereby artists can gain an appreciation of the place and its archaeological remains, its role in early Sydney and its heritage significance. The collection of plans and images contained in the historical section of the AMP will provide ample background material to input into the artists' interpretation of the place and to inspire creative interpretation approaches.

Since the building of the present MCA building (originally constructed for the MSB), interpretation of the place as an important part of Sydney's early history has been virtually non-existent except for the frieze above the eastern doorway showing the Commissariat building and its operations. The docks within the current car park area were unsuspected until the archaeological testing in 1997. The development of the area will leave the remains substantially unaffected but, as part of the new works, the MCA has undertaken to provide for their interpretation, which will provide benefit in educating and informing both the general public and MCA visitors. To retain the docks remains undisturbed is a good conservation outcome.

3.5 Traffic and access

3.5.1 Access through First Fleet Park

Issue

Proposal offers the opportunity to eliminate the existing vehicular access through First Fleet Park. The proposal does not demonstrate why access cannot be provided via the central service corridor. The Authority seeks a consolidation of access to remove the requirement for vehicle access through the high pedestrian areas of First Fleet Park and the West Circular Quay Promenade.

Submission/s

SHFA

Response

Most access for equipment delivery to the Foundation Hall is presently through the western Service Corridor. On infrequent occasions, when a large object exceeds the dimensions of the western Service Corridor, the Pitt Street extension footpath, immediately to the east of the MCA, is used.

Throughout the design process, we have reviewed the opportunity to deliver such large objects through the existing building and this has proven to be unfeasible without severely compromising the existing building fabric.

The western Service Corridor does not accommodate large objects and any passage would need to be considerably larger than the existing dimension. To achieve this, a passage would need to be created



from the northern loading dock through the original historic central entry Foyer or the main services risers and lift core or areas to the western side of the building which provide the main structural zones.

None of the above are considered to be practical solutions as they would involve considerable intervention and redesign of the existing building.

Using the link to the Foundation Hall in front of the lifts as suggested by SHFA is not feasible due to the requirements of public circulation, particularly on Level 1 for education groups and disabled visitors, and the associated OH&S and public liability issues.

As outlined in Section 6.3.1 of the Environmental Assessment, the MCA has agreed to develop an operational plan with SHFA to manage the ongoing use of this access.

3.5.2 Argyle Street driveway design

Issue

Argyle St driveways should be designed as 2 separate driveways with kerb laybacks and footway over at one level. Drop kerbs and pedestrian ramps should not be permitted.

Suitable sight triangles as per AS and appropriate pedestrian advisory signs at egress from car park and loading dock.

Submission/s

City of Sydney, RTA

Response

Tight constraints dictate the need for two loading dock entry doors in the northern elevation.

NSW Police require access to their Prisoner Run Dock. They must be able to get their van into their dock and close the roller door to be able to process prisoners. This activity is outside the control of the MCA.

The existing access to the loading dock from Argyle Street includes kerb returns and pedestrian/pram ramps. The Rocks NSW Police building is located on this corner which has vehicle access via the MCA driveway. Therefore, to maintain appropriate access to the dock, and to provide appropriate grades for vehicles entering and exiting the dock, the levels on this corner, including kerb returns and pedestrian/pram ramps, are not proposed to change.

Everyday MCA loading and car parking access to the MCA is through the same opening although through a different roller door. This combined opening is expressed the same as other deep openings in the addition.

Roughly once a month, the MCA receives delivery of new exhibition artworks that often come in a truck that requires a clear height of 4.5m. The only place on Argyle Street that can allow this height due to the falling grade of Argyle Street, and given the floor over is contiguous with the existing levels of the old MCA (with minimal structural depth), is at the point closest to the east as possible. This is also the only place a garbage truck can enter the site. This door will be detailed as an automatic opening hinged door clad in glass reinforced cement to match the rest of the facade so it appears as part of the wall element.

It is proposed to have one crossover for these two entry points which will be finished in paving matching the adjacent paving. The intention is that this strip read as pavement and not as road.



The existing structure (the adjacent police building) on the western side of the MCA driveway exit prevents the provision of a sight triangle on that side of the driveway. The adjacent Rocks NSW Police building has been approved in a separate application and it is not proposed to amend that consent.

Appropriate pedestrian advisory signage could be included as a condition of consent. As noted in the Traffic Impact Study (Appendix H of the Environmental Assessment), the number of parking spaces in the parking/loading area is proposed to reduce.

3.5.3 VIP parking

Issue

All VIP parking should be provided wholly within the site.

Submission/s

City of Sydney

Response

This matter is noted and could be included as a condition of consent.

3.5.4 Bus and disability access and parking

Issue

Documentation does not clarify the location of bus pick up and drop off zones or disabled parking and drop off zones. Disability and group access remains an important element of all new public buildings. Proponent needs to further explore this issue and provide clarity on how it is intended to be managed.

Of the 4 car parking spaces provided 1 should meet disabled parking requirements.

Submission/s

SHFA, City of Sydney

Response

No disabled car parking or pick up and drop off areas for car or buses currently exist on George Street from Alfred Street to the junction of Hickson Road nor on Circular Quay West. Disabled car parking available until recently on George Street south of the MCA when SHFA removed it to widen the footpaths. We understand the need for such amenities and will work with SHFA to re-establish them.

The requirement for one disabled car parking space could be included as a condition of consent.

3.5.5 Reversing truck movements

Issue

Reversing truck movements would be required to manoeuvre over the adjacent pedestrian crossing. This should not be permitted on safety grounds. Should there be no other option, then a management plan should be prepared and submitted to ensure the safety of the other road users and pedestrians can be maintained.



Concern at proposed reversal of heavy rigid trucks on Argyle Street to loading dock. Requests that trucks enter and leave the site in a forward direction. If not possible then all reversing movements should be undertaken under the control of certified traffic controllers to ensure public safety.

Submission/s

City of Sydney, RTA

Response

As noted in the Traffic Impact Study (Appendix H of the Environmental Assessment), large vehicles (including semi trailers) delivering to the MCA currently reverse into the car park from Argyle Street. This occurs relatively infrequently, generally during exhibition changeovers.

It is proposed that the size of vehicles reversing into the dock be reduced in size to 12.5 metre large rigid trucks. Therefore, the size of trucks reversing from Argyle Street will reduce.

A condition of consent could be included requiring the reversing of trucks to be supervised by appropriately qualified traffic controllers.

3.5.6 Truck deliveries

Issue

All deliveries to site should be in trucks no larger than 12.5m and loading/unloading should occur within site.

Submission/s

City of Sydney

Response

These matters are noted and could be included as conditions of consent.

3.5.7 Internal circulation

Issue

To ensure appropriate circulation, two of the four car parks should not be in use when 12.5m truck servicing the site.

Submission/s

City of Sydney

Response

This matter is noted, and was discussed in the Traffic Impact Study (Appendix H of the Environmental Assessment).

3.5.8 Police sweep paths

Issue

Confirmation that NSW Police accepts the proposed swept path analysis for their vehicles.



Submission/s

City of Sydney

Response

Consultation has occurred with NSW Police in relation to the proposed development, including access to the loading dock. NSW Police has agreed to the proposed design and signed a Memorandum of Understanding with SHFA and the MCA in regard to the proposed redevelopment.

A letter from NSW Police, confirming their support for the proposal and acceptance of the swept path analysis, is anticipated shortly.

3.5.9 Pedestrian and cycle movements

Issue

Detail the existing pedestrian and cycle movements within the vicinity of the subject site and determine the adequacy of the proposal to meet the likely future increase in demand for increased pedestrian and cycle access. This may include provision of secure bike storage & cyclist amenities (specify in detail).

Provision of suitable bicycle storage facilities including showers and change rooms.

The Construction Impact Report does not address safe cycle access. Identify measures to mitigate potential impacts for cyclists during the construction of the project.

Submission/s

Ministry of Transport, RTA

Response

Pedestrian movements in the vicinity of the site include along George Street and Argyle Street, through First Fleet Park and adjacent to Circular Quay. Bicycle movements also occur in these areas. These routes will continue to provide good access to the development.

As noted in the Traffic Impact Study (Appendix H of the Environmental Assessment), ten bicycle parking spaces for employees are proposed in the loading dock. Showers and change rooms are also located nearby.

The issue relating to construction impacts on safe cycle access would be dealt with as part of a construction traffic management plan.

3.5.10 Public transport

Issue

Opportunities to optimise the site's proximity to public transport by preparing a Travel Access Guide (TAG) as detailed in the Producing and Using Transport Access Guides (RTA).

Submission/s

Ministry of Transport



Response

The MCA currently includes travel, public transport and access information in its promotional material and on its website, and intends to continue to do so when the proposed redevelopment is completed.

3.5.11 Overseas Passenger Terminal

Issue

Impacts arising from major ship arrivals at the Overseas Passenger Terminal on vehicle access to the loading dock needs to be adequately addressed. Proponent needs to clarify how their delivery operations will be managed on ship days and when the Rocks Markets are on operating. A security plan to be developed to address security issues on ship days.

Construction and future operation could impact on the effective functioning of the Overseas Passenger Terminal. Ongoing consultation with Sydney Ports will be required.

Sydney Ports request that a condition of approval be imposed to prevent MCA construction vehicles accessing the site via Argyle Street on ship days.

Submission/s

SHFA, Sydney Ports Corporation

Response

The MCA currently operates on ship days and when the Rocks Markets are operating. This practice will continue and it is noted that with the proposed reduction in the provision of car parking within the MCA loading dock it is expected that traffic levels are likely to reduce.

The restriction of access during construction activities on the project would be a significant and unreasonable financial burden on the redevelopment proposal and would provide an unacceptable level of risk to the Principal of the Building Contract as these arrangements are outside of their control and would be subject to change. It is noted that a restriction was not imposed on the recent upgrade of The Rocks NSW Police Building at 132 George Street.

3.5.12 Construction traffic management

Issue

MCA Construction Traffic Management Plan to be submitted for review and endorsement by Sydney Ports prior to commencement of construction. Plan to include the number of construction vehicles and movements, staging location of construction trucks traffic management arrangements for construction vehicles and interface management arrangements b/w the activities of the construction works and the Overseas Passenger Terminal.

Submission/s

Sydney Ports Corporation

Response

The Statement of Commitments includes a commitment to prepare a Construction Management Plan, addressing traffic and parking management, among other things.



A condition of consent could be included requiring preparation of a specific Construction Traffic Management Plan.

3.5.13 Operational traffic management

Traffic Management Plan for operation of the MCA redevelopment submitted for review and endorsement prior to the commencement of operation to ensure impacts on OPT minimised. Plan should include information on access to the site from Argyle Street, number of vehicles accessing the site, parking spaces for the MCA, general traffic and pedestrian management arrangements, management of truck deliveries to minimise impact on Argyle Street access and interface management between MCA and OPT activities.

Submission/s

Sydney Ports Corporation

Response

The following information is provided to respond to this issue:

- » Access is proposed from Argyle Street, in the location of the existing driveway, with an additional driveway to the east for larger service vehicles;
- » Four on site parking spaces are proposed;
- » As parking provision is not proposed to increase (in fact it is proposed to decrease), the museum is likely to generate a similar (or lower) volume of traffic to today;
- » Section 3.5.5 and 3.5.6 deal with deliveries by larger trucks. Smaller trucks will enter and exit the site in a forward direction; and
- » It is not anticipated that the proposed interface management arrangements between the MCA and OPT will operate in a significantly different manner than today.

Given this, it is considered that an Operational Traffic Management Plan is unnecessary.

3.5.14 Construction traffic

Issue

If using cranes or other construction vehicles, which occupy the road reserve - Council and NSW Police approval under S 186 of Law Enforcement Power and Responsibilities Act is required.

Submission/s

RTA

Response

This matter is noted.

3.5.15 Signposting

Issue

All works / regulatory signposting shall be at no cost to RTA.



Submission/s

RTA

Reponses

This matter is noted.

3.5.16 Australian Standards for parking

Issue

Layout of proposed car parking areas should be in accordance with AS 2890.1 - 2004 and AS 2890.2 - 2002 for heavy vehicle usage.

Submission/s

RTA

Response

This matter could be included as a condition of consent.

3.6 ESD initiatives

Issue

The Authority supports the proposal to adopt the Green Building Council of Australia education rating tool for the design development of the project and to incorporate ESD principles into the design and construction of the building.

Submission/s

SHFA

Response

Noted.

3.6.1 Stormwater detention

Issue

Provision for stormwater detention / rainwater tanks should be included.

Proposed rainwater storage tank is not indicated on the drawings. Authority seeks a location and size to determine if any impacts are likely on the archaeological remains directly below this level.

Submission/s

City of Sydney, SHFA

Response

The location of the proposed rainwater tank is shown on Drawing STW-01 in the Statement on Provision of Site Stormwater Drainage (Appendix M of the Environmental Assessment).



The rainwater tank has been located away from the known archaeological remains; however this will be confirmed during the early archaeological investigation works as described in Section 3.4.1.

3.7 Miscellaneous

3.7.1 Standard conditions

Issue

Recommended that standard conditions relating to building, construction management, health waste collection, noise, ongoing management and use, new signage and sustainability be applied.

Submission/s

City of Sydney

Response

Noted.

3.7.2 Commercial building signage

Issue

Building identification and commercial signage does not form part of the current submission. All future signage should comply with The Rocks "Commercial Signage Policy", and should be part of a separate submission.

Submission/s

SHFA

Response

Noted.

3.7.3 Fire booster

Issue

Plans show a proposed freestanding fire booster structure on the George Street level. This will be a disruptive and unattractive element to an appreciation of the streetscape. The Authority would seek to have this structure relocated and integrated into the building architecture.

Submission/s

SHFA

Response

The fire booster is essential in the safety of the occupants of the building. Australian Standards regulations and the Fire Brigade are very prescriptive as to the clear location of such. The current location satisfies these regulations.



The booster enclosure will be detailed the same as the facade of the addition so it reads as one of the facade elements, free standing and on the ground.

We believe it is an appropriate design response to an essential life safety requirement.

3.7.4 Adjustments to Sydney Water infrastructure

Issue

MCA must fund any adjustments required to Sydney Water infrastructure as a result of any development. MCA should engage a Water Servicing Coordinator to obtain a Section 73 Certificate and manage the serving aspects.

Sydney Water will assess the impact of the proposal when an application for a Section 73 Certificate is made.

Submission/s

Sydney Water

Response

Noted

3.8 Support for the proposal

Issue

Support the proposed additions and alterations to the MCA. Obviously a great deal of thought and professional planning has gone into the process. It is time for Sydney to have an outstanding Contemporary Art Museum, which will foster and increase the site as a destination.

Sydney needs this building, it is of an international standard of the highest order and we are lucky and honoured that is a design of one of our local and esteemed architects.

Thrilled that there will be a building that people will feel excited to come in to rather than intimidated. A contemporary building that has wonderful educational facilities and the ability to help to help enrich people's lives in a way that is not possible in the existing building alone. New façade is the perfect foil to the forbidding thirties architecture of the old Maritime Services Building.

I would like to commend the plan to expand the Museum. It is a very good plan and will be a great asset to our city.

Support the alterations to the Museum. Support the proposal for clearer and more direct access and are very supportive of the improvements to educational and viewing facilities. Also support the co-operation between the museum and the Police so that both can function well in a limited space. We value the museums role in this area and are pleased that the alterations are designed to preserve the impact heritage building involved.

Submission/s

Public submissions 2, 4, and 5, Millers Point, Dawes Point, The Rocks Resident Action Group, Harbour Rocks Hotel



Response

These submissions are noted.

3.9 Against the proposal

Issues

The aesthetics of the project are very troubling, the proposed building is almost objectively ugly. I'm strongly in favour of contemporary design for the addition, and if it's an eye-catching one, all the better. But it should be a good one, controversial goes not equal worthy. Please consider and select another design for this project.

The proposed extension will be an eyesore and will spoil natural beauty of Circular Quay. Square shape out of context with the surroundings. Set a precedent for more ugliness within the CBD.

Submission/s

Public Submissions 1 and 3

Response

The design rationale for the proposed redevelopment was outlined in Section 6.1.1 of the Environmental Assessment. The comments expressed in these submissions are based on personal and therefore subjective views.



4. Preferred Project

On the basis of the submissions received and consultation with the Department of Planning and other government agencies, a number of minor amendments have been made to the project. Accordingly, the Environmental Assessment Report and the amendments discussed below comprise the Preferred Project.

Plans of the Preferred Project are included in Appendix B.

George Street elevation

We have re-examined this elevation and modified the height as seen from the street and introduced more detail in the material palette to visually lessen the volume. We have reconsidered the Level 4 gallery and lowered the height of the middle section further revealing the strong central tower of the existing building and acting to further segment and lessen the scale of this elevation from the streetscape.

The northern 'block' of the Level 4 gallery has been reduced in height from RL 22.614 to RL 21.464 (reduction of 1.17 metres) and the central 'block' has been reduced in height from RL 21.662 to RL 21.116 (reduction of 2.429 metres).

Level 6 stair

Approximately three quarters of this stair from above the existing building parapet has been removed.

Level 7 rooftop addition

The detailing of the roof edge has been developed to present a finer more feathered edge to lessen its visual weight and will adjust this edge to more sympathetically meet the existing fabric. The connection to the existing building has been reconsidered in the light of the submissions and the roof at the George Street edge has been reduced from RL 32.877 to RL 32.156 to achieve alignment with the adjacent existing sandstone moulding.

In achieving this alignment, following exhibition of the EA, it was identified that the drawings of the existing building, as prepared by Allen, Jack and Cottier were incorrect as they related to the height of some ornamentation on the central tower of the existing building.

The sandstone moulding to which the Level 7 now aligns was found to be higher than previously measured. This, along with the need to accommodate reconfigured roof structure and air conditioning ducts arising from the feathering of the edge of the roof, has meant that the top RL of the proposed Level 7 roof is now marginally higher than previously proposed.

In this process the floor to the ceiling height within the function spaces has been reduced in an effort to keep the overall roof height down.

In summary, the height of the Level 7 rooftop addition has increased from RL 32.877 to RL 33.331.

A revised Application for Variation of the Sydney Cove Redevelopment Authority (SCRA) Scheme, including a revised amended SCRA Scheme drawing reflecting this minor increase, is included in Appendix C.



Lift

To improve the transparency of the southern lift and its shaft, the concrete blade on the south side of the core has been replaced with glass.

Other modifications

A number of other minor modifications have been made, and are incorporated into the plans in Appendix B.



5. Statement of commitments

This section provides the final statement of commitments for the proposal. New or amended text is indicated by blue text.

Table 1 Final Statement of Commitments

Issue	Commitments
General	The development will be undertaken generally in accordance with the Environmental Assessment dated November 2008 and Preferred Project Report dated March 2009, as prepared by GHD Pty Ltd, and the accompanying architectural plans prepared by Architect Marshall Pty Ltd in association with the Government Architects Office.
Ecologically sustainable development	<p>The MCA is committed to the principles of sustainability as defined in the Environmental Planning and Assessment Act 1979.</p> <p>A range of ESD initiatives will be incorporated into the proposed redevelopment to minimise the impact of the development on the environment. These initiatives are detailed in the Steensen Varming report dated 28 October 2008.</p> <p>The MCA also commits to developing an environmental policy for both the project and general MCA operations, including establishing an environmental management system to implement it.</p>
Requirements of legislation	All necessary approvals, licences, certificates and authorisations will be obtained prior to the commencement of construction, and as appropriate during the construction period.
Building Code of Australia	All building works will be carried out in accordance with the Building Code of Australia as relevant at the time of lodgement of the Construction Certificate.
Construction management	<p>Prior to the issue of a Construction Certificate, a Construction Management Plan will be submitted to and approved by the Certifying Authority. The Plan will address the following, including making recommendations regarding procedures to be adopted to minimise the impacts of construction activities:</p> <ul style="list-style-type: none"> » Construction staging; » Traffic and parking management; » Pedestrian management; » Noise and vibration management; » Tree protection; » Dust control; » Erosion and sediment control; » Waste generation and management; » Hazardous materials management.
Landscaping	Prior to the issue of a Construction Certificate, the proponent will submit a final detailed landscaping plan prepared by a suitably qualified landscape architect. The landscaping plan will be prepared in consultation with SHFA and the City of Sydney, and will be consistent with the public domain principles outlined in the Preferred Project Report.
Construction hours	Hours of construction will be limited to 7am to 6pm Monday to Friday and 7am to 4pm on Saturdays, with no work on Sundays or public holidays.



Issue	Commitments
	Work may be undertaken outside these hours following consultation with SHFA and City of Sydney Council, and for emergency measures.
Contamination	The recommendations of the Preliminary Environmental Site Screening and Waste Classification Assessment (EIS, 2008) will be implemented for capping of the site to provide an effective barrier between the contamination and the site occupants.
Access	The MCA commits to developing an operational plan in conjunction with SHFA to manage access arrangements through the southern doors of the Foundation Hall located on the northern edge of First Fleet Park.
Archaeology	Prior to the issue of a Construction Certificate, the MCA will submit an Interpretation Strategy. This Strategy will be prepared in consultation with SHFA and the City of Sydney, and will incorporate an artistic project/s for the interpretation of the site, and the development and history of the place.



Appendix A
Letter from Design Advisory Panel



Appendix B
Plans of the Preferred Project



Appendix C

Revised application for variation to the Sydney Cove Redevelopment Authority Scheme



GHD

10 Bond Street Sydney NSW 2000



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T: 2 9239 7100 F: 2 9239 7199 E: sydmail@ghd.com.au

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