

LISMORE BASE HOSPITAL
NEW PSYCHIATRIC UNIT PROJECT
PREFERRED PROJECT REPORT
And
STATEMENT OF COMMITMENTS

Prepared for
NSW Health
and
North Coast Area Health Service

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Revision 05

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INTRODUCTION

The Environmental Assessment Report and supporting documents for a Project Application for the redevelopment of Lismore Base Hospital New Psychiatric Unit were exhibited for 30 days from 24 July 2006 to 22 August 2006. During that time submissions were received from Lismore City Council, the Roads Traffic Authority, NSW Department of Planning and two private residents. One private submission fully supporting the development as a positive benefit to the community, the second private submission has raised issues concerning safety and security associated with the redevelopment of the mental health unit. A copy of all the submissions received is attached at Appendix 1.

Representatives of North Coast Area Health Service (NCAHS) and NSW Health have formally met with key representatives of Lismore City Council on 12 September 2006 and key representatives from the RTA northern Regional Office on 20 September 2006 to discuss issues raised in the two organisation's submissions. The outcomes of these discussions are documented in Section 3 -RESPONSE AND OUTCOMES TO ISSUES RAISED IN SUBMISSIONS.

NSW Health has considered all submissions made and has completed revisions to the original design and Statement of Commitments contained within the Environmental Assessment Report, in response to relevant issues raised by the submissions. The revised design and Statement of Commitments are incorporated within this report.

REVISED DESIGN TO ADDRESS ISSUES RAISED IN SUBMISSIONS

Overview

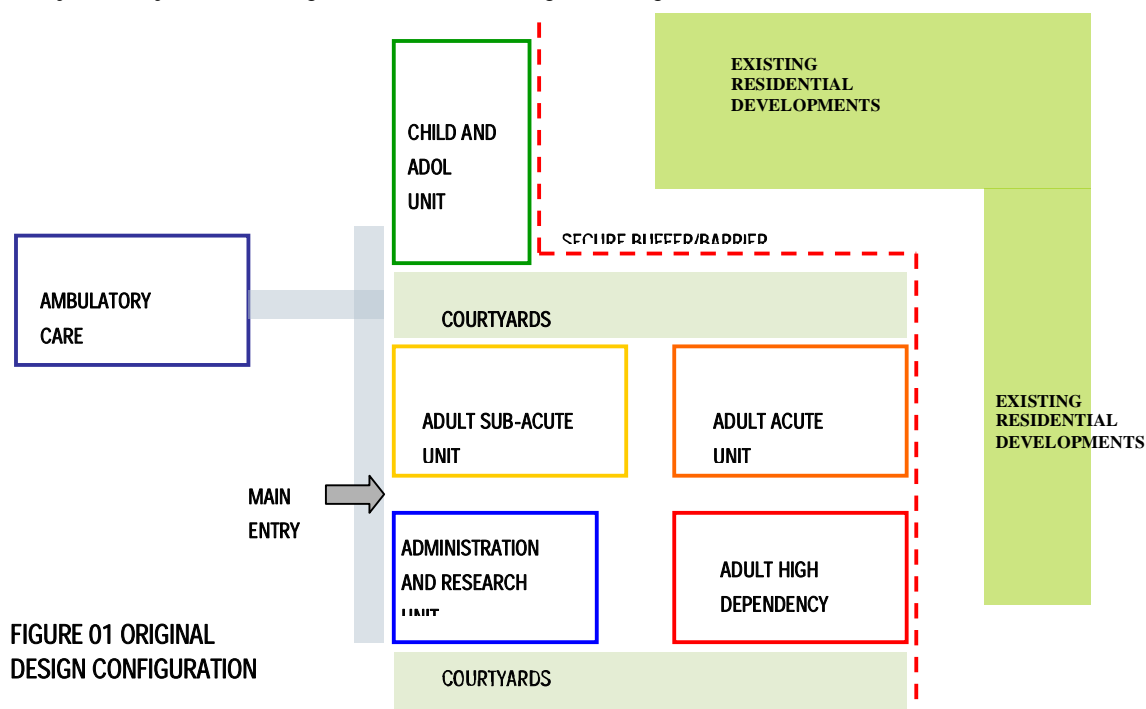
As part of the process of reviewing all submissions, certain issues have arisen that have required a revision to the original design presented in the Environmental Assessment Report. The two primary issues raised which have influenced the design revisions were:

1. Community concerns in relation to safety and security.
2. Issues raised in relation to parking strategies to facilitate staff and short term parking and improve access from parking areas to clinical areas.

In addition some general design revisions have allowed the visual impact to be reduced and improved functionality to be achieved in the redevelopment.

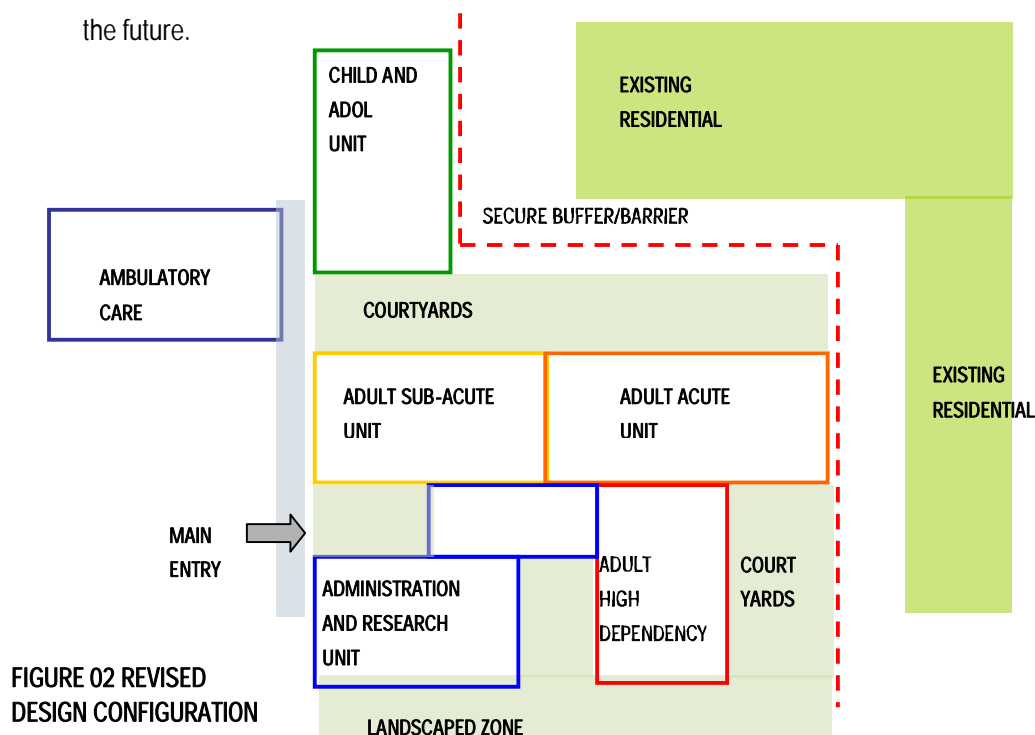
Safety and Security

It should be noted from the outset that the new facility will have one of the most sophisticated systems of security and high level of design to ensure the safety of the patients, staff, visitors and the community. The original design of the Adult Inpatient component of the Stage 1 development planned the Acute and Sub-Acute inpatient facilities to the south of Fermoy Avenue, and located the Adult High Dependency unit directly to the south of the Acute unit. The 8 patients contained in the Adult High Dependency unit are considered to require the most intense clinical intervention this in turn necessitates that these patients receive the highest degree of observation and security. The layout below (Figure 01) shows the original configuration.



In addressing the concerns raised by the community, in relation to safety and security issues, the revised configuration has relocated the 8 bed Adult High Dependency unit further west of the site and away from the existing residential developments, see Figure 02 below. This has established the following benefits:

1. With the Hight Dependency Unit (HDU) relocated further west of the Little Uralba Street boundary, the patients requiring the highest observation, containment and clinical intervention are located furthest away form the residential areas, therefore further improving the facility's security.
2. The revised configuration of the HDU will facilitate better patient transport to and from the main Hospital Complex, again this will facilitate better patient management, further reinforcing security and safety.
3. The revised location of the HDU and the rationalisation of the overall planning of the Adult mental health unit, will establish a smaller foot print for the overall facility and allow additional areas for landscaped courtyards, therefore reducing the visual impact of the redevelopment.
4. The revised planning has also relocated the ambulance and police patient drop-off area for the adult inpatient units from an open secure compound on the corner of Weaver St and Fermoy Ave to within the underground parking facility in a secured facility with direct access to the lift system. This will further improve the function of patient delivery to and from the new unit by police or ambulance.
5. The additional open site area created by the smaller footprint of the facility will support the future master planning of the Hospital site and allow a greater degree of flexibility in developing the psychiatric unit in the future.



Parking Strategies

In addressing the issues raised by the RTA and NSW Planning in relation to parking strategy, the parking configuration has been modified to address the following issues:

1. Provision of parking facilities to enhance the operational needs of the facility where visitors, due to their shorter stay, should occupy closer facilities and longer-term parking should be located further away.
2. Provision of additional parking spaces to reduce the parking impact on surrounding streets.
3. Development of a parking strategy that will allow expansion of on-site parking facilities to support future master planning of the Hospital campus.

The original design of the redevelopment establish 222 new car parking spaces consisting of the following

CAR PARK LOCATION	ACCESS	USE	PARKING SPACES
Main car park lower level RL 16.000	Hunter Street	Public & Staff	72
Main car park under croft Ambulatory Care Bldg RL 16.000	Hunter Street	Public & Staff	26
Main car park under croft Child & Adol Bldg RL 16.000	Hunter Street	Public & Staff	49
Main car park upper level RL 18.500	Hunter Street	Public & Staff	66
Main entry road Child & Adol Bldg	Main entrance Hunter Street	Public & Staff	7
Vehicular Setdown Area RL 19.400	Main entrance Hunter Street	Disabled parking	2
Total			222

The original parking configuration was based on a two level open deck car parking structure on the site adjacent to Hunter Street providing 138 spaces (72 on the lower level and 66 on the upper level) with additional spaces located in under croft areas under the Ambulatory Care and Child and Adolescent Buildings providing a further 75 spaces. The main entry road way provided 7 parking spaces with 2 disabled spaces located off the vehicular set down area. This provided a total of 222 new on-site parking spaces as part of the Stage 1 redevelopment. This inturn provided a total of 406 car parking spaces in total for the Lismore Base Hospital campus at the completion of the Stage 1 redevelopment, it should be noted that based on Lismore City Council's DCP18-Off-Street Parking Code the total number of required parking spaces for the Hospital, at the completion of Stage 1, is 400.

The revised parking strategy has established a basement parking area under the Adult Inpatient Unit providing spaces for 44 cars, this level is also served by the new building lift system facilitating direct and secure access to

the clinical facilities for staff and short term visitors. The provision of the basement parking facilities has allowed the elimination of the previous two level open deck car parking structure on the site adjacent to Hunter Street. This open deck facility has been replaced with an on-grade parking facility with future provision of upper level parking structure to be built to support future master planning of the overall Hospital campus.

The revised parking configuration has established 232 car parking spaces on site in the following on-site parking facilities:

CAR PARK LOCATION	ACCESS	USE	PARKING SPACES
Main car park lower level	Hunter Street	Public & Staff	93
Main car park under croft Ambulatory Care Bldg RL 16.000	Hunter Street	Public & Staff	31
Main car park under croft Child & Adol Bldg RL 16.000	Hunter Street	Public & Staff	46
Main entry road	Main entrance Hunter Street	Public & Staff	19
Vehicular Setdown Area RL 19.400	Main entrance Hunter Street	Disabled parking	4
Basement Parking under Adult Inpatient Unit	Hunter Street		39
Total			232

The reconfiguration of the on-site parking facilities has established the following benefits for the redevelopment:

1. The establishment of a basement parking area under the Adult Inpatient unit for 44 parking spaces with direct lift access to clinical facilities allowing secure and direct access for staff and close parking facilities for short-term visitors.
2. The elimination of the previous two level open deck car parking structure off Hunter Street reducing the impact of the redevelopment on the Hunter Street precinct while allow future options for the site master planning.
3. The increase of the total number of new on-site parking facilities established as part of Stage 1 from the original 222 presented in the Environmental Assessment Report to the current figure of 232. This will establish a total of 416 car parking spaces for the Lismore Base Hospital at the completion of Stage 1.

General Design Revisions

In addition to the above, other design revisions to the original layout, which have enhanced the overall redevelopment, are:

1. The reconfiguration and lowering of the level of the Ambulatory Care Unit footprint allowing this building to be moved away from the Hunter Street boundary and has reduced the visual impact of the redevelopment on the surrounding precinct.
2. The lowering of the floor level of the Child and Adolescent Unit from the original RL19.5 to RL18.5 has reduced the overall height for this building and allowed the establishment of secure courtyard space for the inpatients in lieu of the previous raised secure balcony spaces provided as part of the original design. This will inturn provide better secure external facilities for patients and reduce the impact of this building on surrounding residential developments.

The overall design philosophy of the original layout presented in the Environmental Assessment Report has been maintained and enhanced. The main access points, adjacencies and internal links have been kept reflecting the design intent of the original design.

All buildings are single storey, with the Adult Inpatient Unit over basement car parking. The roofs are in pitched metal deck and the walls are a combination of face brickwork and painted fibre cement cladding. This is keeping with the original design, with minor variations introduced to reduce the impact on the surrounding areas whilst increasing the area of useable courtyard space.

As a result of the changes, all the buildings will appear lower in height and softened by additional landscaping.

Summary of the changes from the original design to the current proposal.

In addressing the submissions raised to the design presented in the Environmental Assessment Report a number of changes have been carried out to the original design, following is a description of the key areas of review.

Building Levels

The Ambulatory Care building and Child and Adolescent Mental Health Unit were previously located at the same level as the Adult Mental Health Inpatient Unit. To reduce the impact of both these buildings and diminish the degree of over viewing from the Child and Adolescent unit to neighbouring properties, the Ambulatory Care building has been lowered by 0.5m whilst the Child and Adolescent Mental Health Unit has been lowered by 1m.

The lowering of the Child and Adolescent unit results in the lowering of the adjacent courtyard and its expansion to the east, thus providing more useable external recreation area for patients of this unit.

The level of the Adult Mental Health Inpatient Unit to the south has not been changed.

Ambulatory Care

The Adult Ambulatory Care building has been reconfigured to reduce its length in the east-west direction and wider in the north south direction. The area and functionality of this unit remains unchanged. The benefit of this change is that the unit is more compact, allowing for possible future expansion to the west in addition to reducing the visual impact of the facility on the surrounding community developments. Alternatively, a future multi-storey car park may be constructed to the west, serving the future master plan stages of the Lismore Base Hospital campus. The shortening of the length of this unit will make the design and construction of such a future car park more efficient and practical. Refer to Drawing No. MHU-PL-L1_OP3 in APPENDIX 2 illustrating the proposed design and overlaying the outline of the previous design footprint.

Child and Adolescent Mental Health Unit northern wing

The original design establishes a setback of the northern bed wing against the west façade of the building. It is proposed to move the northern wing marginally to the west to line up with the balance of the unit. This automatically increases the size of the courtyard to the north east of the unit. This courtyard is also expanded further east as a result of the change to the levels described above. As explained under "Building Levels" above, the enlarged garden is also lower to the street level, making it a better fit for the area. Refer to Drawing No. MHU-PL-L1_OP3 in APPENDIX 2

Adult Mental Health Inpatient Units and Administration and Research Unit

The 4 components of this building are in the same relative positions as presented in the original design. However, after discussions with the clinical groups, we have fully integrated the High Dependency Unit (HDU), back to back with the Acute Unit. This makes it a great deal more compact whilst keeping the area virtually unchanged. A comparison of the original vs new plan shows that the HDU is now set back further from the eastern boundary, allowing for a large east facing patient courtyard which is highly desirable and preferred to the original south facing courtyard. This configuration also increases the distance between the HDU and private residences on Little Uralba Street. The Sub-acute and Acute units have now been modified to be almost identical and mirror imaged. These have been planned with the same "T" shape as before, however the reduced length of the bed wing has been swapped with the living/ dining wing. This allows for a greater depth of courtyard outside the living areas. The above changes will result in just 2 integrated staff stations rather than 3, and facilitate better staff observation and security within the unit. The two staff stations are directly connected to each other via a staff access corridor and remain in direct visual contact. This change will enhance the operation and staffing efficiency of the 3 units.

The office area to the south west has been altered to a minor degree to increase the setback from the southern retaining wall.

Roofs

Generally an attempt has been made to reduce the height of all buildings by lowering the roofs. This, together with the lowering of the Ambulatory Unit and Child and Adolescent Unit will reduce the impact of the new facility on the existing private residents adjacent to the site. The lowering of the roof has been achieved by changing some of the original skillion roofs to gabled roofs. This configures a ridge in the centre with the roof falling to either side.

The roof and façade materials remain the same. The lowering of the roof also has an impact on the height of the external walls facing north and west, reducing the heat gain on the building and improving environmental performance.

The roofs to the inpatient units are very similar to the original design, only adjusted to suit the planning changes described above.

Lift Access

The lift, which originally linked level 1 of the new facility with level 3 of the existing Hospital, has been extended to the new basement level. The lift location has been adjusted to suit this arrangement. This will improve access for visitors to the new facility allowing access to the lift at the basement car park directly linking the adult inpatient and walkways to Ambulatory Care and Child and Adolescent units.. Another benefit of the basement lift access is the facility to transport the patients delivered to the unit by police or ambulance directly to the adult inpatient units via the lift from the new basement secure dock. This facility was previously located on the corner of Weaver Street and Fermoy Avenue in a secure but open dock, this change will further improve the visual impact and security measures of the new facility. The previous location of the police and ambulance bay has been redesigned to incorporate landscaped areas.

The Child and Adolescent unit will require occasional vehicle access to its rear door, but not as a secure dock. Instead a conventional domestic style carport has been provided to the east of the Child and Adolescent Unit.

Roof Plant Rooms

The revised design of the HDU has resulted in additional open area to the south of the facility, against the retaining wall. It is proposed to use this area at ground level for the placement of air conditioning condensers. With this strategy, it has been possible to delete 3 roof level plant rooms and plant platforms. There is now only one roof platform over a section of the Child and Adolescent Unit. This again simplifies the roof profile and

reduces the impact of the new facility. In addition this location for plant will reduce the operational acoustic impact of plant areas.

Car Parking Facilities

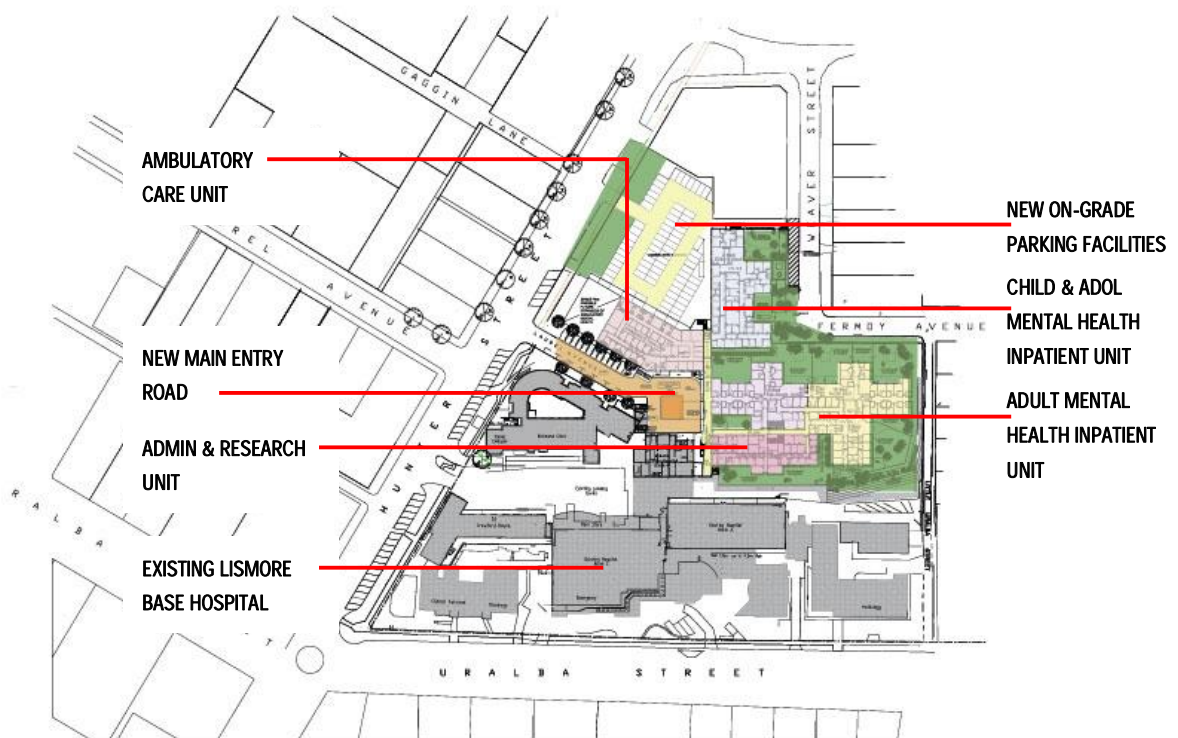
The car parking numbers have been increased from 222 to 232. The additional car parking has been located in a semi-basement area under the Sub-Acute and Acute inpatient units. This semi-basement car parking area has direct access to the lift.

Inpatient Unit Fences

The fences to the north and east of the inpatient units will be anti-climb, based on a steel frame and expanded metal mesh system. This type of detail appears semi-transparent from a distance, reducing the impact of 4m high security walls. In order to maintain reasonable visual privacy, there will be an attached 2m high colorbond fence to the outside of the expanded metal fence. This additional fence will be to the north and east of the adult inpatient units.

With this arrangement, it is possible provide improved viewing from the courtyards above the fence height of 2m while maintaining privacy and security.

Below is the revised facility master plan, it should be noted that the overall site strategy has been maintained which will support and enhance the future master plan stages of Lismore Base Hospital.



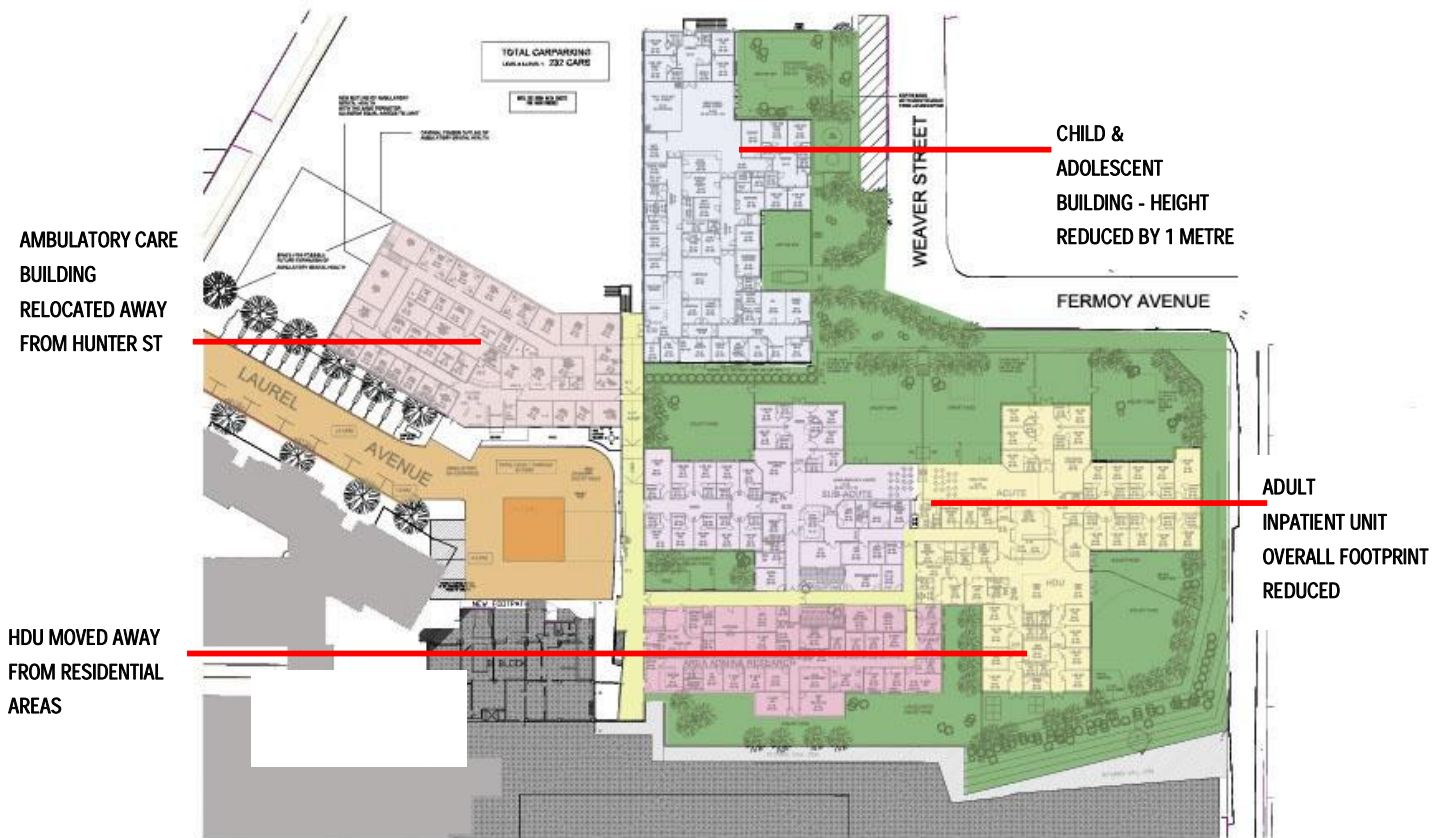


FIGURE 03 STAGE 1 NEW PSYCHIATRIC FACILITY REVISED MASTER PLANNING LEVEL 1

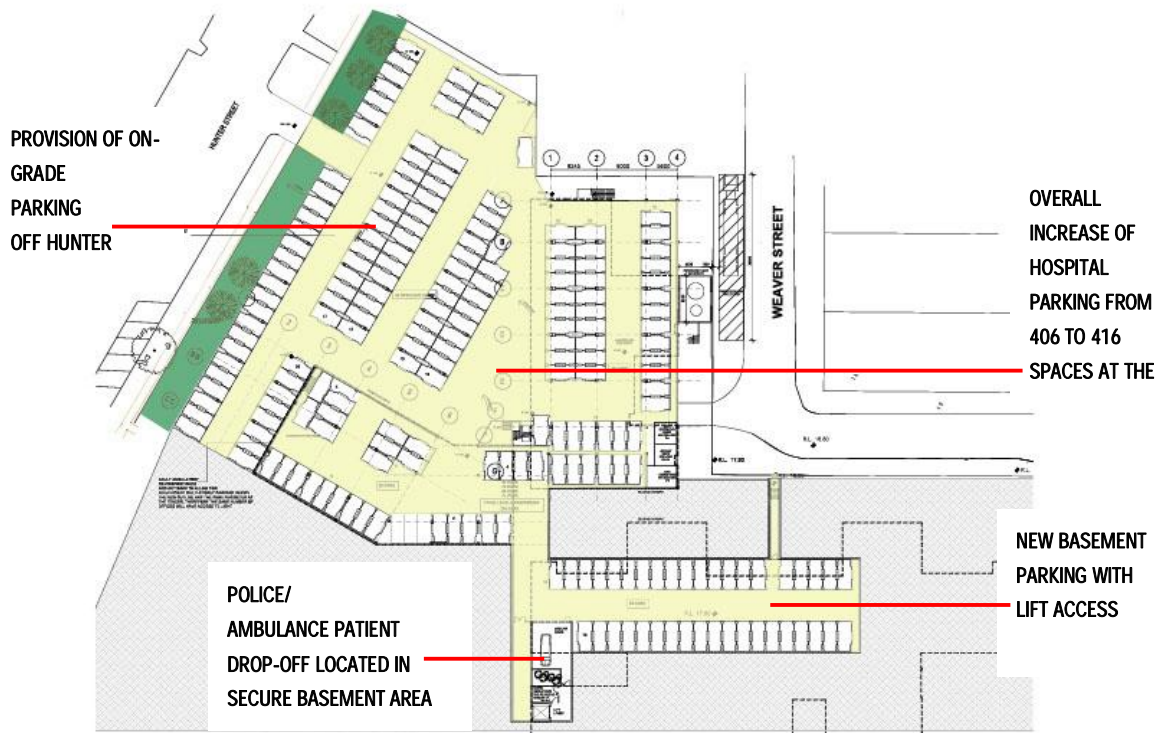


FIGURE 03 STAGE 1 NEW PSYCHIATRIC FACILITY REVISED MASTER PLANNING LEVEL 0

RESPONSE AND OUTCOMES TO ISSUES RAISED IN SUBMISSIONS

Following is a list of all issues raised in the submissions to the Environmental Assessment Report and NSW Health's response and outcomes.

ITEM	ISSUE RAISED	NSW HEALTH RESPONSE & OUTCOMES
	Lismore City Council (LCC)	
01	<p>The relocation of the proposed oxygen tank from Hunter Street to Weaver Street will impact upon the amenity of the residents of the adjoining residential area and the condition of the existing roads. The oxygen facility will be serviced by a heavy articulated vehicle (semi trailer) which in order to service the facility must travel through Fermoy Ave, Weaver St, Bent St and Orion St. These streets have been designed for local traffic, primarily for access to residential dwellings, and are not designed for use by heavy vehicles, particularly in relation to the standard of construction, which may be unsuitable for use by this type of vehicle. The use of residential streets by heavy vehicles may have an impact on regard to safety for the existing residential environment and an increase in traffic noise.</p> <p>Should the bulk oxygen tank be located within Weaver Street then it is considered reasonable that the operational environmental management plan restricts the deliveries to day time periods as specified within the NSW EPA Industrial Noise Policy.</p> <p>In addition, the works proposed for the servicing area for the oxygen tank located within the road reserve of Weaver Street will require approval under section 138 of the Roads Act.</p>	<p>In relation to the location of the Hospital Bulk Oxygen Tank on Weaver Street, NSW Health will seek approval for the proposed service area under Section 138 of the Roads Act. Due to the need for this service area to be accessed by the bulk oxygen tank to refill the tank it will not be possible to limit the times when the tanker can access the service area, this is due to unforeseen emergencies where additional requirements for medical oxygen will require the tank to be refilled more often. It should also be noted that this is function will take approximately 20-30 minutes once every two weeks and poses less of a safety issue than other heavy vehicles, garbage truck and other delivery vehicles, currently utilising Fermoy Ave, Weaver St, Bent St and Orion St.</p> <p>It has been agreed in discussions with LCC that the proposed location of the oxygen tank is acceptable. The following actions have been agreed:</p> <ol style="list-style-type: none"> 1. The applicant shall submit to Council prior to the commencement of works associated with the relocation of the bulk oxygen tank certification from an appropriately qualified person stating that the proposed location and construction of the bulk oxygen tank and storage area comply with appropriate Australian Standards and WorkCover requirements. 2. Deliveries to the proposed gas tank shall be limited to daylight periods (except for emergency deliveries) as specified within the NSW EPA Industrial Noise Policy. 3. The proposed loading area on Weaver Street should be a sealed

		<p>road with kerb and gutter in conjunction with the further development of the site</p> <p>4. Prior to the commencement of works approval shall be obtained under S138 of the Roads Act, for works proposed for the servicing area for the gas tank located within the road reserve of Weaver Street</p>
02	<p>The Environmental Assessment does not address the suitability of the existing road infrastructure to cater for the development. The adjoining roads are only partly sealed, with the kerbside parking areas being gravel only, and open drains in lieu of kerb and gutter. This is not an acceptable road standard for an urban area. The development has proposed to use the unsealed gravel areas for parking. The dust and sediment that will be generated from these areas due to the use by vehicles associated with the development is not an acceptable impact on adjoining residents and the environment. Works should be undertaken to upgrade this infrastructure to a standard that is appropriate for use as on-street parking.</p> <p>None of the above issues appear to have been addressed within the traffic impact assessment provided.</p>	<p>In relation to the suitability of the existing road infrastructure. The proposed redevelopment of Lismore Base Hospital New Psychiatric Unit will reduce the frequency of traffic on Weaver Street, Fermoy Avenue and Little Uralba Street in that all traffic accessing the facility will be utilising Hunter Street. This will improve the current situation, which sees a high degree of vehicles associated with the Hospital utilising the above roadways.</p> <p>The following actions have been agreed with LCC:</p> <ol style="list-style-type: none"> 1. Works should be undertaken to upgrade Hunter Street to a standard that is appropriate for use as on-street parking along the frontage to the Hospital. This will incorporate the sealing of road surfaces adjacent to the Hospital boundary, which is currently unsealed, and provision of kerb and guttering along the same boundary where this does not exist. 2. Signage shall be erected to clearly indicate that off-street parking is available.
03	<p>The future stage proposal shows the part closure of Hunter St. The closure of this section of road would increase traffic volumes in surrounding residential areas due to traffic accessing the hospital car park not being able to travel directly from Uralba into Hunter Street. This is unlikely to be supported by Council given that this closure would significantly increase traffic volumes in surrounding residential streets.</p>	<p>In relation the part closure of Hunter Street, this redevelopment does not propose to close any portion of Hunter Street.</p> <p>It has been confirmed in discussions with LCC that this stage of the redevelopment will not close any portion of Hunter Street.</p>
04	<p>The traffic impact report has identified that Stage 1 of the development will generate in the vicinity of 54 additional vehicle</p>	<p>In relation to additional traffic generation by the proposed redevelopment, the application has identified the need for 54 additional</p>

	<p>movements per hour, peak hour. In preparing its section 94 plan, Council has undertaken detailed analysis of the existing urban traffic network and its ability to handle future traffic expansion generated by expected development. In response to this demand Council has developed a section 94 plan to ensure that the required expansion of the existing traffic network can be funded to accommodate the traffic growth. The plan is based on a user pays scenario where a development pays for the increase in demand generated by the increase in traffic associated with its development. In accordance with Council's Section 94 plan the additional traffic that will be generated by the expansion of the hospital would attract a levy of \$263,570.</p> <p>The Environmental Assessment Report has stated that the extent and magnitude of contributions will be reviewed and assessed by the Department of Health once the development is established. It is Council's concern that should these charges not be levied as part of the approval then they will not be provided. This would result in a short fall in funding for the required works of which Council has no other means of funding. It is council's opinion that it is equitable that these levies be required as part of the approvals process.</p>	<p>vehicle movements per hour in the morning and afternoon commuter peak hours, as stated within the Traffic Report supporting the application this is considered a negligible proportion of the existing traffic volumes in the vicinity of the Hospital.</p> <p>NSW Health and the North Coast Area Health Service redevelopment of the New Psychiatric Facility at Lismore Base Hospital does not place a significant infrastructure burden on the existing services surrounding the Hospital campus.</p> <p>LCC has confirmed that based on a revised review of the traffic impact study that the Section 94 levy would be revised.</p> <p>LCC has confirmed that the Section 94 levy has been revised to \$76,000 - Refer to the revised Statement of Commitments in relation to this issue.</p>
05	<p>Public Transport Infrastructure: the development concept indicates allowance for taxis and private cars to enter, drop off passengers and leave in a forward direction. However there is no indication of how pedestrians are to find their way to the facility from the bus stop in Uralba Street as the final master plan design appears to be impermeable to pedestrians. If pedestrian access through the Hospital is to be denied, it is recommended that the Hospital negotiate with Kirklands Bus Services to move the Hunter St bus stop closed to the pedestrian entry to the Psychiatric Unit.</p>	<p>Noted - discussions will be held with the relevant Bus service provider to establish the optimal outcome for visitors to the new facility utilising public transport.</p> <p>This was confirmed in discussions with LCC.</p>

06	<p>The plans provided show the disabled car parking spaces at the principle public entrance are not to be constructed as part of this stage of the development but will be constructed at a later date. Disabled car parking should be provided with this first stage of development and within a reasonable distance of the principle public entrance.</p>	<p>In relation to the provision of disabled parking space, within the original Environmental Assessment Report the proposed development provided 2 disabled parking spaces at the entry level of the new facility, as part of the revised parking strategy this has been increased to 4 disabled car parking spaces</p>
	Environmental Management	
07	<p>Stormwater Management</p> <p>The Environmental Assessment states '<i>Council approved gross pollutant trap will be provided to the outlet of the on-site detention system of each car parking area</i>'. The EA does not introduce or recognise water sensitive urban design principles. Council's Stormwater Management Plan and Draft DCP - Water Sensitive Urban Design provides the following water quality targets that should be addressed through appropriate contemporary WSUD design for the proposed development.</p>	<p>In relation stormwater management the NSW Health are committed to water sensitive urban design principles and will incorporate design philosophy to ensure water quality targets are addressed.</p> <p>This was confirmed in discussions with LCC.</p> <p>The following actions have been agreed with LCC:</p> <ol style="list-style-type: none"> 3. Hard surface areas, landscaped areas, roof water and subsoil drainage systems shall be designed so that all water is directed to a Council approved drainage system to prevent an intensification of discharge runoff onto adjoining land. This system shall be constructed in accordance with Council's Development, Design & Construction Manuals.
08	<p>Containment Land Management</p> <p>The EA recommends '<i>it is prudent to remove the under ground storage tank (UST) or assess its integrity and associated pipework and establish a monitoring program which could provide an early warning of any leakage</i>'. It is considered that if the UST has not further use on site then the system should be remediate in accordance with the Department of Environment and Conservation Guideline - <i>Contaminated Sites: Guideline for Assessing Service Stations Sites</i>. If the UST is to continue to be utilised then assessment of the system integrity and installation of a monitoring system would be supported.</p>	<p>In relation to the UST, the tank is currently being utilised as the primary supply for the Hospitals Emergency Backup Generator, in addition the Hospital has in place operational protocols in relation to the monitoring of the integrity of the tank.</p> <p>This was confirmed in discussions with LCC.</p>

09	<p>Noise Management</p> <p>The EA nominates for the engagement of acoustic consultants to advise on appropriate acoustic treatments for the building and plant. It is recommended that at the completion of the project the applicant engage an acoustic consultant to provide a final report that the noise emissions from the site satisfy the projects noise performance targets and does not generate offensive noise to adjoining sensitive receivers.</p>	<p>In relation to the noise management, the facility has been designed to exceed all requirements for building acoustics for a healthcare facility; the contractor will be required to certify that the construction of the facility meets these acoustic design requirements.</p> <p>This was confirmed in discussions with LCC.</p>
10	<p>Food Safety</p> <p>The regulation of food safety matters of a hospital development would fall to the responsibility of the NSW Food Authority and therefore it is recommended that the Department of Planning seeks comment from the NSW Food Authority prior to the determination of the application.</p>	Noted
11	<p>External Lighting</p> <p>It is recommended that external and security lighting associated with the development satisfy the requirements of AS 4282 - Control of Obtrusive Effects of Outdoor Lighting.</p>	In relation to external lighting, the facilities external general and security lighting will conform to AS 4282 - Control of Obtrusive Effects of Outdoor Lighting.
12	It is not clear which authority will be responsible for the day to day regulatory management of the site during and post construction. This is important when considering the sites environmental performance against the targets of the Environmental Management Plan.	Noted - In discussions with LCC it was agreed that a point of contact be established during and post construction in relation to the regulatory management of the site. This point of contact will be the NCAHS
13	<p>Trees</p> <p>Landscaping plans indicate replacement of street trees in Hunter Street, beyond hospital property boundaries. No such replacement should occur without consultations with Council regarding suitable species and avoidance of underground services.</p>	<p>In relation to the replacement of street trees on Hunter Street beyond the Hospital boundary, the applicant will consult with Lismore City Council to establish suitable species and impact on in ground services.</p> <p>This was confirmed in discussions with LCC.</p>
14	A previous consent issued by Council for preparatory earthworks required the removal of a mature hoop pine. This proved to be controversial and as a result a replacement mature hoop pine should be planted in a local public	Noted. NSW Health has met all conditions related to the previous Development Application. All landscaping works related to the current redevelopment of the New Psychiatric Unit at Lismore Base Hospital will

	<p>amendment or modification outlined in these conditions.</p> <p>and be substantially in accordance with the plans 100D; MPOO1a A; A102 C; A103B A104 B; A105C; A200C; A201C; A202C; I220B; L001A; L002A; L003A and documents submitted with the Environmental Assessment.</p> <p><i>Reason: To correctly describe what has been approved (EPA Act Sec 79C).</i></p>	<p>Noted. As the original design has been revised, the facility will be built in accordance with the approved plans, note that plan numbers have been modified.</p> <p>It was agreed in discussions with LCC that their submission would be revised and re-issued to NSW Planning based on agreed outcomes with NCAHS and NSW Health.</p>
17	<p>Lot Consolidation</p> <p>All separate parcels of land shall be consolidated into one allotment under one title and lodged with the Registrar General prior to commencement of works.</p> <p><i>Reason: the property should become one consolidated site to prevent future dealing in separately titled land.</i></p>	<p>Noted and Agreed</p> <p>This was confirmed in discussions with LCC.</p>
18	<p>Road Works</p> <p>3. Prior to commencement of work the contractor shall lodge a bond with Council to cover any damage to infrastructure within the road reserve arising during the construction of the proposed development. Evidence is to be provided to Council indicating the pre development condition of the surrounding public infrastructure. Any damage arising from construction activities to roads, kerb and gutter, footpaths or any other infrastructure within the road reserve shall be reinstated in accordance with Council's Development, Design and Construction Manuals (as amended) to Council's satisfaction.</p> <p><i>Reason: To ensure an adequate road network in accordance with adopted standards. (EPA Act Sec 79C(a)).</i></p> <p>4. Prior to commencement of work a Construction Traffic Management Plan shall be submitted to, and approved by, Lismore City</p>	<p>Noted and agreed. In discussions with LCC, Council will establish the sum of the bond and forward this to NCAHS & NSW Health.</p> <p>Note and Agreed. The Project Contractor will provide a Construction Traffic Management Plan as part of the overall Construction Management Plan</p>

	<p>council. Such plan shall include: details of haulage routes to be utilised for delivery and removal of material from the site and traffic control plans prepared by an RTA accredited person for any works that impact upon the movement of vehicles or pedestrians upon the public road.</p> <p><i>Reason: To ensure activities relating to the development do not interfere with the movement of traffic along the public road. (EPA Act Sec 79C(b)).</i></p> <p>5. Prior to the commencement of works approval shall be obtained under S138 of the Roads Act, for works proposed for the construction of on street parking located within the road reserve of Hunter Street.</p> <p><i>Reason: to comply with the Roads Act 1993</i></p>	<p>Noted and agreed.</p>
<p>19</p>	<p>Oxygen Tank</p> <p>6. The applicant shall submit to Council prior to the commencement of works associated with the relocation of the bulk oxygen tank certification from an appropriately qualified person stating that the proposed location and construction of the bulk oxygen tank and storage area comply with appropriate Australian Standards and WorkCover requirements.</p> <p><i>Reason: to protect human health and the environment. (EPA Act Sec 79C(b)).</i></p> <p>7. Deliveries to the proposed gas tank shall be limited to daylight periods as specified within the NSW EPA Industrial Noise Policy. Outside of these hours the area shall be available for on-street car parking. Appropriate regularity signage identifying the above</p>	<p>Noted and agreed.</p> <p>This matter has been dealt with in Item 01. It has been discussed and agreed with LCC that the refilling area will not be made available for public parking as there will be requirements for the tanker to refill the oxygen tank outside of scheduled times, due to medical emergencies at</p>

	<p>requirements shall be placed prior to commencement of use.</p> <p><i>Note: The proposed loading area on Weaver Street should be a sealed road with kerb and gutter in conjunction with the further development of the site.</i></p> <p>8. Prior to the commencement of works approval shall be obtained under S138 of the Roads Act, for works proposed for the servicing area for the gas tank located within the road reserve of Weaver Street.</p> <p><i>Reason: to comply with the Roads Act 1993</i></p>	<p>the Hospital.</p> <p>Noted and agreed.</p>
20	<p>Parking</p> <p>9. Works should be undertaken to upgrade Hunter Street to a standard that is appropriate for use as on-street parking along the frontage to the Hospital.</p> <p><i>Reason: to preserve the amenity of the area, particularly in regard to preventing dust nuisance (EPA Act S79Cb)</i></p> <p>10. Provision shall be made for at least 400 car parking spaces with a bitumen sealed/paved or equivalent surface constructed and landscaped in accordance with the requirements of council's Development Control Plans for Off Street Car Parking requirements and Crime Prevention Through Environmental Design, Australian Standard AS2890.1 Parking Facilities Off Street Parking and Council's Development, Design and Construction Manuals.</p> <p><i>Reason: to provide adequate off street parking space for the anticipated traffic that will be generated by the development (EPA Act S79C(a)).</i></p>	<p>Noted and agreed</p> <p>Noted. The redevelopment will provide parking facilities that will meet Councils DCP for off street parking it should be noted that within the Environmental Assessment Report the redevelopment provided 406 parking spaces which is in excess to Lismore City Council's DCP 18 for Off-Street Parking for the hospital. The revised parking strategy has increased the original number of parking spaces to 416.</p>

	<p>11. Signage shall be erected to clearly indicate that off street parking is available.</p> <p><i>Reason: to ensure the free flow of traffic and comply with traffic regulations (EPA act S79C(c)).</i></p>	Noted and agreed
21	<p>Trade Waste</p> <p>17. An application to discharge trade waste, to include a plan containing all details of the propose trade waste installation, shall be submitted to Lismore City Council. The trade waste application must be approved prior to occupation of the building. Designs are to be in accordance with Australian Standard 3500, the NSW Code of Practice – Plumbing and Drainage.</p>	Noted and agreed

ITEM	ISSUE RAISED	NSW HEALTH RESPONSE
	Department of Planning North Coast Regional Office	
01	Interim Car Parking, the proposed psychiatric unit is to replace a car parking area linked to the existing Lismore Based Hospital (LBH) (see map on page 12 of the Environmental Assessment). The Traffic Study at Appendix 5 prepared by Traffic, Environmental and Forensic Engineers (TEF) Consulting and dated 26 June 2006 concluded that the existing LBH parking demand is greater than the 230 formal and semi-formal off-street parking spaces at LBH. This creates a parking demand overflow that is exerted onto surrounding streets (paragraph 2.2 Existing parking provision and demands, page 3). However, the applicant does not appear to provide on site interim car parking provisions while the Unit is under construction. While it is acknowledged that the applicant has addressed the issue of car parking for the completed project, concerns are raised in relation to the level of car parking available to patients and staff during the construction period;	<p>Currently the North Coast Area Health Service (NCAHS) has as part of the staging process for the New Psychiatric Unit Redevelopment moved a number of vehicle parked on the LBH site off-site to other facilities this included 50 NCAHS vehicles and 30 staff vehicles a total of 80 . In addition, as part of the staged works for the mental Health redevelopment NCAHS has currently maintained 70% of the original vehicle parking area as well as made vehicle parking for an additional 68 vehicles.</p> <p>This new parking combined with the removal of 80 NCAHS official and employee cars provides Lismore Base Hospital a total of 148 extra vehicle parks over the vehicle parking available prior to the development. This number is considered more than adequate as an interim measure during the construction phase of the project.</p>
02	Heritage, the environmental assessment estimates the existing buildings of the LBH buildings to have been constructed between the 1950's and 1980's and have minimal architectural or heritage value. However, as there are buildings of the Federation period (e.g. Armstrong House, "Kiaora") and Cultural Landscape Items (e.g. Canary Island Palms, grounds of LBH, Uralba Street between Hunter and Diadem Street) see attachments, in the vicinity and this proposal forms part of staged redevelopment of LBH there should be some consideration of these and any other relevant architectural/ landscape features.	Noted - The cited heritage items are located on Uralba St will not have a direct bearing on this project. As subsequent stages of the overall master plan are developed, these stages will address relevant heritage impacts.
03	In addition, the applicant would need to determine, before construction work begins, the likelihood of significant archaeological features being present on the site.	Enabling works have been carried under development applications DA05/689 and DA06/239 approved by Lismore City Council. To date no significant archaeological features have been encountered on the site. If in the future items of archaeological significance are discovered as part of the construction works the relevant agencies will be contact to provide direction.

ITEM	ISSUE RAISED	NSW HEALTH RESPONSE & OUTCOMES
	Road & Traffic Authority Northern Region Office (RTA)	
01	The proposed development has not undertaken a proper traffic impact study to assess its impact on the operation of the surrounding road network and infrastructure.	A traffic study has been undertaken by a qualified traffic consultant, the result of which has been incorporated into the design of the new facility. The Traffic Study was issued to the RTA for review. The RTA has confirmed that the traffic study adequately assesses the impact of traffic generated by the development. and therefore the RTA supports the redevelopment application
02	Any access to a public road should be designed to maintain safety and efficiency. It should be designed in accordance with AUSTRROADS guidelines and Australian Standards 280 to safely manage turning traffic in and out of the development.	Noted and agreed
03	In regional centres the parking demand is greater than the metropolitan areas due to the lack of an adequate public transport network to service its total catchment.	Noted. The hospital is serviced by public transport
04	Current Parking problems in the area are exacerbated by the establishment of other adjacent ancillary services that do not provide adequate off-street car parking.	NSW Health should not be encumbered by the lack of parking provided by private individuals surrounding the Hospital site. LBH has over the last two years leased a number of properties around the Hospital and has provided on-site vehicle parking for all facilities through the development consent process.
05	Lismore Base Hospital has had a history of unresolved traffic management and parking problems.	Noted. This project has addressed the needs for parking and traffic management in relation to the impact it will have on the overall Hospital Campus. The proposed redevelopment will provide more car parking spaces than is required by the current DCP.
06	The Committee considers the DCP parking rates inadequate for the development. 1 space/ 5 beds is inadequate for visitor parking. Staff parking does not cater for the additional need created with the	Noted - The parking requirements have been based on Lismore City Councils current DCP for off street parking.

	overlap of shifts.	
07	A parking strategy should reflect the operational needs where visitors, due to their shorter stay, should occupy closer facilities and longer-term parking should be located further away.	Noted, this strategy has been applied to the redevelopment and this was discussed with key representatives of the RTA. RTA has confirmed their concurrence on this matter. The revised planning further enhances the new facilities ability to address this requirement.
08	The lack of adequate off-street parking impacts on the adjoining residents and their visitors.	Noted - This project has addressed the needs for parking and traffic management in relation to the impact it will have on the overall Hospital Campus. The proposed redevelopment will provide more car parking spaces than is required by the current DCP. This matter was discussed with key representatives of the RTA. RTA have confirmed their concurrence on this matter.
09	It is proposed to create an on-street servicing facility in Weaver Street. This is not supported by the Committee for safety reasons. It should be located off-street in a proper service area.	The facility is for the refilling of the bulk oxygen tank supplying the Hospital medical oxygen requirements. This function will occupy a period of 20-30 minutes once every two weeks. This matter was discussed with key representatives of the RTA. The RTA have confirmed that they will support LCC requirements for this facility and will not raise objections to the proposed location of the bulk oxygen tank or refilling lay-by.

ITEM	ISSUE RAISED	NSW HEALTH RESPONSE
	Submission by Private Residents (The identity of both submissions have been withheld)	
01	<p>... Subjected to unsettling noises at anytime of the day or night and possibly even unnerving sights due to the relocation and expansion of the psychiatric unit. My family is worried that many disturbed individuals will be housed close to our house whilst they undergo treatment. We are worried about escapees committing dangerous offences upon the surrounding residents. We don't want to have to witness any more anti social behaviour than what we already witness due to our proximity to the hospital because of increased traffic and an increase in the immediate population of disturbed individuals.</p> <p>I think that our increased fears for our own safety and peace of mind would be mimicked by potential buyers of our house and would therefore reduce the land values of the immediate surrounding houses. In short the proposed relocation and expanding development of the psychiatric unit would forever change the peaceful family amenities of the location for the surrounding community.</p> <p>If the development were to go ahead I would seek compensation for the reduction in living standards that I believe the proposal would force upon my family and our neighbourhood.</p>	<p>In response to a private submission outlining issues related to the safety and security on surrounding residents by the redevelopment of the Mental Health Unit, NSW Health is committed that the facility is designed to ensure the safety of all patients, staff, visitors and surrounding residents. This has been achieved through the following principles:</p> <ol style="list-style-type: none"> 1. In response to the issues raised concerning safety and security, the design of the new facility has been revised to locate the High Dependency unit away from the residential zone of Weaver St and Fermoy Avenue. This High Dependency unit has been relocated further west into the site. Refer to the revised drawings in Appendix 2 2. The new facility will have one of the most sophisticated systems of security to ensure both the safety of the clients and the general community. This will include closed circuit television cameras to the interior, exterior and perimeter of the new buildings; in addition movement sensors within the building will alert staff of any client movement that is unauthorised. 3. The physical design of the facility has been focused on ensuring that patients are not able to leave the building if they are not authorised to do so. All perimeter walls to the courtyards are design to be secure but unobtrusive. 4. Police and ambulance patient delivery areas for adult acute mental health have been relocated from an open secure area on the corner of Weaver St and Fermoy Ave to an enclosed and much more secure area within the new basement car park area with direct access to a lift facilitating improved and secure patient transfer.. 5. The Child and Adolescent building to Weaver Street has only eight patients and the majority of patient activity areas face away from Weaver

		<p>Street.</p> <ol style="list-style-type: none">6. The courtyards to the Child and Adolescent building on Weaver Street have been lowered in height along with the building, thus reducing the impact and over viewing on neighbouring residents.7. Glazing to the new buildings will be toughened glass ensuring further safety8. It should be noted that these buildings have been designed with the latest design philosophy in regards to security and will be replacing a 25 year old building that does not meet current security standards. The existing mental health facility is located approximately 20 metres to the south of the new facility.
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ITEM	ISSUE RAISED	NSW HEALTH RESPONSE
	Submission by Private Residents (cont') (The identity of both submissions have been withheld)	
02	<p>... I am writing in support of the proposed development.</p> <p>Rationale:</p> <p>Having held a position within NSW Department of Health C.M.H (based at SSWAHS and Sydney University) over the past five years managing patients with Eating disorders, I have long been aware of the inappropriate facilities for adolescents and young women requiring in-patient Mental Health in the Northern Rivers Area.</p> <p>The proposal would seem to provide an opportunity for NRAHS patients with eating disorders and other serious mental illnesses to be treated locally rather than transferred to Sydney away from family and friends for lengthy periods to receive treatment.</p> <p>I thank you for the opportunity to support the proposal.</p> <p>As I am currently on maternity leave I would prefer my name and address are not made public however I am happy for my comments to be made public.</p>	Noted

ITEM	ISSUE RAISED	NSW HEALTH RESPONSE
	NSW Department of Planning Sydney Head Office	
	The following issues were received from NSW Department of Planning in the form of an email from the reviewing officer, Mr Nicholas Hall on 8 September 2006	
	<p>Section 94 Contributions</p> <p>As I set out in my previous email, Circular D6 solely relates to Crown Projects that are determined under Part 5A of the Act and does not apply to project applications determined under Part 3A of the Act. Consequently, none of the exemptions set out within the "matrix of appropriate categories for contributions towards off site works for crown developments..." are applicable to the current (or future) project application(s) if assessed and determined by the Minister under Part 3A.</p> <p>There are three options to consider at this stage:</p> <p>1) Pay the Section 64 and Section 94 contributions in accordance with Council's Contributions DCP's - the figure could be subject to negotiation between NSW Health, NCAHS and Lismore City Council (who, incidentally, require further information to accurately calculate the Section 94 contribution so it may be subject to change).</p> <p>2) Request that the Minister establish and apply an alternative financial contributions framework - this can be achieved through use of Section 94B(2) of the Act but may delay the Department's assessment whilst an independent consultant is engaged by the Department to review the existing contributions and determine whether an alternative framework should be applied.</p> <p>3) Volunteer to enter into a planning agreement with Council (and other</p>	<p>NSW Health has held discussions with LCC in relation to the extent of the Section 64 and Section 94 contributions. LCC has revised the extent of both contributions- refer to previous sections of this report dealing with this matter and to the revised Statement of Commitments.</p>

<p>agencies if appropriate) as part of the preferred project report - this may be an option if you are seeking to reduce the financial contribution through provision of an alternative form of contribution such as works in kind.</p> <p>The three options could be implemented through the imposition of approval conditions but all would require resolution prior to the issuing of any approval and would be tied to issuance of the construction certificate or commencement of works.</p> <p>Options 1 and 3 require concurrence with Council. If this cannot be reached the Department will consider the issue in detail, in consultation with both parties and establish what it considers to be a reasonable financial contribution. Appropriate conditions of approval will then be imposed accordingly.</p>	
<p>Car Parking</p> <p>Car parking demand is extremely high for LBH primarily given that it is operating as a regional health facility devoid of an effective public transport system as it is located outside existing metropolitan areas. Ancillary health services located in Uralba Street, Weaver Street and Laurel Avenue further exacerbate demand. The Department, in consultation with the RTA, is of the view that Lismore City Council's car parking rates are inadequate for the development and a higher car-parking rate may need to be applied to cater for the hospital redevelopment.</p> <p>The mix of formal and informal car parking available for the site is sporadic and problematic relying heavily upon informal on-street car parking (which occurs in some areas without line marking, proper kerbs and guttering) to the detriment of existing residential amenity. It is the Department's view that the proposed redevelopment of LBH provides an opportunity to rectify this problem. Provision of well serviced off-street car</p>	<p>Noted. This project has addressed the needs for parking and traffic management in relation to the impact it will have on the overall Hospital Campus and surrounding infrastructure. The proposed redevelopment will provide more car parking spaces than is required by the current DCP. Discussions on this matter have been held with LCC and the RTA and resolution has been achieved. Refer to previous sections of this report.</p> <p>Noted. In discussions with LCC it is agreed that the on-street parking facilities will be provided on sealed areas with appropriate kerb and guttering. In response to the parking issues raised a revised parking strategy has been developed to facilitate parking facilities closer to inpatient areas facilitating staff and short term parking needs in addition to disabled parking facilities.</p>

<p>parking should be maximised whilst on-street car parking should be limited as far as possible.</p> <p>Detailed plans and documentation should be submitted showing a detailed off-street car parking layout including appropriate line markings and delineation of staff, visitor and disabled car parking (both above and below ground level). All car parking provision should be in accordance with relevant standards and guidance (e.g. AS2890.1 and Council policies). Detailed plans (including sections and elevations) should be lodged to show car parking development above ground level where appropriate.</p> <p>A car parking strategy should be developed which (amongst other things) reflects the operational needs of the hospital where short stay visitors occupy closer facilities with longer-term users being located further away.</p> <p>An appropriate level of disabled car parking must be provided at this first stage of development within a reasonable distance of the principal public entrance to the mental health unit. Construction of this cannot be deferred until a later date.</p>	<p>Refer to revised plans in Appendix 2 for details of parking strategy. The location of staff parking areas is currently being developed by the Hospital as part of the revised operational policies for the new facility. This will be forwarded when completed.</p> <p>A revised parking strategy has been developed top address this issue, refer to previous sections of this report for details</p> <p>The number of disabled car parking spaces have been increased from 2 to 4 located adjacent to the main entry facilities of the redevelopment.</p>
<p>Transport and Access</p> <p>The RTA is of the view that the Traffic Impact Study is inadequate to assess the effect of the proposal on the operation on the surrounding road network and infrastructure. This will need to be resolved with the RTA before the Department can finalise its assessment and make a recommendation to the Minister.</p> <p>Further detailed plans and documentation should be provided which better articulates how the three storey pedestrian link from the existing hospital building (Block A) into the new health facility will function.</p>	<p>A traffic study has been undertaken by a qualified traffic consultant, the result of which has been incorporated into the design of the new facility and formed part of the EA issued to NSW Planning.</p> <p>The Traffic Study was issued to the RTA for review. The RTA has confirmed verbally that traffic study adequately assesses the impact of traffic generated by the development. and therefore the RTA supports the redevelopment application</p> <p>Refer to plans in Appendix 4 in relation to the three storey link to Block A Drawing A501.</p>

	<p>Construction Management</p> <p>The proponent should make clear who is to be responsible for the day to day regulatory management of the site during and post construction and make a commitment to providing an appropriate point of contact.</p>	<p>Noted - In discussions with LCC it was agreed that a point of contact be established during and post construction in relation to the regulatory management of the site. This point of contact will be the NCAHS</p>
	<p>Relocation of the Oxygen Tank</p> <p>Development consent was granted by Lismore City Council earlier this year for relocation of the oxygen tank to an area in close proximity to the hospital and ancillary health facilities (Development Application 2006/239). No justification has been provided to demonstrate why it needs to be further relocated from its consented position and there is a concern that the new location of the oxygen tank to Weaver Street may be problematic as it is understood that the oxygen tank is to be serviced by a large articulated vehicle.</p> <p>Weaver Street, Fermoy Avenue and Bent Street are narrow residential streets and it is unclear whether they can accommodate an articulated vehicle. What route will the vehicle use? Can an articulated vehicle achieve the necessary turning circle (sweeping path) around these streets? What will be the effect on the condition of existing roads? What will be the effect on existing residential amenity? Can the road reserve on Weaver Street accommodate the service area (note: this will also requires approval under Section 138 of the Roads Act)?</p>	<p>The matter of the location and servicing of the bulk oxygen tank and the proposed route of the tanker has been discussed with both the RTA and LCC. Both Authorities have agreed in principle to the proposal. The relocation from Hunter St was based on issues related to infrastructure services in Hunter St not supporting the proposed facility</p> <p>Refer to previous sections of this report for details of this issue.</p>
	<p>Landscaping</p> <p>One minor point. Plan 7455 L003 refers to "Landscaping Courtyard Detailed Plans (1 of 2)". Is there a "2 of 2" or is this a typo?</p>	<p>Drawing 2 of 2 is further detail of the courtyard landscaping, a copy has been included in the report for information. Refer to Appendix 4</p>

PREFERRED PROJECT REPORT

On the basis of the submissions made and discussions held with Lismore City Council, RTA Northern Region Office and NSW Planning, revisions to the design have been made to the project as exhibited to address safety and security and parking functionality. It should be noted that in principal the revised design meets the requirements set out in the Environmental Assessment Report, and that key issues related to the following items have been maintained or enhanced in the revised design:

1. Facility - The revised design incorporates the full extent of Clinical, Public Amenity, Support and Allied Health services currently within the original design.
2. Bulk, Setback and Overshadowing - Due to its smaller footprint the revised design achieves improved setbacks from the site boundaries and therefore reduces the impact on the site and surrounding environs in relation to bulk and overshadowing.
3. Traffic Impact - The revised design utilises the same traffic patterns as set out in the original design in relation to street entries, but improves functionality in that parking is located closer the primary population of the facility.
4. Parking - The layout increases the onsite parking numbers originally planned for the Stage 1 development of the project and therefore exceeds the requirements of Lismore City Councils off-street parking DCP. The parking distribution has been revised to better facilitate staff, short term and disabled parking facilities. In addition the revised parking layout will reduce the visual impact on Hunter St as a substantial number of parking bays have been placed in a new basement level under the main Adult Inpatient Unit, this with the provision of a lift service will assist secure parking for staff and after hours visitors.
5. Infrastructure Impact - The revised design will have the same impact in relation to the effect on public infrastructure as does the reference design.
6. Environmental Impact - revised design will reduce the requirement for retaining structures as part of the redevelopment.
7. Community Impact - With the relocation of the High Dependency unit away from existing residential development on Weaver St and Fermoy Avenue the issue of safety raised by the private submission have been supported.
8. Future Development - The revised design will further support future master planning stages for the Hospital through development of better parking strategies and provision of site area for future development.

In addition to deal with the issues raised, NSW Health has revised its Statement of Commitments to take into account all issues which were considered reasonable and which minimised the environmental impact of the proposal.

Note - The revised Statement of Commitments following, in Section 5 show amendments in *bold italics*.

STATEMENT OF COMMITMENTS

General

- 1) The development will be undertaken generally in accordance with the Environmental Assessment Report dated June 2006 prepared by HASSELL Sydney (including accompanying Appendices) and revised drawings.
- 2) The proponent is committed to the principles of sustainability as defined in the Environmental Planning and Assessment Act 1979. The construction and operation of the hospital will be undertaken in accordance with Premier's Memorandum No.2003-2 *High Environmental Performance for Buildings* and the requirements of the *Environmental Performance Guide for Buildings* (EPGB).
- 3) The proponent will obtain all necessary approvals required by State and Commonwealth legislation while undertaking the project.
- 4) The proponent will continue to liaise with the local community during the development process.
- 5) The proponent will continue to liaise with Lismore City Council during the development process.
- 6) The proponent will develop an environmental management plan, which identifies and demonstrates a commitment to positive environmental management for the project.
- 7) *The buildings will be set out by a registered surveyor to verify the correct position of each structure in relation to property boundaries and the approved alignment levels. The registered surveyor will provide evidence to Council that structural works are in accordance with the approved project application.*
- 8) *Site notices will be prominently displayed at the boundaries of the site for the purposes of informing the public of project details including, the details of the builder, proponent and structural engineer. The notices will satisfy the following requirements:*
 - a. *Minimum dimensions of the notice will measure 841mm x 594mm (A1) with any text on the notice to be a minimum of 30 point type size;*
 - b. *The notice will be durable and weatherproof and will be displayed throughout the works period;*
 - c. *The approved hours of work, the name of the site/project manager, the responsible managing company, its address and 24 hour contact phone number for any inquiries, including construction/noise complaint are to be displayed on the site notice; and*
 - d. *The notices will be mounted at eye level on the perimeter hoardings/fencing and will state that unauthorised entry to the site is not permitted.*
- 9) *A separate application will be made to Council for approval under Section 68 of the Local Government Act, 1993, for the erection of hoardings or scaffolding in a public place.*
- 10) *All necessary permits will be obtained for the use of mobile cranes on or surrounding the site, prior to the commencement of works.*
- 11) *The Roads and Traffic Authority's Traffic Management Centre (TMC) will be notified of the truck route(s) to be followed by trucks transporting waste material from the site, prior to the commencement of the removal of any waste material from the site.*
- 12) *Should any Aboriginal relics be unexpectedly discovered then all excavations or disturbance to the area will stop immediately and the National Parks and Wildlife Service shall be informed in accordance with Section 91 of the National Parks and Wildlife Act, 1974.*

- 13) *Should any historical relics be unexpectedly discovered then all excavations or disturbance to the area will stop immediately and the Heritage Council of NSW shall be informed in accordance with Section 146 of the Heritage Act, 1977.*
- 14) *A certificate from a suitably qualified tradesperson will be obtained, certifying that the kitchen, food storage and food preparation areas have been fitted in accordance with the National Code for the Construction and Fitout of Food Premises.*
- 15) *An approval under Section 68 of the Local Government Act 1993 will be obtained from the Council for the erection of the temporary structures. The application will be supported by a report detailing compliance with the provisions of the Building Code of Australia. Structural certification from an appropriately qualified practicing structural engineer will be submitted to the Council with the application under Section 68 of the Local Government Act 1993 to certify the structural adequacy of the design of the temporary structures.*
- 16) *NSW Health will provide Lismore City Council with 24 hour contact details in relation to construction issues related to the new facility.*

Demolition

- 1) Minor demolition will be undertaken in accordance with the requirements of Australian Standard AS2601 - 2001: The Demolition of Structures incorporated into the Occupational Health and Safety Act 2000 administered by WorkCover NSW.
- 2) A Hazardous Building Materials Management Plan will be prepared prior to demolition commencing and will form part of the Construction Management Plan.
- 3) Measures to control soil erosion during demolition will be introduced in accordance with currently accepted principles, as described in Managing Urban Stormwater (EPA NSW) and Soil Erosion and Sediment Control (The Institution of Engineers, Australia).

Access and Movement

- 1) An Access and Safety plan will be prepared to maintain access and use of the site during the redevelopment of the New Psychiatric Unit site to ensure the safety of staff, visitors and patients. This will form part of the Construction Management Plan.

Services

- 1) The proponent will comply with the requirements of the relevant public authorities in regard to the connection to, relocation and/or adjustment of services affected by the construction of the proposed development.

Building Fabric

- 17) *The visible light reflectivity from building materials used on the facades of the buildings will not exceed 20% and will be designed so as not to result in glare that causes any nuisance or interference to any person or place.*
- 18) *Final design details of the proposed external materials and finishes, including schedules and a sample board of materials and colours for above ground works, will be submitted to and approved by the Director of Major Sites Assessment (or successor) within six months of the Minister approving Major Project 06_0078*

Water Management

- 1) The design of the stormwater disposal system will be based on the latest edition of AR&R and Bureau of Meteorology ARI statistics, Authority guidelines and AS3500.
- 19) *NSW Health is committed to water sensitive urban design principles and will incorporate design philosophy to ensure water quality targets are addressed. Hard surfaced areas, landscaped areas, roof water and subsoil drainage systems will be designed so that all water directed to a Council approved drainage system will prevent an intensification of discharge runoff onto adjoining land. This system will be designed and constructed in accordance with Council's Development, Design & Construction Manuals. Final design plans of the stormwater drainage systems will be prepared by a qualified practicing Civil Engineer and in accordance with the requirements of Council. The hydrology and hydraulic calculations will be based on models described in the current edition of Australian Rainfall and Runoff. Stormwater quality will meet the following targets:*

Contaminant	Target
Coarse Sediment (0.1 to 0.5mm)	80% mean annual reduction from baseline
Fine Particles (<0.1mm)	50% mean annual reduction from baseline
Total Phosphorous	45% mean annual reduction from baseline
Total Nitrogen	45% mean annual reduction from baseline
Litter	70% mean annual reduction from baseline
Hydrocarbons, motor fuels, oils and greases	90% mean annual reduction from baseline

- 20) *Seepage or rainwater collected on-site during construction will be managed in accordance with Council guidelines.*
- 21) *Works that involve water supply, sewerage and stormwater drainage work or management of waste as defined by Section 68 of the Local Government Act, 1993 applications for these works will be submitted to Council.*

Environmental Management

- 1) *The new facility's external general and security lighting will conform to all relevant Australian Standards.*

Traffic Access and Parking

- 1) *NSW Health will seek approval for the proposed bulk oxygen service area on Weaver Street, under Section 138 of the Roads Act. The bulk oxygen service area on Weaver Street will meet all relevant Australian Standards, Codes and WorkCover requirements for construction and servicing of the facility.*
- 2) *All roadways constructed as part of the new development external to the site will be designed in accordance with the relevant Council requirements and RTA/Austrroads. Final road design plans shall be prepared by a qualified practising Civil Engineer and lodged with Council prior to commencement of works.*
- 3) *NSW health will hold discussions with the relevant Bus service provider to establish the optimal outcome for visitors to the new facility utilising public transport.*
- 4) *The revised design presented in this report improves parking functionality and increases the overall number of off-street parking for the Hospital.*
- 5) *Prior to the commencement of works approval will be obtained under Section 138 of the Roads Act for the construction of on street car parking located within the road reserve of Hunter Street, and*

Vegetation

- 1) The proponent will provide landscaping in accordance with the attached landscape drawings.
- 2) *NSW Health will consult with Lismore City Council to establish suitable species and impact on in ground services for proposed landscaping outside the Hospital boundary.*
- 3) *All trees on or off the site that are not approved for removal will be suitably protected by way of tree guards, barriers or other measures as necessary are to be provided to protect root system, trunk and branches, during construction.*

Construction Management

Prior to commencing construction, a Construction Environmental Management Plan will be prepared. All construction activity will be undertaken strictly in accordance with this plan. This plan will include:

- 1) *Development of a site specific soil erosion and sediment control plan, An Erosion and Sediment Control Plan (ESCP) will be prepared and implemented in accordance with the requirements of Managing Urban Stormwater–Soils & Construction Volume 1 (2004) by Landcom and other relevant guidance. Soil erosion and sediment control measures will be designed in accordance with the document Managing Urban Stormwater–Soils & Construction Volume 1 (2004) by Landcom and other relevant guidance.*
- 2) *Construction hours - The hours of construction, including the delivery of materials to and from the site, will be restricted as follows:*
 - a. Between 7:00 am and 6:00 pm, Mondays to Fridays inclusive;
 - b. Between 8:00 am and 1:00 pm, Saturdays;
 - c. No work on Sundays and public holidays.

Works may be undertaken outside these hours where:

 - d. The delivery of materials is required outside these hours by the Police or other authorities;
 - e. It is required in an emergency to avoid the loss of life, damage to property and/or to prevent environmental harm;
 - f. The work is approved through the Construction Noise and Vibration Management Plan; and
 - g. Residents likely to be affected by the works are notified of the timing and duration of these works at least 48 hours prior to the commencement of the works.
- 3) *Site contamination review and remediation Prior to the commencement of any works on the site, a Contamination Management Plan will be prepared by a suitably qualified person in accordance with relevant legislation and guidelines, identifying any contaminants on site and the required procedure for removal of contaminants and remediation of the site.*
- 4) *Prior to the commencement of building works, a Site Audit conducted by a suitably qualified person will be undertaken to ascertain that all identified hazardous materials have been removed from the site.*
- 5) *48 hours notice will be given to Lismore City Council prior to the commencement of excavation, shoring or underpinning works on the site.*
- 6) *Documentation of basement drainage will be issued to Lismore City Council prior to commencement of drainage and stormwater works. The documentation will incorporate:*
 - a. *The pump system.*
 - b. *The areas draining to the pump.*

- c. *Perimeter subsoil drainage*
 - d. *Reticulation and discharge system*
 - e. *Backup and safety systems*
- 7) *Pre-construction dilapidation report - a qualified structural engineer will be engaged to prepare a Pre-Construction Dilapidation Report detailing the current structural condition of existing and adjoining buildings, infrastructure and roads. The report shall be submitted to Council prior to commencement of works.*
- 8) *Construction Traffic management - a Construction, Traffic and Pedestrian Management Plan will be prepared by a suitably qualified person in accordance with relevant legislation and guidelines and be submitted to Council. The Plan shall address the following matters:*
- h. *Ingress and egress of vehicles to the site,*
 - i. *Loading and unloading, including construction zones,*
 - j. *Predicted traffic volumes, types and routes,*
 - k. *Pedestrian and traffic management methods,*
 - l. *Details of haulage routes to be utilised for delivery and removal of material from the site, and*
 - m. *Traffic control plans prepared by an accredited person for works that impact upon the movement of vehicles or pedestrians upon the public road.*
- 9) *Air quality/dust control procedures, Measures will be taken to prevent dust from affecting the amenity of the neighbourhood during construction. In particular, the following measures will be adopted:*
- a. *Physical barriers will be erected at right angles to the prevailing wind direction or will be placed around or over dust sources to prevent wind or activity from generating dust emissions,*
 - b. *Earthworks and scheduling activities will be managed to coincide with the next stage of development to minimise the amount of time the site is left cut or exposed,*
 - c. *All materials will be stored or stockpiled at the best locations,*
 - d. *The surface will be dampened slightly to prevent dust from becoming airborne*
 - e. *All vehicles carrying spoil or rubble to or from the site will at all times be covered to prevent the escape of dust or other material,*
 - f. *All equipment wheels will be washed before exiting the site using manual or automated sprayers and drive-through washing bays,*
 - g. *Gates will be closed between vehicle movements and will be fitted with shade cloth, and*
 - h. *Cleaning of footpaths and roadways will be carried out regularly.*
- 10) *Noise and vibration management procedures, - a Noise and Vibration Management Plan will be prepared by a suitably qualified person in accordance with relevant legislation and guidelines and be submitted to Council.*
- 11) *Waste management plan, the design and management of facilities for the storage and handling of waste will comply with the requirements of Council's Policy.*
- 12) *Flora and Fauna Protection Plan,*
- 13) *Community Safety Plan,*
- 14) *Arrangements for temporary pedestrian and vehicular access,*
- 15) *Storage and Handling of Materials Procedures,*
- 16) *Environmental Training and Awareness,*

- 17) *Contact and complaints handling procedures, contact details for a 24-hour phone number will be supplied to Lismore City Council.*
- 18) Emergency Preparedness and Response.
- 19) *Construction noise on site during construction will meet the limits set by the EPA and relevant authorities*
- 20) *Construction Vibration will meet relevant authority and Australian Standards.*

Erosion Management

Measures to control soil erosion during construction will be introduced in accordance with currently accepted principles, as described in *Managing Urban Stormwater* (EPA NSW) and *Soil Erosion and Sediment Control* (The Institution of Engineers, Australia)

Access for People with Disabilities

The design of the facilities will permit effective, appropriate, safe and dignified use by all people, including those with disabilities and will be in accordance with:

- NSW Health Facility Guidelines, in particular Part B - Design for Access, Mobility, OH&S and Security.
- DDS32 Improved Access for Health Care Facilities.
- AS 1428.
- The Building Code of Australia.

Operation

An operational environmental management plan will be prepared prior to the opening of the hospital to the public.

The plan will address, but will not be limited to, the following matters:

- Measures to ensure protection of buildings and assets.
- Protection of flora and fauna and minimisation of anti-social behaviour.
- Visitor safety.
- Site security.
- Noise management.
- Traffic and pedestrian management.
- Storage of materials.
- Emergency and evacuation procedures.
- Fire safety.
- Waste management and ESD initiatives.
- Lighting.
- Signage.

- 1) *A Fire Safety Certificate will be furnished to Council for all the Essential Fire or Other Safety Measures forming part of this approval. A copy of the Fire Safety certificate will be submitted to Council. An Annual Fire Safety Statement will be provided to Council and the NSW Fire Brigade commencing within 12 months after the date on which the approval authority initial Fire Safety Certificate is received.*
- 2) *An application to discharge trade waste, including a plan containing all details of the proposed trade waste installation, will be submitted to Council in accordance with Australian Standard 3500, the NSW Code of Practice –Plumbing and Drainage and the Department of Energy, Utilities & Sustainability guidelines for the On-site Pre-treatment of Trade Waste Discharges to Sewer.*
- 3) *A post-construction dilapidation report will be submitted to council at the completion of all construction works to ascertain whether the construction works created any structural damage to adjoining buildings, infrastructure and roads. The report will be submitted to Council. Safety and Security of Surrounding Residents*
- 4) *NSW Health is committed to the design of the New Psychiatric Unit that ensures the safety of all patients, staff, visitors and surrounding residents.*
- 5) *The revised design will located the most acute patient facilities away from existing residential development.*
- 6) *All driveways and parking areas will be unobstructed at all times. Driveways and car spaces will not be used for the manufacture, storage or display of goods, materials or any other equipment and will be used solely for vehicular access and for the parking of vehicles associated with the use of the premises.*
- 7) *Any hazardous or toxic materials will be stored in accordance with WorkCover Authority requirements and all tanks, drums and containers of toxic and hazardous materials will be stored in a bunded area. The bund walls and floors will be constructed of impervious materials and will be of sufficient size to contain 110% of the volume of the largest tank plus the volume displaced by any additional tanks within the bunded area.*
- 8) *Public ways will at all times be kept clear of any materials, vehicles, refuse, skips or the like.*
- 9) *A Compliance Certificate will be provided to Council showing that the development has met with the detailed requirements of the relevant water supply authority for the region that the subject site is located within.*
- 10) *The proponent shall comply with the requirements of any public authorities (e.g. Energy Australia, Sydney Water, Telstra Australia, AGL, etc) in regard to the connection to, relocation and/or adjustment of the services affected by the construction of the proposed structure. Any costs in the relocation, adjustment or support of services shall be the responsibility of the proponent.*

Contributions

- 1) *Meetings have been held with Lismore City Council in relation to Section 94 and 64 contributions. As a result of these meetings LCC have revised the contribution sums to the degree that NSW Health can perceive a clear nexus between the levies and development impact.*
- 2) *In relation to the Section 94 Contributions the reduced sum of \$76,055 can be directly related to the concessions made by Lismore City Council in the provision of 89 car parking space on street as part of the overall parking requirements for the Hospital. Under this circumstance NSW Health will support this level of payment to Lismore City Council on a without prejudice and without precedent basis.*
- 3) *In relation to the Section 64 Contributions the reduced sum of \$34,280 can be directly related to the increase of 23 beds as part of the redevelopment of the New Psychiatric unit from the existing 25 bed unit to the proposed 48 bed unit. Under this circumstance NSW Health will support this level of payment to Lismore City Council on a without prejudice and without precedent basis.*

CONCLUSION

This report, together with the comprehensive Environmental Assessment, which accompanied the Major Project Application, represents the Preferred Project for the redevelopment of Lismore Base Hospital New Psychiatric Unit project

The proposed development involves the redevelopment of Psychiatric Inpatient and Ambulatory Care facilities at Lismore Base Hospital. It represents an appropriate use of the site, and will continue the strong historical association of the site and its buildings with the provision of health care. The development is strongly in the public interest and will provide substantial benefits to the local community.

The proposal demonstrates a high level of consistency with prevailing planning instruments including the provisions of the Lismore LEP 2000 and relevant SEPPs.

The assessment has concluded that the site is suitable for the proposal and that the implementation of the Lismore Base Hospital New Psychiatric Unit Project is consistent with the public interest.

The Minister is requested to consider the application favourably.

APPENDIX 1 SUBMISSIONS RECEIVED

all communication to
GENERAL MANAGER

our ref: HIM,P9872; 06-6580

your ref: 9042058/MP_06_0078

contact: Helen Manning

August 17, 2006

The Director
Strategic Assessment
Department of Planning
GPO Box 39
SYDNEY 2001

Dear Sir,

Major Project 06_0078: New Psychiatric Unit, Lismore Base Hospital

Thank you for the opportunity to comment on the above major project. Lismore City Council recognises the benefit of this facility to the regional and local community, and fully supports its development.

The Environmental Assessment has been considered and the following comments and proposed conditions are provided for the Minister's consideration.

Traffic, access and parking.

a) The relocation of the proposed oxygen tank from Hunter Street to Weaver Street will impact upon the amenity of the residents of the adjoining residential area and the condition of the existing roads. The oxygen facility will be serviced by a heavy articulated vehicle (semi trailer) which in order to service the facility must travel through Fermoy Ave, Weaver St, Bent St and Orion St. These streets have been designed for local traffic, primarily for access to residential dwellings, and are not designed for use by heavy vehicles, particularly in relation to the standard of construction which may be unsuitable for use by this type of vehicle. The use of residential streets by heavy vehicles may have an impact in regard to safety for the existing residential environment and an increase in traffic noise.

Should the bulk oxygen tank be located within Weaver Street then it is considered reasonable that the operational environmental management plan restricts the deliveries to day time periods as specified within the NSW EPA Industrial Noise Policy.

In addition, the works proposed for the servicing area for the oxygen tank located within the road reserve of Weaver Street will require approval under section 138 of the Roads Act.

b) The Environmental Assessment does not address the suitability of the existing road infrastructure to cater for the development. The adjoining roads are only partly sealed, with the kerbside parking areas being gravel only, and open drains in lieu of kerb and gutter. This is not an acceptable road standard for an urban area. The development has proposed to use the unsealed gravel areas for parking. The dust and sediment that will be generated from these areas due to the use by vehicles associated with the development is not an acceptable impact on adjoining residents and the environment. Works should be undertaken to upgrade this infrastructure to a standard that is appropriate for use as on-street parking.

None of the above issues appear to have been addressed within the traffic impact assessment provided.

- c) The future stage proposal shows the part closure of Hunter St. The closure of this section of road would increase traffic volumes in surrounding residential areas due to traffic accessing the hospital carpark not being able to travel directly from Uralba into Hunter Street. This is unlikely to be supported by Council given that this closure would significantly increase traffic volumes in surrounding residential streets.

- d) The traffic impact report has identified that Stage 1 of the development will generate in the vicinity of 54 additional vehicle movements per hour, peak hour. In preparing its section 94 plan Council has undertaken detailed analysis of the existing urban traffic network and its ability to handle future traffic expansion generated by expected development. In response to this demand Council has developed a section 94 plan to ensure that the required expansion of the existing traffic network can be funded to accommodate the traffic growth. The plan is based on a user pays scenario where a development pays for the increase in demand generated by the increase in traffic associated with its development. In accordance with Council's Section 94 plan the additional traffic that will be generated by the expansion of the hospital would attract a levy of \$263,570.

The Environment Assessment Report has stated that the extent and magnitude of contributions will be reviewed and assessed by the Department of Health once the development is established. It is Council's concern that should these charges not be levied as part of the approval then they will not be provided. This would result in a short fall in funding for the required works of which Council has no other means of funding. It is Council's opinion that it is equitable that these levies be required as part of the approval process.

- e) Public Transport Infrastructure: the development concept indicates allowance for taxis and private cars to enter, drop off passengers and leave in a forward direction. However there is no indication of how pedestrians are to find their way to the facility from the bus stop in Uralba Street as the final master plan design appears to be impermeable to pedestrians. If pedestrian access through the Hospital is to be denied, it is recommended that the Hospital negotiate with Kirklands Bus Services to move the Hunter St bus stop closer to the pedestrian entry to the Psychiatric Unit.
- f) The plans provided show the disabled car parking spaces at the principle public entrance are not to be constructed as part of this stage of the development but will be constructed at a later date. Disabled carparking should be provided with this first stage of development and within a reasonable distance of the principle public entrance.

2. Environmental Management

a) Stormwater Management

The Environmental Assessment states 'Council approved gross pollutant trap will be provided to the outlet of the on-site detention system of each car parking area'. The EA does not introduce or recognise water sensitive urban design principles. Council's Stormwater Management Plan and Draft DCP – Water Sensitive Urban Design provides the following water quality targets that should be addressed through appropriate contemporary WSUD design for the proposed development.

Table 1: Storm water Quality Targets

Contaminant	Target
Coarse Sediment (0.1 to 0.5mm)	80% mean annual reduction from baseline
Fine Particles (<0.1mm)	50% mean annual reduction from baseline
Total Phosphorus	45% mean annual reduction from baseline
Total Nitrogen	45% mean annual reduction from baseline
Litter	70% mean annual reduction from baseline
Hydrocarbons, motor fuels, oils and greases	90% mean annual reduction from baseline

- b) **Contaminated Land Management**
 The EA recommends 'it is prudent to remove the under ground storage tank (UST) or assess its integrity and associated pipework and establish a monitoring program which could provide an early warning of any leakage'. It is considered that if the UST has no further use on site then the system should be remediated in accordance with the Department of Environment and Conservation Guideline – *Contaminated Sites: Guideline for Assessing Service Station Sites*. If the UST is to continue to be utilised then assessment of the system integrity and installation of a monitoring system would be supported.
- c) **Noise Management**
 The EA nominates for the engagement of acoustic consultants to advise on appropriate acoustic treatments for the building and plant. It is recommended that at the completion of the project the applicant engage an acoustic consultant to provide a final report that the noise emissions from the site satisfy the projects' noise performance targets and does not generate offensive noise to adjoining sensitive receivers.
- d) **Food Safety**
 The regulation of food safety matters of a hospital development would fall to the responsibility of the NSW Food Authority and therefore it is recommended that the Department of Planning seeks comment from the NSW Food Authority prior to the determination of the application.
- d) **External Lighting**
 It is recommended that external and security lighting associated with the development satisfy the requirements of AS 4282 – Control of Obtrusive Effects of Outdoor Lighting.
- e) It is not clear which authority will be responsible for the day to day regulatory management of the site during and post construction. This is important when considering the site's environmental performance against the targets of the Environmental Management Plan.
- f) **Trees.**
 Landscaping plans indicate replacement of street trees in Hunter Street, beyond hospital property boundaries. No such replacement should occur without consultation with Council regarding suitable species and avoidance of underground services.

A previous consent issued by Council for preparatory earthworks required the removal of a mature hoop pine. This proved to be controversial and as a result a replacement mature hoop pine should be planted in a local public park, the park and location of pine within the park to be determined by Council.

3. Water & Sewer

a) A Trade Waste Application will be requested as a condition of consent.

b) Section 64 Levies (per Water Directorate : bed - 0.9 ET water, 1.43 ET sewer)
A credit of 19 ET's will be given for this stage (i.e. 14 existing lots & 5 ET's Structures removed). This will result in 24.2 ET's being levied against the water & 49.64 ET's against the sewer after the credit is given

Section 64 Levies for Water Rous	\$81,675	(24.2 ET's)
" LCC	\$21,078	(24.2 ET's)
Sewer LCC	\$122,511	(49.64 ET's)

Total **\$225,264.00**

4. Recommended conditions of consent

1. Council requires:

- All proposed buildings be constructed in accordance with any amendment or modification outlined in these conditions
- All proposed works be carried out in accordance with any amendment or modification outlined in these conditions
- Any proposed use of buildings or land be in accordance with any amendment or modification outlined in these conditions

and be substantially in accordance with the plans 100 D: MP001a A; A102 C; A103B A104B; A105C; A200C; A201C; A202C; A220B; DA221B; L001A; L002A; L003A and documents submitted with the Environmental Assessment.

Reason: *To correctly describe what has been approved. (EPA Act Sec 79C)*

Lot Consolidation

2. All separate parcels of land shall be consolidated into one allotment under one title and lodged with the Registrar General prior to commencement of works.

Reason: *the property should become one consolidated site to prevent future dealing in separately titled land.*

Road works

3. Prior to commencement of work the contractor shall lodge a bond with Council to cover any damage to infrastructure within the road reserve arising during the construction of the proposed development. Evidence is to be provided to Council indicating the pre development condition of the surrounding public infrastructure. Any damage arising from construction activities to roads, kerb and gutter, footpaths or any other infrastructure within the road reserve shall be reinstated in accordance with Council's Development, Design and Construction Manuals (as amended) to Councils satisfaction.

Reason: *To ensure an adequate road network in accordance with adopted standards. (EPA Act Sec 79C(a))*

4. Prior to commencement of work a Construction Traffic Management Plan shall be submitted to, and approved by, Lismore City Council. Such plan shall include: details of haulage routes to be utilised for delivery and removal of material from the site and traffic control plans prepared by an RTA accredited person for any works that impact upon the movement of vehicles or pedestrians upon the public road.

Reason: *To ensure activities relating to the development do not interfere with the movement of traffic along the public road. (EPA Act Sec 79C(b))*

5. Prior to the commencement of works approval shall be obtained under S138 of the Roads Act, for works proposed for the construction of onstreet parking located within the road reserve of Hunter Street.

Reason: *to comply with the Roads Act 1993*

Oxygen Tank

6. The applicant shall submit to Council prior to the commencement of works associated with the relocation of the bulk oxygen tank certification from an appropriately qualified person stating that the proposed location and construction of the bulk oxygen tank and storage area comply with appropriate Australian Standards and WorkCover requirements.

Reason: *To protect human health and the environment. (EPA Act Sec 79C(b))*

7. Deliveries to the proposed gas tank shall be limited to daylight periods as specified within the NSW EPA Industrial Noise Policy. Outside of these hours the area shall be available for on-street carparking. Appropriate regulatory signage identifying the above requirement shall be placed prior to commencement of use.

Note: The proposed loading area on Weaver Street should be a sealed road with kerb and gutter in conjunction with the further development of the site

8. Prior to the commencement of works approval shall be obtained under S138 of the Roads Act, for works proposed for the servicing area for the gas tank located within the road reserve of Weaver Street.

Reason: *to comply with the Roads Act 1993*

Parking

9. Works should be undertaken to upgrade Hunter Street to a standard that is appropriate for use as on-street parking along the frontage to the Hospital.

Reason: *to preserve the amenity of the area, particularly in regard to preventing dust nuisance (EPA Act S79Cb)*

10. Provision shall be made for at least 400 car parking spaces with a bitumen sealed/paved or equivalent surface constructed and landscaped in accordance with the requirements of Council's Development Control Plans for Off Street Carparking Requirements and Crime Prevention Through Environmental Design, Australian Standard AS2890.1 Parking Facilities- Offstreet Parking and Council's Development, Design & Construction Manuals.

Reason: *to provide adequate offstreet parking space for the anticipated traffic that will be generated by the development (EPA Act S79C(a))*

11. Signage shall be erected to clearly indicate that off-street parking is available.

Reason: *To ensure the free flow of traffic and comply with traffic regulations (EPA Act S79C(c))*

Stormwater

12. Hard surface areas, landscaped areas, roof water and subsoil drainage systems shall be designed so that all water is directed to a Council approved drainage system to prevent an intensification of discharge runoff onto adjoining land. This system shall be constructed in accordance with Council's Development, Design & Construction Manuals.

Reason: to ensure that the land or adjoining land is not damaged by the uncontrolled discharge of concentrated runoff from buildings and paved areas (EPA Act Sec79C(b)).

13. Stormwater quality shall meet the following targets:

Contaminant	Target
Coarse Sediment (0.1 to 0.5mm)	80% mean annual reduction from baseline
Fine Particles (<0.1mm)	50% mean annual reduction from baseline
Total Phosphorus	45% mean annual reduction from baseline
Total Nitrogen	45% mean annual reduction from baseline
Litter	70% mean annual reduction from baseline
Hydrocarbons, motor fuels, oils and greases	90% mean annual reduction from baseline

Reason: to ensure that the land or adjoining land is not damaged by the uncontrolled discharge of concentrated runoff from buildings and paved areas (EPA Act Sec79C(b)).

Lighting

14. External and security lighting associated with the development is to satisfy the requirements of AS 4282 – Control of Obtrusive Effects of Outdoor Lighting.

Reason: to protect the amenity of surrounding properties (EPA Act Sec79C(b)).

Trees

15. No planting or replacement of street trees should occur without consultation with Council regarding preferred species and avoidance of underground services.

Reason: to ensure that appropriate landscaping is provided. (EPA Act Sec79C(b)).

16. One mature hoop pine is to be planted in a local public park in replacement for the hoop pine removed during site demolition works. The tree is to be planted in a location within a selected park as determined by the Lismore City Council's Executive Director, Development and Governance.

Reason: to provide replacement value for the mature hoop pine removed during site preparation works) (EPA Act Sec79C(b)).

Trade Waste

17. An application to discharge trade waste, to include a plan containing all details of the proposed trade waste installation, shall be submitted to Lismore City Council. The trade waste application must be approved prior to occupation of the building. Designs are to be in accordance with Australian Standard 3500, the NSW Code of Practice – Plumbing and

Drainage and the Department of Energy, Utilities & Sustainability guidelines for the On-site Pre-treatment of Trade Waste Discharges to Sewer.

Reason: *to ensure adequate protection of utility services. (EPA Act Sec79Cb)*

S94 Contributions

18. Payment of contributions levied under Section 94 of the Environmental Planning and Assessment Act and Lismore Contributions Plan 1999 (as amended) are required. Such levies shall contribute towards the provision of public services and/or amenities identified in the attached schedule. The rates and amounts applying at the date of this notice, totalling **\$263,570** are set out in the schedule for your information. Where the total contribution payable exceeds \$20,000 payment to Council must be by bank cheque or cash. Personal cheques are not acceptable. All contributions, bonds etc. shall be paid prior to the occupation of the building.

Should levies set out in the attached schedule not be paid within twelve (12) months of the date of this consent, the rates shall be increased in accordance with the listing of rates applicable for the financial year in which payment is made. This listing of rates reflects the adjustment made for the Consumer Price Index (CPI) on an annual basis.

The contributions set out in the schedule are exclusive of any GST (if any) and where the provision of any services or the construction of any infrastructure or any other thing with those contributions occurs, then in addition to the amount specified above the Applicant will pay to the Council the GST (as defined below) which is payable by the Council in respect of the provision of such services or construction of any infrastructure or any other thing.

If the contributions set out in the schedule, or part thereof, are to be met by the dedication of land or other approved Material Public Benefit, then the Applicant will pay to Council the GST (defined below) applicable to the value of land dedicated or (Material Public Benefit) which is payable by the Council in respect of the provision of such services or construction of any infrastructure or any other thing.

GST means any tax levy charge or impost under the authority of any GST Law (as defined by the GST Act) and includes GST within the meaning of the GST Act.

The GST Act means A New Tax System (Goods and Services Tax) Act 1999 or any amending or succeeding legislation.

Reason: *To provide funds for the provision of services and facilities identified in Lismore City Council's Section 94 Contributions Plan dated July 1999 as required by the increased population or activity. (EPA Act Sec 94)*

Water/Sewer levies

19. The proponent shall pay to Council all Water and Sewerage headworks levies deemed necessary by Council and in accordance with Section 64 of the Local Government Act 1993. The rates and amounts applying at the date of this notice are set out in the schedule for your information. Such levies shall be paid prior to occupation of the building.

Should levies set out in the attached schedule not be paid within twelve months of the date of this consent, the rates shall be increased in accordance with the listings of rates applicable for the financial year in which payment is made. This listing of rates reflects the adjustment made for the Consumer Price Index (CPI, Sydney) on an annual basis.

Reason: *to provide funds for the provision of services and facilities identified in Lismore City Council's Section 64 Contributions Plan as required by the increased activity (Water Management Act 2000, S306)*

NOTE 1: Water and/or Sewerage Headworks levies totalling **\$225,264** are stated in the attached Schedule.

Conclusion

If you have any enquiries regarding the matters raised in this submission please contact the undersigned on 0266 250565.

Yours faithfully,

A handwritten signature in black ink, appearing to read 'H Manning', written in a cursive style.

Helen Manning
Manager, Planning Services

LISMORE CITY COUNCIL

Major Project 06_0078: New Psychiatric Unit, Lismore Base Hospital

IMPORTANT TO NOTE

The rates and amounts shown against the various items below are those applicable as at date of original consent. If these levies are not paid within twelve (12) months of the date of original consent, the rates shall then be increased on an annual basis in accordance with the prevailing Australian Bureau of Statistics Consumer Price Index (Sydney), as applicable at the time of payment.

To be read in conjunction with advice of development consent.

The levies imposed by Condition No. 15, as contributions towards the cost of meeting increased demand for public services and amenities that will result from the development, are identified in this Schedule.

The following Levies are charged under Section 94 of the Environmental Planning and Assessment Act 1979 and amounts payable are set out below.

** Note: For discount see Lismore Contributions Plan 2004*

Levy Area	Account No.	No. of ET's/m ²	Cost Per ET/m ²	Amount Payable
Residential	1655.1	55.5	4749	\$263,570
Total				\$263,570

ET'S CORRECT - PLANNING SERVICES OFFICER DATE

The cost per ET is in accordance with the relevant Lismore Contributions Plan and/or Section 64 Plan applicable, as at the date of development application approval.

LEVIES CORRECT - FINANCIAL SERVICES OFFICER DATE

Total levies at current rates (actual amount to be calculated when final plan submitted).

A COPY OF THIS ADVICE MUST BE PRESENTED WHEN MAKING PAYMENT DATE: CASHIER: RECEIPT NO:

COUNCIL USE ONLY

Cashier to Note:

This section must be completed by a Financial Services Officer prior to receipt.

I hereby certify that the fees payable have been checked to ensure that:

the Consumer Price Index has been applied to the schedule of Section 94 fees and Section 64 fees, where the period between the date of consent and the date of payment is in excess of twelve (12) months.

LISMORE CITY COUNCIL

Major Project 06_0078: New Psychiatric Unit, Lismore Base Hospital

IMPORTANT TO NOTE

The rates and amounts shown against the various items below are those applicable as at date of original consent. If these levies are not paid within twelve (12) months of the date of original consent, the rates shall then be increased on an annual basis in accordance with the prevailing Australian Bureau of Statistics Consumer Price Index (Sydney), as applicable at the time of payment.

To be read in conjunction with advice of development consent.

The levies imposed by Note No. 1 are identified in this Schedule.

The rates and amounts shown against the various items are those current at the date of this notice. If these levies are not paid within twelve (12) months of the date of this consent these rates shall be increased annually from the date of this notice, in accordance with the percentage increase from the date of approval to the date of payment, as notified by the CPI (Sydney).

The following Levies are charged under and amounts payable are set out below.

Levy Area	Account No.	No. of ET's	Cost Per ET	Amount Payable
Water and Sewerage Headworks Levies are charged under Section 306 of the Water Management Act 2000 and amounts payable are set out below.				

Water Headworks				
Urban Reservoir Zone	8175.1	24.2	871	\$21,078
- Central				
Rous County Council (Except Nimbin)	9200.2	24.2	3375	\$81,675
Sewerage Headworks				
South Lismore Treatment Plant	7175.1	49.64	2468	\$122,511
Total				\$225,000

ET'S CORRECT - PLANNING SERVICES OFFICER DATE/...../.....

The cost per ET is in accordance with the relevant Lismore Contributions Plan and/or Section 64 Plan applicable, as at the date of development application approval.

LEVIES CORRECT - FINANCIAL SERVICES OFFICER DATE/...../.....

Total levies at current rates (actual amount to be calculated when final plan submitted).

A COPY OF THIS ADVICE MUST BE PRESENTED WHEN MAKING PAYMENT DATE: RECEIPT NO: CASHIER:



NSW GOVERNMENT
Department of Planning

Facsimile

To	Leone McEntee Urban Planning Outcomes	Fax	02 9661 5919
		Phone	02 9661 8018
From	Nick Hall Strategic Assessments	Fax	02 9228 6101
		Phone	02 9228 6570
		Email	nicholas.hall@planning.nsw.gov.au
Date	29 August 2006	Total pages (incl cover sheet)	7

Re: Lismore Base Hospital Redevelopment Stage 1

I understand that Stephanie Ballango has previously forwarded you two submissions from local residents and Lismore City Council's response.

Please find attached copies of the two outstanding submissions regarding Lismore Base Hospital; the RTA and the Department's North Coast Regional Office.

Once you have had a chance to consider all the submissions can you advise me when you envisage submission of your preferred project report?

Thanks,

cc: Ben Jon Abraham, HASSELL f.92732345

File No. 2575395/N00733 GS:js
Mr Greg Sciffer

Director Strategic Assessments
Department of Planning
GPO Box 39
SYDNEY NSW 2001

FILE COPY

**Lismore City Council. MP 06-0078. Proposed Psychiatric Unit,
Lismore Base Hospital, Hunter Street, Lismore.**

Dear Sir

You are advised that the Regional Advisory Committee (RAC) met on 17 August 2006 at Tweed Shire Council Chambers to discuss the above development application.

The proposed development was considered by the Committee and the following concerns in relation to traffic management and the following comments in relation to road safety and traffic management are made:

- The proposed development has not undertaken a proper traffic impact study to assess its impact on the operation of the surrounding road network and infrastructure.
- Any access to a public road should be designed to maintain safety and efficiency. It should be designed in accordance with AUSTRROADS guidelines and Australian Standards 2890 to safely manage turning traffic in and out of the development.
- In regional centres the parking demand is greater than the metropolitan areas due to the lack of an adequate public transport network to service its total catchment.
- Current parking problems in the area are exacerbated by the establishment of other adjacent ancillary services that do not provide adequate off-street car parking.
- Lismore Base Hospital has had a history of unresolved traffic management and parking problems.
- The Committee considers the DCP parking rates inadequate for the development. 1 space/5 beds is inadequate for visitor parking. Staff parking does not cater for the additional need created with the overlap in shifts.
- A parking strategy should reflect the operational needs where visitors due to their shorter stay should occupy closer facilities and longer term parking should be located further away.
- The lack of adequate off-street parking impacts on the adjoining residents and their visitors.
- It is proposed to create an on-street servicing facility in Weaver Street. This is not supported by the Committee for safety reasons. It should be located off-street in a proper service area.

At this time the Committee recommends that the development application should be rejected until a proper traffic impact study is undertaken to properly identify all of the impacts and the required mitigation measures.

Yours faithfully


Peter Collins
Regional Manager, Northern Region

28 AUG 2006

Copy for Lismore City Council



NSW GOVERNMENT
Department of Planning

Memorandum

To Nicholas Hall, Strategic Assessment

cc

From Jenny Vallis
North Coast
Phone 02 6640 2160 Fax 02 6640 2134 Email northcoast@planning.nsw.gov.au

Date 2 August 2006 **File no** 9042058 **File**

SUBJECT: ASSESSMENT - MAJOR PROJECT MP 06_0078 - LISMORE BASE HOSPITAL, NEW PSYCHIATRIC UNIT, HUNTER STREET AND URALBA STREET, LISMORE.

Purpose - To provide assessment requirements in relation to the above development

Background - The subject land is zoned 5 (Special Uses) (Hospital) under the provisions of the Lismore LEP 2000.

Issue - The following are some of the key issues that appear to be relevant to this proposed development:

INTERIM CAR PARKING, the proposed psychiatric unit is to replace a car parking area linked to the existing Lismore Based Hospital (LBH) (see map on page 12 of the Environmental Assessment). The Traffic Study at Appendix 5 prepared by Traffic, Environmental and Forensic Engineers (TEF) Consulting and dated 26 June 2006 concluded that the existing LBH parking demand is greater than the 230 formal and semi-formal off-street parking spaces at LBH. This creates a parking demand overflow that is exerted onto surrounding streets (paragraph 2.2 Existing parking provision and demands, page 3) However, the applicant does not appear to provide on site interim car parking provisions while the Unit is under construction. While it is acknowledged that the applicant has addressed the issue of car parking for the completed project, concerns are raised in relation to the level of car parking available to patients and staff during the construction period; and

HERITAGE, the environmental assessment estimates the existing buildings of the LBH buildings to have been constructed between the 1950s and 1980s and have minimal architectural or heritage value. However, as there are buildings of the Federation period (e.g. Armstrong House, "Kiaora") and Cultural Landscape Items (e.g. Canary Island Palms, grounds of LBH, Uralba Street between Hunter and Diadem Street) see attachments, in the vicinity and this proposal forms part of staged redevelopment of LBH there should be some consideration of these and any other relevant architectural/landscape features. In addition, the applicant would need to determine, before construction work begins, the likelihood of significant archaeological features being present on the site.

Recommendation

The above issues to be addressed are included in any requirements provided to the developer.

Jenny Vallis

Jenny Vallis
Local Planning Officer
(North Coast)

OK

[Signature]
Team Leader
Local Planning
3.8.06

LISMORE HERITAGE STUDY

Prepared by Perumal Murphy Wu Pty Ltd for
Lismore City Council and the NSW Department of Planning

ITEM NAME House
Armstrong House (formerly Kia-ora)
LOCATION 81-83 Uralla Street
Lismore
GROUP (See also Listing 30072)

ITEM TYPE
 Architectural
 Landscape
 Archeological

RSP. NO.
20028

STATEMENT OF SIGNIFICANCE

Large Federation residence, two storeys, rare locally. Adopts a diagonal plan arrangement common on corner sites elsewhere. Essentially unaltered following recent removal of unsympathetic additions. Double verandah of special note. Social and historic interest for its continuing hospital use. Long associated with the base hospital opposite. Prominent in local views on a well trafficked street. Local significance

REAL DESCRIPTION

MAP REFERENCE

RECOMMENDATIONS

PHOTOGRAPH

17/28

EVALUATION OF SIGNIFICANCE



Rare	
Historic	
Aesthetic	
Social	
Scientific	
Other	
Representative	
Historic	Local
Aesthetic	Local
Social	Local
Scientific	
Other	

LISMORE HERITAGE STUDY

Prepared by Perumal Murphy Wu Pty Ltd for
Lismore City Council and the NSW Department of Planning

ITEM Grounds
NAME Richmond Base Hospital
LOCATION Uratba Street
East Lismore
GROUP

ITEM TYPE

- Architectural
- Landscape
- Archaeological

REF. NO.

35029

STATEMENT OF SIGNIFICANCE

Row of mature Canary Island palms planted c1940's enhancing building from 1930's period. Local significance.

REAL DESCRIPTION

MAP REFERENCE

RECOMMENDATIONS

PHOTOGRAPH
L23A



EVALUATION OF SIGNIFICANCE

	State
Historic	
Aesthetic	
Social	
Scientific	
Other	
Representative	
Historic	
Aesthetic	Local
Social	
Scientific	
Other	



URALBA ST. CANARY ISLAND PALMS
(BETWEEN HUNTER AND DIADEM STS)



Education

Hospital

Hospital

Water Reservoir

2(a)

2(b)

6(a)

ROTARY

Medical

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File No. 257.5395/N00733
Reference. 06/1297

Director, Strategic Assessments
Department of Planning
GPO Box 39
SYDNEY NSW 2001

RECEIVED
11 AUG 2006

Major Project MP 06-0078.
Proposed New Psychiatric Unit, Lismore Base Hospital.

Dear Sir

The above development application has been listed for discussion at the next Regional Development Committee Meeting, scheduled for 17 August 2006. Following the meeting a response will be prepared and is expected to be completed during the week ending 25 August 2006 and will be sent to you as soon as possible.

If Council has any further enquiries please contact Mr Greg Sciffer on 6640 1344 or email land_use_northern@rta.nsw.gov.au.

Yours faithfully

Peter Collins
Regional Manager, Northern Region

- 8 AUG 2006

Roads and Traffic Authority



31 Victoria Street
Grafton NSW 2460

PO Box 576
Grafton NSW 2460

T 02 6640 1300

www.rta.nsw.gov.au

re Director
 Strategic Assessments
 Department of Planning
 PO Box 39
 SYDNEY NSW 2001

RECEIVED

31 JUL 2006

Dear Sir or Madam

Re: Application Ref # 06_0078
 New Psychiatric Unit LBH

I am writing in support of the proposed development. Rationale:

Having held a position within NSW DOW C.M.H. (based at SSWATS + Sydney University) over the past 5 years managing patients with Eating Disorders, I have long been aware of the inappropriate facilities for adolescents + young women requiring in-patient Mental Health in the Northern Rivers Area.

The proposal would seem to provide an opportunity for NRATS patients with eating disorders and other serious mental illnesses to be treated locally rather than transferred to Sydney, away from family and friends, for lengthy periods, to receive treatment.

I thank you for the opportunity to support the proposal.

As I am currently on maternity leave, I would prefer my name and address are not made public, however I am happy for my comments to be made public.

Yours sincerely,

26/7/06

Strategic Assessments – The Director
Department of Planning
GPO Box 39
Sydney NSW 2001

RECEIVED

31 JUL 2006

Re: application reference number (06_0078) Major Project: Lismore Base Hospital – New Psychiatric Unit Project situated north of the existing Lismore Base Hospital, bounded by the Hospital to the south, Weaver and Little Uralba Sts to the east and Hunter St to the west.

[REDACTED]
[REDACTED] subjected to unsettling noises at anytime of the day or night and possibly even unnerving sights due to the relocation and expansion of the psychiatric unit. My family is worried that many disturbed individuals will be housed close to our house whilst they undergo treatment. We are worried about escapees committing dangerous offences upon the surrounding residents. We don't want to have to witness any more anti social behaviour than what we already witness due to our proximity to the hospital because of increased traffic and an increase in the immediate population of disturbed individuals.

I think that our increased fears for our own safety and peace of mind would be mimicked by potential buyers of our house and would therefore reduce the land values of the immediate surrounding houses. In short the proposed relocation and expanding development of the psychiatric unit would forever change the peaceful family amenities of the location for the surrounding community.

If the development were to go ahead I would seek compensation for the reduction in living standards that I believe the proposal would force upon my family and our neighbourhood.

APPENDIX 2 REVISED DESIGN DRAWINGS

FOR FURTHER INFORMATION, ONLY TO BE USED FOR DISCUSSION PURPOSES. THIS DRAWING IS THE PROPERTY OF THE CONSULTANT AND IS NOT TO BE REPRODUCED OR TRANSMITTED IN ANY FORM OR BY ANY MEANS, ELECTRONIC OR MECHANICAL, WITHOUT THE WRITTEN PERMISSION OF THE CONSULTANT.

THIS DRAWING IS A REVISION OF AN EXISTING DRAWING. THE EXISTING DRAWING IS IDENTIFIED BY THE REFERENCE NUMBER IN THE REVISIONS SECTION.

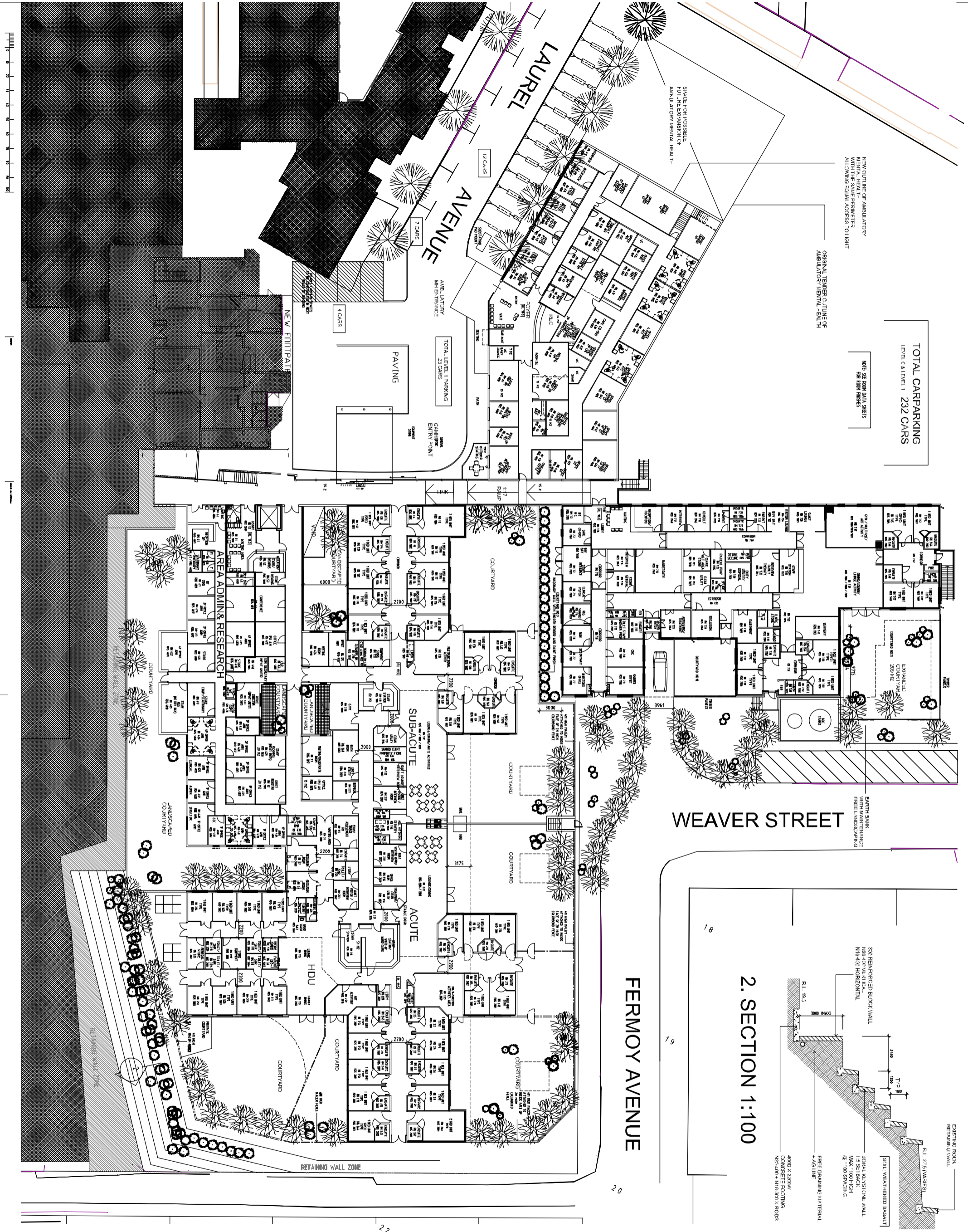
TOTAL CARPARKING
LEVEL 3 & LEVEL 1: 232 CARS

NOTE: SEE EXISTING DATA SHEETS FOR ROOM NUMBERS

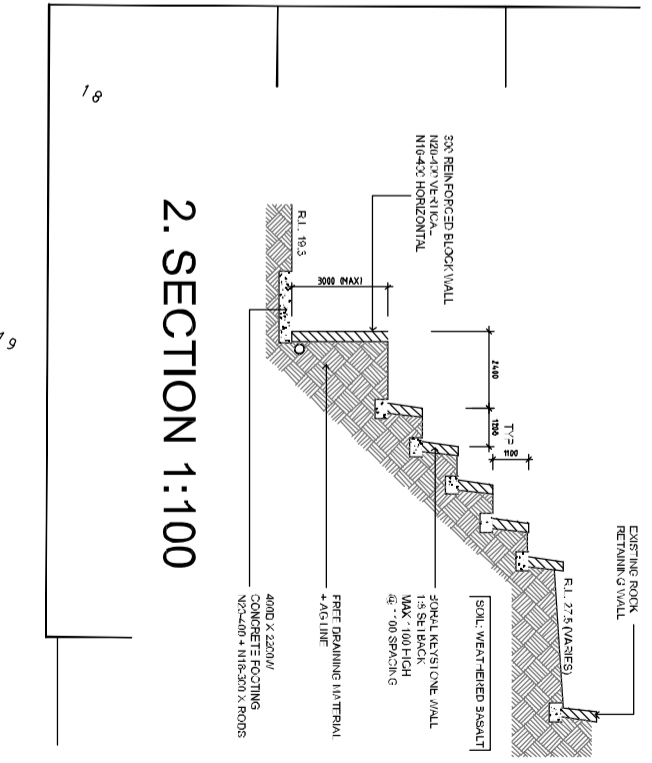
NEW GUTTER OF AMBULATORY WITH A 10% SLOPE TO PERMIT WATER TO RUN TO THE PERIPHERY ALLOWING TOTAL ACCESS TO LIGHT

ORIGINAL TENDERS OUTLINE OF AMBULATORY - HEALTH

SPACE FOR POSSIBLE FUTURE EXPANSION OF AMBULATORY HEALTH



2. SECTION 1:100



EXISTING ROCK RETAINING WALL

R.L. 277.5 (VARIES)

SOIL WEATHERED BASALT

300ML REINFORCING WALL 13.5% BACK FILL 100% SAND 40% SPREADS

300ML REINFORCING WALL 13.5% BACK FILL 100% SAND 40% SPREADS

PRECAST BRICKWORK 100% TYPICAL 1:1.5

4000V 500MM CONCRETE FOOTING 100% TYPICAL 1:1.5

FERMOY AVENUE

WEAVER STREET

LAUREL AVENUE

6	RAIN REDUCED	23/05/16	HC
5	USED ADDRESS 24/106	24/11/16	AN
4	RAIN REDUCED	23/05/16	HC
3	ADDED AREA CHANGED TO SMALL STREET	17/05/16	HC
2	RAIN REDUCED	13/05/16	HC
1	PREPARED	15/04/16	HC
	REV/ DESCRIPTION	DATE BY	

NSW HEALTH

AREA HEALTH
NORTH COAST AREA HEALTH SERVICE

PROJECT NAME
LISMORE BASE HOSPITAL
RICHMOND CLINIC REDEVELOPMENT

BRAND NAME
LEVEL 1 PLAN
TENDER PROPOSAL OPTION 3

SCALE
AS SHOWN

DATE
12/01/2016

PROJECT NUMBER
APP
APP

PROJECT DIRECTOR
APP

CONSULTANT
Hillier Group International
100/100 St Georges Street, Sydney NSW 2000

DATE
12/01/2016

PROJECT NAME
LISMORE BASE HOSPITAL
RICHMOND CLINIC REDEVELOPMENT

BRAND NAME
LEVEL 1 PLAN
TENDER PROPOSAL OPTION 3

SCALE
AS SHOWN

DATE
12/01/2016

PROJECT NUMBER
APP
APP

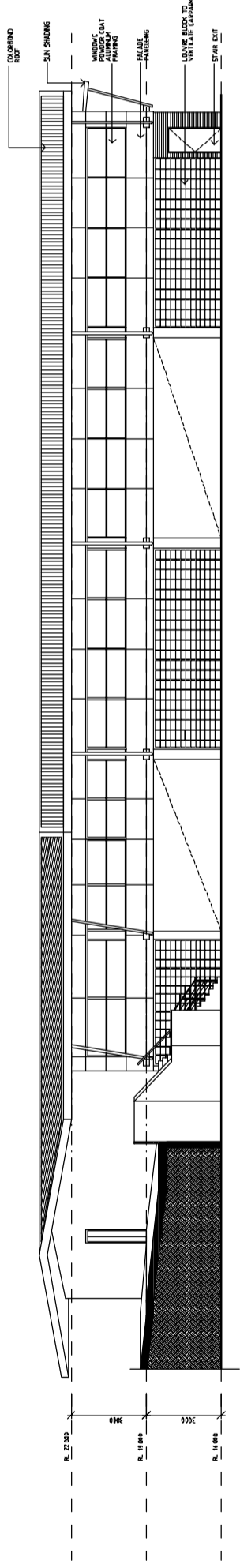
PROJECT DIRECTOR
APP
APP

CONSULTANT
Hillier Group International
100/100 St Georges Street, Sydney NSW 2000

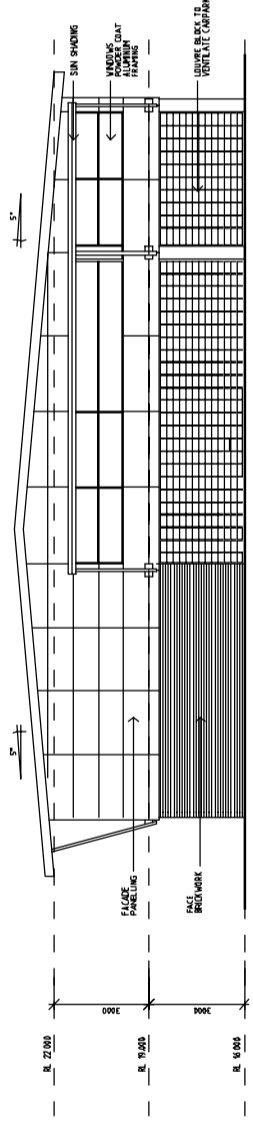
DATE
12/01/2016

USE DIMENSIONS SHOWN ONLY ON THIS SCALE. ALL DIMENSIONS SHALL BE VERIFIED ON SITE AND DISCREPANCIES IMMEDIATELY REPORT TO THE ARCHITECT FOR RESOLUTION.

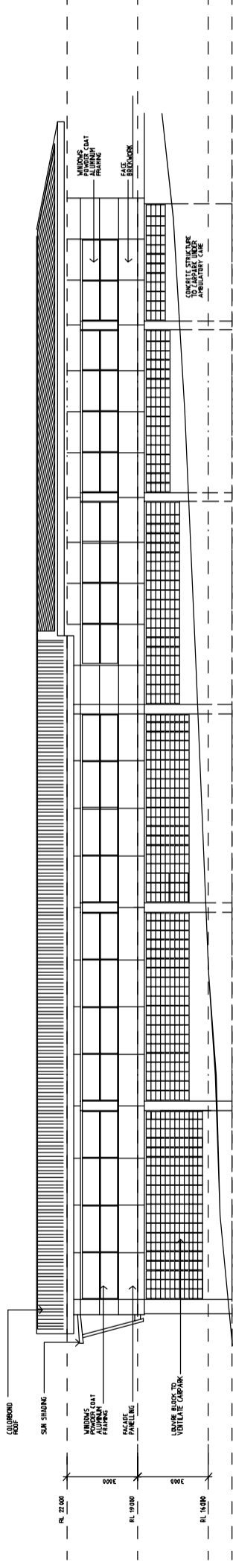
*DIMENSIONS ARE INTENDED AS GUIDES AND DO NOT NECESSARILY DESCRIBE THE FULL SCOPE OF WORK. ALL DIMENSIONS SHALL BE VERIFIED ON SITE AND DISCREPANCIES IMMEDIATELY REPORT TO THE ARCHITECT FOR RESOLUTION.



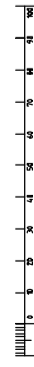
01 NORTH ELEVATION 1:100



02 WEST ELEVATION 1:100

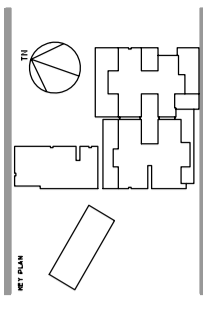


03 SOUTH ELEVATION 1:100



2	GENERAL REVISION	19/11/14	AK
1	FIRST ISSUE	03/11/14	AK
	REV/DESCRIPTION	DATE	BY
ARCHITECT HP Health Partners International 10/10/14 10/10/14 10/10/14 10/10/14			
CONSULTANT			

PROJECT NUMBER
D/C
PROJECT DIRECTOR
APP



AREA HEALTH
NORTH COAST AREA HEALTH SERVICE
PROJECT NAME
LISMORE BASE HOSPITAL
RICHMOND CLINIC REDEVELOPMENT
DRAWING NAME
ELEVATIONS:
AMBULATORY CARE
DESIGNED APPROVED SCALES DIMERS
AN AN 1:100
CADD FILE NAME
MHU-A-E1
PAPER DRAWING ID
MHU-A-E1

DESIGNER'S OFFICE ONLY - NOT SCALE
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 ATTACHED TO THE ORIGINAL FOR RECORD
 ATTACHED TO THE ORIGINAL FOR RECORD

THIS PLAN IS REVISED AND INTENDS TO CORRECT
 THE EXTENT OF REVISIONS AND THE
 EXTENT OF REVISIONS BY COMPARISON WITH
 PREVIOUS VERSIONS

OUTLINE OF ORIGINAL DA

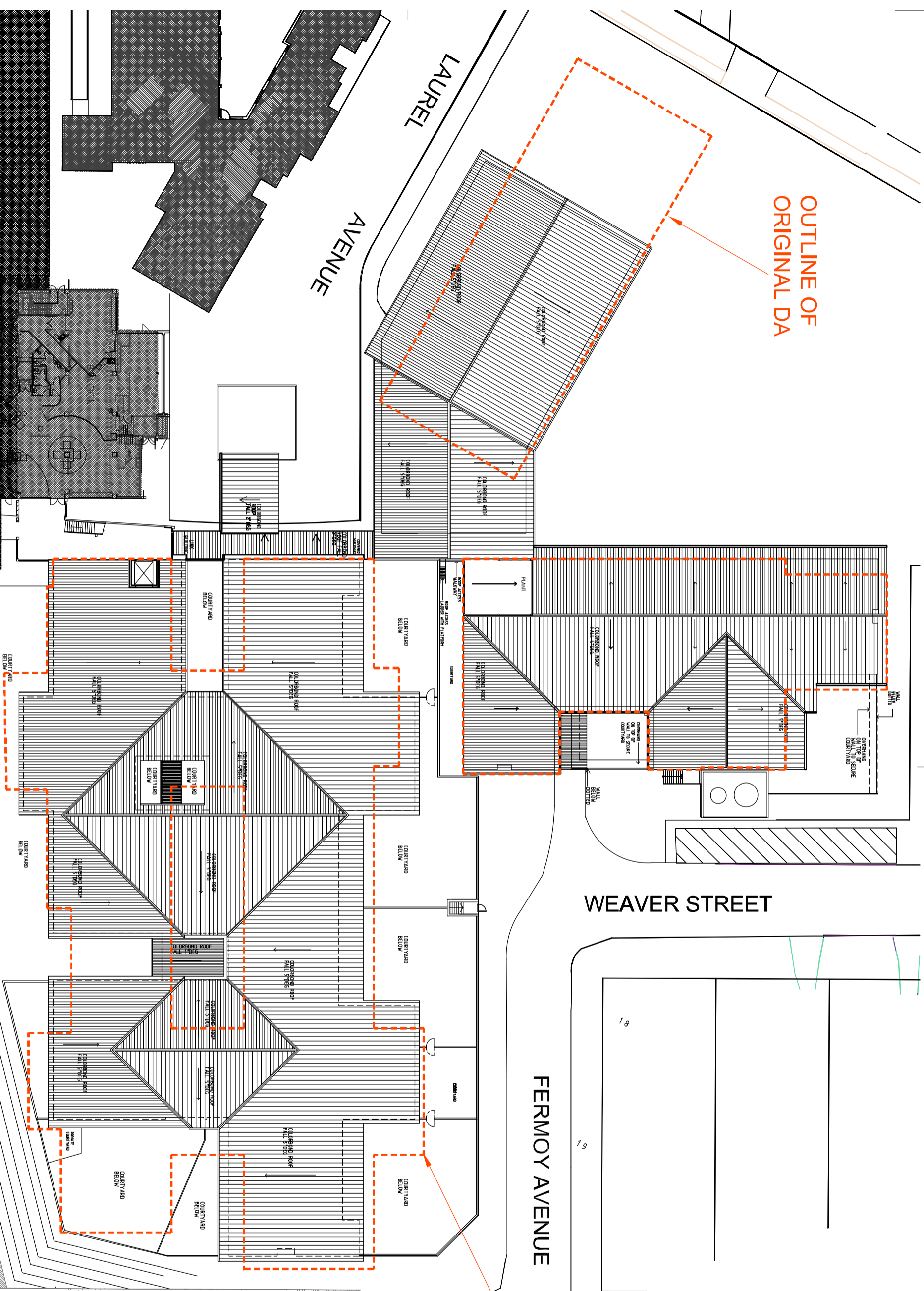
WEAVER STREET

FERMOY AVENUE

OUTLINE OF ORIGINAL DA

LAUREL AVENUE

COMPARISON OF ORIGINAL AND REVISED DA



NO.	DESCRIPTION	DATE	BY
5	ROOF PLAN REVISED	20 01 04	AS
4	ROOF PLAN UPDATES	27 09 03	HS
3	LEFT REVISED	19 09 03	BS
2	REVISED ROOFS AND COURTYARDS	21 06 03	AN
1	REST ISSUE	21 06 03	AN
	REV. DESCRIPTION		DATE BY

APPROVED: **HP** Health Planning International
 PROJECT MANAGER
 DATE: 27/09/03

PROJECT MANAGER
 DUC
 PROJECT DIRECTOR
 APP
 ALL PLAN



AREA HEALTH
 NORTH COAST AREA HEALTH SERVICE
 PROJECT NAME
 LEISHORE BASE HOSPITAL
 RICHMOND CLINIC REDEVELOPMENT

BRAND NAME
 LEVEL 2 PLAN
 1 LEISURE PHOTO-OSAL
 SCALE(S)
 APPROVED: 12/00
 ALL
 CAD FILE NAME: MHU-PL-12
 PAPER DRAWING: MHU-PL-12

APPENDIX 3 REVISED CONDITIONS FROM LISMORE CITY COUNCIL

CS: P9872

9042058 / MP 06_0078

Chris Soulsby

October 12, 2006

The Director
Strategic Assessment
Department of Planning
GPO Box 39
SYDNEY 2001

Attention: Nicholas Hall

Dear Sir

Major Project 06_0078 New Psychiatric Unit, Lismore Base Hospital

I refer to our recent meeting with representatives from NSW Health on Council's submission to the Department dated August 17, 2006. Following this meeting Council officers reassessed the quantum of section 94 and section 64 charges that Council requests to be imposed on the Ministers approval. This has resulted in a significant reduction in the charges. The reduction in the S64 charges is as a result of allocating a 25 bed credit from the closure of the existing mental health unit. Council requests that NSW Health acknowledge in writing that these credits have been allocated to this development and are not available for future uses. Attached is a report adopted by Council that outlines Council's position with respect to the imposition of section 94 charges. This report provides the justification for the imposition of the section 94 charges.

Council and NSW Health also agreed to the minor alteration of some of the conditions in our initial submission to the Department.

Council's amended recommended conditions are as follows:

1. Council requires:

- All proposed buildings be constructed in accordance with any amendment or modification outlined in these conditions
- All proposed works be carried out in accordance with any amendment or modification outlined in these conditions
- Any proposed use of buildings or land be in accordance with any amendment or modification outlined in these conditions

and be substantially in accordance with the plans 100 D; MP001a A; A102 C; A103B A104B; A105C; A200C; A201C; A202C; A220B; DA221B; L001A; L002A; L003A and documents submitted with the Environmental Assessment.

Reason: *To correctly describe what has been approved. (EPA Act Sec 79C)*

Lot Consolidation

2. All separate parcels of land shall be consolidated into one allotment under one title and lodged with the Registrar General.

Reason: *the property should become one consolidated site to prevent future dealing in separately titled land.*

Road works

3. Prior to commencement of work the contractor shall lodge a bond of \$50,000 with Council to cover any damage to infrastructure within the road reserve arising during the construction of the proposed development. Evidence is to be provided to Council indicating the pre development condition of the surrounding public infrastructure. Any damage arising from construction activities to roads, kerb and gutter, footpaths or any other infrastructure within the road reserve shall be reinstated in accordance with Council's Development, Design and Construction Manuals (as amended) to Councils satisfaction.

Reason: *To ensure an adequate road network in accordance with adopted standards. (EPA Act Sec 79C(a))*

4. Prior to commencement of work a Construction Traffic Management Plan shall be submitted to, and approved by, Lismore City Council. Such plan shall include: details of haulage routes to be utilised for delivery and removal of material from the site and traffic control plans prepared by an RTA accredited person for any works that impact upon the movement of vehicles or pedestrians upon the public road.

Reason: *To ensure activities relating to the development do not interfere with the movement of traffic along the public road. (EPA Act Sec 79C(b))*

5. Prior to the commencement of works approval shall be obtained under S138 of the Roads Act, for works proposed for the construction of onstreet parking located within the road reserve of Hunter Street.

Reason: *to comply with the Roads Act 1993*

Oxygen Tank

6. The applicant shall submit to Council prior to the commencement of works associated with the relocation of the bulk oxygen tank certification from an appropriately qualified person stating that the proposed location and construction of the bulk oxygen tank and storage area comply with appropriate Australian Standards and WorkCover requirements.

Reason: *To protect human health and the environment. (EPA Act Sec 79C(b))*

7. Deliveries to the proposed gas tank shall be limited to daylight periods (except for emergency deliveries) as specified within the NSW EPA Industrial Noise Policy.

Note: The proposed loading area on Weaver Street should be a sealed road with kerb and gutter in conjunction with the further development of the site

8. Prior to the commencement of works approval shall be obtained under S138 of the Roads Act, for works proposed for the servicing area for the gas tank located within the road reserve of Weaver Street.

Reason: *to comply with the Roads Act 1993*

Parking

9. Works should be undertaken to upgrade Hunter Street to a standard that is appropriate for use as on-street parking along the frontage to the Hospital.

Reason: *to preserve the amenity of the area, particularly in regard to preventing dust nuisance (EPA Act S79Cb)*

10. Provision shall be made for at least 406 (inclusive of 89 on Hunter Street) car parking spaces with a bitumen sealed/paved or equivalent surface constructed and landscaped in accordance with the requirements of Council's Development Control Plans for Off Street Carparking Requirements and Crime Prevention Through Environmental Design, Australian Standard AS2890.1 Parking Facilities-Offstreet Parking and Council's Development, Design & Construction Manuals.

Reason: to provide adequate offstreet parking space for the anticipated traffic that will be generated by the development (EPA Act S79C(a))

11. Signage shall be erected to clearly indicate that off-street parking is available.

Reason: To ensure the free flow of traffic and comply with traffic regulations (EPA Act S79C(c))

Stormwater

12. Hard surface areas, landscaped areas, roof water and subsoil drainage systems shall be designed so that all water is directed to a Council approved drainage system to prevent an intensification of discharge runoff onto adjoining land. This system shall be constructed in accordance with Council's Development, Design & Construction Manuals.

Reason: to ensure that the land or adjoining land is not damaged by the uncontrolled discharge of concentrated runoff from buildings and paved areas (EPA Act Sec79C(b)).

13. Stormwater quality shall meet the following targets:

Contaminant	Target
Coarse Sediment (0.1 to 0.5mm)	80% mean annual reduction from baseline
Fine Particles (<0.1mm)	50% mean annual reduction from baseline
Total Phosphorus	45% mean annual reduction from baseline
Total Nitrogen	45% mean annual reduction from baseline
Litter	70% mean annual reduction from baseline
Hydrocarbons, motor fuels, oils and greases	90% mean annual reduction from baseline

Reason: to ensure that the land or adjoining land is not damaged by the uncontrolled discharge of concentrated runoff from buildings and paved areas (EPA Act Sec79C(b)).

Lighting

14. External and security lighting associated with the development is to satisfy the requirements of AS 4282 – Control of Obtrusive Effects of Outdoor Lighting.

Reason: to protect the amenity of surrounding properties (EPA Act Sec79C(b)).

Trees

15. No planting or replacement of street trees should occur without consultation with Council regarding preferred species and avoidance of underground services.

Reason: to ensure that appropriate landscaping is provided. (EPA Act Sec79C(b)).

16. Deleted.

Trade Waste

17. An application to discharge trade waste, to include a plan containing all details of the proposed trade waste installation, shall be submitted to Lismore City Council. The trade waste application must be approved prior to occupation of the building. Designs are to be in accordance with Australian Standard 3500, the NSW Code of Practice – Plumbing and Drainage and the Department of Energy, Utilities & Sustainability guidelines for the On-site Pre-treatment of Trade Waste Discharges to Sewer.

Reason: *to ensure adequate protection of utility services. (EPA Act Sec79Cb)*

Water/Sewer levies

19. The proponent shall pay to Council all Water and Sewerage headworks levies deemed necessary by Council and in accordance with Section 64 of the Local Government Act 1993. The rates and amounts applying at the date of this notice are set out in the schedule for your information. Such levies shall be paid prior to occupation of the building.

Should levies set out in the attached schedule not be paid within twelve months of the date of this consent, the rates shall be increased in accordance with the listings of rates applicable for the financial year in which payment is made. This listing of rates reflects the adjustment made for the Consumer Price Index (CPA, Sydney) on an annual basis.

Reason: *to provide funds for the provision of services and facilities identified in Lismore City Council's Section 64 Contributions Plan as required by the increased activity (Water Management Act 2000, S306)*

NOTE 1: Water and/or Sewerage Headworks levies totalling **\$34,280** are stated in the attached Schedule.

S94 Contributions

18. Payment of contributions levied under Section 94 of the Environmental Planning and Assessment Act and Lismore Contributions Plan 1999 (as amended) are required. Such levies shall contribute towards the provision of public services and/or amenities identified in the attached schedule. The rates and amounts applying at the date of this notice, totalling **\$76,055** are set out in the schedule for your information. Where the total contribution payable exceeds \$20,000 payment to Council must be by bank cheque or cash. Personal cheques are not acceptable. All contributions, bonds etc. shall be paid prior to the occupation of the building.

Should levies set out in the attached schedule not be paid within twelve (12) months of the date of this consent, the rates shall be increased in accordance with the listing of rates applicable for the financial year in which payment is made. This listing of rates reflects the adjustment made for the Consumer Price Index (CPI) on an annual basis.

The contributions set out in the schedule are exclusive of any GST (if any) and where the provision of any services or the construction of any infrastructure or any other thing with those contributions occurs, then in addition to the amount specified above the Applicant will pay to the Council the GST (as defined below) which is payable by the Council in respect of the provision of such services or construction of any infrastructure or any other thing.

If the contributions set out in the schedule, or part thereof, are to be met by the dedication of land or other approved Material Public Benefit, then the Applicant will pay to Council the GST (defined below) applicable to the value of land dedicated or (Material Public Benefit) which is payable by the Council in respect of the provision of such services or construction of any infrastructure or any other thing.

GST means any tax levy charge or impost under the authority of any GST Law (as defined by the GST Act) and includes GST within the meaning of the GST Act.

The GST Act means A New Tax System (Goods and Services Tax) Act 1999 or any amending or succeeding legislation.

Reason: To provide funds for the provision of services and facilities identified in Lismore City Council's Section 94 Contributions Plan dated July 1999 as required by the increased population or activity. (EPA Act Sec 94)

LISMORE CITY COUNCIL

Major Project 06_0078: New Psychiatric Unit, Lismore Base Hospital

IMPORTANT TO NOTE

The rates and amounts shown against the various items below are those applicable as at date of original consent. If these levies are not paid within twelve (12) months of the date of original consent, the rates shall then be increased on an annual basis in accordance with the prevailing Australian Bureau of Statistics Consumer Price Index (Sydney), as applicable at the time of payment.

To be read in conjunction with advice of development consent.

The levies imposed by Condition No. 15, as contributions towards the cost of meeting increased demand for public services and amenities that will result from the development, are identified in this Schedule.

The following Levies are charged under Section 94 of the Environmental Planning and Assessment Act 1979 and amounts payable are set out below.

** Note: For discount see Lismore Contributions Plan 2004*

Levy Area	Account No.	Amount Payable
Residential	1655.1	\$76,055
Total		\$76,055

LISMORE CITY COUNCIL

Major Project 06_0078: New Psychiatric Unit, Lismore Base Hospital

IMPORTANT TO NOTE

The rates and amounts shown against the various items below are those applicable as at date of original consent. If these levies are not paid within twelve (12) months of the date of original consent, the rates shall then be increased on an annual basis in accordance with the prevailing Australian Bureau of Statistics Consumer Price Index (Sydney), as applicable at the time of payment.

To be read in conjunction with advice of development consent.

The levies imposed by Note No. 1 are identified in this Schedule.

The rates and amounts shown against the various items are those current at the date of this notice. If these levies are not paid within twelve (12) months of the date of this consent these rates shall be increased annually from the date of this notice, in accordance with the percentage increase from the date of approval to the date of payment, as notified by the CPI (Sydney).

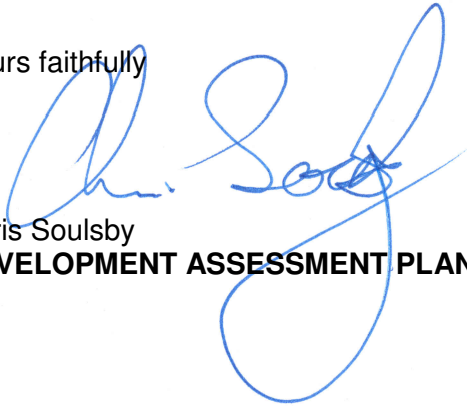
The following Levies are charged under and amounts payable are set out below.

Levy Area	Account No.	No. of ET's	Cost Per ET	Amount Payable
Water and Sewerage Headworks Levies are charged under Section 306 of the Water Management Act 2000 and amounts payable are set out below.				
Water Headworks				
Urban Reservoir Zone - Central	8175.1	0	871	\$0
Rous County Council (Except Nimbin)	9200.2	0	3375	\$0
Sewerage Headworks				
South Lismore Treatment Plant	7175.1	13.89	2468	\$34,280
Total				\$34,280

Council and NSW health also reached agreement on modification of a significant number of conditions

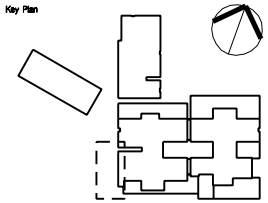
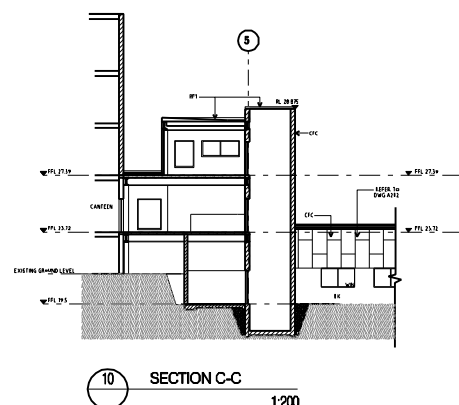
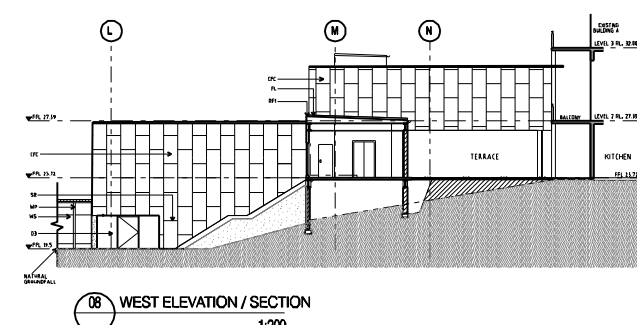
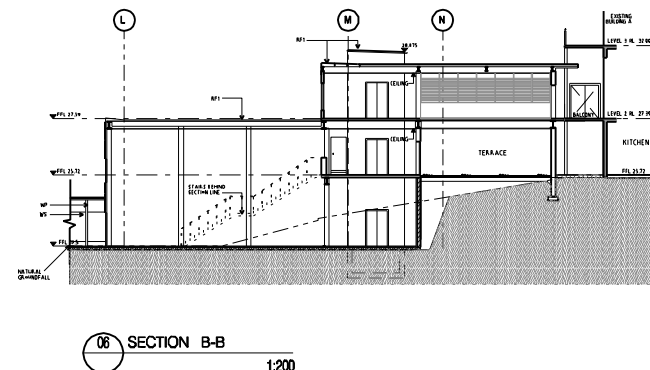
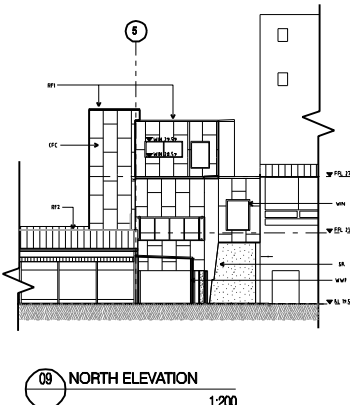
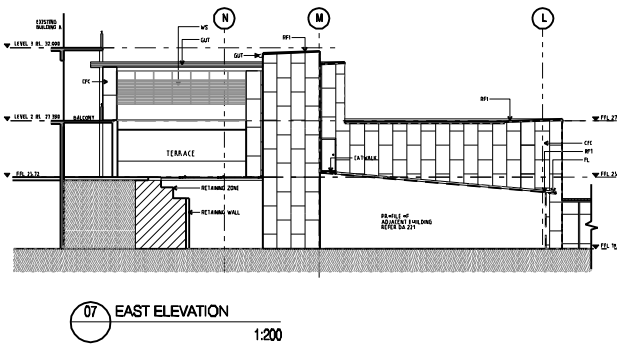
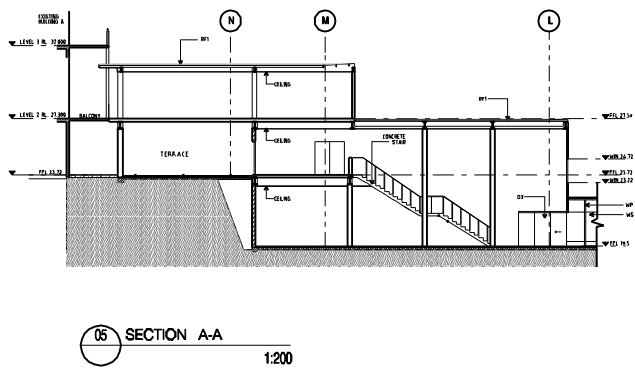
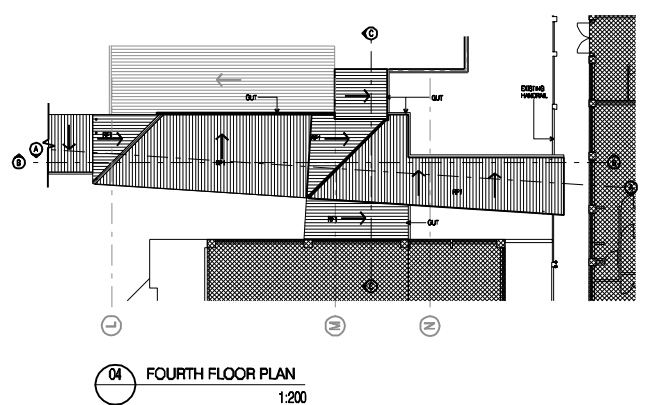
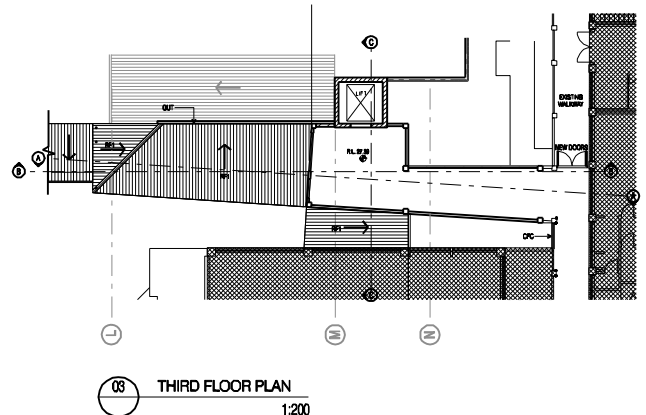
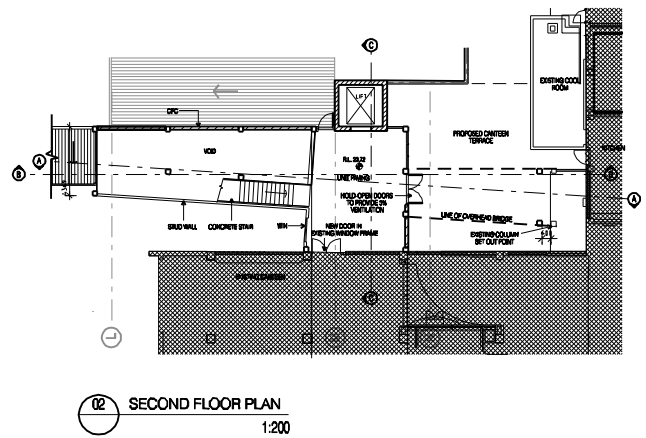
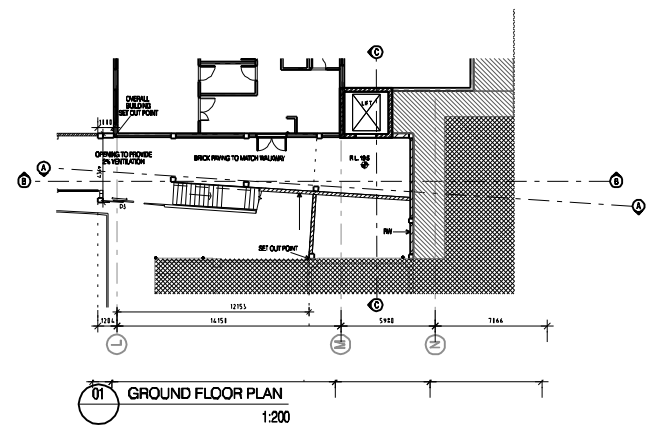
Should you have any further enquiries regarding this matter, please contact Chris Soulsby at Council's Administration Centre, Oliver Avenue, Goonellabah, on ☎ (02) 6625 0565, between the hours of 8.30am and 10.00am, Monday to Friday.

Yours faithfully



Chris Soulsby
DEVELOPMENT ASSESSMENT PLANNER

APPENDIX 4 ADDITIONAL DRAWINGS ISSUED FOR INFORMATION



- Note:
1. Do not scale drawings. Dimensions govern.
 2. All dimensions are in millimetres unless noted otherwise.
 3. All dimensions must be verified on site before proceeding with the work. HASSELL must be notified in writing in any discrepancies.
 4. This drawing must be read in conjunction with all relevant contracts, specifications, reports and drawings.
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Issue	Rev.	Reason for issue	App'd	Date
D		FOR TENDER		07.05.06
C		FOURTH DOCUMENT ISSUE		24.05.06
B		REVISIONS MADE		18.05.06
A		THIRD DOCUMENT ISSUE		09.05.06

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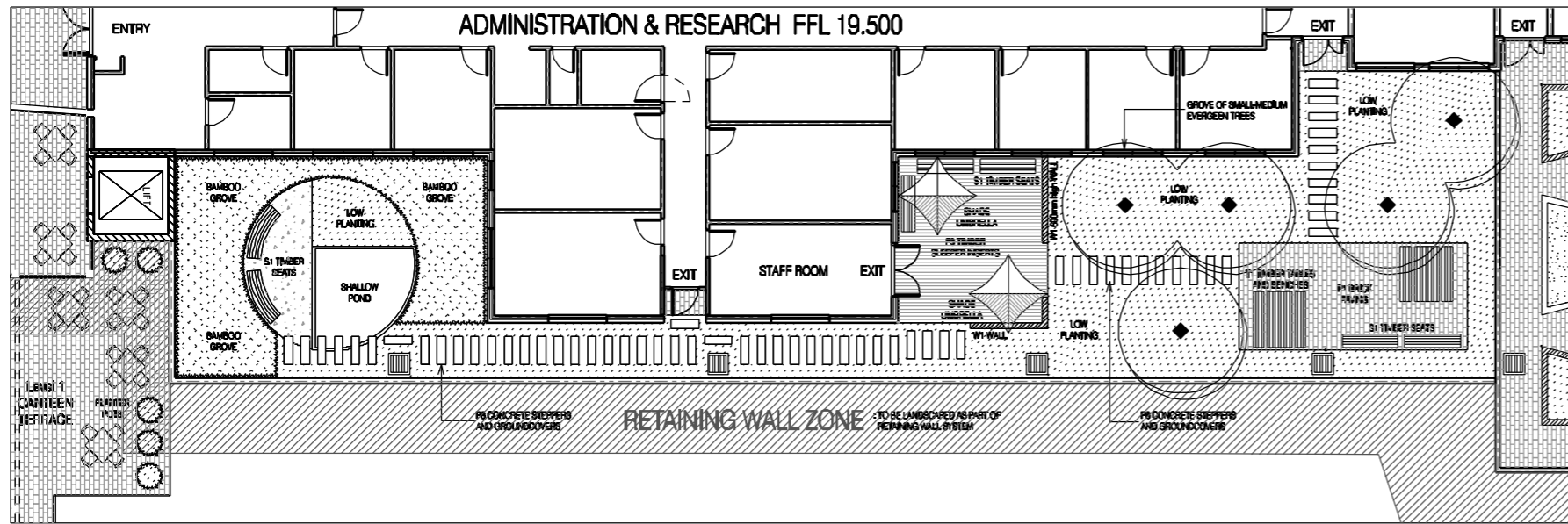
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HASSELL
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 Tel: 61 2 9279 2000 Fax: 61 2 9279 2545
 hys@hassell.com.au

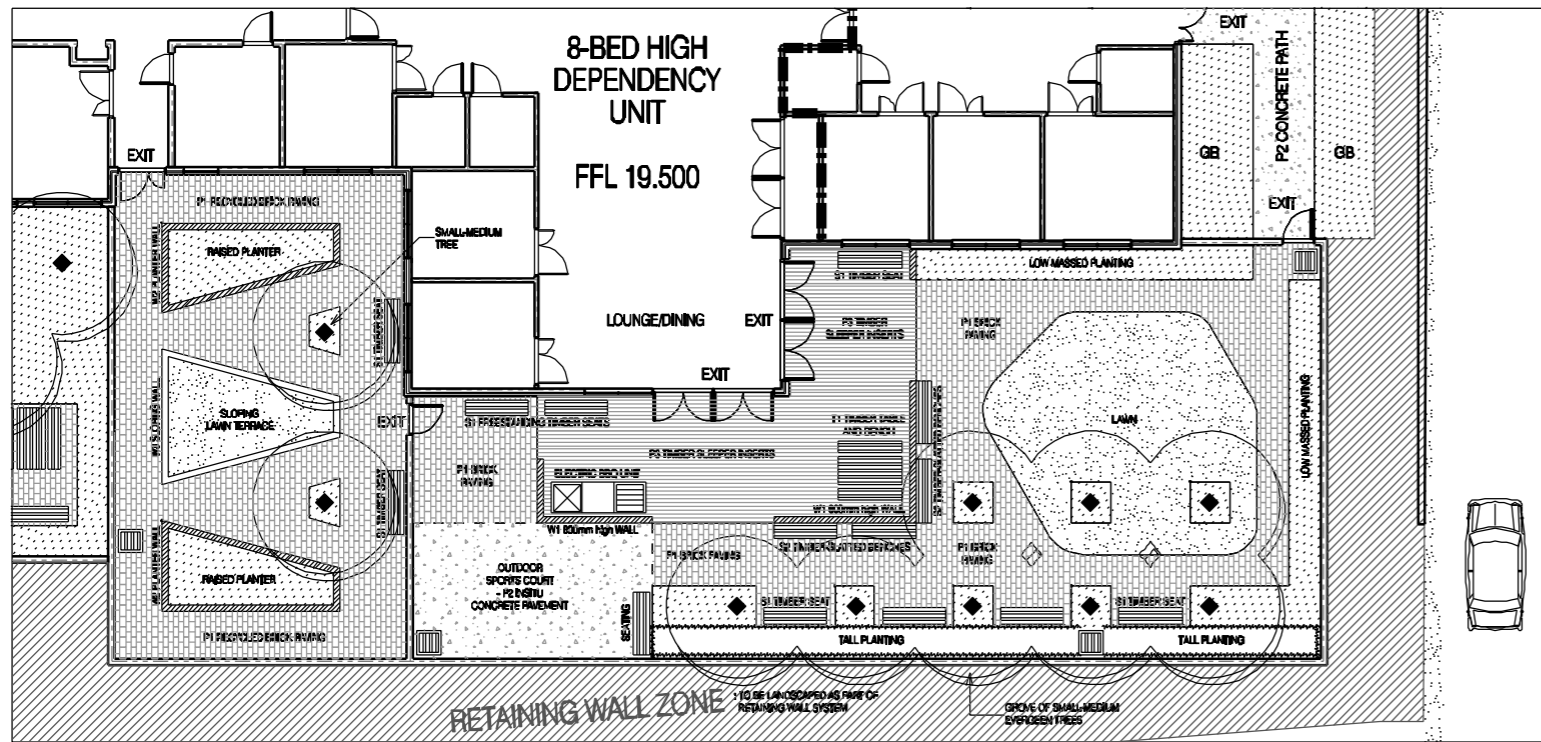
Project
**LISMORE BASE HOSPITAL
 RICHMOND CLINIC
 REDEVELOPMENT**

Drawing Title
**PLANS / ELEVATIONS /
 SECTIONS
 ENTRANCE LINK AREA**

Scale	Date	Drawn	Co-ordinated	Checked	Approved
1:200	29.05.06	SR			
Job No	Package	Drawing Number	Issue		
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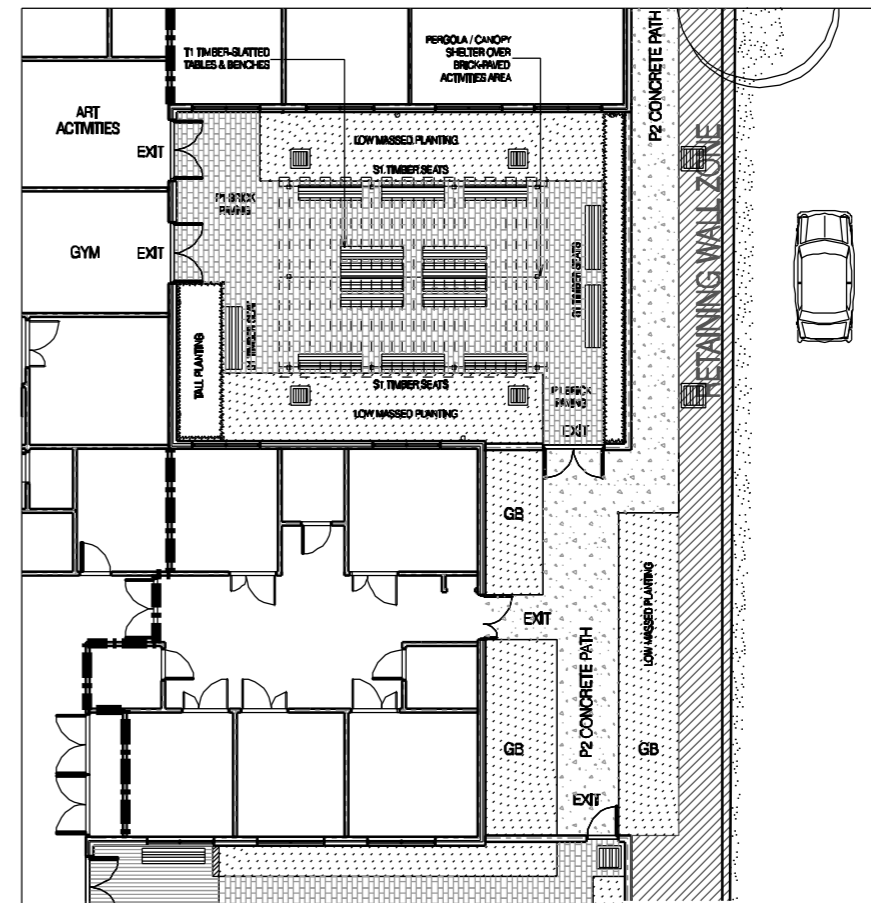
ADMINISTRATION COURTYARD



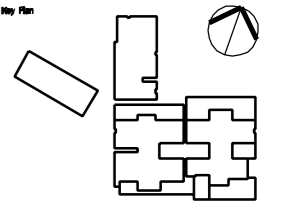
HIGH-DEPENDENCY COURTYARDS

LANDSCAPE MATERIALS AND FINISHES SCHEDULE

P1 RECYCLED BRICK PAVING, STRETCHER BOND PATTERN - colour to match typical hospital brick	W1 600mm high FREESTANDING RENDERED MASONRY WALL	S2 TIMBER-SLATTED BENCH SEATS, attached to wall
P2 INSITU CONCRETE PAVEMENT WITH STEEL FLOAT FINISH, UNPIGMENTED - saw cuts at 1200mm centres	W2 600mm high RENDERED MASONRY PLANTER RETAINING WALL to accommodate raised planting beds	S3 TIMBER-SLATTED PLANTER SEATS 450 high x 2000mm square with 1000 x 1000mm planter opening
P3 INSITU CONCRETE PAVEMENT WITH RECYCLED TIMBER SLEEPER INSERTS, PLANED SMOOTH BEFORE SETTING	W3 SLOPING MASONRY WALL (height varies) TO COURTYARD LAWN TERRACES	T1 TIMBER PICNIC TABLE (to match S1 timber seats)
P4 INSITU CONCRETE PAVEMENT WITH PAINTED SURFACE - saw cuts at 1200mm centres	W4 1200mm high CURVING FREESTANDING, PAINTED INSITU CONCRETE WALL, with seal attached	A1 RETRACTABLE CANVAS AWNINGS, TIMBER-FRAMED (manually operated)
P5 ARTIFICIAL TURF ON SAND BED / LAWN AREAS	W5 FREESTANDING BOUNDARY WALL, rendered & lime wash painted internal courtyard faces	A2 SHADE UMBRELLAS
P6 CONCRETE STEPPERS 1000 x 300mm LAID FLUSH WITH ADJACENT SURFACE, equivalent to Stonevue conc. pavers	S1 FREESTANDING TIMBER BENCH SEATS	CONCRETE PLANTER POT



ACTIVITIES COURTYARD



- Notes:
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Rev.	Issue	Reason for Issue	Appr.	Date
A	ISSUED FOR TENDER			JUNE 06

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Project:
LISMORE BASE HOSPITAL
RICHMOND CLINIC
REDEVELOPMENT

Drawing Title:
LANDSCAPE COURTYARD
DETAILED PLANS (2 of 2)

Scale	Date	Drawn	Co-ordinated	Checked	Approved
1:300@S1	MAY 06	PD	PD	PA	
Job No.	Package	Drawing Number	Sheet		
7455		L004	A		