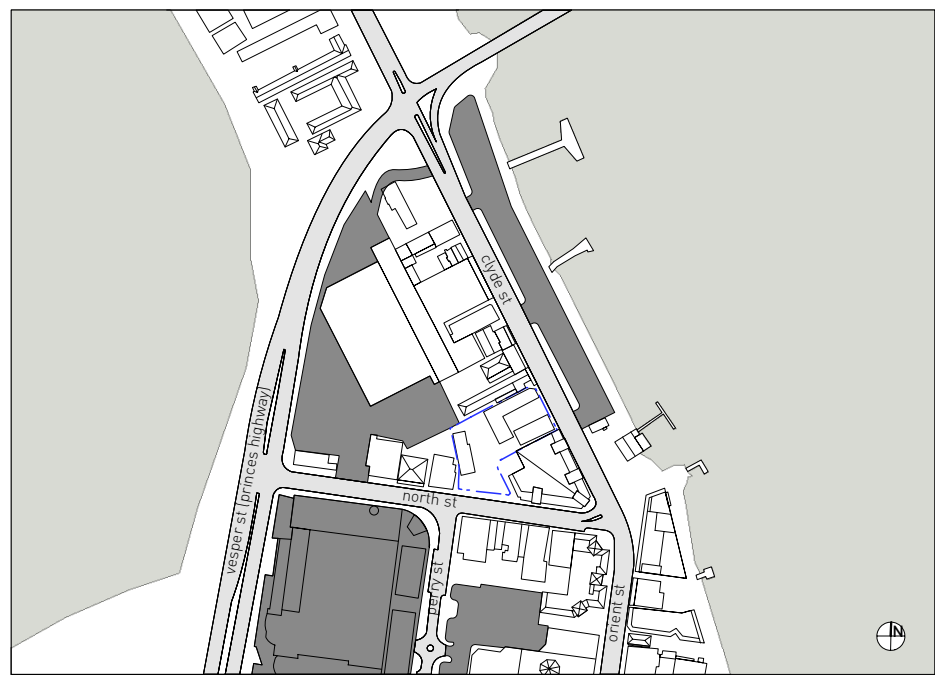
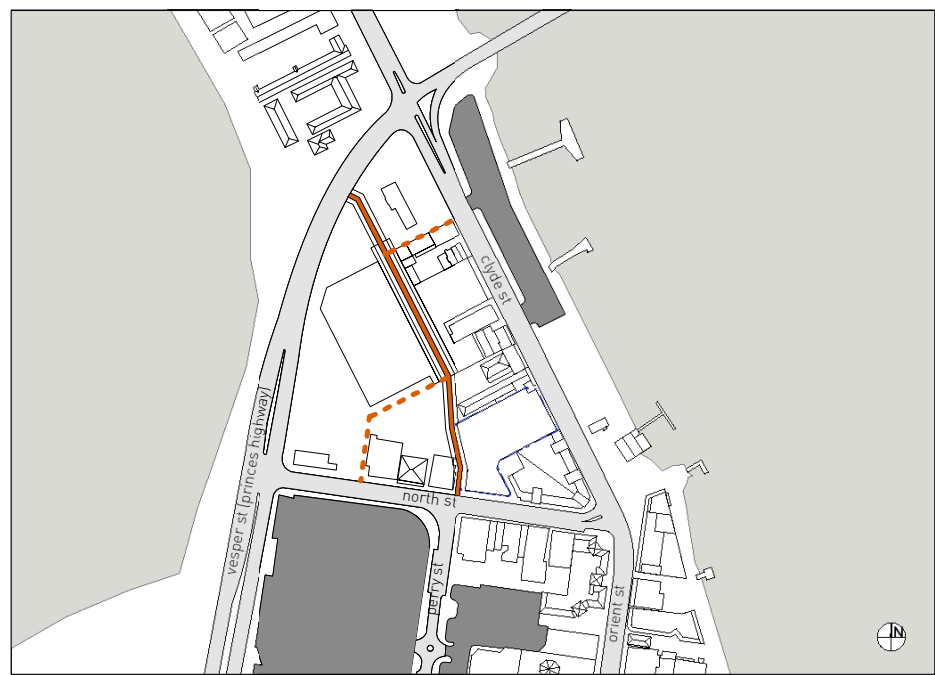
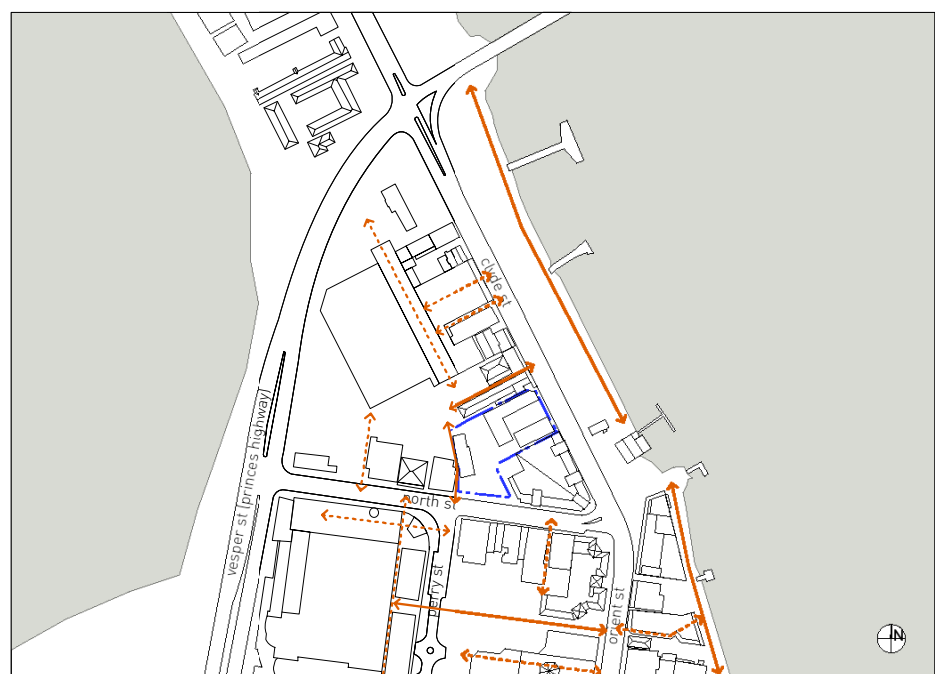




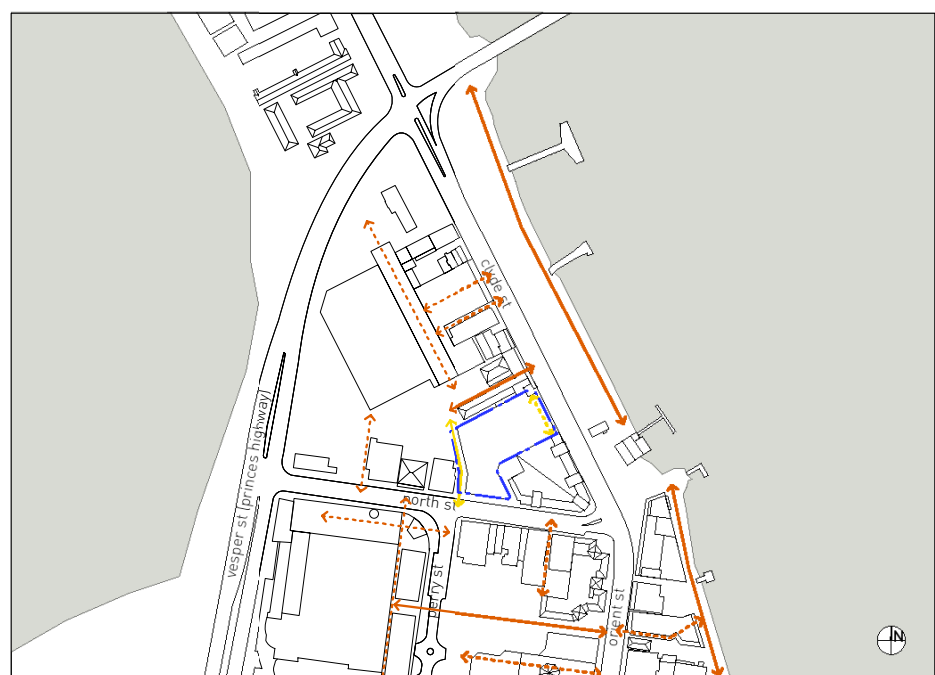
Road Network - Existing scale 1:5000



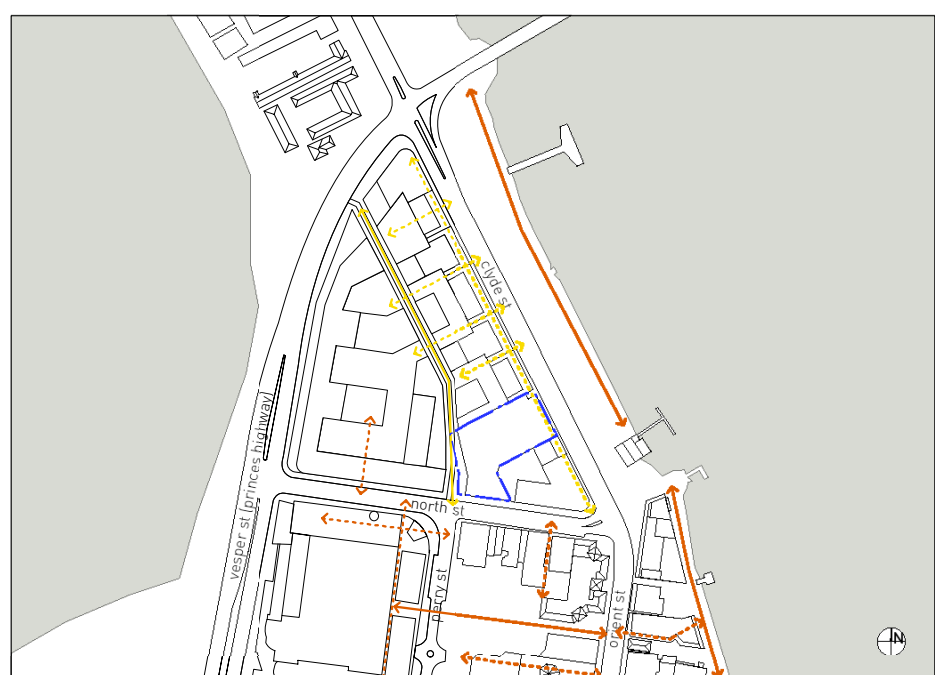
Road Network - Future scale 1:5000



Pedestrian Network - Existing Scale 1:5000



Pedestrian Network - Proposed Scale 1:5000



Pedestrian Network - Future Scale 1:5000

Vehicle Driveway
A driveway is proposed running along the western boundary of the site. This incorporates a council easement of approximately 1800mm and uses 5200mm of the privately owned land of 3 Clyde Street. This will provide vehicle access to the car park and pedestrian access to the rear of the building on 3 Clyde Street. Public pedestrian access through to the shopping centre car park to the north of the site will be maintained and safety and amenity will be dramatically improved.

The building setbacks have been calculated to allow the driveway to become a shared pedestrian and one way vehicle laneway in the future. The laneway would run from North Street to Vesper Street or the north end of Clyde Street. The laneway would offer the potential for a pedestrianised shopping street with single lane vehicle access to the rear of the neighbouring Clyde Street blocks. Vehicular access to the rear of these blocks would be in accordance with the Batemans Bay Draft Structure Plan, that states that the Clyde Street frontage should not be broken up with vehicle driveways.

- alternative future lane
- future lane
- car parking

Pedestrian Links.
Batemans Bay Town Centre has an existing network of pedestrian linkages through private sites, either through open space, easements and car parks or in covered arcades and malls. It is considered desirable to continue that pattern in all new development. The pedestrian link along the western boundary of 3 Clyde St will be considerably improved by the widening and new architectural treatment of the lane. It is not considered necessary to create a public pedestrian link from Clyde St through to the lane on the 3 Clyde St because of it's proximity to the corner, though residents of the development will be able to traverse the courtyard at podium level from Clyde St to the lane.

- new pedestrian linkages - private land
- pedestrian linkages - public land
- pedestrian linkages - private land

top of building with 6m setback from street

top of building at street facade

Upper floors of buildings to a height of 12m above natural ground level to extend out over the colonnade to the property boundary

flood planning level - AHD 2.7m
existing pavement level

Areas not used for ramps and stairs can be used for tables and chairs for outdoor dining. Tables and chairs can also be placed on the footpath in exchange for providing the colonnade.

continuous access is provided between lots at flood planning level

facade line of building

Ramps for disabled access can serve multiple properties as continuous level access is provided by the colonnade. Reducing the number of ramps required and making it possible for narrower lots that don't have the frontage to provide disabled access accessible to disabled persons.

Stairs between levels provided by every lot fronting Clyde Street

Stage 1 - hard paved pedestrian zone (vehicle access for servicing plant equipment only)
Stage 2 - 1 way laneway connecting to neighbouring sites and exiting to North St. with Separated pedestrian zone

main switch room,
mechanical switch room
+ comms room

Stage 1- shared driveway and pedestrian zone with 2 way vehicle access to carpark
Stage 2- one way traffic along future laneway with separated pedestrian access

NORTH STREET

Colonnade in Section scale 1:200

Colonnade

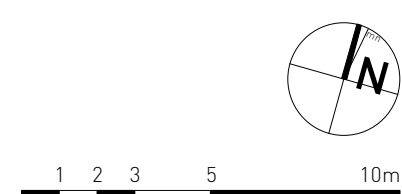
The Batemans Bay Coastline Hazard Management Plan has identified the need for all retail fronting Clyde Street to have a minimum floor level of 2.7m AHD. The flood planning level is an average of 700mm above the existing level of the footpath. Whilst being a very pragmatic form of dealing with the possibility of flood inundation the change in levels causes several problems for streetscape and pedestrian access. In response to these issues the NSW Department of Planning and Eurobodalla Shire Council have decided to implement a colonnade along the street frontages of all buildings along Clyde St.

Each new building fronting Clyde Street is required to use the first 1.5 meters of their site along the Clyde Street frontage at ground level to provide stairs and or disabled access ramps to allow pedestrians to negotiate the change in levels. The next 4 meters of the site are to be used to provide a continuous covered walkway between sites at retail flood planning level.

In exchange for providing the colonnade Council has agreed to allow retailers to us an equivalent area of footpath to that used for the provision of ramps and stairs for tables and chairs to allow outdoor dining along Clyde Street.

Colonnade in Plan scale 1:200

Ground Floor Plan scale 1:200



key

- studio apartment
- 1 bed apartment
- 1 bed terrace apartment
- 2 bed terrace apartment
- 3 bed terrace apartment
- reception + conferencing
- back of house
- retail
- circulation
- plant and storage zones
- fh fire hydrant
- fhr fire hose reel
- pfe portable fire extinguisher
- ffs first flush diverter
- dp down pipe
- hp heat pump hot water service
- AHD australian height datum
- FFL finished floor level
- FRL finished roof level
- FGL finished ground level

Consultants

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Ph: 02 9810 6911
Geotechnical Engineer - Douglas Partners
Ph: 02 9271 1838
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Landscape Architect - Ayling + Drury Landscape Architecture
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Project	Clyde River Motor Inn Lot 4 DP 585 556
Issue	Development Approval
Date	Monday, 28 April 2008 9:58:03 AM
Scale	1:200 @ A1
Drawing	Ground Floor Plan
Dwg No.	DA 03 - 02