

SSDA RESPONSE TO SUBMISSIONS REPORT



MELROSE PARK SOUTH – EAST – BLOCK 05

REPORT PREPARED BY FK ON BEHALF OF WHARF AND HUGHES DEVELOPMENTS PTY LTD

JAN 2026



ARTIST IMPRESSION

FK acknowledges the Traditional Custodians of the land on which we operate, live and gather. We recognise their continuing connection to land, water and community. We pay respect to Elders past and present and extend that respect to all Aboriginal and Torres Strait Islander peoples today.

The FK team is made up of 120 designers and architects located across our three Australian studios in Melbourne, Sydney and Brisbane. Our practice is diverse and built around a broad and varied project portfolio including residential, commercial, hospitality, and retail projects.

Our team is built on a strong and trusted foundation in the architecture and design industry. Our knowledge and expertise spans decades, while our experience and body of work is highly valued for its contribution and longstanding impact within the industry. Our unique offer includes a highly sophisticated integration of our architectural and interior design teams which allows us to take on and deliver projects of all scales, across all disciplines.



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EXECUTIVE SUMMARY

EXECUTIVE SUMMARY

PURPOSE OF THIS REPORT

This report has been compiled to respond to concerns raised in the Key Issues letter provided by DPHI and the Council Submissions. FK endevours with the support of the entire consultant group to see Melrose Park South – East become a thriving, livable and sustainable part of greater Sydney

This report will focus on Block 05 and will accompany reports from Block 04 + 06.

PROJECT LOCATION

Melrose Park South – East is a mixed-use development located at the addresses 112 Wharf Road, 30 & 32 Waratah Street, Melrose Park NSW 2114, on land zoned as R4 high-density residential and RE1 Public Recreation under the Parramatta Local Environmental Plan 2023.

The site is located within the City of Parramatta LGA and is bounded by industrial properties to the west, Mary Street to the north, Wharf Road to the east, Waratah Street to the southeast, and Ermington Bay to the south, and is approximately 4.2ha.

PROJECT DESCRIPTION

FK was engaged by Wharf and Hughes Developments Pty Ltd. to provide architectural services for Melrose Park South – East Block 5 at 112 Wharf Road, 30& 32 Waratah Street, Melrose Park.



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DPHI KEY ISSUES LETTER



AFFORDABLE HOUSING GFA CONFIRMATIONS

EAST PRECINCT TOTAL GFA: 94, 433m²
SEPP AFFORDABLE GFA : 14, 222m²
15%

LEGEND

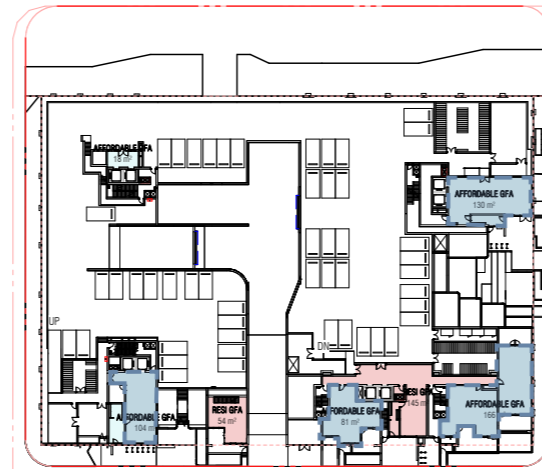
- RESIDENTIAL GFA
INCLUDES VPA AFFORDABLE
- AFFORDABLE GFA
- OUTLINE OF VPA+SEPP AFFORDABLE HOUSING

DPHI ISSUES

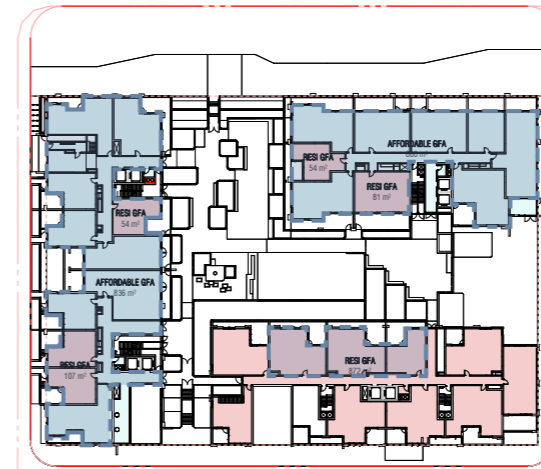
- The amount of affordable housing GFA provided under the Housing SEPP is only 14.17%. This must be increased to a minimum of 15%, excluding the affordable housing GFA to be delivered under the Voluntary Planning Agreement (VPA). Provide revised plans and calculations confirming a minimum 15% affordable housing GFA is achieved. Note any change to the Housing SEPP affordable housing numbers will require an updated letter of agreement from the Community Housing Provider and updated car parking calculations.
- Please review the GFA and affordable housing GFA calculations in relation to:
 - the architectural floorplans for Block B5 indicate GFA on basement levels 1 and 2 that are not included in the GFA calculations on drawing DA-E-B5-AR-500
 - drawing DA-E-B5-AR-500 indicates the GFA of lobbies and corridors that also serve market apartments (up to level 1) have been allocated entirely as affordable housing GFA.

FK RESPONSE

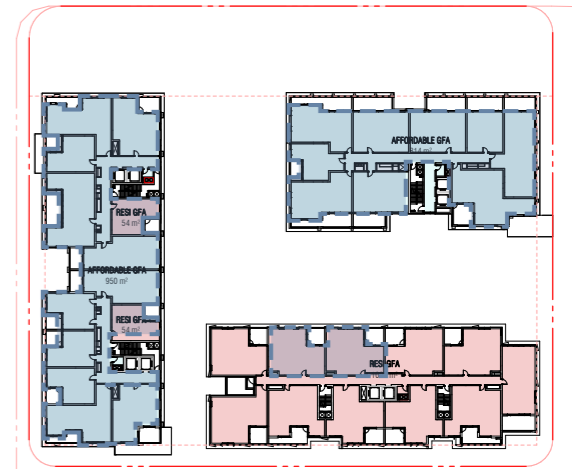
- An additional 2-bedroom affordable housing unit has been incorporated on Level 06. Architectural plans have been updated confirming the revised affordable housing GFA and allocation, which now achieve a minimum of 15% affordable housing GFA in accordance with the Housing SEPP requirements across the Eastern Precinct.
- The lift lobbies shown on Basement Levels 01 and 02 in the architectural floor plans have been removed and are not included in the GFA calculations. Please refer to the corresponding basement floor plans for clarification.
- The GFA associated with lobbies and corridors is allocated to affordable housing only where the relevant building level is fully occupied by affordable housing dwellings (whether delivered under the Housing SEPP or the VPA). Where these common areas also serve market apartments, the GFA has not been attributed to affordable housing. Drawings DA-E-B5-AR-500 and DA-E-B5-AR-501 provide the updated GFA breakdown and allocation.



GFA - LOWER GROUND FLOOR LEVEL (B5)



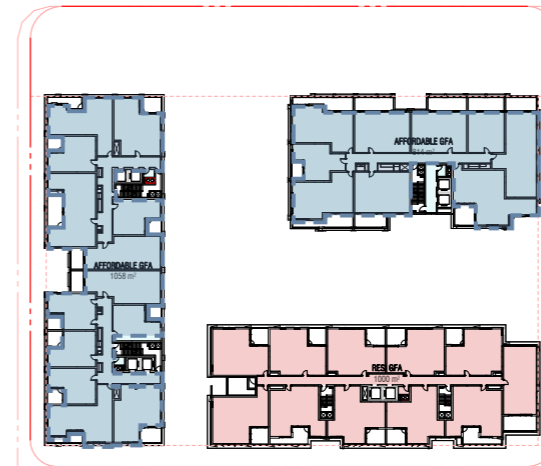
GFA - GROUND FLOOR LEVEL (B5)



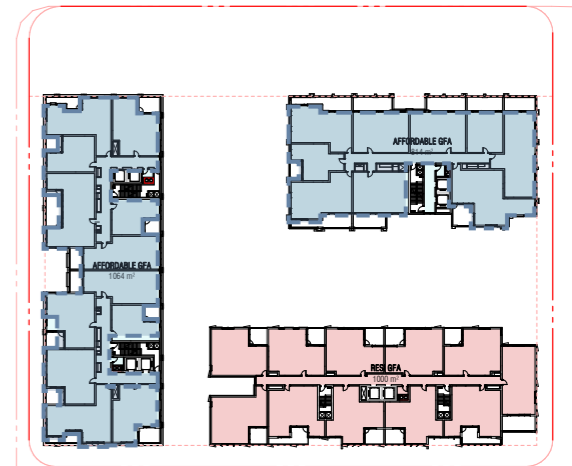
GFA - LEVEL 01 (B5)



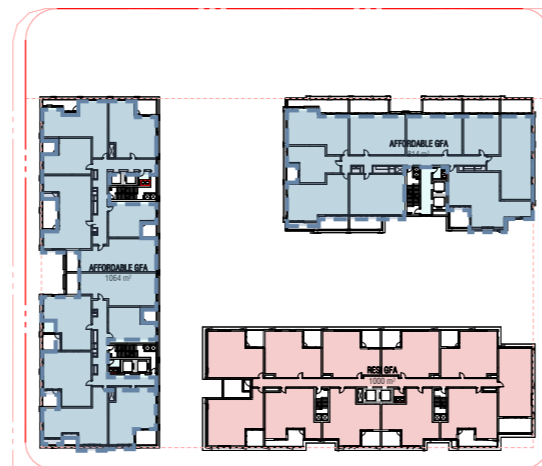
GFA - LEVEL 02 (B5)



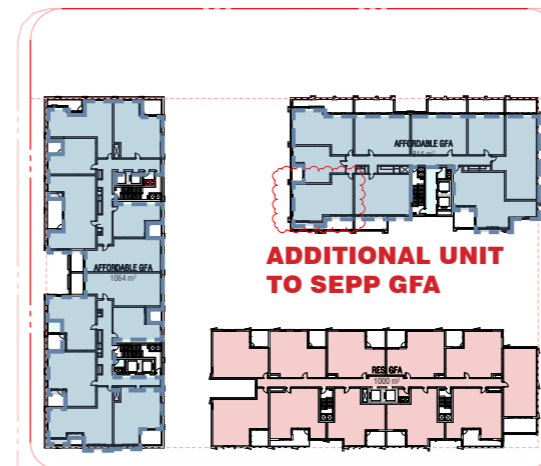
GFA - LEVEL 03 (B5)



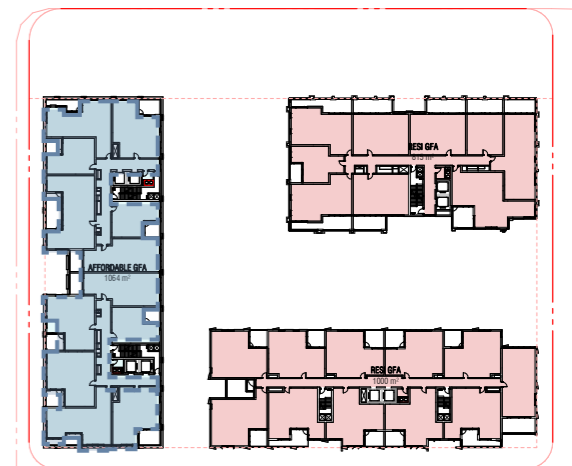
GFA - LEVEL 04 (B5)



GFA - LEVEL 05 (B5)



GFA - LEVEL 06 (B5)



GFA - LEVEL 07 (B5)

DA-E-B5-AR-500 COMPLIANCE - GFA
 (REFER ALSO DA-E-B5-AR-501 AFFORDABLE HOUSING ALLOCATION)



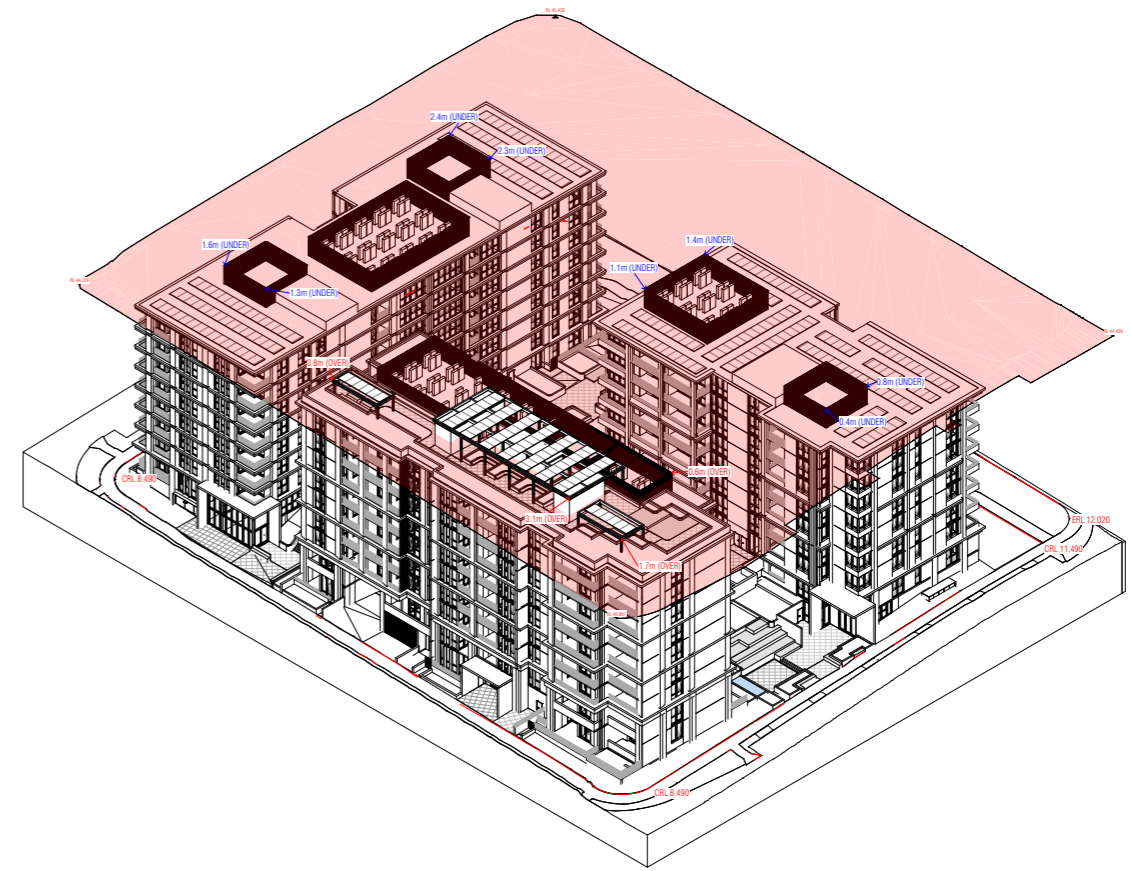
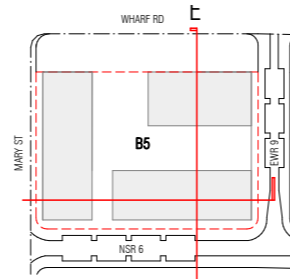
ARCHITECTURE ROOF FEATURE

DPHI ISSUES

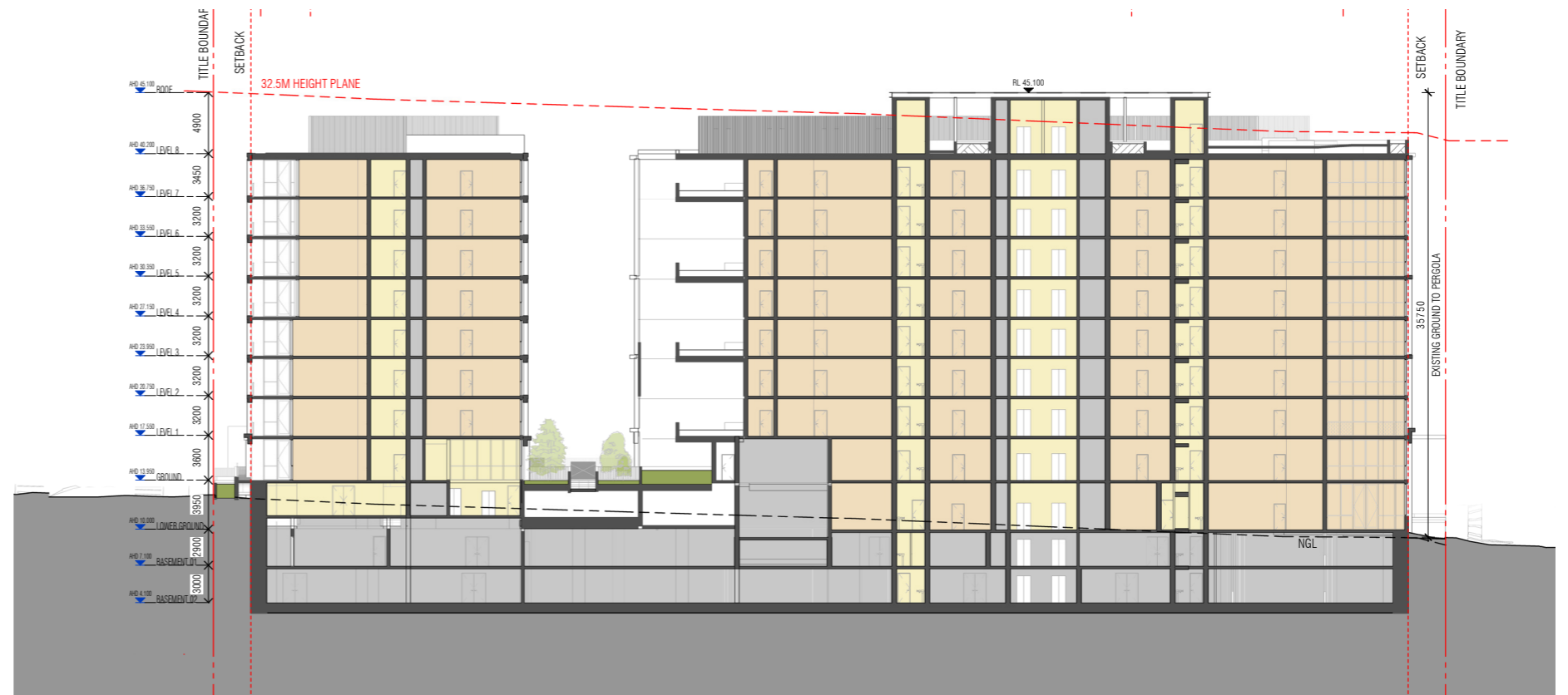
- The Department raises concern that the proposed roof top structure on Block B5 does not satisfy the provisions of clause 5.6(3) of the Parramatta Local Environmental Plan 2023 and therefore does not constitute an architectural roof feature. The Department also raises concern in relation to the design integration, scale and shadow impacts of this structure element and recommends it be redesigned, reduced in scale or eliminated from the proposal.
- Should any structure be retained above the building height control, you will need to provide a long section indicating the proposed height, as well as a clause 4.6 variation request.
- Provide a 3D image of the height plane for Block B5 (as per drawing SSDA 508 for Block B4).
- Ensure elevations and sections include the existing ground line and existing and proposed RLs.

FK RESPONSE

- We acknowledge the Department's concerns regarding the rooftop structure on Block B5 and its compliance with Clause 5.6(3) of the Parramatta Local Environmental Plan 2023. The proposal reduces the extent of roofing to reduced and setback from the parapet edge to reduce the bulk and scale presented to the street
- As the structure exceeds the building height control, a Clause 4.6 variation request will be prepared to address the non-compliance and to demonstrate that the proposal achieves an appropriate planning outcome notwithstanding the numerical breach.
- Refer DA-E-B5-AR-252 illustrating the proposed height of the rooftop structure relative to the height control has also been provided in support of the variation request.



DA-E-B5-AR-512 HEIGHT PLANE DIAGRAM



DA-E-B5-AR-252 SECTIONS (SECTION F)



9 am to 3 pm		
2 Hr	-	137 (53%)
15 Mins+	-	66 (25%)
0 Hr	-	57 (22%)
Total	-	260

8:30 am to 3:30 pm		
2 Hr	-	165 (64%)
15 Mins+	-	41 (15%)
0 Hr	-	54 (21%)
Total	-	260

LEGEND

- 2 HOUR SOLAR 9 AM - 3 PM
- 15 MIN + HOUR SOLAR 9 AM - 3 PM
- 0 HOUR SOLAR 9 AM - 3 PM
- 15MIN + SOLAR FROM 8:30 AM - 3:30 PM
- 2 HOUR + SOLAR FROM 8:30 AM - 3:30 PM

SOLAR

DPHI ISSUES

- Solar access performance across each block and the overall development does not meet the Apartment Design Guide (ADG) solar access recommendations. The Department notes 70% solar access in midwinter is still not achieved across the development, even with consideration of an additional hour in midwinter (8.30 am to 3.30 pm). You are therefore requested to:
 - Further investigate the design to achieve an improved solar access outcome
 - Confirm the design measures undertaken to maximise solar access within each block to living areas/private open space and to communal open space.

FK RESPONSE

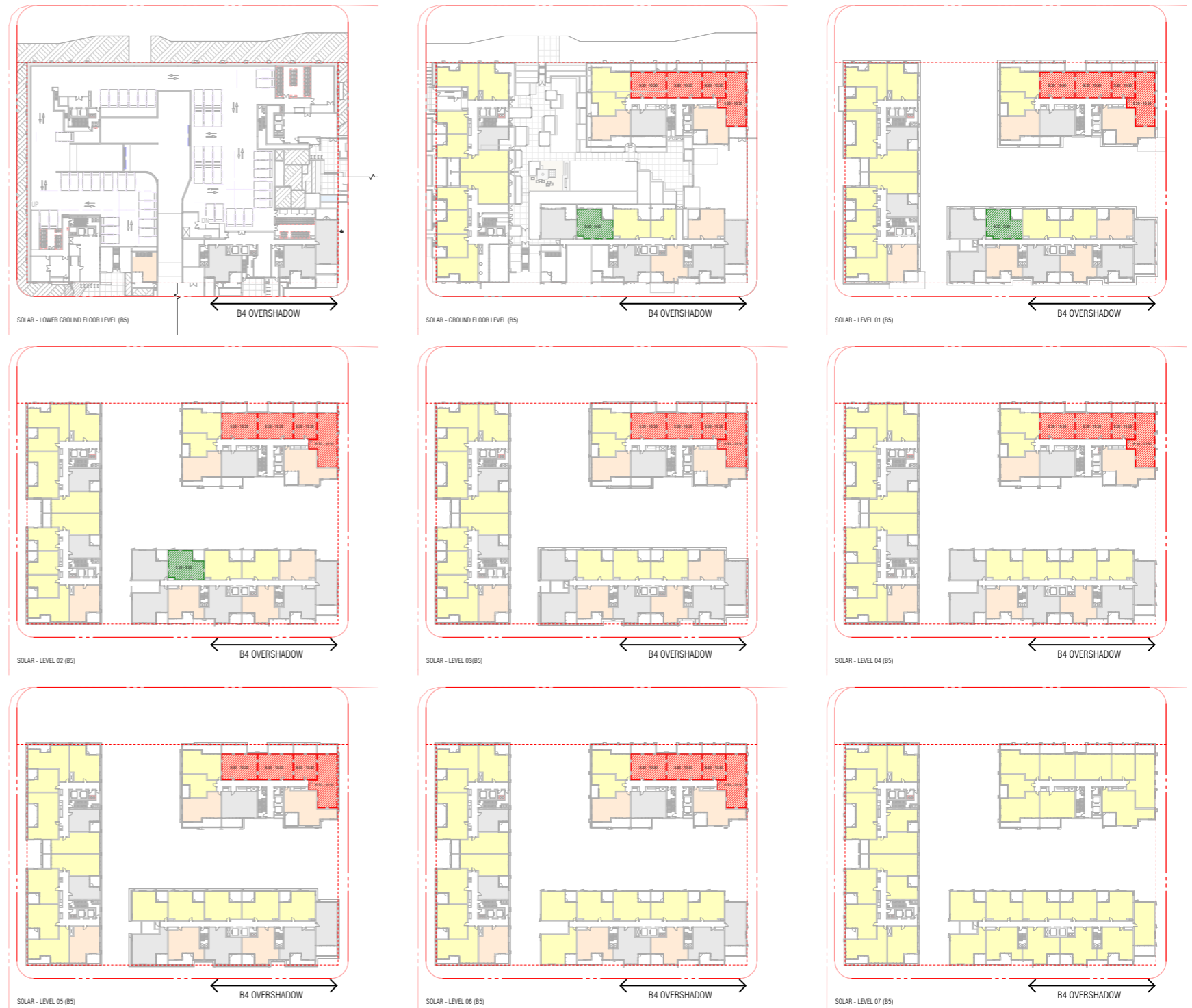
While the proposal does not fully achieve the ADG's 70% mid-winter solar access benchmark, the design reflects the maximum feasible outcome given site orientation, and surrounding context.

FK have incorporated multiple strategies to improve solar access, including optimised building orientation, prioritisation of northern/eastern aspects for living areas, and refined massing and courtyards to increase sunlight to apartments and communal spaces.

Earlier schemes included stepped building forms that improved solar access; however, these were removed following DRP to achieve a calmer built form more fitting with the precinct.

Further precinct development from the Design Review Panel's feedback led to adopting the proposed block form. This allowed for greater solar access to B6, which in turn reduced B5's overall performance, prioritising precinctual amenity. (this testing can be seen in the B6 report on solar)

Although full ADG compliance metric has not been achieved, the design intent of optimising residential amenity through solar access has been met. (see section ADG Apartment amenity)



DA-E-B5-AR-502 COMPLIANCE - SOLAR

61%
NAT VENT

ADG NATURAL VENTILATION

DPHI ISSUES

- Review the natural ventilation calculations for each block noting a number of apartments do not appear to be naturally ventilated despite being counted as naturally ventilated in the ADG calculations for each building. Provide justification for any blocks not achieving the ADG natural ventilation recommendations if required following review.
- Provide further information where natural ventilation is proposed via facade articulation.

FK RESPONSE

- The natural ventilation calculations for B5 have been reviewed and verified against the Apartment Design Guide (ADG) criteria. Anywhere compliance is achieved through facade articulation, apartments have been tested by a registered engineer (refer RWDI report). No updates to the cross ventilation apartments calculation as the submitted DA present the correct natural ventilation counts for B5.
- Where natural ventilation is proposed via facade articulation it has been indicated on the drawing, please refer to DA-E-B5-AR-503.

LEGEND

- CROSS VENTILATED APARTMENT
- NOT CROSS VENTILATED APARTMENT
- CROSS VENTILATION ACHIEVED VIA FACADE ARTICULATION



DA-E-B5-AR-503 COMPLIANCE - CROSS VENTILATION

ADG APARTMENT AMENITY

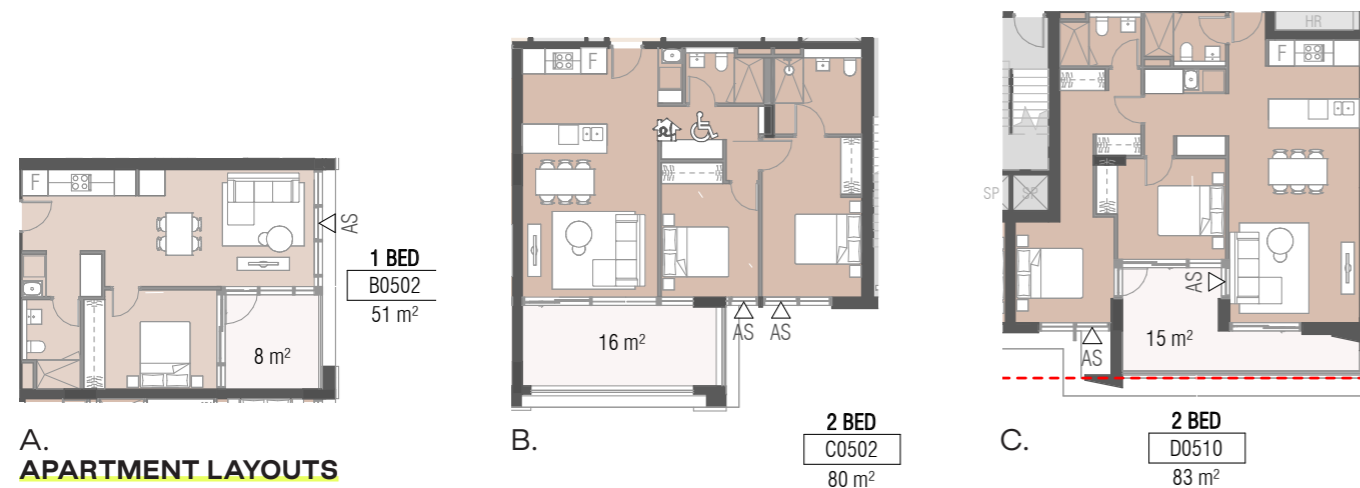
DPHI ISSUES

- Identify apartments that would receive nil solar access and are not cross ventilated and provide further information regarding how these apartments would still achieve an acceptable residential amenity outcome.

FK RESPONSE

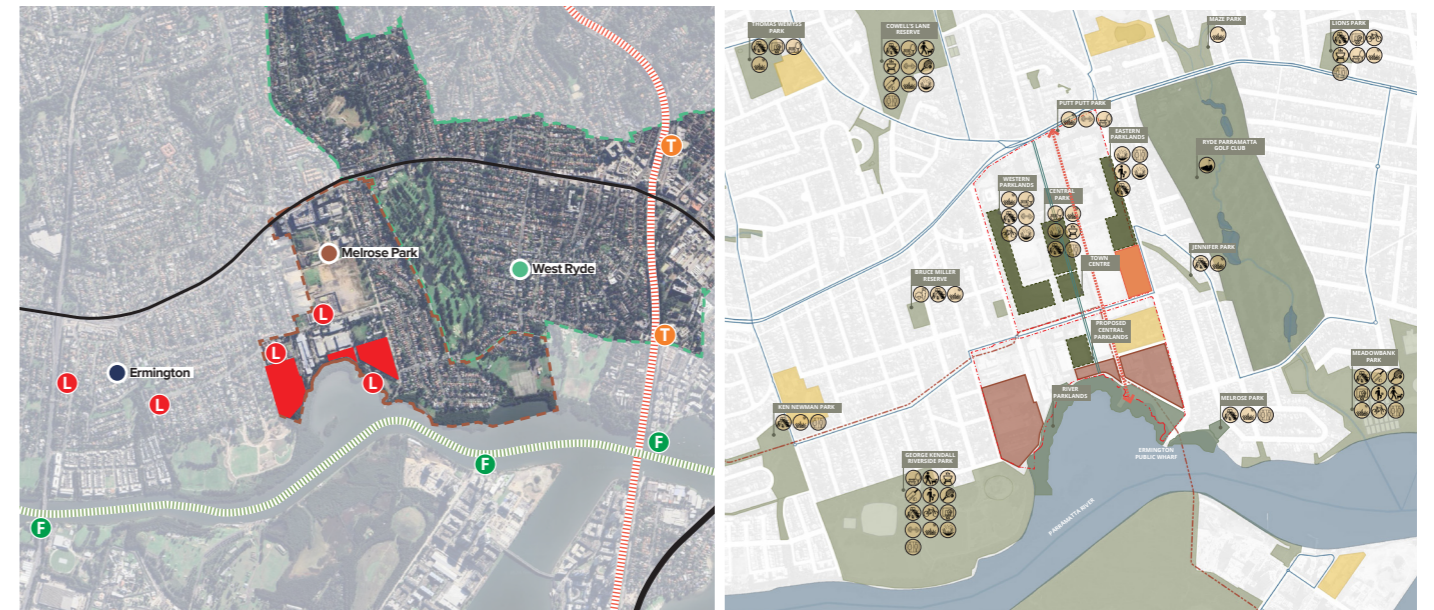
- FK Architects has identified several apartments within Block B5 that receive less than two hours of solar access on 21 June and do not achieve natural cross ventilation. A detailed internal review has been undertaken to evaluate the overall living quality and amenity of these dwellings.

- The findings confirm that these apartments will continue to deliver a high level of residential amenity, supported by the following key factors:
 - The wider precinct provides exceptional lifestyle benefits, including immediate access to convenient public transport, extensive communal open spaces, landscaped podium areas, and a curated mix of retail and community offerings.
 - Residents will enjoy dedicated communal facilities, such as shared communal open space, a rooftop terrace with amenity spaces for relaxation, social gatherings, and community connection.
- The apartments themselves incorporate well-resolved layouts and functional planning, complemented by generous private balconies.
 - Each apartment benefits from a premium outlook, with beautiful river views, open landscape area (OS1) or outlooks toward internal communal gardens.
 - While these apartments may not achieve two hours of direct mid-winter sunlight, they will continue to receive adequate natural daylight across all other periods of the year.
 - To further improve residential amenity the client group has nominated these apartments to also receive ceiling fans throughout the apartment to better manage user comfort



A. **APARTMENT LAYOUTS**

Apartment layouts meet ADG standards, achieving over and above minimum internal areas with the two bedroom units having larger balcony spaces. All apartments have clear sight-lines from the entry to the external facade. Open plan Kitchen/living/dining spaces are provided to all typologies. Private and utilities spaces are separated from public living spaces.



PG 17-19 ARCADIA REPORT - GREATER PRECINCT TRANSPORT AND PUBLIC OPEN SPACE

Melrose Park South is a highly connected precinct that is highly serviced by residential amenities. Not only within the proposal but also within the local area as well. Parks, nature reserves and lifestyle opportunities are abundant.



APARTMENT STACKS WITH LIMITED SOLAR AND CROSS VENTILATION

Apartments demonstrated above will be provided with ceiling fans throughout to improve their ventilation performance. Generally all of the apartments are able to leverage quality views from their living spaces towards internal or external views.



ADG DEEP SOIL AND COMMUNAL OPEN SPACE

DPHI ISSUES

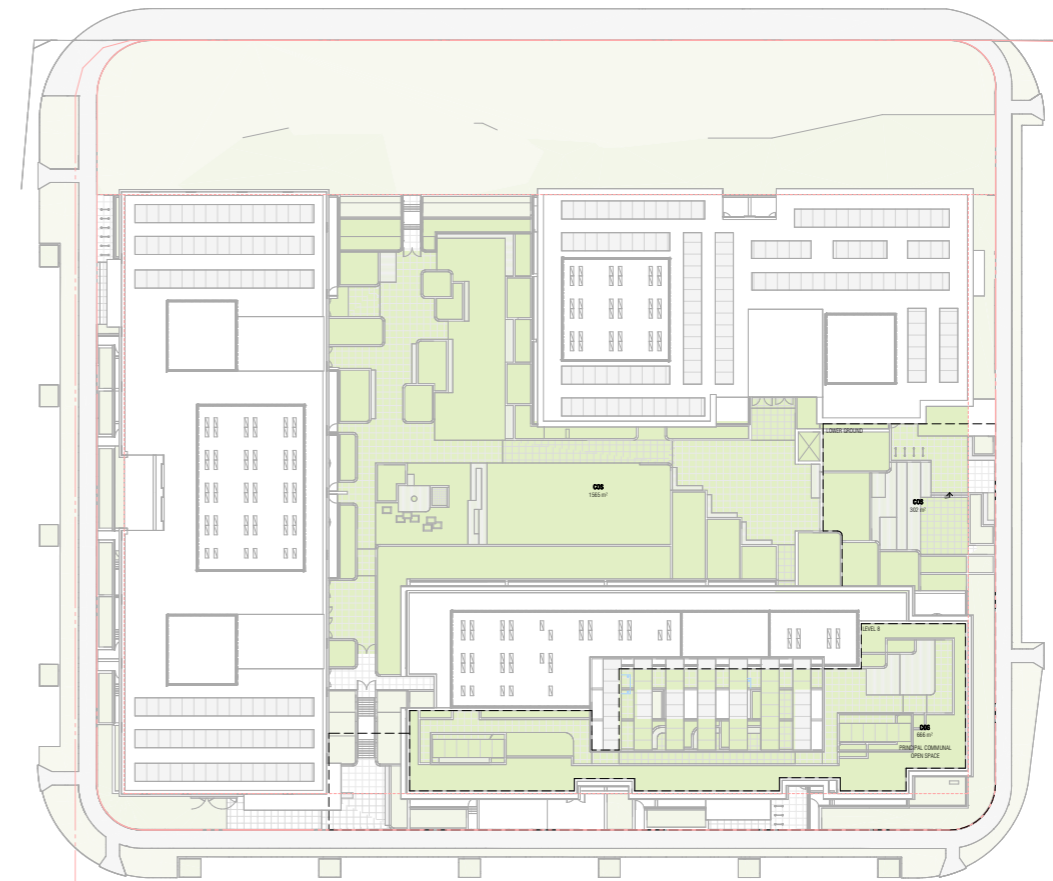
- Provide a table confirming deep soil zones (with a minimum dimension of 6 m and separately, between 3 m and 6 m) and landscaped areas within each block, the overall developable area of the site and the overall site.
- Confirm rooftop communal open space areas would be available to residents of each building within the respective block.

FK RESPONSE

- Table which indicates deep soil zones (with a minimum dimension of 6 m and separately, between 3 m and 6 m) and landscaped areas within B5 has been provided, please refer to DA-E-B5-AR-504 and DA-E-B5-AR-505.
- Rooftop communal open space areas would be available to residents of each building within B5.
- Refer planner's report for justification of deep soil allocation having been part of the Open Space offering provided as part of the SSDA

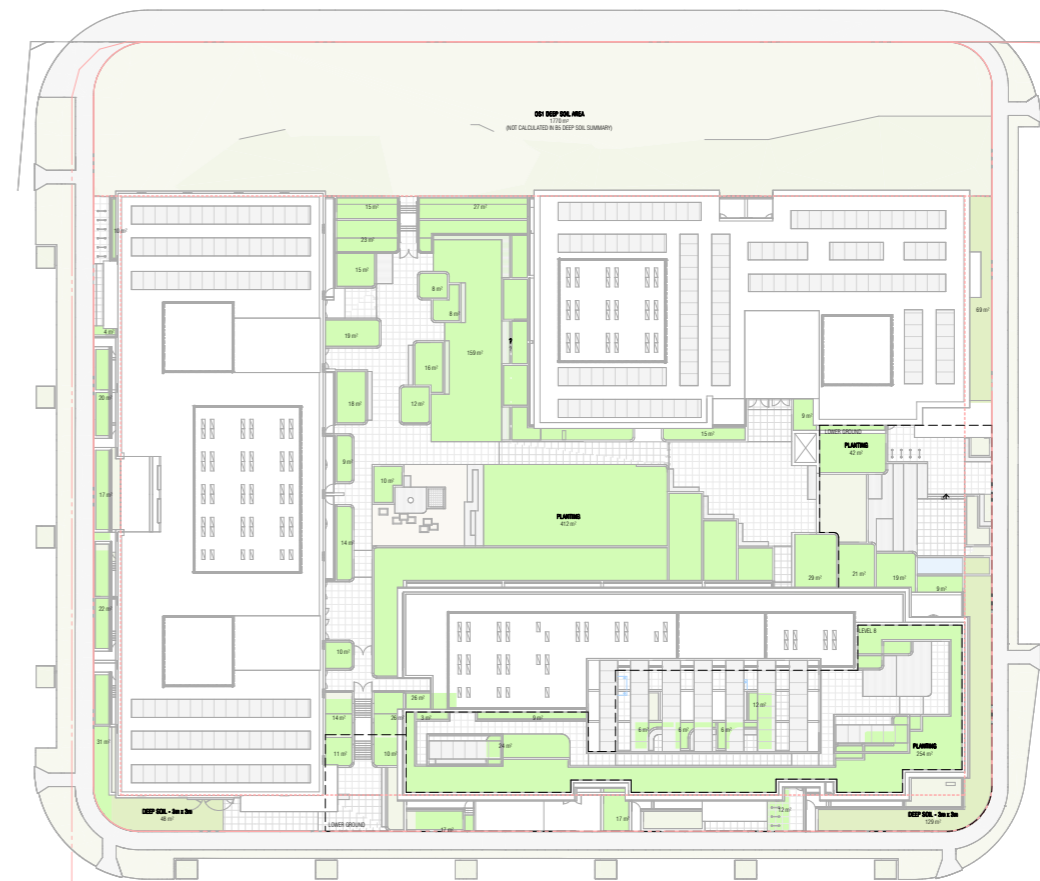
COMMUNAL OPEN SPACE	
LEVEL	AREA
GROUND	1565 m ²
LEVEL 08	666 m ²
LOWER GROUND	302 m ²
TOTAL	2533 m ²

AREA SCHEDULE - DEEP SOIL & PLANTING	
AREA TYPE	AREA
PLANTING	159 m ²
GROUND	
DEEP SOIL - 3m x 3m	69 m ²
PLANTING	897 m ²
LEVEL 08	
PLANTING	319 m ²
LOWER GROUND	
DEEP SOIL - 3m x 3m	178 m ²
PLANTING	158 m ²
TOTAL	1780 m ²



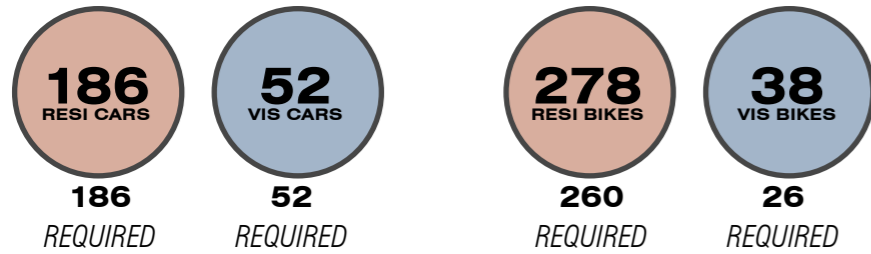
LEGEND
 COMMUNAL OPEN SPACE

DA-E-B5-AR-504 COMPLIANCE - C.O.S



LEGEND
 DEEP SOIL - >3M X 3M
 DEEP SOIL COMPLIANT - >6M X 6M
 PLANTING

DA-E-B5-AR-505 COMPLIANCE - D.S.Z & PLANTING



VISITOR BICYCLE PARKING

RESPONDS ALSO TO ISSUES RAISED IN COUNCIL SUBMISSIONS

DPHI ISSUES

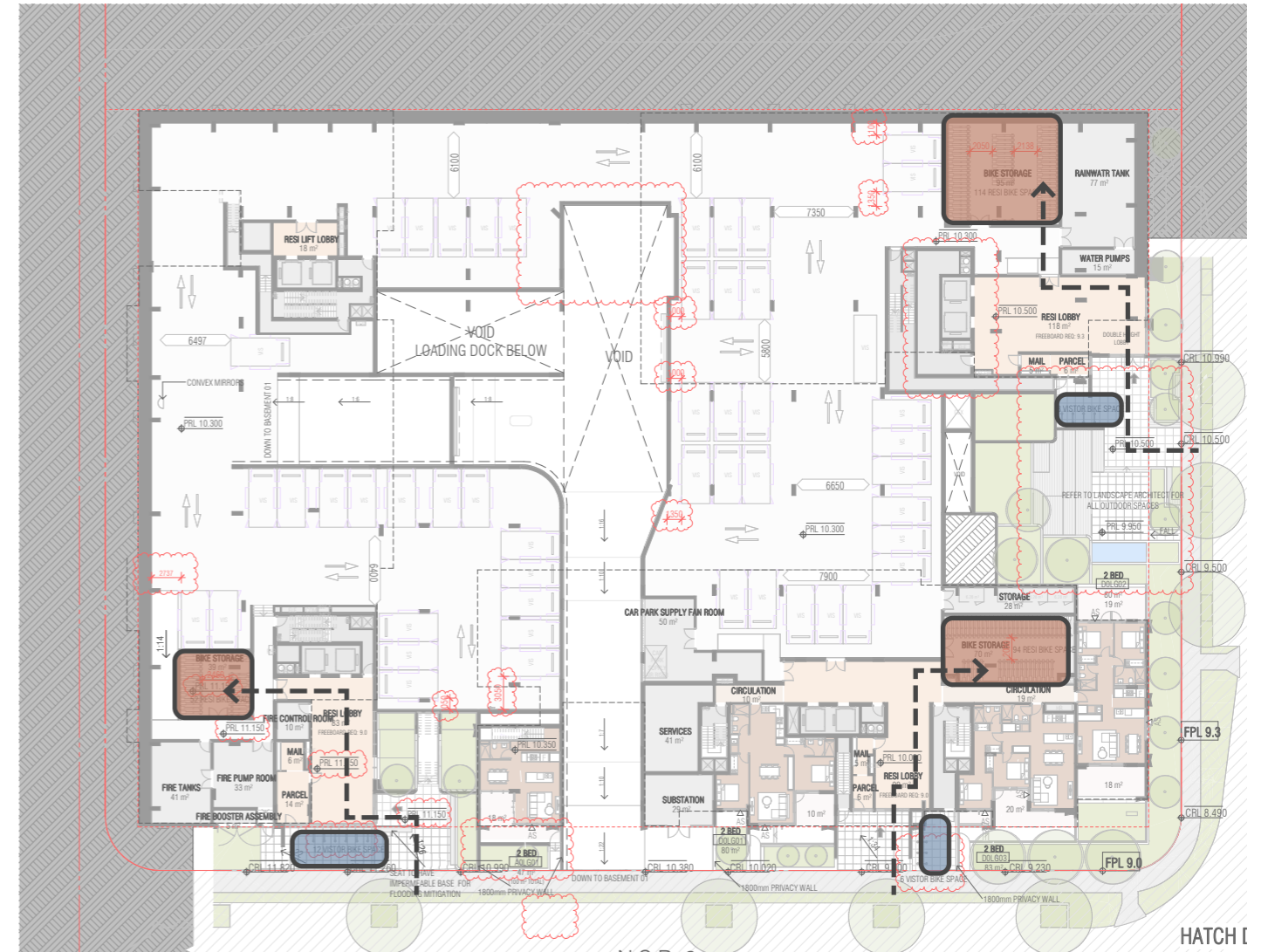
- Review the location of residential visitor bicycle parking spaces to ensure they are located as close to ground level as possible within each block.

SUBMISSION ISSUES

- For all 3 Blocks, the incline of bicycle parking egress ramp(s) must not exceed 8% for a ramp length of up to 20m or 7% for a ramp length of up to 40m as per Austroads Guide to Road Design Part 6A: Pedestrian and Cyclist Paths, Figure 7.1: Desirable uphill gradients for ease of cycling, to encourage use by slower, less confident 15 percentile recreational bicycle riders per Cycling Aspects of Austroads Guides, Figure 4.1: Bicycle operating speeds.
- To encourage active transport, Council recommends that all bicycle parking facilities (across all blocks) be provided at basement level 1 for ease of access.

FK RESPONSE

- Location of residential visitor bicycle parking spaces are located at the lobby entries external to the basements. Please refer to DA-E-B5-AR-099 and DA-E-B5-AR-100. This is consistent with Council's recommendation to provide convenient access that supports and encourages active transport.
- No access will be granted to cyclists via the traffic ramp. The client has confirmed all access to the basements shall be via the lobbies/lifts. Where possible residential bicycle parking is on the lower ground basement and localized near lifting/lobbies



DA-E-B5-AR-099 LOWER GROUND FLOOR PLAN

- Residential bicycle parking (Access via lobbies)
- Visitor bicycle parking (External to lobbies)
- Access pathways

3

COUNCIL SUBMISSIONS

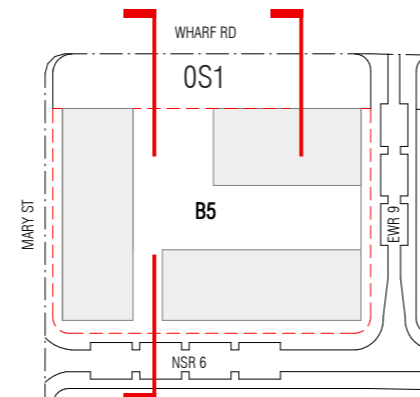
STREETSCAPE INTERFACE SECTIONS

SUBMISSION ISSUES

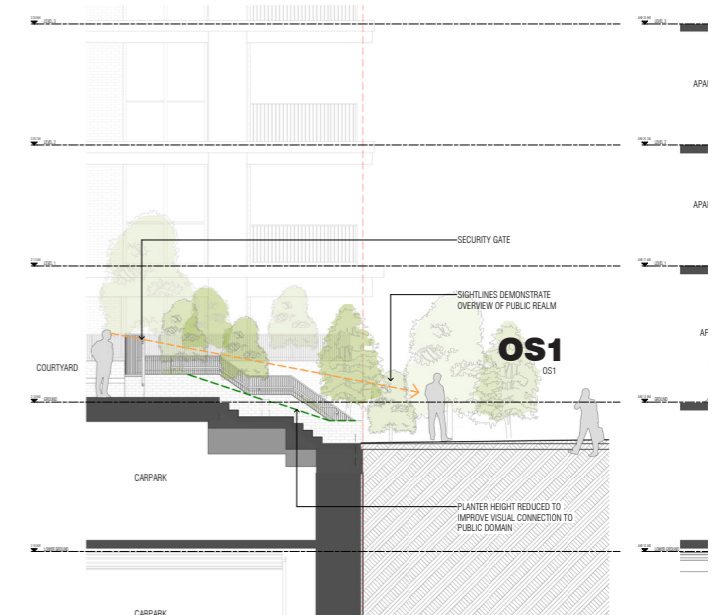
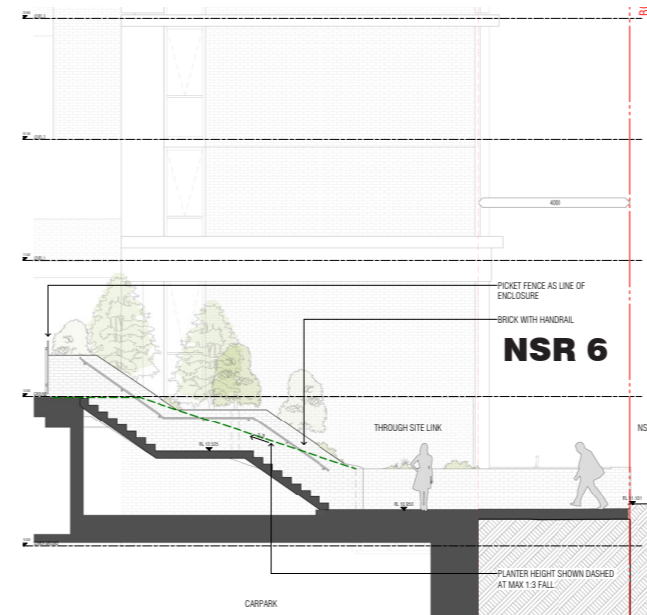
- The proposal fails to achieve design excellence creating a poor streetscape to Wharf Road.
- Basements are located 1m to 3m above the streets and park frontages creating a poor streetscape. To ensure that the basements are integrated into the landscape they are to be located so that the top of the basements are level with most of the streets surrounding the block.[...]
- Communal courtyards are located on the top of the basements so that views from the streets to the courtyards (EW Rd 9, NS Rd 6 and the Wharf Road Gardens) are limited by the stairs and solid walls.

FK RESPONSE

- The updated design integrates landscape terrain mounding up to the building edge, softening the interface with Wharf Road while maintaining privacy for residents. This approach enhances the pedestrian experience and contributes to a cohesive and visually appealing streetscape, as well as mitigating portions of the basement projection.
- The courtyards are intentionally elevated so that views from both the buildings and courtyards are lifted above street level, improving privacy for residents while maintaining the intent of the ADG section 4L-2 Ground floor apartments. Landscape and level changes are sensitively designed to maintain filtered visual connections to surrounding streets and open spaces while providing residents with usable communal areas

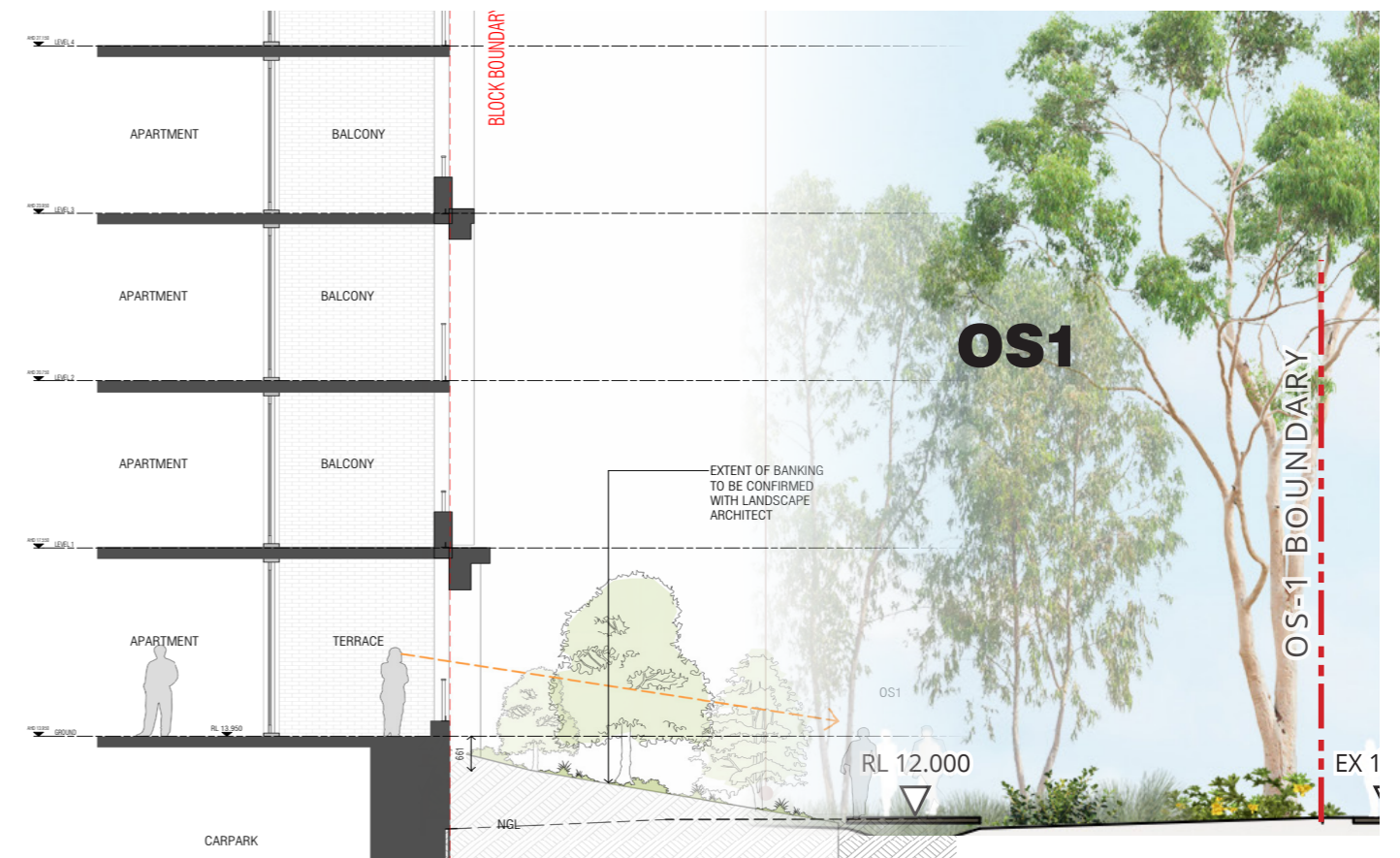


- The courtyards have been designed as multifunctional open spaces that balance circulation requirements with areas for recreation, passive enjoyment, and planting. Level changes are minimised where possible and integrated into the landscape to ensure the courtyards remain functional, attractive, and well-connected.



B5 COS ENTRIES DA-E-B5-AR-255 SERIES

Planters are reduced in height towards street fronts, landscaping falls to the street to reduce visible planter walls and enable visual connections between public and private areas.



BLOCK 5 AND OS1 CONNECTION DA-E-B5-AR-255 SERIES

To retain the internal RL for privacy and flooding levels, the external landscape is manipulated to alleviate the presence of solid walls to the public domain. Heights will vary as the paths meander through OS1 however this will bring the design in line with the intent of the DCP standards.

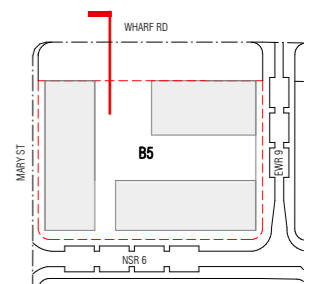


SUBMITTED | EASTERN COS ENTRY



AMENDED | EASTERN COS ENTRY

Planters are reduced in height towards street fronts, landscaping falls to the street to reduce visible planter walls and improve visual connections between public and private areas.



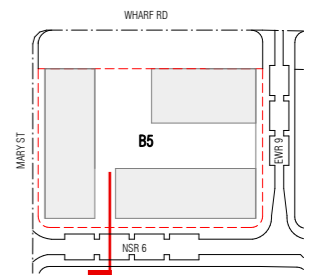


SUBMITTED | WESTERN COS ENTRY



AMENDED | WESTERN COS ENTRY

Planters are reduced in height towards street fronts, landscaping falls to the street to reduce visible planter walls and improve visual connections between public and private areas.



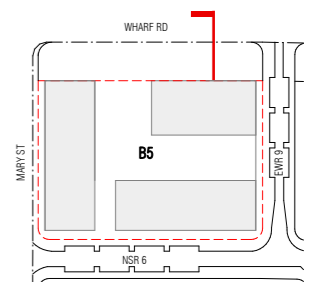


SUBMITTED | S-EASTERN BUILDING INTERFACE



AMENDED | S-EASTERN BUILDING INTERFACE

To retain the internal RL for privacy and flooding levels, the external landscape is manipulated to alleviate the presence of solid walls to the public domain. Heights will vary as the paths meander through OS1 however this will bring the design in line with the intent of the DCP standards.



STREETSCAPE

BLOCK LAYOUT AND FORMING

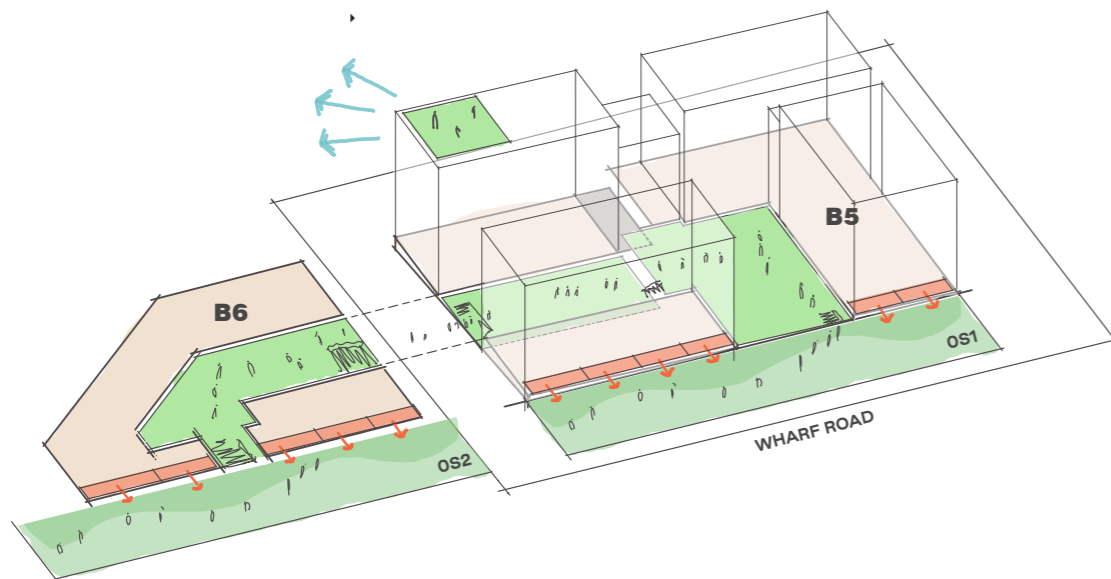
SUBMISSION ISSUES

- The proposal does not relate to the development on Blocks 4 and 6 to form legible streets that provide a discrete, disciplined edge the foreshore parklands and riverine environment.
- The development has a 9-storey wall of apartments approximately 70 metres long facing Wharf Rd on the 93 metre frontage.

FK RESPONSE

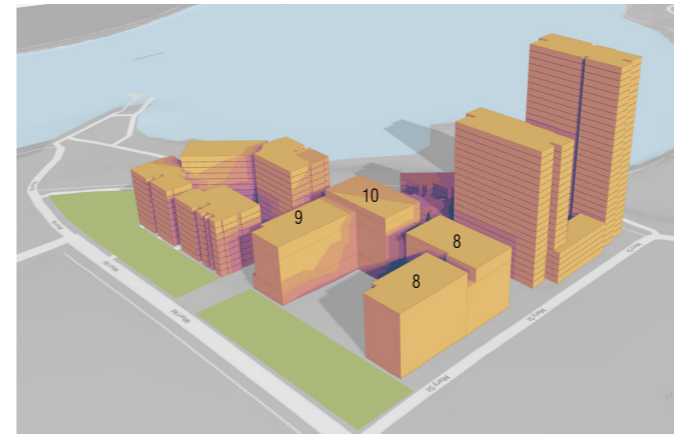
- The layout responds positively to the surrounding development, with the Design Review Panel preferring the current orientation of the southern block over the C-shape identified in the Reference Design. This configuration strengthens connections to Block 6 and leverages the significant open space network provided as part of the MPS, including the east, west, and broader precinct parklands, resulting in a legible and cohesive streetscape structure.

- While the building presents a long frontage to Wharf Road, the proposal incorporates strategic building breaks to reduce visual massing and improve articulation. These breaks, in combination with the landscape strategies and façade variation, significantly reduce the perception of bulk and provide a more human-scaled interface.



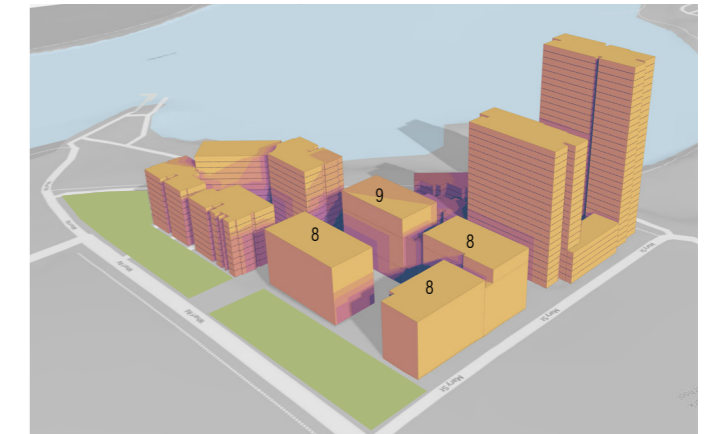
ALIGNED COMMUNAL SPACE

The proposed block layouts following the initial Design Review Panel allowed for continuity through both B5+B6



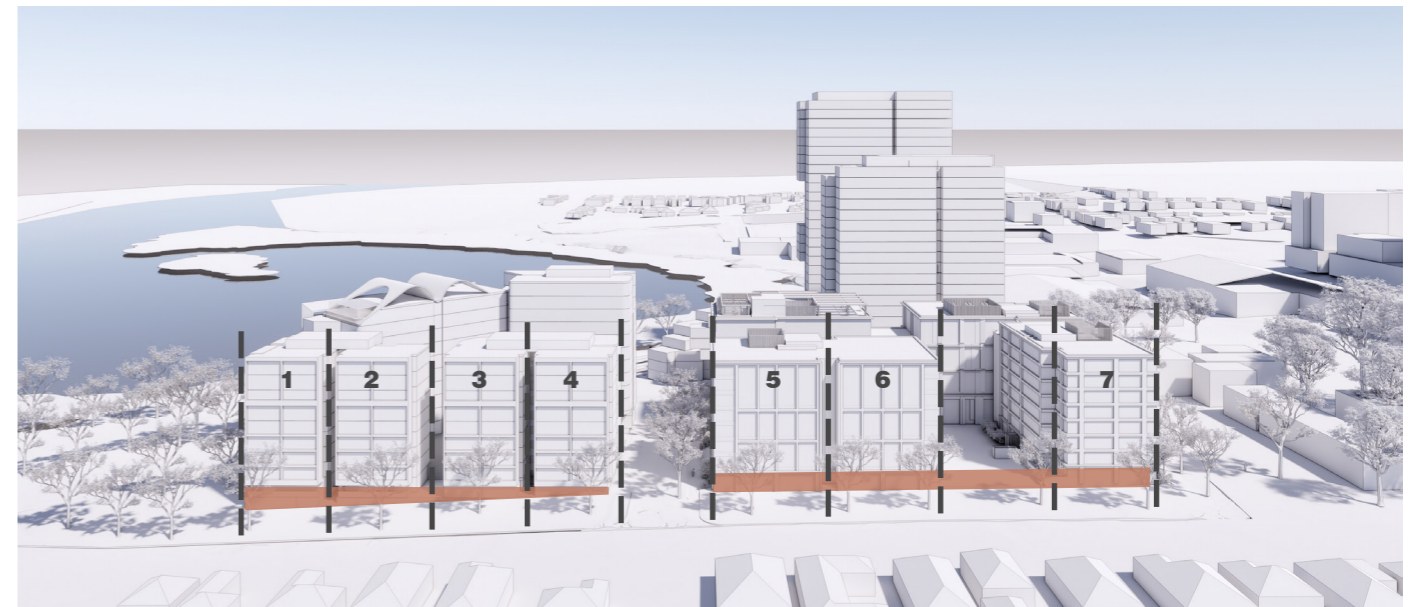
PRECINCT PLAN DRP 01

As agreed in the first Design Review Panel, the block layouts included in the reference design left the Wharf Rd interface quite blank resulting in poor passive surveillance



PRECINCT PLAN DRP 02

The block diagrams above demonstrate the improvements developed through the Design review process. Block O5 improves passive surveillance to OS1 and creates a continuous COS through to B6



WHARF RD STREETSCAPE DRP 02

Built form is broken down into 7 masses which share similar scales. With a large setback to the street the scale of the buildings are also tempered by retaining street tree's (tall eucalypts). The ground floor interface also offers passive surveillance to OS1, (nominated in amber above)

VEHICULAR AND BICYCLE PARKING

SUBMISSION ISSUES

- Bicycle parking and motorcycle parking is to be provided as specified in the TIA and must comply with the relevant Australian Standards.
- With regards to the design of the carpark, the following issues have been identified:
- [...] 1m blind aisle extensions have consistently not been provided in accordance with Australian Standard requirements. Plans are to be updated to provide these to ensure satisfactory manoeuvring into the parking spaces.
- There are a number of locations throughout the car park where columns appear to be conflicting with the vehicle envelope in accordance with the Australian Standards. Plans are to be updated and the vehicle envelope should be illustrated on revised plans.
- The location of the driveway may be contradicting with a street tree. Accordingly, the design plans for the Infrastructure DA are to be amended to take into account the driveway location. It is recommended that a condition be included in this SSD for those plans to be updated to ensure this is not missed.

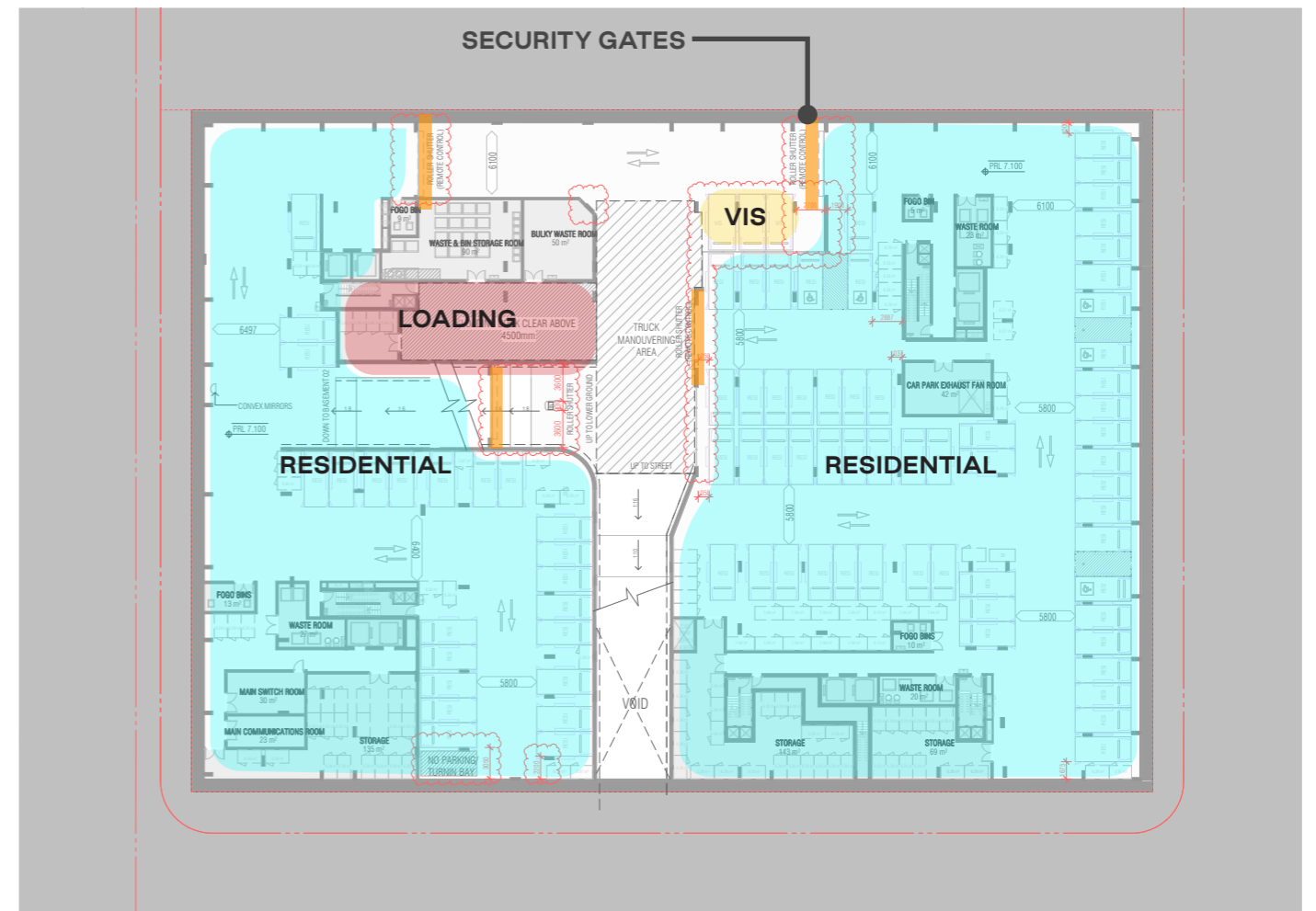
- The location of the control point into the basement car park is not shown. The control point must comply with the requirements of the Australian Standards and must ensure that no vehicles are obstructing the road reserve including the footpath. Swept paths for loading vehicles will also need to be checked to ensure service vehicles will be able to travel around any intercom/card reader if being used. An updated TIS should be required to perform this assessment.
- Devices, such as convex mirrors, will need to be installed at locations where vehicles will not have clear view of oncoming traffic and the aisle width is narrow preventing continuous two-way movements. The TIA is to perform this assessment and nominate the devices and locations they are to be installed.

FK RESPONSE

- The car park layout has been reviewed and the plans have updated to include compliant blind aisle extensions to ensure satisfactory maneuvering into all parking spaces.
- A detailed check of all structural column locations relative to vehicle envelopes has been

undertaken. Vehicle envelopes have been illustrated on the revised plans to demonstrate full compliance with Australian Standard requirements.

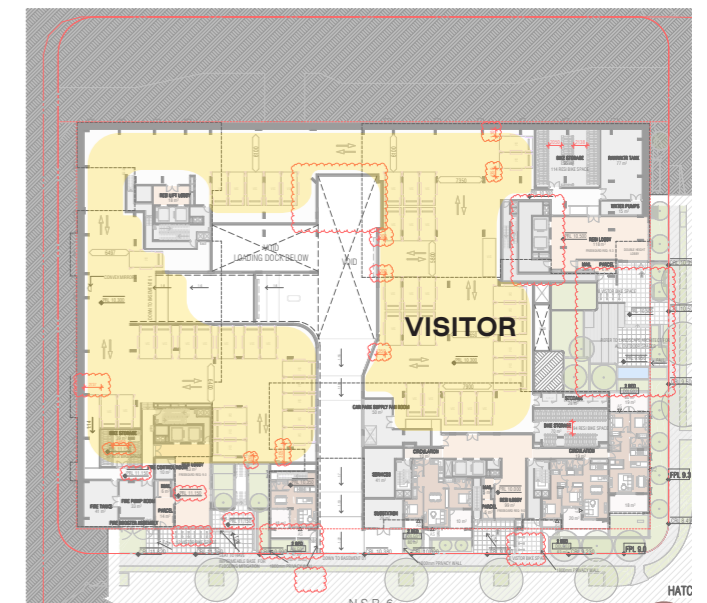
- The Infrastructure DA design plans will be amended to remove street tree contradicting with driveway location. Please refer to DA-E-B5-AR-099
- The location of the vehicle control point are shown on the updated plans. The design will ensure compliance with Australian Standards, including confirmation that queued vehicles will not obstruct the road reserve or footpath. Swept paths for loading and service vehicles around the control point/intercom/card reader will be provided as part of the updated traffic report.
- The Traffic engineer have reviewed all internal car park locations. Where sight lines may be restricted convex mirrors or equivalent traffic control treatments have been nominated in their report. These will be adopted by the architectural drawings in DD



DA-E-B5-AR-098 BASEMENT O1 FLOOR PLAN

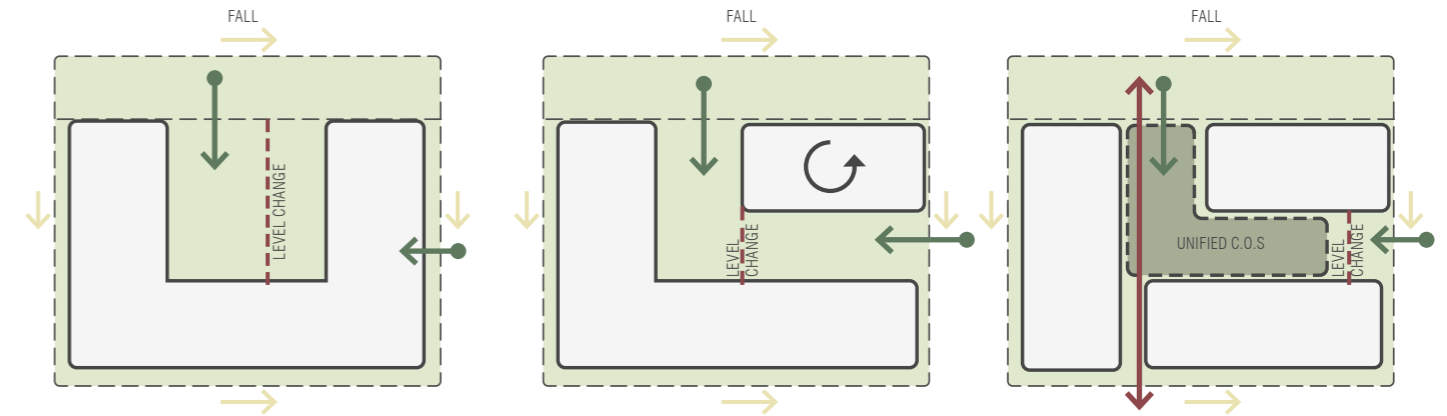


DA-E-B5-AR-097 BASEMENT O2



DA-E-B5-AR-099 LOWER GROUND

ACCESSIBILITY



DRP 01 SCHEME

Original scheme based off the reference design aimed to use the buildings to front the streets and manage the fall of the street within the COS

DRP 02 SCHEME

With the development of the block layout FK took the opportunity to connect with the street RL's into the COS to better manage lobbies and passive surveillance

DRP 03 SCHEME

With Advice from the DRP an additional cross block connection was introduced and the COS levels were unified to maximise the potential uses for the residents. Ultimately this lifted the areas above the street. Refer streetscape section of report on level management.

SUBMISSION ISSUES

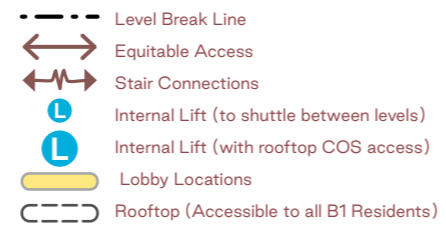
- The public interface around these blocks must be built to interface with & as per design levels of the surrounding streets as approved via DA/75/2024. [...]
- All site entries, exits, driveways, through-site link levels, ground levels and building entry points of all buildings must be coordinated [...]
- These levels must be noted [...] All building entrances must have DDA compliant equitable accesses. [...] if there is a compliant entry within 50 m of that entrance.
- All communal areas like courtyards and rooftop gardens for all buildings, must be accessible to users of all abilities.
- All building setbacks must be free of basements underneath, contain deep soil and house a second row of trees to the street trees. Basements cannot protrude out into the designated setbacks unless a built form is specified as zero lot.
- All courtyards and communal spaces within all buildings must be accessible from ground levels and be at grade with the public domain from most sides. Current design of courtyards are too

high from the public domain and accessible by many flights of stairs.[...] at grade entries from the public footpaths.

- The ground floor of most of the buildings seems to sit much higher than the public domain on all sides. Level differences of 1m to 5.5 m between the external footpath and ground floor seem to exist. Please lower the ground floors to make them more on level with the public domain. The freeboard required is only 500mm, but the level differences are much bigger than that. This is not a good outcome for the buildings.
- Apartments abutting the public domain along the streets, must have secondary entries coming down to the footpaths to encourage passive surveillance. IN the current design, the ground floor apartments seem segregated from the street and do not 'talk' to the public domain.

FK RESPONSE

- Internal levels have been defined by flooding mitigations and site contours. Through the DRP process the COS was unified in RL which has increased the variance from the street to internal levels. Levels are coordinated
- Equitable access has been demonstrated and are supported by the accessibility consultant.
- Setbacks only contain terraces and soil for planting
- Ground floor apartments all have street entries where appropriate. OS1 does not allow access.



ACCESSIBILITY PATHWAY DIAGRAM

Diagram above demonstrates how equitable access is provided through the site and up to the COS on the rooftop.

4

RESPONSE TO DRP 04

ACCESS TO ENTRIES

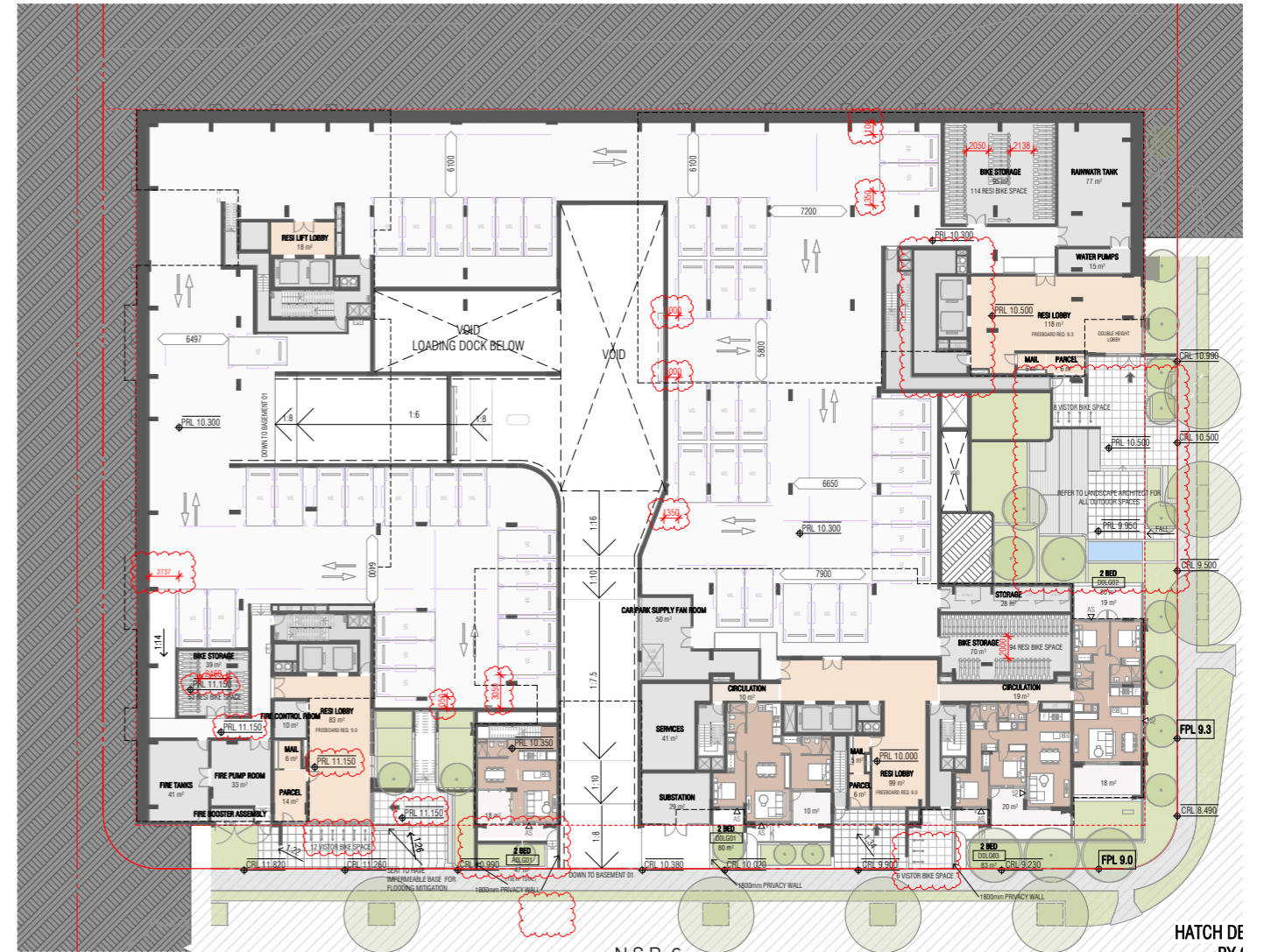
DRP04 ISSUES

- The SE entry lobby and its adjacent entry court are located more than a metre below street level – which is counter to the prevailing slope. The SE entry is further compromised by the location of the lifts, which require walking around the core via a narrow corridor, which is also below ground with no natural light.
- The courtyard space adjacent to this entry requires further refinement and simplification to improve its relationship to the street level and avoid a sunken unsafe space.
- The SW and NE lobbies still need further refinement, in particularly column placement and access to entry doors.
- The NW lobby includes a double height void that is unresolved, as it currently straddles the ground level mail, parcel and plant rooms. Further review of this entry is required to resolve the form and character of this double height void.
- There does not appear to be a gate to the open steps leading to the courtyard adjacent to the NW lobby, which may raise CPTED issues.

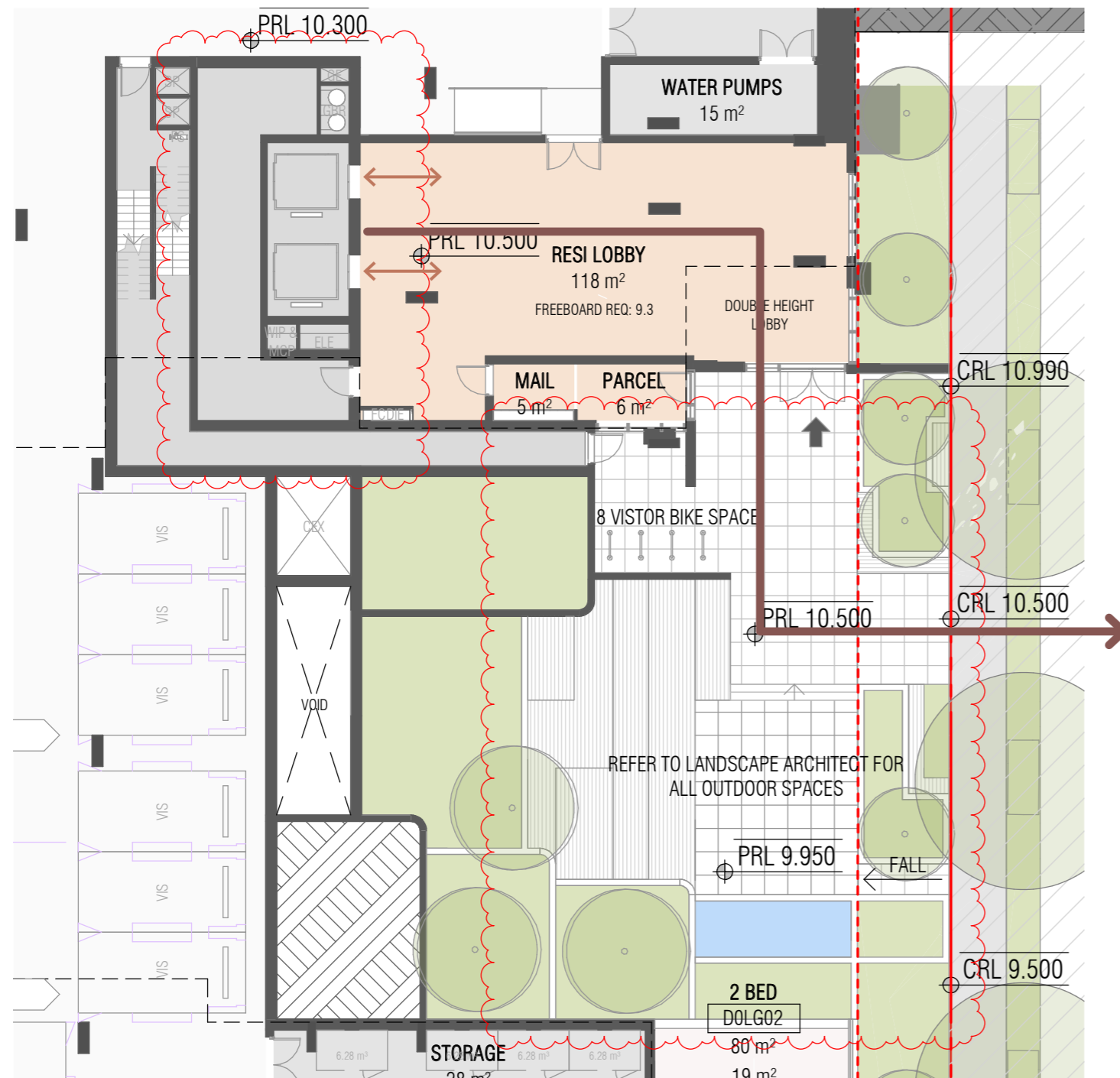
FK RESPONSE

- The SE entry court to lobby has been relocated further north to align with the prevailing street level, resulting in a level and more legible entrance condition. This adjustment removes the previously sunken entry and significantly improves accessibility, safety, and passive surveillance from the public realm.
- To address concerns regarding lift access in the SE lobby, a direct-through lift arrangement has been introduced. This eliminates the need to walk behind the lift core via a narrow, below-ground corridor and improves wayfinding, convenience, and amenity. The revised lift access is now naturally integrated within the entry sequence.
- Both the SW and NE lobbies have undergone further refinement, with particular attention to column placement and clear, direct access to entry doors. These adjustments improve circulation, sightlines, and the overall clarity of arrival for residents and visitors. Please refer to floor plans for details
- In the NW lobby, a lounge area has been introduced above the mail room, parcel room, and fire control room on the ground floor. This provides a

more resolved architectural outcome, enhances resident amenity, and establishes a clear form and character for the NW entry without compromising functional spaces at ground level.



DA-E-B5-AR-099 LOWER GROUND



DA-E-B5-AR-099_LOWER GROUND PLAN - SE LOBBY

Lobby RL lifted to 10.6 To align with street interface

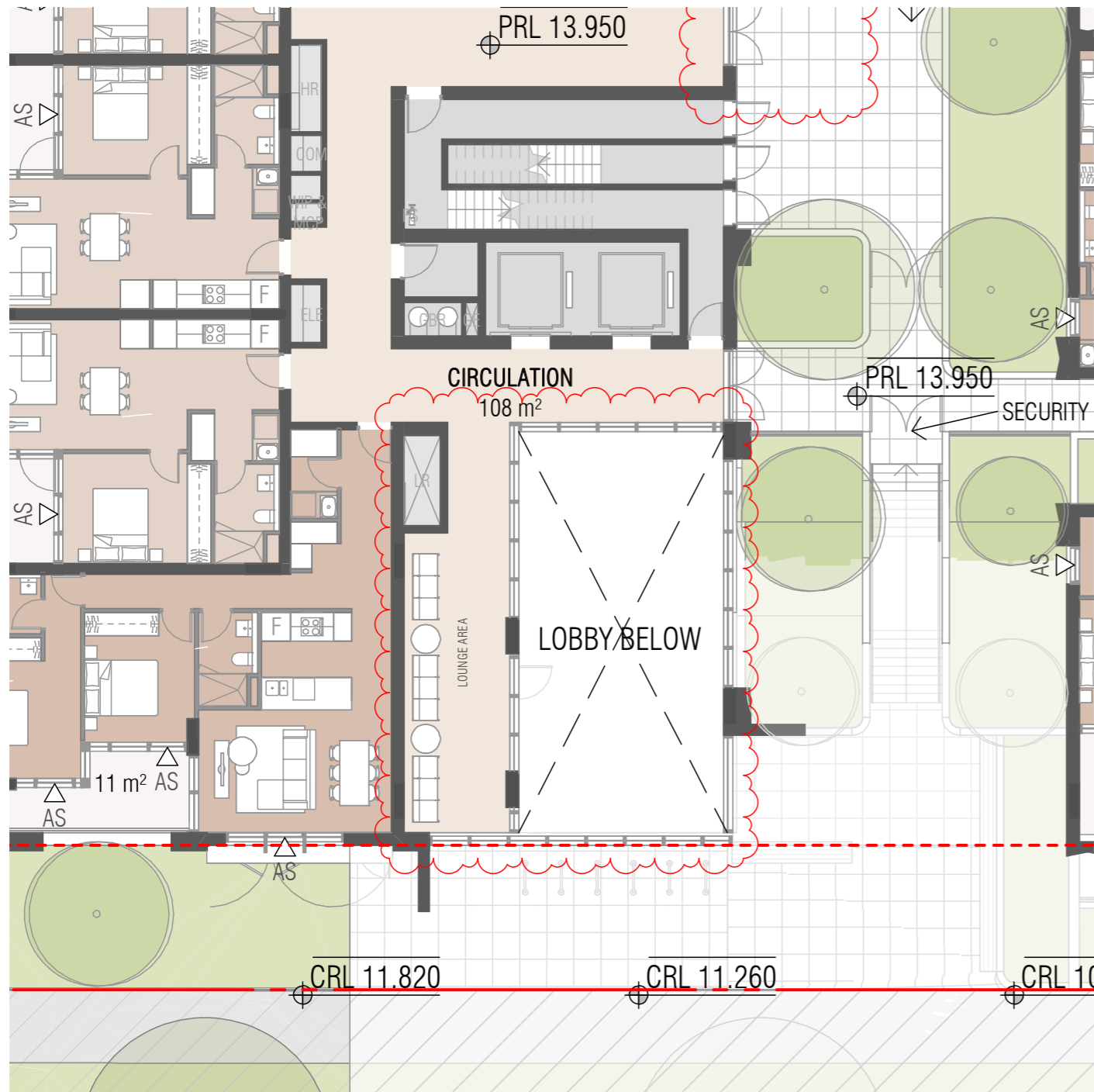
Refer landscape drawings nominating the visual connections between B5 and B6



SE LOBBY

The SE lobby has been relocated north to align with street level, creating a clear and accessible entry.





DA-E-B5-AR-100_GROUND FLOOR PLAN - LOUNGE AREA



NW LOBBY

- A gate to the open steps leading to the courtyard adjacent to the NW lobby has always been provided as part of the design. This element remains unchanged and ensures controlled access and appropriate CPTED outcomes.



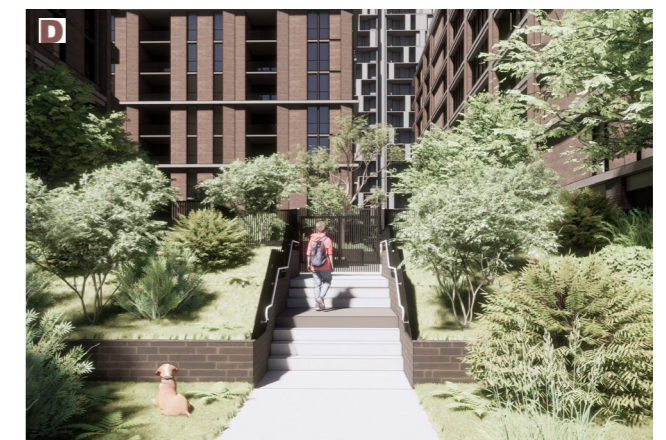
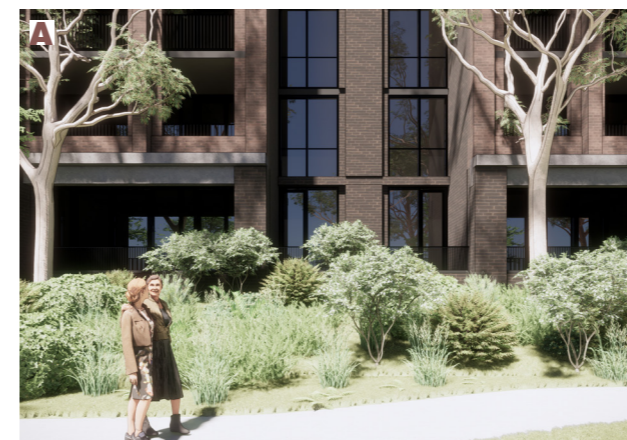
RESPONSE TO TOPOGRAPHY

DRP04 ISSUES

- Building C levels have a poor relationship to natural ground along its eastern elevation (too high) and along its southern elevation (too low). The design team is therefore strongly encouraged to further review proposed levels to ensure that they appropriately align with adjacent natural ground lines.

FK RESPONSE

- The proposed levels are primarily driven by the applicable flood planning requirements, and the ADG's aspiration to lift ground floor apartments a minimum 500mm above the FFL of the street.
- Through the DRP process the development was encouraged to standardise the levels of the COS. This further standardised the internal levels between each building. Due to the significant variation in flooding and road levels across the site, the building interfaces differently with each street. these are demonstrated in the architectural drawing package ground plane interface sections, and in the images opposite. The proposed flood levels are indicated on the floor plans.
- Apartment levels provide privacy and amenity to the residents while ensuring ADG, CPTED and DCP aspirations are achieved



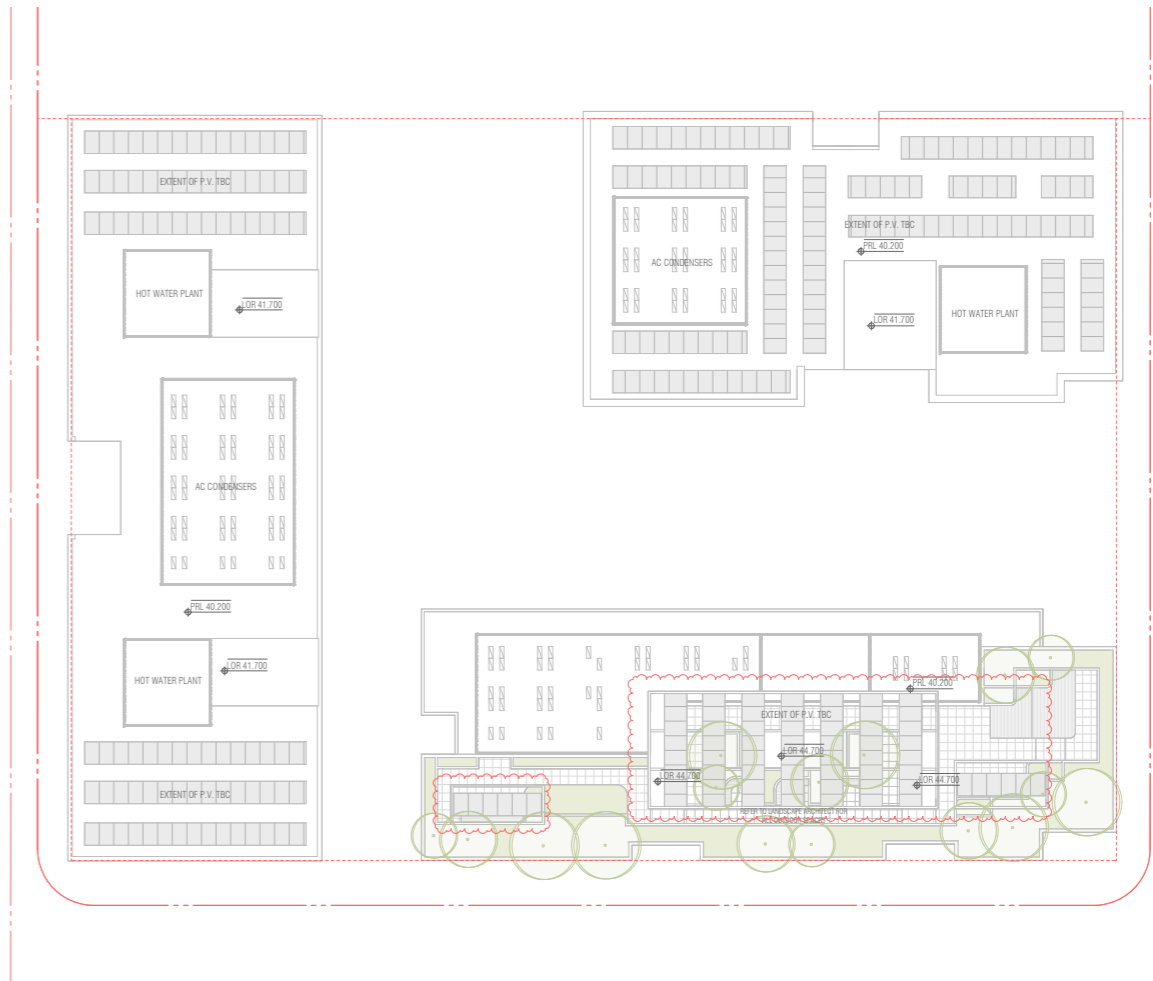
ROOFTOP SHADE STRUCTURE

DRP04 ISSUES

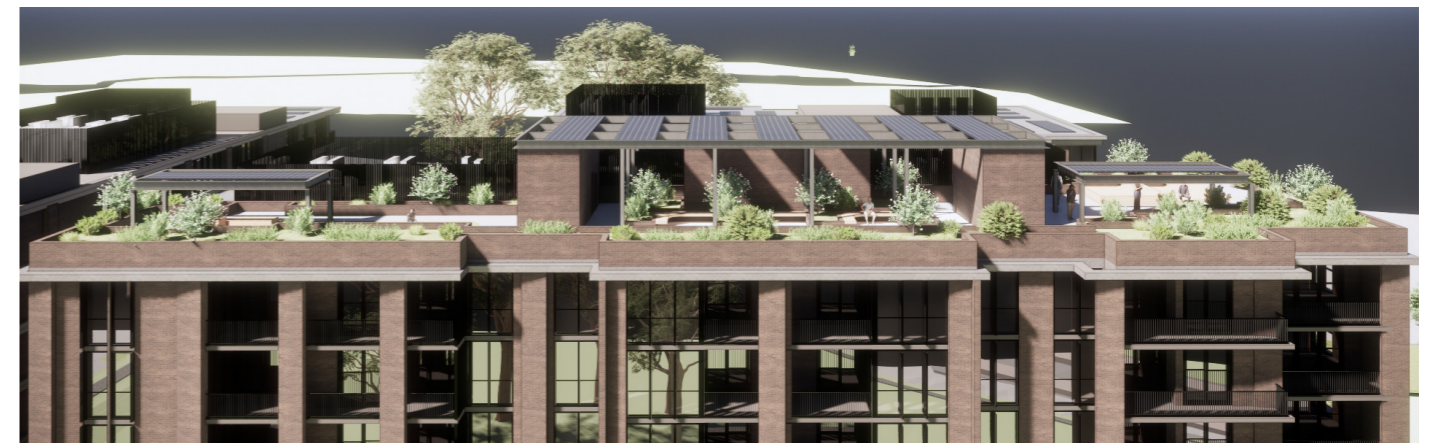
- The roof feature is excessively large and does not specifically respond to the amenity requirements of the rooftop open space. It should be reduced in scale to a simple roof form spanning the service cores only, which will reduce its visual and physical bulk. The covered areashould relate to the function of the spaces it protects and needs to demonstrate a relationship with the adjoining buildings. The Panel suggests decreasing its current size to increase the amenity of the space.

FK RESPONSE

- The roof feature has been revised and its footprint significantly reduced, as shown on the updated drawings. The amended design adopts a simpler roof form spanning the service cores only, substantially reducing both its visual and physical bulk.
- Smaller, lower-scale pergolas are proposed to provide shade to select seating areas only, ensuring coverage is directly related to the use and function of the rooftop spaces.
- The overall reduction in built form increases access to open sky and views, enhances the usability and amenity of the rooftop open space, and improves the building's relationship with adjoining developments. The revised scale and form result in a more contextually appropriate and visually restrained rooftop outcome.



DA-E-B5-AR-150_ROOF PLAN



ROOFTOP COS



NATURAL VENTILATION

DRP04 ISSUES

Design Excellence requires that a minimum of 60% of proposed units cross ventilate in the first nine levels. The Panel previously expressed concerns that with long corridors, cross ventilation compliance would not be possible. To achieve compliance, the proposal relies on 32 single sided units; without these units, only 131/260 units achieve cross ventilation (50%). Although a report has been submitted in support of the inclusion of these units, they do not align with the unit types specified by the ADG as capable of cross ventilating (see ADG pp. 82 – 85).

In the provided documentation, it has not been explained how the likely acoustic visual and privacy impacts caused by opening windows so close to adjacent balconies – such as along the northern elevation – can be resolved.

The Panel notes that separate advice has been provided to the project team by RWDI, stating that natural ventilation through façade articulation is generally not supported for single sided units. However,

noting the technical advice being provided to the design team, this matter is to be scrutinised to the satisfaction of the consent authority.

RESPONSE

Refer supporting letter from RWDI for natural ventilation report.

LANDSCAPE

DRP04 ISSUES

- The architectural drawings illustrate no general set note for landscape across the main 'ground' level communal open space. Planters in the architectural sections appear to be built up by approximately 1m to allow for planting and trees, landscape plans note TOW levels at 14.70 and 14.55 relying heavily on mounding within relatively small planter zone which appear not to be capable of provide sufficient soil depths or volumes for the trees shown given the FFL level of RL 14.10. Extensive high planters have a detrimental impact on the quality and character of on slab landscapes. Planters must be set down, stepped, or mounded within reason and integrated with seating and low walls. Depths and volumes must be provided in accordance with Council and ADG requirements to ensure the success of the future landscape. The preferred wall height at the interface with walkways and spaces is 600mm or less.
- The circulation route(s) through the eastern portion of the main 'ground' level communal open space should be simplified to provide one though route and utilise the remainder as a usable open space, clearly not primarily

a though walkway.

- The rooftop communal open space needs to be coordinated with the proposed roof feature. At present the landscape and uses below are not coordinated with the roof above including significant planting under the roof and BBQ gathering area partially not covered by the roof. Planting under impervious roof should be avoided. Specifically, it is noted that canopy trees are shown under the roof form.
- On the rooftop planter wall heights are not evident in the landscape or architectural drawings. Planter depths and volumes must be provided in accordance with Council and ADG requirements to ensure the success of the future landscape.
- The two banks of bicycle parking along the western elevation appear to block the NW entry in the one case, and impact on the visual and physical amenity of the private open space of an adjacent street facing apartment in the other. Both of these bicycle parking locations appear not to be suitable and should be relocated.

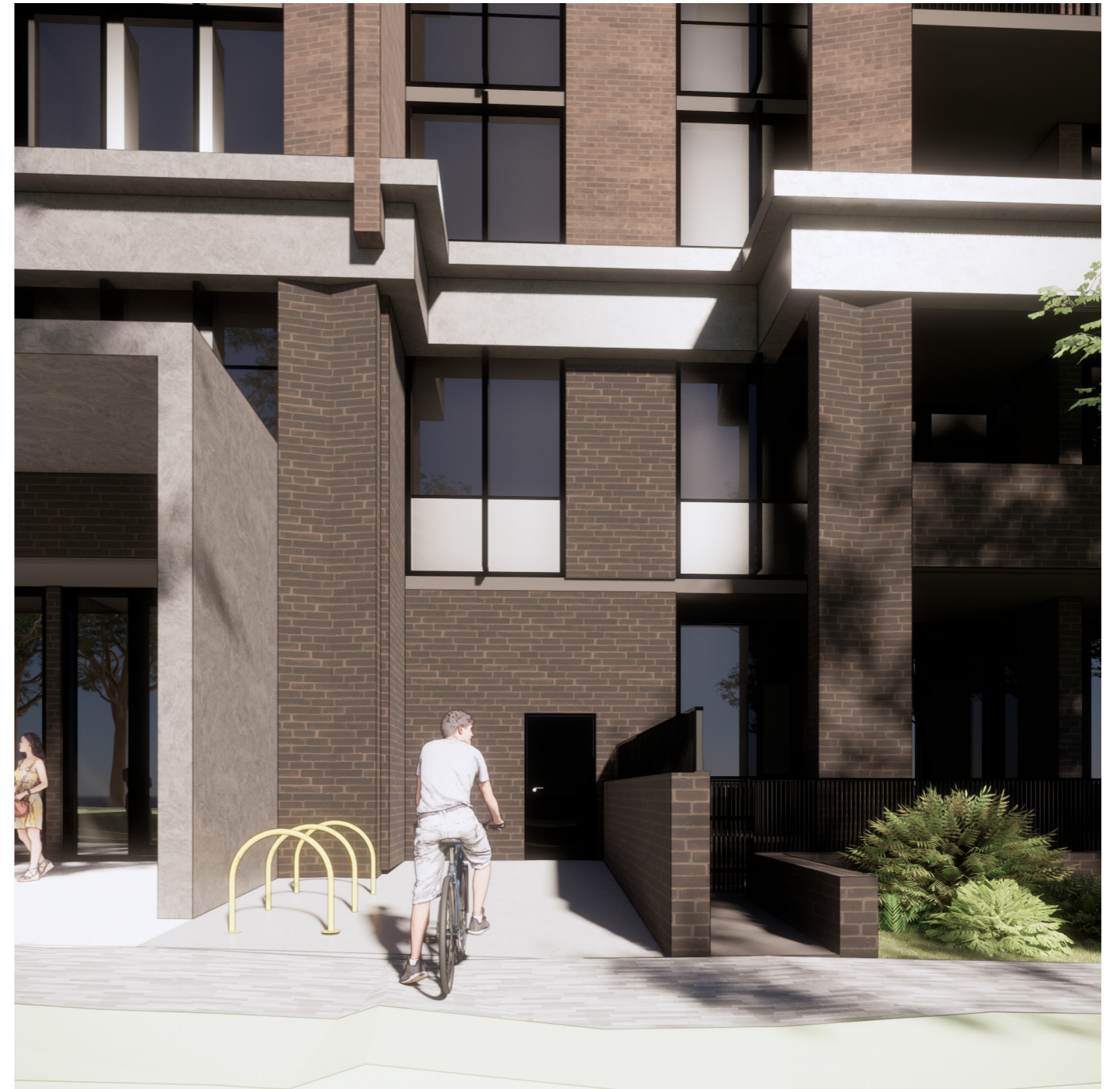
FK RESPONSE

- Refer to Landscape architect for responses to planter levels, Integrated seating and rooftop terrace design amendments.
- See the following pages demonstrating improved bicycle parking solutions for the public domain ensuring residential amenity.



NW VISITOR BIKE LOCATION

The bicycle parking located adjacent to the NW lobby does not obstruct the NW entry. The entry is located on the side elevation rather than the street frontage, and clear, unobstructed access is maintained at all times.



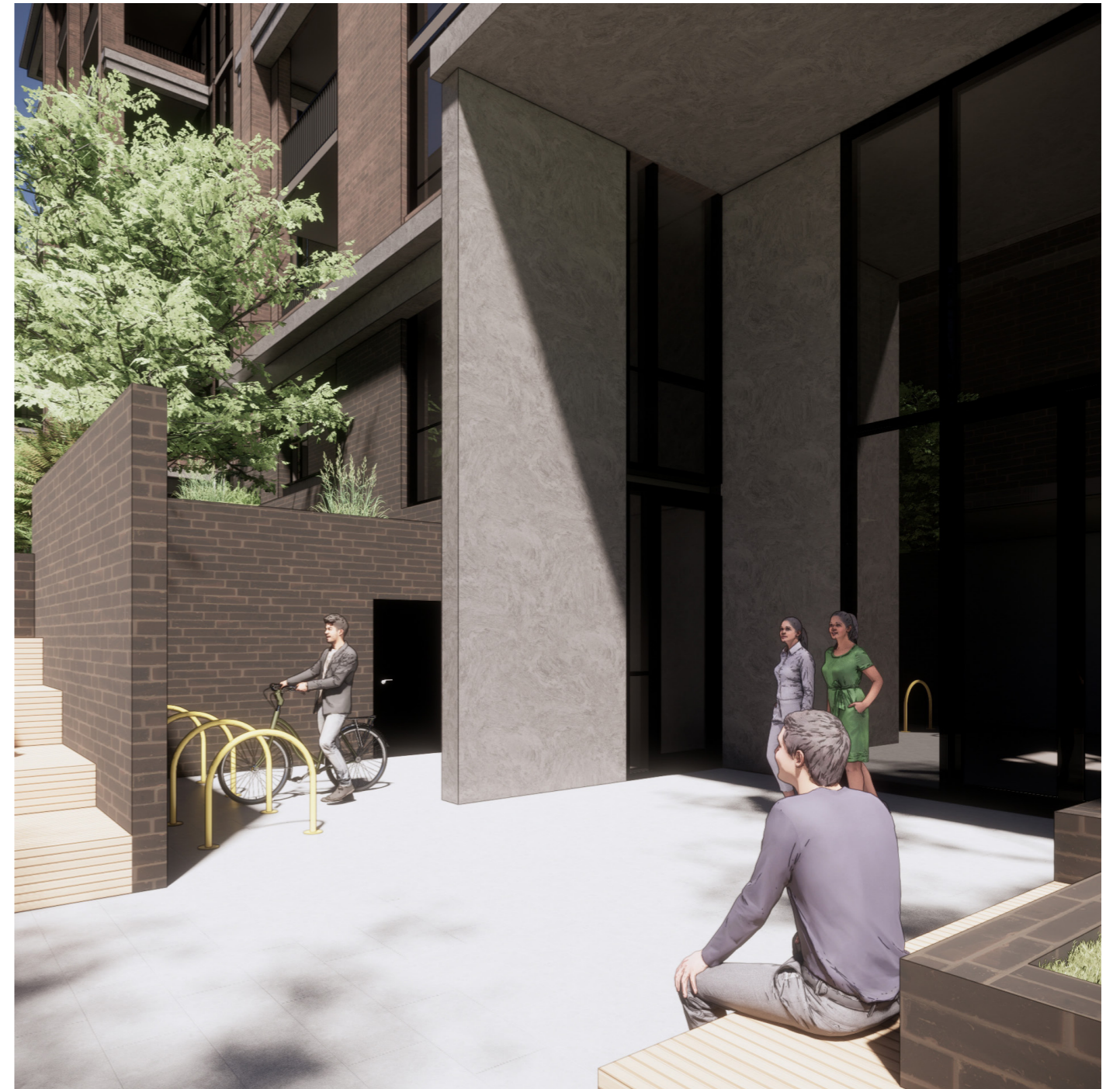
SW VISITOR BIKE LOCATION

The bicycle parking associated with the SW area has been relocated away from the adjacent street-facing apartment to minimise visual impact and avoid any physical encroachment on the private open space.



NE VISITOR BIKE LOCATION

The visitor bicycle parking adjacent to the NE lobby does not impact apartment privacy.



SE VISITOR BIKE LOCATION

The visitor bicycle parking adjacent to the SE lobby does not impact apartment privacy.

