

# Central Precinct Renewal

PRECINCT INVENTORIES

Precinct 3: Sydney Terminal

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# TERMINOLOGY

## Assessing Heritage Significance

### Significance assessment criteria

Determining the significance of heritage items or a potential archaeological resource is undertaken by utilising a system of assessment centred on the Burra Charter of Australia ICOMOS.<sup>1</sup> The principles of the charter are relevant to the assessment, conservation and management of sites and relics. The assessment of heritage significance is outlined through legislation in the Heritage Act and implemented through the NSW Heritage Manual, the Archaeological Assessment Guidelines and the 2009 Assessing Significance for Historical Archaeological Sites and 'Relics'.

If an item meets one of the seven heritage criteria, and retains the integrity of its key attributes, it can be considered to have heritage significance. The significance of an item or potential archaeological site can then be assessed as being of local or state significance.

'*State heritage significance*', in relation to a place, building, work, relic, moveable object or precinct, means significance to the State in relation to the historical, scientific, cultural, social, archaeological, architectural, natural or aesthetic value of the item.

'*Local heritage significance*', in relation to a place, building, work, relic, moveable object or precinct, means significance to an area in relation to the historical, scientific, cultural, social, archaeological, architectural, natural or aesthetic value of the item.

**Table 1: NSW heritage assessment criteria**

Criteria	Description
<b>A – Historical Significance</b>	An item is important in the course or pattern of the local area's cultural or natural history.
<b>B – Associative Significance</b>	An item has strong or special associations with the life or works of a person, or group of persons, of importance in the local area's cultural or natural history.
<b>C – Aesthetic or Technical Significance</b>	An item is important in demonstrating aesthetic characteristics and/or a high degree of creative or technical achievement in the local area.
<b>D – Social Significance</b>	An item has strong or special association with a particular community or cultural group in the local area for social, cultural or spiritual reasons.
<b>E – Research Potential</b>	An item has potential to yield information that will contribute to an understanding of the local area's cultural or natural history.
<b>F – Rarity</b>	An item possesses uncommon, rare or endangered aspects of the local area's cultural or natural history.
<b>G - Representativeness</b>	An item is important in demonstrating the principal characteristics of a class of NSW's cultural or natural places of cultural or natural environments (or the cultural or natural history of the local area).

<sup>1</sup> Australia ICOMOS 2013. *The Burra Charter: The Australia ICOMOS Charter for places of cultural significance*. Australia ICOMOS, Burwood.

## Levels of Significance of Site Components

Individual areas and elements of the Precinct have been assessed and a level of significance has been applied. This detailed assessment is provided to enable decisions on the future conservation and development of the place.

Five levels of cultural significance have been used in the assessment of the Precinct. These categories have been developed based on *Assessing Heritage Significance*,<sup>2</sup> prepared by the NSW Heritage Office. The categories provide a framework for conservation policies, interpretation and recommended treatment of the fabric.

**Table 2: Levels of Cultural Significance**

No	Level	Definition
E	Exceptional	<p>Where an individual space, element, tree or shrub is assessed as making a rare or outstanding contribution to the overall significance of the place. Spaces, elements or fabric exhibit a high degree of intactness and quality. Minor alterations or degradation may be evident, but does not detract from the overall significance of the place.</p> <p>Demolition/removal of the element would diminish the heritage significance of the place.</p>
H	High	<p>Where an individual space, element, tree or shrub is assessed as making considerable contribution to the overall significance of the place. Spaces, elements or fabric exhibit a considerable degree of intactness and were originally of substantial quality. Considerable alteration may have been undertaken, which may alter the presentation and completeness, but does not detract substantially from the overall significance of the place.</p> <p>Demolition/removal of the element would diminish the heritage significance of the place.</p>
M	Moderate	<p>Where an individual space, element, tree or shrub is assessed as making a moderate contribution to the overall significance of the place. Original spaces, elements or fabric may exhibit considerable alteration and/or degradation which detracts from the overall significance of the place. Original space, elements or fabric which were of some intrinsic quality, but are relatively intact may be included. Elements with little heritage value but contribute to the overall cumulative significance of the place may also be included. New elements of high-quality design and aesthetic value may be considered to contribute to the significance of the place.</p> <p>Demolition/removal of the element may diminish the heritage significance of the place. Elements or spaces can be altered or adaptively reused.</p>
L	Little	<p>Where an individual space, element, tree or shrub is assessed as making a minor contribution to the overall significance of the place, particularly compared with other elements. Original elements may exhibit extensive alterations or degradations which impact their significance and ability to interpret. New elements of little intrinsic quality or aesthetic value may be considered in this category.</p> <p>Demolition/removal of the element would not diminish the heritage significance of the place. Elements or spaces can be altered or adaptively reused.</p>

<sup>2</sup> NSW Heritage Office, *Assessing Heritage Significance*, 2001



No	Level	Definition
I	Intrusive	Where an individual space, element, tree or shrub is assessed as detracting from the appreciation and overall significance of a place. The element may be adversely affecting or obscuring other significant areas, elements or items.  Demolition/removal of the element is recommended.

## Integrity

Integrity relates to whether all the attributes that convey heritage significance are extant within the subject site and not eroded or under threat<sup>3</sup>. Integrity is a measure of the wholeness and intactness of the place and its attributes. Examining the conditions of integrity, therefore requires assessing the extent to which the subject site or element:

- a) includes all elements necessary to express its heritage significance;
- b) is of adequate size to ensure the complete representation of the features and processes which convey the property's heritage significance;
- c) suffers from adverse effects of development and/or neglect.

**Table 3: Levels of Integrity**

Level	Definition
High	The physical fabric of the property and/or its significant features is in good condition, and the impact of deterioration processes controlled. A significant proportion of the elements necessary to convey the totality of the heritage significance conveyed by the property is included <sup>4</sup> .
Moderate	The physical fabric of the property and/or its significant features have undergone some modifications. The changes may be reversible.
Low	The physical fabric of the property and/or its significant features have undergone substantial modifications and the original is irretrievable
N/A	Modern and / or intrusive fabric
Unknown	Elements that cannot be evaluated (ie. natural ventilation systems where their continued operation cannot be determined, fabric that cannot be inspected)

<sup>3</sup> ICOMOS 2011, 10. Patiwaël [full reference TBS]

<sup>4</sup>.Sheridan Burke, The long and winding road: a challenge to ICOMOS members, in *Changing World, Changing Views of Heritage: heritage and social change* ICOMOS, 2010

## PRECINCT 3: SYDNEY TERMINAL

**Table 4: Overall significance for items in Precinct 3: Sydney Terminal**

Item no.	Item	Overall Significance
3.0	Overall Sydney Terminal Precinct	Exceptional
3.1	Main Terminus Building: Façade and Roof	Exceptional
3.2	Eddy Avenue Colonnade, Arcades and Shops	Exceptional
3.3	Porte-cochere	Exceptional
3.4	Tram Ramps	Exceptional
3.5	Western Forecourt	Exceptional
3.6	Pitt Street Arcade	Exceptional
3.7	East Deck	Moderate
3.8	Main Terminus Building: Grand Concourse	Exceptional
3.9	Main Terminus Building: Office Spaces	High
3.10	Clocktower	Exceptional
3.11	West Wing Extension	High
3.12	Country and Interstate Platforms	High
3.13	Subway Passage Systems	High
3.14	Devonshire Street Tunnel	High
3.15	Basement	Moderate
3.16	Luggage Dock	Moderate
3.17	Parcels Area	High
3.18	YHA Railway Square (Former Inwards Parcel Shed)	Moderate
3.19	Former Parcels Post Office	High

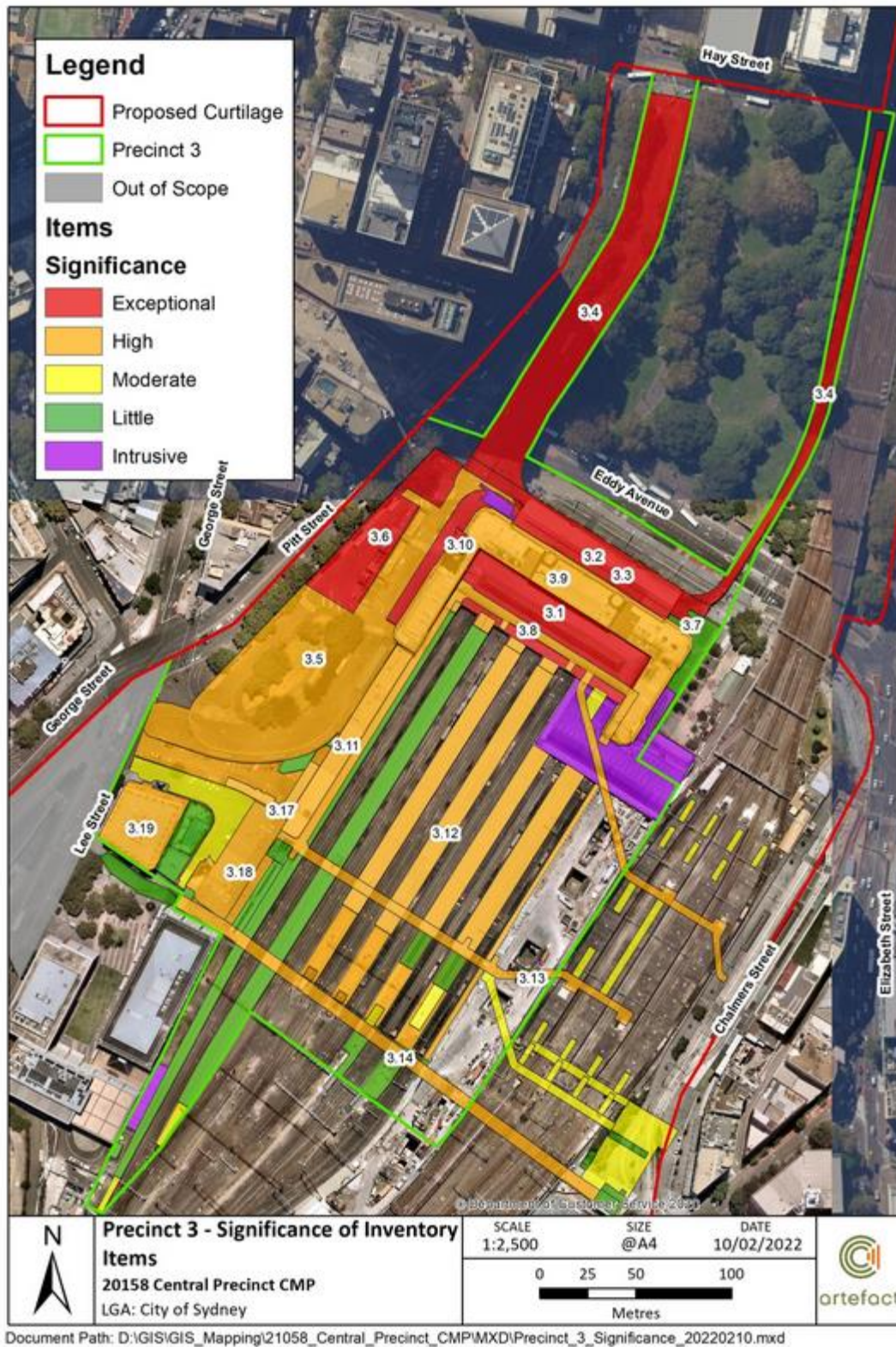


Figure 1: Overall levels of significance for inventory items.

# CONTENTS

<b>3.0 Overall Precinct .....</b>	<b>1</b>
3.1.1 History of the Sydney Terminal .....	1
3.1.2 Site Description and Condition .....	9
3.1.3 Aboriginal Archaeological Potential .....	11
3.1.4 Historical Archaeological Potential .....	11
3.1.5 Statement of Significance .....	11
3.1.6 Site Specific Conservation Policies .....	12
3.1.7 Precinct Opportunities and Constraints .....	13
<b>3.1 Main Terminus Building: Facades and Roof .....</b>	<b>17</b>
3.1.1 History of the Main Terminus Building: Facades and Roof .....	17
3.1.2 Site Description and Condition .....	19
3.1.3 Statement of Significance .....	23
3.1.4 Gradings of Significance and Integrity .....	24
3.1.5 Site Specific Conservation Policies .....	24
<b>3.2 Eddy Avenue Colonnade, Arcades and shops .....</b>	<b>28</b>
3.2.1 History of the Eddy Avenue Colonnade, Arcades and Shops .....	28
3.2.2 Site Description and Condition .....	30
3.2.3 Statement of Significance .....	35
3.2.4 Gradings of Significance and Integrity .....	35
3.2.5 Site Specific Conservation Policies .....	36
<b>3.3 Porte-Cochere .....</b>	<b>39</b>
3.3.1 History of the Porte-Cochere .....	39
3.3.2 Site Description and Condition .....	40
3.3.3 Statement of Significance .....	43
3.3.4 Gradings of Significance and Integrity .....	44
3.3.5 Site Specific Conservation Policies .....	44
<b>3.4 Tram Ramps .....</b>	<b>47</b>
3.4.1 History of the Tram Ramps .....	47
3.4.2 Site Description and Condition .....	50
3.4.3 Statement of Significance .....	55
3.4.4 Gradings of Significance and Integrity .....	55
3.4.5 Site Specific Conservation Policies .....	56
<b>3.5 Western Forecourt .....</b>	<b>59</b>
3.5.1 History of the Western Forecourt .....	59
3.5.2 Site Description and Condition .....	62
3.5.3 Statement of Significance .....	67

3.5.4	Gradings of Significance and Integrity .....	68
3.5.5	Site Specific Conservation Policies .....	68
<b>3.6</b>	<b>Pitt Street Arcade.....</b>	<b>71</b>
3.6.1	History of the Pitt Street Arcade .....	71
3.6.2	Site Description and Condition .....	73
3.6.3	Statement of Significance.....	76
3.6.4	Gradings of Significance and Integrity.....	76
3.6.5	Site Specific Conservation Policies .....	77
<b>3.7</b>	<b>East Deck .....</b>	<b>80</b>
3.7.1	History of the East Deck .....	80
3.7.2	Site Description and Condition .....	81
3.7.3	Statement of Significance.....	83
3.7.4	Gradings of Significance and Integrity.....	84
3.7.5	Site Specific Conservation Policies .....	84
<b>3.8</b>	<b>Main Terminus Building: Grand Concourse and Related Spaces .....</b>	<b>87</b>
3.8.1	History of the Main Terminus Building: Grand Concourse and Related Spaces .....	87
3.8.2	Site Description and Condition .....	94
3.8.3	Statement of Significance.....	107
3.8.4	Gradings of Significance and Integrity.....	108
3.8.5	Site Specific Conservation Policies .....	112
<b>3.9</b>	<b>Main Terminus Building: Office spaces .....</b>	<b>116</b>
3.9.1	History of the Main Terminus Building: Office Spaces .....	116
3.9.2	Site Description and Condition .....	117
3.9.3	Statement of Significance.....	124
3.9.4	Gradings of Significance and Integrity.....	125
3.9.5	Site Specific Conservation Policies .....	126
<b>3.10</b>	<b>Clocktower .....</b>	<b>128</b>
3.10.1	History of the Clocktower.....	128
3.10.2	Site Description and Condition .....	130
3.10.3	Statement of Significance.....	134
3.10.4	Gradings of Significance and Integrity.....	134
3.10.5	Site Specific Conservation Policies .....	135
<b>3.11</b>	<b>West Wing Extension .....</b>	<b>138</b>
3.11.1	History of the West Wing Extension .....	138
3.11.2	Site Description and Condition .....	139
3.11.3	Statement of Significance.....	147
3.11.4	Gradings of Significance and Integrity.....	147
3.11.5	Site Specific Conservation Policies .....	149



<b>3.12 Country and Interstate Platforms .....</b>	<b>152</b>
3.12.1 History of the Country and Interstate Platforms .....	152
3.12.2 Site Description and Condition .....	156
3.12.3 Statement of Significance .....	163
3.12.4 Gradings of Significance and Integrity .....	164
3.12.5 Site Specific Conservation Policies .....	165
<b>3.13 Subway Passage Systems .....</b>	<b>168</b>
3.13.1 History of the Subway Passage Systems .....	168
3.13.2 Site Description and Condition .....	169
3.13.3 Statement of Significance .....	174
3.13.4 Gradings of Significance and Integrity .....	174
3.13.5 Site Specific Conservation Policies .....	175
<b>3.14 Devonshire Street Tunnel.....</b>	<b>177</b>
3.14.1 History of the Devonshire Street Tunnel .....	177
3.14.2 Site Description and Condition .....	179
3.14.3 Statement of Significance .....	181
3.14.4 Gradings of Significance and Integrity .....	181
3.14.5 Site Specific Conservation Policies .....	182
<b>3.15 Basement .....</b>	<b>185</b>
3.15.1 History of the Basement .....	185
3.15.2 Site Description and Condition .....	186
3.15.3 Statement of Significance .....	191
3.15.4 Gradings of Significance and Integrity .....	191
3.15.5 Site Specific Conservation Policies .....	192
<b>3.16 Luggage Dock.....</b>	<b>194</b>
3.16.1 History of the Luggage Dock .....	194
3.16.2 Site Description and Condition .....	195
3.16.3 Statement of Significance .....	197
3.16.4 Gradings of Significance and Integrity .....	198
3.16.5 Site Specific Conservation Policies .....	198
<b>3.17 Parcels Area.....</b>	<b>200</b>
3.17.1 History of the Parcels Area .....	200
3.17.2 Site Description and Condition .....	202
3.17.3 Statement of Significance .....	205
3.17.4 Gradings of Significance and Integrity .....	205
3.17.5 Site Specific Conservation Policies .....	206
<b>3.18 YHA Railway Square (Former Inwards Parcel Shed).....</b>	<b>208</b>
3.18.1 History of the YHA Railway Square (Former Inwards Parcel Shed) .....	208



3.18.2	Site Description and Condition .....	214
3.18.3	Statement of Significance .....	217
3.18.4	Gradings of Significance and Integrity .....	217
3.18.5	Site Specific Conservation Policies .....	218
<b>3.19</b>	<b>Former Parcels Post office.....</b>	<b>221</b>
3.19.1	History of the Former Parcels Post Office .....	221
3.19.2	Site Description and Condition .....	224
3.19.3	Statement of Significance .....	227
3.19.4	Gradings of Significance and Integrity .....	228
3.19.5	Site Specific Conservation Policies .....	229

## FIGURES

Figure 1: Overall levels of significance for inventory items. ....	vii
Figure 2: Precinct 3 sub-precinct boundaries. ....	1
Figure 3: 1849 map of Devonshire Street Cemetery and the area which would become the Central Railway Precinct. ....	6
Figure 4: Proposed extension of Sydney Terminus 1900. ....	7
Figure 5: Central from Clocktower 1920. ....	8
Figure 6: Proposed design for the third Sydney Terminus, as published in 1902 in the <i>Evening News</i> . ....	8
Figure 7: Photograph of Sydney Terminal, over Western Forecourt 1930s. ....	8
Figure 8: Aerial of Sydney Terminal Precinct. Unknown date. ....	8
Figure 9: View of Western Forecourt from the former Parcels Office, 1980. ....	8
Figure 10: Eastern Elevation of the Sydney Terminus, 1920s. ....	9
Figure 11: Central Aerial view, date unknown. ....	9
Figure 12: View of Sydney Terminal Precinct from Railway Square, George Street. ....	10
Figure 13: View of Sydney Terminal Precinct from Railway Colonnade Drive, Ambulance Ave side. ....	10
Figure 14: View of Sydney Terminal Precinct from Belmore Park. ....	10
Figure 15: View down Eddy Avenue with Sydney Terminal Precinct on the left. ....	10
Figure 16: View of Sydney Terminal Precinct from Adina Hotel. ....	11
Figure 17: View of Western Forecourt of Sydney Terminal Precinct from Railway Colonnade Drive, Ambulance Ave side. ....	11
Figure 18: Precinct 3: Sydney Terminal – Opportunities and Constraints. ....	14
Figure 19: Main Terminus Building. ....	17
Figure 20: Central Station Main Terminus Building northern façade. ....	17
Figure 21: Construction work taking place Central Station Devonshire Street end 1905. ....	18
Figure 22: Sydney Terminal 1948. ....	19
Figure 23: Sydney Yard aerial 1955. ....	19
Figure 24: View from over Belmore Park of the Main Terminus Building, date unknown. ....	19
Figure 25: Postcard of Central Main terminus over Pitt St and Eddy Ave intersection, pre-1914. ....	19
Figure 26: Central Station Main Terminus Building north façade. ....	21
Figure 27: Central Station Main Terminus as viewed from the intersection of Eddy Avenue and Pitt Street. ....	21
Figure 28: Sandstone detailing to the ashlar north façade. ....	21
Figure 29: Sandstone detailing to the ashlar façade, NSW and GR insignia badges, north facade. ....	21
Figure 30: Ashlar and rusticated sandstone, north-east facades. ....	22
Figure 31: Clocktower and sandstone on the west façade, including entrance canopy. ....	22
Figure 32: Rusticated sandstone, west façade at Grand Concourse level. ....	22

Figure 33. Brick east façade features sandstone banding and arches, with ashlar sandstone at upper levels.....	22
Figure 34. Grand Concourse steel roof structure. ....	23
Figure 35. Original cast iron gutters to the Grand Concourse roof lantern. ....	23
Figure 36. Brick and sandstone internal façade walls of the Main Terminus Building (north-east corner) and Grand Concourse roof. ....	23
Figure 37. East roof and sandstone balustraded parapet wall. Note the replaced sandstone balusters, carved to match the originals.....	23
Figure 38. Eddy Avenue Colonnade, Arcades and Shops. ....	28
Figure 39. Eddy Avenue Colonnade, Arcades and Shops. ....	28
Figure 40: Eddy Avenue 1901 .....	29
Figure 41: Eddy Avenue arcades 1905. ....	29
Figure 42: Eddy Avenue facing towards Pitt Street, c. 1940s. ....	30
Figure 43: Eddy Arcade, 1995s. ....	30
Figure 44: Eddy Avenue arcade entrance, date unknown. ....	30
Figure 45. Eddy Avenue arcade and colonnade as viewed from Belmore Park and Eddy Avenue. ....	31
Figure 46. Eddy Avenue Colonnade looking west. ....	32
Figure 47. Eddy Avenue Colonnade looking east. ....	32
Figure 48. Eddy Avenue Colonnade looking east. ....	32
Figure 49. Intersection of Eddy Avenue and Pitt Street Arcades, original Central Railway Station Signage.....	32
Figure 50. Eddy Avenue Colonnade at intersection with western tram ramp. ....	33
Figure 51. Installation of escalators to the Porte Cochere from the north-west corner of the Eddy Avenue Arcade. ....	33
Figure 52. RRR Store. ....	34
Figure 53. inside an arcade store, original leadlight glazing to shop window fanlight.....	34
Figure 54. Inside store, intertenancy walls, some may have been infilled or opened to change spatial arrangements of the shops.....	34
Figure 55. An original cast iron spiral staircase located at the rear of the tenancy.....	34
Figure 56. St Pancras Iron Work Company stamp on spiral stair tread. ....	34
Figure 57. Original vaulted ceiling retained, intrusive mezzanine additions in some tenancies. ....	34
Figure 58. One of the original female bathroom spaces located at the mezzanine and accessed from the spiral stairs. Finish and fittings are not original. ....	35
Figure 59. Inside store, intertenancy walls, some may have been infilled or opened to change spatial arrangements of the shops.....	35
Figure 60. Porte-Cochere. ....	39
Figure 61. Porte-Cochere. ....	39
Figure 62: Tram Platform and Porte-Cochere, 1951.....	40
Figure 63: Porte-Cochere in Postcard from 1908. ....	40

Figure 64: Porte-Cochere, 1909. ....	40
Figure 65: Colonnade Tram Stop with Porte-Cochere Roof top right, 1924. ....	40
Figure 66: Porte-Cochere, eastern end showing timber valance and infilled glazing. ....	41
Figure 67: Inside the porte-cochere. ....	41
Figure 68: Low sandstone balustrade wall and columns on the north edge of the porte-cochere form part of the original 1906 north façade of the Main Terminus Building. ....	42
Figure 69: Early clock suspended from the metal truss work. ....	42
Figure 70: Three-pin truss roof of porte-cochere. ....	42
Figure 71: Iron support brackets to truss roof. ....	42
Figure 72: Entrance to the Grand Concourse from the porte-cochere. ....	42
Figure 73: Commemorative bronze tablet which reads 'This Tablet – Commemorative of Opening the Station for Public Traffic – was affixed on Aug. 4 <sup>th</sup> 1906 – Hon. JH Carruthers, Premier – Hon. CA Lee, Min. for Pub. Works'. ....	42
Figure 74: Decorative leadlight glazing to windows associated with the former booking hall and dining room. ....	43
Figure 75: Other plate glass windows have a decorative scroll pattern etched into the bottom. ....	43
Figure 76: Tram ramps. ....	47
Figure 77: Eastern tram ramp. ....	47
Figure 78: Tram line leaving Sydney Railway Station, 1935. ....	49
Figure 79: Tram ramp, date unknown. ....	49
Figure 80: Central Station and Tram ramp, 1906. ....	49
Figure 81: Post card with overall Porte-Cochere, date unknown. ....	49
Figure 82: Western approach ramp leading into the Main Terminus building. This ramp is now used for light rail departure. ....	49
Figure 83: Central tram ramp, date unknown. ....	49
Figure 84: Tram Ramp, c. 1920s. ....	50
Figure 85: Pitt Street side of the tram ramp, 2001. ....	50
Figure 86: Eastern tram ramp approach from the north-eastern end of Belmore Park. ....	51
Figure 87: Western tram ramp approach from the north-western end of Belmore Park. ....	51
Figure 88: c.1906 sandstone pier signifying the start of the western tram ramp at Hay Street. ....	51
Figure 89: Sandstone pillars on the eastern tram ramp signifying the change from Belmore Park to Eastern Underbridge. ....	51
Figure 90: Eastern tram ramp as viewed from the Eastern Deck. ....	52
Figure 91: Eastern tram ramp looking towards Belmore Park. ....	52
Figure 92: Eastern tram ramp from Eddy Avenue looking south. ....	52
Figure 93: Eastern tram ramp, as viewed from Eddy Avenue. ....	52
Figure 94: Eastern tram ramp parallel to the Elizabeth Street viaduct. ....	52
Figure 95: Underside of the Eastern tram ramp supported by sandstone abutments. ....	52

Figure 96. Reinforced concrete underside, supported by steel on sandstone abutments. ....	53
Figure 97. Eastern tram ramp looking towards the Main Terminus Station from Eddy Avenue. ....	53
Figure 98. Western tram ramp at the exit from the porte-cochere. ....	53
Figure 99. Western tram ramp looking north. ....	53
Figure 100. Western underbridge as viewed from Eddy Avenue. ....	54
Figure 101. Western underbridge from Eddy Avenue. ....	54
Figure 102. Under the western underbridge. ....	54
Figure 103. Western tram ramp shop fronts along Pitt Street. ....	54
Figure 104. Western underbridge looking south towards the Eddy Avenue and Pitt Street Arcades. .	54
Figure 105. Western underbridge and western tram ramp shop fronts along Pitt Street. ....	54
Figure 106. Western tram ramp shop fronts along Pitt Street. ....	55
Figure 107. Western tram ramp sandstone wall extends and terminates at a sandstone pier at the corner of Hay and Pitt Streets, at which point the tramline continues at street level. ....	55
Figure 108. Western Forecourt. ....	59
Figure 109. Western Forecourt. ....	59
Figure 110: Proposed design for the Western Forecourt, as published in a local newspaper. ....	60
Figure 111: Western Forecourt, early photo – date unknown. ....	61
Figure 112: Western Forecourt Garden in 1927. ....	61
Figure 113: Western Forecourt 1930. ....	61
Figure 114: Western Forecourt, 1954. ....	62
Figure 115: Western Forecourt heralding the arrival of the Queen in 1954. ....	62
Figure 116. West Wing Extension and eastern boundary of the forecourt. ....	63
Figure 117. Eastern boundary wall, constructed from brick with a sandstone saddleback coping. ....	63
Figure 118. Eastern boundary wall terminates in a sandstone pier and lampstand. ....	63
Figure 119. West Wing Extension and ‘Governor’s Arch’ as viewed from the Central Garden. ....	63
Figure 120. Main Terminus Building awning, with timber vallance with arched wire glazed inserts. ....	64
Figure 121. Southern extent of the Main Terminus Building awning. ....	64
Figure 122. Main Terminus Building awning wraps around the north-west of the building to join with the porte-cochere. ....	64
Figure 123. Awning is supported by steel bracing trusses on cast iron columns and sandstone brackets. ....	64
Figure 124. Sandstone west boundary wall seen from entry to Western Forecourt. ....	65
Figure 125. West boundary wall and London Plane trees on Pitt Street. ....	65
Figure 126. Northern lightwell as viewed from the Central Garden. ....	65
Figure 127. Northern lightwell, sandstone plinth and cast-iron palisade fencing. ....	65
Figure 128. Northern lightwells as viewed from west looking towards the Main Terminal Building. ....	66
Figure 129. Looking down into the Northern lightwell to loading dock below. ....	66

Figure 130. Western Forecourt, the Central Garden to the left. ....	66
Figure 131. The Central Garden, non-original path bisects north-south through the garden. ....	66
Figure 132. Original sandstone boundary wall with low cast iron balustrade and chain fencing. ....	67
Figure 133. Looking south down the Western Forecourt to the Central Garden. ....	67
Figure 134. Original sandstone plinth with replica cast iron lampstand. ....	67
Figure 135. Detail of lampstand embossing which has the NSW GR insignia. ....	67
Figure 136. Pitt Street Arcade. ....	71
Figure 137. Pitt Street Arcade. ....	71
Figure 138: Pitt Street Arcade 1930s. ....	72
Figure 139: Pitt Street Arcade intersection, 1923. ....	72
Figure 140: Pitt Street Arcade, 1963. ....	72
Figure 141: Pitt Street Arcade, 1963. ....	72
Figure 142: Corner of Eddy Ave and Pitt Street Arcade, 1959. ....	72
Figure 143: Corner of Eddy Ave and Pitt Street Arcade, 1996. ....	72
Figure 144. Pitt Street Arcade. ....	74
Figure 145. Corner of Pitt Street and Eddy Avenue Arcades, with original Central Railway signage. ....	74
Figure 146. Pitt Street rusticated sandstone wall. ....	74
Figure 147. Termination of the Pitt Street wall into ashlar sandstone and pier with lampstand at the entrance to the Western Forecourt. ....	74
Figure 148. Pitt Street Arcade. ....	75
Figure 149. Pitt Street Arcade. ....	75
Figure 150. Shop front 480 in Pitt Street Arcade. ....	75
Figure 151. Shop front 490 in Pitt Street Arcade. ....	75
Figure 152. Typical interior of Pitt Street Arcade, original arched party walls, intrusive mezzanines and fitouts. ....	76
Figure 153. Typical interior of Pitt Street Arcade, original arched party walls, intrusive mezzanines and fitouts. ....	76
Figure 154. East Deck. ....	80
Figure 155. East Deck looking north. ....	80
Figure 156: Eddy Avenue Forecourt and shops with east deck on the right hand side, 1984. ....	81
Figure 157. The eastern façade of the Main Terminus Building as presented to the East Deck. ....	82
Figure 158. East Deck looking north. ....	82
Figure 159. The eastern façade of the Main Terminus Building features a juxtaposition of rusticated sandstone on the ‘primary elevations’ and brick with sandstone banding on the ‘secondary elevations’ ....	82
Figure 160. East Deck looking south. ....	82
Figure 161. North-eastern corner of the East Deck at its intersection with the eastern tram ramp. ....	82
Figure 162. Main Terminus Building and East Deck. ....	82



Figure 163. East Deck looking towards the porte-cochere. ....	83
Figure 164. Escalator insertions and canopy at the southern end of the East Deck. ....	83
Figure 165. Original stair leading from the East Deck to the Eddy Avenue forecourt. ....	83
Figure 166. Under the East Deck within the Eddy Avenue forecourt colonnades. ....	83
Figure 167. Escalator insertions into the East Deck. ....	83
Figure 168. East Deck as viewed from Eddy Avenue forecourt. ....	83
Figure 169. Grand Concourse and related spaces. ....	87
Figure 170. Grand Concourse. ....	87
Figure 171: Central Railway Grand Concourse with fruit and pastry stands c.1900. ....	88
Figure 172: Central Railway Grand Concourse soda fountain, 1906. ....	88
Figure 173: Grand Concourse 1927. ....	89
Figure 174: Grand Concourse 1956. ....	89
Figure 175: Grand Concourse 1962. ....	89
Figure 176: Central Assembly Platform, 2 tone source, date unknown -presumably early 1900s .....	89
Figure 177: Roof and Truss of the Grand Concourse in 1906. ....	90
Figure 178: Roof and Truss of Grand Concourse in 1986. ....	90
Figure 179: Design for the main dining hall and ladies dining hall, date unknown. ....	91
Figure 180: Main Dining Hall 1906. ....	92
Figure 181: The Bar 1906. ....	92
Figure 182: People eating in the Dining Hall in 1948. ....	92
Figure 183: Dining Hall, date unknown. ....	92
Figure 184: Two people purchasing tickets in the Booking Hall, date unknown. ....	93
Figure 185: Booking Hall, date unknown. ....	93
Figure 186: Booking Hall in 1906. ....	93
Figure 187: Central Booking Hall, early 1900s. ....	93
Figure 188: Booking Hall around time of the 1960s modifications. ....	94
Figure 189: Booking Hall around time of the 1960s modifications with Avis Rental Offices. ....	94
Figure 190. Entrance into the Grand Concourse from the Western Forecourt. ....	96
Figure 191. Historic signage in the Western Forecourt entrance into the Grand Concourse. ....	96
Figure 192. Grand Concourse and the northern wall/openings. ....	96
Figure 193. Grand Concourse, western wall and entrance portal to the Western Forecourt. ....	96
Figure 194. Grand Concourse, northern wall and War Memorials. ....	96
Figure 195. Close up of the War Memorials. ....	96
Figure 196. Sandstone entrance to the porte-cochere portal on the northern wall. ....	97
Figure 197. Original or early Central clock on the northern wall west of the porte-cochere portal entrance. ....	97
Figure 198. Commemorative bust and plaque to John Whitton 'Father of the NSW Railways'. ....	97

Figure 199. Commemorative plaque for the centenary of Central Station. ....	97
Figure 200. Grand Concourse and barrel roof looking west. ....	97
Figure 201. Grand Concourse and barrel roof looking east. ....	97
Figure 202. Grand Concourse barrel roof and skylight, with contemporary indicator board to the south, early clock suspended from roof. ....	98
Figure 203. Grand Concourse looking south to the Country and Intercity platforms. ....	98
Figure 204. Grand Concourse roof supported by cast iron columns and cast iron lattice at the intersection with the platform assembly area roof. View looking west includes historic ventilator for the former Men's latrines. ....	98
Figure 205. Grand Concourse roof supported by cast iron columns and cast iron lattice at the intersection with the platform assembly area roof. View looking east includes historic ventilator for the former Men's latrines. ....	98
Figure 206. Commemorative plaque for the passenger train service linking Sydney to Perth, c.1970 outside the TrainLink office.....	99
Figure 207. TrainLink located on platform 1, south of the Grand Concourse. ....	99
Figure 208. Clocktower foyer.....	99
Figure 209. Clocktower foyer and Police Command entrance.....	99
Figure 210. Clocktower stairwell. ....	100
Figure 211. Doors from the clocktower foyer to the Grand Concourse.....	100
Figure 212. Eternity Bar and Grill within the former Booking Hall. ....	102
Figure 213. Within the former Booking Hall looking south to the glazed entrance into the Grand Concourse. ....	102
Figure 214. Decorative lead glass windows, marble and sandstone to the north wall of the space..	102
Figure 215. Large arched windows and door openings with sandstone console brackets and decorative leadlight windows on the north wall. ....	102
Figure 216. Large arched windows and door openings with sandstone console brackets and decorative leadlight windows on the north wall. ....	102
Figure 217. Decorative blind sandstone arches and console brackets continue the pattern from the north wall onto the western wall. ....	102
Figure 218. Non-original coffered ceiling divides the original Booking Hall volume.....	103
Figure 219. Commemorative plaque for the new Booking Hall Lounge, c.1964.....	103
Figure 220. Former Ladies Dining Room. ....	104
Figure 221. Decorative coffered plaster ceiling supported by columns. ....	104
Figure 222. Melocco Brothers Mural and inlay floor.....	104
Figure 223. Melocco Brothers inlay floor. ....	104
Figure 224. Melocco Brothers signage within the inlay floor.....	104
Figure 225. Detail of the Melocco Brothers mural.....	104
Figure 226. Decorative plaster ceiling. ....	105
Figure 227. Former Ladies Dining Room. ....	105

Figure 228. Former Ladies Dining Room. ....	105
Figure 229. Melocco Brothers inlay floor and mural.....	105
Figure 230. Former Dining Hall entrance from the Grand Concourse. ....	106
Figure 231. Newslink, looking east.....	107
Figure 232. Newslink, looking west. ....	107
Figure 233. Main Terminus Building: Office Spaces. ....	116
Figure 234. Level 1 – north office.....	116
Figure 235. Grand internal staircase at level 2.....	118
Figure 236. View up through stair void.....	118
Figure 237. Grand internal staircase . ....	118
Figure 238. Grand internal staircase lobby. ....	118
Figure 239. Entry to grand internal staircase from light rail.....	118
Figure 240. Staircase at basement level. ....	118
Figure 241. Staircase at Level 2.....	119
Figure 242. Staircase looking down from Level 3. ....	119
Figure 243. Staircase looking down from Level 3. ....	119
Figure 244. Staircase looking down from Level 3. ....	119
Figure 245. Office space level 1 North. ....	120
Figure 246. Office space level 2 office fitout. ....	120
Figure 247. Office space level 3 office fitout. ....	120
Figure 248. Office space level 1 North. ....	121
Figure 249. Office space level 1 North. ....	121
Figure 250. Office space level 1 North. ....	121
Figure 251. Office space level 1 office fitout. ....	121
Figure 252. Office space level 1 office fitout. ....	121
Figure 253. Office space level 1.....	121
Figure 254. Office space level 1 West.....	122
Figure 255. Office space stair level 1 West.....	122
Figure 256. Office space level 2 Police Offices.....	122
Figure 257. Office space level 2 Police Offices.....	122
Figure 258. Office space level 2 post fitout. ....	123
Figure 259. Office space level 2 post fitout. ....	123
Figure 260. View towards Clock tower from office space level 2. ....	123
Figure 261. View down stair void from level 2.....	123
Figure 262. Office space level 2 east. ....	123
Figure 263. Office space level 2 west. ....	123

Figure 264. Office space level 3. ....	124
Figure 265. Office space level 3 post fitout. ....	124
Figure 266. Office space level 3 post fitout. ....	124
Figure 267. Office space roof area. ....	124
Figure 268. Central Station Clocktower. ....	128
Figure 269. Central Station Clocktower. ....	128
Figure 270: Central Station Clocktower under construction ....	129
Figure 271: Man holding the hands for the clock tower, date unknown. ....	129
Figure 272: Central Station Clocktower under construction ....	130
Figure 273: Central Station Clocktower, date unknown. ....	130
Figure 274. View of Clock tower from Roof. ....	132
Figure 275. Figure 276. View of Balustrades from Clock tower from Roof. ....	132
Figure 277. Figure 278. View from Clock tower roof over Central station. ....	132
Figure 279. View up through stair void to clock tower. ....	132
Figure 280. View of circular stair to Clock tower. ....	133
Figure 281. View of upper level circular stair to clock tower. ....	133
Figure 282. View of upper level circular stair to clock tower. ....	133
Figure 283. Street view of Clock tower from Eddy Ave. ....	133
Figure 284. View of basement of clock tower. ....	134
Figure 285. View in basement foyer of Clock tower. ....	134
Figure 286. West Wing Extension ....	138
Figure 287. West Wing Extension to the right of the photo looking north. ....	138
Figure 288: West Wing Extension, 1920s. ....	139
Figure 289: West Wing Extension during the Queen's visit in 1958. ....	139
Figure 290. View of West Wing looking towards the Clock Tower. ....	141
Figure 291. View of West Wing looking away from Clock Tower. ....	141
Figure 292. View of connection of the West Wing to the main terminus. ....	142
Figure 293. View of West Wing balustrades ....	142
Figure 294. View of West Wing entry from Western Forecourt. ....	142
Figure 295. View of Platform 1, on the West Wing extension ....	142
Figure 296. Former Lavatory, part of the West Wing Extension. ....	143
Figure 297. External view of Concourse level Waiting Room from the platform 1. ....	144
Figure 298. Internal view of Concourse level Waiting Room Tower. ....	144
Figure 299. Internal view of Concourse level Waiting Room Tower. ....	144
Figure 300. Internal view of Platform Level Offices. ....	145
Figure 301. Internal view of Lower-Level Office corridor. ....	145

Figure 302. Internal view of Lower-Level Office corridor.....	145
Figure 303. Internal view of stairs leading out of the Basement level. ....	146
Figure 304. Internal view of stairs looking down towards the basement level. ....	146
Figure 305. Internal view of Basement level office.....	146
Figure 306. Country and Interstate Platforms. ....	152
Figure 307. Country and Interstate Platforms. ....	152
Figure 308: Country and Interstate Platform 3, date unknown. ....	154
Figure 309: Country and Interstate Platform historic timber seating, c. 1951. ....	154
Figure 310: Country and Interstate Platform, date unknown. ....	155
Figure 311: Royal Tour, with Royal train parked on Platform 1, c1954. ....	155
Figure 312. View of Platform 1 buildings.....	157
Figure 313. View looking out from Platform 1. ....	157
Figure 314. View looking out from Platform 1. ....	157
Figure 315. View looking down the platform. ....	157
Figure 316. View of the end of the Country and Interstate Train Platforms. ....	158
Figure 317. View of the end of the Country and Interstate Train Platforms. ....	158
Figure 318. View from above of the Country and Interstate platforms.....	158
Figure 319. View of end of platform building facing down.....	158
Figure 320. View of the roof structure on platform 5.....	159
Figure 321. View of the roof and awning structure at Platform 5. ....	159
Figure 322. View of the awning structure at Platform 5. ....	159
Figure 323. View from Platform 5.....	159
Figure 324. View of roof structure at Platform 5.....	160
Figure 325. View of 'Central' station sign. ....	160
Figure 326. View of plaque.....	160
Figure 327. View of underside of Goods Lift. ....	161
Figure 328. View of awnings and roof structure at Grand Concourse over country and interstate platforms. ....	162
Figure 329. View of awnings and roof structure of platforms entering Grand Concourse. ....	162
Figure 330. View of awnings and roof structure at Platform 9. ....	162
Figure 331. View of awnings and roof structure at Platform 2-3. ....	162
Figure 332. Former Lavatory signage. ....	163
Figure 333. Subway passage system. ....	168
Figure 334. Subway passage system. ....	168
Figure 335. View of north tunnel. ....	170
Figure 336. View of north tunnel. ....	170
Figure 337. View of signage in tunnel. ....	170

Figure 338. View of south tunnel.....	172
Figure 339. View of south tunnel looking towards platform 4/5.....	172
Figure 340. View of south tunnel.....	172
Figure 341. View of south tunnel.....	172
Figure 342. View of temporary pedestrian tunnel. ....	173
Figure 343. View of temporary pedestrian tunnel. ....	173
Figure 344. View of view pedestrian tunnel.....	173
Figure 345. View of temporary pedestrian tunnel leading to Platforms 1-23. ....	173
Figure 346. View of pedestrian tunnel looking towards the entry of Platform 16/17. ....	173
Figure 347. Devonshire Street tunnel.....	177
Figure 348. Devonshire Street tunnel looking east. ....	177
Figure 349: Inside the Devonshire Street Tunnel c.1968. ....	178
Figure 350: Aerial view of Devonshire Street tunnel from west, at its opening near the Parcels Post Office on Lee Street.....	178
Figure 351: Devonshire Tunnel opening to the south of the Parcels Post Office, c.1986. ....	179
Figure 352. View of Devonshire street pedestrian tunnel . ....	180
Figure 353. View of Devonshire street pedestrian tunnel . ....	180
Figure 354. View of Devonshire street pedestrian tunnel exit towards Railway Square.....	180
Figure 355. View of Devonshire street pedestrian tunnel exit towards Elizabeth Street. ....	180
Figure 356. View of ticket machines when entering Central station from the Devonshire street pedestrian tunnel. ....	180
Figure 357. View of Devonshire street interpretation signage. ....	180
Figure 358. View of Devonshire street interpretation signage. ....	181
Figure 359. View of Devonshire street interpretation signage. ....	181
Figure 360. Main Terminus Building basement.....	185
Figure 361. Electricians Workshop in basement of Main Terminus Building.....	185
Figure 362. View of Grand Staircase leading up to level 1 from the Basement.....	187
Figure 363. View of Basement Clock Room.....	187
Figure 364. View of Mezzanine above cleaners store (B-44). ....	187
Figure 365. View of Basement entry from Eddy Ave. ....	187
Figure 366. View of Basement entry from Eddy Ave. ....	188
Figure 367. View of Basement entry from Eddy Ave. ....	188
Figure 368. View of Basement baggage tunnel. ....	188
Figure 369. View of Basement baggage tunnel. ....	188
Figure 370. View of Basement street level doors into BW-05.....	188
Figure 371. Internal view of Basement BW-05.....	188
Figure 372. View of Basement cleaners store B-26.....	189



Figure 373. View of Basement Mezzanine BN-08. ....	189
Figure 374. View of Basement southern disused room. ....	189
Figure 375. View of Basement southern disused room. ....	189
Figure 376. View of Basement southern disused room. ....	189
Figure 377. View of Basement electricians workshop.....	189
Figure 378. View of Basement electricians workshop.....	190
Figure 379. View of Basement electricians workshop.....	190
Figure 380. View out of room to the east of RRR Loading dock. ....	190
Figure 381. View of RRR loading dock. ....	190
Figure 382. View of RRR loading dock. ....	190
Figure 383. View of south west mezzanine.....	190
Figure 384. View of south west mezzanine.....	191
Figure 385. View of south west mezzanine.....	191
Figure 386. Luggage Dock. ....	194
Figure 387. Luggage Dock. ....	194
Figure 388: Loading luggage into the luggage dock, date unknown. ....	195
Figure 389: Loading luggage into the luggage dock, date unknown. ....	195
Figure 390: Loading luggage from a truck, with Tivoli signage, date unknown. ....	195
Figure 391. View within luggage dock.....	196
Figure 392. View of storage cage within luggage dock.....	196
Figure 393. View of Basement auction room. ....	196
Figure 394. View leading into Basement luggage dock. ....	196
Figure 395. Rear external view of luggage dock. ....	197
Figure 396. Rear external view of luggage dock from Eddy Ave. ....	197
Figure 397. View above luggage dock.....	197
Figure 398. Parcels Area.....	200
Figure 399. Parcels Area.....	200
Figure 400: Parcels Area on the right hand side, at the end of Ambulance Ave, 1938. ....	201
Figure 401. External view of Post Office Building at the beginning of Ambulance Ave. ....	204
Figure 402. External view of up Ambulance Ave. ....	204
Figure 403. External view of delivery entrance . ....	204
Figure 404. External view of delivery entrance. ....	204
Figure 405. View up Ambulance Ave.....	204
Figure 406. External view of Parcels Building. ....	204
Figure 407. External view of brick retaining wall.....	205
Figure 408. External view of sandstone column and lamp post.....	205

Figure 409. Former Inwards Parcel Shed (YHA). .....	208
Figure 410. Former Inwards Parcel Shed (YHA). .....	208
Figure 411: 1906 photograph of the western façade of the Former Inwards Parcels Shed. The second Central Station is still extant to the right of frame while the present Central Station is to the left of frame. The Parcels Post Office has not been constructed; however, the area has been boarded up around the site. ....	211
Figure 412: c.1906-1912 photograph of Railway Square, showing the Former Inwards Parcels Shed on the right side of the frame (indicated by red arrow). ....	211
Figure 413: 1965 Plan of Sydney Terminal and Central Station, demonstrating the use of the Inwards Parcels Shed for outwards parcels (the Parcels Area, Inwards Parcels Shed and the ramp outlined in red). ....	212
Figure 414: Inside the Former Inwards Parcels Shed, date unknown. ....	212
Figure 415: Outside the Former Inwards Parcel Shed, date unknown. ....	212
Figure 416: Worker inside the Former Inwards Parcel Shed, date unknown. ....	213
Figure 417: Workers on the deck of the Former Inwards Parcel Shed, date unknown. ....	213
Figure 418: A 1928 photograph from the Western Forecourt, showing the northern elevation of the Former Inwards Parcels Shed and the brick retaining wall along the ramp of Upper Carriage Lane. ....	214
Figure 419. Interior of the Inwards Parcel Shed .....	214
<b>Figure 420. Eastern end of Ambulance Avenue, showing west wing extension building in background and entrance to Former Inward Parcels Shed basement level. (Source: Artefact Heritage, 2021.) .....</b>	<b>216</b>
Figure 421. External view of YHA Hostel extension. ....	216
Figure 422. External view of YHA Hostel, facing former Parcels Office, now Adina Hotel. ....	216
Figure 423. External view of YHA Hostel car park. ....	217
Figure 424. External view of YHA Hostel along deck. ....	217
Figure 425. Former Parcels Post Office (now Adina Apartment Hotel). ....	221
Figure 426. Former Parcels Post Office (now Adina Apartment Hotel) as viewed from Railway Square. ....	221
Figure 427: Railway Square with the Parcels Post Office in the front right and the Main Terminus Building beyond. Note the Clocktower was in constructions. ....	223
Figure 428: Southern view of the Parcels Post Office under construction .....	223
Figure 429: Parcel Post Office building c. 1989 prior to the removal of the awning. ....	223
Figure 430: Parcel Post Office building date unknown. ....	223
Figure 431. External view of Post Office building as seen from Pitt Street. ....	225
Figure 432. External view of Post Office Building as seen from Lee Street entrance. ....	225
Figure 433. External view from Post Office Building looking towards Lee Street / George St. ....	226
Figure 434. External view of Post Office Building looking towards Railway Square. ....	226
Figure 435. External view of Post Office Building as seen from Railway Square. ....	226
Figure 436. External view of Post Office Building signage. ....	226
Figure 437. External view of Post Office Building loading dock entry. ....	226

Figure 438. Part of former fence/signage to Parcels Office on retaining wall. ....	226
Figure 439. View of lamp post. ....	227

## TABLES

Table 1: NSW heritage assessment criteria .....	iii
Table 2: Levels of Cultural Significance .....	iv
Table 3: Levels of Integrity .....	v
Table 4: Overall significance for items in Precinct 3: Sydney Terminal .....	vi
Table 5: Site Specific Conservation Policies for the Overall Sydney Terminal Precinct .....	12
Table 6: Precinct 3: Sydney Terminal – Opportunities and Constraints .....	14
Table 7: Gradings of Significance and Integrity for the Main Terminus Building: Facades and Roof ..	24
Table 8: Site Specific Conservation Policies for the for the Main Terminus Building: Facades and Roof .....	24
Table 9: Gradings of Significance and Integrity for the Eddy Avenue Colonnade, Arcades and Shops .....	35
Table 10: Site Specific Conservation Policies for Eddy Avenue Colonnade, Arcades and Shops .....	36
Table 11: Gradings of Significance and Integrity for the Porte-Cochere .....	44
Table 12: Site Specific Conservation Policies for the Porte-Cochere .....	44
Table 13: Gradings of Significance and Integrity for the Tram Ramps .....	55
Table 14: Site Specific Conservation Policies for the for the Tram Ramps .....	56
Table 15: Gradings of Significance and Integrity for the Western Forecourt .....	68
Table 16: Site Specific Conservation Policies for the for the Western Forecourt .....	68
Table 17: Gradings of Significance and Integrity for the Pitt Street Arcade .....	76
Table 18: Site Specific Conservation Policies for the for the Pitt Street Arcade .....	77
Table 19: Gradings of Significance and Integrity for the East Deck .....	84
Table 20: Site Specific Conservation Policies for the for the East Deck .....	84
Table 21: Gradings of Significance and Integrity for the Main Terminus Building: Grand Concourse and Related Spaces .....	108
Table 22: Site Specific Conservation Policies for the for the Main Terminus Building: Grand Concourse and Related Spaces .....	112
Table 23: Gradings of Significance and Integrity for the Main Terminus Building: Office Spaces .....	125
Table 24: Site Specific Conservation Policies for the for the Main Terminus Building: Office Spaces .....	126
Table 25: Gradings of Significance and Integrity for the Clocktower .....	134
Table 26: Site Specific Conservation Policies for the for the Clocktower .....	135
Table 27: Gradings of Significance and Integrity for the West Wing Extension .....	147
Table 28: Site Specific Conservation Policies for the for the West Wing Extension .....	149
Table 29: Gradings of Significance and Integrity for the Country and Interstate Platforms .....	164
Table 30: Site Specific Conservation Policies for the for the Country and Interstate Platforms .....	165
Table 31: Gradings of Significance and Integrity for the Subway Passage Systems .....	174

Table 32: Site Specific Conservation Policies for the for the Subway Passage Systems .....	175
Table 33: Gradings of Significance and Integrity for the Devonshire Street Tunnel .....	181
Table 34: Site Specific Conservation Policies for the for the Devonshire Street Tunnel .....	182
Table 35: Gradings of Significance and Integrity for the Basement .....	191
Table 36: Site Specific Conservation Policies for the for the Basement .....	192
Table 37: Gradings of Significance and Integrity for the Luggage Dock .....	198
Table 38: Site Specific Conservation Policies for the for the Luggage Dock .....	198
Table 39: Gradings of Significance and Integrity for the Parcels Area .....	205
Table 40: Site Specific Conservation Policies for the for the Parcels Area .....	206
Table 41: Gradings of Significance and Integrity for the YHA Railway Square (Former Inwards Parcel Shed) .....	217
Table 42: Site Specific Conservation Policies for the for the YHA Railway Square (Former Inwards Parcel Shed) .....	218
Table 43: Gradings of Significance and Integrity for the Former Parcels Post Office .....	228
Table 44: Site Specific Conservation Policies for the for the Former Parcels Post Office .....	229



### 3.0 OVERALL PRECINCT

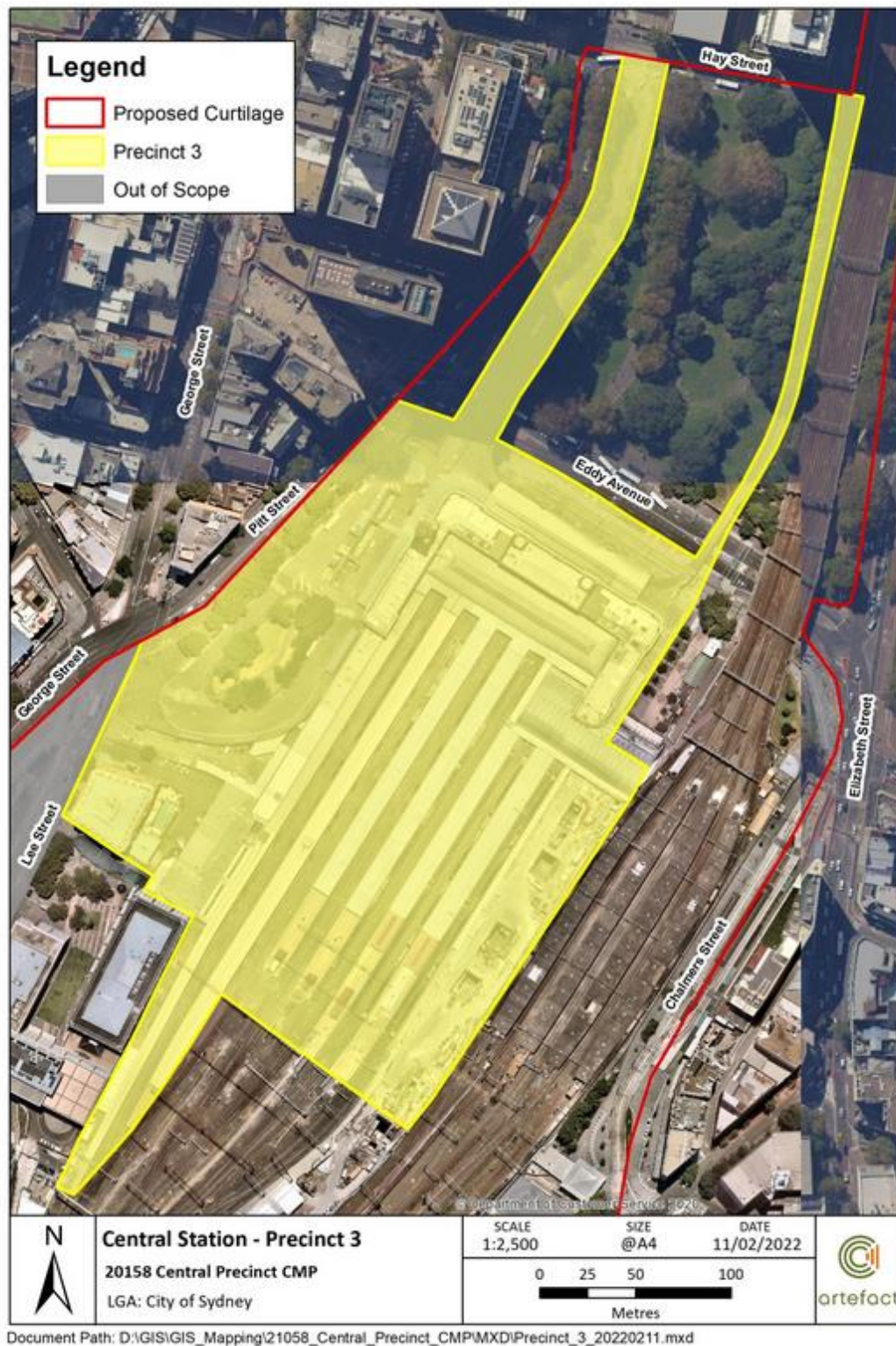
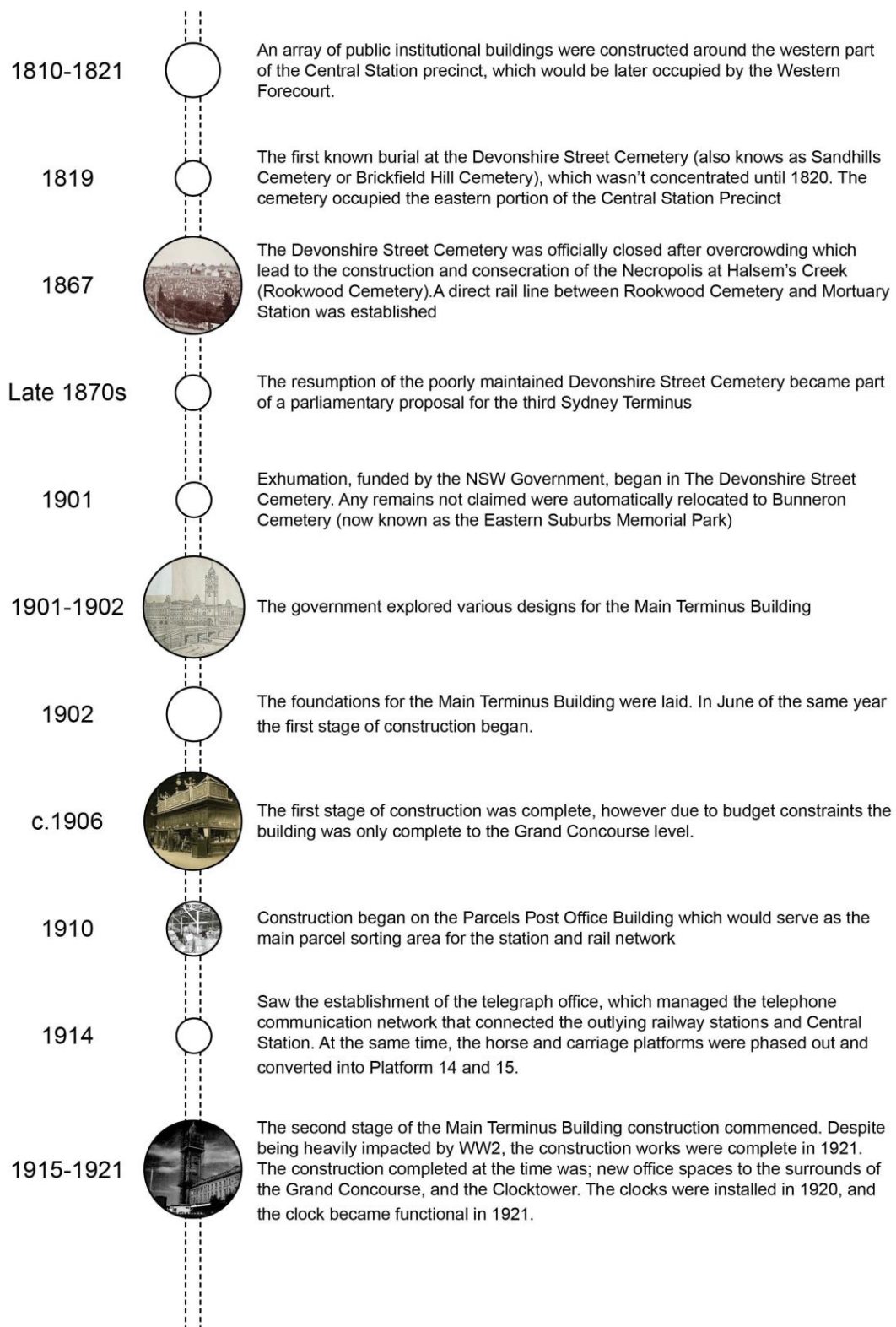


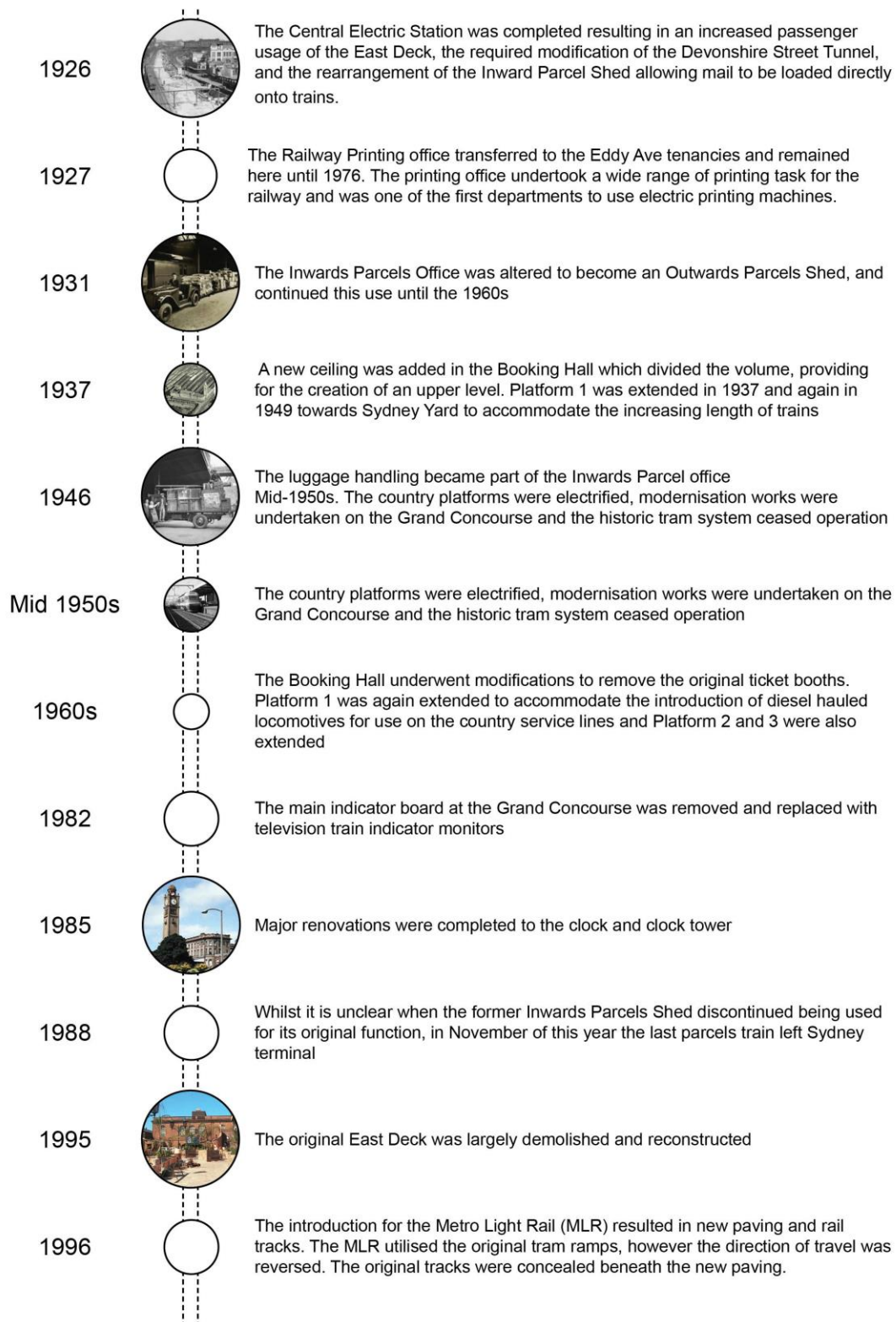
Figure 2. Precinct 3 sub-precinct boundaries.<sup>5</sup>

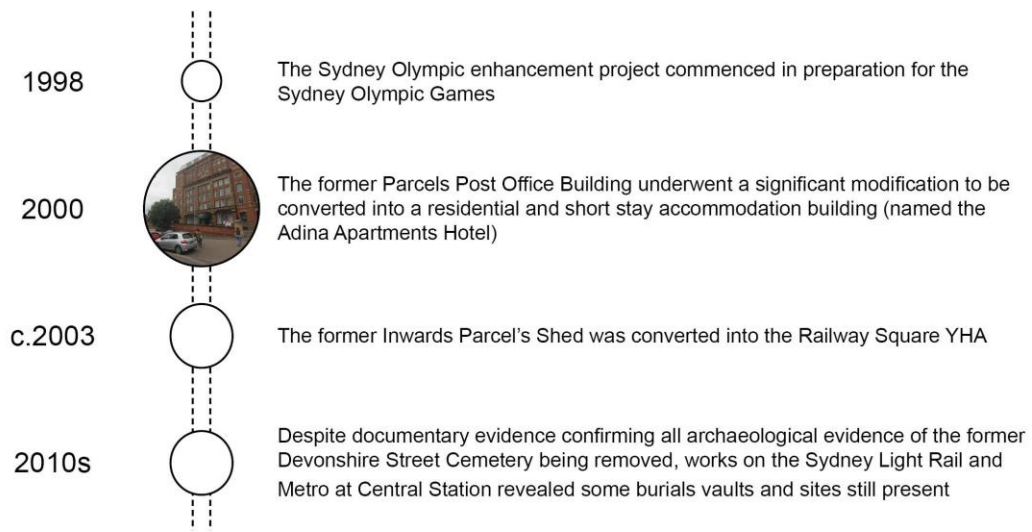
<sup>5</sup> Artefact, 2021



### 3.1.1 History of the Sydney Terminal







Prior to European occupation and development in the early 19th century, the land that is currently occupied by the Sydney Terminus was a large area of sand dunes and trees.

It was first developed in the Macquarie Period (1810-1821), when the government constructed an array of public institutions in the western part of the precinct. These included The Benevolent Asylum (1820), the Government Cottage (c. 1820), the Christ Church Parsonage (1855), the Carters Barracks (1818), Police Barracks (1820), the Police Magistrate's residence (1820s) and a Steam Tram Depot (1879). The Carters Barracks was located on Pitt Street and was later used as the Sydney Female Refuge and the Convent of the Good Samaritan, with other structures located in and around the location of the current Western Forecourt. All of these buildings were resumed and demolished in the early 20th century in order to construct the third Sydney Terminal Precinct.

The eastern area of the Precinct was occupied by the Devonshire Street Cemetery (also known as Sandhills Cemetery or Brickfield Hill Cemetery), which was consecrated in 1820, although the first known burial in the Cemetery was undertaken in 1819.<sup>6</sup> It was Sydney's second major cemetery, following George Street Burial Ground (also known as Old Sydney Burial Ground) which was located north of St Andrew's Cathedral, and in the area now occupied by Sydney Town Hall. The George Street Burial Ground was in operation between 1792-1819 when it was replaced by the Devonshire Street Cemetery.<sup>7</sup>

The Devonshire Street Cemetery was originally 4 acres (1.6 hectares) of land which was set aside for the Church of England burials after the closure of the George Street Burial Ground. Subsequent land allotments were allocated for other denominations upon application to the Colonial Government.<sup>8</sup> However, by the 1840s the cemetery was becoming increasingly overcrowded, and by 1867 was formally closed, after the opening and consecration of the Necropolis at Halsem's Creek (Rookwood Cemetery), and the provision of a direct train line connecting Sydney to the new cemetery (via Mortuary Station). By the late 1870s, the Devonshire Street Cemetery was poorly maintained and calls for its complete closure and removal were discussed, particularly in light of parliamentary proposals to resume the Cemetery for railway purposes.<sup>9</sup>

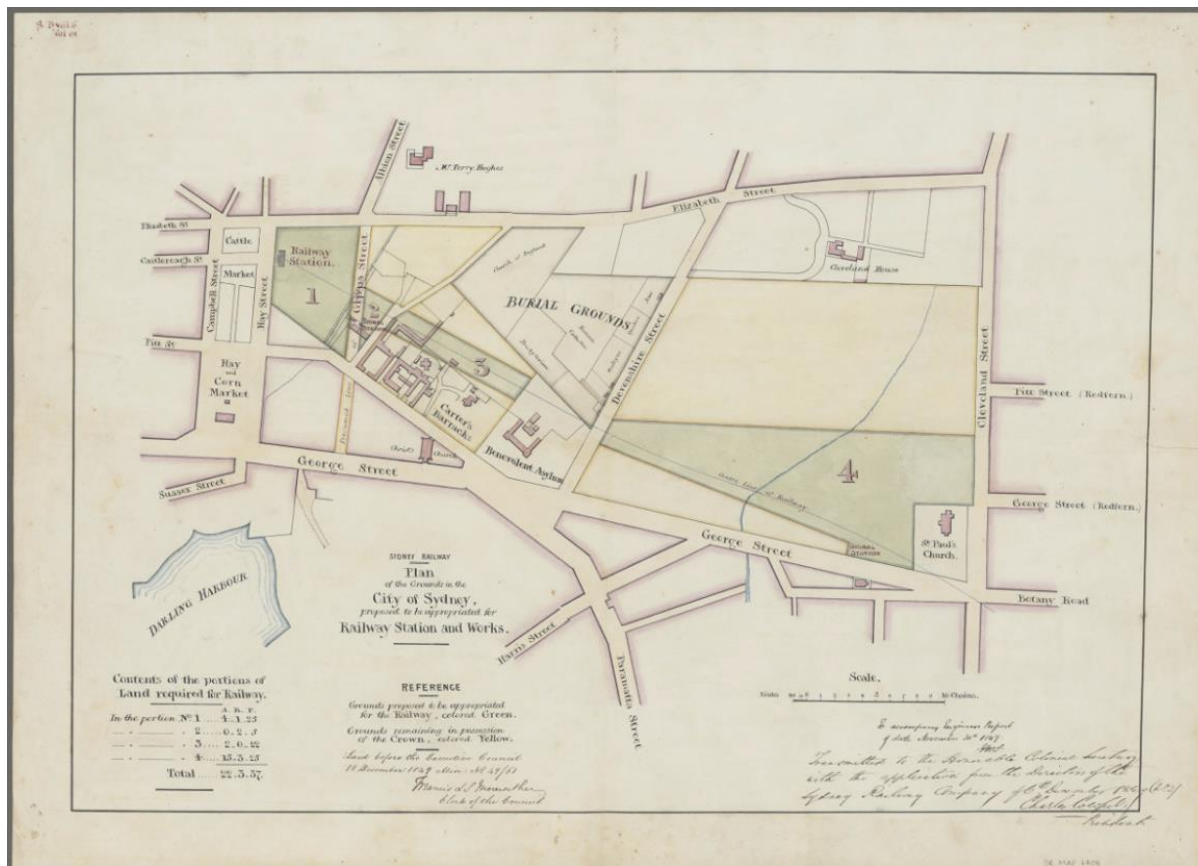
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<sup>6</sup> 'Government and General Orders', Sydney Gazette and New South Wales Advertiser, 29 January 1820, p1; Hugh McDonald, Quartermaster of the 46th Regiment was buried in the new cemetery on 9 September 1819, Sydney Gazette and New South Wales Advertiser, 11 September 1819, p3

<sup>7</sup> City of Sydney, 2019, 'Old Sydney Burial Ground', <https://www.cityofsydney.nsw.gov.au/history/old-sydney-burial-ground>, accessed 5/11/21.

<sup>8</sup> Murray, Lisa, 2019, 'Devonshire Street Cemetery', [https://dictionaryofsydney.org/entry/devonshire\\_street\\_cemetery](https://dictionaryofsydney.org/entry/devonshire_street_cemetery), accessed 5/11/21.

<sup>9</sup> 'Parliament', The Sydney Daily Telegraph, 21 October 1882, p6



**Figure 3: 1849 map of Devonshire Street Cemetery and the area which would become the Central Railway Precinct.<sup>10</sup>**

It was not until 1901 however, that exhumation of the remains began at the Devonshire Street Cemetery. Relatives of the deceased interred at the Cemetery were invited to apply for the exhumation and relocation of their relatives at the expense of the NSW Government to other cemeteries across greater Sydney. Remains which were not claimed by relatives, were relocated, and reburied at the purpose-built cemetery known as Bunnerong Cemetery, which was amalgamated with the neighbouring Botany Cemetery in the 1970s to create the Eastern Suburbs Memorial Park. Documentary evidence from the time indicated that all the remaining burial sites were completely exhumed, and that no archaeological evidence relating to the Cemetery remained, however in the 2010s as work for the Sydney light rail and Metro at Central Station commenced, a number of sealed burial vaults and other burial sites and remains were discovered during excavation works.

Development of the third Sydney Terminal began a few years after the exhumation of the cemetery. Designed and built in two stages: 1906 and 1915-1921, various structures were built to support and facilitate rail operations. These buildings included the Main Terminus building, the Eddy Avenue Colonnade, the Western Forecourt, country and interstate platforms, a Parcels Post Office and a subway tunnel system.

Extensive modifications occurred to the precinct at the turn of the 21st century. This was due to a shift in rail operations and the modification of rail technology. Numerous structures located in the precinct were demolished or adapted for new uses, including the Inwards Parcel Dock, the Parcels Post Office and the Inwards Parcel Shed. The Inwards Parcel Dock was demolished in c.1999-2000 due to the decline in the demand for the delivery of parcels by rail, and to accommodate the new development of the Henry Deane Plaza, while the Parcels Post Office was repurposed into a hotel and the Inwards

<sup>10</sup> NSW State Records, map ID: 6408



Parcels Shed underwent transformation into a Youth Hostel. In the process of adaptive reuse, some demolition of original fabric did occur, however, both structures retain original features and remain extant today.

Other components of the Sydney Terminal precinct that were modified in the late 20th century include the East Deck, Platforms and the Eddy Avenue Colonnade. The original East Deck was demolished and rebuilt in mid-1990s. At this time, the brick colonnade adjacent to the Eddy Avenue Forecourt was also constructed. Extensive modifications have also occurred to both the platforms and the tracks, including the truncation of the platforms, and the paving over of the original tram tracks to instal the Metro Light Rail tracks within the tram ramps and porte-cochere.

In 2018, works began to create the new Sydney Metro network and connection at Central Station. This has involved the removal and alteration of existing platform structures.

The Sydney Terminal is the central core of the Central Station Precinct and was listed on the State Heritage Register under *Sydney Terminal and Central Railway Stations Group* in 1999 (SHR no. 01255).

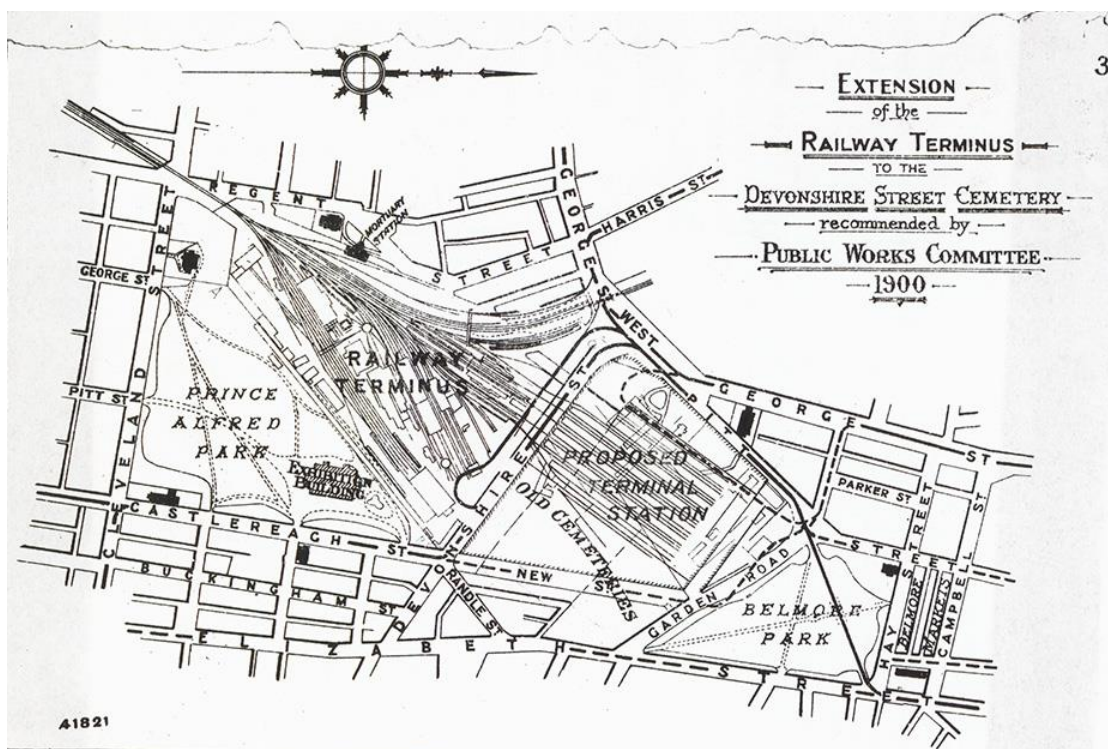
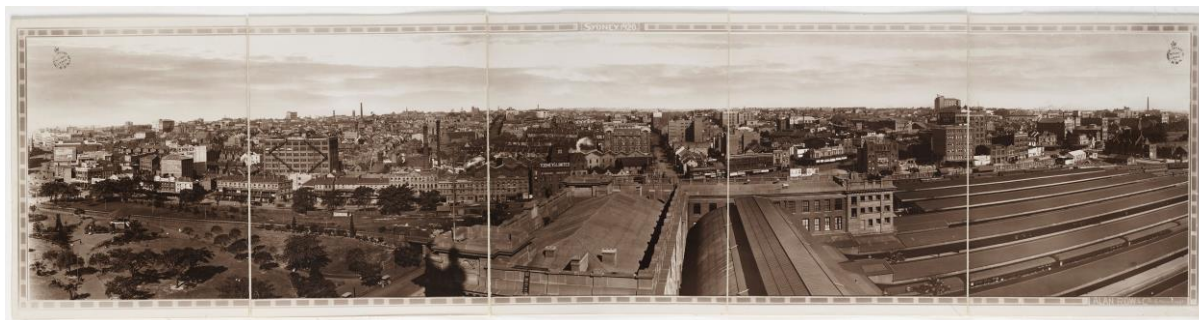


Figure 4: Proposed extension of Sydney Terminus 1900. 11



<sup>11</sup> Sydney Trains Archives, 1900. ID 140216.



Figure 5: Central from Clocktower 1920. <sup>12</sup>



Figure 6: Proposed design for the third Sydney Terminus, as published in 1902 in the *Evening News*.<sup>13</sup>



Figure 7: Photograph of Sydney Terminal, over Western Forecourt 1930s. <sup>14</sup>



Figure 8: Aerial of Sydney Terminal Precinct. Unknown date. <sup>15</sup>



Figure 9: View of Western Forecourt from the former Parcels Office, 1980. <sup>16</sup>

<sup>12</sup> Alan Row and Co, Sydney Trains Archives, 1920.

<sup>13</sup> 'Proposed design for the third Sydney Terminus', *Evening News*, 29 November 1902, page unknown

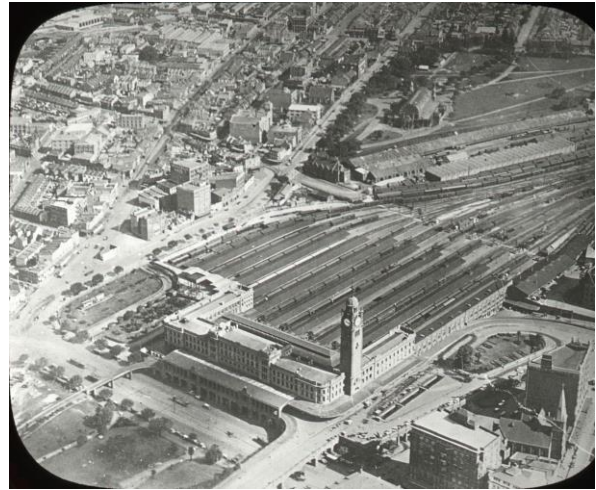
<sup>14</sup> State Library of NSW, 1930s. ID: 07193r.

<sup>15</sup> Sydney Trains Archives, ID: FB\_IMG\_1481925305635

<sup>16</sup> City of Sydney Archives, 1980s, ID: 066691



**Figure 10: Eastern Elevation of the Sydney Terminus, 1920s.**<sup>17</sup>



**Figure 11: Central Aerial view, date unknown.**<sup>18</sup>

### 3.1.2 Site Description and Condition

The Sydney Terminal Precinct is located in the north-west portion of the Central Precinct and is bounded by Pitt and Lee Streets to the west and the Central Electric precinct to the east. While Belmore Park forms an important element of the setting of the site, this element is not included within the curtilage of the Central Station Precinct. The Sydney Terminal Precinct encompasses the majority of the station's grand structures. This includes not only the Main Terminus buildings, but also the former Parcels Post Office and the tram approach ramps that flank the western and eastern edges of Belmore Park.

The dominating feature of the precinct is the Main Terminus Building, a large sandstone building that is elevated above street level. It features a grand northern façade, with a porte-cochere that is connected to the street level via two approach ramps to the east and west. The ramps are constructed from sandstone and run in a north-south axis to either side of Belmore Park. They were originally designed to separate pedestrian and vehicular traffic, with the ramps allowing trams to directly access the Main Terminus building without disturbing the flow of pedestrian traffic on Eddy Avenue below. While being functional, these ramps are also dominant features of the precinct and contribute to the overall character of the Central Station site.

To the north-west of the building is a sandstone clocktower, one of the most recognisable structures of the Central Station site. The terminus building is surrounded by open space, with Belmore Park to the North and the Western Forecourt and central garden to the west. This contributes to the visual prominence of the Main Terminus Building and the landmark quality of the overall Central Station site. To the southern edge of the precinct is the former Parcels Post Office. The building has been adaptively reused as a hotel, now operating as Adina Apartment Hotel. The building is located adjacent to the western border of the Main Terminus Building and to the south of the Western Forecourt.

A Neo-Classical architectural style is used consistently throughout the Sydney Terminal Precinct. It is featured not only in the facades of the Main Terminus building and its associated elements, including the main concourse and clock tower, but also in the facades of the former Parcels Post Office. As the

<sup>17</sup> AHRS, Sydney Trains Archives, 1920s. ID: 003082.

<sup>18</sup> Facebook, date unknown.



sandstone facades of the Main Terminus are visible throughout the Central Station Precinct, these elements form a key component of the significance of the entire place.



**Figure 12: View of Sydney Terminal Precinct from Railway Square, George Street.<sup>19</sup>**



**Figure 13: View of Sydney Terminal Precinct from Railway Colonnade Drive, Ambulance Ave side.<sup>20</sup>**



**Figure 14: View of Sydney Terminal Precinct from Belmore Park.<sup>21</sup>**



**Figure 15: View down Eddy Avenue with Sydney Terminal Precinct on the left.<sup>22</sup>**



<sup>19</sup> Artefact, 2021.

<sup>20</sup> Artefact, 2021.

<sup>21</sup> Artefact, 2021.

<sup>22</sup> Artefact, 2021.

**Figure 16: View of Sydney Terminal Precinct from Adina Hotel.<sup>23</sup>**

**Figure 17: View of Western Forecourt of Sydney Terminal Precinct from Railway Colonnade Drive, Ambulance Ave side.<sup>24</sup>**

### 3.1.3 Aboriginal Archaeological Potential

This precinct includes Aboriginal site AHIMS ID 45-6-3654. Further discussion of Aboriginal archaeological potential is included in the Archaeological Site Plan (ASP).

### 3.1.4 Historical Archaeological Potential

Historical archaeological excavations have been undertaken as part of various works undertaken within Central Station for the last few decades, such as for services upgrades or the more recent Metro Line. These excavations have uncovered both the remains of individuals and structures associated with Devonshire Street Cemetery and infrastructure associated with previous railway stations on site. However, despite this, the precinct still retains historical archaeological potential associated with all former uses of the site, including the Devonshire Street Cemetery. While low, there is the possibility of further human remains within grave cuts and redeposited sands within the former extent of the Devonshire Street Cemetery being uncovered. If so, this would be of state significance. There is also the potential for further rail infrastructure such as turntables associated with previous train stations on the site. A full assessment of archaeological potential within the precinct is included in the ASP.

### 3.1.5 Statement of Significance

Central Station was the first major metropolitan rail terminus to be constructed in Australia and the first in NSW. Three successive termini were constructed within the overall Central Station Precinct, with each successive station designed to provide additional facilities and services to the expanding Sydney population. The overall Central Station Precinct is a rare example of an innovative and grand urban plan, of which the Sydney Terminal is a significant and integral component.

The Sydney Terminal Precinct is of exceptional historic significance as a continuously operating transportation terminus for over 100 years, and for its association with the development of the Central Station Precinct, and for the early government and institutional uses previously located on the site. The station is also located on Sydney's second major burial ground, the Devonshire Street Cemetery, which was closed in the 1870s and was redeveloped into the Sydney Terminal in 1901 after the exhumation of burial sites. The association with the Devonshire Street Cemetery is furthermore significant for the archaeological remains of burial vaults which were located within the Precinct, uncovered as part of excavation works for the Sydney Metro.

The archaeological remains of the Benevolent Asylum and Carters Barracks which were constructed on the site during Governor Macquarie's would be rare and important surviving examples from the early Colonial period of Sydney.

The Sydney Terminal has landmark qualities and is of exceptional aesthetic significance for its high-quality architectural design, workmanship, character and materiality.

As a landmark and continuously operating transport hub, the Sydney Terminal has an exceptional social significance for the local people of Sydney, interstate and international travellers who use the building, but also the railway staff over the years. It also is significant for its association with

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<sup>23</sup> Artefact, 2021.

<sup>24</sup> Artefact, 2021.

architects, engineers and designers, namely Gorrie McLeish, Henry Deane and Walter Liberty Vernon.

Overall, the Central Station Precinct has been assessed as having state heritage significance.

### 3.1.6 Site Specific Conservation Policies

**Table 5: Site Specific Conservation Policies for the Overall Sydney Terminal Precinct**

Policy	Policy Discussion and Guidelines
1. Follow Overall Conservation Policies	See Section 15 of this CMP for Conservation Policies.
2. Implement Fabric Guidelines	See Section 15.4.4 of this CMP
3. Item Specific Policy: Heritage Process	<p>When considering change follow the Heritage Flow Charts that form Appendix C of this CMP:</p> <p>A) Major Works Heritage Process Flow Chart; or B) Minor Works Heritage Process Flow Chart.</p>
4. Item Specific Policy: Heritage Listing	<p>See Section 3 in this CMP for Legislative Context, and Section 14.4 for Statutory Context.</p> <p>Ensure appropriate statutory approvals or exemptions are obtained prior to change on the site because:</p> <ul style="list-style-type: none"> <li>The site is within the overall listing for <b>Central Station</b> on the State Heritage Register under the <i>NSW Heritage Act 1977</i> for Sydney Terminal and Central Railway Stations Group.</li> <li>The site is within the overall listing of the <b>Central Railway Station and Sydney Terminal Group</b> on Transport Asset Holding Entity's (TAHE) Heritage and Conservation Register under Section 170 of the <i>NSW Heritage Act 1977</i>.</li> <li>The area is within the Railway Square/Central Station Special Character Area under the Sydney DCP 2012.</li> </ul>
5. Precinct Specific Policy: Archaeology	<p>See Sections 10 and 15.6 in this CMP and Appendix B. Where possible, retain archaeological evidence in situ.</p> <p>Seek advice from a suitably qualified historical archaeologist prior to undertaking any excavation work below track level. Archaeological monitoring of work may be required. Approval for such work may be required pursuant to s57(1) of the NSW Heritage Act, 1997. Where possible, retain archaeological evidence in situ.</p> <p>If excavation in the Central Station Precinct is occurring and any archaeological evidence of the nineteenth Century buildings is found, notify the NSW Heritage, Department of Planning &amp; Environment and seek advice from a suitably qualified historical archaeologist prior to work recommencing. Approval pursuant to s57(1) of the NSW Heritage Act will be required if remains need to be removed</p>
6. Precinct Specific Policy: Development	<p>Careful and sympathetic development within this precinct is possible, as long as any development does not detract from the general character, significant views or significant fabric within the Sydney Terminal Precinct.</p> <p>Development should allow a buffer which does not encroach on the significant elements of the Precinct.</p>

Policy	Policy Discussion and Guidelines
7. Precinct Specific Policy: Development	Prior to the commencement of any proposed development or works, photographic archival records of the Sydney Terminal Precinct should be undertaken.
8. Item Specific Policy: Interpretation	<p>See Sections 9 and 14.8 'Heritage Interpretation' in this CMP.</p> <p>Prepare a heritage interpretation strategy, based on the precinct-wide Central Precinct Heritage Interpretation Strategy, specific to the Sydney Terminal sub-precinct as part of the larger Central Precinct.</p> <p>Following this, develop and implement a detailed site-specific heritage interpretation plan, aligned with the themes and recommendations of the heritage interpretation strategy.</p>
9. Precinct Specific Policy: Interpretation in Western Forecourt	Make use of the public open space in the Western Forecourt to interpret the institutional and early railway uses of the site.
10. Precinct Specific Policy: Lighting	Lighting at Central Station is to be guided by the <i>Central Station Lighting Strategy</i> .

### 3.1.7 Precinct Opportunities and Constraints

The following section outlines the opportunities and constraints within Precinct 3: Sydney Terminal. The various zones within the precinct which have been designated for opportunities and constraints is outlined in Figure 18 below. The respective opportunities and constraints have been further detailed in Table 6.



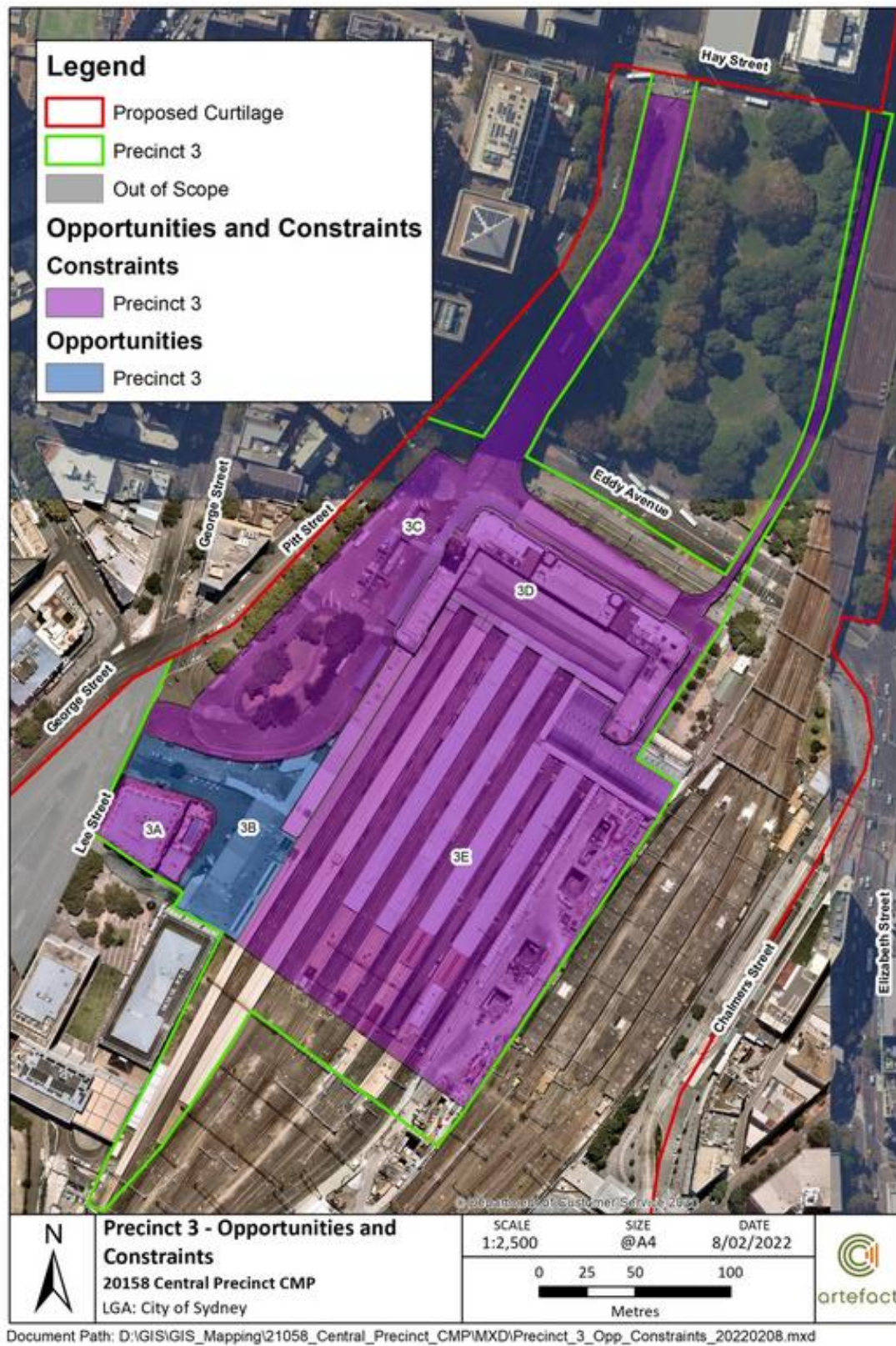


Figure 18: Precinct 3: Sydney Terminal – Opportunities and Constraints.

Table 6: Precinct 3: Sydney Terminal – Opportunities and Constraints

Precinct/Zone	Opportunities	Constraints
Precinct 3 Zone 3.A  <i>Former Parcels Post Office</i>	<ul style="list-style-type: none"> <li>Opportunity to adaptively reuse the former Post Office Building for a sympathetic new use should it be vacated by its current owners</li> <li>Opportunity to reinstate original layout and details which have been removed internally</li> <li>Opportunity to remove the single storey addition to the east (fronting Henry Deane Plaza) to regain the original building footprint and understanding of the building in the round</li> </ul>	<ul style="list-style-type: none"> <li>Maintain and conserve the former Post Office Building and its external presentation to Henry Deane Plaza, Railway Square, Lee, George and Pitt Streets</li> <li>Vertical additions are not permitted above the former Post Office Building</li> <li>New development is not permitted in this zone</li> </ul>
Precinct 3 Zone 3.B  <i>Parcels Area YHA Railway, Square (Former Inwards Parcel Shed)</i>	<ul style="list-style-type: none"> <li>Opportunity for sympathetic new development in this zone</li> <li>Opportunity to create and enhance the urban setting of the Station and provide new public spaces</li> </ul>	<ul style="list-style-type: none"> <li>New development should take into consideration the visual dominance of a tower to the Central Clocktower</li> <li>New development should not impact the brick retaining wall with sandstone saddle back coping to the north of the zone</li> <li>New development should retain significant view corridors and vistas to the Sydney Terminal buildings as outlined Section 7.6 of the CMP</li> </ul>
Precinct 3 Zone 3.C  <i>Porte-cochere, Tram Ramps, Western Forecourt, East Deck, Luggage Dock</i>	<ul style="list-style-type: none"> <li>Opportunity to enhance public access and urban setting of the Station</li> <li>Opportunity to interpret historic external lighting</li> <li>Opportunities to enhance public access and the urban setting of the station, including heritage interpretation should be explored</li> </ul>	<ul style="list-style-type: none"> <li>All buildings within this zone should be maintained and conserved, particularly their external presentation, their form and scale</li> <li>Vertical additions are not permitted above buildings and spaces within this zone</li> <li>The Western Forecourt should remain an open public space</li> <li>Significant view corridors and vistas within this zone as outlined in Section 7.6 of the CMP should be conserved</li> </ul>
Precinct 3 Zone 3.D  <i>Main Terminus Building (including interiors), Eddy Avenue Colonnade Arcades and Shops, Pitt Street Arcade, Clocktower, West Wing Extension, Basement</i>	<ul style="list-style-type: none"> <li>Opportunities to adaptively reuse spaces within West Wing Extension and Main Terminus Building for new functions where suitable rail or transport functions are not available</li> <li>Opportunity to reinstate former Booking Hall volume and architectural features</li> <li>Opportunity to interpret historic lighting</li> <li>Internal fit-outs in these spaces are permissible and should be consistent with an internal fit-out guideline prepared for the whole Precinct</li> <li>Opportunities to enhance public access and the urban setting of the station, including heritage interpretation should be explored</li> </ul>	<ul style="list-style-type: none"> <li>All buildings within this zone should be maintained and conserved, particularly their external presentation, their form and scale</li> <li>Vertical additions are not permitted above buildings and spaces within this zone</li> <li>Alterations and additions to internal spaces of the West Wing Extension and Main Terminus Building should be conducted within areas identified as of little heritage significance</li> <li>Significant view corridors and vistas within this zone as outlined in Section 7.6 of the CMP should be conserved. Including key internal views between the Grand Concourse and Country and Interstate platforms.</li> </ul>

Precinct/Zone	Opportunities	Constraints
Precinct 3 Zone 3.E  <i>Country and Interstate Platforms, Subway Passage Systems, Devonshire Tunnel</i>	<ul style="list-style-type: none"> <li>• Opportunity to open more or enhance the existing subway network for use as public pedestrian access across the Station</li> <li>• Opportunity to reinstate missing elements such as the skylights at the end of the platforms onto the Devonshire Street Tunnel</li> <li>• Opportunities for the development of new public spaces and urban settings which include heritage interpretation should be explored</li> </ul>	<ul style="list-style-type: none"> <li>• The Country and Interstate Platforms should be conserved and continue to be understood as open air spaces with awning shelters. Development above the platform area is not appropriate</li> <li>• New development should not impact significant archaeological elements and other subterranean elements in this precinct, including the Bondi Sewer (beneath the Country and Intercity platform area) Significant view corridors and vistas within this zone as outlined in Section 7.6 of the CMP should be conserved. Including key internal views between the Grand Concourse and Country and Interstate platforms.</li> </ul>



## 3.1 MAIN TERMINUS BUILDING: FACADES AND ROOF

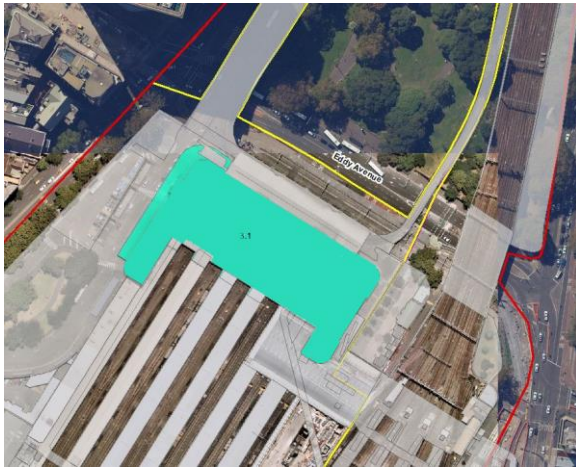


Figure 19. Main Terminus Building.<sup>25</sup>



Figure 20. Central Station Main Terminus Building northern façade.<sup>26</sup>

### 3.1.1 History of the Main Terminus Building: Facades and Roof

Relocating the railway station from its terminus south of Devonshire Street to a location closer to the city was first proposed by the Parliamentary Standing Committee on Public works in 1895. It was around this time, the term 'Central' Station began to be used, and a hierarchy of building materials was implemented, with sandstone to be used for the more prestigious buildings on site.

With the Government Architect's Branch, Public Works would designing the new terminus, Initial planning suggested that the main concourse (known as the 'assembly platform') and the platforms would be located above a basement level (the ground level). Gorrie McLeish Blair, employed by the Government Architect's Branch at the time submitted a design for the terminal in 1901. McLeish Blair's design was chosen, but was modified in 1902 to provide a larger building. This modified design increased the building height by one storey, with offices above the main concourse level, a clocktower and an east wing. The 1902 design was further modified over the course of planning and construction of the building.

On 30 April 1902, the foundation stone for the terminus was laid. Construction on the first stage of the third Sydney Terminus, now known as the Central Station Building, began in June 1902. The following month, the first order of sandstone was placed with Robert Saunders of Pymont Quarries, and eleven stonemasons began work on the building, with the first sandstone being placed on the piers of the tramway approach (now departure) bridge.

In 1904, the management of the design and construction of the Central Railway Station Buildings was transferred from the Railway Construction Branch in the Public Works, under Engineer-in-Chief for Railways Henry Deane, to the Government Architects Branch under the Government Architect Walter Liberty Vernon. William Ferrier acted as Clerk of Works on site providing stone and marble expertise during the construction of the buildings.

The Main Terminus building was constructed in stages, and as a result the design of the building was simplified and modified from the original concept over time. The first stage of construction was completed in August 1906 and involved the completion of the terminus building up to and including the main concourse level and establishment of the Eddy Avenue colonnade. The colonnade

<sup>25</sup> Artefact, 2021

<sup>26</sup> Artefact, 2021

encompassed the Eddy Avenue Arcade and shops on the northern façade of the building and the tram porte-cochere.

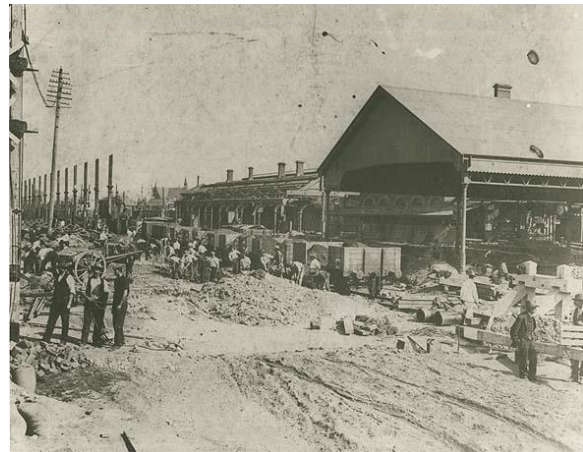
The second stage of construction did not commence until 1916, ten years after stage one was completed. While the foundation stone for the clocktower was laid in September 1903, the construction on the tower did not commence until 1916 and was not completed until 1920. As per the 1902 design, office levels were constructed above the upper concourse level during the second stage. The 1902 design for the building also included cupolas which were constructed, however the two flanking parapets which were constructed on the northern façade's central bay appear to indicate that further work stages were anticipated, as the parapets were constructed to allow for the later addition of two cupolas which were never constructed.

The facades and roofs of the Main Terminus Building remained relatively unchanged until conservation works were undertaken to the building in the mid-1990s. These included the water proofing of all the horizontal surfaces adjacent to the facades on the Eddy Avenue and the Western Forecourt side of the Main Terminus. Paving was also removed and replaced, with waterproofing applied to assist with building maintenance. The reconstruction of some original elements also took place, including the trachyte kerbs. The original honed Pyrmont stonework on the northern facade behind the Eddy Avenue Arcade was conserved, with significantly damaged stones replaced with sourced Pyrmont stone to match the original. More extensive modifications also took place, such as the demolition and rebuilding of the east deck in 1995. These works were undertaken to incorporate a new brick colonnade below the deck, which was located adjacent to the Eddy Avenue forecourt entrance to the Central Electric station.

Some additional works were also undertaken at the beginning of the 21st century. The parapets to the upper office levels were conserved in 2000, as were the balusters and the coffered cornice. Between 2009 and 2011, the Pitt Street and Eddy Avenue (including the colonnade) facades also underwent conservation processes.



**Figure 21: New Central Station being constructed in 1903**



**Figure 21: Construction work taking place Central Station Devonshire Street end 1905.<sup>27</sup>**

<sup>27</sup> State Archives, 1905. ID: 17420\_a014\_a014000265.



Figure 22: Sydney Terminal 1948.<sup>28</sup>

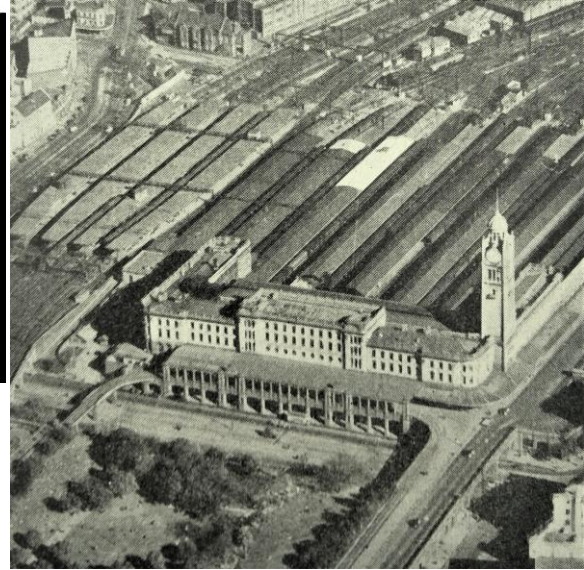


Figure 23: Sydney Yard aerial 1955.<sup>29</sup>



Figure 24: View from over Belmore Park of the Main Terminus Building, date unknown.<sup>30</sup>



Figure 25: Postcard of Central Main terminus over Pitt St and Eddy Ave intersection, pre-1914.<sup>31</sup>

### 3.1.2 Site Description and Condition

In order to emphasise the importance of specific buildings across the precinct, a hierarchy of building materials was adopted during the precincts design. As such, the Main Terminus Building was predominantly constructed from sandstone, while the Parcels Post Office was mainly constructed with brick, with sandstone detailing. Consistent with this approach and despite both structures being constructed at the same time, the West Wing extension of the Main terminus was also constructed with brick with sandstone detailing. Only a small portion of the Main Terminus Building was constructed using brick. The first level of the eastern façade faced the working-class terraces of Surry Hills, and was not a prominent or public facing façade like the northern and western facades, and thereby the use of brick rather than sandstone reiterated the hierarchy of the building materials and the building's public perception.

<sup>28</sup> Mark Foys, Australian Railway Historic Society, 1948. Sydney Terminal.

<sup>29</sup> Department of Railways NSW Annual Report, 1954-55 cover page.

<sup>30</sup> Sydney Trains Archives, date unknown.

<sup>31</sup> Peter Gough, Sydney Trains, pre 1914.



## Facades

The Main Terminus Building is designed in the Neo-Classical style, and features the following Neo-Classical elements:

- A symmetrical primary façade (Eddy Avenue) with projecting central bay
- A continuous parapet containing balusters
- One-metre-high coffered cornice with dentils and fielded panels in the modillions underneath
- Rusticated sandstone to lower levels contrasting with fine ashlar sandstone of the upper levels
- Large strongly rusticated arched openings
- Arches that are comprised of voussoir panels and are surmounted by key stones
- Symmetrically grouped circular, square headed and semi-circular headed openings
- Pilasters containing cartouches
- Triglyph panels below string courses
- Timber framed sash windows
- Door and window openings (on concourse level) arched with fanlights and fielded panels beneath sills.

The Main Terminus Building has a 'U' shaped plan which is enclosed by the classically proportioned facades of the building. This plan wraps around the Grand Concourse, which is situated adjacent to and north of platforms 1-15. The Grand Concourse is located on the first level of the Main Terminus Building, which also contains a basement level and two levels of offices above. A third level of offices is contained in the central projecting bay of the northern portion of the building, meaning this area of the building spans across five levels.

The Main Terminus Building was constructed in two stages, with contrasts visible between the materials used in each stage. While rusticated Pyrmont sandstone was used in the construction of the first stage, which comprised works up to and including the first level above the basement, a finer Maroubra ashlar dressed sandstone was utilised in the second stage. This stage included the upper-level office facades. Despite the use of different sandstones, each stage of construction reflects the desired Neo-Classical character.

Except for a section of brickwork on the eastern façade, sandstone is the primary material used on the northern, western, and eastern facades of the Main Terminus Building. This section of brickwork on the eastern façade is located near the east deck and contains sandstone banding and sandstone detailed archways. The area of the northern façade above the Eddy Avenue Arcade contains oeil-de-boeuf clerestory windows, which are located towards the top of the building and were originally a feature of the double-heightened booking office. As the southern façade is a less public element of the building, the area above the Grand concourse level was constructed in brickwork which has been rendered and painted. However, the fenestrations on the southern façade also feature sandstone sills and the wall is surmounted by a sandstone parapet. The parapet is taller on the higher central bay portion of the façade and is consistent with the parapets to the north, east and west.

The northern façade which fronts Eddy Avenue features rusticated sandstone that has been used in the construction of the Eddy Avenue Arcade arches and up to the concourse level. This sandstone sits upon a honed Pyrmont sandstone plinth which is visible on the street level walls (basement) on Eddy Avenue. The upper levels of the northern façade were constructed in a lighter and highly decorative ashlar sandstone. The sandstone is generally in good condition and undergoes periodic maintenance and cleaning as part of the building's cyclical maintenance and conservation. Most of

the original downpipes remain in situ on the facades and are comprised of gun metal heads with cast iron downpipes.



Figure 26. Central Station Main Terminus Building north façade.<sup>32</sup>



Figure 27. Central Station Main Terminus as viewed from the intersection of Eddy Avenue and Pitt Street.<sup>33</sup>



Figure 28. Sandstone detailing to the ashlar north façade.<sup>34</sup>



Figure 29. Sandstone detailing to the ashlar façade, NSW and GR insignia badges, north façade.<sup>35</sup>

<sup>32</sup> Artefact, 2021

<sup>33</sup> Artefact, 2021

<sup>34</sup> Artefact, 2021

<sup>35</sup> Artefact, 2021



**Figure 30. Ashlar and rusticated sandstone, north-east facades.<sup>36</sup>**



**Figure 31. Clocktower and sandstone on the west façade, including entrance canopy.<sup>37</sup>**



**Figure 32. Rusticated sandstone, west façade at Grand Concourse level.<sup>38</sup>**



**Figure 33. Brick east façade features sandstone banding and arches, with ashlar sandstone at upper levels.<sup>39</sup>**

### Passive Ventilation System

Conduits that were a component of an early passive ventilation system remain in-situ on the facades. These connect to interior vents, which are contained on the interior walls of the rooms in the building. Exterior vents are also visible on the perimeter sandstone parapets of the west, north and east facades. These are located at regular intervals and contain metal covers.

### Roof

The roof of the Main Terminus Building is located behind the parapet walls which feature on each of the facades. The roof structure is constructed from corrugated metal sheeting and wraps around the Grand Concourse in three main portions. The first portion is located above the west wing, which has a gabled roof, with a ridge that runs the length of the west wing and then turns eastwards to connect with the west wall of the central projecting bay. The second portion of the roof skirts around the clocktower which extrudes out of Main Terminus Building and has a gabled roof with a ridge which runs the length of the east wing and turns westward to run to the east wall of the central projecting bay. There is also a hipped roof which is located above the third level offices in the projecting bay which completes the third roofing portion.

<sup>36</sup> Artefact, 2021

<sup>37</sup> Artefact, 2021

<sup>38</sup> Artefact, 2021

<sup>39</sup> Artefact, 2021



Above the Grand Concourse is a wide-span barrel-vaulted roof which is supported by metal-arch trusses and has metal roof sheeting and a central skylight.

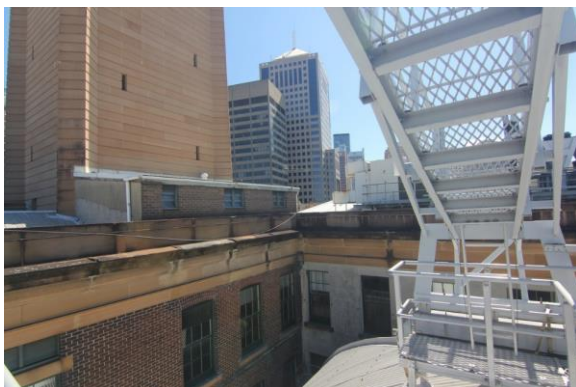
Introduced services, including air-conditioning, are contained within the roof of the building. Metal platforms have been erected behind parapet level, which some of these services are mounted on. A metal maintenance access walkway has also been constructed behind the parapet wall to the south of the clocktower and to the northern portion of the west wing. To the south of the clocktower there is a metal ladder above the west wing roof. This connects the west wing access system to the roof structure of the Grand Concourse, which has its own separate system.



**Figure 34. Grand Concourse steel roof structure.**<sup>40</sup>



**Figure 35. Original cast iron gutters to the Grand Concourse roof lantern.**<sup>41</sup>



**Figure 36. Brick and sandstone internal façade walls of the Main Terminus Building (north-east corner) and Grand Concourse roof.**<sup>42</sup>



**Figure 37. East roof and sandstone balustraded parapet wall. Note the replaced sandstone balusters, carved to match the originals.**<sup>43</sup>

### 3.1.3 Statement of Significance

The Main Terminus Building is a landmark building in Sydney. The building is aesthetically and technically significant for its complex design, innovative construction techniques and technology – such as passive ventilation design. It is also the only railway terminus building in Australia to have a high-level entrance, rather than at street level.

The building is historically significant for its association with two different departments within the New South Wales Department of Works, and for the architects, engineers and designers who helped

<sup>40</sup> Artefact, 2021

<sup>41</sup> Artefact, 2021

<sup>42</sup> Artefact, 2021

<sup>43</sup> Artefact, 2021

design and develop the building through its two-stage construction, namely Gorrie McLeish (Architect associated with designing the original building design), Henry Dean (Engineer-in-Chief, Public Works) and Walter Liberty Vernon (Government Architect, Public Works).

The building is aesthetically significant for its Neo-Classical architectural style, and predominant use of sandstone and fine decorative detailing, which provides a distinctive and landmark quality to the building as an important public building in Sydney. This landmark quality has historically been part of key vistas in downtown Sydney, vistas which continue to provide significant view corridors to the site. The facades also demonstrate a hierarchy of building materials, a practice which had evolved on important public buildings by the early 20th Century.

The Main Terminus Building has been assessed as having state heritage significance.

### 3.1.4 Gradings of Significance and Integrity

**Table 7: Gradings of Significance and Integrity for the Main Terminus Building: Facades and Roof**

Element	Grading	Integrity
<b>Main Terminus Facades and Roof Overall</b>	Exceptional	High
<b>Views and Vistas Facades &amp; Roof</b>	High	High
<b>Context and Setting</b>	Exceptional	Moderate
<b>Northern Sandstone Façade - Eddy Avenue</b>	Exceptional	High
<b>Eastern Façade</b>	Exceptional	High
<b>Western Façade - Pitt Street</b>	Exceptional	High
<b>Southern Façade</b>	Exceptional	High
<b>Parapets</b>	Exceptional	High
<b>Natural Ventilation</b>	Exceptional	Unknown
<b>Access and Walkways</b>	Intrusive	N/A
<b>Gutters and Drainage</b>	High	Low-Moderate
<b>Introduced Services; Mechanical, Electrical, Lighting and Data</b>	Intrusive	N/A

### 3.1.5 Site Specific Conservation Policies

**Table 8: Site Specific Conservation Policies for the for the Main Terminus Building: Facades and Roof**

Policy	Policy Discussion and Guidelines
1. Follow Overall Conservation Policies	See Section 15 of this CMP for Conservation Policies.
2. Implement Fabric Guidelines	See Section 15.4.4 of this CMP.

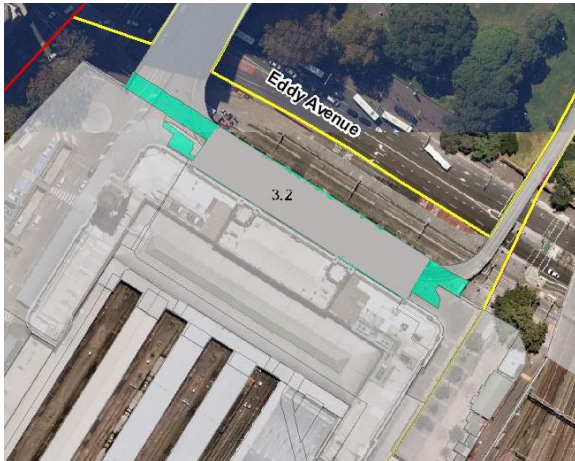
Policy	Policy Discussion and Guidelines
3. Item Specific Policy: Heritage Process	<p>When considering change follow the Heritage Flow Charts that form Appendix C of this CMP:</p> <ul style="list-style-type: none"> <li>A) Major Works Heritage Process Flow Chart; or</li> <li>B) Minor Works Heritage Process Flow Chart.</li> </ul>
4. Item Specific Policy: Heritage Listing	<p>See Section 3 in this CMP for Legislative Context, and Section 14.4 for Statutory Context.</p> <p>Ensure appropriate statutory approvals or exemptions are obtained prior to change on the site because:</p> <ul style="list-style-type: none"> <li>• The site is within the overall listing for <b>Central Station</b> on the State Heritage Register under the <i>NSW Heritage Act 1977</i> for Sydney Terminal and Central Railway Stations Group.</li> <li>• The site is within the overall listing of the <b>Central Railway Station and Sydney Terminal Group</b> on Transport Asset Holding Entity's (TAHE) Heritage and Conservation Register under Section 170 of the <i>NSW Heritage Act 1977</i>.</li> <li>• The area is within the Railway Square/Central Station Special Character Area under the Sydney DCP 2012.</li> </ul>
5. Item Specific Policy: Masonry Conservation	<p>Conservation of the sandstone and brick facades should be undertaken as part of the cyclical maintenance of the building. Particular care and inspection should be undertaken for any projecting or decorative elements (such as parapets, dentils and mouldings) which has a higher risk of public injury. These should be regularly inspected to make sure the risk of falling is minimised and mitigated.</p> <p>A stone and brick strategy for the facades should be prepared to guide the conservation of the masonry.</p> <p>A strategy for the protection of the masonry facades during any major works should be undertaken by heritage architectural specialists and qualified stonemasons with heritage experience prior to the commencement of any works.</p>



Policy	Policy Discussion and Guidelines
6. Item Specific Policy: Fabric	<p>Specific elements for conservation include:</p> <ul style="list-style-type: none"> <li>• A symmetrical primary façade (Eddy Avenue) with projecting central bay</li> <li>• A continuous parapet containing balusters</li> <li>• One-metre-high coffered cornice with dentils and fielded panels in the modillions underneath</li> <li>• Rusticated sandstone to lower levels contrasting with fine ashlar sandstone of the upper levels</li> <li>• Large strongly rusticated arched openings</li> <li>• Arches that are comprised of voussoir panels and are surmounted by key stones</li> <li>• Symmetrically grouped circular, square headed and semi-circular headed openings</li> <li>• Pilasters containing cartouches</li> <li>• Triglyph panels below string courses</li> <li>• Timber framed sash windows</li> <li>• Door and window openings (on concourse level) arched with fanlights and fielded panels beneath sills.</li> <li>• Extant historic colour schemes and finishes</li> </ul>
7. Item Specific Policy: Development	<p>Prior to the commencement of any proposed development or works, photographic archival records of the Sydney Terminal Precinct should be undertaken.</p> <p>Ensure the original external colour schemes and finishes are documented and archivally recorded.</p>
8. Item Specific Policy: Alterations and Additions	<p>Alterations and additions to the Main Terminus Building facades and roof should be avoided. Do not paint the sandstone or masonry façades.</p> <p>Changes to external colours and finishes of window/door joinery, steelwork or roofing material should be sympathetic to the heritage building and consider where possible the implementation of original or early schemes. If changes to the external colours occur, this should be implemented consistently across all façades and roof areas of the Main Terminus Building.</p>
9. Item Specific Policy: Vertical additions	<p>Vertical additions are not permitted above the Main Terminus Building.</p>
10. Item Specific Policy: Interpretation	<p>See Sections 9 and 14.8 'Heritage Interpretation' in this CMP.</p> <p>Prepare a heritage interpretation strategy, based on the precinct-wide Central Precinct Heritage Interpretation Strategy, specific to the Main Terminus Building as part of Central Precinct. Particular focus should be given to the former landscape and cultivation of the space.</p> <p>Following this, develop and implement a detailed site-specific heritage interpretation plan, aligned with the themes and recommendations of the heritage interpretation strategy.</p>

Policy	Policy Discussion and Guidelines
11. Item Specific Policy: Vistas	<p>Ensure vistas to the Main Terminus Building and the clocktower outlined in Section 8.6 of this CMP are not obscured. These include vistas from:</p> <ul style="list-style-type: none"><li>• Belmore Park</li><li>• Eddy Avenue</li><li>• Eddy Avenue Forecourt</li><li>• Pitt Street</li><li>• Western Forecourt</li><li>• Elizabeth Street</li><li>• Grand Concourse</li><li>• Country and Interstate Platforms</li></ul>
12. Item Specific Policy: Intrusive elements	<p>Implement change to remove intrusive fabric and visual clutter along the facades.</p>
13. Item Specific Policy: Urban Design	<p>Ensure there is an integrated urban design language for the street frontage of Central Station including signage, lighting, seating, paving etc.</p>
14. Item Specific Policy: Lighting	<p>Lighting across the Main Terminus Building facades should be consistent and in accordance with the guidelines contained in the <i>Central Station Lighting Strategy</i>.</p>

## 3.2 EDDY AVENUE COLONNADE, ARCADES AND SHOPS



**Figure 38. Eddy Avenue Colonnade, Arcades and Shops.**<sup>44</sup>



**Figure 39. Eddy Avenue Colonnade, Arcades and Shops.**<sup>45</sup>

### 3.2.1 History of the Eddy Avenue Colonnade, Arcades and Shops

The Eddy Avenue Colonnade, Arcade and Shops were constructed c. 1906 as part of the first phase of construction at the third Sydney Terminal. It was predicted that the new terminus would be a busy commercial centre that was attractive to privately run businesses, and consequently the initial design provided for 22 shops along the Eddy Avenue and Pitt Street frontages. However, by 1916 the nationalisation of railway catering services meant this intention changed, resulting NSW Railways having little consideration for private enterprises. As a result, the design was altered, and the proposed shops were modified to provide for various railway functions.

In August 1906, the Railway Printing Office was transferred from Redfern Station to the basement of the northern wing at Central. The printing office was responsible to the Controller of Accounts and Audit in the Finance Branch, and as part of their output, the staff covered “Special Train Notices, circular and handbills for passengers about timetable information”.<sup>46</sup> By 1917, the offices had relocated to three shops facing Eddy Avenue and were staffed with nine people who were engaged to undertake a wide range of printing tasks for the railway, and were one of the first departments to use electric printing machines. In the years to follow, the offices expanded into shops 2 and 9 of the Eddy Avenue arcade. Use of the offices continued until 1976 when they became increasingly underutilised and fell into disrepair.

The mezzanine level of each shop originally contained a female toilet and wash area. Each shop had a cast iron spiral staircase that allowed access to the mezzanine and while these remained in-situ, the toilets were removed in 1994 as part of modifications to the whole Eddy Avenue Arcade. These modifications also involved restoration of the shops, with new concrete floor slabs laid to replace the original deteriorated timber and concrete floors, as well as restoration of the early shop front joinery and layouts. Where possible, the majority of the original timber joinery was also retained. As part of the 1994 modifications, some of the original blind arches to the partition walls between the shops were opened to form double shops. Once the works were complete, the shops were leased out to food, clothing and other retailers, and continue to be used for a variety of tenancies today.

<sup>44</sup> Artefact, 2021

<sup>45</sup> Artefact, 2021

<sup>46</sup> Rappaport Pty Ltd & NSW Government Architects Office, 2013.

Another original feature of the precinct was the Railway Refreshment Rooms (RRR). These rooms were located within the Eddy Avenue Arcade, in an area to the east of the shops. Prior to the establishment of the RRR in 1916, this area of the Arcade originally contained the kitchen and staff dining room. The purpose of the RRR was to manage the bars, staff rooms, dining rooms and kiosks at Central. To facilitate operations, the area also contained a loading dock, mass production kitchen and storage area. However, the most extensive facilities were located in the basement, which included a scullery, boiler room and kitchen with two serveries. One kitchen contained a dumb waiter, which was used to transfer food from the basement to a server adjacent to the concourse level dining areas. The original kitchen and staff dining room were replaced by the RRR mass production kitchen. While this took up majority of the space, the remaining staff area was utilised as a loading dock for the RRR. Operations at the RRR began to wind down in 1976, with the spaces being converted into storage areas and staff rooms. The loading dock area, which opens onto the Eddy Avenue Arcade, is the only element of the RRR that is still operational today.

A major change to the precinct was the introduction of the Metro Light Rail (MLR). New paving and the new MLR rails were laid over the original tram tracks. The entry direction of the tram into the porte-cochère was also reversed, so that the entry was from the east tram ramp. In 1996, an original stair connection between the porte-cochère and Pitt Street was removed. This was located at the north-west corner of the area, where Eddy Avenue meets Pitt Street. The works required the removal of an original iron balustrade and stone sills from two original archways in the north-west corner. In their place, new stairs and escalators were installed.

Further conservation works were undertaken in 2009-2011, with the cleaning of the sandstone facades of both Eddy Avenue and Pitt Street, including the arcade and colonnade, being undertaken.



Figure 40: Eddy Avenue 1901<sup>47</sup>



Figure 41: Eddy Avenue arcades 1905.<sup>48</sup>

<sup>47</sup> Sydney Trains Archives, 1901.

<sup>48</sup> National Archives of Australia, 1905. ID: C4076, HN16147



Figure 42: Eddy Avenue facing towards Pitt Street, c. 1940s. <sup>49</sup>



Figure 43: Eddy Arcade, 1995s. <sup>50</sup>



Figure 44: Eddy Avenue arcade entrance, date unknown. <sup>51</sup>

### 3.2.2 Site Description and Condition

#### The Eddy Avenue Colonnade

The Eddy Avenue Colonnade spans over the lower two levels of the northern façade of the Main Terminus Building. While the ground level fronting Eddy Avenue protects the arcade and shops, the first-floor colonnade contains the tram porte-cochère. Although the colonnade is symmetrical, it is divided into three distinct sections. This is achieved by a set of decorative arches which are aligned with the eastern and western edges of the Main Terminus Building's central projecting bay. The

<sup>49</sup> Sydney Trains Archives, 1940s.

<sup>50</sup> Sydney Trains Archives, 1995. ID: 077705.

<sup>51</sup> Sydney Trains Archives, date unknown.



decorative arches feature engaged piers which are topped by a sandstone entablature and decorative sandstone pediment. The pediments feature carvings of the Australian Coat of Arms with kangaroo and emu on the western arch, while the eastern arch features the British Coat of Arms with a lion and unicorn. At the centre of the colonnade is a narrow archway, which frames and locates the timber panelled door that provides access to the basement rooms of the Main Terminus Building.

Although the ground level and first floor arches of the colonnade both feature rusticated sandstone, they each have a slightly different form. The arches of the Eddy Avenue colonnade have a voussoir arch form which features rounded voussoir panels and a central key stone. However, the arcade on the upper level is in a *piano nobile* arrangement, with squared columns in rusticated sandstone topped with an ashlar sandstone entablature with fielded panels. The columns at first floor are slenderer than the piers at the ground floor arches and are spaced at a different pattern to the ground floor piers.



**Figure 45. Eddy Avenue arcade and colonnade as viewed from Belmore Park and Eddy Avenue.<sup>52</sup>**

### The Eddy Avenue Arcade

The Eddy Avenue Arcade is bounded by the ground floor colonnade to the north and the northern façade of the Main Terminus Building to the south. It is characterised by a long walkway, which features a series of arched openings on either side. While the archways of the colonnade correspond with the arched storefront openings on the Main Terminus façade, reinforcing the rhythm of the arcade at its western and central sections, the eastern section of the arcade's southern wall does not contain arched openings. Instead, it has a series of windows and doors that are associated with the

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<sup>52</sup> Artefact, 2021

former Railway Refreshment Rooms (RRR) Store. Together, the central and western portions of façade contain nine retail shopfronts. Although the sandstone of the colonnade has a rusticated sandstone finish, the northern façade of the Main Terminus Building has a honed finish.

The arcade features a barrel-vaulted ceiling, which is supported on iron beams. While majority of the ceiling has a plastered finished, the north-west corner is finished with a contemporary corrugated sheet metal. This is due to modification works in the 1996, which resulted in the addition of an escalator and stair connection to the arcade to the porte-cochere above. As of 2021, the escalator and stairs have been upgraded with more contemporary structures. Despite the modifications to the ceiling, original wrought ironwork remains in-situ in the north-west corner of the Main Terminus Building. This corner is the meeting point between the Eddy Avenue and Pitt Street arcades and as such, the wrought-iron detailing is consistent across the first archway of each façade.



Figure 46. Eddy Avenue Colonnade looking west.<sup>53</sup>



Figure 47. Eddy Avenue Colonnade looking east.<sup>54</sup>



Figure 48. Eddy Avenue Colonnade looking east.<sup>55</sup>



Figure 49. Intersection of Eddy Avenue and Pitt Street Arcades, original Central Railway Station Signage.<sup>56</sup>

<sup>53</sup> Artefact, 2021

<sup>54</sup> Artefact, 2021

<sup>55</sup> Artefact, 2021

<sup>56</sup> Artefact, 2021



**Figure 50. Eddy Avenue Colonnade at intersection with western tram ramp.<sup>57</sup>**



**Figure 51. Installation of escalators to the Porte Cochere from the north-west corner of the Eddy Avenue Arcade.<sup>58</sup>**

### The Eddy Avenue Arcade Shops

Located at the eastern end of the arcade is the RRR store. This area features a large opening with a contemporary metal roller door, as well as several double-height windows. Above the roller door opening there is a keystone that is inscribed with 'RRR'. Today, this entrance is used to access the loading dock.

Each of the shopfronts to the west of the RRR store are contained within an archway on the northern façade of the Main Terminus Building. Although the configuration of the shopfronts varies, each features a recessed double-leafed timber framed door, a series of double height glazing windows and a fanlight. The fanlight contains timber framed, coloured glazing panels that create a symbol of a square within a circle. This motif is used throughout the Main Terminus Building and is visible in the fenestrations of the Main Concourse. In 1994, the timber joinery to the shop fronts was repaired and as a result the majority of the original timber remains in-situ. As of 2021, the arcade contains a convenience store, a clothing store and a COVID-19 testing clinic. However, the other stores are either for lease or vacant.

The interior of the shops is generally consistent, with the majority of the shops retaining their double-height space, with some additional mezzanine levels. Each shop originally contained a cast iron spiral stair constructed by the 'St Pancras Iron Work Company', which suggest that the stairs were designed and constructed in the UK around the turn of the 20th century. These cast iron spiral stairs were located to the rear of the shop interiors and were originally used to access female staff toilet facilities on a small rear mezzanine level. While some of the staircases have been removed, the original mezzanine levels remain in-situ. Of the remaining spiral staircases, some have been painted with intrusive colours to match the interior fit-out. However, the branding of 'St Pancras Iron Work Company' remains visible.

The interior of the shops also features a barrel-vaulted ceiling, which runs parallel to Eddy Avenue and is a continuation of the arcade's barrel-vaulted ceiling. The ceiling consists of three plastered barrel vaults which are supported by load bearing, inter-tenancy walls. Contemporary ceilings have been added to some of the tenancies, concealing the original ceilings, while in other tenancies, the original vaulted ceiling has been modified through the insertion of intrusive building services. The original shop flooring which was constructed of timber board over ash has also been removed as part of the 1994 modification works, at which time the current concrete floor slabs were laid.

<sup>57</sup> Artefact, 2021

<sup>58</sup> Artefact, 2021



Other items related to the Eddy Avenue colonnade, arcade and shops include the facades of the Main Terminus Building, which is discussed in Inventory Sheet 3.1 and the porte-cochere of the Main Terminal Building, which is discussed in Inventory Sheet 3.3.



Figure 52. RRR Store.<sup>59</sup>



Figure 53. inside an arcade store, original leadlight glazing to shop window fanlight.<sup>60</sup>



Figure 54. Inside store, intertenancy walls, some may have been infilled or opened to change spatial arrangements of the shops.<sup>61</sup>



Figure 55. An original cast iron spiral staircase located at the rear of the tenancy.<sup>62</sup>



Figure 56. St Pancras Iron Work Company stamp on spiral stair tread.<sup>63</sup>



Figure 57. Original vaulted ceiling retained, intrusive mezzanine additions in some tenancies.<sup>64</sup>

<sup>59</sup> Artefact, 2021

<sup>60</sup> Artefact, 2021

<sup>61</sup> Artefact, 2021

<sup>62</sup> Artefact, 2021

<sup>63</sup> Artefact, 2021

<sup>64</sup> Artefact, 2021



**Figure 58.** One of the original female bathroom spaces located at the mezzanine and accessed from the spiral stairs. Finish and fittings are not original.<sup>65</sup>



**Figure 59.** Inside store, intertenancy walls, some may have been infilled or opened to change spatial arrangements of the shops.<sup>66</sup>

### 3.2.3 Statement of Significance

The Eddy Avenue Colonnade, Arcade and Shops are of state significance as an original feature and design of the Main Terminus Building and its presentation to Eddy Avenue, and its interaction with the streetscape and users. It contributes to the landmark vistas of the Main Terminus Building as viewed from Belmore Park and Eddy Avenue.

The area is significant for its aesthetic contribution, exemplifying the Neo-Classical Architectural style which has been used throughout much of the Sydney Terminal Precinct, and the predominant use of sandstone which is typical of important public buildings in the early 20th century. The Eddy Avenue Colonnade and Arcades are technologically innovative through their construction which includes colonnades and steel supported barrel vaults. The shops and RRR areas contribute to the significant and constant use of the spaces for retail/commercial uses and/or railway functions.

The precinct retains a high level of integrity in its retention of original configuration, fabric and decorative features.

### 3.2.4 Gradings of Significance and Integrity

**Table 9: Gradings of Significance and Integrity for the Eddy Avenue Colonnade, Arcades and Shops**

Element	Grading	Integrity
<b>Eddy Avenue Colonnade, Arcade and Shops Overall</b>	Exceptional	High (exterior) Moderate (interior)
<b>Views and Vistas</b>	High	High
<b>Context and Setting</b>	Exceptional	High
<b>Sandstone Façade Wall, Doors, Windows and Hardware</b>	Exceptional	High
<b>Sandstone Colonnade</b>	Exceptional	High
<b>RRR Store Entrance and Signage</b>	Exceptional	High

<sup>65</sup> Artefact, 2021

<sup>66</sup> Artefact, 2021



Element	Grading	Integrity
Paving, Trachyte Kerbs, Street Furniture and Bus Signage	Moderate	Moderate
Drainage and Downpipes	High	Unknown
Timber and Glass Shop Fronts, Fanlights, Ventilation	Exceptional	High
Plaster Barrel Vaulted Ceilings and Lighting	Exceptional	High
Northwest Corner Escalators and Stairs	Intrusive	N/A
Northwest Corner Barrel Vaulted Ceiling	High	Moderate Plaster replaced with corrugated iron
Original double height spatial quality of the shops	High	Moderate Some spaces have intrusive mezzanine levels which divides the original double height spaces
Shop Interiors, Brick Arched Party Walls, Vaulted Ceilings and Spiral Stairs	High	Moderate - High Most spiral stairs have been removed; most brick arched party walls remain
Applied Finishes	Neutral	N/A
Introduced Services; Mechanical, Electrical, Lighting and Data	Intrusive	N/A

### 3.2.5 Site Specific Conservation Policies

**Table 10: Site Specific Conservation Policies for Eddy Avenue Colonnade, Arcades and Shops**

Policy	Policy Discussion and Guidelines
1. Follow Overall Conservation Policies	See Section 15 of this CMP for Conservation Policies
2. Implement Fabric Guidelines	See Section 15.4.4 of this CMP
3. Item Specific Policy: Heritage Process	When considering change follow the Heritage Flow Charts that form Appendix C of this CMP: A) Major Works Heritage Process Flow Chart; or B) Minor Works Heritage Process Flow Chart.

Policy	Policy Discussion and Guidelines
4. Item Specific Policy: Heritage Listing	<p>See Section 3 in this CMP for Legislative Context, and Section 14.4 for Statutory Context.</p> <p>Ensure appropriate statutory approvals or exemptions are obtained prior to change on the site because:</p> <ul style="list-style-type: none"> <li>• The site is within the overall listing for <b>Central Station</b> on the State Heritage Register under the <i>NSW Heritage Act 1977</i> for Sydney Terminal and Central Railway Stations Group.</li> <li>• The site is within the overall listing of the <b>Central Railway Station and Sydney Terminal Group</b> on Transport Asset Holding Entity's (TAHE) Heritage and Conservation Register under Section 170 of the <i>NSW Heritage Act 1977</i>.</li> <li>• The area is within the Railway Square/Central Station Special Character Area under the Sydney DCP 2012.</li> </ul>
5. Item Specific Policy: Use	Encourage use of the shops for retail and commercial purposes.
6. Item Specific Policy: Stone Conservation	<p>Conservation of the sandstone facades should be undertaken as part of the cyclical maintenance of the building. Particular care and inspection should be undertaken for any projecting or decorative elements (such as pediments, entablatures, dentils and mouldings) which has a higher risk of public injury. These should be regularly inspected to make sure the risk of falling is minimised and mitigated.</p> <p>A stone strategy for the facades should be prepared to guide the conservation of the masonry.</p> <p>A strategy for the protection of the masonry facades during any major works should be undertaken by heritage architectural specialists and qualified stonemasons with heritage experience prior to the commencement of any works.</p>
7. Item Specific Policy: Development	<p>Prior to the commencement of any proposed development or works, photographic archival records of the precinct should be undertaken.</p> <p>Ensure the original external colour schemes and finishes are documented and archivally recorded.</p>
8. Item Specific Policy: Alterations and Additions	<p>Alterations and additions to the Eddy Avenue Colonnade and Arcades should be avoided. Do not paint sandstone facades.</p> <p>Changes to the interior fit-out of the shops is permitted but should not impact original features such as double heightened space, barrel vaulted ceilings and internal inter-tenancy walls.</p> <p>An interior fit-out guide for the shop tenancies should be prepared to help guide future tenancy fit-outs and to provide a level of consistency across the stores in their treatment and protection of the heritage fabric and spaces.</p> <p>Changes to external colours and finishes of window/door joinery should be sympathetic to the heritage fabric and consider where possible the implementation of original or early schemes. If changes to the external colour occur, this should be implemented consistently across all facades of the Main Terminus Building.</p> <p>Installation of street furniture associated with associated transport networks (ie. Light Rail) can be installed along the colonnade. Furniture should be sympathetic to the historic fabric and setting, but distinctly contemporary.</p>

Policy	Policy Discussion and Guidelines
9. Item Specific Policy: Significant configurations & elements	<p>Conserve the significant original configuration and elements of the Eddy Avenue Arcade and Shops including:</p> <ul style="list-style-type: none"> <li>• Arches</li> <li>• Barrel vaults</li> <li>• Pavement widths</li> <li>• Wrought iron 'Railway Station Central' sign at the western arch, intersection with Pitt Street</li> <li>• Left and right-handed shop fronts</li> <li>• Double height spaces</li> <li>• Rendered internal walls to the shops</li> <li>• Spiral stairs</li> </ul>
10. Item Specific Policy: Management of Signage	<p>Ensure all tenants of the shop spaces (including any Responsible government Agency use of the spaces) follow a Commercial Signage Manual for the overall Central Station site.</p> <p>Ensure all other signage is also managed under an overarching Signage Manual for the site.</p>
11. Item Specific Policy: Lighting	<p>Lighting to the colonnade should be consistent with the guidelines contained in the <i>Central Station Lighting Strategy</i>.</p>
12. Item Specific Policy: Interpretation	<p>See Sections 9 and 14.8 'Heritage Interpretation' in this CMP.</p> <p>Prepare a heritage interpretation strategy, based on the precinct-wide Central Precinct Heritage Interpretation Strategy, specific to the Eddy Avenue Colonnade, Arcade and Shops as part of Central Precinct. Particular focus should be given to the former landscape and cultivation of the space.</p> <p>Following this, develop and implement a detailed site-specific heritage interpretation plan, aligned with the themes and recommendations of the heritage interpretation strategy.</p>
13. Item Specific Policy: Vistas	<p>Ensure vistas to the Eddy Avenue Colonnade, Arcades and Shops are not obscured. Significant vistas are from Belmore Park, Eddy Avenue and at the corner of Pitt Street and Eddy Avenue.</p>
14. Item Specific Policy: Excavation & Archaeology	<p>Seek advice from a suitably qualified archaeologist prior to undertaking any excavation work in the western half of the Eddy Avenue Colonnade. Approval may be required pursuant to s57(1) of the NSW Heritage Act, 1977.</p>
15. Item Specific Policy: Conservation of Significant Archaeology	<p>See Sections 10 and 15.6 in this CMP and Appendix B. Where possible, retain archaeological evidence in situ.</p>

### 3.3 PORTE-COCHERE

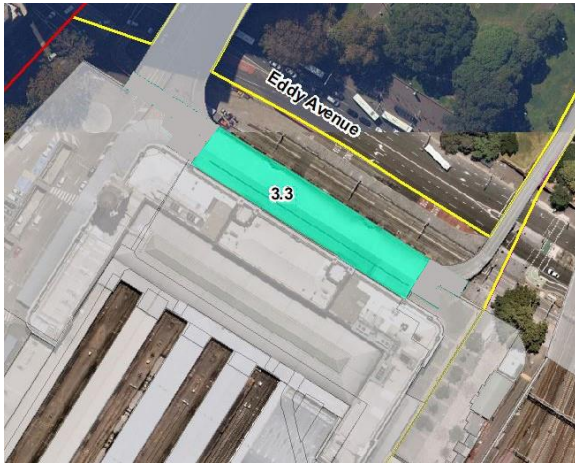


Figure 60. Porte-Cochere.<sup>67</sup>



Figure 61. Porte-Cochere.<sup>68</sup>

#### 3.3.1 History of the Porte-Cochere

The porte-cochere was constructed at the same time as the Eddy Avenue Arcade in 1906 and was designed to provide shelter to passengers who were transferring from the trams to the train platforms, via the Grand Concourse area. The porte-cochère was constructed using a three-pin truss roof system, which was based on the design used at the Galerie des Machines in Paris. The design made the truss fixings a feature and at that time it was a unique construction technique to be used in NSW and Australia

The original floor of the porte-cochere was asphalt, with wood blocks used around the tram lines. As the trams ceased operation in the mid-late 1950s in NSW, after that date the floor level of the porte-cochere was raised as various paving was added around the site. The tram lines were eventually paved over by 1990s, however the tracks remain in-situ below the paving.

The porte-cochere area was used as a carpark until the Metro Light Rail (MLR) was completed in 1996. It follows the path of the original tram line, but operates in the opposite direction, entering the porte-cochère from the eastern end and departing from the west. Conservation works were also undertaken during the construction of the MLR. Such works included the repaving and waterproofing of the paving, which resulted in the discovery of the original in-situ tram tracks. These tracks were not removed during the construction of the MLR, but they were repaved over as a result of the works. While the original tracks are now concealed, the new MLR line was laid in their same approximate position providing insight into the intended design and use of the porte-cochère.

Conservation works were also undertaken in 1996. These included 'the seismic bracing of the colonnade through the roof of the porte-cochère; repair of the string courses at the base and the cornice of the colonnade; and the repair and replacement of stone balusters in the stone balustrade'.<sup>69</sup>

<sup>67</sup> Artefact, 2021

<sup>68</sup> Artefact, 2021

<sup>69</sup> Rappaport Pty Ltd & NSW Government Architects Office, 2013. p. 2.1-4



Figure 62: Tram Platform and Porte-Cochere, 1951.<sup>70</sup>



Figure 63: Porte-Cochere in Postcard from 1908.<sup>71</sup>



Figure 64: Porte-Cochere, 1909.<sup>72</sup>



Figure 65: Colonnade Tram Stop with Porte-Cochere Roof top right, 1924.<sup>73</sup>

### 3.3.2 Site Description and Condition

The porte-cochere is located above the Eddy Avenue Arcade. It functions as the meeting point between the east and west tram ramps, with the light rail entering to the east and departing to the south, with the MLR tracks located in approximately the same position as the original tram tracks. The northern façade of the Main Terminus building, which forms the southern perimeter of the porte-cochere features several segmented arches of varying size, with the larger arches providing access to the Main Terminus Building. The most eastern of these larger arches provides pedestrian access to the Grand Concourse, whilst the other arches have either been enclosed with glazing, or feature modified original timber panelled doors with fanlight windows. The smaller arches comprise windows that are in two fixed panels and a highlight window in timber joinery. The windows at the centre of the façade (in the location of the former booking hall and dining room, now the Eternity Bar and Grill and Central Food Hall) features leadlight glass which comprises the NSW emblem – a waratah and the initials 'NSWGR' (for New South Wales Government Railways) in the highlight. The other windows on this façade comprise clear plate glass, with etched scroll patterns at the base of the glazing that reflects the pattern of the fauna in the leadlight glass windows.

<sup>70</sup> Sydney Trains Archives, 1951. ID: 044543

<sup>71</sup> Sydney Trains Archives, 1908.

<sup>72</sup> Building Magazine, April 1909, 35.

<sup>73</sup> Sydney Trains Archives, 1924.



The northern perimeter of the porte-cochere is a low sandstone balustrade wall, original to the c.1906 structure, which is installed between the rusticated sandstone columns. Some of these balusters have been replaced over the years due to deterioration or damage. The northern perimeter sits directly above the Eddy Avenue colonnade and forms the *piano nobile* aspect of this façade as viewed from Belmore Park.

A main feature of the porte-cochere is the broad, three pin truss roof which spans the length of the porte-cochere. Although the roof was constructed from corrugated iron and featured transparent panels adjacent to the northern façade of the Main Terminus, this original fabric has since been replaced with a corrugated sheet metal that has a similar profile to the original. To enhance the stability of the Eddy Avenue Colonnade, seismic bracing was added to the truss roof in the 1990s, which extends down to the pavement. The braces for the truss work are situated between the rusticated sandstone arched fenestration of the Main Terminus Building's northern façade. The roof also contains a painted valence of vertical timber to its east and west ends. They are painted in a green which matches the colour of the truss system and the timber window and door joinery on the Main Terminus Building.

Directly to the west of the porte-cochere is an additional awning. This is supported by cast-iron columns and skirts the north-west corner of the Terminus before turning south and spanning the length of the Main Terminus' western façade. Towards the eastern end of this awning there is a piece of original signage that is associated with the staircase from the porte-cochere to the Eddy Avenue arcade below. It reads, "to Pitt & George Streets".

Additional features include an early clock, which is suspended from the trusses towards the western end of the structure, and early down-pipes which are mounted to the northern and southern walls of the porte-cochere. The downpipes feature gun metal heads with cast iron pipes and are located at regular intervals along both walls. Contemporary additions include down lights, architectural services, and signage, all which are suspended from the trusses. There are also some car parking spaces to the northern boundary of the porte-cochere which are reserved for emergency vehicles.



**Figure 66. Porte-Cochere, eastern end showing timber valance and infilled glazing.<sup>74</sup>**



**Figure 67. Inside the porte-cochere.<sup>75</sup>**

<sup>74</sup> Artefact, 2021

<sup>75</sup> Artefact, 2021



Figure 68. Low sandstone balustrade wall and columns on the north edge of the portecochere form part of the original 1906 north façade of the Main Terminus Building.<sup>76</sup>



Figure 69. Early clock suspended from the metal truss work.<sup>77</sup>



Figure 70. Three-pin truss roof of portecochere.<sup>78</sup>



Figure 71. Iron support brackets to truss roof.<sup>79</sup>

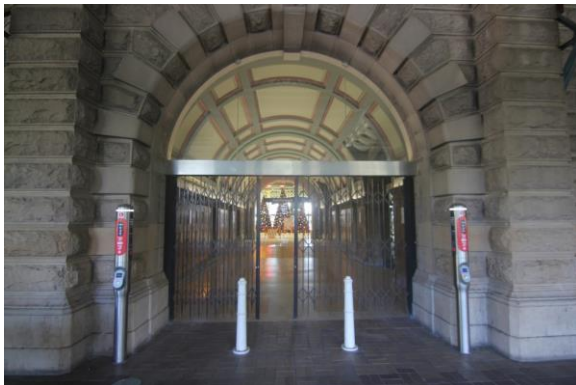


Figure 72. Entrance to the Grand Concourse from the portecochere.<sup>80</sup>



Figure 73. Commemorative bronze tablet which reads 'This Tablet – Commemorative of Opening the Station for Public Traffic – was affixed on Aug. 4th 1906 – Hon. JH Carruthers, Premier – Hon. CA Lee, Min. for Pub. Works'.<sup>81</sup>

<sup>76</sup> Artefact, 2021

<sup>77</sup> Artefact, 2021

<sup>78</sup> Artefact, 2021

<sup>79</sup> Artefact, 2021

<sup>80</sup> Artefact, 2021

<sup>81</sup> Artefact, 2021



**Figure 74. Decorative leadlight glazing to windows associated with the former booking hall and dining room.<sup>82</sup>**



**Figure 75. Other plate glass windows have a decorative scroll pattern etched into the bottom.<sup>83</sup>**

### 3.3.3 Statement of Significance

The porte-cochere is an element of state heritage significance, as part of the original c.1906 design and construction of the Main Terminus Building. The porte-cochere has historical, aesthetic, and technical significance which contributes to the overall significance of the precinct.

Aesthetically, the porte-cochere features fine architectural detailing in its sandstone columns and balusters, the three-pin truss roof, rusticated sandstone walls, arches and decorative leadlight glass of the Main Terminus Building, and early cast iron downpipes and gun metal heads, all which can be viewed prominently from within the porte-cochere and contribute to the aesthetic composition and understanding of the space.

The porte-cochere's three-pin truss roof replicates a design from the Galerie des Machines in Paris, and is evidence of its technical innovation, using an engineering and construction technique which was not used in NSW or Australia at the time.

The area also has archaeological significance for the extant original tram tracks which are beneath the current floor surface and MTR tracks.

The views and vistas to the porte-cochere from Belmore Park and Eddy Avenue and understanding of its *piano nobile* composition above the Eddy Avenue Colonnade contribute to the significance of this element.

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<sup>82</sup> Artefact, 2021

<sup>83</sup> Artefact, 2021

### 3.3.4 Gradings of Significance and Integrity

**Table 11: Gradings of Significance and Integrity for the Porte-Cochere**

Element	Grading	Integrity
<b>Porte-Cochere Overall</b>	Exceptional	High
<b>Views and Vistas</b>	Exceptional	High
<b>Context and Setting</b>	Exceptional	High
<b>Roof, Trusswork, Valance and Seismic Bracing</b>	Exceptional	High
<b>Main Terminus Building northern façade, windows and doors</b>	Exceptional	Moderate-High Original sandstone, timber framed leadlight glass windows, non-original glazing to some windows, modified original timber doors
<b>Passage Connections to Main concourse</b>	Exceptional	Moderate-High Non-original glazing and door operators
<b>Platform and Tracks</b>	Little	Low
<b>Remains of early tram tracks</b>	High	High Condition and extent unknown
<b>Clock, Early Signage and Wayfinding</b>	High	High
<b>Sandstone Colonnade, Balustrades, Parapet and Coats of Arms</b>	Exceptional	High
<b>Current Paving</b>	Little	N/A
<b>Escalator and Stair</b>	Intrusive	N/A
<b>Introduced Services; Mechanical, Electrical, Lighting and Data</b>	Intrusive	N/A

### 3.3.5 Site Specific Conservation Policies

**Table 12: Site Specific Conservation Policies for the Porte-Cochere**

Policy	Policy Discussion and Guidelines
1. Follow Overall Conservation Policies	See Section 15 of this CMP for Conservation Policies.
2. Implement Fabric Guidelines	See Section 15.4.4 of this CMP
3. Item Specific Policy: Heritage Process	When considering change follow the Heritage Flow Charts that form Appendix C of this CMP: A) Major Works Heritage Process Flow Chart; or B) Minor Works Heritage Process Flow Chart.



Policy	Policy Discussion and Guidelines
4. Item Specific Policy: Heritage Listing	<p>See Section 3 in this CMP for Legislative Context, and Section 14.4 for Statutory Context.</p> <p>Ensure appropriate statutory approvals or exemptions are obtained prior to change on the site because:</p> <ul style="list-style-type: none"> <li>• The site is within the overall listing for <b>Central Station</b> on the State Heritage Register under the <i>NSW Heritage Act 1977</i> for Sydney Terminal and Central Railway Stations Group.</li> <li>• The site is within the overall listing of the <b>Central Railway Station and Sydney Terminal Group</b> on Transport Asset Holding Entity's (TAHE) Heritage and Conservation Register under Section 170 of the <i>NSW Heritage Act 1977</i>.</li> <li>• The area is within the Railway Square/Central Station Special Character Area under the Sydney DCP 2012.</li> </ul>
5. Item Specific Policy: Stone Conservation	<p>Conservation of the sandstone facades should be undertaken as part of the cyclical maintenance of the building. Particular care and inspection should be undertaken for any projecting or decorative elements (such as parapets, dentils and mouldings) which has a higher risk of public injury. These should be regularly inspected to make sure the risk of falling is minimised and mitigated.</p> <p>A stone strategy for the facades should be prepared to guide the conservation of the stonework.</p> <p>A strategy for the protection of the sandstone facades during any major works should be undertaken by heritage architectural specialists and qualified stonemasons with heritage experience prior to the commencement of any works.</p>
6. Item Specific Policy: Use	<p>Continue to use the Porte-Cochere as a public assembly area, and for the access of light-rail/trams.</p>
7. Item Specific Policy: Reconstruction of lost elements	<p>Consider the re-construction (or a sympathetic interpretation) of the following:</p> <ul style="list-style-type: none"> <li>• Light standards</li> <li>• Traditional asphalt paving</li> <li>• Original colour scheme to metalwork</li> <li>• Original signage</li> <li>• Glazing to the truss roof of the porte-cochere</li> <li>• Skylights</li> <li>• Glazed valances</li> </ul>
8. Item Specific Policy: Intrusive elements	<p>Implement change to remove intrusive fabric and visual clutter along the facades.</p> <p>Consider the removal of the following intrusive paint finish from the existing valances.</p>
9. Item Specific Policy: Development	<p>Prior to the commencement of any proposed development or works, photographic archival records of the porte-cochere should be undertaken.</p> <p>Ensure the original or early external colour schemes and finishes are documented and archivally recorded.</p>



Policy	Policy Discussion and Guidelines
10. Item Specific Policy: Alterations and Additions	<p>Alterations and additions to the porte-cochere should be avoided.</p> <p>Additions above the porte-cochere is not permissible. The porte-cochere should continue to be understood as a sculptural architectural element. Insertions through the truss roof are not permissible.</p> <p>Changes to external colours and finishes of window/door joinery, steelwork or roofing material should be sympathetic to the heritage building and consider where possible the implementation of original or early schemes. If changes to the external colours occur, this should be implemented consistently across all facades and roof areas of the Sydney Terminal precinct. Do not paint the sandstone facades.</p>
11. Item Specific Policy: Sequence of Spaces.	Enhance the formal progression from the ramps to the Porte-Cochere and into the Grand Concourse of the Terminus.
12. Item Specific Policy: Lighting	Lighting to the porte-cochere should be consistent with the guidelines contained in the <i>Central Station Lighting Strategy</i> .
13. Item Specific Policy: Interpretation	<p>See Sections 9 and 14.8 'Heritage Interpretation' in this CMP.</p> <p>Prepare a heritage interpretation strategy, based on the precinct-wide Central Precinct Heritage Interpretation Strategy, specific to the Porte-Cochere as part of Central Precinct. Particular focus should be given to the former landscape and cultivation of the space.</p> <p>Following this, develop and implement a detailed site-specific heritage interpretation plan, aligned with the themes and recommendations of the heritage interpretation strategy.</p>
14. Item Specific Policy: Vistas	Ensure vistas to the porte-cochere outlined in Section 8.6 of this CMP are not obscured. These include vistas from Belmore Park, Eddy Avenue, from the tram ramps and within the porte-cochere.
15. Item Specific Policy: Management of Signage	Ensure all signage is in accordance with a Signage Manual for the overall Central Station site.
16. Item Specific Policy: Excavation & Early Tramlines	<p>See Sections 10 and 15.6 in this CMP and Appendix B. Where possible, retain archaeological evidence in situ.</p> <p>Removal of the tram tracks may require approval or exemption pursuant to s57(1) of the NSW Heritage Act, 1977. Confirm if any historical documentation of the configuration and appearance of the tracks exist before undertaking the work. Archaeological recording will only be needed if such documentation does not exist.</p>

## 3.4 TRAM RAMPS



Figure 76. Tram ramps.<sup>84</sup>



Figure 77. Eastern tram ramp.<sup>85</sup>

### 3.4.1 History of the Tram Ramps

The Tram Ramps run to the east and the west of the Main Terminus Building, allowing the trams to arrive and depart from the porte-cochère. They were constructed c.1906, during the first phase of construction of the third Sydney Terminus. Both the porte-cochère and Main Terminus Building are raised above ground level and the ramps were designed to provide a grand entrance to these structures. As a hierarchy of materials was implemented to prioritise the importance of the structures within the Central Station Precinct, it was decided that the ramps would be constructed of the finest material; Pyrmont Sandstone.

During a lecture presented to the Sydney University Engineering Society in 1902, Henry Deane discussed the removal of the existing tramway which ran through Belmore Park and connected to Pitt Street. Deane revealed that new tram approaches were to be built as part of the design of the third Sydney Terminus, with ramps to rise up on the periphery of Belmore Park to the elevated concourse and platform areas of the Main Terminus Building.

The ramps were designed by engineers from the Railway Construction Branch of the Public Works. The design of the ramps was complex, incorporating not just the grand sandstone approaches, but also technically advanced underbridges. The intention of the design was to clearly separate the pedestrian, tram, and vehicular functions of the station. As such, both ramps were assigned a different function. The eastern ramp was to facilitate tram activity only and be used for departure from the porte-cochère. Whilst arrival to the porte-cochère was to be accessed by the western ramp, however, this ramp could also be used by pedestrians and motor vehicles. The purpose of this

<sup>84</sup> Artefact, 2021

<sup>85</sup> Artefact, 2021

separation was to create an ordered and formal progression between the street level and the upper level of the Main Terminus Building.

This ordered design approach became a concept that underpinned the design of the whole Terminus, where the safe and efficient flow of pedestrians and vehicular traffic was carefully considered. As a result, the Terminus became a multi-level transport interchange, with a separation between passenger and non-passenger traffic. It was proposed that passenger traffic, from both pedestrians and vehicles, would approach the terminus from the tram ramps, the western forecourt, the east deck and the passenger ramps. While the non-passenger traffic, such as mail, freight and unaccompanied luggage, were restricted to the basement and ground levels to ensure that it was separated from the pedestrian activities.

In July 1902, the first order of sandstone for the construction of the Terminus was placed. It was supplied by Robert Saunders from Pyrmont Quarries. By August 1902, eleven stonemasons had been appointed to begin work, with the first sandstone blocks being placed on the piers of the western tram ramp approach bridge.

It was intended that the Terminus would be the gateway to the city and that approach ramps would create a grand entry to the landmark site. This was achieved through placing the ramps on the perimeter of Belmore Park, as this delineated the landscape. Prior to the clocktower being constructed in as part of the second stage of construction c.1915- 1921, the ramps contributed to the Terminus being regarded as a prominent landmark. By the early to mid-20th century Pitt Street and Elizabeth Street were established as broad tree lined avenues running adjacent to the tram approaches and contributing to the beautification of the area.

Two underbridges were constructed as an original component of the Sydney tramway system. The eastern underbridge was constructed from steel and featured decorative ironwork balustrades and a riveted steel plate girder, while the western underbridge was constructed from sandstone and had five simulated arches. Trams turning to and from Elizabeth Street used the central spanning arch. When the Elizabeth Street viaduct was constructed in the 1920s, the eastern side of the eastern tram ramp was modified to accommodate the new viaduct.

Other modifications occurred in the mid-1990s due to the introduction of the Metro light Rail (MLR). Construction of the MLR required new tracks to be laid, as well as new paving to be installed. The direction of the tram traffic was also reversed, so that while the eastern ramp was originally used for tram departure, it became used as the MLR entry ramp. The MLR tracks were generally laid in the same location as the original tramp tracks. Further works in the late 1990s also resulted in the replacement of the sandstone balusters on the western ramp's underbridge.



**Figure 78: Tram line leaving Sydney Railway Station, 1935.** <sup>86</sup>



**Figure 79: Tram ramp, date unknown.** <sup>87</sup>



**Figure 80: Central Station and Tram ramp, 1906.** <sup>88</sup>



**Figure 81: Post card with overall Porte-Cochere, date unknown.** <sup>89</sup>



**Figure 82: Western approach ramp leading into the Main Terminus building. This ramp is now used for light rail departure.** <sup>90</sup>



**Figure 83: Central tram ramp, date unknown.** <sup>91</sup>

<sup>86</sup> State Library of Victoria, 1935. ID: H91.300\_983

<sup>87</sup> Sydney Trains Archives, unknown, ID: FB\_IMG\_1477287635231.jpg

<sup>88</sup> Sydney Trains Archives., 1906.

<sup>89</sup> Sydney Trains Archives, date unknown.

<sup>90</sup> Online from Living Histories @ UON, Central Railway Station, Sydney, Circa 1910

<sup>91</sup> Sydney Trains Archives, unknown. ID: FB\_IMG\_1528663532865





Figure 84: Tram Ramp, c. 1920s.<sup>92</sup>



Figure 85: Pitt Street side of the tram ramp, 2001.<sup>93</sup>

### 3.4.2 Site Description and Condition

The central terminus has two tram ramps, with the eastern used for approaching trams and the western used for those that are departing. Together with the Main Terminus they form a U shape around Belmore Park, with the porte-cochere being the central commuter pick up and drop off zone. As the ramps cross the busy Eddy Avenue directly to the north of the Main Terminus Building, they each have their own associated bridge structure. The eastern ramp slopes up the eastern side of Belmore Park, travelling from the corner of Elizabeth and Hay Streets, before crossing the eastern underbridge and veering west to enter the porte-cochere located above street level. When exiting the porte-cochere, the tram veers north to travel over the Eddy Avenue underbridge, before ramping down the western side of Belmore Park and reaching street level at the intersection between Hay and Pitt Streets.

#### The ramps and Belmore Park

While the ramps are highly visible from Belmore Park, views towards the western ramp from Pitt Street are more prominent than those towards the eastern ramp from Elizabeth Street. This is due to the Elizabeth Street Viaduct, which runs the length of Belmore Park and is located directly east of the eastern ramp, therefore obscuring views towards it. The Elizabeth Street viaduct was constructed in the 1920s as part of the development of Central Electric. Its construction resulted in some modifications occurring to the eastern wall of the eastern ramp, which due to their location behind the viaduct are predominantly concealed.

From within Belmore Park, views towards both ramps are slightly obscured by the high grass embankments that abut them. As such, the majority of the structure of the tram ramps, including the rusticated sandstone blocks they were constructed from, are concealed. Belmore Park is bounded by a wrought iron fence to the east and west. These fences are not original but do enclose the tram ramps, providing a safety barrier for both the trams and the park users.

Two other built elements within Belmore Park are associated with the tram ramps. The first is a 1906 sandstone pier, which is located at the Hay Street entrance of the western ramp. This pier signifies the beginning of the ramp and features the same detailing as those located at the entrance point to

<sup>92</sup> Sydney Trains, c1920s, ID: FB\_IMG\_1502315610504.

<sup>93</sup> Sydney Trains, 2001.



both the Western Forecourt and the Parcels Area. The second is a series of large sandstone pillars that are situated in the south-eastern corner of Belmore Park, where the ramp transitions into the eastern underbridge. These pillars are highly decorated and feature cornices, recessed panels and plinths.



**Figure 86. Eastern tram ramp approach from the north-eastern end of Belmore Park.<sup>94</sup>**



**Figure 87. Western tram ramp approach from the north-western end of Belmore Park.<sup>95</sup>**



**Figure 88. c.1906 sandstone pier signifying the start of the western tram ramp at Hay Street.<sup>96</sup>**



**Figure 89. Sandstone pillars on the eastern tram ramp signifying the change from Belmore Park to Eastern Underbridge.<sup>97</sup>**

### The Eastern Underbridge

The eastern ramp contains the tracks that allow the MLR to approach Central Station. Its underbridge features highly decorative ironwork balustrades, which are original, as well as a riveted steel plate girder. Large rusticated arched sandstone abutments support the bridge, making it a rare piece of Sydney's tramway infrastructure.<sup>98</sup>

<sup>94</sup> Artefact, 2021

<sup>95</sup> Artefact, 2021

<sup>96</sup> Artefact, 2021

<sup>97</sup> Google Street View, 2022

<sup>98</sup> <sup>3</sup>Railcorp to Section 170 Heritage Register, *Central Railway Station*, p



Figure 90. Eastern tram ramp as viewed from the Eastern Deck.<sup>99</sup>



Figure 91. Eastern tram ramp looking towards Belmore Park.<sup>100</sup>



Figure 92. Eastern tram ramp from Eddy Avenue looking south.<sup>101</sup>



Figure 93. Eastern tram ramp, as viewed from Eddy Avenue.<sup>102</sup>



Figure 94. Eastern tram ramp parallel to the Elizabeth Street viaduct.<sup>103</sup>



Figure 95. Underside of the Eastern tram ramp supported by sandstone abutments.<sup>104</sup>

<sup>99</sup> Artefact, 2021

<sup>100</sup> Artefact, 2021

<sup>101</sup> Artefact, 2021

<sup>102</sup> Artefact, 2021

<sup>103</sup> Artefact, 2021

<sup>104</sup> Artefact, 2021





**Figure 96. Reinforced concrete underside, supported by steel on sandstone abutments.<sup>105</sup>**



**Figure 97. Eastern tram ramp looking towards the Main Terminus Station from Eddy Avenue.<sup>106</sup>**

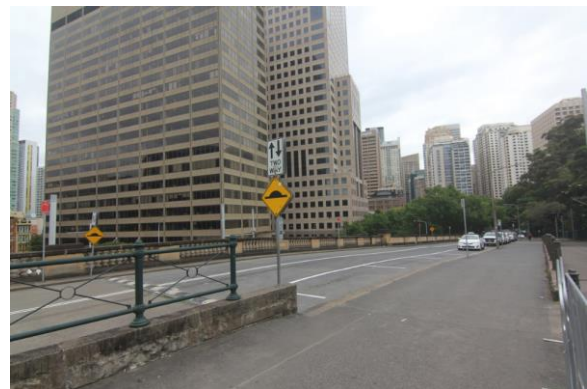
### The Western Underbridge

Three well defined modes of traffic are carried by the western ramp, containing tracks for the MLR tram, a pedestrian footpath, and a two-way road for motor vehicles. The western ramp is defined by five elliptical rusticated sandstone arches that span across Eddy Avenue. The most northern of these is used by pedestrians, while the middle two are used by cars and the southern is used by the light rail. Sandstone abutments support the concrete vaults of the bridge, which are visible on the underside of the structure from pedestrian level. The top of the bridge is decorated with a sandstone parapet wall that matches of the parapet of the Main Terminus Building. On each side of the bridge, the wall terminates at the beginning of Belmore Park. While the majority of the balusters are original, some were replaced in 1996 during conservation works.

To the north of the underbridge, the western ramp surmounts a series of nine shopfronts that face onto Pitt Street. While the sandstone arches surrounding the shopfronts are original, the shopfronts have been replaced with opaque windows. These spaces are used by the Responsible Government Agency Emergency Response unit. Although London Plane trees have been planted outside the Pitt Street Arcade, located to the south of the western ramp, no trees have been planted outside the shopfronts to the north. As such, the arched shop fronts are a dominant feature of that portion of Pitt Street.



**Figure 98. Western tram ramp at the exit from the porte-cochere.<sup>107</sup>**



**Figure 99. Western tram ramp looking north.<sup>108</sup>**

<sup>105</sup> Artefact, 2021

<sup>106</sup> Artefact, 2021

<sup>107</sup> Artefact, 2021

<sup>108</sup> Artefact, 2021



**Figure 100. Western underbridge as viewed from Eddy Avenue.<sup>109</sup>**



**Figure 101. Western underbridge from Eddy Avenue.<sup>110</sup>**



**Figure 102. Under the western underbridge.<sup>111</sup>**



**Figure 103. Western tram ramp shop fronts along Pitt Street.<sup>112</sup>**



**Figure 104. Western underbridge looking south towards the Eddy Avenue and Pitt Street Arcades.<sup>113</sup>**



**Figure 105. Western underbridge and western tram ramp shop fronts along Pitt Street.<sup>114</sup>**

<sup>109</sup> Artefact, 2021

<sup>110</sup> Artefact, 2021

<sup>111</sup> Artefact, 2021

<sup>112</sup> Artefact, 2021

<sup>113</sup> Artefact, 2021

<sup>114</sup> Artefact, 2021



**Figure 106. Western tram ramp shop fronts along Pitt Street.<sup>115</sup>**



**Figure 107. Western tram ramp sandstone wall extends and terminates at a sandstone pier at the corner of Hay and Pitt Streets, at which point the tramline continues at street level.<sup>116</sup>**

### 3.4.3 Statement of Significance

The tram ramps are of state significance. They are an exceptional and original architectural and engineering element of the Main Terminus Building and provides rare evidence of an innovative and grand urban plan for the Sydney Terminus.

The tram ramps are historically associated with Henry Deane (Engineer-in-Chief, Public Works) and Walter Liberty Vernon (Government Architect, Public Works). The collaboration of the innovative skills of two different branches of the Department of Public Works contributes to the significance of this precinct.

The tram ramps are aesthetically significant for their grand architectural sandstone form and landmark qualities, framing the approach to Central from Belmore Park and Eddy Avenue. The tram ramps define the site and exemplify the distinctive and predominant use of sandstone for important public sites by the early 20th century.

The multi-layered access ramps provide evidence of technical innovation in design, in particular the eastern ramp's steel underbridge and riveted steel plate girder, as well as the grand elliptical sandstone arches of the western approach and underbridge.

The complexity of the design and urban planning of the bridges and tram ramps is an innovative and rare design, which prioritised the safety of pedestrians and efficient movement of all modes of vehicular traffic.

### 3.4.4 Gradings of Significance and Integrity

**Table 13: Gradings of Significance and Integrity for the Tram Ramps**

Element	Grading	Integrity
<b>Tram Ramps Overall</b>	Exceptional	High
<b>Views and Vistas</b>	Exceptional	High
<b>Context and Setting</b>	Exceptional	High

<sup>115</sup> Artefact, 2021

<sup>116</sup> Artefact, 2021



Element	Grading	Integrity
Western (Pitt St) Approach including Sandstone Walling and Ground Level Undercroft Spaces	Exceptional	High
Bridge and Sandstone Balustrades (West Ramp)	Exceptional	High
Eastern Approach and Embankment	Exceptional	High
Bridge and Metal Balustrades (East Ramp)	Exceptional	High
Wrought Iron Fencing	Little	N/A
Paving and Trachyte Kerbs	Little/High	N/A Paving non-original
Current glazing to Pitt Street undercroft spaces.	Little	N/A Glazing non-original
Modern street light standards and fittings	Intrusive	N/A

### 3.4.5 Site Specific Conservation Policies

**Table 14: Site Specific Conservation Policies for the for the Tram Ramps**

Policy	Policy Discussion and Guidelines
1. Follow Overall Conservation Policies	See Section 15 of this CMP for Conservation Policies.
2. Implement Fabric Guidelines	See Section 15.4.4 of this CMP
3. Item Specific Policy: Heritage Process	When considering change follow the Heritage Flow Charts that form Appendix C of this CMP: A) Major Works Heritage Process Flow Chart; or Minor Works Heritage Process Flow Chart.
4. Item Specific Policy: Heritage Listing	See Section 3 in this CMP for Legislative Context, and Section 14.4 for Statutory Context.  Ensure appropriate statutory approvals or exemptions are obtained prior to change on the site because: <ul style="list-style-type: none"> <li>The site is within the overall listing for <b>Central Station</b> on the State Heritage Register under the <i>NSW Heritage Act 1977</i> for Sydney Terminal and Central Railway Stations Group.</li> <li>The site is within the overall listing of the <b>Central Railway Station and Sydney Terminal Group</b> on Transport Asset Holding Entity's (TAHE) Heritage and Conservation Register under Section 170 of the <i>NSW Heritage Act 1977</i>.</li> <li>The area is within the Railway Square/Central Station Special Character Area under the Sydney DCP 2012.</li> </ul>

Policy	Policy Discussion and Guidelines
5. Item Specific Policy: Stone Conservation	<p>Conservation of the sandstone facades and elements should be undertaken as part of the cyclical maintenance of the ramps. Particular care and inspection should be undertaken for any projecting or decorative elements (such as parapets, dentils, and mouldings) which have a higher risk of public injury. These should be regularly inspected to make sure the risk of failing is minimised and mitigated.</p> <p>A stone and brick strategy for the facades should be prepared to guide the conservation of the stonework.</p> <p>A strategy for the protection of the sandstone facades during any major works should be undertaken by heritage architectural specialists and qualified stonemasons with heritage experience prior to the commencement of any works.</p>
6. Item Specific Policy: Reconstruction of lost elements	<p>Consider the re-construction of the following:</p> <ul style="list-style-type: none"> <li>• replica light standards</li> <li>• traditional mix of asphalt paving</li> <li>• original colour scheme to the metalwork and original signage</li> </ul>
7. Item Specific Policy: Development	<p>Prior to the commencement of any proposed development or works, photographic archival records of the ramps should be undertaken.</p> <p>Ensure the original colour schemes and finishes are documented and archivally recorded.</p>
8. Item Specific Policy: Alterations and Additions	<p>Alterations and additions to the tram ramps should be avoided. New permanent structures should not be constructed on the tram ramps. Do not paint the sandstone facades.</p>
9. Item Specific Policy: Use	<p>The tram ramps should continue to be used as transportation pathways. Should tram services to Central cease in the future, the tram ramps should continue to provide pedestrian and vehicular access to the Main Terminus Building. The existing and original tram tracks should be retained as interpretation of the former use.</p>
10. Item Specific Policy: Interpretation	<p>See Sections 9 and 14.8 'Heritage Interpretation' in this CMP.</p> <p>Prepare a heritage interpretation strategy, based on the precinct-wide Central Precinct Heritage Interpretation Strategy, specific to the Tram Ramps as part of Central Precinct. Particular focus should be given to the former landscape and cultivation of the space.</p> <p>Following this, develop and implement a detailed site-specific heritage interpretation plan, aligned with the themes and recommendations of the heritage interpretation strategy.</p>
11. Item Specific Policy: Vistas	<p>Ensure vistas to the tram ramps outlined in Section 8.6 of this CMP are not obscured. These include vistas from:</p> <ul style="list-style-type: none"> <li>• Belmore Park</li> <li>• Eddy Avenue</li> <li>• Pitt Street</li> <li>• Hay Street</li> <li>• Railway Colonade Drive</li> <li>• Porte-cochere</li> </ul>

Policy	Policy Discussion and Guidelines
12. Item Specific Policy: Excavation & Early Tramlines	<p>See Sections 10 and 15.6 in this CMP and Appendix B. Where possible, retain archaeological evidence in situ.</p> <p>Seek advice from a suitably qualified archaeologist prior to undertaking any excavation work likely to impact on the original tramlines under the existing paving. Approval may be required pursuant to s57(1) of the NSW Heritage Act, 1977.</p>

## 3.5 WESTERN FORECOURT



Figure 108. Western Forecourt.<sup>117</sup>



Figure 109. Western Forecourt.<sup>118</sup>

### 3.5.1 History of the Western Forecourt

Prior to the construction of the third Sydney Terminus in 1906, the area of the Western Forecourt was occupied by institutional buildings. The part of the site was the location for institutional buildings as early as the 1820s, including The Benevolent Asylum (1820), the Government Cottage (c. 1820), the Christ Church Parsonage (1855), Police Barracks (1820), the Police Magistrate's residence (1820s) and a Steam Tram Depot (1879). Although these facilities were used throughout the Nineteenth Century, they were demolished in the early 20<sup>th</sup> century in order to build the new Terminus.

Located adjacent to Railway Square and due to its proximity to both Pitt and George Streets, the area was designed to take the majority of the incoming and outgoing pedestrian traffic. The flow of the pedestrian traffic into the main Terminus building from the Western Forecourt, combined with the adjacent entry point of the Western Tram Ramp into the porte-cochère resulted in the north-west corner of the Terminus building becoming a prominent junction. As a result of this prominent street frontage it was decided that the north-west corner should be decorated with fine architectural features, including acid etched glazing and an extensive awning. The awning was decorated with a Georgian wire glass insert and wrapped around the Terminus building, stretching from the porte-cochère to the Western Forecourt area. In 1921, the clocktower was constructed adjacent to the north-west corner, with its grand entrance opening onto the Western Forecourt. This further contributed to the architectural beauty of the north-west corner of the Terminus.

The Western Forecourt was incorporated on the western side of the Terminus to accommodate a waiting area for horse-drawn cabs. The area featured a carriageway which circulated around a central garden which comprised a dwarf sandstone wall and sandstone columns. The garden provided amenity for waiting passengers in the area. Early drawings and photographs indicated that the central garden lawn featured plantings which formed the numbers '1906', commemorating the year the third Sydney Terminus was completed and opened to the public. The '1906' remained until the mid-1950s, after which time the lettering was later changed to read 'NSWGR'.

The forecourt was located on a slight incline, navigating the fall in land from the Main Terminus Building and Parramatta Road and Railway Square. Its raised location made it prominent when viewed from the streetscape. The original features of the Western Forecourt utilised fine features including cast iron elbow lights. The lighting had cast iron bases which were fixed to sandstone piers,

<sup>117</sup> Artefact, 2021

<sup>118</sup> Artefact, 2021

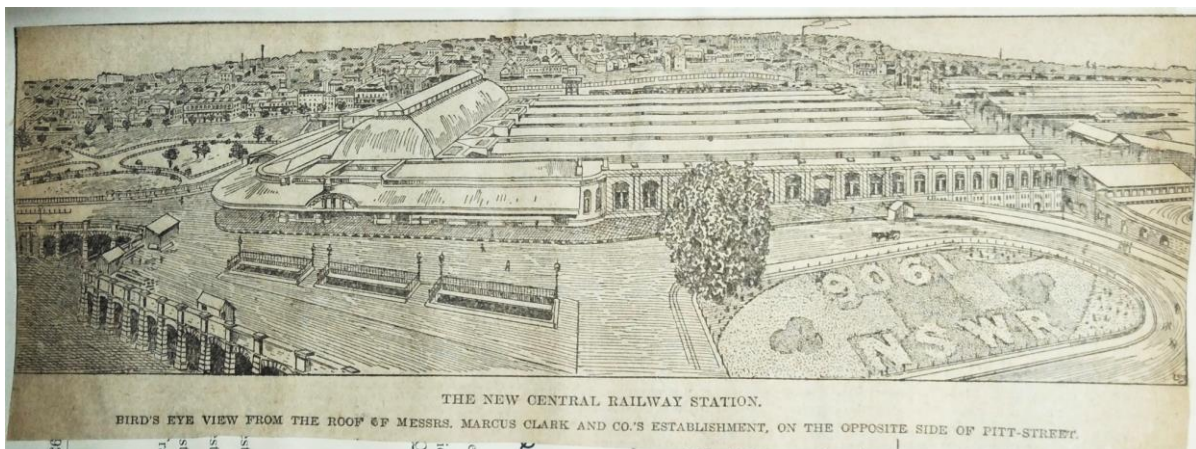


with access panels located within the light standard shaft. Many of the cast iron lights were removed or damaged over the years, with only one sandstone pier retaining an original cast iron base (opposite the former Parcel Post Office) in 2011. In the succeeding decade, restoration to the sandstone and light fixtures have occurred in the Western Forecourt, with reconstruction cast iron elbow lights, which feature the original 'GR' embossing detail to the base.

As part of the design of the Main Terminus Building's West Wing Extension, a grand archway was incorporated at the centre of the western and eastern facades, to be used by the Governor of NSW to access the forecourt from Platform 1. The archway featured a gas light fitting above the entrance, which is no longer extant.

Various modifications occurred to the Western Forecourt during the mid-late 20th century. The awning to the north-west corner of the Terminus Building was reroofed, resulting in the removal of its original glazed panels, while an original fig tree in the forecourt was removed in 1983. A garden path was also established in 1990, which required new entrances to be cut into the original dwarf sandstone wall. Conservation works were undertaken in 1996. These included the waterproofing and repaving of the forecourt, which consequently raised the floor level. The promenade around the central garden was also covered in asphalt and is currently used as bus parking for coaches, vehicular short-term parking and dropoffs.

Archaeological testing was conducted in 2009, with two test trenches excavated in the garden of the Western Forecourt. There were two major findings; firstly, demolition depositions associated with the Benevolent Asylum and secondly, some structural remains of the Christ Church Parsonage. Although remains for the Government Cottage were not searched for or found, it was concluded that all three buildings previously occupied a portion of the garden area.



**Figure 110: Proposed design for the Western Forecourt, as published in a local newspaper.**

119

<sup>119</sup> Unidentified newspaper, The New Central Station, 1906, SLNSW, reference: Q339.5/N



Figure 111: Western Forecourt, early photo – date unknown. <sup>120</sup>



Figure 110: Union March on Western Forecourt, c 1930s. <sup>121</sup>



Figure 112: Western Forecourt Garden in 1927. <sup>122</sup>



Figure 113: Western Forecourt 1930. <sup>123</sup>

<sup>120</sup> Sydney Trains Archives, unknown date. ID: FB\_IMG\_1525645931436

<sup>121</sup> State Library of NSW, 1930s. ID: 04758R.JPG

<sup>122</sup> State Library of NSW, 1927. ID: a6821019h.

<sup>123</sup> Sydney Trains Archives, 1930.



Figure 114: Western Forecourt, 1954.<sup>124</sup>



Figure 115: Western Forecourt heralding the arrival of the Queen in 1954.<sup>125</sup>

### 3.5.2 Site Description and Condition

The Western Forecourt is located at the north-west corner of the Sydney Terminal Precinct. The area is bound by the former Parcels Area to the south, the Main Terminus Building to the east, Eddy Avenue to the north and Pitt Street to the west. It is comprised of a large open space that contains a garden, roundabout, and carpark. The floor level of the forecourt slopes upwards towards the western pedestrian entrance of the Main Terminus Building, which is located at concourse level. The lowest point of the area is at the entrance to the forecourt, which is at the south-west intersection between Railway Colonnade Drive and George Street.

In 2009, archaeological testing was undertaken in the Western Forecourt. This confirmed that remains of the 1820s Benevolent Asylum and Christ Church Parsonage were still present on site. As such, this area is considered to have high archaeological potential.

#### The Eastern Boundary

As the Western Forecourt primarily functions as a commuter pickup and dropoff zone for buses, taxis and cars, the majority of the ground surface is covered in bitumen. The eastern boundary of the forecourt is lined with a pedestrian walkway that has intermittent bus stops and coach stops. This walkway is covered in asphalt from the forecourt entrance at the intersection of George, Pitt and Lee Streets to the northern boundary of the West Wing Extension, which abuts the Main Terminus building to its north. The portion of the walkway that wraps around the Main Terminus Building is paved and edged with early trachyte kerb stones. Despite the area being repaved and waterproofed in the late 1990s, the kerb stones were retained and re-laid at this time.

Several original building features are visible in the area adjacent to the West Wing Extension. Directly adjacent to the building's western façade is a series of semi-circular areas beneath the windows which are fenced off with cast iron balustrades. These areas enclose semi-circular pavement lights, which originally provided light to the basement mezzanine level of the West Wing Extension. These pavement lights have since been boarded up, with the pavement light to the south of the West Wing Extension entrance infilled with concrete. The balustrades help to protect the historic fabric.

The western façade also contains an original archway that was used as the Governor's Entrance into the station. This features glass brick pavement lights at the entrance, as well as remnant fabric from the original gas light fitting that surmounted the sandstone archway. This entrance is now reserved for emergency services vehicle access. From the southern end of the West Wing Extension the pathway

<sup>124</sup> State Records, 1954.

<sup>125</sup> State Records, 1954. ID: 17420\_A014\_A014000157.



curves west to meet the forecourt's Pitt Street entrance. A brick retaining wall that features original sandstone saddle coping borders this portion of the pathway. At the bottom of the pathway there are two, rounded sandstone piers that signify the entrance to the forecourt. These are from the same era and feature the same design as those at the Hay Street entrance and the Western Tram Ramp. Surmounting these sandstone piers are cast iron elbow lighting with embossed 'GR' at the base. These light fittings are reconstructions of the originals which sat above the piers. Contemporary bollards have been erected between the piers to separate vehicular and pedestrian traffic.

There is a large awning attached to the Main Terminus Building. This runs the length of the western façade before wrapping around the north-west corner of the building to abut with the western end of the porte-cochre. The structure is supported by profiled steel bracing that is set on sandstone wall brackets. It features wrought iron trusses that arch onto and then cantilever beyond circular cast iron column supports. A vertical timber valance similar to those adorning the platform awnings, decorates this awning. Above the arched pedestrian entrance into the Main Terminus Building, the awning contains an arched wire glazed insert in the timber valance. This signifies the entrance and contributes to the aesthetic contribution of the structure. Contemporary services have been attached to the awning, with electrical services mounted to the bottom and floodlights mounted to the top.



**Figure 116. West Wing Extension and eastern boundary of the forecourt.**<sup>126</sup>



**Figure 117. Eastern boundary wall, constructed from brick with a sandstone saddleback coping.**<sup>127</sup>



**Figure 118. Eastern boundary wall terminates in a sandstone pier and lampstand.**<sup>128</sup>



**Figure 119. West Wing Extension and 'Governor's Arch' as viewed from the Central Garden.**<sup>129</sup>

<sup>126</sup> Artefact, 2021

<sup>127</sup> Artefact, 2021

<sup>128</sup> Artefact, 2021

<sup>129</sup> Artefact, 2021





**Figure 120. Main Terminus Building awning, with timber vallance with arched wire glazed inserts.<sup>130</sup>**



**Figure 121. Southern extent of the Main Terminus Building awning.<sup>131</sup>**



**Figure 122. Main Terminus Building awning wraps around the north-west of the building to join with the porte-cochere.<sup>132</sup>**



**Figure 123. Awning is supported by steel bracing trusses on cast iron columns and sandstone brackets.<sup>133</sup>**

## The Western Boundary

The Western Forecourt's western boundary is dictated by a low ashlar sandstone wall with a sandstone saddle coping. The wall is divided by squat sandstone columns which are topped by a sandstone column cap. The wall is an original feature of the precinct and surmounts the Pitt Street Arcade running the length of the forecourt and abuts the southern end of the Western Tram Ramp to the north. There is also a clear distinction between the rusticated sandstone wall of the Pitt Street Arcade and the ashlar sandstone boundary wall which surmounts it.

A series of London Plane trees line Pitt Street, and run the length of the Western Forecourt, beautify the streetscape, but also create a visual barrier between the forecourt and the adjacent busy road. From the south-west entrance to the forecourt, the gradual slope provides prominent views towards the Main Terminus Building and its landmark clocktower. Similarly, extensive views of Railway Square are visible when looking south-west from the West Wing Extension.

<sup>130</sup> Artefact, 2021

<sup>131</sup> Artefact, 2021

<sup>132</sup> Artefact, 2021

<sup>133</sup> Artefact, 2021



**Figure 124. Sandstone west boundary wall seen from entry to Western Forecourt.<sup>134</sup>**



**Figure 125. West boundary wall and London Plane trees on Pitt Street.<sup>135</sup>**

### The Northern Light Wells

Located underneath the northern end of the Western Forecourt is a loading dock. This sits directly behind the Pitt Street Arcade at pedestrian level but is visible from the Western Forecourt via three large, rectangular light wells. The light wells are located at concourse level and sit to the west of the pedestrian entrance to the Main Concourse Building. They are separated by narrow pedestrian walkways and surrounded by dwarf sandstone walls that feature curved wrought iron balustrades in the Art Nouveau style. These balustrades are original to the forecourt and are examples of the type of safety features used at that time.



**Figure 126. Northern lightwell as viewed from the Central Garden.<sup>136</sup>**



**Figure 127. Northern lightwell, sandstone plinth and cast-iron palisade fencing.<sup>137</sup>**

<sup>134</sup> Artefact, 2021

<sup>135</sup> Artefact, 2021

<sup>136</sup> Artefact, 2021

<sup>137</sup> Artefact, 2021



**Figure 128. Northern lightwells as viewed from west looking towards the Main Terminal Building.<sup>138</sup>**



**Figure 129. Looking down into the Northern lightwell to loading dock below.<sup>139</sup>**

### The Central Garden

Central to the Forecourt is the garden area. This large, grassed area contains a central asphalt footpath that curves through the garden to lead to the Main Terminus Building. The garden, which features mature plantings, is partially enclosed by a fence. This is comprised of a dwarf sandstone wall, with wrought iron posts that are connected by a heavy iron link chain. There are two early sandstone piers: one to the garden path entry at the south and the other to the mid of the western perimeter. These piers feature cast iron elbow lighting fixtures with embossed 'GR' detail at their base, replicas of the original light fixtures which were mounted on these piers. The path, which is not original, dates to the 1990s. The path has a concrete block edging with contemporary low steel posts connected with a steel link chain. Construction of the path resulted in damage to and the removal of some portions of the original dwarf sandstone wall.



**Figure 130. Western Forecourt, the Central Garden to the left.<sup>140</sup>**



**Figure 131. The Central Garden, non-original path bisects north-south through the garden.<sup>141</sup>**

<sup>138</sup> Artefact, 2021

<sup>139</sup> Artefact, 2021

<sup>140</sup> Artefact, 2021

<sup>141</sup> Artefact, 2021





**Figure 132. Original sandstone boundary wall with low cast iron balustrade and chain fencing.<sup>142</sup>**



**Figure 133. Looking south down the Western Forecourt to the Central Garden.<sup>143</sup>**



**Figure 134. Original sandstone plinth with replica cast iron lampstand.<sup>144</sup>**



**Figure 135. Detail of lampstand embossing which has the NSW GR insignia.<sup>145</sup>**

### 3.5.3 Statement of Significance

The Western Forecourt is of state significance and forms an original part of the exceptionally significant Sydney Terminal precinct and is evidence of the rare urban planning and amenities which were provided as part of the design of the Station.

The Western Forecourt is integral to the setting of the Main Terminus Building and is a prominent landscape feature in views to and from the western area of the Central Station complex.

The Western Forecourt has aesthetic significance for its landscape design and architectural features include fine ashlar sandstone wall to its western perimeter and brick wall with sandstone coping to its eastern perimeter. Art Nouveau cast iron palisade fencing, detailed sandstone carving to piers and decorative cast iron light fixtures add to the overall aesthetic contribution of the area.

<sup>142</sup> Artefact, 2021

<sup>143</sup> Artefact, 2021

<sup>144</sup> Artefact, 2021

<sup>145</sup> Artefact, 2021



The area also has technical significance for its design across a gradient site, making use of the area underneath as a loading dock, and providing light amenity through lightwells.

The area is historically significant for its construction as part of the original elements of the Sydney Terminal, but also for its high archaeological potential for evidence of the 19th century institutional buildings, which are rare and important surviving artefacts from the early Colonial period in Sydney.

### 3.5.4 Gradings of Significance and Integrity

**Table 15: Gradings of Significance and Integrity for the Western Forecourt**

Element	Grading	Integrity
<b>Western Forecourt Overall</b>	Exceptional	Moderate-High
<b>Views and Vistas</b>	Exceptional	High
<b>Context and Setting (including ramped roadway)</b>	Exceptional	High
<b>Northwest Forecourt</b>	High	Moderate Changed traffic conditions and setting
<b>Wrap around Awning, Cast Iron Columns, Trusswork, Wire Glass and Timber Valance</b>	Exceptional	High
<b>Brick Walls, Sandstone Walls, Parapets and Pillars</b>	Exceptional	High
<b>Glass Brick Pavement lights</b>	Exceptional	High Integrity of pavement lights unknown as beneath boarding
<b>Lightwells (above Luggage Dock)</b>	High	High
<b>Garden, Fencing, Landscape Elements and Lighting</b>	High	Moderate Garden path non-original, changes to boundary wall to allow openings for path
<b>Paving, Trachyte Kerbs and Bollards</b>	High/Little	Low-High Trachyte kerbs original (high)
<b>Archaeological Evidence</b>	Exceptional	High potential

### 3.5.5 Site Specific Conservation Policies

**Table 16: Site Specific Conservation Policies for the for the Western Forecourt**

Policy	Policy Discussion and Guidelines
1. Follow Overall Conservation Policies	See Section 15 of this CMP for Conservation Policies.
2. Implement Fabric Guidelines	See Section 15.4.4 of this CMP

Policy	Policy Discussion and Guidelines
3. Item Specific Policy: Heritage Process	<p>When considering change follow the Heritage Flow Charts that form Appendix C of this CMP:</p> <ul style="list-style-type: none"> <li>A) Major Works Heritage Process Flow Chart; or</li> <li>B) Minor Works Heritage Process Flow Chart.</li> </ul>
4. Item Specific Policy: Heritage Listing	<p>See Section 3 in this CMP for Legislative Context, and Section 14.4 for Statutory Context.</p> <p>Ensure appropriate statutory approvals or exemptions are obtained prior to change on the site because:</p> <ul style="list-style-type: none"> <li>• The site is within the overall listing for <b>Central Station</b> on the State Heritage Register under the <i>NSW Heritage Act 1977</i> for Sydney Terminal and Central Railway Stations Group.</li> <li>• The site is within the overall listing of the <b>Central Railway Station and Sydney Terminal Group</b> on Transport Asset Holding Entity's (TAHE) Heritage and Conservation Register under Section 170 of the <i>NSW Heritage Act 1977</i>.</li> <li>• The area is within the Railway Square/Central Station Special Character Area under the Sydney DCP 2012.</li> </ul>
5. Item Specific Policy: Use	The Western Forecourt should remain an open public space.
6. Item Specific Policy: Masonry Conservation	<p>Conservation of the sandstone and brick facades should be undertaken as part of the cyclical maintenance of the building. Particular care and inspection should be undertaken for any projecting or decorative elements (such as parapets, dentils and mouldings) which has a higher risk of public injury. These should be regularly inspected to make sure the risk of falling is minimised and mitigated.</p> <p>A stone and brick strategy for the facades should be prepared to guide the conservation of the masonry.</p> <p>A strategy for the protection of the masonry facades during any major works should be undertaken by heritage architectural specialists and qualified stonemasons with heritage experience prior to the commencement of any works.</p>
7. Item Specific Policy: Configuration	<p>Conserve the heritage configuration of the Western Forecourt including:</p> <ul style="list-style-type: none"> <li>• the garden bed (including lawn, perimeter fence and drains)</li> <li>• the light wells</li> <li>• the curved southern wall</li> <li>• the retaining wall to Pitt street</li> <li>• the Railway Square entrance including sandstone pier and cast-iron lamp base</li> <li>• awning to Main Terminus Building adjacent to the Western Forecourt</li> </ul>
8. Item Specific Policy: Development	<p>Prior to the commencement of any proposed development or works, photographic archival records of the Western Forecourt should be undertaken.</p> <p>Ensure the original external colour schemes and finishes are documented and archivally recorded.</p>

Policy	Policy Discussion and Guidelines
9. Item Specific Policy: Alterations and Additions	<p>Alterations and additions to the Western Forecourt should be avoided. Do not paint the sandstone or masonry facades.</p> <p>Temporary structures may be permissible but should not be erected for longer than 3 months and in keeping with the City of Sydney's DCP requirements.</p>
10. Item Specific Policy: Interpretation	<p>See Sections 9 and 14.8 'Heritage Interpretation' in this CMP.</p> <p>Prepare a heritage interpretation strategy, based on the precinct-wide Central Precinct Heritage Interpretation Strategy, specific to the Western Forecourt as part of Central Precinct. Particular focus should be given to the former landscape and cultivation of the space.</p> <p>Following this, develop and implement a detailed site-specific heritage interpretation plan, aligned with the themes and recommendations of the heritage interpretation strategy.</p>
11. Item Specific Policy: Vistas	<p>Ensure vistas to the Main Terminus Building and the Western Forecourt outlined in Section 8.6 of this CMP are not obscured. These include vistas from:</p> <ul style="list-style-type: none"><li>• Lee Street</li><li>• Pitt Street</li><li>• George Street</li><li>• Railway Square</li><li>• Western Tram Ramp</li><li>• From the Main Terminus Building looking south to Railway Square</li></ul>
12. Item Specific Policy: Lighting	<p>Lighting within the Western Forecourt should be consistent with the guidelines in the <i>Central Station Lighting Strategy</i>.</p>
13. Item Specific Policy: Archaeology	<p>See Sections 10 and 15.6 in this CMP and Appendix B. Where possible, retain archaeological evidence in situ.</p> <p>Seek advice from a suitably qualified archaeologist prior to undertaking any excavation work in the Western Forecourt. Approval for the work may be required pursuant to s57(1) of the NSW Heritage Act, 1977.</p>

## 3.6 PITT STREET ARCADE



Figure 136. Pitt Street Arcade.<sup>146</sup>



Figure 137. Pitt Street Arcade.<sup>147</sup>

### 3.6.1 History of the Pitt Street Arcade

Construction of the third Sydney Terminus occurred in multiple phases. The first was in 1906, at which time the Eddy Avenue Colonnade, Arcade and Shops, and the Pitt Street Arcade were built. These were constructed in anticipation that the new terminus would be a busy commercial centre that was attractive to privately run businesses. The design provided for 22 shops along the Eddy Avenue and Pitt Street frontages. However, with the nationalisation of the railway catering services in 1916, the NSW Railways soon lost interest in private enterprises. This resulted in the shops being modified into spaces that accommodated for various railway functions. Such spaces included the Staff Credit Union at shop 11, the Weighbridge Station at shop 18 and the Survey Section at shop 19. These were all located with frontages along Pitt Street.

As the function of the shops changed over time, the Pitt Street Arcade has undergone several modifications. There are some original shop fronts dating from 1906 and the 1920s, while others were altered post WWII. While the Eddy Avenue shops were restored in the mid-1990s, no conservation works were undertaken to the Pitt Street shops at this time. In order to construct new stairs and an elevator to provide upgraded access to the porte-cochere from the Eddy Avenue and Pitt Street intersection, the original stair connection at the north-west corner of the Arcade was removed. The demolition also removed the original wrought iron balustrades and stone sills from two archways. The balustrades were salvaged and placed in secure storage at the time of their removal.

The Pitt Street stores are leased by various private coach operators and retail, with some stores continuing to be used by railway operations and staff.

<sup>146</sup> Artefact, 2021

<sup>147</sup> Artefact, 2021



The Eddy Avenue Colonnade, Arcade and Shops, and the Pitt Street Arcade, underwent conservation works from 2009-2011. These works included cleaning of the sandstone facades.



Figure 138: Pitt Street Arcade 1930s. <sup>148</sup>



Figure 139: Pitt Street Arcade intersection, 1923. <sup>149</sup>

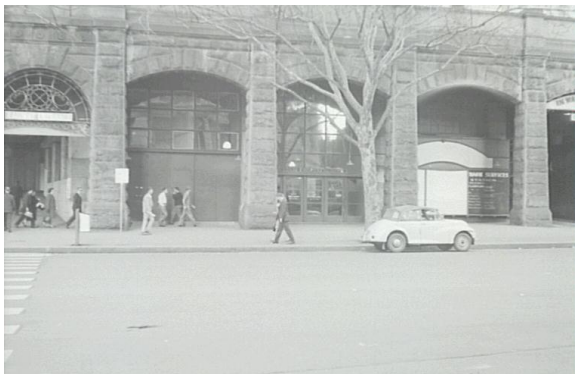


Figure 140: Pitt Street Arcade, 1963. <sup>150</sup>



Figure 141: Pitt Street Arcade, 1963. <sup>151</sup>



Figure 142: Corner of Eddy Ave and Pitt Street Arcade, 1959. <sup>152</sup>



Figure 143: Corner of Eddy Ave and Pitt Street Arcade, 1996. <sup>153</sup>

<sup>148</sup> Sydney Trains Archives, 1930.

<sup>149</sup> Sydney Trains Archives, 1923.

<sup>150</sup> City of Sydney Archives, 1930s.

<sup>151</sup> City of Sydney Archives, 1963. ID: 013522A3.

<sup>152</sup> City of Sydney Archives, 1959. ID: 077702.

<sup>153</sup> City of Sydney Archives, 1996.

### 3.6.2 Site Description and Condition

The Pitt Street Arcade lines the east side of Pitt Street at street level, running south from the intersection of Eddy Avenue and Pitt Street towards the entrance to the Western Forecourt at Railway Square. Near Railway Square the arcades end in a sandstone end wall, which was constructed c.1996 in front of the original sandstone end wall. The end wall was built in order to provide a cavity between the damp original end wall (part of the Western Forecourt retaining wall) and to help improve views to the end of the arcade. The arcade converts to a sandstone retaining wall at its southern end, which retains the lower ramped portion of the Western Forecourt.

The façade is comprised of a series of rusticated sandstone arches that are decorated with voussoir panels and surmounted by keystones. The rhythm of the colonnade is a continuation of the Eddy Avenue Arcade, with the arches framing the shopfronts beyond. Surmounting the colonnade is a sandstone balustrade wall that runs the entire length of the Arcade and its associated retaining wall. This is a component of the Western Forecourt, which is located above the Pitt Street Arcade. A series of London Plane Trees were planted along Pitt Street in the mid-20<sup>th</sup> Century. These remain in-situ and partially obscure the views towards the arcade's facade.

There are two entrance/ exit points in the Pitt Street Arcade that provide access to the luggage dock area which is located beyond. The first of these is the original entrance at the north, which has signage that reads 'Inwards Parcels Way In', while the exit is located at the southern end near the sandstone end wall. Each contain original iron gates that remain in-situ.



Figure 144. Pitt Street Arcade.<sup>154</sup>



Figure 145. Corner of Pitt Street and Eddy Avenue Arcades, with original Central Railway signage.<sup>155</sup>



Figure 146. Pitt Street rusticated sandstone wall.<sup>156</sup>



Figure 147. Termination of the Pitt Street wall into ashlar sandstone and pier with lampstand at the entrance to the Western Forecourt.<sup>157</sup>

### Within the arcade

The arcade is comprised of a double height pedestrian walkway that is enclosed by a street-facing colonnade of rusticated sandstone arches to one side and sandstone arched shopfronts to the other. It contains 13 shops, whose frontages date from c.1906, c.1920 and the mid-20<sup>th</sup> century. Unlike the Eddy Avenue Arcade, the shopfronts are not contained in archways but are instead separated by ashlar sandstone engaged piers. The earlier shop fronts are defined by two main styles: the first containing a central recessed entrance door and the second containing a recessed entrance to one side of the shop front. One of the middle shops varies from both styles and instead features a central recessed entrance with two entry doors. While the earlier shopfronts contain timber framed glazed fenestrations with glazing bars, those from the mid-20<sup>th</sup> century are constructed from aluminium framed glazing. The shopfronts from the later period are easily identifiable as they do not contain recessed entrances.

In 1996, the north-west corner of the Eddy Avenue Arcade ceiling was modified to accommodate the installation of a new escalator and stair. While this corner of the Eddy Avenue Arcade was significantly altered, the original barrel-vaulted ceiling of the Pitt Street Arcade remains in-situ. Like the ceiling in the Eddy Avenue Arcade, this ceiling of the Pitt Street Arcade is supported by the

<sup>154</sup> Artefact, 2021

<sup>155</sup> Artefact, 2021

<sup>156</sup> Artefact, 2021

<sup>157</sup> Artefact, 2021



colonnade and shop front walls, however features corrugated metal finish rather than the original plaster finish found in the Eddy Avenue Arcade.

Other early features include two wrought iron sign brackets and original wrought ironwork. The sign brackets, which may be original fabric or an early adaptation, are fixed to the sandstone wall and contain contemporary signage. The original wrought ironwork is located at the intersection between the Pitt Street and Eddy Avenue arcades. It sits at the crown of the first arched sandstone opening to both colonnades, with the detail identical on both facades. The wrought iron features the words 'Railway Station Central' and has the state badge of NSW at its centre.



Figure 148. Pitt Street Arcade.<sup>158</sup>

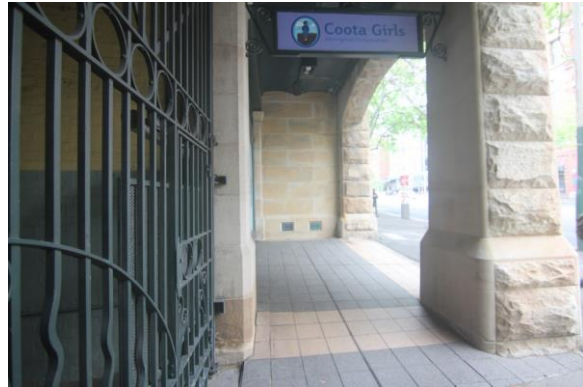


Figure 149. Pitt Street Arcade.<sup>159</sup>



Figure 150. Shop front 480 in Pitt Street Arcade.<sup>160</sup>



Figure 151. Shop front 490 in Pitt Street Arcade.<sup>161</sup>

## Shop Interiors

Although some shops have had mezzanine levels added, the configuration of the shop interiors is mostly original. As such, they feature original concrete rendered rear walls, partition walls with blind arches and a barrel-vaulted ceiling. To the rear of the shops there are original pairs of arched timber framed windows. However, the rear of some shops was altered in the 20th century to allow for the insertion of an access door to the car parking area in the luggage dock. Contemporary modifications to some of the shops include the insertion of ceilings that conceal the original barrel-vaulted ceilings, the addition of partition walls and the introduction of intrusive services. Some of the original blind arches have also been opened up to combine two shop interiors into one.

<sup>158</sup> Artefact, 2021

<sup>159</sup> Artefact, 2021

<sup>160</sup> Artefact, 2021

<sup>161</sup> Artefact, 2021



One of the shops contains a small contemporary mezzanine floor which is accessed by a cast iron spiral staircase. This staircase matches those found in the Eddy Avenue Arcade shops which were designed in the UK by St Pancras Iron Work Company at the turn of the 20th century. While it is unknown whether these shops originally contained mezzanine levels, in this instance the position of the stair and the configuration of the mezzanine level are not consistent with the 1906 of the Eddy Avenue Shops. It is considered unlikely that the stair is in its original position and is more likely that , it may have been relocated from one of the shops along Eddy Avenue.



**Figure 152. Typical interior of Pitt Street Arcade, original arched party walls, intrusive mezzanines and fitouts.**<sup>162</sup>



**Figure 153. Typical interior of Pitt Street Arcade, original arched party walls, intrusive mezzanines and fitouts.**<sup>163</sup>

### 3.6.3 Statement of Significance

The Pitt Street Arcade is of State significance as an original feature and design of the Main Terminus Building and its presentation to Pitt Street, and its interaction with the streetscape and users.

The area is significant for its aesthetic contribution, exemplifying the Neo-Classical Architectural style which has been used throughout much of the Sydney Terminal Precinct, and the predominant use of sandstone which is typical of important public buildings built in the early 20<sup>th</sup> century. The Pitt Street Arcade also exemplifies technical innovation through its construction including colonnades and steel supported barrel-vaults. The shops contribute to the significant and constant use of the spaces for retail/commercial uses.

The precinct retains a high level of integrity in its retention of original configuration, fabric and decorative features

### 3.6.4 Gradings of Significance and Integrity

**Table 17: Gradings of Significance and Integrity for the Pitt Street Arcade**

Element	Grading	Integrity
<b>Pitt Street Arcade Overall</b>	Exceptional	Moderate
<b>Views and Vistas</b>	High	High
<b>Context and Setting</b>	Exceptional	High
<b>Sandstone Façade Wall, Doors, Windows and Hardware</b>	Exceptional	High

<sup>162</sup> Artefact, 2021

<sup>163</sup> Artefact, 2021

Element	Grading	Integrity
Sandstone Colonnade	Exceptional	High
Original/early shop fronts with recessed entrances, glazing and glazing bars	Exceptional	High
Original barrel vaulted ceilings	Exceptional	Low-High Extent of undamaged ceiling underneath false ceilings is unknown
Blind arched party walls	Exceptional	Moderate Arches have been opened up in some spaces
Double height spatial volume	Exceptional	Low Intrusive alterations including mezzanines

### 3.6.5 Site Specific Conservation Policies

**Table 18: Site Specific Conservation Policies for the for the Pitt Street Arcade**

Policy	Policy Discussion and Guidelines
1. Follow Overall Conservation Policies	See Section 15 of this CMP for Conservation Policies.
2. Implement Fabric Guidelines	See Section 15.4.4 of this CMP.
3. Item Specific Policy: Heritage Process	When considering change follow the Heritage Flow Charts that form Appendix C of this CMP: A) Major Works Heritage Process Flow Chart; or B) Minor Works Heritage Process Flow Chart.
4. Item Specific Policy: Heritage Listing	See Section 3 in this CMP for Legislative Context, and Section 14.4 for Statutory Context.  Ensure appropriate statutory approvals or exemptions are obtained prior to change on the site because: <ul style="list-style-type: none"> <li>The site is within the overall listing for <b>Central Station</b> on the State Heritage Register under the <i>NSW Heritage Act 1977</i> for Sydney Terminal and Central Railway Stations Group.</li> <li>The site is within the overall listing of the <b>Central Railway Station and Sydney Terminal Group</b> on Transport Asset Holding Entity's (TAHE) Heritage and Conservation Register under Section 170 of the <i>NSW Heritage Act 1977</i>.</li> <li>The area is within the Railway Square/Central Station Special Character Area under the Sydney DCP 2012.</li> </ul>
5. Item Specific Policy: Use	Encourage use of the shops for retail and commercial purposes.

Policy	Policy Discussion and Guidelines
6. Item Specific Policy: Stone Conservation	<p>Conservation of the sandstone facades should be undertaken as part of the cyclical maintenance of the building. Particular care and inspection should be undertaken for any projecting or decorative elements (such as pediments, entablatures, dentils and mouldings) which has a higher risk of public injury. These should be regularly inspected to make sure the risk of falling is minimised and mitigated.</p> <p>A stone strategy for the facades should be prepared to guide the conservation of the masonry.</p> <p>A strategy for the protection of the masonry facades during any major works should be undertaken by heritage architectural specialists and qualified stonemasons with heritage experience prior to the commencement of any works.</p>
7. Item Specific Policy: Shopfronts	<p>Consider the replacement of the existing modern shopfronts with a design which celebrates the historic precedent.</p>
8. Item Specific Policy: Development	<p>Prior to the commencement of any proposed development or works, photographic archival records of the precinct should be undertaken.</p> <p>Ensure the original external colour schemes and finishes are documented and archivally recorded.</p>
9. Item Specific Policy: Alterations and Additions	<p>Alterations and additions to the Pitt Street Arcade should be avoided. Do not paint sandstone facades.</p> <p>Changes to the interior fit-out of the shops is permitted, but should not impact original features such as double heightened space, barrel vaulted ceilings and internal inter-tenancy walls.</p> <p>An interior fit-out guide for the shop tenancies should be prepared to help guide future tenancy fit-outs and to provide a level of consistency across the stores in their treatment and protection of the heritage fabric and spaces.</p> <p>Changes to external colours and finishes of window/door joinery should be sympathetic to the heritage fabric and consider where possible the implementation of original or early schemes. If changes to the external colour occur, this should be implemented consistently across all facades of the Main Terminus Building.</p>
10. Item Specific Policy: Significant configurations & elements	<p>Conserve the significant original configuration and elements of the Pitt Street Arcade including:</p> <ul style="list-style-type: none"> <li>• Arches</li> <li>• Barrel vaults</li> <li>• Pavement widths</li> <li>• Wrought iron signage brackets</li> <li>• Wrought iron 'Railway Station Central' sign at the northern arch, intersection with Eddy Avenue</li> <li>• Shop fronts</li> <li>• Double heightened spaces</li> </ul>

Policy	Policy Discussion and Guidelines
11. Item Specific Policy: Management of Signage	<p>Ensure all tenants of the shop spaces (including any Responsible government Agency use of the spaces) follow a Commercial Signage Manual for the overall Central Station site.</p> <p>Ensure all other signage is also managed under an overarching Signage Manual for the site.</p>
12. Item Specific Policy: Lighting	<p>Lighting within the Pitt Street Arcade should be consistent with the guidelines outlined in the <i>Central Station Lighting Strategy</i>.</p>
13. Item Specific Policy: Interpretation	<p>See Sections 9 and 14.8 'Heritage Interpretation' in this CMP.</p> <p>Prepare a heritage interpretation strategy, based on the precinct-wide Central Precinct Heritage Interpretation Strategy, specific to the Pitt Street Arcade as part of Central Precinct. Particular focus should be given to the former landscape and cultivation of the space.</p> <p>Following this, develop and implement a detailed site-specific heritage interpretation plan, aligned with the themes and recommendations of the heritage interpretation strategy.</p>
14. Item Specific Policy: Vistas	<p>Ensure vistas to the Pitt Street Colonnade are not obscured. Significant vistas are from Pitt Street, Eddy Avenue/Rawson Place intersection, St Laurence Lane and George/Quay Street intersection.</p>
15. Item Specific Policy: Archaeology	<p>See Sections 10 and 15.6 in this CMP and Appendix B. Where possible, retain archaeological evidence in situ.</p> <p>Seek advice from a suitably qualified archaeologist prior to undertaking any excavation work in identified areas of archaeological potential. Approval may be required pursuant to s57(1) of the NSW Heritage Act, 1977.</p>



## 3.7 EAST DECK



Figure 154. East Deck.<sup>164</sup>



Figure 155. East Deck looking north.<sup>165</sup>

### 3.7.1 History of the East Deck

The first stage of construction of the third Sydney Terminal occurred between 1901 and 1906, at which time the East Deck was constructed. The purpose of the East Deck was to facilitate access between the tram porte-cochère and the original stairs of the eastern archway, which lead to Eddy Avenue below. However, due to lack of funding, the first phase of construction of the Terminus was expedited and the full intent of the Eastern Deck was never realised.

At the end of the first phase of construction, the Main Terminus was completed up to and including the first level which included the Grand Concourse. The East Deck faced the working-class terraces of Surry Hills and as such, less importance was given to its elements. Part of the façade was completed in brick, not only due to budget constraints but also because it was considered a less prestigious fabric. The rest of the façade was completed in stonework. Despite detailing the stone for the insertion of awning trusses, an awning structure was never constructed. If it was constructed, the design of the awning would have been similar those found on the Western Forecourt. The Eastern Deck was also designed to be considerably smaller than the Western Forecourt. It was originally paved in asphalt, with a cast iron balustrade and sandstone dwarf wall to its eastern and northern edges. More design planning and consideration were afforded to both the Northern and Western facades and decks, as these were more prominent facades.

In the 1920s, the Central Electric Station was erected in the 1920s, to the east of the Sydney Terminus building. This resulted in increased pedestrian traffic, as passengers travelled from the North Concourse on the east to the main platforms on the west. This increased the usage of the East Deck, as passengers used the narrow stairs in the east archway to transfer between both Central Electric and the Grand Concourse. By the late 20th century, the stairs were often closed off during peak hours due to heavy use and congestion.

The East Deck was largely demolished and rebuilt in 1995. It was originally constructed from a concrete slab with supporting sandstone arches. However, this was replaced by a brick arcade, which was designed to line the Eddy Avenue Forecourt on its western side. The demolition works allowed the East Deck to be waterproofed at the main concourse level and once this was installed, the ground was paved. The original east archway stairs that were closed in late 20th century were also reopened, while new escalators were also installed to link the East Deck and Eddy Avenue forecourt. Although

<sup>164</sup> Artefact, 2021

<sup>165</sup> Artefact, 2021

the original stairs east stairs to the Grand Concourse were reopened, its original cast iron balustrade was replaced with a contemporary metal balustrade. The dwarf sandstone wall was also replaced with a metal balustrade.



**Figure 156: Eddy Avenue Forecourt and shops with east deck on the right hand side, 1984.** <sup>166</sup>

### 3.7.2 Site Description and Condition

The East Deck is located to the east of the Main Terminus Building, to the east of the Grand Concourse. It is bound by Eddy Avenue to the North and wraps around the Main Terminus Building to the west to connect to the porte-cochere. The Eddy Avenue Forecourt, with several western shopfronts, is located underneath the East Deck. To the southern end of the East Deck, there are a series of escalators that connect the deck to the Eddy Avenue Forecourt and the North Concourse of Central Electric below. On the concourse level, the escalators are protected by an intrusive steel awning with corrugated sheeting that extends north to cover the pedestrian entrance into the Grand Concourse area. This entrance is comprised of a large archway that is located in the eastern facade of the Main Terminus Building.

The archways of the deck are constructed in brick and have an arched form that imitates the Eddy Avenue Arcade, which is located to the west of the East Deck on the northern façade of the Main Terminus Building. At concourse level, the East Deck is paved with contemporary concrete pavers and has a metal balustrade that surrounds the perimeter. The design of the balustrade is based on the original balustrade, and the lanterns that line the deck are also replicas of the originals. These lanterns provide lighting to the area and have a similar form to those mounted on the eastern façade of the Main Terminus Building. The only original feature that remains associated with the East Deck is a stair that connects the Eddy Avenue Forecourt to the Grand Concourse of the Main Terminus building. At forecourt level, this is located slightly to the north of the escalators. Today, the East Deck is prominently used as a thoroughfare between the escalators and the MLR stop in the porte-cochere.

<sup>166</sup> Sydney Trains Archives, 1984.



However, at forecourt level it does provide shelter for outdoor seating which is used by the adjacent food outlets.



**Figure 157. The eastern façade of the Main Terminus Building as presented to the East Deck.<sup>167</sup>**



**Figure 158. East Deck looking north.<sup>168</sup>**



**Figure 159. The eastern façade of the Main Terminus Building features a juxtaposition of rusticated sandstone on the 'primary elevations' and brick with sandstone banding on the 'secondary elevations'.<sup>169</sup>**



**Figure 160. East Deck looking south.<sup>170</sup>**



**Figure 161. North-eastern corner of the East Deck at its intersection with the eastern tram ramp.<sup>171</sup>**



**Figure 162. Main Terminus Building and East Deck.<sup>172</sup>**

<sup>167</sup> Artefact, 2021

<sup>168</sup> Artefact, 2021

<sup>169</sup> Artefact, 2021

<sup>170</sup> Artefact, 2021

<sup>171</sup> Artefact, 2021

<sup>172</sup> Artefact, 2021



Figure 163. East Deck looking towards the porte-cochere.<sup>173</sup>



Figure 164. Escalator insertions and canopy at the southern end of the East Deck.<sup>174</sup>



Figure 165. Original stair leading from the East Deck to the Eddy Avenue forecourt.<sup>175</sup>



Figure 166. Under the East Deck within the Eddy Avenue forecourt colonnades.<sup>176</sup>



Figure 167. Escalator insertions into the East Deck.<sup>177</sup>



Figure 168. East Deck as viewed from Eddy Avenue forecourt.<sup>178</sup>

### 3.7.3 Statement of Significance

The East Deck is part of the original form of the Main Terminus Building as designed in c.1906. Whilst the East Deck has largely been demolished, the reconstructed structure is of a similar scale as the original and is a sympathetic addition to the area, providing additional amenity to through a sheltered

<sup>173</sup> Artefact, 2021

<sup>174</sup> Artefact, 2021

<sup>175</sup> Artefact, 2021

<sup>176</sup> Artefact, 2021

<sup>177</sup> Artefact, 2021

<sup>178</sup> Artefact, 2021



undercroft in the Eddy Avenue Forecourt. The design of the reconstructed East Deck aesthetically contributes to the surrounding significant areas of the Sydney Terminal and Central Electric Precincts, through its sympathetic arcade design and use of brick materials.

The original eastern stair which connects the Eddy Avenue Forecourt to the Grand Concourse is of historic significance as the only extant element from the original East Deck. The form and scale of the stair has been retained, however its integrity has been impacted due to the altered material finishes.

The East Deck has been assessed as having local heritage significance.

### 3.7.4 Gradings of Significance and Integrity

**Table 19: Gradings of Significance and Integrity for the East Deck**

Element	Grading	Integrity
<b>East Deck Overall</b>	Moderate	Moderate Not original East Deck
<b>Views and Vistas</b>	Moderate	Moderate
<b>Context and Setting</b>	Moderate	Moderate
<b>Escalator</b>	Intrusive	N/A Non original, but in good condition and intact
<b>Awning</b>	Intrusive	High Non original, but in good condition and intact
<b>Terrace and Paving</b>	Little	N/A Non original, but in good condition and intact
<b>Signage and Wayfinding</b>	Little	Moderate
<b>Sandstone Balustrade</b>	High	High
<b>Metal Balustrade</b>	Little	N/A Non original, in good condition and sympathetic
<b>Reproduction carriage lamps</b>	Moderate	Moderate

### 3.7.5 Site Specific Conservation Policies

**Table 20: Site Specific Conservation Policies for the for the East Deck**

Policy	Policy Discussion and Guidelines
1. Follow Overall Conservation Policies	See Section 15 of this CMP for Conservation Policies.
2. Implement Fabric Guidelines	See Section 15.4.4 of this CMP

Policy	Policy Discussion and Guidelines
3. Item Specific Policy: Heritage Process	<p>When considering change follow the Heritage Flow Charts that form Appendix C of this CMP:</p> <p>A) Major Works Heritage Process Flow Chart; or B) Minor Works Heritage Process Flow Chart.</p>
4. Item Specific Policy: Heritage Listing	<p>See Section 3 in this CMP for Legislative Context, and Section 14.4 for Statutory Context.</p> <p>Ensure appropriate statutory approvals or exemptions are obtained prior to change on the site because:</p> <ul style="list-style-type: none"> <li>• The site is within the overall listing for <b>Central Station</b> on the State Heritage Register under the <i>NSW Heritage Act 1977</i> for Sydney Terminal and Central Railway Stations Group.</li> <li>• The site is within the overall listing of the <b>Central Railway Station and Sydney Terminal Group</b> on Transport Asset Holding Entity's (TAHE) Heritage and Conservation Register under Section 170 of the <i>NSW Heritage Act 1977</i>.</li> <li>• The area is within the Railway Square/Central Station Special Character Area under the Sydney DCP 2012.</li> </ul>
5. Item Specific Policy: Masonry Conservation	<p>Conservation of the sandstone and brick facades should be undertaken as part of the cyclical maintenance of the building. Particular care and inspection should be undertaken for any projecting or decorative elements (such as parapets, dentils and mouldings) which has a higher risk of public injury. These should be regularly inspected to make sure the risk of falling is minimised and mitigated.</p> <p>A stone and brick strategy for the facades should be prepared to guide the conservation of the masonry.</p> <p>A strategy for the protection of the masonry facades during any major works should be undertaken by heritage architectural specialists and qualified stonemasons with heritage experience prior to the commencement of any works.</p>
6. Item Specific Policy: Use	<p>Continue to use the East Deck as a pedestrian access point between the Grand Concourse, porte-cochere and Eddy Avenue Forecourt. Ensure the sense of progression from the deck to the Grand Concourse and on to the platforms is maintained.</p>
7. Item Specific Policy: Alterations and Additions	<p>Alterations and additions to the East Deck should be avoided where possible. Retain an open uncluttered space which is used as a pedestrian thoroughfare or for the use of emergency vehicles. Minimise obstructing views and understanding of the significant sandstone facades of the Main Terminus Building.</p>
8. Item Specific Policy: Code Compliance	<p>Alterations to the East Deck to upgrade the space to meet current code compliance, such as AS1428.1 is permissible.</p>
9. Item Specific Policy: Intrusive fabric.	<p>Demolition of, or changes to, the existing intrusive building structures and services should be recommended without formal approval provided no significant fabric is impacted on by the change.</p>
10. Item Specific Policy: Lighting	<p>Lighting within the East Deck should be consistent with the guidelines outlined in the <i>Central Station Lighting Strategy</i>.</p>
11. Item Specific Policy: Maintenance	<p>Ensure the significant fabric in the area is conserved through a maintenance program, particularly for fabric in poor condition.</p>

Policy	Policy Discussion and Guidelines
12. Item Specific Policy: Interpretation	<p>See Sections 9 and 14.8 'Heritage Interpretation' in this CMP.</p> <p>Prepare a heritage interpretation strategy, based on the precinct-wide Central Precinct Heritage Interpretation Strategy, specific to the East Deck as part of Central Precinct. Particular focus should be given to the former landscape and cultivation of the space.</p> <p>Following this, develop and implement a detailed site-specific heritage interpretation plan, aligned with the themes and recommendations of the heritage interpretation strategy.</p>

## 3.8 MAIN TERMINUS BUILDING: GRAND CONCOURSE AND RELATED SPACES



Figure 169. Grand Concourse and related spaces.<sup>179</sup>



Figure 170. Grand Concourse.<sup>180</sup>

### 3.8.1 History of the Main Terminus Building: Grand Concourse and Related Spaces

#### Grand Concourse

The Grand Concourse was designed as a feature of the new Sydney Terminus building. Originally called the ‘assembly platform’ it was comprised of a voluminous space that provided a shelter and waiting area for passengers. The concourse and its related space covered two-thirds of an acre, being 348 feet long and 72 feet wide. Due to the large scale of the space, it was intended that the passengers would have more room and experience less commotion during their travels. The concourse was described by Henry Deane as being ‘*specially commodious*’, as it was able to accommodate double the number of passengers (40,000) and provided access to a larger platform area.<sup>181</sup>

It was proposed that the arrival and departures platforms would be built above the Eddy Avenue street level, meaning that the new Terminus Building would span across three levels. The building was designed to contain a basement (at street level), whilst the Grand Concourse and platforms would be located above. This allowed a network of subway tunnels to be constructed beneath the station, which Henry Deane believed would provide a safer and more efficient way of transporting baggage, parcels, and mail. This subway network would then connect to the platforms via luggage lifts. In addition to the subway network and their associated baggage and luggage stores, the basement area under the Grand Concourse was also designed to contain a boiler room, barber’s salon, cellar, store, kitchen, electrical substation, records store, and staff facilities.

As the design intention for the Grand Concourse was for a clear, open, and efficient space, little furniture was installed inside the passenger areas. There were however some items including a timber information board, book stalls located between platforms 6/7 and 8/9 and an Inquiry and Telegraph Booth between platforms 2/3 and 4/5. These items are no longer extant.

<sup>179</sup> Artefact, 2021

<sup>180</sup> Artefact, 2021

<sup>181</sup> Rappaport Pty Ltd & NSW Government Architects Office, 2013.



Modernisation programs during the mid-20th century resulted in modifications to the Grand Concourse area. These were undertaken in both 1955 and 1965, and included the following changes:

- Addition of bulkhead murals in the cafeteria
- Replacement of asphalt paving with terrazzo flooring (including map of Australia and other symbols)
- New timber parquetry
- Reconfiguration and refurbishment of the booking hall
- New screens and barriers, and some automated sliding doors
- New seating and planter boxes
- New lighting
- Refurbishment of kiosks and retail concessions<sup>182</sup>

In c.1982, the main indicator board was removed and replaced with television train indicator monitors. Prior to its removal, the main indicator board was a focal point of the southern extent of the Grand Concourse. On its removal it was donated to the Powerhouse Museum collection. In c.2006 the television train indicator monitors were replaced by electronic indicator boards.



**Figure 171: Central Railway Grand Concourse with fruit and pastry stands c.1900.**<sup>183</sup>



**Figure 172: Central Railway Grand Concourse soda fountain, 1906.**<sup>184</sup>

<sup>182</sup> Rappaport Pty Ltd & NSW Government Architects Office, 2013.

<sup>183</sup> Sydney Trains Archives, c.1900s.

<sup>184</sup> Sydney Trains Archives, 1906.



Figure 173: Grand Concourse 1927.<sup>185</sup>



Figure 174: Grand Concourse 1956.<sup>186</sup>



Figure 175: Grand Concourse 1962.<sup>187</sup>



Figure 176: Central Assembly Platform, 2 tone source, date unknown -presumably early 1900s<sup>188</sup>

## Roof and Truss Work

Inspired by the train sheds of Victoria and St Pancras Stations in London, an early design for the third Sydney Terminal building also incorporated a large train shed. The proposed shed was to run parallel to and enclose the platforms, however, this design was never realised. Instead, the porte-cochère, and Grand Concourse area of the c.1906 Terminus was constructed with a two-barrel vaulted truss roof system.

The roof was designed by prominent bridge and structural engineer, J W Roberts, from the Railway Construction Branch of Public Works. Due to the large size of the Grand Concourse, the roof had one of the largest spans in Sydney. At the time, the Grand Concourse was one of the city's largest and most accessible public spaces. This contributed to the landmark quality of the terminus.

This roof system was a special feature of the Grand Concourse and the Main Terminus Building. It was externally visible from multiple vantage points around the terminus, including Belmore Park and the elevated approach ramps, making it a prominent focal point of the precinct. During the second phase of construction of the terminus, from 1915-1921, office levels were added in a 'U' shape around

<sup>185</sup> State Library of NSW, photograph by Sam Hood, 1927. ID: ON204\_42\_59.

<sup>186</sup> Sydney Trains Archives, 1956.

<sup>187</sup> Sydney Trains Archives, 1962.

<sup>188</sup> Sydney Trains Archives, date unknown.

the main concourse area. Due to the height of the additional storeys, the views of the roof from these vantage points were obstructed. This reduced the external visual prominence of the structure.

A modernization program in the mid-20th century resulted in various modifications to the Main Terminus Building's roof. This included the replacement of the original corrugated iron roof sheeting with metal roof sheeting, as well as installation of fiberglass to the roof and the roof monitor (similar to a roof lantern). However, the structure of the roof was also altered, as strips of the barrel vault's skylight were removed on either side of the roof monitor. The roof monitor ran the length of the vault's apex. The trusses were also repainted as part of these works.



**Figure 177: Roof and Truss of the Grand Concourse in 1906.** <sup>189</sup>



**Figure 178: Roof and Truss of Grand Concourse in 1986.** <sup>190</sup>

## Related Spaces

The Grand Concourse was designed to prioritize the comfort of passengers. Not only did the new structure provide more shelter from the weather than the former station, but its configuration also provided passengers with clearer directions, connections and paths of travel. In order to provide a sheltered link between the platforms and the Grand Concourse, a small, covered access area was constructed. Various facilities were also established adjacent to the concourse, including a booking hall, waiting rooms, luggage rooms and segregated dining rooms. As these facilities were to be used by passengers arriving on the country lines, they were constructed to a high standard. Exclusive materials, including marble, were used in the design of the interior spaces of the Grand Concourse and its related spaces. The marble was used in the arched entrances to the Grand Concourse and throughout the booking hall. It was locally sourced from the Borenore Quarries, located near Orange.

The first stage of construction of the third Sydney Terminal Building was completed in 1906. At this time, the following related spaces adjacent to the Grand Concourse included:

- the east, west and both northern (tram) arched passages
- the former main dining hall
- the former ladies dining room
- the former booking hall
- the former ladies' room
- the luggage room
- the foyer for the proposed clock tower

<sup>189</sup> Sydney Trains, 1906.

<sup>190</sup> City of Sydney Archives, 1986. ID: 047894 1986



- the cloak room and stairs to the under main concourse level men's toilets.<sup>191</sup>

By the 1920s, the Stationmaster's Office was being used as pay offices and a toilet, and in 1952, the cloak room was converted into the Interstate Booking Office (now known as the NSW TrainLink). Despite other booking offices containing barriers and ticket windows, the Interstate Office was installed with open counters

Today only some of the spaces had maintained their original purpose, with others being modified or adaptively reused to suit other purposes. The spaces that retained their original purpose included:

- the Grand Concourse
- the platform access area
- the clock tower
- the east, west and both northern (tram) arched passages.

At this time the main dining hall was being used as a wine bar and buffet, while the Ladies' Dining Room was used as a cafeteria and fast-food outlet, and the luggage room had been converted into the lost property area and office spaces.

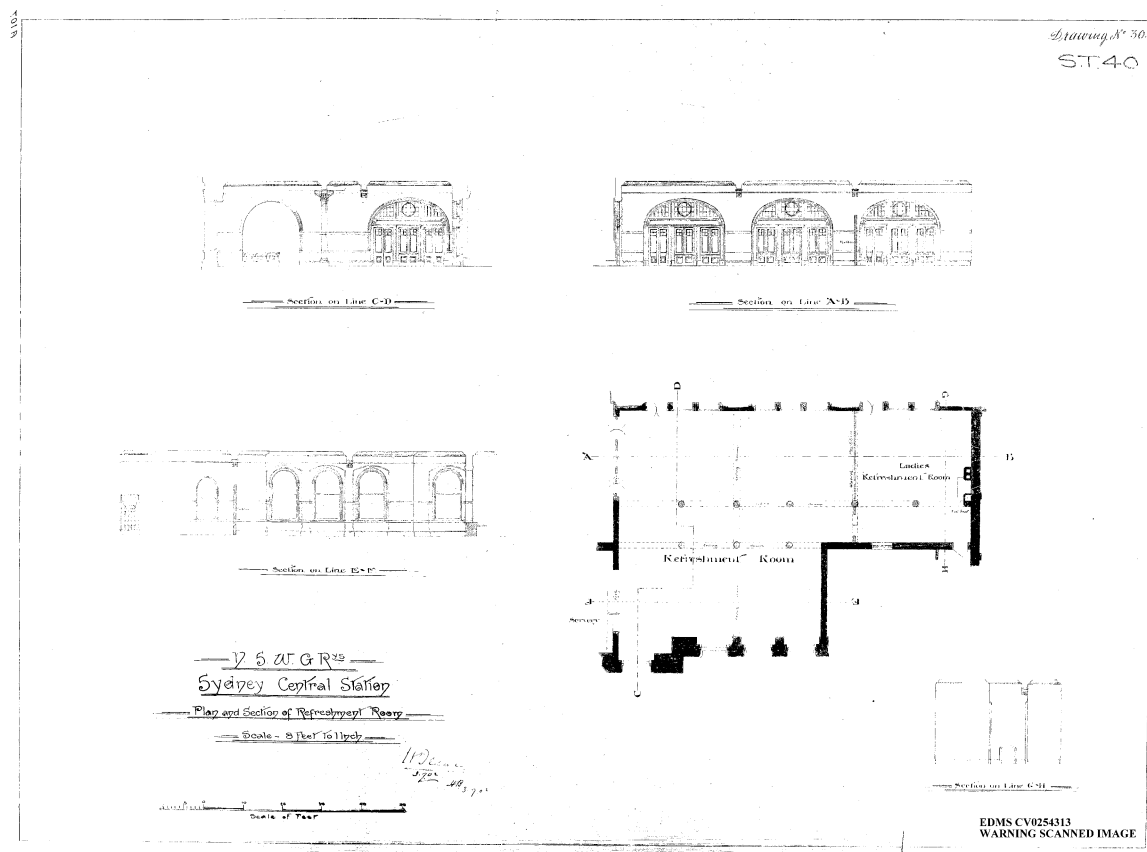


Figure 179: Design for the main dining hall and ladies dining hall, date unknown. <sup>192</sup>

<sup>191</sup> Rappaport Pty Ltd & NSW Government Architects Office, 2013.

<sup>192</sup> Sydney Trains Archives, ID: RRR 0254313\_00C





Figure 180: Main Dining Hall 1906. <sup>193</sup>



Figure 181: The Bar 1906. <sup>194</sup>



Figure 182: People eating in the Dining Hall in 1948. <sup>195</sup>



Figure 183: Dining Hall, date unknown. <sup>196</sup>

### Booking Hall (Office of Rail Heritage)

The Booking Hall was constructed between 1902 and 1906. It was designed by Henry Deane who, at a Sydney University lecture in 1902, noted that the proposed space would be 110 feet long by 54 feet wide by 36 feet high. It was to be a grand Federation era room, that was impressively detailed with marble dado, elaborate console brackets, *oeil-de-boeuf* clerestory windows and a 12-meter double height ceiling. The *oeil-de-boeuf* clerestory windows were located at the upper height of the room, with a stone corbelled gallery located just beneath them. Masonry walls supported the stone gallery and were supported by console brackets and arches. As the room was to be used for ticket booking purposes, a series of ticket booths were installed in the middle of the southern portion of the hall. These were constructed from highly polished timber and were finely crafted.

A new ceiling was added in the Booking Hall in 1937 in order to divide the double height space and add new offices in the area above. The ceiling was added just above the crown of the wall arches and below the level of the stone gallery, significantly reducing the ceiling height of the existing space. To support the new ceiling, columns were added in the Booking Hall. The lowered ceiling also resulted in many of the fine architectural details, such as the original coffered ceiling with moulded cornices and

<sup>193</sup> Sydney Trains Archives, 1906. ID: RRR d1\_10629r

<sup>194</sup> Sydney Trains Archives, State Library of NSW, 1906. ID: D1\_10630R.

<sup>195</sup> Sydney Trains Archives, 1948. ID: 17420\_a014\_a014000098.

<sup>196</sup> Sydney Trains Archives, date unknown. ID: 17420\_a014\_a014000090.

the *oeil-de-boeuf* clerestory windows, being concealed from the Booking Hall. However, these were incorporated into the design of the new offices above.

Extensive modifications to the Booking Hall occurred in the 1960s. This included the removal of the original ticket booths and the addition of a new circular counter, lighting and air conditioning. At this time, timber parquetry flooring and doors to the arched entrances were also added.

Further modifications occurred in 2006, when the Booking Hall was adapted as a base for the Office of Rail Heritage, the RTM ticket sales and ARHS bookshop. At this time, the mid-20th century lowered ceiling was replaced with a new coffered ceiling with deep beams. A contemporary fit out was also undertaken, with the 1960s circular ticket counter removed, and unframed glazed doors replacing the c.1960s doors to the arched entries. Most of the c.1960s parquetry was left in situ.



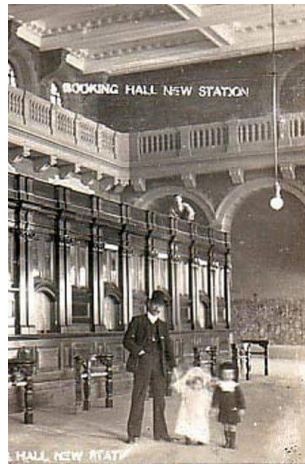
**Figure 184: Two people purchasing tickets in the Booking Hall, date unknown.<sup>197</sup>**



**Figure 185: Booking Hall, date unknown.<sup>198</sup>**



**Figure 186: Booking Hall in 1906.<sup>199</sup>**



**Figure 187: Central Booking Hall, early 1900s.<sup>200</sup>**

<sup>197</sup> Sydney Trains Archives, ID: FB\_IMG\_1501367316470

<sup>198</sup> Sydney Trains Archives, ID: 023474.

<sup>199</sup> Sydney Trains Archives, State Library of NSW. ID: d1\_10628r.

<sup>200</sup> Sydney Trains Archives, 1900s. ID: FB\_IMG\_1529189095858.





**Figure 188: Booking Hall around time of the 1960s modifications.** <sup>201</sup>



**Figure 189: Booking Hall around time of the 1960s modifications with Avis Rental Offices.** <sup>202</sup>

### 3.8.2 Site Description and Condition

#### Grand Concourse and Roof

The Grand Concourse is the primary commuter area of Central Station. It is situated behind the Main Terminus Building, which is located directly adjacent to the north, east and west, whilst adjacent to the south are Platforms 1-15. Being approximately 106 meters (348 feet) long and 21.9 meters (72 feet), the concourse contains an area of approximately 0.3 hectares. Its extensive size contributes to its grandness, as does the high-quality finishes and architectural features that it contains. The Grand Concourse is accessible from the north, east and west via generously proportioned pedestrian arched walkways which are in their original location and configuration.

The most recognisable feature of the Grand Concourse is its wide-span barrel-vaulted roof. This contains exposed painted metal-arch trusses, with metal roof sheeting and a central skylighted roof monitor that surmounts the length of the vault's apex. The truss structure is supported by the southern wall of the Main Terminus Building (at the Grand Concourse's north) and to the south it is supported by slender cast iron columns, above which sits a metal lattice. The skylighted roof monitor is constructed from fiberglass sheeting on its roof and walls (replacing the original glazing), letting in an abundance of natural light. While the roof structure and roof monitor are original, evidence suggests that the size of the skylight has been reduced. Although the original corrugated iron roof sheeting has been replaced, the current metal roof sheeting has a similar profile to the original.

Although the skylighted roof monitor provides natural light, the Grand Concourse also has various artificial lighting systems. The first of these is a series of heritage style pendant lights that are mounted to the ceiling trusses. These are located to either side of the skylight and are placed at regular intervals across the length of the concourse. Similarly, flood lights are mounted at regular intervals to the top of the façade walls, at the intersection with the barrel-vaulted roof. These light the roof-structure and contribute to the general ambiance of the concourse. Further lighting systems include contemporary up-lights that wash the wall and highlight the coffered ceiling of the arched entry passageways.

Another architectural feature is the concrete floor. This is supported by a brick vaulted support system at basement level, which contains wrought iron beams that span between brick piers. Rammed fly

<sup>201</sup> Sydney Trains Archives, post 1960s. ID: 047648.

<sup>202</sup> Sydney Trains Archives, post 1960s. ID: 047651.

ash is packed into the voids between the beams, with the floor constructed above it. Due to this support system, it is described as a brick reinforced floor, rather than a reinforced concrete floor. Above the concrete slab, the floor area of the main concourse, including its related pedestrian entranceways, are paved in terrazzo.

Red brickwork with sandstone detailing is used on the south, east and west elevations of the Main Terminus Building which encloses the Grand Concourse to its north, east and west. These elevations feature similar architectural detailing, each containing a sandstone perimeter cornice to the upper rim and substantial masonry archways. The archways are of rusticated sandstone and are decorated with quoins in a rising sun motif. This motif is more prominent over the pedestrian entry archways that lead to the exterior of the building but is visible on all the archways on the concourse.

While the northern façade contains two pedestrian entrance archways, the east and west facades each contain one. These are in their original configuration and feature painted plasterwork, coffered ceilings and marble wainscoting. The widest archway, which leads to the Western Forecourt and Railway Square, is on the western facade. Leading onto the East Deck is the eastern archway, which is slightly narrower. To the northern façade, the two arched entries connect the Grand Concourse to the porte-cochere. Both archways contain two entrances into the former booking hall, with the north-eastern entry having an additional entrance that leads into the cafeteria. Metal framed fanlights have been added to the interior extent of the eastern and western archways. These contemporary structures are supported by columns and mirror the three-pane form of the original fanlights adjacent.

The interior archways frame the entrances of the interior spaces within the Grand Concourse. These include, from east to west: the station office, a newsagent, the dining hall, a restaurant, male and female bathrooms, the railway bookshop, baggage store, the clocktower foyer and the NSW Train Link office. Each of the arched entrances feature finely crafted cedar doors and joinery, as well as decorated glazing windows.

To the south of the concourse is a widespread, open area. This contains the entry gates to Platforms 1-15, as well as a small platform access area. The entry gates are aligned with a large contemporary glazing partition that runs in an east-west direction between the station office and the Transport Information booth. Beyond this partition is the platform access area. This area is bounded by the partition wall to the north and a series of columns to the south. These columns support an arched, decorative iron lattice work grille that connects to southern end of the barrel-vaulted roof. This grille is original to the construction of the concourse and signifies the boundary between the Grand Concourse and Platform area.

The Grand Concourse contains an array of furniture, food outlets, kiosks, and signage, all which obscure the open space and visual relationships between various original elements. A cluster of contemporary timber bench seating is located to the west of the concourse, adjacent to Platforms 1-3. This is the only public waiting seating available on the concourse, however, cafe seating associated with the food retail shops is also located in the north-east corner. The most obstructive element is the contemporary Transport Information Desk, which is located at the centre of the concourse. This features a reception desk structure which surrounds the location of the original men's toilet block, as well as queue organiser ropes and signage. A large digital timetable board surmounts this structure and due to its scale, much like the original indicator boards has become a defining visual feature of the space. To the west of the Information Desk is a small, pop-up style coffee shop and a set of vending machines. Rubbish bins are scattered around the perimeter of the concourse for use of commuters. Contemporary signage is also restricted to the perimeter and is either hung from the ceiling trusses or mounted onto the internal facades.

Several moveable heritage items are also contained within the concourse area. This includes the early concourse clock, which is mounted from the ceiling trusses at the centre of the concourse, as well as a bronze statue and early wall mounted clock to the northern façade. The bronze statue,



which dates to c.1985, is the bust of John Whitton by Vesco Totev. The two items mounted on the northern façade sit adjacent to the pedestrian archways that lead to the porte-cochère beyond. Restored timber honour boards are mounted on the columns of the arches at the centre of the northern wall.



Figure 190. Entrance into the Grand Concourse from the Western Forecourt.<sup>203</sup>



Figure 191. Historic signage in the Western Forecourt entrance into the Grand Concourse.<sup>204</sup>



Figure 192. Grand Concourse and the northern wall/openings.<sup>205</sup>



Figure 193. Grand Concourse, western wall and entrance portal to the Western Forecourt.<sup>206</sup>

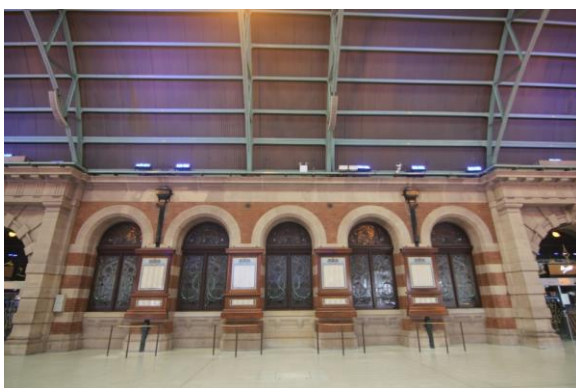


Figure 194. Grand Concourse, northern wall and War Memorials.<sup>207</sup>

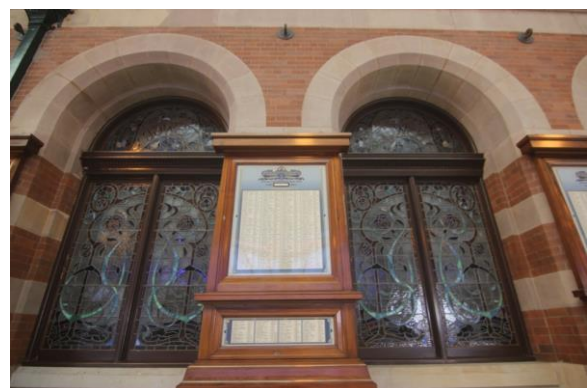


Figure 195. Close up of the War Memorials.<sup>208</sup>

<sup>203</sup> Artefact, 2021

<sup>204</sup> Artefact, 2021

<sup>205</sup> Artefact, 2021

<sup>206</sup> Artefact, 2021

<sup>207</sup> Artefact, 2021

<sup>208</sup> Artefact, 2021



Figure 196. Sandstone entrance to the porte-cochere portal on the northern wall.<sup>209</sup>



Figure 197. Original or early Central clock on the northern wall west of the porte-cochere portal entrance.<sup>210</sup>



Figure 198. Commemorative bust and plaque to John Whitton 'Father of the NSW Railways'.<sup>211</sup>

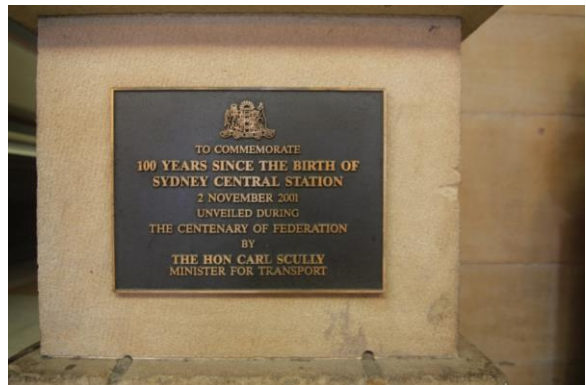


Figure 199. Commemorative plaque for the centenary of Central Station.<sup>212</sup>

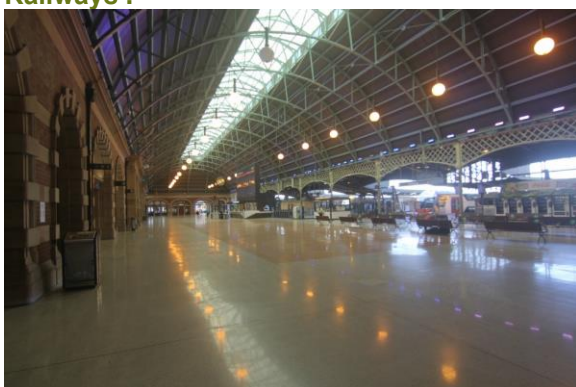


Figure 200. Grand Concourse and barrel roof looking west.<sup>213</sup>

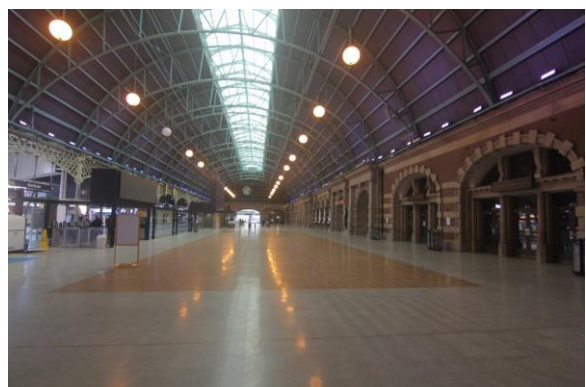


Figure 201. Grand Concourse and barrel roof looking east.<sup>214</sup>

<sup>209</sup> Artefact, 2021

<sup>210</sup> Artefact, 2021

<sup>211</sup> Artefact, 2021

<sup>212</sup> Artefact, 2021

<sup>213</sup> Artefact, 2021

<sup>214</sup> Artefact, 2021

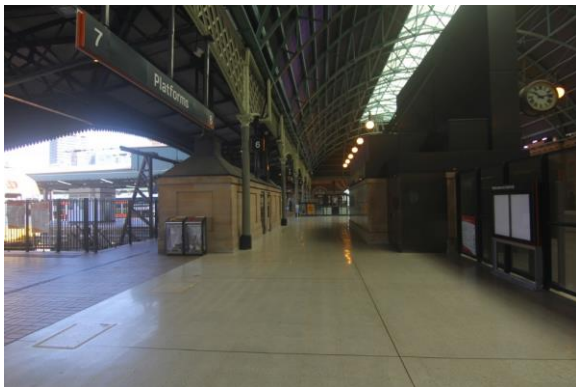




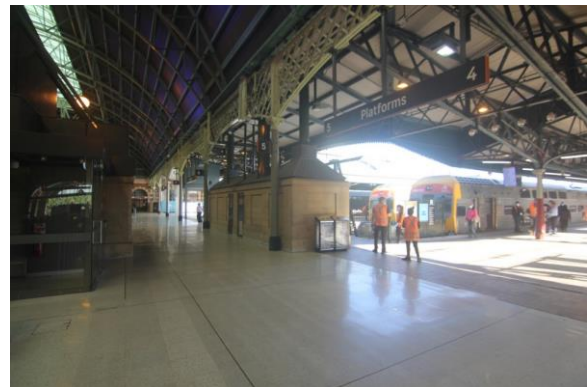
**Figure 202. Grand Concourse barrel roof and skylight, with contemporary indicator board to the south, early clock suspended from roof.<sup>215</sup>**



**Figure 203. Grand Concourse looking south to the Country and Intercity platforms.<sup>216</sup>**



**Figure 204. Grand Concourse roof supported by cast iron columns and cast iron lattice at the intersection with the platform assembly area roof. View looking west includes historic ventilator for the former Men's latrines.<sup>217</sup>**



**Figure 205. Grand Concourse roof supported by cast iron columns and cast iron lattice at the intersection with the platform assembly area roof. View looking east includes historic ventilator for the former Men's latrines.<sup>218</sup>**

## NSW TrainLink

NSW TrainLink is located in the western portion of the Main Terminus Building, to the south of the pedestrian archway that connects the Grand Concourse to the Western Forecourt. A single arched entranceway that opens onto the Grand Concourse is located to the eastern wall. The design of this is typical to those facing onto the Grand Concourse, however, the original timber framed doors have been replaced with contemporary glazed automatic sliding doors. Internally, the space contains a modern fit out, open counters and a circular ceiling detail.

<sup>215</sup> Artefact, 2021

<sup>216</sup> Artefact, 2021

<sup>217</sup> Artefact, 2021

<sup>218</sup> Artefact, 2021



**Figure 206. Commemorative plaque for the passenger train service linking Sydney to Perth, c.1970 outside the TrainLink office.<sup>219</sup>**



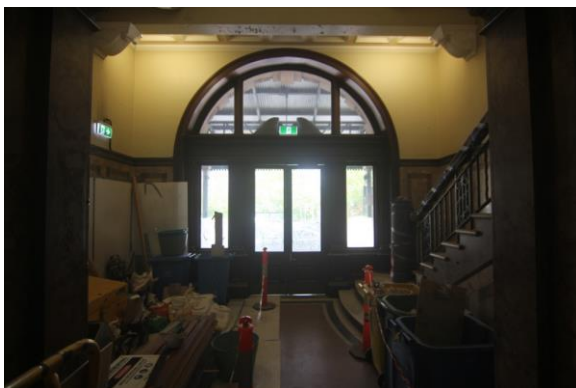
**Figure 207. TrainLink located on platform 1, south of the Grand Concourse.<sup>220</sup>**

### Clocktower foyer

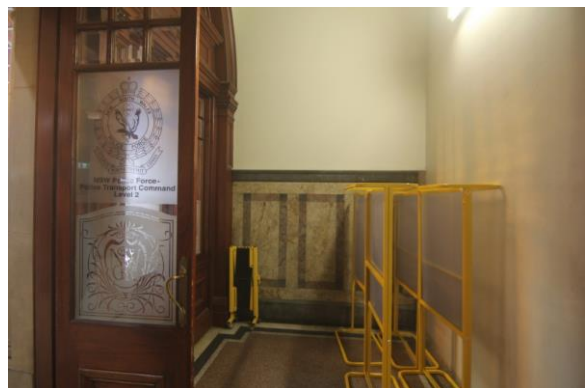
The Clocktower Foyer is contained within the western portion of the Main Terminus Building. To its south is the pedestrian walkway that connects the Main Terminus to the porte-cochere and to the north it is bounded by the Baggage Store.

The eastern wall contains one archway entrance to the Grand Concourse. The design of the archway is consistent with those that face onto the concourse. It contains original western red cedar doors with etched glass, as well as a sandstone quoined archway. Apart from the entryway, the only remaining original fabric is a small portion of marble wall paneling to the southern wall.

This space dates to 1906 and connects to the Clock Tower Lobby and Stair which date from 1915-1921. Today, it operates as a foyer for the late 20th Century access lift and stairs which connect to the offices above and the coach area on Eddy Avenue below.



**Figure 208. Clocktower foyer.<sup>221</sup>**



**Figure 209. Clocktower foyer and Police Command entrance.<sup>222</sup>**

<sup>219</sup> Artefact, 2021

<sup>220</sup> Artefact, 2021

<sup>221</sup> Artefact, 2021

<sup>222</sup> Artefact, 2021



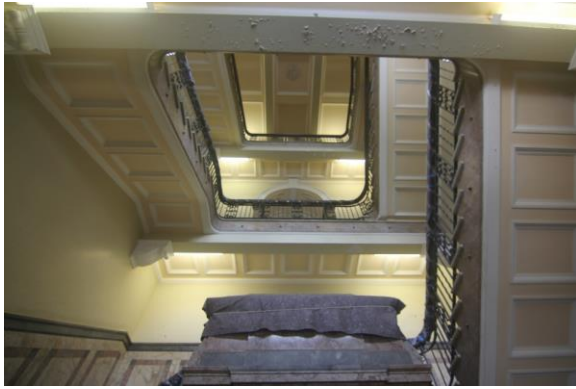


Figure 210. Clocktower stairwell.<sup>223</sup>



Figure 211. Doors from the clocktower foyer to the Grand Concourse.<sup>224</sup>

### Baggage Storage

The Baggage Storage area is located in the north-west portion of the Main Terminus Building. It is bounded by the Clocktower foyer to the south and the Railway Bookshop to the east. The southern wall contains one archway entrance to the Grand Concourse. The design of the archway contemporary with the other archways which face onto the concourse. As such, the sandstone quoined archway contains original western red cedar framed doors with fielded panels and etched glass.

Little original fabric remains in-situ within the interior of the space. It has been largely modified, with contemporary reconfiguration and partitioning. Although the space previously contained the Lost Property area, it currently contains a large conference room that is used by Sydney Trains. A contemporary glazed door has been erected just north of the stone archway entry. The door is inscribed with 'Central Bradfield Room', named after John Bradfield, who played an important role in the development of Central Station. The walls of the space are painted orange and white, the floor is carpeted, and it has a suspended tile ceiling. A mezzanine level has also been added above this space and is used as a staff facility space for Sydney Trains.

### The Railway Bookshop

The Railway Bookshop is located in the north-west portion of the Main Terminus Building. It is bounded by the Baggage Store to the west and the Ablution Areas to the east. The southern wall contains one archway entrance to the Grand Concourse. The design of the archway is a narrower version of the typical design of the arches which face onto the concourse. The sandstone quoined archway contains original western red cedar framed doors with fielded panels and etched glass. The finishes of the space are consistent with the adjacent Baggage Storage.

### Ablution Areas

The Ablution Area is located in the north-west portion of the Main Terminus Building. It is bounded by the Railway Bookshop to the west and the western pedestrian archway which connects the Main Terminus to the tram porte-cochere to the east. The southern wall contains two archway entrances to the Grand Concourse. The western archway leads to the female bathrooms and the eastern leads to the male bathrooms. The design of the archway is contemporary with that the archways which face onto the concourse, and the sandstone quoined archway contains original western red cedar framed doors with fielded panels and acid etched glass.

<sup>223</sup> Artefact, 2021

<sup>224</sup> Artefact, 2021

There is no extant fabric remaining to this space, apart from the location and materials of the archway entry. Internally, both bathrooms have been tiled and fitted out with contemporary toilets and basins, resulting in neither space retaining original fabric.

### **Former Booking Hall (Eternity Bar and Grill)**

The former Booking Hall is located at the centre of the northern portion of the Main Terminus Building. To the north of the hall is the porte-cochere and to the south is the Grand Concourse area. These areas are connected by pedestrian archways, which flank the hall to the east and west. Although the main entry to the hall is from the concourse area, each of these archways contain two entry passages into the space.

The entrances to the space are defined by a sandstone archway that features classical columns surmounted by an arched fanlight window. While the entry points are original, they contain glazed sliding doors and highlight windows that are contemporary. The north and south facades each contain five glazed fenestrations. Each fenestration is comprised of a large, timber-framed rounded head window that features original coloured leadlight glass with floral motifs, featuring the NSW state flower the Waratah in the Arts and Crafts style. The fanlights of the windows also feature the floral motif, as well as an inlay of the initials NSWGR.

Although the Booking Hall was originally double-heighted, an additional floor had been added into the space. The current plaster ceiling dates to 2006 and features deep recessed panels that highlight the supporting beams of the office level above. However, original features remain visible to the perimeter wall of the interior. These include elaborate sandstone console brackets and arches to all walls. Although the original purpose of these elements was to shore up and support the original stone perimeter walls, they also continue to contribute to the grandness of the space. The corners of the room feature triple console brackets, while the wall archways are eloquently carved and well defined. Consistent with the wall arches, the window and door archways are also carved from sandstone. The wall fabric is original, featuring marble wainscoting that is mounted on a sandstone plinth and finished with a sandstone dado edge.

In 2013, it was recorded that the Booking Hall featured pendant lights and a mid-20<sup>th</sup> Century timber parquet floor. However, the space was refurbished and fitted out for the Eternity Bar and Grill in 2018, and included the “installation of finishing food preparation area, back-of-house, bar, point-of sale area, reflooring, construction of new serving counters, retention of original fabric, air conditioning plant & equipment, lighting & other associated work”.<sup>225</sup> As such, the space now contains a bar and ample seating. However, the retention of the original features has allowed it to maintain its character.

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<sup>225</sup> <https://www.corelogic.com.au/cordell/7154015>



Figure 212. Eternity Bar and Grill within the former Booking Hall.<sup>226</sup>



Figure 213. Within the former Booking Hall looking south to the glazed entrance into the Grand Concourse.<sup>227</sup>

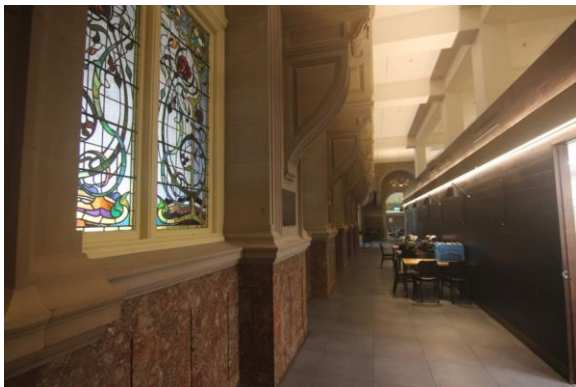


Figure 214. Decorative lead glass windows, marble and sandstone to the north wall of the space.<sup>228</sup>

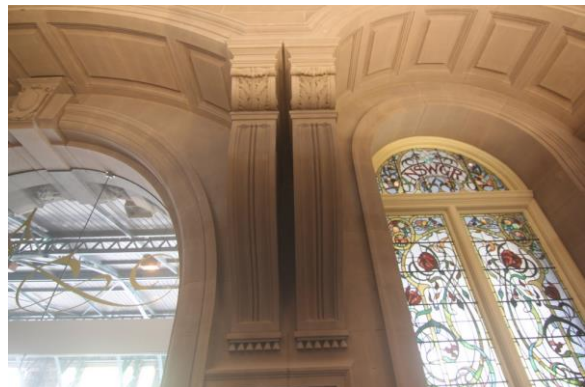


Figure 215. Large arched windows and door openings with sandstone console brackets and decorative leadlight windows on the north wall.<sup>229</sup>

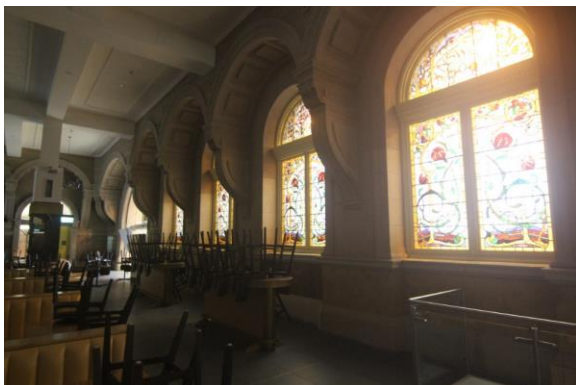


Figure 216. Large arched windows and door openings with sandstone console brackets and decorative leadlight windows on the north wall.<sup>230</sup>



Figure 217. Decorative blind sandstone arches and console brackets continue the pattern from the north wall onto the western wall.<sup>231</sup>

<sup>226</sup> Artefact, 2021

<sup>227</sup> Artefact, 2021

<sup>228</sup> Artefact, 2021

<sup>229</sup> Artefact, 2021

<sup>230</sup> Artefact, 2021

<sup>231</sup> Artefact, 2021





**Figure 218. Non-original coffered ceiling divides the original Booking Hall volume.<sup>232</sup>**



**Figure 219. Commemorative plaque for the new Booking Hall Lounge, c.1964.<sup>233</sup>**

### Former Ladies Dining Room (Central Food Hall)

The former Ladies Dining Room is situated to the west of the former Dining Hall, to the northern portion of the Main Terminus Building. To the west, it is bounded by a pedestrian archway connection between the Grand Concourse and the tram Porte-Cochere beyond. The western wall of the Dining Hall contains an opening into this tunnel, and its southern wall contains two entrances onto the Grand Concourse area. Although the form of the Grand Concourse entrances appears consistent with others leading to the Grand Concourse, the transom windows contain a single pane rather than six panes of glass. This suggests that it is unlikely that the doors and windows are original. Similarly, the entrance onto the pedestrian archway contains a mid 20<sup>th</sup> Century timber framed glazed door.

In 1955, the space underwent extensive modifications. It was at this time that the map of Australia terrazzo floor insert was laid, and the carved plaster bulkhead mural panels were installed. The floor design was created by the Melocco Brothers and comprises the terrazzo motifs and the surrounding marble floor, which contains some green Mudgee marble. The mural wall panels, titled the *Transport Progress in Frieze* were designed by Italian artists Pietro Melocco and Guido Zuliani and depict the evolution of railway transport in Australia from 1788 to the 1950s. Covering a wall span of 132 square meters, the murals were crafted using plaster in a technique called scagliola which polishes plaster to appear like marble.<sup>234</sup> The Melocco brothers were renowned for their craftsmanship and use of marble and the scagliola technique, as can be found in other key public and commercial buildings including the crypt at St Mary's Cathedral, Tasman map on the floor of the entrance of the Mitchell Library and the scagliola work in the banking chamber of 48-50 Martin Place (the former Commonwealth Bank Building).<sup>235</sup> Other modifications include the insertion of a plain, lowered ceiling that contains recessed lights and a plain cornice.

In 2015 a fire broke out in the fast food restaurant kitchen which was housed in the space, resulting in its closure. Although the mural wall panels suffered some smoke and water damage from the fire, the area did not suffer extensive damage.<sup>236</sup> The cafeteria space, along with other areas of the Grand Concourse, were refurbished by the Delaware North in 2018.<sup>237</sup> It is now the 'Central Food Market' which contains contemporary take-away food shops, as well as a back of house kitchen area.

<sup>232</sup> Artefact, 2021

<sup>233</sup> Artefact, 2021

<sup>234</sup> <https://www.smh.com.au/national/nsw/oh-my-god-we-ve-lost-central-how-a-fire-brought-a-hidden-artwork-back-to-life-20190502-p51jch.html>

<sup>235</sup> <https://www.abc.net.au/news/2017-11-24/sydneys-central-station-peter-melocco-mural-restored/9181486>

<sup>236</sup> <https://www.smh.com.au/national/nsw/oh-my-god-we-ve-lost-central-how-a-fire-brought-a-hidden-artwork-back-to-life-20190502-p51jch.html>

<sup>237</sup> <https://www.hospitalitydirectory.com.au/industry-news/15009-delaware-north-opens-new-dining-venues-at-sydney-s-central-station/>





Figure 220. Former Ladies Dining Room.<sup>238</sup>



Figure 221. Decorative coffered plaster ceiling supported by columns.<sup>239</sup>



Figure 222. Melocco Brothers Mural and inlay floor.<sup>240</sup>



Figure 223. Melocco Brothers inlay floor.<sup>241</sup>



Figure 224. Melocco Brothers signage within the inlay floor.<sup>242</sup>



Figure 225. Detail of the Melocco Brothers mural.<sup>243</sup>

<sup>238</sup> Artefact, 2021

<sup>239</sup> Artefact, 2021

<sup>240</sup> Artefact, 2021

<sup>241</sup> Artefact, 2021

<sup>242</sup> Artefact, 2021

<sup>243</sup> Artefact, 2021



Figure 226. Decorative plaster ceiling.<sup>244</sup>



Figure 227. Former Ladies Dining Room.<sup>245</sup>

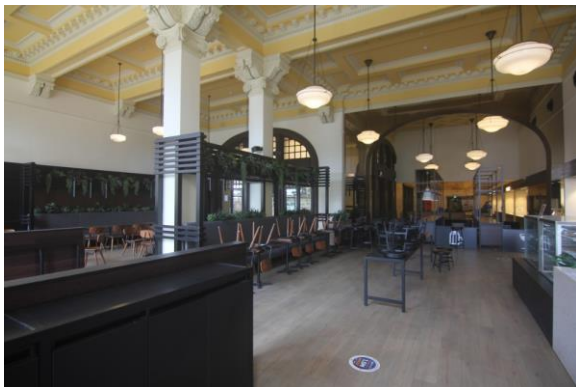


Figure 228. Former Ladies Dining Room.<sup>246</sup>



Figure 229. Melocco Brothers inlay floor and mural.<sup>247</sup>

### Former Dining Hall (2000 Acres)

The former Dining Hall is located in the north-eastern corner of the Main Concourse Building. It is bounded by adjacent tenancies to the south and west and the East Deck to the north and east. It is accessed through entryways in the northern and eastern internal walls of the Grand Concourse and has a connection to the East Deck through a doorway to the eastern façade of the Main Terminus Building.

The three entries into the former Dining Hall are original. The two facing onto the Grand Concourse each feature timber framed glazed doors that are surmounted by a large, semi-circular fanlight. While the entry to the northern façade contains three doors, and to the eastern façade there are two outer doors with a central window structure in the same design. Unlike the doors to the northern façade, the glazing to the eastern entry is acid etched. The doors contain the NSWGR logo and the windows feature a floral pattern. The fanlight has three panels, each with brass glazing bars. The central section of the fanlight contains a symbol of a square within a circle. This motif is present in the Eddy Avenue Arcade shop fronts, as well as throughout the Grand Concourse. The third entry, which opens onto the East Deck, is timber framed and contains a single double-door with a round head opening. It is possible that this is an original window opening that was modified into a door opening during the 20<sup>th</sup> century.

Several areas of the Grand Concourse, including the former Dining Hall were refurbished by Delaware North in 2018.<sup>248</sup> The space is now used as the 2000 Acres café and as such, contains an

<sup>244</sup> Artefact, 2021

<sup>245</sup> Artefact, 2021

<sup>246</sup> Artefact, 2021

<sup>247</sup> Artefact, 2021

<sup>248</sup> <https://www.hospitalitydirectory.com.au/industry-news/15009-delaware-north-opens-new-dining-venues-at-sydney-s-central-station/>

array of contemporary tables and seating, as well as a service area with a back of house kitchen. The walls and ceiling were repainted at this time and a mirrored finish was added to the internal archway.

The former Dining Hall retains several original features including Iconic capitals which decorate the squared columns that support the ceiling beams, and the original height of the space has been retained, highlighting the grand panelled ceiling. The ceiling, which has been painted yellow, contains deep beams with console brackets and decorative moulding. Although the pendant lights are not original, their 'heritage-style' aesthetic is sympathetic to the character of the space.



**Figure 230. Former Dining Hall entrance from the Grand Concourse.**<sup>249</sup>

### **Newslink**

Newslink is located in the eastern portion of the Main Terminus Building. It is bounded by the former Dining Hall to the north and the pedestrian tunnel that links the Grand Concourse to the East Deck to the south. The western wall contains one archway entrance to the Grand Concourse. The design of the archway is consistent with the other archways that face onto the concourse, and include sandstone quoining and original western red cedar framed doors with fielded panels and etched glass. The interior of the space has a contemporary fit-out that is appropriate to its use as a newsagent. It contains a suspended tile ceiling, a sale counter and display stands, as well as a false timber board floor.

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<sup>249</sup> Artefact, 2021





Figure 231. Newslink, looking east.<sup>250</sup>



Figure 232. Newslink, looking west.<sup>251</sup>

### Stationmaster's Office

The Stationmaster's Office is located in the eastern portion of the Main Terminus Building, to the south of the pedestrian archway that connects the Grand Concourse to the East Deck. The only publicly accessible room of the offices is the foyer, which contains a single entranceway that opens onto the Grand Concourse. This is located on the western wall of the foyer and is a narrower version of the other archway design which opens onto the Grand Concourse. It contains an original round head fenestration with decorative metalwork to the fanlights.

Prior to being used as the Stationmaster's Office, this space was used as a pay office and ticket collection area. At the time of this transformation, most of the original features were removed or modified. These include skirtings, architraves, linings, ceilings and cornices. The foyer area is a small room containing a reception desk and waiting area. Beyond the foyer, the space has a contemporary office fit out. It also contains a mezzanine level that dates to the late 20<sup>th</sup> century.

### 3.8.3 Statement of Significance

The Grand Concourse and its related spaces form the central core of the Main Terminus Building and are landmark spaces in the overall Sydney Terminal precinct and are of state heritage significance. These spaces have a high degree of integrity and have historic, aesthetic, social and technical significance.

The Grand Concourse's design is evidence of the strategic planning undertaken by Henry Deane to provide a large relatively open assembly space, which prioritised the efficient and safe movement of passengers, with transportation of baggage, parcels and mail removed to the basement tunnels. This planning reflects the design of the exterior approaches to the building, which separates vehicles and entry/exit for the efficient movement of passengers, trams, and vehicles to and from the Station.

The Grand Concourse roof is an element of both aesthetic and technological significance, as one of the largest spanning steel truss vaulted structures of the era, constructed to cover the expanse of the Concourse. The height and volume of the space, the exposed truss work and roof monitor which bathes the Grand Concourse in daylight adds to the aesthetic grandness of this space. The roof retains a high level of integrity.

The perimeter walls of the Grand Concourse retain a high level of integrity and feature high quality material and detailing and contribute to the overall aesthetic significance and grandeur of the main passenger area. This area features sandstone quoined arches in a sunrise motif, with original red

<sup>250</sup> Artefact, 2021

<sup>251</sup> Artefact, 2021



western cedar doors and glazing. There is also original leadlight glazing on the northern wall. The space also features original or early features such as the clock hanging from the truss work and the timber honour boards on the northern wall. These moveable heritage items are significant to the overall understanding of the space.

The spaces which surround the Grand Concourse, housed within the Main Terminus Building are exceptionally significant for their use of high-quality materials and their architectural designs. They feature marble panelling, decorative coffered ceilings, elaborate sandstone arches and console brackets, original timber window and door joinery including leadlight glass windows, and terrazzo floors. The former Ladies Dining Room also features a terrazzo and marble floor with the map of Australia, and a large decorative scagliola mural designed and crafted by the renowned Melocco Brothers, which was a finalist in the 1951 Sulman Prize.

The Grand Concourse and its related spaces are historically and socially significant for their constant use as a passenger thoroughfare, meeting/waiting areas, and amenities in relation to the use of the area for railway transportation. They are a rare example of an early 20th century covered major transport assembly platform in Australia and is an outstanding example of a covered public heritage space in Sydney, complete with period associated spaces such as booking halls and dining rooms.

### 3.8.4 Gradings of Significance and Integrity

**Table 21: Gradings of Significance and Integrity for the Main Terminus Building: Grand Concourse and Related Spaces**

Element	Grading	Integrity
<b>MAIN CONCOURSE</b>		
<b>Upper Concourse and related spaces Overall</b>	Exceptional	High
<b>Main Concourse and Roof Space</b>	Exceptional	High
<b>Information counter and timetable display</b>	Little	N/A
<b>Leadlight Windows</b>	Exceptional	High
<b>Terrazzo Flooring</b>	Moderate	Moderate
<b>Surrounding brick and sandstone Façade Walls</b>	Exceptional	High
<b>Barrel Vaulted Roof including Trusswork and Lanterns</b>	Exceptional	High
<b>Arched Passage Entries</b>	Exceptional	High
<b>Arched Entries to Related Spaces</b>	Exceptional	High
<b>East Archway Stair Connection to Central Electric Station</b>	High	Moderate Original space, non-original materials to stairs
<b>Recent Vending machines, ATMs and furniture</b>	Little	N/A
<b>Recent Advertisement, Signage and Wayfinding</b>	Little	N/A

Element	Grading	Integrity
Recent Retail Furniture and Paraphernalia	Little	N/A
Introduced Services; Mechanical, Electrical, Lighting and Data	Little	N/A
Suspended Clock	Exceptional	High
Wall Mounted Clock	High	High
Whitton Bust	Moderate	High
Timber honour boards	High	High
Colorbond Zincalume Steel Roof Cladding	Little	N/A
<b>FORMER BOOKING HALL (ETERNITY BAR AND GRILL)</b>		
Sandstone Console Brackets	Exceptional	High
Leadlight Windows	Exceptional	High
Columns	Moderate	Low Original capitals removed or bisected by later ceilings. Integrity of original panelling and finishes beneath accretions unknown
Doors, Windows and Hardware	Little/Exceptional	Moderate Doors are contemporary glazed sliding doors  High Windows original timber framed leadlights
Floors and Paving	Little	N/A
Ceilings and Cornices	Little	N/A
Marble Wall Paneling and Skirtings	Exceptional	High
Architraves and Linings	Exceptional	High
Recent Applied Finishes	Little	N/A
Recent Glazed Panels and Fitout	Little	N/A
Introduced Services; Mechanical, Electrical, Lighting and Data	Little	N/A
<b>FORMER DINING HALL (2000 ACRES)</b>		
Acid Etched Glass Windows	Exceptional	High
Doors, Windows and Hardware	Exceptional	High
Recent Floors and Paving	Little	N/A
Console Brackets, Columns, Ceilings and Cornices	Exceptional	High

Element	Grading	Integrity
Skirtings, Architraves and Linings	Exceptional	High
Recent Applied Finishes	Little	N/A
Recent Fitout	Little	N/A
Recent Signage	Little	N/a
Introduced Services; Mechanical, Electrical, Lighting and Data	Little	N/A
<b>FORMER LADIES DINING ROOM (CENTRAL FOOD HALL)</b>		
Doors, Windows and Hardware	Exceptional	High
Floor Map, Symbolic Motifs and Paving	High	High
Ceilings and Cornices	Moderate	High
Skirtings, Architraves and Linings	Exceptional	High
<i>Transport Progress in Frieze Mural</i>	Exceptional	High
Timber and Marble Panelling	Exceptional	High
Recent Applied Finishes	Little	N/A
Recent Fitout	Little	N/A
Introduced Services; Mechanical, Electrical, Lighting and Data	Little	N/A
<b>STATION MASTER'S OFFICE</b>		
Doors, Windows and Hardware	Exceptional	High
Recent Floors and Paving	Little	N/A
Recent Ceilings and Cornices	Little	N/A
Skirtings, Architraves and Linings	Moderate	Moderate
Staircases and Mezzanines	Moderate	Moderate
Lower Offices and Function Rooms	Little	N/A
Recent Applied Finishes	Little	N/A
Recent Fitout	Little	N/A
Introduced Services; Mechanical, Electrical, Lighting and Data	Little	N/A
"NSWGR" Clock by Seth Thomas	Moderate	High
<b>FOYER (CLOCKTOWER)</b>		
Acid Etched Glazing	Exceptional	High
Doors, Windows and Hardware	Exceptional	High
Remnant Marble Panelling	High	High

Element	Grading	Integrity
Recent Flooring	Little	N/A
Ceilings and Cornices	Moderate	Moderate
Recent Applied Finishes	Little	N/A
Introduced Services; Mechanical, Electrical, Lighting and Data	Little	N/A
<b>BAGGAGE STORAGE</b>		
Acid Etched Glazing	Exceptional	High
Current Configuration	Little	Moderate
Doors, Windows and Hardware	Exceptional	High
Recent Floors and Paving	Little	N/A
Recent Ceilings and Cornices	Little	N/A
Skirtings, Architraves and Linings	Moderate	Moderate
Introduced Services; Mechanical, Electrical, Lighting and Data	Little	N/A
<b>ABLUTION AREAS</b>		
Doors, Windows and Hardware	Exceptional	High
Floors and Paving	Little	N/A
Ceilings and Cornices	Little	N/A
Skirtings, Architraves and Linings	High	High
Recent Applied Finishes	Little	N/A
Recent Fitout	Little	N/A
Introduced Services; Mechanical, Electrical, Lighting and Data	Little	N/A
<b>NEWSLINK</b>		
Doors, Windows and Hardware	Exceptional	High
Floors and Paving	Little	N/A
Ceilings and Cornices	Little	N/A
Skirting, Architraves and Linings	High	N/A
Recent Applied Finishes	Little	N/A
Recent Fitout	Little	N/A
Introduced Services; Mechanical, Electrical, Lighting and Data	Little	N/A



### 3.8.5 Site Specific Conservation Policies

**Table 22: Site Specific Conservation Policies for the for the Main Terminus Building: Grand Concourse and Related Spaces**

Policy	Policy Discussion and Guidelines
1. Follow Overall Conservation Policies	See Section 15 of this CMP for Conservation Policies
2. Implement Fabric Guidelines	See Section 15.4.4 of this CMP.
3. Item Specific Policy: Heritage Process	<p>When considering change follow the Heritage Flow Charts that form Appendix C of this CMP:</p> <ul style="list-style-type: none"> <li>A) Major Works Heritage Process Flow Chart; or</li> <li>B) Minor Works Heritage Process Flow Chart.</li> </ul>
4. Item Specific Policy: Heritage Listing	<p>See Section 3 in this CMP for Legislative Context, and Section 14.4 for Statutory Requirements for New Development. Ensure appropriate statutory approvals or exemptions are obtained prior to change on the site because:</p> <ul style="list-style-type: none"> <li>• The site is within the overall listing for <b>Central Station</b> on the State Heritage Register under the <i>NSW Heritage Act 1977</i> for Sydney Terminal and Central Railway Stations Group.</li> <li>• The site is within the overall listing of the <b>Central Railway Station and Sydney Terminal Group</b> on Transport Asset Holding Entity's (TAHE) Heritage and Conservation Register under Section 170 of the <i>NSW Heritage Act 1977</i>.</li> <li>• The area is within the Railway Square/Central Station Special Character Area under the Sydney DCP 2012.</li> </ul>
5. Item Specific Policy: Masonry Conservation	<p>Conservation of the sandstone and brick facades should be undertaken as part of the cyclical maintenance of the building. Particular care and inspection should be undertaken for any projecting or decorative elements (such as parapets, dentils and mouldings) which has a higher risk of public injury. These should be regularly inspected to make sure the risk of falling is minimised and mitigated.</p> <p>A stone and brick strategy for the facades should be prepared to guide the conservation of the masonry.</p> <p>A strategy for the protection of the masonry facades during any major works should be undertaken by heritage architectural specialists and qualified stonemasons with heritage experience prior to the commencement of any works.</p>

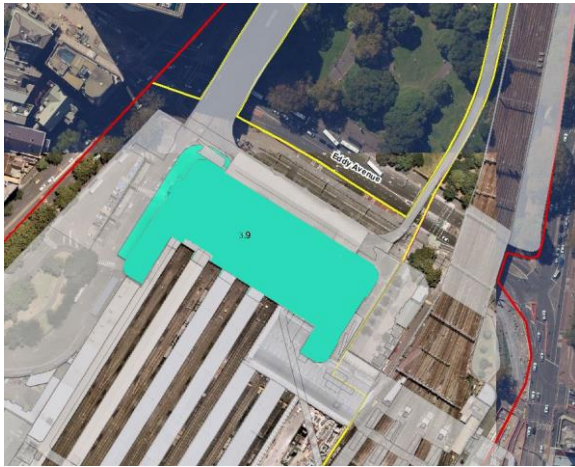
Policy	Policy Discussion and Guidelines
6. Item Specific Policy: Fabric	<p>Specific elements for conservation include:</p> <ul style="list-style-type: none"> <li>• Grand Concourse volume and spatial qualities</li> <li>• Sandstone quoined arches in sunrise motif</li> <li>• Sandstone skirting, dado panelling and cornice</li> <li>• Sandstone arches and columns</li> <li>• Original timber window and door joinery</li> <li>• Acid etched glazing</li> <li>• Leadlight glazing</li> <li>• Metal barrel vaulted roof truss work, support columns and lattices</li> <li>• Lanterns</li> <li>• Suspended clock</li> <li>• Sandstone console brackets</li> <li>• Marble and terrazzo finishes</li> <li>• Scagliola mural</li> </ul>
7. Item Specific Policy: Use	<p>Continue to use the area as the Grand Concourse for inter- city and country trains at Central Station. Ensure uninterrupted passage from porte-cochere to platforms through the Grand Concourse is maintained including use of the existing wide linking corridors.</p> <p>Encourage the use of related grand spaces for their original intended use, e.g. booking halls and passenger amenities such as bar and restaurant facilities.</p> <p>Retain the spatial relationship between the Grand Concourse, platforms and porte-cochere.</p>
8. Item Specific Policy: Reconstruction of lost elements	<p>Consider the re-construction (or at least a sympathetic interpretation) of the following:</p> <ul style="list-style-type: none"> <li>• lighting (particularly in the former bar and refreshment areas)</li> <li>• original colour schemes</li> <li>• original signage</li> <li>• glazing</li> <li>• original central glazed roof area</li> <li>• skylights towards the Country and Interstate platform area</li> </ul>
9. Item Specific Policy: Development	<p>The Grand Concourse and related spaces form the central core of the Main Terminus Building and are largely intact and should be conserved as required in relation to their significance and integrity.</p> <p>Prior to the commencement of any proposed development or works, photographic archival records of the precinct should be undertaken.</p> <p>Ensure the original external colour schemes and finishes are documented and archivally recorded.</p> <p>Future development should enhance the building and spaces' historic, aesthetic and social significance through considerate design strategies which avoid negative impacts to the significant fabric and spaces.</p>

Policy	Policy Discussion and Guidelines
10. Item Specific Policy: Alterations and Additions	<p>Alterations and additions to the Grand Concourse and its related spaces should be avoided. Do not paint the sandstone or brick facades.</p> <p>Additional kiosks in the Grand Concourse area should not be constructed, to minimise visual clutter and encroachment on the intended open space.</p> <p>Changes to the interior fit-out of the shops is permitted, but should not impact original features such as double heighted space, barrel vaulted ceilings and internal inter-tenancy walls.</p> <p>An interior fit-out guide for the shop tenancies should be prepared to help guide future tenancy fit-outs and to provide a level of consistency across the stores in their treatment and protection of the heritage fabric and spaces.</p> <p>Alterations and additions within spaces should protect the significant fabric, be temporary and readily reversible.</p> <p>Changes to colours and finishes of window/door joinery should be sympathetic to the heritage fabric and consider where possible the implementation of original or early schemes. If changes to the external colour occur, this should be implemented consistently across all facades of the Main Terminus Building.</p>
11. Item Specific Policy: Intrusive fabric.	Demolition of, or changes to, the existing intrusive building services should be allowed without formal approval provided no significant fabric is impacted on by the change.
12. Item Specific Policy: New Colours & Finishes	Ensure a colour and finishes strategy is prepared for the Main Concourse and Related Areas sympathetic to the heritage of the place (including consideration of original or early schemes) and that it is implemented consistently across the Main Concourse and Related Areas.
13. Item Specific Policy: Modern fittings	<p>Do not clutter the Main Concourse with new elements. Carefully design new elements so that they are either concealed, or present as a coordinated collection of contemporary minimalist elements and services.</p> <p>Base the design of contemporary fittings such as lighting and seating on the original traditional prototypes.</p>
14. Item Specific Policy: Short term uses and changes.	Do not allow negative impacts on significant fabric, spatial qualities and setting for short term gain. E.g. commercial or advertising signage.
15. Item Specific Policy: Management of Signage	<p>Ensure all tenants of the shop spaces (including any Responsible government Agency use of the spaces) follow a Commercial Signage Manual for the overall Central Station site.</p> <p>Ensure all other signage is also managed under an overarching Signage Manual for the site.</p>
16. Item Specific Policy: Lighting	Lighting within the Grand Concourse and its related spaces should be consistent with the guidelines outlined in the <i>Central Station Lighting Strategy</i> .

Policy	Policy Discussion and Guidelines
17. Item Specific Policy: Interpretation	<p>See Sections 9 and 14.8 'Heritage Interpretation' in this CMP.</p> <p>Prepare a heritage interpretation strategy, based on the precinct-wide Central Precinct Heritage Interpretation Strategy, specific to the Main Terminus Building interior spaces as part of Central Precinct. Particular focus should be given to the former landscape and cultivation of the space.</p> <p>Following this, develop and implement a detailed site-specific heritage interpretation plan, aligned with the themes and recommendations of the heritage interpretation strategy.</p>
18. Item Specific Policy: Vistas	<p>Ensure vistas through the Grand Concourse are retained and not obscured. Maintain vistas from each entrance archway and through east-west and north-south axes of the Concourse.</p> <p>Significant internal views between the Grand Concourse and the Country and Interstate platforms are to be retained.</p>



## 3.9 MAIN TERMINUS BUILDING: OFFICE SPACES



**Figure 233. Main Terminus Building: Office Spaces.**<sup>252</sup>



**Figure 234. Level 1 – north office.**<sup>253</sup>

### 3.9.1 History of the Main Terminus Building: Office Spaces

In 1902, when Henry Deane described the plan for the new Sydney Terminus, he explained that the upper floors of the building were to be used by the Railway Commissioners and their officers. The area was to be connected by stairs and a lift to the Dining Room and Reading Room facilities on the Grand Concourse (formerly the assembly platform) for the convenience of the staff, many of whom would be travelling into the city from outside the city centre.

The foundation stone was laid on 20 April 1902, however the third Sydney Station was only completed up to the Grand Concourse level by August 1906. As changes in management and funding constraints impacted the construction of the Station, the planned office levels and clock tower were left to be constructed at a later stage. As a result of budget constraints, the design for the upper levels and clock tower were simplified. The construction of these elements did not commence until 1916 and were completed in 1921. It is also thought that there was an anticipated additional stage of works, as the northern façade bay to the office levels includes flanking parapets which were constructed to allow the later addition of two cupolas. These cupolas however were never constructed.

Similarly, the proposed grand staircase that was to connect the Grand Concourse to the office levels was not completed until the second phase of construction. Although provisions were made for it to be built during the first phase of construction, this was delayed until the Upper Office levels were completed.

The Booking Hall was designed with a double height space, however its ceiling was lowered in 1937, allowing the reconfiguration of the office floor above. As the new floor required support, columns were added in the booking hall. The upper height of the booking hall had decorative features, such as oeil-de-boeuf clerestory windows. Rather than being removed, these were incorporated into the design of the offices.

Since the turn of the 21<sup>st</sup> century, the upper office levels have received various modifications. The third level office space was originally used for railway administration; however, it became vacant in 1996. It remained vacant until 2006, when it was converted into the main control room of Central Station. At this time, the level one and two offices spaces also received a contemporary fit-out.

<sup>252</sup> Artefact, 2021

<sup>253</sup> Artefact, 2021

Modifications included the installation of air-conditioning and the double-glazing of all windows. In 2011, the staircase was altered to meet the standards of contemporary fire regulations. Other works were also undertaken at this time, including the repainting of the stair area and the rebuffing of the marble areas and handrail.

### 3.9.2 Site Description and Condition

#### Grand Internal Staircase

Located within the northern portion of the Main Terminus Building is the Grand Internal Staircase. This staircase connects all four levels of the Terminus Building, from the basement level to the second storey. Although it has an entrance point at each level, the main entrance is to the northern façade of the Main Terminus Building at concourse level. From the basement level, the staircase is accessed via the eastern entry tunnel to the Eddy Avenue Arcade. At the upper levels, it is connected via large foyer areas. The form of staircase is similar to the stair in the Clock Tower, with stair fliers that follow the surrounding walls to create a central well.

Despite having the same construction, the materiality of the stair varies between levels. The stair fliers, up until the second-floor landing, are constructed from concrete. They are supported on concrete encased steel beams, with the landings having plastered concrete console brackets. Due to the utilitarian nature of the basement, the portion between basement level and concourse level is untiled, exposing the concrete structure. In this portion, the walls are also undecorated. From concourse level and above, the staircase is intricately detailed. Red terrazzo tiles with a border of white marble tiles features at each landing and half space landing, and the walls are decorated with a red marble wall panelling that has a black marble dado trim. The entire soffit of the staircase is coffered.

From basement to the second-floor level, the stair is decorated with an ornate wrought iron balustrade. This features a cedar handrail and is an original component of the staircase. It is supported by two dark marble newel posts to concourse level and one dark newel post to basement level. On both levels, these posts sit on the foot of the curtail steps.

Alterations to the surrounding rooms have resulted in some modifications to the staircase. On the eastern wall of each floor, portions of the wall panelling and dado have been removed to allow for the construction of a small access door to the adjacent lift lobbies. While the first and second floors each originally contained two archways, which provided access to the central northern corridor of the Main Terminus Building, however the archways on the second floor have been infilled.

Extensive refurbishment works to the staircase were undertaken in 2011. At this time, the soffit to the underside of the staircase was repainted pink, white and grey and lighting which replicates the original fittings were installed to the walls across all levels. The lift access doors to the eastern walls were also replaced with red fire doors for code compliance.



Figure 235. Grand internal staircase at level 2.<sup>254</sup>



Figure 236. View up through stair void.<sup>255</sup>



Figure 237. Grand internal staircase.<sup>256</sup>

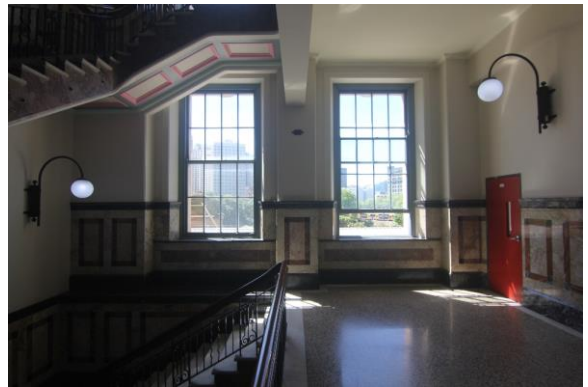


Figure 238. Grand internal staircase lobby.<sup>257</sup>



Figure 239. Entry to grand internal staircase from light rail.<sup>258</sup>

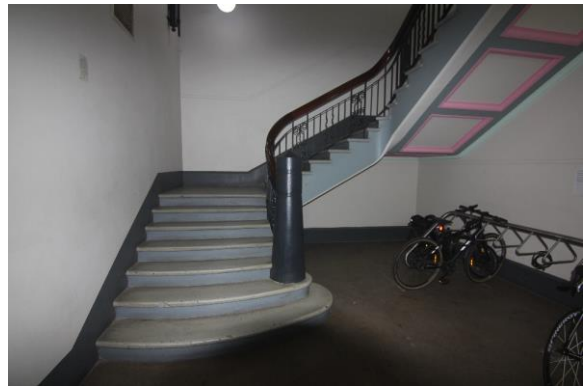


Figure 240. Staircase at basement level.<sup>259</sup>

<sup>254</sup> Artefact, 2021

<sup>255</sup> Artefact, 2021

<sup>256</sup> Artefact, 2021

<sup>257</sup> Artefact, 2021

<sup>258</sup> Artefact, 2021

<sup>259</sup> Artefact, 2021





Figure 241. Staircase at Level 2.<sup>260</sup>



Figure 242. Staircase looking down from Level 3.<sup>261</sup>



Figure 243. Staircase looking down from Level 3.<sup>262</sup>

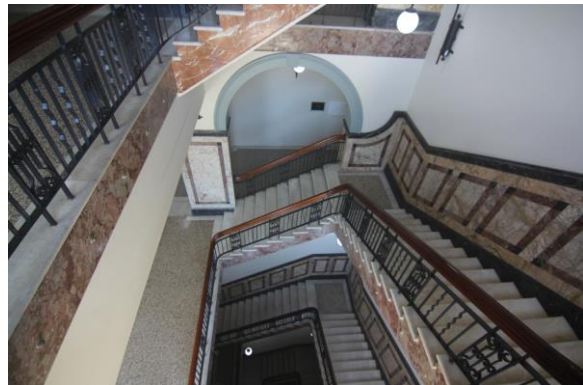


Figure 244. Staircase looking down from Level 3.<sup>263</sup>

### General Office Spaces

The offices follow the U-form shape of the Main Terminus Building, wrapping around the Grand Concourse and located above the concourse level spaces including the former Booking Office and Dining Hall. The office spaces are divided into three wings: western, central (northern) and eastern. While the Central Projecting Bay of the northern wing contains three levels of office space, the rest of the offices are contained on two levels.

Contemporary fit outs have been installed to all the office spaces. Modifications include the installation of new floor coverings, wall linings and suspended ceilings. To all spaces, electrical services, such as lighting, have been altered and mechanical services, including air conditioning, have been added. While the exterior fenestration openings are original, the glass panes have been replaced with double glazing. No other original joinery remains in situ.

<sup>260</sup> Artefact, 2021

<sup>261</sup> Artefact, 2021

<sup>262</sup> Artefact, 2021

<sup>263</sup> Artefact, 2021





Figure 245. Office space level 1 North.<sup>264</sup>



Figure 246. Office space level 2 office fitout.<sup>265</sup>



Figure 247. Office space level 3 office fitout.<sup>266</sup>

## Level One

Level one occupies the entire western, central and eastern wings of the Main Terminus Building. Its primary access point is through the Grand Internal Staircase and its associated lift. The majority of the level is occupied by Sydney Trains. The eastern wing contains the crew shift change area, the central wing contains the transport office and the western wing contains further associated meeting rooms and facilities.

The current fit out of the office dates to 2006. Apart from the fenestrations to the exterior walls, little original fabric remains. The upper height of the original Booking Hall has been incorporated into the offices of the northern wing. As such, a portion of the oeil-de-boeuf clerestory windows are visible in this space. All the original fenestrations are framed in timber.

<sup>264</sup> Artefact, 2021

<sup>265</sup> Artefact, 2021

<sup>266</sup> Artefact, 2021

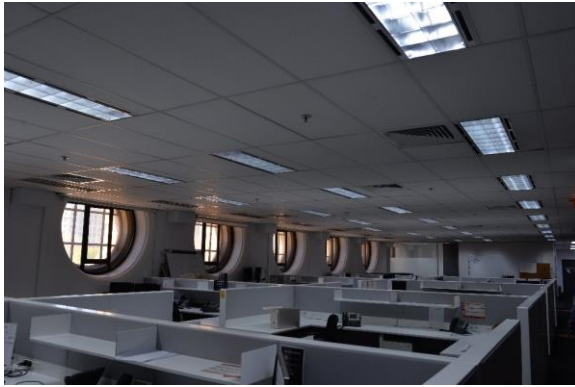


Figure 248. Office space level 1 North.<sup>267</sup>



Figure 249. Office space level 1 North.<sup>268</sup>



Figure 250. Office space level 1 North.<sup>269</sup>



Figure 251. Office space level 1 office fitout.<sup>270</sup>

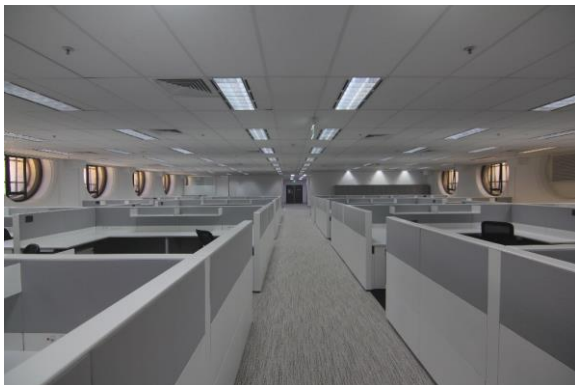


Figure 252. Office space level 1 office fitout.<sup>271</sup>



Figure 253. Office space level 1.<sup>272</sup>

<sup>267</sup> Artefact, 2021

<sup>268</sup> Artefact, 2021

<sup>269</sup> Artefact, 2021

<sup>270</sup> Artefact, 2021

<sup>271</sup> Artefact, 2021

<sup>272</sup> Artefact, 2021



Figure 254. Office space level 1 West.<sup>273</sup>

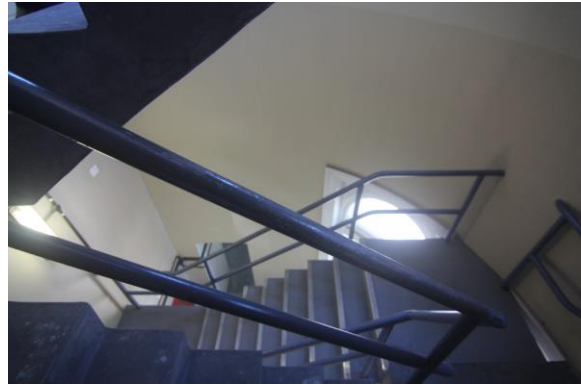


Figure 255. Office space stair level 1 West.<sup>274</sup>

## Level Two

Level Two occupies the entire western, central and eastern wings of the Main Terminus Building. Its primary access point is through the Grand Internal Staircase and its associated lift. Like Level One, this level contains little original features, apart from the existing fenestrations to the external walls. In the early 2010s, the timber framed windows in the west wing underwent conservation works.

The level was fitted out as an office space in 2006. While the eastern wing contains transport offices, the north and west wings are utilised by the NSW Police. A stairwell to the southern end of the eastern wing provides access to the external roof space.

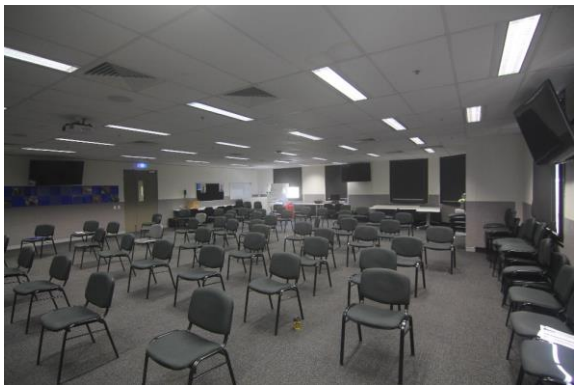


Figure 256. Office space level 2 Police Offices.<sup>275</sup>

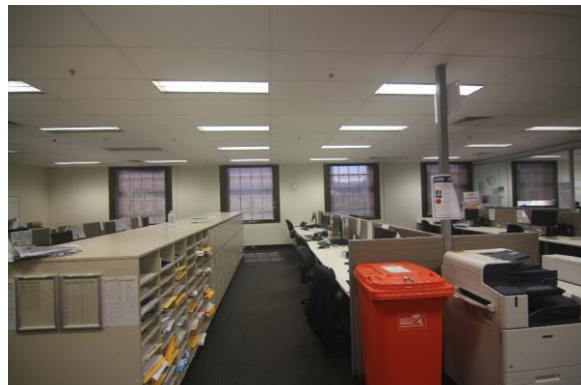


Figure 257. Office space level 2 Police Offices.<sup>276</sup>

<sup>273</sup> Artefact, 2021

<sup>274</sup> Artefact, 2021

<sup>275</sup> Artefact, 2021

<sup>276</sup> Artefact, 2021



Figure 258. Office space level 2 post fitout.<sup>277</sup>



Figure 259. Office space level 2 post fitout.<sup>278</sup>



Figure 260. View towards Clock tower from office space level 2.<sup>279</sup>



Figure 261. View down stair void from level 2.<sup>280</sup>

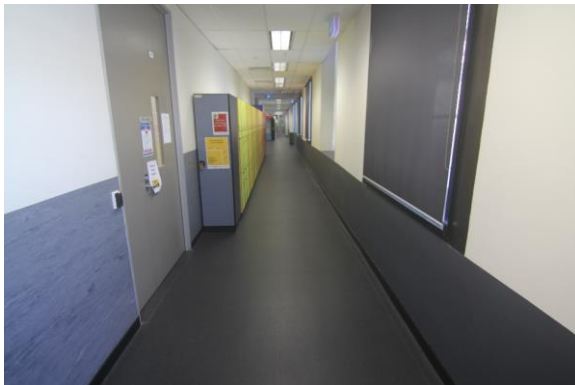


Figure 262. Office space level 2 east.<sup>281</sup>

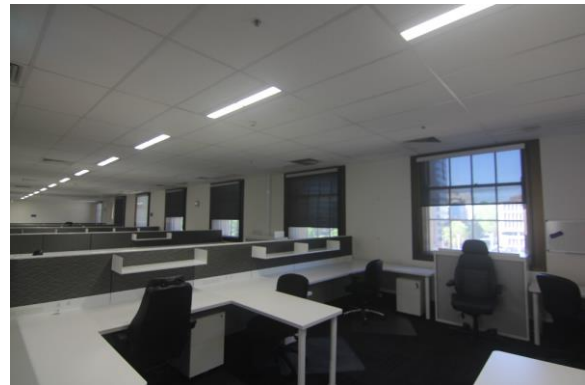


Figure 263. Office space level 2 west.<sup>282</sup>

<sup>277</sup> Artefact, 2021

<sup>278</sup> Artefact, 2021

<sup>279</sup> Artefact, 2021

<sup>280</sup> Artefact, 2021

<sup>281</sup> Artefact, 2021

<sup>282</sup> Artefact, 2021



### Level Three

Level three occupies the central projecting bay in the northern wing of the Main Terminus Building, and is a of a much smaller scale than the two lower levels.

While this area previously contained the offices of the main control room for all the Central Station lines, it is currently unoccupied. Like the other levels, the fit out is contemporary, with the only original features being the fenestrations to the external facades.

From the eastern end of the level, the roof cavity space of the eastern wing is accessible.



Figure 264. Office space level 3.<sup>283</sup>



Figure 265. Office space level 3 post fitout.<sup>284</sup>



Figure 266. Office space level 3 post fitout.<sup>285</sup>



Figure 267. Office space roof area.<sup>286</sup>

### 3.9.3 Statement of Significance

The upper levels of the Main Terminus Building including the office spaces and grand internal staircase are of state significance, deriving their significance from their association with the overall construction and operation of the Main Terminus Building.

Whilst the office spaces retain little original fabric and are of low integrity, the grand internal staircase is largely intact and is an aesthetically significant architectural feature. The staircase exhibits a delineation between spaces, with the lower basement levels left relatively unadorned compared to the upper levels which were considered more important and public facing. The staircase consists of high

<sup>283</sup> Artefact, 2021

<sup>284</sup> Artefact, 2021

<sup>285</sup> Artefact, 2021

<sup>286</sup> Artefact, 2021

quality finishes including marble, terrazzo, timber detailing, wrought iron balustrades and decorative coffered soffits.

The extant oeil-de-boeuf clerestory windows and original fenestrations visible in the upper office levels are important architectural elements and provide evidence that the original Booking Hall space has been altered and divided.

### 3.9.4 Gradings of Significance and Integrity

**Table 23: Gradings of Significance and Integrity for the Main Terminus Building: Office Spaces**

Element	Grading	Integrity
<b>OVERALL</b>		
<b>Main Terminus Office Spaces Overall</b>	High	Moderate
<b>GRAND INTERNAL STAIRCASE</b>		
<b>Doors, Windows and Hardware</b>	High	High-Moderate
<b>Floors and Paving</b>	High	High
<b>Ceilings and Cornices</b>	High	High-Moderate
<b>Skirtings, Architraves and Linings</b>	High	Moderate
<b>Stairs, Balustrades and Soffits</b>	Exceptional	High
<b>Marble Panelling and Dado</b>	Exceptional	High
<b>Applied Finishes</b>	Little	N/A
<b>Introduced Services; Mechanical, Electrical, Lighting &amp; Data</b>	Little	N/A
<b>GENERAL OFFICE SPACE</b>		
<b>Doors, Windows and Hardware</b>	High	High
<b>Oeil-de-boeuf clerestory windows</b>	Exceptional	High
<b>Original ceiling and cornices</b>	Exceptional	Unknown The integrity of the extant ceilings and cornices are unknown, as they are concealed beneath recent ceilings. There is potential for this element to be of high integrity should they found to be relatively intact.
<b>Recent Floor Finishes</b>	Little	N/A
<b>Recent Ceilings and Cornices</b>	Little	N/A
<b>Recent Skirtings, Architraves and Linings</b>	Little	N/A
<b>Recent Applied Finishes</b>	Little	N/A
<b>Recent Fitout</b>	Little	N/A

Element	Grading	Integrity
Introduced Services; Mechanical, Electrical, Lighting and Data	Little	N/A

### 3.9.5 Site Specific Conservation Policies

**Table 24: Site Specific Conservation Policies for the Main Terminus Building: Office Spaces**

Policy	Policy Discussion and Guidelines
1. Follow Overall Conservation Policies	See Section 15 of this CMP for Conservation Policies.
2. Implement Fabric Guidelines	See Section 15.4.4 of this CMP.
3. Item Specific Policy: Heritage Process	<p>When considering change follow the Heritage Flow Charts that form Appendix C of this CMP:</p> <ul style="list-style-type: none"> <li>A) Major Works Heritage Process Flow Chart; or</li> <li>B) Minor Works Heritage Process Flow Chart.</li> </ul>
4. Item Specific Policy: Heritage Listing	<p>See Section 3 in this CMP for Legislative Context, and Section 14.4 for Statutory Context.</p> <p>Ensure appropriate statutory approvals or exemptions are obtained prior to change on the site because:</p> <ul style="list-style-type: none"> <li>• The site is within the overall listing for <b>Central Station</b> on the State Heritage Register under the <i>NSW Heritage Act 1977</i> for Sydney Terminal and Central Railway Stations Group.</li> <li>• The site is within the overall listing of the <b>Central Railway Station and Sydney Terminal Group</b> on Transport Asset Holding Entity's (TAHE) Heritage and Conservation Register under Section 170 of the <i>NSW Heritage Act 1977</i>.</li> <li>• The area is within the Railway Square/Central Station Special Character Area under the Sydney DCP 2012.</li> </ul>
5. Item Specific Policy: Fabric	<p>Specific elements for conservation include:</p> <ul style="list-style-type: none"> <li>• Oeil-de-boeuf clerestory windows</li> <li>• Original timber door and window joinery and fenestrations</li> <li>• Grand internal staircase</li> <li>• Marble panelling and dados</li> <li>• Terrazzo and marble flooring</li> <li>• Coffered plaster soffits and plaster ceilings</li> </ul>
6. Item Specific Policy: Use	Continue to use the area as office accommodation related to the operation of Central Station and the NSW Railways. Continue to use the grand internal staircase as the principal access stair.
7. Item Specific Policy: Development	<p>Prior to the commencement of any proposed development or works, photographic archival records of the precinct should be undertaken.</p> <p>Ensure the original external colour schemes and finishes are documented and archivally recorded.</p>
8. Item Specific Policy: Entrances and Fire Separation	Investigate the introduction of a more sympathetic design to the fire separation of the lift lobbies and the entrances to the stairs and office spaces.

Policy	Policy Discussion and Guidelines
9. Item Specific Policy: Internal Fitout	<p>During the design of new internal fitout ensure that past intrusive fabric and structure is removed.</p> <p>In particular remove false ceilings wherever possible to reveal the original ceilings, and full height of the windows. Design new service routes so that they have minimal impact and allow for ease of maintenance and change.</p> <p>Minimise mezzanine areas so that the sense of the original height of each area can be appreciated at the main entrance.</p> <p>Finish mezzanine floor away from exterior walls so that impact on the original fenestration is minimised.</p> <p>Carefully design new elements so that they are either concealed, or present as a coordinated collection of contemporary minimalist elements and services.</p> <p>Base the design of contemporary fittings such as lighting and seating on the original traditional prototypes.</p> <p>An interior fit-out guide for the offices should be prepared to help guide future fit-outs and to provide a level of consistency across the spaces in their treatment and protection of the heritage fabric and spaces.</p> <p>Items which are identified as suitable for interpretation should be considered and integrated as part of the internal fitout of the spaces.</p>
10. Item Specific Policy: Intrusive fabric.	<p>Demolition of, or changes to, the existing intrusive building services should be encouraged and should be allowed without formal approval provided no significant fabric is impacted on by the change.</p>
11. Item Specific Policy: Colour Schemes	<p>Investigate, through paint scrapes, the original colour schemes of the Office Spaces. Reinstate these colour schemes unless there is a strong imperative not to.</p>
12. Item Specific Policy: Short term uses and changes.	<p>Do not allow negative impacts on significant fabric, spatial qualities and setting for short term gain. E.g commercial or advertising signage.</p>



## 3.10 CLOCKTOWER



Figure 268. Central Station Clocktower.<sup>287</sup>



Figure 269. Central Station Clocktower.<sup>288</sup>

### 3.10.1 History of the Clocktower

Walter Liberty Vernon, Government Architect, was responsible for the original design of the third Sydney Terminus, including the clocktower. It was designed in the same Neo-Classical architectural style as the Main Terminus Building, however due to budget constraints, the design was simplified during its construction. The construction of the clocktower was completed as part of the second phase 1915-22, which was overseen by Vernon's successors – George McRae and Gorrie McLeish Blair. When discussing the original design, Henry Deane, explained that the clocktower was a significant feature and part of the grand vision for Central Station, describing the tower as "commanding feature...provided with a clock that will be visible from most parts of the city It is expected that the whole will produce an imposing architectural effect"<sup>289</sup>.

The first stage of the clocktower's construction was completed in 1906 as part of the first stage of works, however progress was stalled until 1915. This coincided with the beginning of the First World War, and resulted in slow progress until the war ended in 1918. By 1919, masonry was completed up to the level of the clock chamber. Due to the high cost and shortage of steel, the clock tower shaft, girders, and floors were constructed from reinforced concrete. The shaft was connected to the main concourse level by a marble staircase, which was also built during this time.

The clocks were installed in the clocktower in 1920 and became operational the following year. This was a monumental occasion in the development of the Sydney Terminus, as not only was the clocktower complete, but as it could be seen throughout the city, it also fulfilled its purpose as a bearer of time, later becoming known as the 'working man's watch'. The clock dials were constructed off-site at the Randwick Tramway Workshops and then transported to the station for installation, while the electricity-controlled mechanism was supplied by Gent & Company of England. McRae also show bells in a 1916 drawing of the clocktower, however these were never installed.

Extensive restoration of the Central Station precinct commenced in 1980. The purpose of these works was to revitalise the railway system and its buildings, with Stage 2 works focussing on the restoration of the clocktower. These works were undertaken between 1984 and 1985 and costed a total of

<sup>287</sup> Artefact, 2021

<sup>288</sup> Artefact, 2021

<sup>289</sup> McKillop, Century of Central, p 25

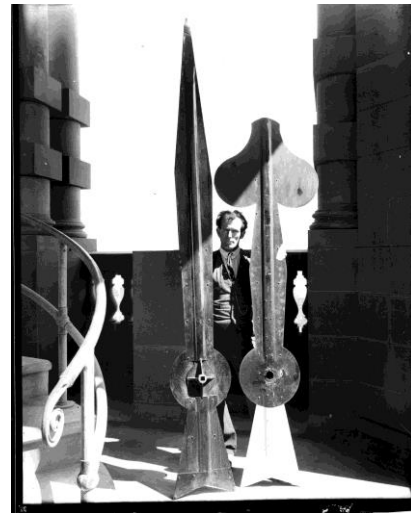
\$592,000.<sup>290</sup> Severe rusting had occurred to the three-tonne iron frame that supported the clock faces. This was causing the sandstone to fail and crack where the frame had been bolted in. As a result, the dilapidated sandstone masonry needed to be replaced and the clock mechanism required repairing. The sandstone works were undertaken by Gosford Quarries, with measures taken to mitigate the potential of future damage. This included building new frames and attaching them to girders inside the tower, rather than on the sandstone face. 368 sheets of glass were required to restore the clock face, with each frame being cleaned or replaced, as necessary. Replacement of the original clock hands also occurred. Although the clock hands were quite large, with the original minute hand measuring 3 meters long and the hour hand measuring 2.1 metres long, they were put on display in the former Booking Hall. They have been removed since the Booking Hall underwent refurbishment in 2018.

In March 1985, the newly restored clock was unveiled. The following year, it failed to operate for the first time in its history. However, it quickly returned to its full working order and has not failed to operate since then. Ten years later in March 1996, the Clocktower celebrated its 75<sup>th</sup> Anniversary.

Further restoration works were undertaken in 2011-2012. This involved the installation of new copper roof cladding to the sandstone cupola, sheet lead lining to the top of the clock faces and coping around the tempietto and the balconies. While the lightning conductor mounting the cupola was replaced with a flagpole, other original features, such as the sandstone balustrades, details, and figures, were cleaned and restored.



**Figure 270: Central Station Clocktower under construction** <sup>291</sup>



**Figure 271: Man holding the hands for the clock tower, date unknown.** <sup>292</sup>

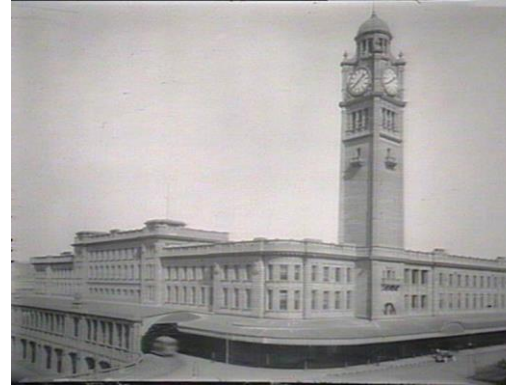
<sup>290</sup> McKillop, *Century of Central*, p. 115

<sup>291</sup> ARHS, date unknown.

<sup>292</sup> Sydney Trains Archives, date unknown. ID: 023470



**Figure 272: Central Station Clocktower under construction**<sup>293</sup>



**Figure 273: Central Station Clocktower, date unknown.**<sup>294</sup>

### 3.10.2 Site Description and Condition

The clocktower is situated in the north-western corner of the Main Terminus Building, accessed via a grand lobby that has its main entrance fronting the Western Forecourt, just north of the main western arched entry to the Grand Concourse. The clocktower is square in plan, with its form partially projecting out from the western elevation of the Main Terminus Building. Due to its height and location, direct and prominent views of the clocktower are possible from Railway Square and the Western Forecourt, as well as the nearby streets surrounding Central Station, including, but not limited to Pitt Street, Eddy Avenue, Railway Colonnade, St. Laurence Lane, Elizabeth Street and George Street.

The clocktower is nine storeys tall, with six of these above the Main Terminus Building's roof, with the top of the tower's dome sitting 64.3m above the Concourse. The external facades of the clocktower shaft make up the bulk of the vertical structure and consist of four identical elevations which comprise finely carved ashlar sandstone with deep rebated banded rustication and buttresses which are set at 45 degree angles at each respective corner.

The ground floor level of the clocktower exhibits rough-faced rusticated sandstone blocks atop a moulded sandstone plinth of ashlar sandstone masonry, with a large central doorway on the western elevation, comprising a rounded arch with alternating smooth and rough-faced sandstone blocks and a decorative keystone. A decorative timber-framed French door flanked by fixed glazing on each side and a fanlight above is inset into this archway. The door is inset with glass panes featuring etched glass and is surmounted by a carved 'swan-neck' style broken pediment supported by Ionic timber columns on tall timber plinths.. Inscribed trachyte cornerstones, dated 1903, have been incorporated into both of the buttresses along the western elevation, directly above the level of the plinths. The northern cornerstone reads 'THIS STONE / WAS LAID ON 26<sup>TH</sup> SEPTEMBER 1903, BY / THE HON. SIR JOHN SEE . K.C.M.G. / PREMIER / THE HON. E.W. O'SULLIVAN. / MINISTER FOR PUBLIC WORKS / H. DEANE. M. INST C.E. / ENGINEER IN CHIEF' and the southern cornerstone reads 'ADMIRAL SIR HARRY H. RAWSON K.C.B. / GOVERNOR OF NEW SOUTH WALES / 1903'.

<sup>293</sup> Sydney Trains Archives, date unknown. ID: 023472.

<sup>294</sup> Central State Library, date unknown, ID: nd d1\_17578r

The first-floor western elevation exhibits a Diocletian window with a sandstone lintel featuring banded rustication, decorative sandstone mullions and multi-paned timber-framed windows. The western elevation of the second-floor features two windows and a central door with square-headed hood mouldings and timber-framing. The central French door leads to a cantilevered sandstone balcony with a decorative balustrade, base and four corbels supporting the balcony. At the top of the second floor is a projecting cornice with dentils below and lead flashing above. Above the cornice at the top of the second floor are two courses of sandstone blocks surmounted by a cornice, the first course comprising simple smooth-faced ashlar and the second featuring smooth-faced embossed ashlar blocks, visually connecting the balustrades on each side of the tower at this level.

The third to ninth levels of the tower are identical on all four elevations. The third and fourth floors on all sides exhibit banded rusticated walls punctuated by narrow loophole windows on alternating end of each elevation. The fifth floor continues the banded rustication, but exhibits a central small, cantilevered sandstone balcony with a decorative balustrade and base supported by three decorative corbels. The doors leading onto the balconies feature square-headed hood mouldings and timber-framed glazed French doors. Simple cornices at the junction between the fifth and sixth floors feature a dentillated band. The sixth floor features large square-headed openings supported by four coupled sandstone columns (accouplements) with Ionic capitals and inset with sandstone balustrades along the base of the openings, forming a partially open space. The junction between the sixth and seventh floor is decorated with a highly ornamental entablature, featuring an embossed cartouche pattern below a dentillated cornice.

The seventh floor comprises the clockfaces, each 4.77m in diameter, set in circular smooth sandstone edging with an anthemion ornamental keystone, and flanked by ornamental diagonal buttresses surmounted by Neo-Classical urns. Elaborate figures carved into the sandstone feature in the spaces between the clockface, the buttresses and the cornices below. The clocktower is surmounted by a circular-planned tempietto comprising four piers and Tuscan columns supporting a curved entablature and sandstone cupola lined with copper sheeting; the latter was installed in c.2011-2012. Lead flashing has been applied to the top of the entablature, balcony floors and buttress piers on the clocktower. A flagpole extends from the top of the cupola.

The interior of the clocktower on the ground to second floors features a grand staircase running along the internal walls of the tower, creating a central stairwell. The ground floor hall, lift lobby and staircase landings feature red terrazzo bordered by black and white marble tiles on the floor, but the higher levels and landings exhibit the red terrazzo bordered by white marble tiles. The walls are lined with dados of grey, white, black and red marble, with inset rectangular motifs, although isolated sections of the dado have been removed adjacent to the lifts on the first and second floors. The curtain step of the staircase is flanked by two black marble newel posts. The staircase comprises concrete steps with marble tread surfaces supported by concrete-encased steel beams and console brackets, with landings providing access to the offices on the first and second floors, and decorative wrought iron balustrade with polished cedar handrails. The staircase is lined with a plaster coffered soffit, painted white with a green and pink border and supported by beams on decorative plaster corbels. On the ground floor, entrance to the lobby exhibits an ornamental marble architrave featuring pilasters on either sides of the doorway and a tall, relatively plain, entablature. The ceiling on the second floor and in the archway near the lift exhibits coffered plaster ceilings. The round-arched opening on the second-floor landing of the clocktower leading to the West Wing exhibits a rounded architrave and coffered soffit above marble pilasters. The arch has been infilled with light-weight partition walls and a fire door. Along the southern wall of the second floor of the tower, an empty alcove exhibits a red marble architrave. From the second floor, a continuation of the flight of stairs provides access to a timber-framed French door flanked by timber-framed windows along a mezzanine. The door leads to a balcony on the second floor of the western wall of the clocktower, affording a view of the western forecourt. Lighting in the first and second floors appears to rely on natural light from large multi-light timber-framed windows and lamps fixed to walls along the landings.



The infilled archway inset with the fire door on the second floor leads to the West Wing main corridor, in which a small staircase with terrazzo tread and steel handrail with a volute end leads to the interior of the tower shaft. Beneath the staircase is a timber-panelled under-stair storage area inset that is comprised of timber doors with four-light windows. Adjacent to the storage cupboard under the stair is an elevated small timber-panelled door leading to the roof of the West Wing. The tower shaft from floors three to four comprises a large void with a cantilevered concrete staircase with a steel balustrade along the interior side of the walls. The steel balustrade is simple in design, but also ends with a volute detail. Loophole embrasure windows puncture the walls of the shaft, providing light into the area. Consolidated services run up the corners of the tower shaft and below the staircase. The staircase leads up to the level of the balcony on the fifth floor, ending in a landing filling the floor space of the shaft. A concrete spiral staircase with a voluted-ended steel balustrade leads from the landing on the fifth floor up to the landing on the sixth floor, which contains the partially open floor of the tower, exhibiting the large square-headed openings supported by four coupled sandstone columns (accouplements). The steel balustrade is painted white on the lower level and black on the upper level, while the staircase and supporting central column are left exposed on the lower levels and painted white on the upper level. The spiral stair continues up to the seventh floor, which contains the clocks. Ladders provide access to the top of the clocktower, including the clock mechanism chamber and the tempietto protected by the cupola.

The clocktower is generally in good condition, with few discernible defects aside from contemporary graffiti on the spiral staircase, in addition to historic graffiti on this element.



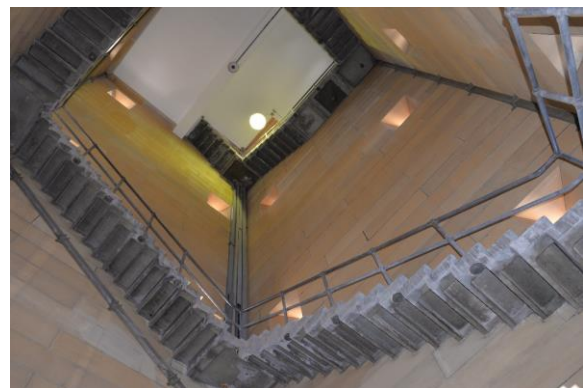
**Figure 274. View of Clock tower from Roof.**<sup>295</sup>



**Figure 275. Figure 276. View of Balustrades from Clock tower from Roof.**<sup>296</sup>



**Figure 277. Figure 278. View from Clock tower roof over Central station.**<sup>297</sup>



**Figure 279. View up through stair void to clock tower.**<sup>298</sup>

<sup>295</sup> Artefact, 2021

<sup>296</sup> Artefact, 2021

<sup>297</sup> Artefact, 2021

<sup>298</sup> Artefact, 2021



Figure 280. View of circular stair to Clock tower.<sup>299</sup>



Figure 281. View of upper level circular stair to clock tower.<sup>300</sup>



Figure 282. View of upper level circular stair to clock tower.<sup>301</sup>



Figure 283. Street view of Clock tower from Eddy Ave.<sup>302</sup>

<sup>299</sup> Artefact, 2021

<sup>300</sup> Artefact, 2021

<sup>301</sup> Artefact, 2021

<sup>302</sup> Artefact, 2021



Figure 284. View of basement of clock tower.<sup>303</sup>



Figure 285. View in basement foyer of Clock tower.<sup>304</sup>

### 3.10.3 Statement of Significance

The clocktower is a well-known landmark in Sydney, an ever-present feature in the skyline in wide sweeping vistas across Sydney. The clocktower is assessed as having state heritage significance for its landmark status, its historic, aesthetic, technical and social significance.

Historically, the clocktower is associated with the original design by Walter Liberty Vernon for the third terminus and is also associated with succeeding Government Architects George McRae and Gorrie McLeish Blair. The clocktower is a key feature of the completion of the second stage of construction at the Terminus.

The clocktower is aesthetically significant for its Neo-Classical architectural style and the use of high quality materials which are largely intact. These include the fine ashlar sandstone, decorative sandstone carvings, grand internal marble staircase, terrazzo, and timber detailing.

The clocktower is also technically significant for its design, which was one of the first uses of reinforced concrete and includes sandstone buttress piers at 45 degree angles to hold the clock faces to the clock chamber and alleviate downward thrust from the tempietto.

Socially as a landmark in Sydney, the clocktower became known as the 'work man's watch' providing an accurate time to measure against across Sydney.

### 3.10.4 Gradings of Significance and Integrity

Table 25: Gradings of Significance and Integrity for the Clocktower

Element	Grading	Integrity
<b>Clocktower Overall</b>	Exceptional	High
<b>Views and Vistas</b>	Exceptional	High
<b>Context and Setting</b>	Exceptional	High
<b>Cupola</b>	Exceptional	High Copper sheeting replaced c.2011
<b>Tempietto</b>	Exceptional	High

<sup>303</sup> Artefact, 2021

<sup>304</sup> Artefact, 2021

Element	Grading	Integrity
Spiral Stair	High	High
Clock	Exceptional	High
Tower Shaft	Exceptional	High
Juliet Balconies	High	High
Sandstone Detailing	Exceptional	High
Leadwork	Moderate	High
Cantilevered Concrete Stair	High	High
Lobby Grand Stair Balustrades and Marble Panelling	Exceptional	High

### 3.10.5 Site Specific Conservation Policies

**Table 26: Site Specific Conservation Policies for the for the Clocktower**

Policy	Policy Discussion and Guidelines
1. Follow Overall Conservation Policies	See Section 15 of this CMP for Conservation Policies.
2. Implement Fabric Guidelines	See Section 15.4.4 of this CMP.
3. Item Specific Policy: Heritage Process	<p>When considering change follow the Heritage Flow Charts that form Appendix C of this CMP:</p> <p>A) Major Works Heritage Process Flow Chart; or B) Minor Works Heritage Process Flow Chart.</p>
4. Item Specific Policy: Heritage Listing	<p>See Section 3 in this CMP for Legislative Context, and Section 14.4 for Statutory Context. Ensure appropriate statutory approvals or exemptions are obtained prior to change on the site because:</p> <ul style="list-style-type: none"> <li>The site is within the overall listing for <b>Central Station</b> on the State Heritage Register under the <i>NSW Heritage Act 1977</i> for Sydney Terminal and Central Railway Stations Group.</li> <li>The site is within the overall listing of the <b>Central Railway Station and Sydney Terminal Group</b> on Transport Asset Holding Entity's (TAHE) Heritage and Conservation Register under Section 170 of the <i>NSW Heritage Act 1977</i>.</li> <li>The area is within the Railway Square/Central Station Special Character Area under the Sydney DCP 2012.</li> </ul>



Policy	Policy Discussion and Guidelines
5. Item Specific Policy: Stone Conservation	<p>Conservation of the sandstone facades should be undertaken as part of the cyclical maintenance of the building. Particular care and inspection should be undertaken for any projecting or decorative elements (such as pediments, entablatures, dentils and mouldings) which has a higher risk of public injury. These should be regularly inspected to make sure the risk of falling is minimised and mitigated.</p> <p>A stone strategy for the facades should be prepared to guide the conservation of the masonry.</p> <p>A strategy for the protection of the masonry facades during any major works should be undertaken by heritage architectural specialists and qualified stonemasons with heritage experience prior to the commencement of any works.</p>
6. Item Specific Policy: Development	<p>Prior to the commencement of any proposed development or works, photographic archival records of the precinct should be undertaken.</p> <p>Ensure the original external colour schemes and finishes are documented and archivally recorded.</p>
7. Item Specific Policy: Alterations and Additions	<p>Alterations and additions to the exterior of the clocktower are not permitted. Do not paint the sandstone facades. Do not paint the external entry door and timber surround, retain a stained and French polish finish.</p> <p>Alterations to the interior of the clocktower are not permitted. Do not paint or damage the significant marble and terrazzo finishes in the grand staircase.</p>
8. Item Specific Policy: Fabric	<p>Conserve significant fabric including:</p> <ul style="list-style-type: none"> <li>• Sandstone facades</li> <li>• Marble panelling and dados</li> <li>• Terrazzo and marble flooring</li> <li>• Coffered plaster soffits and plaster ceilings</li> <li>• Grand staircase</li> <li>• Timber window and door joinery</li> <li>• Acid etched glazing</li> </ul>
9. Item Specific Policy: Vistas:	<p>Ensure vistas to the Clocktower outlined in Section 8.6 of this CMP are not obscure. These include vistas from:</p> <ul style="list-style-type: none"> <li>• Pitt Street</li> <li>• Western Tram Ramp</li> <li>• Railway Colonnade Drive</li> <li>• Lee Street</li> <li>• Pitt Street</li> <li>• George Street</li> <li>• Eddy Avenue</li> <li>• Belmore Park</li> <li>• Platforms 1-23</li> <li>• From within Sydney Yards</li> </ul>

Policy	Policy Discussion and Guidelines
10. Item Specific Policy: Interpretation	<p>See Sections 9 and 14.8 'Heritage Interpretation' in this CMP.</p> <p>Prepare a heritage interpretation strategy, based on the precinct-wide Central Precinct Heritage Interpretation Strategy, specific to the Clocktower as part of Central Precinct. Particular focus should be given to the former landscape and cultivation of the space.</p> <p>Following this, develop and implement a detailed site-specific heritage interpretation plan, aligned with the themes and recommendations of the heritage interpretation strategy.</p>
11. Item Specific Policy: Modern fittings	<p>Fixing new elements to the external sandstone face of the clocktower is not permitted and is to be avoided. Telecommunication services, if required, should be coordinated and housed at the base of the tower above the roof of the main building so to conceal them and minimise any visual impact.</p>
12. Item Specific Policy: Short term uses and changes.	<p>Do not allow negative impacts on significant fabric, spatial qualities and setting for short term gain. E.g. temporary plant.</p>

## 3.11 WEST WING EXTENSION

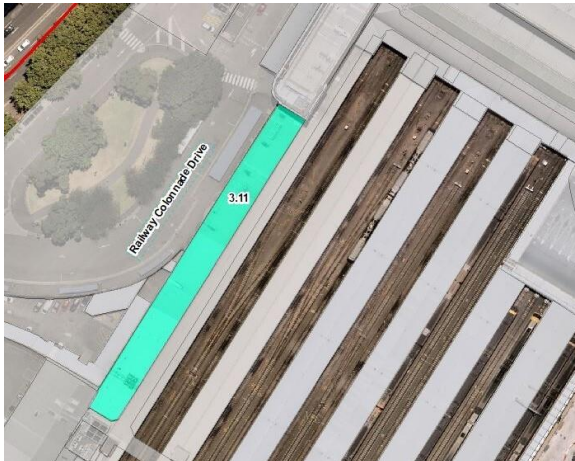


Figure 286. West Wing Extension<sup>305</sup>



Figure 287. West Wing Extension to the right of the photo looking north.<sup>306</sup>

### 3.11.1 History of the West Wing Extension

In 1899, it was decided by a Parliamentary Standing Committee that sandstone would be the appropriate material of which to construct the Main Terminus Building. Although brick was a more durable material, sandstone had been used in the construction of many major public buildings of the time and had become associated with prestige. It could also be used to create intricate details, which could not be achieved with brick. The Committee considered both materials and decided to establish a hierarchy of materials; where sandstone was used for the most prestigious buildings, such as the Main Terminus Building, and brick was used for other structures.

The hierarchical approach towards the choice of building materials was exercised during the extension of the West Wing. Although the Main Terminus Building was constructed almost entirely from sandstone, the West Wing Extension was completed in brick, with some sandstone detailing. The choice of brick signified that the extension was not as important as the Main Terminus Building.

A variety of rooms are contained within the West Wing Extension, most of which now have different functions to their original use. These include the former telegraph office and VIP waiting room at platform level and the Guards and Porters rooms on the lower mezzanine level.

Established in 1914, the telegraph office was located at the platform level in the northernmost room. Its operations were imperative to the function of the rail network, as the room contained the telephone and telegraph communications network that were used to connect the outlying railway stations to Central Station. When the telegraph operations room was relocated to an office at the south of the West Wing, the room was taken over as the Stationmasters office. It remained as such until 2006, when the former telegraph office became part of the CountryLink booking office and the Stationmaster's office was relocated to the East Wing of the Terminus Building.

The West Wing also had a VIP waiting room that could be used for passengers of the Platform 1 country bound services. This was established in 1921, however sometime prior to 1946, it was converted to the Ticket Examiner's Office. At this time, it was functioning as a public telephone room,

<sup>305</sup> Artefact, 2021

<sup>306</sup> Artefact, 2021

however, by the turn of the century it converted into a waiting room for the use of CountryLink passengers.

Located adjacent to the Governor's Archway fronting Platform 1, there were also toilets that were originally associated with the Ticket Examiner's Office. In 2006, the space was converted into a Drug and Alcohol Testing room, however, it became a First Aid room shortly after. An office for ticket collectors was also located to the south of Governor's Archway, but by 1976 this had become a refreshment room and after 2006 was converted into a changing room and locker space for the Responsible Government Agency staff. Other related spaces included the former linen and cashier rooms. These were located to the southern end of the West Wing extension and as of 2013, were being used for administrative purposes.

Prior to 1996, the rooms in the lower level mezzanine of the West Wing Extension were used as the Guards and Porters room, as well as a band room for the NSW Band. However, post 1996, these rooms became vacant. At this time the Governor may have been appointed the use of the rooms adjacent to the Governor's Archway, at the entrance to Platform 1. However, this is undetermined. By 2013, all three rooms were left vacant and were in a poor state repair.



Figure 288: West Wing Extension, 1920s.<sup>307</sup>



Figure 289: West Wing Extension during the Queen's visit in 1958.<sup>308</sup>

### 3.11.2 Site Description and Condition

#### Location

The West Wing Extension building is located to the south-west of the Main Terminus Building, to the north-west of Platform 1, to the north-east of the Former Inwards Parcels Office (YHA Railway Square) and to the south-east of the Western Forecourt. Prominent views of the West Wing Extension are afforded from the Western Forecourt and the Parcels Area, as well as from Lee Street and George Street. Views of the West Wing Extension's south-eastern elevation from within the Country and Interstate platforms area are, for the most part, obscured by the c.1960s Platform 1 awning which runs along the south-eastern elevation of the building.

#### Exterior

The West Wing Extension comprises a rectilinear two-storey building oriented north-east to south-west and constructed from English bonded red-brown brick with sandstone detailing and a low-pitched corrugated metal skillion roof. Due to the location of the West Wing Extension between the platforms, the Western Forecourt and the lower Parcels Area, the basement level of the West Wing Extension is at the level of the Parcels Area and the ground floor is at the level of Platform 1 and the

<sup>307</sup> State Library of NSW, photographed by S. Hood, 1920s. West Wing Extension. ID: hood\_07458h

<sup>308</sup> State Records of NSW, 1954. Queens Visit. ID: 17420\_a014\_a014000157



Western Forecourt. The West Wing building comprised 22 bays along the north-western and south-eastern elevations and two bays along the south-western elevation, with bays delineated by engaged brick piers with sandstone plinths along the north-western elevation and ashlar sandstone engaged piers along the south-eastern elevation.

The ground floor on the north-western elevation is delineated by a sandstone stringcourse/cornice at the top of the elevation and a chamfered ashlar sandstone plinth at the base of the elevation in the forecourt area that seamlessly transforms into a stringcourse within the Parcels Area, due to the basement level below. The majority of the bays on the ground floor level of the north-western elevation feature square-headed double windows separated by chamfered brick mullions with rough-faced rusticated sandstone segmental arched triptych transom windows, with chamfered ashlar intradoses, above. The lintels and sills of the double windows, and the sills of the transoms are constructed from chamfered smooth-face sandstone. The double windows consist of double-hung timber-framed sash windows, while the transoms feature timber-framed casement windows. The glazing appears to be a mix of original and late twentieth-century glazing. Many of the timber frames exhibit flaking paint, some of which appears to indicate decay of the timber substrate.

The eighth bay from the north-eastern end of the north-western elevation at ground floor level, unlike the rest of the elevation, exhibits a tall semi-circular archway of rough-faced rusticated sandstone masonry with chamfered ashlar intradoses. The archway exhibits a late twentieth-century corrugated steel roller door, painted maroon. The archway provides access between the western forecourt and Platform 1, originally named the 'Governor's Archway', and though formerly used as an Ambulance access until c.2017, it appears to function as an unofficial storage, passenger waiting area, and access area to the platform.

Three of the bays along the north-western elevation display a door adjacent to a window, instead of double windows. All three doors display timber-framed timber-panelled doors with four-light transom windows. Small concrete staircases lined with late 20<sup>th</sup> century orange and white tiles, flanked by green-painted wrought iron balustrades, lead to the doors. The doorways, transoms, concrete staircases and balustrades likely date to c.1930. In front of each bay at the level of the forecourt are decorative, green-painted curved iron balustrades, preventing pedestrian access to glass brick pavement lights which provided light to the basement level, which is below the level of the forecourt. The glass brick pavement lights are currently covered with temporary boarding likely dating to the late 20<sup>th</sup> century, although a small number have been infilled with asphalt or concrete. The pavement lights beneath the archway displays the original glass bricks set in iron framing, visible at the sides where not covered by an asphalt ramp. It is unknown as to the extent of the retention of the rest of the skylights.

Two of the balustrades have been cut and re-configured for the staircases, while one is missing to the front of the northern-most bay and the bay to the south of the archway. Contrary to previous analyses of the West Wing Extension,<sup>309</sup> historical photographs indicate that the brick parapet along the top of the north-western elevation of the building and the skillion roof were originally constructed contemporaneously with the rest of the building in c.1906. However, the existing corrugated roofing dates to c.2000. A small number of the original face-brick chimneys and later ducts project above the skillion roof.

In the Parcels Area, the north-western elevation exhibits the full two storey height of the building from bays 12 to 22, with the basement divided into a basement level and a mezzanine along bays 16 to 21. Bay 22 is not visible, as it is obscured by its connection to the northern extent of the YHA Railway Square building (Former Inwards Parcels Building). The windows on the ground floor (second storey) continue the same flat-arched double timber-sashed windows surmounted by rusticated sandstone

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<sup>309</sup> Rappaport Pty Ltd & NSW Government Architect's Office, 2013. Central Station CMP. 3.3.11 West Wing Extension, p. 4.

segmental arched triptych transom windows. Each bay along the basement level from bays 12 to 15 and the mezzanine level along bays 16 to 21 exhibits triptych windows with decorative brick relieving arches laid in soldier courses with chamfered sandstone lintels and sills, inset with timber-framed double-hung sash windows. The basement level, comprising the Parcels Area displays bays punctuated with large doors exhibiting segmental arched sandstone arches, small doors exhibiting flat-arched lintels and windows exhibiting flat-arched sandstone lintels and chamfered sandstone sills. A large c.1960s steel framed awning with late 20th century/21st century corrugated steel roofing runs along the western elevation of the West Wing Extension at ground level, connecting to the brick garage at the northern end. The openings display a mix of early timber-framed glazing and later 20th century brick infill and timber doors.

The south-eastern elevation runs along Platform 1, abutting the western interior wall of the Main Terminus Building to the north. As its construction mirrors that of the eastern interior Main Terminus Building elevation, it is difficult to distinguish the junction between both areas. However, it is defined by a sandstone column to the south of the Luggage Area entry. The façade features brick construction with sandstone detailing to the walls and fenestrations. To the northern end of the façade, the arch of the windows have been obscured by the low height of the platform awning. The arches of these windows are outlined by rusticated sandstone and contain chamfered ashlar stone intradoses. To the south of this, the windows vary in height and width. Some contain a single pane, while others are double paned. All the windows on this façade are timber framed, with some containing acid etched glazing that matches those to the interior walls of the Main Terminus Building. Unlike the western façade of the West Wing Extension, the south-eastern façade does not have a brick parapet. There is a string course detail, however, a metal box gutter that runs the length of the building has been attached to this.



**Figure 290. View of West Wing looking towards the Clock Tower.<sup>310</sup>**



**Figure 291. View of West Wing looking away from Clock Tower.<sup>311</sup>**

<sup>310</sup> Artefact, 2021

<sup>311</sup> Artefact, 2021



Figure 292. View of connection of the West Wing to the main terminus.<sup>312</sup>



Figure 293. View of West Wing balustrades<sup>313</sup>



Figure 294. View of West Wing entry from Western Forecourt.<sup>314</sup>



Figure 295. View of Platform 1, on the West Wing extension<sup>315</sup>

<sup>312</sup> Artefact, 2021

<sup>313</sup> Artefact, 2021

<sup>314</sup> Artefact, 2021

<sup>315</sup> Artefact, 2021



**Figure 296. Former Lavatory, part of the West Wing Extension. <sup>316</sup>**

## Interior

The West-Wing Extension contains several rooms at both platform level and mezzanine level, however, majority of these are underutilised or vacant. At platform level, the building contains, from north to south: Luggage collection and drop off space, NSW TrainLink Offices, a waiting room, bathrooms, first aid room and the 'Governor's Archway'. To the south of the 'Governor's Archway' is a series of rooms occupied by Sydney Trains. A detailed description of these rooms follows:

### Concourse Level Interior: Waiting Room

This room is located to the south of the Sydney Trains Offices. The interior is reasonably intact and contains original architectural features. These include the location of the fenestrations, the timber joinery and an original marble fireplace to the south side of the interior wall. Evidence of original natural ventilation brackets, which are consistent with those found in the Main Terminus Building, are located along the western wall. The flooring, paintwork, seating and services are all contemporary.

Bathrooms are located directly to the south of the Waiting Room. To the south of these is the first aid room. The Governor's Archway, which abuts the first aid room to the north, is currently being used as a waiting room for the NSW TrainLink Regional Coaches. As such, it contains a series of contemporary timber benches.

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<sup>316</sup> Artefact, 2021





**Figure 297. External view of Concourse level Waiting Room from the platform 1.**<sup>317</sup>



**Figure 298. Internal view of Concourse level Waiting Room Tower.**<sup>318</sup>



**Figure 299. Internal view of Concourse level Waiting Room Tower.**<sup>319</sup>

### **Concourse Level Interior: Platform Level Offices**

Sydney Trains Offices are located within two areas of the West Wing Extension. The first is to the north, bounded between the Luggage Area of the Main Terminus Building to the north and the Waiting Room to the south. The second, known as the Locker Room, is to the south of the Governor's Archway.

Although the first area retains its original configuration and detailing, it features a contemporary fit out that is suitable for office use. Original features of this space include timber joinery, fenestration locations and original fireplace mantelpieces.

The Locker Room is a large room, containing lockers and changing areas for the use of Sydney Trains staff. Original architectural features that remain in-situ include the fenestrations and joinery. Evidence of original natural ventilation brackets, which are consistent with those found in the Main Terminus Building, are located along the western wall. The walls and ceiling have been painted white.

<sup>317</sup> Artefact, 2021

<sup>318</sup> Artefact, 2021

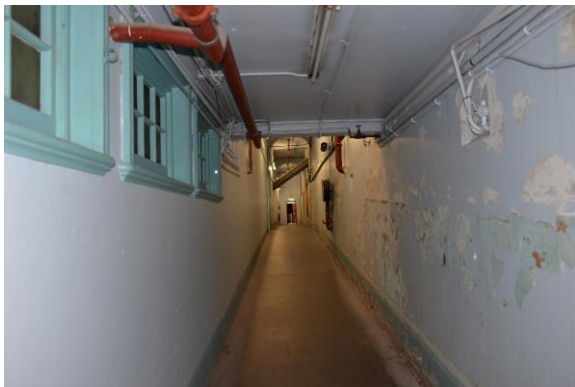
<sup>319</sup> Artefact, 2021



**Figure 300. Internal view of Platform Level Offices.**<sup>320</sup>

### Lower Level Interior: Offices

The lower level is located between the platform level offices above and the Parcel Areas rooms below. The Parcel Area is at the level of Lower Carriage Lane (accessed via Ambulance Avenue), while the Platforms are at concourse level. This level only exists to the lower portion of the West Wing Extension, adjacent to Lower Carriage Lane. From the Lane, the windows associated with this level are visible. Internally, the office spaces have a low ceiling height. They are accessed via a service stair, which connects the Platform level above with the Parcels Area below.



**Figure 301. Internal view of Lower-Level Office corridor.**<sup>321</sup>



**Figure 302. Internal view of Lower-Level Office corridor.**<sup>322</sup>

### Basement Level Interior: Mezzanine Level Rooms

Below the West Wing Extension's platform level offices there is a corridor running in the north-south axis. This is located to the west of and runs parallel to Subway No. 1. The corridor and its associated spaces are located at a slightly higher floor level than the basement level of the Main Terminus Building. As such, there is a staircase at the northern end of the corridor which connects the corridor with the basement level of the Terminus Building. At the southern end, the corridor is accessed via an entrance point to the western and northern walls of the adjoining Subways. This end also connects to the Parcels Area. To the east, the corridor is bounded by a series of rooms. The most southern of these contains an access stair which connects the basement to the Mezzanine Offices and platform level above. At the middle of the corridor there is a large double-heighted space containing a half-turn staircase. This connects to the Platform level of the West Wing Extension and is constructed from

<sup>320</sup> Artefact, 2021

<sup>321</sup> Artefact, 2021

<sup>322</sup> Artefact, 2021

concrete, featuring metal balusters with a continuous, volute ended timber handrail. Between this stair and the southernmost room there is another stair, accessed to the west of the corridor. This stair links to both upper levels.

The walls of the corridor are finished with lead paint, which is beginning to flake in areas. Original vents are visible to the walls. Contemporary lighting has been installed; however, the area remains poorly lit.

The rooms accessed to the west of the corridor are mostly vacant. Although many of them contain original features, including floors, architraves, doors, windows and hardware, they are in a dilapidated state due to lack of use and maintenance, which is further impacted by the lack of light and abundance of moisture. To the north of the corridor, the double-heighted rooms are mostly empty. The largest of these, known as the 'band room', which was associated with the NSW Railway Band. To the west of the rooms, the original light wells remain in-situ, but have been boarded up. The boarding structure is visible at street level from the Western Forecourt. These rooms also contain fireplaces and chimneys, but the mantelpieces have been removed. Contemporary services have also been attached to the ceilings.

Bathrooms are contained within the rooms abutting the southern portion of the corridor. Due to lack of waterproofing, these are in a dilapidated state. The area is filled with debris and tiles have been broken and degraded.



**Figure 303. Internal view of stairs leading out of the Basement level.**<sup>323</sup>



**Figure 304. Internal view of stairs looking down towards the basement level.**<sup>324</sup>



**Figure 305. Internal view of Basement level office.**<sup>325</sup>

<sup>323</sup> Artefact, 2021

<sup>324</sup> Artefact, 2021

<sup>325</sup> Artefact, 2021

### 3.11.3 Statement of Significance

The West Wing Extension has historic significance as part of the original design and development of the third Sydney Terminus, completed in 1906. The extension also holds associative significance for its association to Engineer-in-Chief, Henry Deane and Government Architect, Walter Liberty Vernon, and their works.

The West Wing Extension exhibits aesthetic significance, through its complex neo-classical architectural style as constructed in brick. This use of brick represents the hierarchy of material which was a common philosophy of the time. Thus, the type of material used on the Extension hold aesthetic and historic significance and is representative of the innovative urban and architectural design implemented at the Sydney Terminal.

The Extension exhibits further historic significance in accommodating various services. In 1914, the West Wing was home to the telegraph office. This office was imperative to the function of the railway network. Thus, the West Wing holds historical significance pertaining to this use. The West Wing also had a VIP waiting room, which represented historical class distinctions. The West Wing is also historically significant for its association with NSW Governors and the Royal Visit 1954, which used the 'Governor's Archway' and throughlink from Platform 1.

Moreover, the West Wing Extension proves to be a vital pedestrian thoroughfare for commuters since its inception. For this reason, the Wing holds social significance.

The West Wing Extensions has been assessed as having state heritage significance.

### 3.11.4 Gradings of Significance and Integrity

**Table 27: Gradings of Significance and Integrity for the West Wing Extension**

Element	Grading	Integrity
<b>West Wing Extension Overall</b>	High	High
<b>WEST WING EXTENSION</b>		
<b>Views and Vistas</b>	High	Moderate
<b>Context and Setting</b>	High	Moderate
<b>Façades</b>	Exceptional	High
<b>Roof</b>	Moderate	Unknown
<b>Governors Entry (Ambulance Entry)</b>	High	High
<b>Chimneys, Fireplaces</b>	High	Moderate
<b>Service Stairs &amp; Connections to Subway Passage System</b>	Moderate	Low
<b>Doors, Windows and Hardware</b>	High	High
<b>Floors and Paving</b>	Little	Low
<b>Ceilings and Cornices</b>	High	N/A
<b>Skirtings, Architraves and Linings</b>	High	N/A



Element	Grading	Integrity
Fireplaces	High	High
Natural Ventilation	High	Moderate
Applied Finishes	Little	N/A
Fitout	Little	N/A
Introduced Services; Mechanical, Electrical, Lighting & Data	Intrusive	N/A
<b>LOCKER ROOM</b>		
Doors, Windows and Hardware	High	High
Floors and Paving	Little	N/A
Ceilings and Cornices	High	High
Skirtings, Architraves and Linings	High	High
Fireplaces	High	High
Natural Ventilation	High	Unknown
Applied Finishes	Little	N/A
Fitout	Little	N/A
Introduced Services; Mechanical, Electrical, Lighting & Data	Intrusive	N/A
<b>PLATFORM OFFICES</b>		
Acid Etched Glass Windows	High	High
Doors, Windows and Hardware	High	High
Floors and Paving	Little	Low-N/A
Ceilings and Cornices	High	Moderate
Skirtings, Architraves and Linings	High	Moderate
Fireplaces	High	High
Natural Ventilation	High	Unknown
Applied Finishes	Little	N/A
Fitout	Little	N/A
Introduced Services; Mechanical, Electrical, Lighting & Data	Intrusive	N/A
<b>MEZZANINE LEVEL</b>		
Skylights	Exceptional	Moderate
Windows	High	High
Doors and Hardware	High	High

Element	Grading	Integrity
Timber Floors	High	High
Tiling and Paving	High	High
Ceilings and Cornices	High	high
Skirtings, Architraves and Linings	High	High
Fireplaces	High	High
Natural Ventilation	High	High
Applied Finishes	Little	N/A
Introduced Services; Mechanical, Electrical, Lighting & Data	Intrusive	N/A

### 3.11.5 Site Specific Conservation Policies

**Table 28: Site Specific Conservation Policies for the for the West Wing Extension**

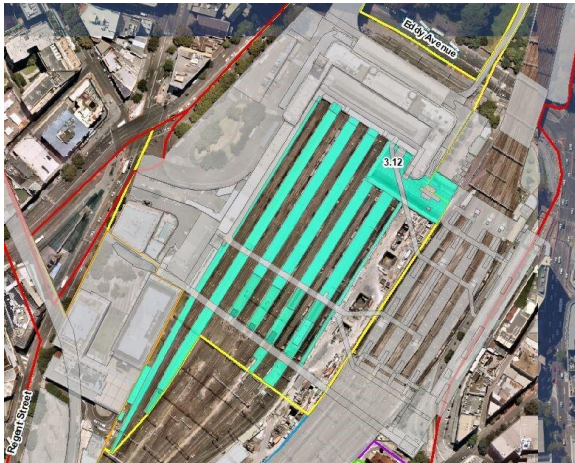
Policy	Policy Discussion and Guidelines
1. Follow Overall Conservation Policies	See Section 15 of this CMP for Conservation Policies
2. Implement Fabric Guidelines	See Section 15.4.4 of this CMP
3. Item Specific Policy: Heritage Process	<p>When considering change follow the Heritage Flow Charts that form Appendix C of this CMP:</p> <ul style="list-style-type: none"> <li>A) Major Works Heritage Process Flow Chart; or</li> <li>B) Minor Works Heritage Process Flow Chart.</li> </ul>
4. Item Specific Policy: Heritage Listing	<p>See Section 3 in this CMP for Legislative Context, and Section 14.4 for Statutory Context.</p> <p>Ensure appropriate statutory approvals or exemptions are obtained prior to change on the site because:</p> <ul style="list-style-type: none"> <li>• The site is within the overall listing for <b>Central Station</b> on the State Heritage Register under the <i>NSW Heritage Act 1977</i> for Sydney Terminal and Central Railway Stations Group.</li> <li>• The site is within the overall listing of the <b>Central Railway Station and Sydney Terminal Group</b> on Transport Asset Holding Entity's (TAHE) Heritage and Conservation Register under Section 170 of the <i>NSW Heritage Act 1977</i>.</li> <li>• The area is within the Railway Square/Central Station Special Character Area under the Sydney DCP 2012.</li> </ul>

Policy	Policy Discussion and Guidelines
5. Item Specific Policy: Masonry Conservation	<p>Conservation of the sandstone and brick facades should be undertaken as part of the cyclical maintenance of the building. Particular care and inspection should be undertaken for any projecting or decorative elements (such as parapets, dentils and mouldings) which has a higher risk of public injury. These should be regularly inspected to make sure the risk of falling is minimised and mitigated.</p> <p>A stone and brick strategy for the facades should be prepared to guide the conservation of the masonry.</p> <p>A strategy for the protection of the masonry facades during any major works should be undertaken by heritage architectural specialists and qualified stonemasons with heritage experience prior to the commencement of any works.</p>
6. Item Specific Policy: Fabric	<p>Specific elements for conservation include:</p> <ul style="list-style-type: none"> <li>• Sandstone and brick facades</li> <li>• Timber window and door joinery</li> <li>• Glass brick pavement lights (consider removing covers and undertake required repairs and conservation works)</li> <li>• Coffered plaster ceilings</li> <li>• Timber boarded ceiling to Governor's Archway and in some platform level rooms</li> <li>• Original natural ventilation ducts</li> <li>• Timber skirtings</li> </ul>
7. Item Specific Policy: Development	<p>Prior to the commencement of any proposed development or works, photographic archival records of the West Wing Extension should be undertaken.</p> <p>Ensure the original external colour schemes and finishes are documented and archivally recorded.</p>
8. Item Specific Policy: Alterations and Additions	<p>Alterations and additions to the West Wing Extension should be avoided. Do not paint the sandstone or masonry facades.</p> <p>Changes to external colours and finishes of window/door joinery, steelwork or roofing material should be sympathetic to the heritage building and consider where possible the implementation of original or early schemes. If changes to the external colours occur, this should be implemented consistently across all façades and roof areas of the Main Terminus Building.</p>
9. Item Specific Policy: Vertical additions	<p>Vertical additions are not permitted above the West Wing Extension.</p>

Policy	Policy Discussion and Guidelines
10. Item Specific Policy: Internal Fitout	<p>During the design of new internal fitout ensure that past intrusive fabric and structure is removed.</p> <p>In particular remove false ceilings wherever possible to reveal the original ceilings. Design new service routes so that they have minimal impact and allow for ease of maintenance and change.</p> <p>Minimise mezzanine areas so that the sense of the original height of each area can be appreciated at the main entrance.</p> <p>Finish mezzanine floor away from exterior walls so that impact on the original fenestrations is minimised.</p> <p>Carefully design new elements so that they are either concealed, or present as a coordinated collection of contemporary minimalist elements and services.</p> <p>Base the design of contemporary fittings such as lighting and seating on the original traditional prototypes.</p> <p>An interior fit-out guide for the offices should be prepared to help guide future fit-outs and to provide a level of consistency across the spaces in their treatment and protection of the heritage fabric and spaces.</p>
11. Item Specific Policy: Use	<p>Continue to use the area as railway related uses, particularly country train passenger amenities and administration. Ensure the sense of progression from the main concourse to the platforms is maintained.</p>
12. Item Specific Policy: Interpretation	<p>See Sections 9 and 14.8 'Heritage Interpretation' in this CMP.</p> <p>Prepare a heritage interpretation strategy, based on the precinct-wide Central Precinct Heritage Interpretation Strategy, specific to the West Wing Extension as part of Central Precinct. Particular focus should be given to the former landscape and cultivation of the space.</p> <p>Following this, develop and implement a detailed site-specific heritage interpretation plan, aligned with the themes and recommendations of the heritage interpretation strategy.</p>
13. Item Specific Policy: Vistas	<p>Ensure vistas to the West Wing Extension outlined in Section 8.6 of this CMP are not obscured. These include vistas from:</p> <ul style="list-style-type: none"> <li>• Western Forecourt</li> <li>• Parcels Area</li> <li>• George Street</li> <li>• Lee Street</li> <li>• Former Parcels Post Office</li> <li>• Platforms 1-5</li> <li>• Grand Concourse</li> </ul>
14. Item Specific Policy: Intrusive fabric.	<p>Demolition of, or changes to, the existing intrusive fabric such as building services should be encouraged and allowed without formal approval provided no significant fabric is impacted on by the change.</p>
15. Item Specific Policy: Modern fittings	<p>Do not clutter the West Wing Extension with new elements. Carefully design new elements so that they are either concealed, or present as a coordinated collection of contemporary minimalist elements and services.</p> <p>Base the design of contemporary fittings such as lighting and seating on the original traditional prototypes.</p>
16. Item Specific Policy: Short term uses and changes	<p>Do not allow negative impacts on significant fabric, spatial qualities and setting for short term gain. E.g. commercial or advertising signage.</p>



## 3.12 COUNTRY AND INTERSTATE PLATFORMS



**Figure 306. Country and Interstate Platforms.**<sup>326</sup>



**Figure 307. Country and Interstate Platforms.**<sup>327</sup>

### 3.12.1 History of the Country and Interstate Platforms

Located to the south of the Main Terminus Building, the Country and Interstate Platforms were built on the area of the former Devonshire Street Cemetery. The Cemetery was established in 1820 but due to overcrowding, new burials were stopped, and the cemetery was officially closed by 1867. The exhumation and relocation of the bodies occurred in 1901, in preparation for the construction of the new (third) Sydney Terminus, which was to be located on this site. The first and second Redfern Stations were located to the south of the Devonshire Street Tunnel, however these were demolished to make way for the expansion of the Sydney Yards and construction of the third Terminus.

In 1906, the third Central Station Terminus was officially opened. It included thirteen terminus platforms, two horse and carriage platforms (to the far east) and four short dock platforms. The dock platforms were located to the west of Platform 1 and were used for loading parcels, mail, and luggage. It was intended that the station's platforms would accommodate up to 40,000 people, which was double the number of passengers that the previous station could accommodate. The platforms lengths were designed specifically to withstand this capacity.

In the years following the establishment of the third Sydney Terminus, commuter traffic via rail significantly increased. This resulted in horse and carriage transport being slowly being phased out with the designated carriage platforms being converted into Platforms 14 and 15 by 1914. Platform 1 was designated for the arrival and departure of important express and special trains, including trains which carried the NSW Governor. To accommodate the increasing length of trains, Platform 1 was first extended towards the Sydney Yard in October 1937 and then again in 1949. In the 1960s, the introduction of diesel hauled locomotives for the use of country service trains required that the platform be extended a final time, to a length of 370 meters, which has remained its length to this day. At this time, Platforms 2/3 were also lengthened to 324 meters. Despite electrification of the lines commencing in the 1930s, the Country Platforms were not electrified until the 1950- 1960s. Due to the installation of escalators to the Platforms Access Area in 1980, the northern end of Platforms 12/13 and 14/15 were truncated.

In order to provide light and ventilation to the Devonshire Street Subway, the Terminus platforms originally had skylights and vents installed to their southern end. The Subway was built in 1906,

<sup>326</sup> Artefact, 2021

<sup>327</sup> Artefact, 2021

coinciding with the construction of the Main Terminus Building. Due to the remodelling of Platforms 1 and 2/3 in 1965, the two skylights at the end of the platforms were demolished. The skylights on Platforms 8/9 and 10/11 remained in-situ until the late 1990s, however they were removed as part of the extension of the platforms prior to the 2000 Olympics.

The Terminus also had a room that was used for cleaning water bottles and glasses. This was opened in 1913 and was used by the porter and carriage cleaners, who would prepare the carriages departing from Central Station. The bottle room was located on the outer end of Platforms 10 and 11 and sat adjacent to the carriage cleaner's store. It was used to fill clean bottles with cold water, which were then distributed throughout the carriages. A water bottle and two glasses were assigned to each train compartment.

Due to the high cost of importing steel, timber was originally used to construct the platform awnings. These were then clad with corrugated iron. Although Platforms 1-15 had a uniform construction, only Platforms 4-15 still retain their original materiality. The awning on Platform 1 was replaced in the 1960s and features a cantilevered awning which is anchored into the West Wing Building. The roofing on the Platform 1 awning and on Platforms 2/3 was replaced in the 1990s, resulting in the removal of original fabric on Platform 2/3. 'Hand' signs were originally painted onto each platform awning's valance and were used to alert steam locomotive train drivers on where to stop. The colour scheme of the awning structure for each platform also varied and it is speculated that these colours were incorporated into staff uniforms.

Facilities including the ticket stalls and an Inquiry and Telegraph Booth were located within the platform area to ensure the Main Concourse space remained unobstructed. While the ticket stalls were placed between Platforms 6/7 and 8/9, the Inquiry and Telegraph Booth was located between Platforms 2/3 and 4/5. As part of the original scheme, each platform had its own ticket barrier, which was located in the area just before the track terminated. In the late 20th century, the ticket barrier was reconfigured to be in line with the concessions in the Grand Concourse. This ticket barrier is automated, with Opal card readers and is shared by all platforms, except for Platforms 1 and 2/3, which exits straight onto the Grand Concourse and is serviced by Opal card reader posts.

The Olympic Enhancement Project commenced in 1998 in preparation for the Sydney 2000 Olympics and resulted in major modifications to the platform area. As part of the works the small concourse at the northern end of platforms 11-14 was extended, to accommodate this, the northern end of Platforms 8/9 and 10/11 were truncated. As part of the works original buffer stops which were moved southwards and newly installed paving and balustrades were designed to match the existing. Multiple platforms and their associated awnings were also extended southwards, including Platforms 8/9, 10/11, 12/13 and 14/15. The supporting walls of the extended platform area was constructed from concrete, while brick paving was used to match the existing older sections.

The platforms were originally designed with baggage and goods lifts which connected to the network of baggage tunnels below, however these lifts were decommissioned c.2000. These lifts were removed and replaced by stairs which connected to the subway network on Platforms 4 to 7. At this time, the network of subterranean tunnels was opened for the first time to pedestrian use, to provide cross Station access to different platforms and areas of the Station. On Platforms 1, 2/3, the goods lifts were retained (Lifts 1 and 2) at the southern end of the platforms and were used by CountryLink catering services.

Men's toilet facilities were built underneath the platform access area in 1906. However, in 2006, these were relocated to a section of the Ladies Room, at the main concourse ablution area.



Figure 308: Country and Interstate Platform 3, date unknown. <sup>328</sup>



Figure 309: Country and Interstate Platform historic timber seating, c. 1951. <sup>329</sup>

<sup>328</sup> Sydney Trains archives, date unknown

<sup>329</sup> Sydney Trains archives, c.1951





Figure 310: Country and Interstate Platform, date unknown. <sup>330</sup>



Figure 311: Royal Tour, with Royal train parked on Platform 1, c1954. <sup>331</sup>

<sup>330</sup> Flickr, date unknown.

<sup>331</sup> Sydney trains archives, c1954



### 3.12.2 Site Description and Condition

The Country and Interstate Platforms are located to the south-west of the concourse of the Main Terminus, to the north-west of the Sydney Electric Platforms, to the north-east of Sydney Yard, and to the south-east of the West Wing Extension. Platform 1 to the north-west runs along the south-eastern elevation of the West Wing Extension.

The Country and Interstate Platforms area currently comprises five double platforms (Platforms 2-11) and one single platform (Platform 1), servicing thirteen tracks numbered west to east. Previously, this area contained seven double platforms and one single platform, but while Platforms 1-11 are still extant, Platforms 13-15 have been demolished as part of the Central Station Metro (CSM) works. The north-western side of Platform 12, however, has been retained as part of the works. Platforms 13-15 are currently being reconstructed and will service automated Metro tracks. The platforms extend from the northern side of Sydney Yards in the south-west to the Platform Access Area to the south of the Grand Concourse in the Main Terminus Building in the north-east. Platforms 1-3 service the CountryLink and interstate services, while Platforms 4 to 12 currently service suburban services. The existing layout of the platforms is similar to the original layout of 13 rail platforms and two horse and carriage platforms, despite 20th and 21st century alterations. The majority of the elements of the platforms are in good or fair condition, although the brickwork on some of the brick storage huts at the south-western ends of the platforms appear to be in fair to poor condition.

#### Platforms

Platforms 2 to 13 of the Country and Interstate platforms were originally constructed as terminal island rail platforms, while Platform 1 was constructed as a terminal side rail platform,<sup>332</sup> exhibiting a retaining wall running perpendicular to each platform at the north-eastern end, forming the Platform Access Area. Platform 14/15 were originally island horse and carriage platform. Although Platforms 4/5 and 6/7 have retained their c.1906 configurations, Platforms 1 and 2/3 include mid-20th century extensions, Platforms 8/9, 10/11 and 12 display c.1990s extensions and Platforms 13 and 14/15 have been demolished.

The original lengths of Platforms 1 to 12 exhibit straight earth-filled platforms faced with English-bonded face-brick walled platforms with vertical profiles and corbelled brick copings. The c.1906 corbelled brick copings are constructed of header courses set on their sides, but the top one or two courses tend to have been removed or rendered and overlaid by concrete slabs laid with tiles dating to the 1990s. Both the original sections and extensions exhibit the 1990s concrete capping and brown parquet patterned tiles with cream straight-stacked tile edges beyond the blue and yellow tactiles. The side platform (Platform 1) exhibits a number of extensions dating to the early c.1940s and the c.1950s, comprising vertical English bonded curved platform extensions generally with corbelled brick coping, although one section in the vicinity of the replica train carriages for the YHA exhibits a concrete capping. Platform 2, also extended in the c.1950s, exhibits a recessed English-bonded brick vertical platform extension with a cantilevered concrete coping and an abutting English bonded staircase at the south-western end of the platform, cut in two to allow for a overhead wiring structure (OHWS) portal. The south-western ends of Platforms 4/5 and 6/7 exhibit c.1906 English-bonded liver coloured brick skylight structures with segmental brick arches and sandstone coping, originally installed to provide light into the Devonshire Street Tunnel. The skylights are currently covered with corrugated iron sheeting and the arches are infilled. The brick and earth access ramps flanking the sides of the skylight structures exhibit late 20th century rendered capping and c.2020 stainless steels stairs. The c.1990s extensions at the south-western ends of Platforms 8/9, 10/11 and 12 feature

<sup>332</sup> Australian Museum Consulting 2015. *Heritage Platforms Conservation Management Strategy*. Report prepared for Sydney Trains, May 2015. p. 11.

concrete-faced recessed extensions and projecting concrete capping. Each platform consists of contemporary furniture, services and signage.



Figure 312. View of Platform 1 buildings.<sup>333</sup>



Figure 313. View looking out from Platform 1.<sup>334</sup>



Figure 314. View looking out from Platform 1.<sup>335</sup>



Figure 315. View looking down the platform.<sup>336</sup>

<sup>333</sup> Artefact, 2021

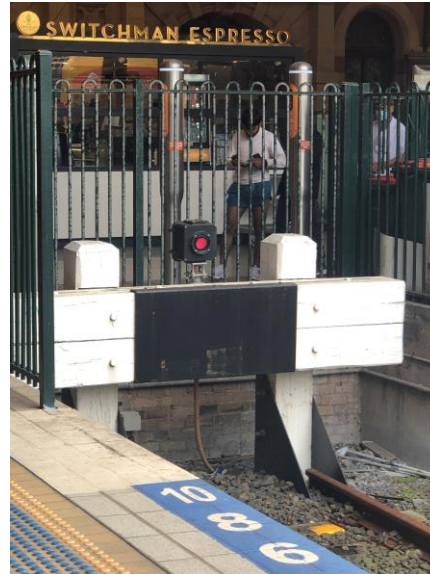
<sup>334</sup> Artefact, 2021

<sup>335</sup> Artefact, 2021

<sup>336</sup> Artefact, 2021



**Figure 316. View of the end of the Country and Interstate Train Platforms.<sup>337</sup>**



**Figure 317. View of the end of the Country and Interstate Train Platforms.<sup>338</sup>**



**Figure 318. View from above of the Country and Interstate platforms.<sup>339</sup>**



**Figure 319. View of end of platform building facing down..<sup>340</sup>**

## Platform Structures

Abutting the skylight structures on Platforms 4/5 and Platforms 6/7 are rectilinear liver coloured brick huts constructed from stretcher-bonded cavity brickwork, with terracotta Marseille roof tiles lining gabled roofs. Platform 8/9 also exhibits a slightly larger c.1930s liver coloured brick cavity walled gable-roofed hut, but a c.2002 extension abuts the south-western elevation, constructed in a similar style to the c.1930s detailing. The huts on Platforms 6/7 and 8/9 exhibit square-headed doors with soldier coursed lintels and timber-panelled doors. These two huts also feature double-hung timber-framed windows with square-headed soldier coursed lintels and chamfered brick sills. The hut on Platform 4/5 exhibits a wider timber-panelled door that was inserted into the north-eastern elevation during the second half of the 20th century, replacing what was likely a smaller, soldier-course linteled door. The installation of the door resulted in the removal of brickwork and the replacement of brickwork with contemporary blonde brickwork along the lintel. The brick hut on Platform 10/11 is also a c.1940s rectilinear stretcher-bonded brick hut with cavity walls with double-hung sash windows with chamfered sills and timber-panelled doors, but exhibits an additional dado soldier course of brick, a

<sup>337</sup> Artefact, 2021

<sup>338</sup> Artefact, 2021

<sup>339</sup> Artefact, 2021

<sup>340</sup> Artefact, 2021



cement-rendered stringcourse above the top of the windows and doors and a metal-clad hipped roof and timber-boarded soffit. The Platform 10/11 hut was extended to the south-west in cream brickwork in the c.1980s, replicating the earlier c.1940s building. The roof of the Platform 10/11 was replaced, and minor refurbishment works to the other huts were completed in c.2000 and are used for storing cleaning equipment.

Two rectilinear brick storage buildings are located near the south-western end of platform 2/3, under the awning level. The northern of two exhibits a stretcher bonded cavity construction and the southern is laid in English bond. Both storage structures likely date to the second half of the 20<sup>th</sup> century. The structures exhibit timber-framed doors.

As the platforms were originally built for steam locomotives, concrete water column plinths have been retained in situ between the tracks and currently support the overhead catenaries. The tracks along Platforms 1 to 10 incorporate central run-around tracks, used for off-peak rail carriage storage. The majority of the hardwood buffers at the termination (north-eastern end) of Platforms 1-14 are not original, but serve as a safety device to prevent trains from running into the main concourse.



Figure 320. View of the roof structure on platform 5.<sup>341</sup>

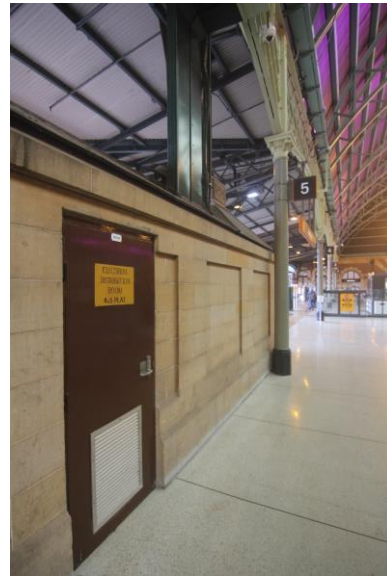


Figure 321. View of the roof and awning structure at Platform 5.<sup>342</sup>



Figure 322. View of the awning structure at Platform 5.<sup>343</sup>



Figure 323. View from Platform 5.<sup>344</sup>

<sup>341</sup> Artefact, 2021

<sup>342</sup> Artefact, 2021

<sup>343</sup> Artefact, 2021

<sup>344</sup> Artefact, 2021





Figure 324. View of roof structure at Platform 5.<sup>345</sup>

### Movable heritage

A number of 'Central' signs at the centre of the platforms, fixed to the underside of the awnings, may date to the original construction of the platforms in c.1906, or at least date to the early 20th century. A clock mounted to the wall of the Terminus Building to the north of Platforms 10/11 may date to c.1906, and may have been originally located on Platform 8/9 as a circular timber bracket along the awning of Platform 8/9 has been retained *in situ*; and its original purpose was as a clock mount.



Figure 325. View of 'Central' station sign.<sup>346</sup>



Figure 326. View of plaque.<sup>347</sup>

### Good Lifts

Good lifts and related infrastructure have been retained in the platform access area between Platforms 7 and 8 and the centre of Platforms 1-7. The lifts on Platforms 1 and 2/3 appear to be operational, servicing the CountryLink catering services within the Parcels Area.<sup>348</sup> The lift on Platform 2/3 is a face-brick construction, constructed in stretcher bond. The lift on Platform 1 is clad in blue-green tiles at platform level and is rendered above the awning. The lift on Platform 4/5 is steel-framed with horizontal timber boarding and vertically-opening steel half-doors at platform level and a corrugated metal clad first-floor with a gable roof. Only the steel-framed, timber clad roof structure remains of the goods lift on Platform 6/7 beneath the awning structure. The brick lifts along the platforms rise two storeys above the level of the platform, above the height of the awnings, with flat

<sup>345</sup> Artefact, 2021

<sup>346</sup> Artefact, 2021

<sup>347</sup> Artefact, 2021

<sup>348</sup> Rappaport Pty Ltd & NSW Government Architect's Office, 2013. 3.12 Country and Interstate Platforms (Platforms 1-15), p. 7.

roofs. The steel structure and some of the horizontal timber board cladding of the lift between Platforms 7 and 8 has been retained for interpretation, but the ground floor lift component has been removed and the floor covered with tiles; this work was likely completed during the c.1990s works. A timber-panelled door has been retained at the first-floor level of this lift.



**Figure 327. View of underside of Goods Lift.**<sup>349</sup>

### Awnings

The original c.1906 gable-ended awnings with exposed timber lattice trusses (Howe trusses) supported by chamfered hardwood timber posts are still partially extant along Platforms 4/5, 6/7, 8/9 and 10/11. The structural elements are fixed with wrought and cast iron brackets and ties, and the roofs comprise c.2000 corrugated metal sheets. Timber valances extend along the sides of the awnings and at the south-western ends of the original awnings on Platforms 4/5 and 6/7, although the saw-like ends of the valances along the sides of Platforms 10/11 and 12/13 and at the gabled ends of Platforms 4 to 9 have been removed. The original end valances have been relocated to the south-western ends of the c.1990s awning extensions on Platforms 8/9 and 10/11. The awning extensions cover the subway tunnel staircases and designed to match the profile of the original c.1906 awnings, but are supported on steel posts and have skylights running along the ridges of the gabled roofs. Cast iron downpipes are fixed to the posts, transferring water from the gutters along each side of the awnings into drains within the platforms. Bolted scarf joints and circular saw marks on the awning posts on Platforms 6/7 and 8/9 may indicated reuse of the awnings, perhaps from the second phase of Sydney Terminal. The c.1960s cantilevered awning on Platform 1 and the self-supporting cantilevered awning on Platform 2/3 exhibit modern steel-framed designs with Clip-Lock corrugated metal sheeting lining and c.1990s corrugated metal roofing.

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<sup>349</sup> Artefact, 2021



**Figure 328. View of awnings and roof structure at Grand Concourse over country and interstate platforms.<sup>350</sup>**



**Figure 329. View of awnings and roof structure of platforms entering Grand Concourse.<sup>351</sup>**



**Figure 330. View of awnings and roof structure at Platform 9.<sup>352</sup>**



**Figure 331. View of awnings and roof structure at Platform 2-3.<sup>353</sup>**

### Platform Access Area and Subterranean Toilets

The Platform Access Area is located between Platforms 1 to 11 and the Grand Concourse, as well as the area between the east wing of the Main Terminus Building and Platforms 12 to 15. A staircase between Platform 11 and Platform 12 provides access to Platforms 16 to 19 and a connection to the ESR Concourse. Stairs and escalators to the south of the eastern wing of the Terminus building provide access to the North Concourse.

Barricades formed by ticket booths, glass barriers, ticket barriers, steel mesh barriers, ticket machines and the information desk and boards, separate the north-western extent of the platform access area from the Grand Concourse at the platform level. This area is additionally visually separate from the Grand Concourse by a decorative wrought iron lattice truss on cast iron columns, which supports the barrel-vaulted roof of the Grand Concourse. The roof structure of the platform access area consists of a steel truss-framed cantilevered awning and gables above the platform awnings, supported by the

<sup>350</sup> Artefact, 2021

<sup>351</sup> Artefact, 2021

<sup>352</sup> Artefact, 2021

<sup>353</sup> Artefact, 2021



cast iron columns along the border between the concourse and platform access area. The roofing is clad with corrugated steel. The gables at Platforms 1 and 2/3 are glazed.

Two c.1906 rectilinear ashlar sandstone structures are located between Platforms 5 and 6 within the Platform Access Area, including a former substation and the current ticket office. The former substation features a pressed metal hipped roof with two vertical steel ventilation shafts and two late 20th century timber doors along the north-eastern elevation. The roof is also punctuated by service pipes and downpipes. The ticket office incorporates c.1906 ashlar sandstone walls in its otherwise contemporary design, surmounted by the electronic boards. The ashlar sandstone walls of the ticket office formerly disguised a stair access to a grand arched entrance leading to a c.1906 men's toilet beneath the level of the country platforms. The arch has been partially infilled. The existing staff toilets with a contemporary fit-out may reuse the space of the c.1906 toilets, but it is not clear.



Figure 332. Former Lavatory signage.<sup>354</sup>

### 3.12.3 Statement of Significance

The Country and Interstate Platforms are of state heritage significance. Located north of the Devonshire Street Tunnel, the Country and Interstate Platforms were built on the area of the former Devonshire Street Cemetery. The Cemetery dates to 1820 and in 1901, bodies were exhumed in preparation for the construction of the third station. Despite this, there is still a high potentiality for archaeological remains to be present. As a result of this the Country and Interstate Platforms hold high research potential.

The site exhibits historic significance forming part of the original design and development of the third Sydney Terminus, completed in 1906. The platforms have aesthetic significance in their layout representing the original 1906 platform and awning designs, as well as some of the original fabric present, which remains in situ.

The platforms have historical significance for their contribution to the country and interstate railway system. The platforms also hold historic and representative significance in their representation of NSW railway technical advancements and infrastructure, including their original use for steam and diesel locomotives before the electrification of the train lines.

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<sup>354</sup> Artefact, 2021



The Country and Interstate Platforms have a high level of social significance for their continued operation as commuter platforms since 1906. Furthermore, as the platforms were initially primarily used for interstate and country travel, they hold social significance for the many commuters who have used the platforms to travel and/or farewell loved ones. The platforms still hold social significance to commuters today.

### 3.12.4 Gradings of Significance and Integrity

**Table 29: Gradings of Significance and Integrity for the Country and Interstate Platforms**

Element	Grading	Integrity
<b>Country and Interstate Platforms Overall</b>	High	High
<b>ELEMENT</b>		
<b>Platforms and Paving</b>	Little	Little
<b>Platforms 1-3 Awnings and Columns</b>	Little	N/A Non-original c.1960s fabric
<b>Platforms 4-7 Awnings, Skylights, Columns and Trusswork</b>	High	High
<b>Platforms 8-15 Awnings, Skylights, Columns and Trusswork</b>	High	High
<b>Lattice screens separating platforms from main concourse</b>	High	High
<b>Platform Roof Connections to Platform Access</b>	High	High
<b>Platform Goods Lifts</b>	High	High
<b>Original Platform Lift Mechanism</b>	High	High
<b>Skylights and Brick Huts (Platforms 4- 7)</b>	High	High
<b>Brick Stores (Platforms 8-11)</b>	Moderate	Moderate
<b>Information Boards, Vending Machines, Signage &amp; Wayfinding</b>	Little	N/A
<b>Platform Furniture</b>	Little	N/A
<b>Introduced Services; Mechanical, Electrical, Lighting &amp; Data</b>	Intrusive	N/A
<b>Platform 10/11 Clock</b>	High	High
<b>PLATFORM ACCESS AREA</b>		
<b>Floors and Paving</b>	Little	N/A
<b>Awning</b>	Little	Moderate
<b>Cast Iron Columns</b>	Exceptional	High Original to Grand Concourse barrel vaulted roof

Element	Grading	Integrity
Decorative Lattice Metalwork	Exceptional	High Original to Grand Concourse barrel vaulted roof
Contemporary Barriers	Little	Little
Former Sandstone Ventilator to Men's Latrines	High	High
Stairs to Lower Toilets	Moderate	Moderate

### 3.12.5 Site Specific Conservation Policies

**Table 30: Site Specific Conservation Policies for the for the Country and Interstate Platforms**

Policy	Policy Discussion and Guidelines
1. Follow Overall Conservation Policies	See Section 15 of this CMP for Conservation Policies.
2. Implement Fabric Guidelines	See Section 15.4.4 of this CMP
3. Item Specific Policy: Heritage Process	<p>When considering change follow the Heritage Flow Charts that form Appendix C of this CMP:</p> <p>A) Major Works Heritage Process Flow Chart; or B) Minor Works Heritage Process Flow Chart.</p>
4. Item Specific Policy: Heritage Listing	<p>See Section 3 in this CMP for Legislative Context, and Section 14.4 for Statutory Context.</p> <p>Ensure appropriate statutory approvals or exemptions are obtained prior to change on the site because:</p> <ul style="list-style-type: none"> <li>The site is within the overall listing for <b>Central Station</b> on the State Heritage Register under the <i>NSW Heritage Act 1977</i> for Sydney Terminal and Central Railway Stations Group.</li> <li>The site is within the overall listing of the <b>Central Railway Station and Sydney Terminal Group</b> on Transport Asset Holding Entity's (TAHE) Heritage and Conservation Register under Section 170 of the <i>NSW Heritage Act 1977</i>.</li> <li>The area is within the Railway Square/Central Station Special Character Area under the Sydney DCP 2012.</li> </ul>
5. Item Specific Policy: Masonry Conservation	<p>Conservation of the sandstone and brick facades should be undertaken as part of the cyclical maintenance of the building. Particular care and inspection should be undertaken for any projecting or decorative elements (such as parapets, dentils and mouldings) which has a higher risk of public injury. These should be regularly inspected to make sure the risk of falling is minimised and mitigated.</p> <p>A stone and brick strategy for the facades should be prepared to guide the conservation of the masonry.</p> <p>A strategy for the protection of the masonry facades during any major works should be undertaken by heritage architectural specialists and qualified stonemasons with heritage experience prior to the commencement of any works.</p>

Policy	Policy Discussion and Guidelines
6. Item Specific Policy: Fabric	<p>Specific elements for conservation include:</p> <ul style="list-style-type: none"> <li>• c.1906 timber awning structures on platforms, including posts, truss work and valances</li> <li>• Extant disused goods/baggage lifts</li> <li>• Cast iron columns at the northern end of the platforms supporting the Platform Access Area's awning</li> <li>• c.1906 brick platform structures</li> <li>• Extant skylights and related structures to Devonshire Street Tunnel</li> <li>• Early to mid 20th Century brick huts at southern end of platforms, including Marseille tiled roofs</li> <li>• Extant sandstone substation in Platform Access Area</li> <li>• Central Station signs</li> </ul>
7. Item Specific Policy: Reconstruction	<p>Consider the re-instatement of original detailing including the following:</p> <ul style="list-style-type: none"> <li>• asphalt paving</li> <li>• skylights to the Devonshire Street Tunnel</li> <li>• valance boards (where damaged or missing)</li> <li>• original awning on Platform 1, which minimises the obstruction of views to the eastern façade of the West Wing Extension</li> </ul> <p>If dictated by functional requirements develop an appropriate modification that embraces the design intent of the original.</p>
8. Item Specific Policy: Development	<p>Prior to the commencement of any proposed development or works, photographic archival records of the Country and Interstate Platforms should be undertaken.</p> <p>Ensure the original external colour schemes and finishes are documented and archivally recorded.</p>
9. Item Specific Policy: Alterations and Additions	<p>Alterations and additions to the Country and Interstate Platforms should be avoided. The Country and Interstate Platforms should be conserved and continue to be understood as open air spaces with awning shelters. Development above the platform are is not appropriate.</p> <p>If additional canopies are required, they should be sympathetic to the scale, materiality and profile of the existing canopies on each platform, in particular the c.1906 platform awnings.</p> <p>Additional platform buildings should be avoided where possible to minimise the visual clutter on the platforms, retain vistas to the canopy structures, and retain vistas across the platforms.</p> <p>Do not paint the sandstone or masonry facades of the platform structures or above platform buildings.</p> <p>Changes to the external colours and finish of the timber and cast-iron structures associated with the platform awnings should be sympathetic to the heritage of the fabric and consider where possible the implementation of original or early schemes. If changes to the colours occur this should be implemented consistently across all platforms.</p>
10. Item Specific Policy: Use	<p>Continue to use the area as passenger rail platforms.</p> <p>Ensure the sense of progression from the Grand Concourse to the platforms is maintained.</p>
11. Item Specific Policy: Intrusive fabric.	<p>Demolition of, or changes to, the existing intrusive building services should be allowed without formal approval provided no significant fabric is impacted on by the change.</p>

Policy	Policy Discussion and Guidelines
12. Item Specific Policy: Modern fittings	<p>Do not clutter the platforms with new elements. Carefully design new elements so that they are either concealed, or present as a coordinated collection of contemporary minimalist elements and services.</p> <p>Base the design of contemporary fittings such as lighting and seating on the original traditional prototypes.</p>
13. Item Specific Policy: Short term uses and changes.	Do not allow negative impacts on significant fabric, spatial qualities and setting for short term gain. E.g. commercial or advertising signage.
14. Item Specific Policy: Lighting	Lighting across the Country and Interstate Platforms should be consistent with the guidelines outlined in the <i>Central Station Lighting Strategy</i> .
15. Item Specific Policy: Signage	Signage on the Country and Interstate Platforms should be consistent with required wayfinding and statutory signage as outlined in the <i>Central Station Signage Strategy</i> .
16. Item Specific Policy: Interpretation	<p>See Sections 9 and 14.8 'Heritage Interpretation' in this CMP.</p> <p>Prepare a heritage interpretation strategy, based on the precinct-wide Central Precinct Heritage Interpretation Strategy, specific to the Country and Interstate Platforms as part of Central Precinct. Particular focus should be given to the former landscape and cultivation of the space.</p> <p>Following this, develop and implement a detailed site-specific heritage interpretation plan, aligned with the themes and recommendations of the heritage interpretation strategy.</p>
17. Item Specific Policy: Vistas	<p>Ensure vistas to the Country and Interstate Platforms outlined in Section 8.6 of this CMP are not obscured. These include vistas from:</p> <ul style="list-style-type: none"> <li>• The Grand Concourse</li> <li>• Between Platforms 1-12</li> </ul>



### 3.13 SUBWAY PASSAGE SYSTEMS

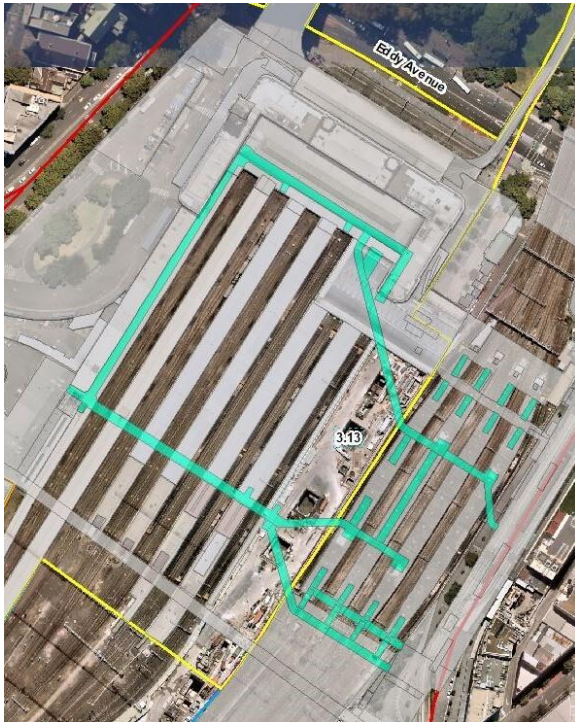


Figure 333. Subway passage system.<sup>355</sup>



Figure 334. Subway passage system.<sup>356</sup>

#### 3.13.1 History of the Subway Passage Systems

It was intended that the 1906 Sydney Terminal would rival the giant railway stations of Europe. This was to be achieved using quality materials, such as sandstone, and innovative design and engineering techniques. The vision of the terminus by Engineer-in-Chief for Railways, Henry Deane, placed focus on the need to provide a safe environment for commuters. As such, the movement around the station was carefully designed to avoid conflict between people and vehicles, with separate entrances designated to pedestrians, trams, trains and vehicles. Deane also proposed that passenger and service activities could be separated through creating a subway system. This system would allow the underground transportation of luggage, mail and other items and ensure the public space at platform level was undisturbed. Hand-pushed trolleys were used to transport the luggage through the subway, while a luggage lift would then be used to transfer it to the relevant platform.

The network of passageways under the platform level for the movement of luggage, mail and parcels was seen as an advanced feature of the station's design. However, due to the deliberate design of the station across multiple levels and the requirement for vertical transportation, such as lifts and escalators to service these levels, construction of the station resulted in enormous costs.

In 1926, on the construction of the Central Electric lines, the northern and southern east-west tunnels were extended to service these platforms.<sup>357</sup> Each platform was fitted with a three-tonne electric goods lift, which was used to convey luggage to a three meter wide baggage subway. This baggage subway then connected to the existing subway network which serviced Platforms 1-12.

<sup>355</sup> Artefact, 2021

<sup>356</sup> Artefact, 2021

<sup>357</sup> *The City Railway* plan dated 29 November 1927.

Passengers could book their luggage to country destinations upon showing their ticket at the Main Terminus Building's Luggage Room or at any suburban station. If the request was lodged at a suburban station, the luggage was transferred to Central by train. Once it arrived, it was unloaded into the subway and then transferred to the appropriate platform via electric lift. Luggage that was not filtered into the subway was transported to the Grand Concourse via a northernmost east-west passageway. The Left Luggage Room at the Main Terminus Building was also accessed by vehicle, via a 'van dock' that was located off Pitt Street.

Due to an increased volume of parcels and mail traffic during the Second World War, the use of hand pushed trolleys through the underground passages became unsustainable. To better manage the traffic, these were replaced with motorised tractors and four-wheel parcel barrows. However, the fumes from the tractor motors, which were converted to Austin motor vehicle engines, produced excessive fumes, rendering them unsuitable for use in the subway, and eventually they were relocated for use on platform level. As the electric lifts could only handle two trolleys at a time, this relocation greatly expedited the handling of luggage and parcels. However, this went against Deane's original intention of creating a safe and efficient public concourse area, as the tractors posed a safety risk to commuters in the Grand Concourse and platform areas. Tractors continued to operate on the concourse level until the 1950s, at which time the motorised tractors were replaced with gas powered tractors, allowing hauling trolleys to continue at platform level.

Various modifications occurred to the Subway Passage System in the late 20th century, as the movement of pedestrians below the platform level became a priority. In 1979, an uninterrupted pedestrian connection between the Grand Concourse and the Eastern Suburbs Railway was established by extending the baggage tunnel under the northern portion of the Main Terminus Building. To link the country platforms to Central Electric, the majority of the southernmost luggage subway tunnels were converted to pedestrian subways. Prior to the conversion, the configuration of most of the tunnels was essentially the same as it was in 1906. However, the works to pedestrianise the space required that the twin trolley track was covered over and that services, such as fire hydrant lines and hose reels, be added. The south tunnel was pedestrianised and extended to connect to the Devonshire Street Tunnel in 1994, as was the north tunnel, which was converted to link the end of Platform 10 to Platforms 16/17, 18/19 and to the Eastern Suburbs Railway Concourse.

### 3.13.2 Site Description and Condition

The Subway Passage System is comprised of a series of underground service, luggage and pedestrian tunnels that are located underneath Platforms 1-23. While Platforms 1-15 are associated with the Main Terminus Building, Platforms 16-23 are associated with Central Electric. The network underneath Platforms 1-15 retains a high level of integrity and dates to 1906. Some of the tunnel areas beneath platforms 13-15 may be impacted by the CMS works to integrate the Metro lines at Central Station. Extensive modifications to the network occurred in the 1920s due the development of Central Electric. Upgrade works were also undertaken in the late 20th Century, resulting in further modifications to the tunnels. However, due to the internal dimensions and fabric used, the extended portions are not easily distinguishable from the original sections. Despite these modifications, some of the services tunnels contain original fabric.

The subway system not only provides access to the platforms above, but also connects to offices, maintenance depots, kitchens and loading docks. As the network was designed to serve the country, interstate and suburban platforms, three main tunnel systems are identifiable. These follows:

- Subways 1 and 4
- Subway 3
- Subway 2

### The north tunnel: Subways 1 and 4

Subway 1 runs continuously from east to west underneath the northern area of Platforms 1-10/11. It is located at the basement level of the Main Terminus Building and connects to the Luggage Dock area (to the west and to the Data Area to the east. Just west of the eastern boundary of Subway 1, Subway 4 branches out to the south. Subway 4 runs southwards underneath Platforms 10/11 for a short distance, before turning eastwards to connect to the Eastern Suburbs Railway (ESR) Concourse.

The north tunnel is portioned off into two separate sections, with the service section to the west and the commuter portion to the east. In the eastward portion of Subway 4, which runs between Platform 16/17 and the ESR Concourse, the southern wall of the tunnel contains stair connections to Platforms 16-19. The tunnel terminates at the junction with the ESR Concourse, which is located below Chalmers Street.

The western portion of the tunnel, which is used for services, retains several original features. This includes the Goods Lift for Platform 8/9 and the connection between Subway 1 and Subway 4, which is located on the southern wall of the tunnel. While two Goods Lift openings remain extant in the tunnel, neither are operational. As well as the shaft, the lift structure remains extant at Platform level.



Figure 335. View of north tunnel.<sup>358</sup>



Figure 336. View of north tunnel.<sup>359</sup>



Figure 337. View of signage in tunnel.<sup>360</sup>

<sup>358</sup> Artefact, 2021

<sup>359</sup> Artefact, 2021

<sup>360</sup> Artefact, 2021

### **The western tunnel: Subway 2**

Subway 2 runs in a north-south direction to the western side of the railway yards. It is located underneath Platform 1 and meets the western ends of Subway 1 to the north and Subway 3 to the south.

While the subway has a role in structurally supporting the West Wing Extension, it also connects to several areas in the basement. This includes the Luggage Dock to the north and Lower Carriage Lane to the south. The entrance to Lower Carriage Lane is located at the intersection between Subway 2 and Subway 3. This area also contains the CountryLink catering area, which is still used for transporting the catering to the CountryLink Train services. The catering is transported to platform level via luggage lifts that service Platforms 1 and 2/3. Unfortunately, this area of the basement is prone to flooding as it is not waterproofed.

### **The south tunnel: Subway 3**

Subway 3 runs continuously from east to west underneath the southern area of Platforms 1-15. At platforms 4/5, the tunnel is partitioned into two sections. The eastern section of the tunnel branches into two tunnels, both of which are used as pedestrian subways, while the western section connects to the southern end of Subway 2 and is used for services.

An original goods lift was located to the northern wall of the tunnel between Platforms 4-7. However, this has been replaced with new stairs that connect the subterranean tunnels to the platforms above. Similarly, the goods lift to Platforms 8-13 has been replaced with stairs to the southern wall of the tunnel.

The eastern portion of the tunnel, which is used for pedestrian activity, divides into two sections underneath Platforms 12/13. The original luggage tunnel is contained within the northern branch, which travels underneath the Central Electric lines to connect to Platforms 16 – 19. Both stairway entrances to the Platforms are located on the northern wall of the tunnel and are contemporary in construction. The stairway leading to Platforms 16/17 is located to the western end of the branch, while the stairway to Platforms 18/19 is located to the east. The stairway to Platform 18/19 blocks off the original north-south subway that connected Subway 3 to Subway 4. To the east of this entry, a section of the tunnel is no longer accessible. This area contains a luggage lift that was used to access Platforms 20/21.

The southern tunnel branch is not a part of the original Subway Passage System but was constructed to extend the tunnel to the ESR Concourse. It contains stairways in its northern wall that connect to Platforms 16-23. Another tunnel was built to the north of this branch to provide further stair connections to Platforms 16-23. This tunnel runs parallel to the southern branch, also contains stairways to its northern wall and connects to the South Concourse at the east.





Figure 338. View of south tunnel.<sup>361</sup>

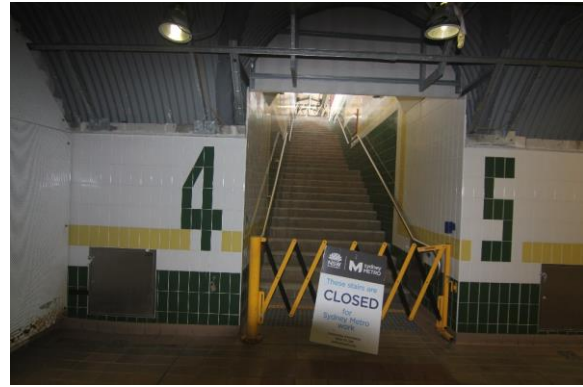


Figure 339. View of south tunnel looking towards platform 4/5.<sup>362</sup>



Figure 340. View of south tunnel.<sup>363</sup>



Figure 341. View of south tunnel.<sup>364</sup>

### Service tunnels: Subway 1, 2 and 3

The areas of the subway tunnels that are reserved for services are not publicly accessible and are instead used for the movement of goods, railway maintenance and passage for railway workers. As this part of the system has not been extensively modified, it remains largely in its original configuration. The tunnels feature concrete floors, concrete vaulted ceiling and exposed service lines to the walls.

### Pedestrian tunnels: Subway 1, 4 and 3

These tunnels are original to their phase of construction, however, they have undergone some modifications. They are of concrete construction and feature concrete vaulted ceiling. . Portions of the tunnels are consistent with the former Central Electric scheme and feature white ceramic wall tiles with a yellow banding and dark green skirting tiles. To the platform entrances, the tiled walls also feature the platform number in a contrasting tile colour.

<sup>361</sup> Artefact, 2021

<sup>362</sup> Artefact, 2021

<sup>363</sup> Artefact, 2021

<sup>364</sup> Artefact, 2021

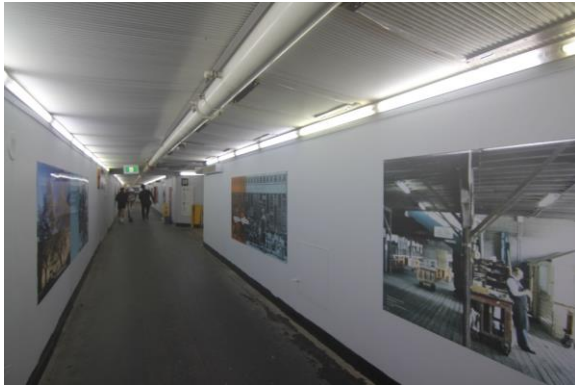


Figure 342. View of temporary pedestrian tunnel.<sup>365</sup>



Figure 343. View of temporary pedestrian tunnel.<sup>366</sup>



Figure 344. View of view pedestrian tunnel.<sup>367</sup>



Figure 345. View of temporary pedestrian tunnel leading to Platforms 1-23.<sup>368</sup>

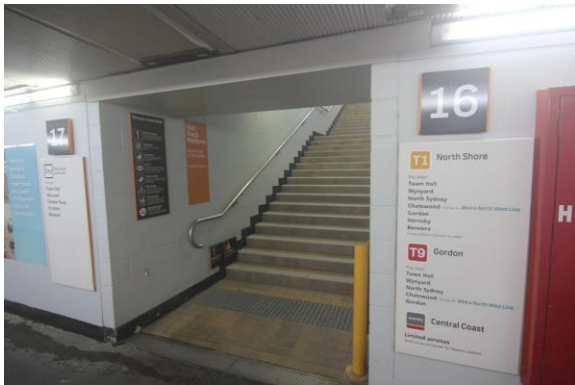


Figure 346. View of pedestrian tunnel looking towards the entry of Platform 16/17.<sup>369</sup>

<sup>365</sup> Artefact, 2021

<sup>366</sup> Artefact, 2021

<sup>367</sup> Artefact, 2021

<sup>368</sup> Artefact, 2021

<sup>369</sup> Artefact, 2021

### 3.13.3 Statement of Significance

The Subway Passage System comprises a series of underground service, luggage and pedestrian tunnels that are located underneath Platforms 1-23. The network underneath Platforms 1-15 remains largely in-situ and dates to 1906. As such, these sections exhibit high historic significance.

Prior to the construction of the third Sydney Terminus in 1906, the land was occupied by the Police Barracks from as early as the 1820s. The Police Barracks were demolished as early as the 20<sup>th</sup> century to make way for the new Central Terminus. Therefore, the site has research potential pertaining to the historic land uses.

The Subway Passage System was part of the overall design for the third Sydney Terminal. The Subway Passage System holds historical associative significance to Henry Deane, who planned the Subway System, along with the rest of the terminus, to organise the movement of people, luggage, and vehicles. This provided a safer environment for commuters. As such, the design of the System has aesthetic significance and rarity value in its innovative urban design. This design was advanced in its features which allowed for the underground transportation of luggage, mail, and other items, as well as ensured the public space at a platform level was undisturbed.

In 1926, the northern and southern east-west underground passage tunnels were extended. These modifications would have resulted in a loss of original fabric and integrity. All platforms also received the addition of a three-tonne electrics goods lift, which conveyed the luggage to a baggage subway. This baggage subway was connected to the existing subway system which served Platforms 1-12. This modification provided further efficacy in luggage travel and handling. For this reason, the subway system holds historical and technical significance in its representation of electric goods systems.

Various modifications work undertaken have resulted in a loss of integrity and some original fabric. However, due to the internal dimensions and fabric used, the extended portions are not easily distinguishable from the original sections. Despite these modifications, some of the services tunnels contain original fabric.

The Subway Passage Systems have been assessed as having state heritage significance.

### 3.13.4 Gradings of Significance and Integrity

**Table 31: Gradings of Significance and Integrity for the Subway Passage Systems**

Element	Grading	Integrity
<b>Subway Passage System Overall</b>	High	Moderate
<b>Luggage Tunnels (Platforms 16-23/1-15)</b>	High	Moderate
<b>Platform Connections to Subways</b>	Moderate	Moderate
<b>Stair Connections to Platforms</b>	Moderate	Moderate
<b>North-South Connection</b>	Moderate	Moderate
<b>East-West Connections</b>	Moderate	Moderate
<b>Paving</b>	Little	N/A
<b>Tiling</b>	Little	N/A
<b>Concrete vaulted ceiling</b>	High	Moderate

Element	Grading	Integrity
Applied Finishes	Little	N/A
Introduced Services; Mechanical, Electrical, Lighting & Data	Intrusive	N/A

### 3.13.5 Site Specific Conservation Policies

**Table 32: Site Specific Conservation Policies for the for the Subway Passage Systems**

Policy	Policy Discussion and Guidelines
1. Follow Overall Conservation Policies	See Section 15 of this CMP for Conservation Policies.
2. Implement Fabric Guidelines	See Section 15.4.4 of this CMP
3. Item Specific Policy: Heritage Process	<p>When considering change follow the Heritage Flow Charts that form Appendix C of this CMP:</p> <p>A) Major Works Heritage Process Flow Chart; or B) Minor Works Heritage Process Flow Chart.</p>
4. Item Specific Policy: Heritage Listing	<p>See Section 3 in this CMP for Legislative Context, and Section 14.4 for Statutory Context.</p> <p>Ensure appropriate statutory approvals or exemptions are obtained prior to change on the site because:</p> <ul style="list-style-type: none"> <li>The site is within the overall listing for <b>Central Station</b> on the State Heritage Register under the <i>NSW Heritage Act 1977</i> for Sydney Terminal and Central Railway Stations Group.</li> <li>The site is within the overall listing of the <b>Central Railway Station and Sydney Terminal Group</b> on Transport Asset Holding Entity's (TAHE) Heritage and Conservation Register under Section 170 of the <i>NSW Heritage Act 1977</i>.</li> <li>The area is within the Railway Square/Central Station Special Character Area under the Sydney DCP 2012.</li> </ul>
5. Item Specific Policy: Use	<p>Continue to use the Subway Passage System for the movement of passengers and ancillary services such as mail, catering and luggage to service the platforms above. Continue to maintain connection with all concourses across the Central Station Precinct.</p> <p>Opportunities to reuse and enhance the existing baggage subway network for use as public pedestrian access across the Station should be explored.</p>



Policy	Policy Discussion and Guidelines
6. Item Specific Policy: Alterations and Additions	<p>Alterations and additions to the Subway Passage System can occur in a sympathetic manner to provide connections and additional access to the tunnel network.</p> <p>Retain the original alignment and configuration of the c.1906 luggage and passenger tunnels. New tunnels or extensions to the tunnels should be easily identified.</p> <p>Alterations to the tunnel fabric, ie. re-lining for the reuse as pedestrian tunnels, should avoid impacting the original fabric. New lining should be separate from the original tunnel surface behind so that it can be removed at a future date without compromising the original appearance of the tunnel.</p> <p>New lighting and services are permissible within the tunnels to ensure the areas are safe and are code compliant.</p>
7. Item Specific Policy: Niches, Cavities, Light Shafts	<p>Retain all existing niches, light well cavities, left shafts and other original features in the wall of the tunnels.</p>
8. Item Specific Policy: Maintenance	<p>Ensure the significant fabric of the area is conserved through a maintenance program, particularly under-used areas and fabric in poor condition.</p>
9. Item Specific Policy: Lighting	<p>Lighting within the Subway Passage System should be consistent with the guidelines outlined in the <i>Central Station Lighting Strategy</i>. Lighting should seek to provide Safety In Design, for users of the area.</p>
10. Item Specific Policy: Short term uses and changes.	<p>Do not allow negative impacts on significant fabric, spatial qualities and setting for short term gain.</p>
11. Item Specific Policy: Service Conduits	<p>Service conduits within the tunnels are not significant and can be replaced. Affix new services in a manner that has minimal impact on significant fabric and spatial qualities.</p> <p>Services should be coordinated and consolidated to minimise long service routes and concealed where possible.</p>
12. Item Specific Policy: Intrusive Elements	<p>Implement change to remove intrusive fabric and visual clutter along the subway passages.</p>
13. Item Specific Policy: Interpretation	<p>See Sections 9 and 14.8 'Heritage Interpretation' in this CMP.</p> <p>Prepare a heritage interpretation strategy, based on the precinct-wide Central Precinct Heritage Interpretation Strategy, specific to the Subway Passage Systems as part of Central Precinct. Particular focus should be given to the former landscape and cultivation of the space.</p> <p>Following this, develop and implement a detailed site-specific heritage interpretation plan, aligned with the themes and recommendations of the heritage interpretation strategy.</p>
14. Item Specific Policy: Archaeology	<p>See Sections 10 and 15.6 in this CMP and Appendix B. Where possible, retain archaeological evidence in situ.</p> <p>If excavation into the tunnel floors is occurring and any archaeological evidence of the early institutional uses of the site is found, notify the NSW Heritage Branch, Department of Environment and Heritage and seek advice from a suitably qualified historical archaeologist prior to work recommencing. Approval pursuant to s57(1) of the NSW Heritage Act will be required if remains need to be removed. Where possible, retain archaeological evidence of the early institutional uses in situ.</p>

## 3.14 DEVONSHIRE STREET TUNNEL



Figure 347. Devonshire Street tunnel.<sup>370</sup>



Figure 348. Devonshire Street tunnel looking east.<sup>371</sup>

### 3.14.1 History of the Devonshire Street Tunnel

An integral part of Henry Deane's urban plan for the development of the new Sydney Terminal was the construction of the Devonshire Street Tunnel. This tunnel was the first subway in Australia and was completed during the first phase of construction of the terminus, from 1903-1906. By transferring the existing Devonshire Street into a subterranean thoroughfare, the existing above ground railway lines were able to continue from the earlier station (south of Devonshire Street) to the new terminus.

During construction of the third terminus, a section of the original alignment of Devonshire Street was excavated. This area was between the Devonshire Street Cemetery, which had been resumed as part of the site works for the new station, and the northern frontage of the second Sydney Station. Excavation of this area allowed for the construction of the subterranean pedestrian tunnel in the original alignment of Devonshire Street. Deane's concept for the tunnel included large skylights on the platform level that would flood the tunnel with natural light. These were constructed at the end of each of the 1906 platforms.

The first modifications to The Devonshire Street Tunnel occurred in the 1920s due to the construction of the Central Electric Station. In order to support the new Central Electric tracks, the roof of the extended subway was constructed from rolled steel joists encased in concrete. As the Central Electric Station required a southern entrance on Chalmers Street, the Devonshire Street Tunnel (which measured 20 feet (6.09 metres) was extended and straightened to connect with the new entrance to the station. The tunnel connected to the Devonshire Concourse (now known as the South Concourse) which was located at the eastern end of the tunnel at the entrance from Chalmers Street. With the tunnel extension completed by 1926, commuters were able to travel in a westward direction from the concourse to Railway Square. This signified a change in pedestrian movement at Central Station, as the tunnels running under the Country and Interstate Platforms and Central Electric Platforms facilitated the travel from an east-west rather than a north-south direction. Due to the establishment of various tertiary institutions to the west of the Central Station Site, the Devonshire Street Tunnel provided a vital pedestrian link that allowed foot traffic to increase throughout the 20th Century.

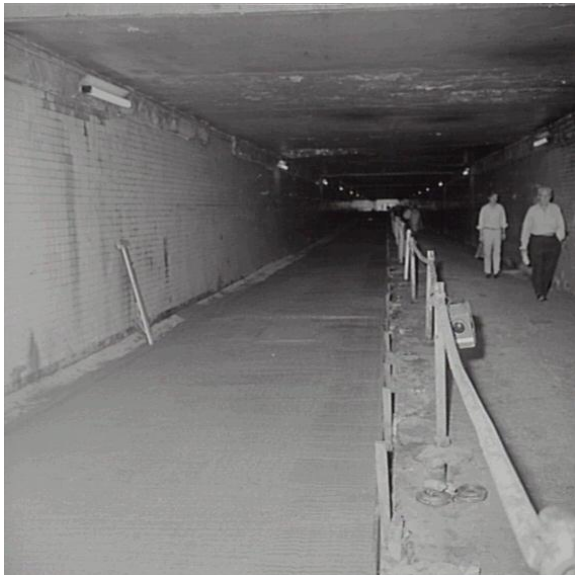
From the mid to late 20th century, the Devonshire Street Tunnel underwent numerous modifications. In 1965, reconfiguration of the Country and Interstate Platforms resulted in the enclosure and/or

<sup>370</sup> Artefact, 2021

<sup>371</sup> Artefact, 2021

demolition of the platform skylights. Although platforms 1-15 all had skylights, only two remain in situ; those at the end of platforms 4/5 and 6/7. These were boarded up and remain so today. Until 1985, the tunnel retained the majority of its original fabric, at which point much of the original fabric was removed due to upgrade works. The upgrade works included the “addition of brighter lighting, new floor tiles and alternating use of bright tiles and murals painted by technical college students on the walls”.<sup>372</sup> An original retaining brick wall that was topped by a sandstone dwarf wall and iron palisade fence was demolished between 1998 and 2000, during the construction of the Henry Deane Plaza. There was also a pergola attached to the western tunnel entrance where the wall was located, however this was also demolished.

The Devonshire Street Tunnel has been continually operational as a pedestrian walkway since it was first established in 1906. With the celebration of Central Stations’ centenary in 2006, the significance of the terminus and the tunnels was commemorated by installing new murals along the tunnel walls. Extensive renovations occurred in 2011, with the installation of a newsagency, new indicator boards and an improved ticketing area to accommodate more passengers. A new disabled ramp and stair connection was also built to the north of the concourse and was fitted out with contemporary ceiling panels and floor tiling. Demolition and replacement of the entrance canopy on Chalmers Street also occurred.



**Figure 349: Inside the Devonshire Street Tunnel c.1968.** <sup>373</sup>



**Figure 350: Aerial view of Devonshire Street tunnel from west, at its opening near the Parcels Post Office on Lee Street**<sup>374</sup>

<sup>372</sup> Rappaport Pty Ltd & NSW Government Architects Office, 2013.

<sup>373</sup> City of Sydney Archives, 1968. ID: A-00050545

<sup>374</sup> <https://archives.cityofsydney.nsw.gov.au/nodes/view/677718>



**Figure 351: Devonshire Tunnel opening to the south of the Parcels Post Office, c.1986.**<sup>375</sup>

### 3.14.2 Site Description and Condition

The Devonshire Street Tunnel is located on an east-west axis and follows the original alignment of Devonshire Street, providing an uninterrupted pedestrian link underneath the railway lines. To the east, the tunnel connects to Chalmers Street and to the west it connects to Henry Deane Plaza. The tunnel connects to the South Concourse, which is a large, open space located on the Chalmers Street side of the tunnel. At the north of the concourse there is a series of entrance gates that lead to the Central Electric Platforms and ESR Concourse and Platforms beyond. The Devonshire Tunnel is a primary commuter area to the south of the Sydney Terminal Precinct.

The Chalmers Street entrance features a contemporary glazed entry structure that contains a set of stairs and escalators that lead to the eastern side of the South Concourse. This entrance was refurbished in 2013 to include a wider set of stairs, acoustic ceiling, red wall tiling and glass walls. Prior to this, the structure was constructed from brick, restricting the amount of natural light to the tunnel below. The Henry Deane Plaza entrance connects to the Devonshire Street Tunnel to the west. The plaza entrance, located just south of the former Parcels Post Office (now Adina Apartment Hotel), contains a covered public space lined with retail and food tenancies.

Constant modifications have been made to the tunnel's interior since its construction and therefore there is little-no original surface finishes which remain in-situ. Concrete mosaic tiles pave the floor, while the walls are lined with ceramic tiles. Air-brushed murals have been added in sections between the wall tiles. These date to the 2006 centenary celebration of the opening of the third Sydney Station and depict the story of Central Station and its workers.

As the walls of the tunnel act as retaining walls, the ceiling is reinforced with steel beams to support the overhead railway lines. To provide natural light into the tunnel, the ceiling originally contained platform skylights. While most of these have been removed, two remain in situ at the ends of Platforms 4/5 and 6/7, however, these are boarded up, and cannot be seen from within the Tunnel

<sup>375</sup> City of Sydney Archives, 1968. ID: A-00050543



due to the corrugated metal ceiling lining. The Tunnel also features contemporary fluorescent lighting which run in two strips down the centre of the ceiling, to provide light to the thoroughfare.

Although the Devonshire Street Tunnel continues to extend underneath Railway Square from the Henry Deane Plaza exit, that section is not included in the study area of this report.



**Figure 352. View of Devonshire street pedestrian tunnel .<sup>376</sup>**



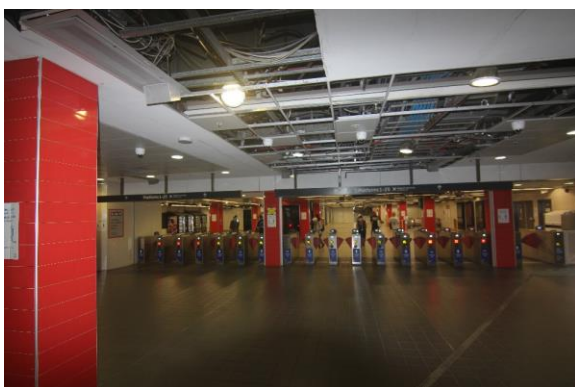
**Figure 353. View of Devonshire street pedestrian tunnel .<sup>377</sup>**



**Figure 354. View of Devonshire street pedestrian tunnel exit towards Railway Square.<sup>378</sup>**



**Figure 355. View of Devonshire street pedestrian tunnel exit towards Elizabeth Street.<sup>379</sup>**



**Figure 356. View of ticket machines when entering Central station from the Devonshire street pedestrian tunnel.<sup>380</sup>**



**Figure 357. View of Devonshire street interpretation signage.<sup>381</sup>**

<sup>376</sup> Artefact, 2021

<sup>377</sup> Artefact, 2021

<sup>378</sup> Artefact, 2021

<sup>379</sup> Artefact, 2021

<sup>380</sup> Artefact, 2021

<sup>381</sup> Artefact, 2021



**Figure 358. View of Devonshire street interpretation signage.<sup>382</sup>**



**Figure 359. View of Devonshire street interpretation signage.<sup>383</sup>**

### 3.14.3 Statement of Significance

The Devonshire Street Tunnel is of state heritage significance. The Devonshire Street Tunnel was an integral part of Henry Dean's plan for the urban development of the Central Site. As such, the site has historical associative significance pertaining to Engineer-in-Chief Henry Deane and his works. Deane's concept for the tunnel included large skylights on the platform level, that would provide natural lighting. The site is historically significant as part of the original design and development of the third Sydney Terminus, completed in 1906. The Devonshire Street Tunnel was constructed along the original alignment of Devonshire Street, and is the first subterranean tunnel for pedestrian use in Australia.

The Tunnel also has historical significance in relation to the construction of the Central Electric Station. The Tunnel was modified and extended to accommodate the Central Electric Station entrance on Chalmers Street. The Tunnel is significant for the technological and architectural innovations of the time. The Devonshire Street Tunnel has been a vital pedestrian thoroughfare for commuters since its construction. For this reason, the Tunnel is socially significant.

Constant modifications have been made to the tunnel interior since its construction and as such, little to no original surface finishes remain in-situ. Various adjustments in the mid to late 20th century, especially the reconfiguration of the country platforms resulted in the demolition of all the original platform skylights, but two; those at the end of platforms 4/5 and 6/7. These have since been boarded up and remain so today. Various other original features such as a retaining brick wall with sandstone sculptures and iron palisade; as well as the pergola attached to the western entrance, were demolished.

### 3.14.4 Gradings of Significance and Integrity

**Table 33: Gradings of Significance and Integrity for the Devonshire Street Tunnel**

<sup>382</sup> Artefact, 2021

<sup>383</sup> Artefact, 2021

Element	Grading	Integrity
<b>Devonshire Street Tunnel Overall</b>	High	Moderate
<b>Views and Vistas</b>	Little	Moderate
<b>Alignment, Context and Setting</b>	Exceptional	Moderate
<b>Evidence of former skylights</b>	High	Moderate Evidence of enclosed skylights remain, however integrity and extent of remaining fabric unknown
<b>Connection to South Concourse</b>	Moderate	Moderate
<b>Eastern Entry (Chalmers Street)</b>	Little	Moderate
<b>Western Entry (Henry Deane Plaza)</b>	Little	Moderate
<b>Tiled Murals and Interpretation Panels</b>	Moderate	N/A
<b>Floors and Paving</b>	Little	N/A
<b>Ceiling</b>	Little	N/A
<b>Applied finishes</b>	Little	N/A
<b>Introduced Services; Mechanical, Electrical, Lighting and Data</b>	Intrusive	N/A

### 3.14.5 Site Specific Conservation Policies

**Table 34: Site Specific Conservation Policies for the for the Devonshire Street Tunnel**

Policy	Policy Discussion and Guidelines
1. Follow Overall Conservation Policies	See Section 15 of this CMP for Conservation Policies.
2. Implement Fabric Guidelines	See Section 15.4.4 of this CMP.
3. Item Specific Policy: Heritage Process	When considering change follow the Heritage Flow Charts that form Appendix C of this CMP: A) Major Works Heritage Process Flow Chart; or B) Minor Works Heritage Process Flow Chart.

Policy	Policy Discussion and Guidelines
4. Item Specific Policy: Heritage Listing	<p>See Section 3 in this CMP for Legislative Context, and Section 14.4 for Statutory Context. .</p> <p>Ensure appropriate statutory approvals or exemptions are obtained prior to change on the site because:</p> <ul style="list-style-type: none"> <li>• The site is within the overall listing for <b>Central Station</b> on the State Heritage Register under the <i>NSW Heritage Act 1977</i> for Sydney Terminal and Central Railway Stations Group.</li> <li>• The site is within the overall listing of the <b>Central Railway Station and Sydney Terminal Group</b> on Transport Asset Holding Entity's (TAHE) Heritage and Conservation Register under Section 170 of the <i>NSW Heritage Act 1977</i>.</li> <li>• The area is within the Railway Square/Central Station Special Character Area under the Sydney DCP 2012.</li> </ul>
5. Item Specific Policy: Use	Promote the continued use as a pedestrian tunnel and celebrate its significance as one of the longest and earliest pedestrian subways in Sydney.
6. Item Specific Policy: Alignment	Retain the existing alignment of the tunnel as a physical interpretation of the original alignment of Devonshire Street.
7. Item Specific Policy: Alterations and Additions	<p>Alterations and additions to the Devonshire Street Tunnel can occur in a sympathetic manner to provide further subterranean connections to Central Station. Additions should not impact the existing alignment of the tunnel.</p> <p>As most much of the original tunnel fabric is no longer extant, alterations to the internal tunnel lining is permissible. Before any works are undertaken, photographic archival records should be taken of the existing murals which depict the history of Central Station.</p> <p>New lighting, services and flooring are permissible within the tunnels to ensure the area is safe and code compliant.</p> <p>No permanent structures should be constructed within the Devonshire Street Tunnel (other than at its Chalmer Street or Henry Deane Plaza ends). The tunnel should remain an unobstructed pedestrian thoroughfare.</p>
8. Item Specific Policy: Reinstatement	Consider the re-instatement of the skylights to the Devonshire Street Tunnel.
9. Item Specific Policy: Original Finishes and lighting	During any works investigate the original finishes and evidence of original lighting. Record these if located and consider their reinstatement where possible.
10. Item Specific Policy: Lighting	Lighting within the Devonshire Tunnel should be consistent with the guidelines outlined in the <i>Central Station Lighting Strategy</i> . Lighting should seek to provide Safety in Design for occupants and users of the tunnel.
11. Item Specific Policy: Intrusive fabric.	Demolition of, or changes to, the existing intrusive building services should be allowed without formal approval provided no significant fabric is impacted on by the change.
12. Item Specific Policy: Maintenance	Ensure the significant fabric of the area is conserved through a maintenance program.
13. Item Specific Policy: Short term uses and changes.	Do not allow negative impacts on significant fabric, spatial qualities and setting for short term gain.



Policy	Policy Discussion and Guidelines
14. Item Specific Policy: Interpretation	<p>See Sections 9 and 14.8 'Heritage Interpretation' in this CMP.</p> <p>Prepare a heritage interpretation strategy, based on the precinct-wide Central Precinct Heritage Interpretation Strategy, specific to the Devonshire Street Tunnel as part of Central Precinct. Particular focus should be given to the former landscape and cultivation of the space.</p> <p>Following this, develop and implement a detailed site-specific heritage interpretation plan, aligned with the themes and recommendations of the heritage interpretation strategy.</p>
15. Item Specific Policy: Archaeology	<p>See Sections 10 and 15.6 in this CMP and Appendix B. Where possible, retain archaeological evidence in situ.</p> <p>If excavation into the floor of the tunnel is occurring and any archaeological evidence is found, notify the Office of Environment and Heritage and seek advice from a suitably qualified historical archaeologist prior to work recommencing. Approval pursuant to s57(1) of the NSW Heritage Act will be required if remains need to be removed. Where possible, retain archaeological evidence in situ.</p>

## 3.15 BASEMENT



**Figure 360. Main Terminus Building basement.**<sup>384</sup>



**Figure 361. Electricians Workshop in basement of Main Terminus Building.**<sup>385</sup>

### 3.15.1 History of the Basement

Construction of the third Sydney Station, up to and including the Grand Concourse level, was completed in August 1906. The first foundation stone was laid on 20 April 1902, with construction commencing in June of the same year. The Central Railway Station buildings were initially controlled by the Railway Construction Branch of Public Works, under the Engineer-in-Chief for Railways Henry Deane. However, in 1904, this was transferred to the Government Architects Branch, with the Government Architect, Walter Liberty Vernon, taking control.

Vernon's Principal Architect, George McRae, oversaw the development of the basement of the Main Terminus Building, including the sewage and drainage elements, during the first stage of construction. Approximately 3.3 million bricks were required to build the premises up to joist level.

As ordered by the Railway Department, extensive alterations on the already completed storerooms and passageways of the north eastern basement area were undertaken around the time of the First World War. This included installation of a coal fired boiler, which was required to provide power for the expanded kitchen. In 1916, the RRR (Railway Refreshment Rooms) were established in part of the basement. These were used to manage the dining rooms, staff rooms, bars, and kiosks at Central. The RRR eventually expanded into the staff dining area and kitchen. Part of the area was used for a mass production kitchen, while a loading dock for the RRR occupied the remaining area. Existing facilities were also present in this area of the basement, including a storage area, scullery, and boiler room. While the RRR kitchen had two serveries, only one contained a dumb waiter. The dumb waiter was used for the transportation of food to the dining rooms and was connected to a server located adjacent to the concourse level dining areas. Passageways in the basement allowed other goods to be transferred to the platforms. They could then be dispatched throughout the state via rail.

Operations at the RRR store were wound down in 1976. The configuration of the basement remained the same until this time, however on the cessation of the RRR, the spaces were converted to storage spaces or otherwise left vacant. The loading dock area, which opens onto the Eddy Avenue Arcade, is the only element in the basement area which retains its original function.

<sup>384</sup> Artefact, 2021

<sup>385</sup> Artefact, 2021

While the north-eastern basement area was used by the RRR, the central northern space was used as a substation area. It was first used for this function on the completion of the third Sydney Terminus in 1906 and has continued to function in this way since.

### 3.15.2 Site Description and Condition

The Basement area is located directly behind the northern façade of the Main Terminus Building. It sits at pedestrian level, behind the shops of the Eddy Avenue Arcade and underneath the Grand Concourse. Original spaces in the basement include two short entry hallways off Eddy Avenue, the east-west corridor that runs behind the Eddy Avenue Arcade shops, the rooms to the south of the east-west corridor and the spaces in and to the south of the RRR loading dock.

Extensive original fabric is retained in the basement entrance hallways from Eddy Avenue. While they both contain their original double-heighted rendered barrel-vaulted ceilings, stairs, joinery and timber panelled entrance doors with glazed fanlights, the westernmost hallway also features an early chain hoist pulley mechanism. This is associated with the Railway Printing Office, which occupied the adjacent shops during the early days of the station.<sup>386</sup> The easternmost hallway connects to the foyer of the grand stair, which leads to the office levels above the Grand Concourse level. Conservation works to the easternmost hallway were undertaken in 2011, at which time it was painted in the same colour scheme as the grand internal stair. Neither of the entranceways are publicly accessible, however, the easternmost hallway is occasionally used by station workers.

The two Eddy Avenue basement entrance hallways connect to the east-west corridor. This corridor is bounded by the Eddy Avenue Arcade shops to the north and a series of rooms to the south. The rooms to the south of the corridor, that are located between the eastern entrance hallway and the western boundary of the corridor, include, from east to west: the grand stair foyer, the electricians workshop, the STR 1 Room (Sydney Transformer Room No.1 Substation), the Honeywell Room, the STR 2 Room (Sydney Transformer Room No.1 Substation) and the Cleaners Store. The electricians workshop contains extant areas of original magnesite composite tiles on a concrete slab. These tiles are likely from the Fama manufacturer, and were laid in a checkerboard pattern featuring orange and cream tiles. Made from a mixture of sawdust, purified magnesia and long fibre asbestos, the tiles had a similar resilience to linoleum and were a hardwearing surface which did not crack.<sup>387</sup> Some other spaces within the Basement also feature remnant Stelcon plate flooring. Within the series of rooms behind the east-west corridor there are storage rooms and the original c.1906 substation, as well as contemporary transformers. STR1, STR2 and the Cleaners room each contain a mezzanine level. These are predominantly used for storage, as are the other smaller rooms that line the corridor.

An extensive area to the east of the eastern basement's entrance hallway and to the south of the east-west corridor contains the Sydney Trains Offices. To the east of these offices, the corridor turns south. This north-south corridor contains a series of rooms to the east and west, the majority of which are used for storage. The corridor terminates at a large open area with a mezzanine level to the south. This is known as the Data Area. At the northern end of the corridor, the path turns east and ramps up to connect with the eastern façade of the Main Terminus Building. This entrance is not publicly accessible, but it is used by railway staff. To the south of this entrance is a row of c.1996 shopfronts that open onto the Eddy Avenue Forecourt, beneath the East Deck. The north-east corner of the basement contains the former facilities and loading dock area of the RRR. The loading dock is indicated by a roller door and 'RRR' signage that fronts the Eddy Avenue Arcade. The room is shared between a Sydney Trains Workshop and a Commercial Leased Area. Of the two rooms to the right of

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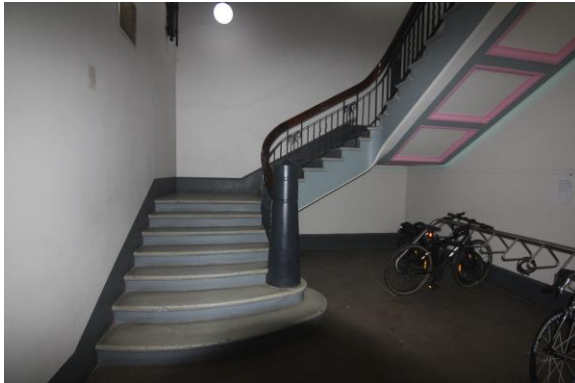
<sup>386</sup> Central Station CMP 1996

<sup>387</sup> Miles Lewis, 'Chapter 11.04: Finishes – Floor Coverings' in *Australian Building*, p. 11.04.14, accessed 26/11/21, <https://www.mileslewis.net/australian-building/pdf/11-finishes/11.04-floor-coverings.pdf>

the loading dock, the first is disused due to storm damage and the second is a disused commercial retail space.

At the western end of the east-west corridor, the path turns south to create a short north-south corridor. This corridor contains three rooms which are, from north to south, a plant room, a clock room and a storage space. Each feature an unlined, double height space with concrete slab floors and painted brickwork walls. The clock room contains a collection of clocks that are associated with the NSWGR.

Other items located within the basement include the Subway Passage System, the Basement Luggage Dock and the Eddy Avenue Colonnade, Arcade and Shops.



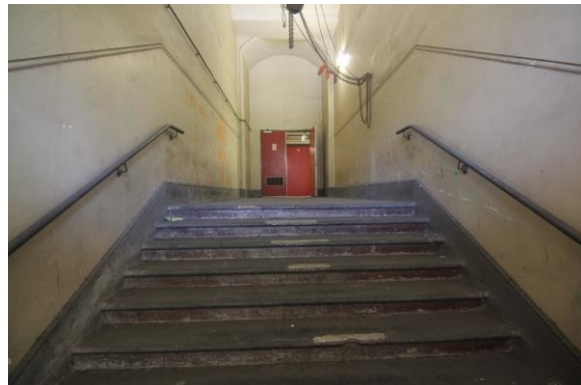
**Figure 362. View of Grand Staircase leading up to level 1 from the Basement.**<sup>388</sup>



**Figure 363. View of Basement Clock Room.**<sup>389</sup>



**Figure 364. View of Mezzanine above cleaners store (B-44).**<sup>390</sup>



**Figure 365. View of Basement entry from Eddy Ave.**<sup>391</sup>

<sup>388</sup> Artefact, 2021

<sup>389</sup> Artefact, 2021

<sup>390</sup> Artefact, 2021

<sup>391</sup> Artefact, 2021



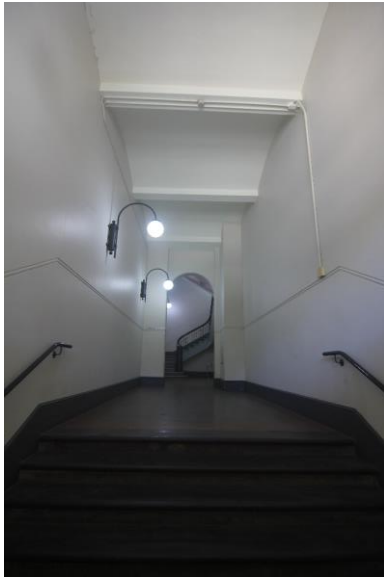


Figure 366. View of Basement entry from Eddy Ave.<sup>392</sup>

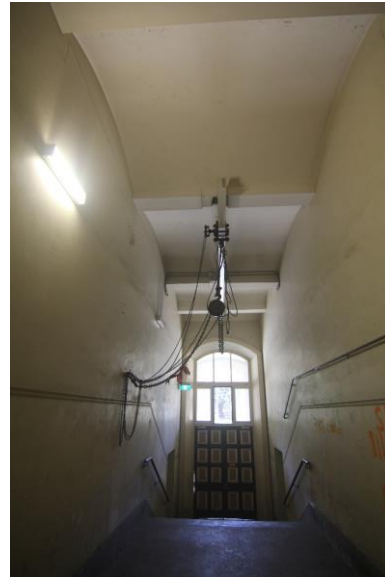


Figure 367. View of Basement entry from Eddy Ave.<sup>393</sup>



Figure 368. View of Basement baggage tunnel.<sup>394</sup>



Figure 369. View of Basement baggage tunnel.<sup>395</sup>



Figure 370. View of Basement street level doors into BW-05.<sup>396</sup>



Figure 371. Internal view of Basement BW-05.<sup>397</sup>

<sup>392</sup> Artefact, 2021

<sup>393</sup> Artefact, 2021

<sup>394</sup> Artefact, 2021

<sup>395</sup> Artefact, 2021

<sup>396</sup> Artefact, 2021

<sup>397</sup> Artefact, 2021

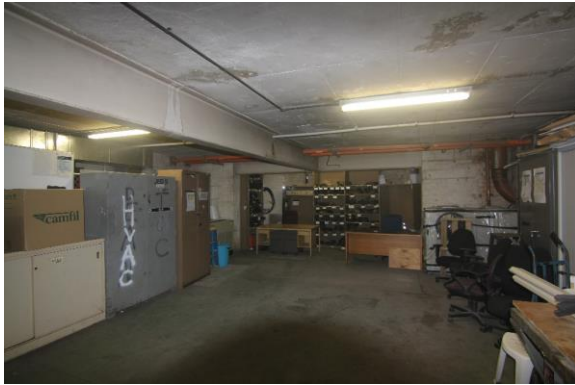


Figure 372. View of Basement cleaners store B-26.<sup>398</sup>



Figure 373. View of Basement Mezzanine BN-08.<sup>399</sup>



Figure 374. View of Basement southern disused room.<sup>400</sup>



Figure 375. View of Basement southern disused room.<sup>401</sup>



Figure 376. View of Basement southern disused room.<sup>402</sup>

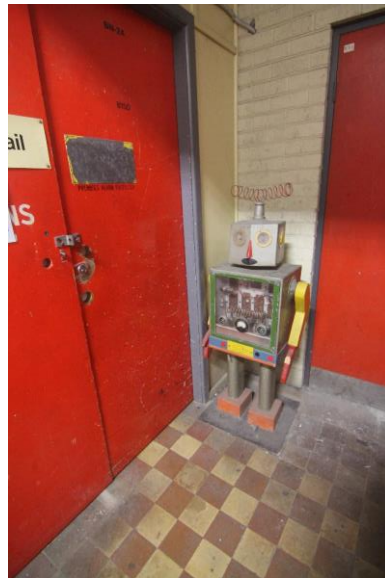


Figure 377. View of Basement electricians workshop.<sup>403</sup>

<sup>398</sup> Artefact, 2021

<sup>399</sup> Artefact, 2021

<sup>400</sup> Artefact, 2021

<sup>401</sup> Artefact, 2021

<sup>402</sup> Artefact, 2021

<sup>403</sup> Artefact, 2021





Figure 378. View of Basement electricians workshop.<sup>404</sup>



Figure 379. View of Basement electricians workshop.<sup>405</sup>



Figure 380. View out of room to the east of RRR Loading dock.<sup>406</sup>



Figure 381. View of RRR loading dock.<sup>407</sup>



Figure 382. View of RRR loading dock.<sup>408</sup>



Figure 383. View of south west mezzanine.<sup>409</sup>

<sup>404</sup> Artefact, 2021

<sup>405</sup> Artefact, 2021

<sup>406</sup> Artefact, 2021

<sup>407</sup> Artefact, 2021

<sup>408</sup> Artefact, 2021

<sup>409</sup> Artefact, 2021



Figure 384. View of south west mezzanine.<sup>410</sup>



Figure 385. View of south west mezzanine.<sup>411</sup>

### 3.15.3 Statement of Significance

The Basement has historic significance in forming part of the original design and development of the third Sydney Terminus, completed in 1906. Prior to this, the land was occupied by Government institutional buildings and the Police Barracks. As such, the area has high research potential and historic significance pertaining to these early institutions, which date back to as early as the 1820s.

The Basement is historically associated to architect George McRae, who oversaw the development of the basement. The basement provided for an above ground level passenger concourse and platform. The basement holds aesthetic significance in its complex design. The basement has a high degree of integrity and exhibits technical significance in its design in connecting to the Main Terminus Building, via various modes of access.

Extensive original fabric is extant in the basement entrance hallways from Eddy Avenue. This contributes to sites aesthetic significance in the remaining original architectural elements like its original double-heighted rendered barrel-vaulted ceilings, stairs and joinery and timber panelled entrance doors with glazed fanlights, the westernmost hallway also features an early chain hoist pulley mechanism. This is associated with the Railway Printing Office, which occupied the adjacent shops during the early days of the station. The site therefore has associative significance pertaining to the Printing Office and its history.

The Basement underwent various modifications and alterations during the first World War. This included the installation of a coal fired boiler. The Railway Refreshment Rooms were established in 1916 in the basement. The space is now used primarily for storage.

The Basement has been assessed as having state heritage significance.

### 3.15.4 Gradings of Significance and Integrity

**Table 35: Gradings of Significance and Integrity for the Basement**

Element	Grading	Integrity
<b>Basement Overall</b>	Moderate	Moderate
<b>Form, Views and Vistas</b>	Moderate	Moderate
<b>Context and Setting</b>	High	Moderate

<sup>410</sup> Artefact, 2021

<sup>411</sup> Artefact, 2021



Element	Grading	Integrity
Passageway and rear entrances to Eddy Avenue Shops	Moderate	High
Entrance Hallways from Eddy Avenue	High	High
Spiral Stair	High	High
Mainframe Data Area	Little	N/A
Ground Floor Offices	Little	N/A
Storage and Transformer Rooms	Little	Little
Paving and Floor Tiling	Non-original: Little Remnant early flooring: Moderate	Little Remnant Stelcon plate flooring and Fama floor paving is in poor condition and integrity
Ceilings	Little	Little
Applied Finishes	Little	N/A
Introduced Services; Mechanical, Electrical, Lighting and Data	Intrusive	N/A
Pulley System in Entrance Hallway	Moderate	Moderate (paint not original)

### 3.15.5 Site Specific Conservation Policies

**Table 36: Site Specific Conservation Policies for the for the Basement**

Policy	Policy Discussion and Guidelines
1. Follow Overall Conservation Policies	See Section 15 of this CMP for Conservation Policies.
2. Implement Fabric Guidelines	See Section 15.4.4 of this CMP
3. Item Specific Policy: Heritage Process	When considering change follow the Heritage Flow Charts that form Appendix C of this CMP: A) Major Works Heritage Process Flow Chart; or B) Minor Works Heritage Process Flow Chart.
4. Item Specific Policy: Heritage Listing	See Section 3 in this CMP for Legislative Context, and Section 14.4 for Statutory Context.  Ensure appropriate statutory approvals or exemptions are obtained prior to change on the site because: <ul style="list-style-type: none"> <li>The site is within the overall listing for <b>Central Station</b> on the State Heritage Register under the <i>NSW Heritage Act 1977</i> for Sydney Terminal and Central Railway Stations Group.</li> <li>The site is within the overall listing of the <b>Central Railway Station and Sydney Terminal Group</b> on Transport Asset Holding Entity's (TAHE) Heritage and Conservation Register under Section 170 of the <i>NSW Heritage Act 1977</i>.</li> <li>The area is within the Railway Square/Central Station Special Character Area under the Sydney DCP 2012.</li> </ul>

Policy	Policy Discussion and Guidelines
5. Item Specific Policy: Development	<p>Prior to the commencement of any proposed development or works, photographic archival records of the Basements should be undertaken.</p> <p>Ensure the original external colour schemes and finishes are documented and archivally recorded.</p>
6. Item Specific Policy: Use	Promote compatible use for the area which allows for the sense of a semi-industrial use and spatial values and fabric of High significance.
7. Item Specific Policy: Configuration	Retain the basic structure of the original configuration including the spaces and corridors.
8. Item Specific Policy: Internal Fitout	<p>During the design of new internal fitout ensure that past intrusive fabric and structure is removed.</p> <p>An interior fit-out guide for the offices should be prepared to help guide future fit-outs and to provide a level of consistency across the spaces in their treatment and protection of the heritage fabric and spaces.</p>
9. Item Specific Policy: Maintenance	Ensure the significant fabric of the area is conserved through a maintenance program, particularly for areas that are currently disused and elements in poor condition.
10. Item Specific Policy: Short term uses and changes.	Do not allow negative impacts on significant fabric, spatial qualities and setting for short term gain.
11. Item Specific Policy: Archaeology	<p>See Sections 10 and 15.6 in this CMP and Appendix B. Where possible, retain archaeological evidence in situ.</p> <p>If excavation into the basement floors is occurring and any archaeological evidence of the Police Barracks is found, notify the NSW Heritage Branch, Department of Environment and Heritage and seek advice from a suitably qualified historical archaeologist prior to work recommencing. Approval pursuant to s57(1) of the NSW Heritage Act will be required if remains need to be removed. Where possible, retain archaeological evidence of the Police Barracks in situ.</p>
12. Item Specific Policy: Interpretation	<p>See Sections 9 and 14.8 'Heritage Interpretation' in this CMP.</p> <p>Prepare a heritage interpretation strategy, based on the precinct-wide Central Precinct Heritage Interpretation Strategy, specific to the Basement as part of Central Precinct. Particular focus should be given to the former landscape and cultivation of the space.</p> <p>Following this, develop and implement a detailed site-specific heritage interpretation plan, aligned with the themes and recommendations of the heritage interpretation strategy.</p>

## 3.16 LUGGAGE DOCK



Figure 386. Luggage Dock.<sup>412</sup>

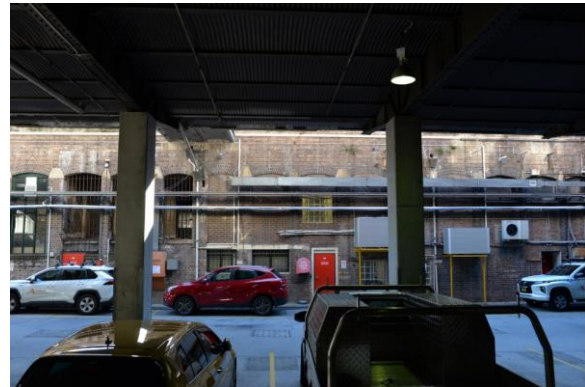


Figure 387. Luggage Dock.<sup>413</sup>

### 3.16.1 History of the Luggage Dock

The Luggage Dock area was built as part of the Main Terminus Building in 1906. Passengers would leave their luggage at the dock, where it would then be distributed to the train via the underground subway passage system. It continued this function until 1946, when the handling of luggage became part of the Inwards Parcels Office operations. The Inward Parcels Office continued this function until the 1980s, after which time it was used as a lost property area.<sup>414</sup>

An associated van dock was located throughout the Luggage Dock, which was used by the shops in the Pitt Street Arcade. These shops back onto the vehicular area of the dock (located beneath the light wells in the Western Forecourt), while passageways within the Pitt Street Arcade linked Pitt Street to the Luggage Dock area. These passageways provide the only entry in and out of the Luggage Dock area and were established at the time of its construction.

Due to the Inwards Parcels Office operations, a mezzanine level was inserted into the Luggage Dock area in the mid-20th century, when it became part of the Inwards Parcels Office operations. The original luggage racks were removed in the late 20th century. The Loading Dock area continued to be used for lost property until the late 20th century, and by 2006 functioned as a rubbish disposal and sorting area.

<sup>412</sup> Artefact, 2021

<sup>413</sup> Artefact, 2021

<sup>414</sup> McKillop, *Century of Central*, p. 136-137



**Figure 388: Loading luggage into the luggage dock, date unknown. <sup>415</sup>**



**Figure 389: Loading luggage into the luggage dock, date unknown. <sup>416</sup>**



**Figure 390: Loading luggage from a truck, with Tivoli signage, date unknown. <sup>417</sup>**

### 3.16.2 Site Description and Condition

The Luggage Dock, which is located in the north-west corner of the Central Station Precinct, is comprised of outdoor and indoor areas. The outdoor area is located behind the Pitt Street Arcade at street level and contains an outside dock, alleyway and parking area. The indoor area, containing two rooms, which is located within the basement level of the Main Terminus Building.

Two passageways that lead into the Loading Dock are located within the Pitt Street Arcade. The entry passageway is to the north, while the exit is to the south. They are separated by approximately ten shops which are located within the arcade and are aligned with the shop fronts. Each passageway features a tall electric painted steel security gates, which appear to be original to the 1906 Terminus Building (however likely modified to become electric at a later date). While the upper portion of the gate is fixed, the lower two thirds are moveable, containing both a pedestrian and vehicular gate. The configuration of the passageways is original, with the walls constructed from brick. Timber frame windows that are associated with the adjacent shops are located to either side of the exit tunnel.

<sup>415</sup> Sydney Trains Archives, AHRS, date unknown. ID: 031092.

<sup>416</sup> Sydney Trains Archives, AHRS, date unknown. ID: 033270.

<sup>417</sup> Sydney Trains Archives, State Library of NSW. Photography by Sam Hood. Date unknown. ID:\_06946r



There is a sign surmounting the Pitt Street Arcade archway that leads to the loading dock which reads “Inwards Parcels Way In”. This indicates that the passageway leads to the Luggage Dock beyond.

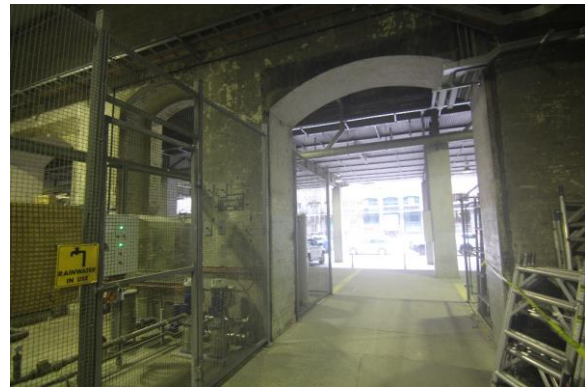
Above the Luggage Dock is the Western Forecourt. This is constructed from a concrete slab which contains corrugated iron barrel vaults that are supported by a series of riveted steel columns and girders. From the Loading Dock, this structure provides a shaded awning for the carpark area. It also contains three light wells which provide light and ventilation for the outdoor space below.

The outside dock, alleyways and parking area are paved with concrete. Parking areas are located underneath the awning and directly adjacent to the eastern façade of the Pitt Street Arcade. Intrusive services have been mounted onto this façade, obscuring some of the windows. To the western façade of the Main Terminus Building there is a concrete baggage platform that steps up to align with floor level of the basement. This contains a series of metal cages that are being used for storage.

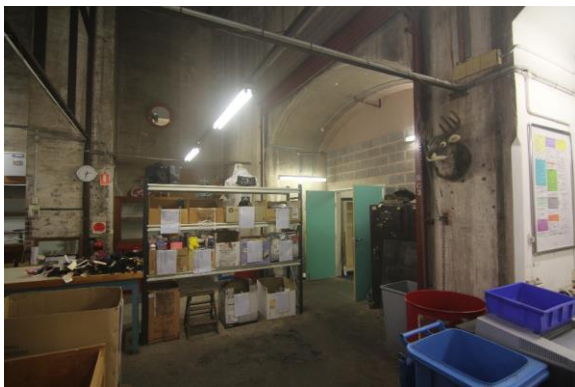
Access to the interior Luggage Dock area is through an original arched opening in the exterior concrete wall of the Main Terminus Building’s western façade. It opens onto a corridor, with a large rectangular space to the south and a smaller enclosed room to the north. The rectangular space is extensive and has been caged off with a contemporary metal fence to be used for storage. The northern space, known as the former ‘Auction Room’, is now used for the storage of lost property. Two roller doors are mounted to the interior of the associated concrete archways and open onto the baggage platform adjacent.



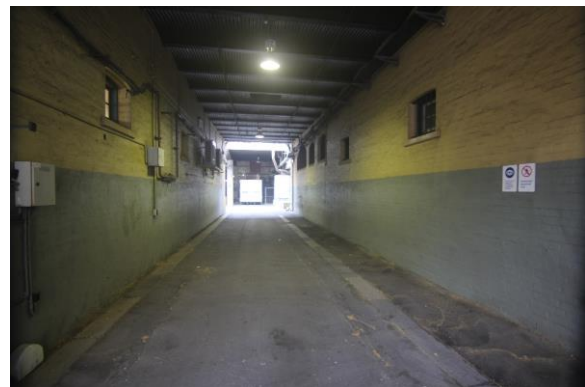
**Figure 391. View within luggage dock.**<sup>418</sup>



**Figure 392. View of storage cage within luggage dock.**<sup>419</sup>



**Figure 393. View of Basement auction room.**<sup>420</sup>



**Figure 394. View leading into Basement luggage dock.**<sup>421</sup>

<sup>418</sup> Artefact, 2021

<sup>419</sup> Artefact, 2021

<sup>420</sup> Artefact, 2021

<sup>421</sup> Artefact, 2021



Figure 395. Rear external view of luggage dock.<sup>422</sup>



Figure 396. Rear external view of luggage dock from Eddy Ave.<sup>423</sup>



Figure 397. View above luggage dock<sup>424</sup>

### 3.16.3 Statement of Significance

The Luggage Dock has state heritage significance. The Luggage Dock is historically significant as part of the original design and development of the third Sydney Terminus, completed in 1906. Prior to this the land was occupied by Government institutional buildings. Some of these buildings include the Carter's Barracks and Sydney Female Refuge, which date back to as early as the 1820s. This previous land history contributes to the luggage dock's high research potential, as well as its historical significance.

The Luggage Dock is also historically significant in relation to its role in providing luggage services to travellers from 1906 up until the 1946, when it became occupied by the parcels office. Then in 1980s it was converted into a lost property area. The dock has since been converted into a rubbish disposal area. The original luggage racks were removed, resulting in a loss of original material and integrity.

<sup>422</sup> Artefact, 2021

<sup>423</sup> Artefact, 2021

<sup>424</sup> Artefact, 2021

### 3.16.4 Gradings of Significance and Integrity

**Table 37: Gradings of Significance and Integrity for the Luggage Dock**

Element	Grading	Integrity
<b>Luggage Dock Overall</b>	Moderate	High
<b>Metal Entry Gates</b>	High	High
<b>Passages</b>	High	High
<b>Light wells</b>	High	High
<b>Vehicular Area</b>	Moderate	Moderate
<b>Baggage Platform</b>	Little	High
<b>Lost Property Room (Basement)</b>	Moderate	Unknown
<b>Doors, Windows and Hardware</b>	Moderate	Moderate – High
<b>Floors and Paving</b>	Little	Little
<b>Ceilings</b>	Moderate	High
<b>Introduced Services; Mechanical, Electrical, Lighting &amp; Data</b>	Intrusive	N/A
<b>Fitout</b>	Intrusive	N/A

### 3.16.5 Site Specific Conservation Policies

**Table 38: Site Specific Conservation Policies for the for the Luggage Dock**

Policy	Policy Discussion and Guidelines
1. Follow Overall Conservation Policies	See Section 15 of this CMP for Conservation Policies.
2. Implement Fabric Guidelines	See Section 15.4.4 of this CMP.
3. Item Specific Policy: Heritage Process	<p>When considering change follow the Heritage Flow Charts that form Appendix C of this CMP:</p> <p>A) Major Works Heritage Process Flow Chart; or</p> <p>B) Minor Works Heritage Process Flow Chart.</p>

Policy	Policy Discussion and Guidelines
4. Item Specific Policy: Heritage Listing	<p>See Section 3 in this CMP for Legislative Context, and Section 14.4 for Statutory Context.</p> <p>Ensure appropriate statutory approvals or exemptions are obtained prior to change on the site because:</p> <ul style="list-style-type: none"> <li>• The site is within the overall listing for <b>Central Station</b> on the State Heritage Register under the <i>NSW Heritage Act 1977</i> for Sydney Terminal and Central Railway Stations Group.</li> <li>• The site is within the overall listing of the <b>Central Railway Station and Sydney Terminal Group</b> on Transport Asset Holding Entity's (TAHE) Heritage and Conservation Register under Section 170 of the <i>NSW Heritage Act 1977</i>.</li> <li>• The area is within the Railway Square/Central Station Special Character Area under the Sydney DCP 2012.</li> </ul>
5. Item Specific Policy: Use	Promote compatible use for the interior and exterior spaces of the Luggage Dock. Preference to semi-industrial uses related to the function of the Station.
6. Item Specific Policy: Development	<p>Prior to the commencement of any proposed development or works, photographic archival records of the Luggage Dock areas should be undertaken.</p> <p>Ensure the original external colour schemes and finishes are documented and archivally recorded.</p>
7. Item Specific Policy: Configuration	<p>Retain the original configuration including:</p> <ul style="list-style-type: none"> <li>• passageways through the arcade</li> <li>• lightwells</li> <li>• steel columns and girders</li> <li>• barrel vaults, including permanent corrugated iron formwork.</li> </ul>
8. Item Specific Policy: Intrusive fabric.	Demolition of, or changes to, the existing intrusive building services should be allowed without formal approval provided no significant fabric is impacted on by the change.
9. Item Specific Policy: Maintenance	Ensure the significant fabric of the area is conserved through a maintenance program.
10. Item Specific Policy: Short term uses and changes.	Do not allow negative impacts on significant fabric, spatial qualities and setting for short term gain.
11. Item Specific Policy: Archaeology	<p>See Sections 10 and 15.6 in this CMP and Appendix B. Where possible, retain archaeological evidence in situ.</p> <p>If excavation below the luggage dock is occurring and any archaeological evidence of the early institutional uses of the site is found, notify the NSW Heritage Branch, Department of Environment and Heritage and seek advice from a suitably qualified historical archaeologist prior to work recommencing. Approval pursuant to s57(1) of the NSW Heritage Act will be required if remains need to be removed. Where possible, retain archaeological evidence of the early institutional uses in situ.</p>



## 3.17 PARCELS AREA



Figure 398. Parcels Area.<sup>425</sup>



Figure 399. Parcels Area.<sup>426</sup>

### 3.17.1 History of the Parcels Area

Following the demolition of Second Sydney Terminal, elements of its fabric were relocated and reused for the construction of the Parcels Area at the new Sydney Terminus. Completed in 1906, the Parcels Area was designed to facilitate the collection, assortment, and distribution of both incoming and outgoing mail. While the Inwards Parcel Shed to the southern end of Platform 1 managed the inward flow of parcels, the outward flow was organised at a designated area in the basement level of the West Wing Extension. In the 1920s, this arrangement was altered with the construction of parcel dock platforms to the south of the Inwards Parcel Shed, allowing mail to be directly loaded onto the trains.

A ramp constructed adjacent to the Parcels Post Office allowed vehicles to deposit mail to the Inwards Parcels Office. The vehicles would then exit the parcels area via a ramp that spanned the Devonshire Street Tunnel and continued onto Lee Street. This ensured that Lower Carriage Lane, formerly known as Ambulance Avenue, was not used for depositing mail. The name 'Ambulance Avenue' directly correlated to its function as a designated waiting area for ambulance officers in horse-drawn carriages. This was designed to prioritise the safety of the passengers, as interference was avoided between the ambulances and commuters who were utilising the Grand Concourse above. By the 1920s, motorised carriages had replaced the horse-drawn ambulance and by the 1930s, many of these were operated by volunteers who were off-duty railway workmen. Eventually the use of the avenue for ambulance operations became redundant and it was renamed 'Lower Carriage Lane'. Today, the former Governors Archway on the western façade of the West Wing Extension is used as an access point for emergency services.

In 1906, a dispatch office was established in the basement room of the West Wing Extension. The purpose of the office was to send out correspondence, such as Special Trains notices, and other information relating to the railways to various stations and offices around NSW.

A newspaper article from 1906 notes the use of an electric lift for transporting parcels from the Inwards Parcels office down to the basement area below (the Parcels Area), which was used by the

<sup>425</sup> Artefact, 2021

<sup>426</sup> Artefact, 2021

contractors, Messrs Whitehead and Co., for loading into vans and despatching them to the city and suburban areas around Sydney.<sup>427</sup>

During the mid-20th century, various buildings were constructed in the yard and an awning was also attached to the West Wing Extension. There was a room that was originally used as offices; however, this has since been converted into the Inter-Station mail room. As part of the conversion, a small timber dock was also installed. The Inter-Station mail room currently remains in operation.

By 1999, the Parcels Area was used as a train catering store.<sup>428</sup> This area is still used by Gate Gourmet, a train catering service for the CountryLink services.<sup>429</sup>

While the most prominent modification to the Parcels Area was the demolition of the parcels dock platforms, other changes have also occurred. Two elements of the area have been adaptively reused, with the Inwards Parcel Office converted to a YHA hostel and the former workshops along southern perimeter used for CountryLink catering services. In 2013, the Parcels Area was reported as being under-utilised and the yard was functioning as a “parking area for Responsible Government Agency staff”.<sup>430</sup> At this time, the West Wing Extension was being used to dispatch and receive parcels, while the Lower Carriage Lane was once again being used to deliver the parcels.



Figure 400: Parcels Area on the right hand side, at the end of Ambulance Ave, 1938. <sup>431</sup>

<sup>427</sup> The Sydney Morning Herald, 2 August 1906, p. 8. Available online at: <http://nla.gov.au/nla.news-article14790533> (accessed 29 October 2021).

<sup>428</sup> GML Heritage, 1999. *Inwards Parcels Shed, Sydney Terminal – Conservation Management Plan*. Report for Toga Building Company Pty Ltd.

<sup>429</sup> Urbis 2020. *Conservation Management Plan: Former Inwards Parcels Shed, 8-10 Lee Street, Haymarket*. Prepared for Vertical First Pty Ltd, 30 September 2020, p. 78.

<sup>430</sup> Rappaport Pty Ltd & NSW Government Architects Office, 2013

<sup>431</sup> Sydney Trains Archives, 1938. ID: MAAS 261584.

### 3.17.2 Site Description and Condition

The Parcels Area is situated between the Western Forecourt to the north, the Main Terminus Building to the east, Lee Street to the west and the Former Inwards Parcels Shed (YHA Railway Square) and Parcels Post Office (Adina Apartment Hotel) to the south. The Parcels Area comprises the basement area of the Former Inwards Parcels Shed, the basement of the West Wing Extension and the internal basement area bounded by the northern and southern sides of Upper Carriage Lane, and the roadway of Lower Carriage Lane (Ambulance Avenue), which provides access to the loading dock in the basement area of the Former Inwards Parcels Shed. The basement area in the Former Inwards Parcels Shed and the basement area bounded by the retaining walls of the Upper Carriage Lane are known collectively as the Former Small Parcels Bagging Room.<sup>432</sup> The Lower Carriage Lane is bounded by the Western Forecourt's retaining wall to the north and the Upper Carriage Lane's northern retaining wall to the south. Both Upper and Lower Carriage Lanes provide access to Lee Street. An awning along the western elevation of the West Wing Extension, awnings along the northern side of the retaining wall between the Upper and Lower Carriages Lanes, a garage along the southern side of the northern retaining wall and a timber loading dock are located within Lower Carriage Lane. A mix of asphalt and concrete line the surface of Lower Carriage Lane.

The c.1906 retaining walls along the northern and southern sides of Lower Carriage Lane are constructed from red brick laid in English bond, featuring a moulded brick string course and a single course of sandstone saddle coping. Both retaining walls terminate at Lee Street in c.1906 decorative cylindrical sandstone piers, surmounted by cast iron lamps which are reproductions of the originals. The sandstone piers show evidence of repairs and conservation works to the sandstone. Openings into the Loading Dock are built into the eastern section of the Upper Carriage Lane's northern retaining wall, contemporaneous with the retaining wall, comprising large openings with later corrugated steel roller doors or timber-framed glazing and smaller openings inset with moulded timber doors. The majority of these openings exhibit segmental brick arches, although a small number display rounded brick arches. The entrance into the catering area exhibits a decorative rusticated sandstone segmental arch with rusticated sandstone engaged piers and a decorative projecting sandstone corbelled keystone. Above the brick arches are corrugated iron awnings with timber fascia boards, supported by wrought iron brackets on decorative sandstone corbels. Although the awnings were original to the c.1906 building, they appear to have been altered over time.

The decorative sandstone arch in the retaining wall spans over a brick-lined passage that leads to the CountryLink catering service area within the basement area below the Former Inwards Parcels Shed. Cast iron girders support the structure of the ramp above the passageway. The wrought iron gate under the decorative sandstone arch is likely to date to the c.1906 construction of the ramp, but may date later in the early 20th century. The five eastern-most original segmental arched brick openings within the retaining wall lead to the Former Small Bagging Room beneath the Former Inwards Parcels Shed, which is currently used by Gate Gourmet. The three western arches lead to the fire escape tunnel on the western side of the Parcels Area and the entrance to a carpark servicing the Adina Apartment Hotel (the Former Parcels Post Office).

The basement of the Former Inwards Parcels Shed and the Upper Carriage Lane ramp was formerly the Small Parcels Bagging Room. The Former Small Parcels Bagging Room comprises an L-shaped space divided into bays by a mix of concrete columns, steel beams and columns, and load-bearing masonry walls. The spacing of the bays in the eastern section matches the four bays of the Former Inwards Parcels Shed above. The steel beams on concrete columns in the eastern section are surmounted by concrete vaults, but the majority of the concrete vaults are obscured by the contemporary suspended ceilings. The existing fit-out utilises partition walls to separate the area into a kitchen/lunch room, office, cool room and warehouse for Gate Gourmet. The eastern area contains

<sup>432</sup> Urbis 2020. *Conservation Management Plan: Former Inwards Parcels Shed, 8-10 Lee Street, Haymarket*. Prepared for Vertical First Pty Ltd, 30 September 2020, p. 28.

the warehouse, a kitchen/lunch room in the north-western corner and cool rooms, as well as a disused stair leading to the Former Inwards Parcels Shed above. Blind arches line the western wall of the area, while services and ducts line the concrete vaults. The western storage area, beneath Upper Carriage Lane, contains four bays by five bays divided by steel columns surmounted by vaulted corrugated iron sheeting. Light-weight partition walls separate a small office space from the storage area along the northern side of the western area. At the junction between the eastern area and the western area, directly beneath the loading dock along the western side of the Former Inwards Parcels Shed, the original glazing and glazing bars in the loading dock wall are visible. A corridor at the eastern end of the basement, next to the kitchen/lunch room, provides access to the Sydney Terminal's subway passage system, however, a concrete block wall separates the tunnels from the Former Small Parcels Bagging Room. The vaulted ceiling in the southern area is lined with corrugated iron sheeting.

The basement/ground floor elevation of the West Wing Extension is constructed from English bonded liver-coloured brick on a chamfered sandstone plinth with engaged brick piers. The elevation is punctuated with large doors exhibiting segmental arched sandstone arches, small doors exhibiting flat-arched lintels and windows exhibiting flat-arched sandstone lintels and chamfered sandstone sills. A large c.1960s steel framed awning with late 20th century/21st century corrugated steel roofing runs along the western elevation of the West Wing Extension at ground level, connecting to the brick garage at the northern end. A steel-framed chicken-wire cage is located beneath the southern section of the awning. The openings display a mix of early timber-framed glazing and later 20th century brick infill and timber doors. The interior of the basement/ground floor elevation of the West Wing Extension is used for yard activities, currently housing the Dispatch and Records area and the Inter Station Mail Room. The layout, joinery and fenestration appear to have changed little since the building's original construction in c.1906, aside from the introduction of services.

An English bonded brick garage on an irregular plan abuts the southern side of the northern retaining wall along Lower Carriage Lane (between the Western Forecourt and Lower Carriage Lane). The garage likely dates to the first half of the 20th century, although the roofing, timber doors along the eastern elevation and the render and corrugated roller doors along the southern elevation of the garage are likely to date to the second half of the 20th century. A small timber loading dock with a timber-framed windows, elevated balcony and steel staircase is nestled in the northern corner of Lower Carriage Lane, between the Western Forecourt (northern) retaining wall and the West Wing Extension.

Overall, the majority of elements are in good condition. However, sections of the sandstone coping above the brick retaining walls exhibit spalling, and two bays of the northern elevation of the brick retaining wall along the northern side of the Upper Carriage Lane exhibit extensive staining. The latter is likely to be caused by an insufficient seal from the awning above this section of the wall, which comprises what appears to be a later extension of the awning. In addition, the timber loading dock is in poor condition, exhibiting decay.





**Figure 401. External view of Post Office Building at the beginning of Ambulance Ave.**<sup>433</sup>



**Figure 402. External view of up Ambulance Ave.**<sup>434</sup>



**Figure 403. External view of delivery entrance.**<sup>435</sup>



**Figure 404. External view of delivery entrance.**<sup>436</sup>



**Figure 405. View up Ambulance Ave**<sup>437</sup>



**Figure 406. External view of Parcels Building.**<sup>438</sup>

<sup>433</sup> Artefact, 2021

<sup>434</sup> Artefact, 2021

<sup>435</sup> Artefact, 2021

<sup>436</sup> Artefact, 2021

<sup>437</sup> Artefact, 2021

<sup>438</sup> Artefact, 2021



**Figure 407. External view of brick retaining wall<sup>439</sup>**



**Figure 408. External view of sandstone column and lamp post.<sup>440</sup>**

### 3.17.3 Statement of Significance

The Parcels Area has state heritage significance. The Parcels Area is historically significant as part of the original design and development of the Third Sydney Terminus in 1906. The Parcels Area is also historically significant for the contribution it had in the collection, assortment, and distribution of mail during the early twentieth century. As such, the area has associative significance pertaining to the Australia Post company and other various contractors who used the space such as Messrs Whitehead and Co. The Parcels Area also holds historic significance in the development and naming of surrounding street, namely, Ambulance Avenue, which was a lane adjacent to a ramp constructed for the Parcels Post Office and functioned as a waiting area for ambulance officers in horse-drawn carriages.

The dispatch office of the Parcels Post Office was established in the basement room of the West Wing Extension in 1906. This dispatch office remains in situ. The space holds aesthetic and historic significance in its office design and its purpose in managing information for railways. Many of the buildings within the Parcels Area have aesthetic significance in their early 20th century architecture and use of materials like brick and sandstone. The layout, joinery and fenestration appear to have changed little since the building's original construction in c.1906, aside from the introduction of services, thus they exhibit a high level of aesthetic significance and integrity.

The area has experienced various modifications, primarily in the demolition of the parcels dock platforms. The area has been predominately adaptively reused, with the Inwards Parcel Office converted into a YHA hostel and the former workshops into a CountryLink catering service. The interior has low integrity due to the introduction of contemporary fitouts and services.

### 3.17.4 Gradings of Significance and Integrity

**Table 39: Gradings of Significance and Integrity for the Parcels Area**

<sup>439</sup> Artefact, 2021

<sup>440</sup> Artefact, 2021

Element	Grading	Integrity
<b>Parcels Area Overall</b>	High	Moderate
<b>Views and Vistas</b>	Moderate	Moderate
<b>Context and Setting</b>	High	Moderate
<b>Ambulance Avenue</b>	High	Moderate
<b>Ramp to former Inwards Parcels Dock</b>	Moderate	Moderate
<b>Sandstone Walls, Brick Walls and Awning</b>	High	High
<b>CountryLink Catering</b>	Moderate	Moderate
<b>Offices (West Wing Extension Basement)</b>	High	Unknown
<b>Doors, Windows and Hardware</b>	High	High
<b>Floors and Paving</b>	Moderate	Moderate
<b>Ceilings and Cornices</b>	High	High
<b>Skirtings, Architraves and Linings</b>	High	High
<b>Applied Finishes</b>	Little	N/A
<b>Fitout</b>	Little	N/A
<b>Introduced Services; Mechanical, Electrical, Lighting and Data</b>	Intrusive	N/A
<b>Small structures on the northern side of lower Carriage Lane</b>	Intrusive	N/A
<b>Timber Loading dock</b>	Little	Little

### 3.17.5 Site Specific Conservation Policies

**Table 40: Site Specific Conservation Policies for the for the Parcels Area**

Policy	Policy Discussion and Guidelines
1. Follow Overall Conservation Policies	See Section 15 of this CMP for Conservation Policies
2. Implement Fabric Guidelines	See Section 15.4.4 of this CMP
3. Item Specific Policy: Heritage Process	When considering change follow the Heritage Flow Charts that form Appendix C of this CMP: A) Major Works Heritage Process Flow Chart; or B) Minor Works Heritage Process Flow Chart.

Policy	Policy Discussion and Guidelines
4. Item Specific Policy: Heritage Listing	<p>See Section 3 in this CMP for Legislative Context, and Section 14.4 for Statutory Context.</p> <p>Ensure appropriate statutory approvals or exemptions are obtained prior to change on the site because:</p> <ul style="list-style-type: none"> <li>• The site is within the overall listing for <b>Central Station</b> on the State Heritage Register under the <i>NSW Heritage Act 1977</i> for Sydney Terminal and Central Railway Stations Group.</li> <li>• The site is within the overall listing of the <b>Central Railway Station and Sydney Terminal Group</b> on Transport Asset Holding Entity's (TAHE) Heritage and Conservation Register under Section 170 of the <i>NSW Heritage Act 1977</i>.</li> <li>• The area is within the Railway Square/Central Station Special Character Area under the Sydney DCP 2012.</li> </ul>
5. Item Specific Policy: Masonry Conservation	<p>Conservation of the sandstone and brick facades should be undertaken as part of the cyclical maintenance of the building. Particular care and inspection should be undertaken for any projecting or decorative elements (such as parapets, dentils and mouldings) which has a higher risk of public injury. These should be regularly inspected to make sure the risk of falling is minimised and mitigated.</p> <p>A stone and brick strategy for the facades should be prepared to guide the conservation of the masonry.</p> <p>A strategy for the protection of the masonry facades during any major works should be undertaken by heritage architectural specialists and qualified stonemasons with heritage experience prior to the commencement of any works.</p>
6. Item Specific Policy: Fabric	<p>Specific elements for conservation include:</p> <ul style="list-style-type: none"> <li>• Sandstone and brick facades</li> <li>• Timber window and door joinery</li> <li>• Sandstone circular piers and reconstruction lamps</li> </ul>
7. Item Specific Policy: Development	<p>Prior to the commencement of any proposed development or works, photographic archival records of the Parcel Area should be undertaken.</p> <p>Ensure the original external colour schemes and finishes are documented and archivally recorded.</p>
8. Item Specific Policy: Use	<p>Promote compatible use for the area which allows for the sense of an open semi-industrial yard and views to fabric of High significance.</p>
9. Item Specific Policy: Intrusive fabric.	<p>Demolition of or changes to the existing intrusive sheds and fitout for should be allowed without formal approval provided no significant fabric is impacted on by the change.</p>
10. Item Specific Policy: Vistas	<p>Consider the impact of any new development on the adjacent significant buildings and elements.</p>
11. Item Specific Policy: Short term uses and changes.	<p>Do not allow negative impacts on significant fabric, spatial qualities and setting for short term gain.</p>



### 3.18 YHA RAILWAY SQUARE (FORMER INWARDS PARCEL SHED)



**Figure 409. Former Inwards Parcel Shed (YHA).**<sup>441</sup>



**Figure 410. Former Inwards Parcel Shed (YHA).**<sup>442</sup>

#### 3.18.1 History of the YHA Railway Square (Former Inwards Parcel Shed)

Built in 1906, the Inwards Parcels Shed was designed by Government Architect Gorrie Blair, supervised by Walter Liberty Vernon, as part of the first stage of the third Sydney Terminal.<sup>443</sup> The building was partially situated over the original site of the Benevolent Asylum, previously demolished in 1901.<sup>444</sup> Located along the western side of Platform 1, the building was purpose-built to receive parcels from across NSW. The building was part of a larger postal complex, which also included the Outwards Parcel Shed, an Inwards Parcels Dock, the Parcels Area and the Parcels Post Office.<sup>445</sup>

Originally constructed as a timber-framed structure with a gabled roof, the façade of the Inwards Parcels Shed was lined with corrugated iron and featured a loading dock and yard along the western side (Figure 411 and Figure 418).<sup>446</sup> Brick chimneys and corrugated fiberglass skylights were spread out along the roof plane on either side of the ridge. The corrugated iron roof extended across the building to cover and protect the platform and loading dock flanking the south-eastern and north-western sides of the building.<sup>447</sup> Sliding timber doors were located along the south-eastern and north-western facades of the building for easy delivery and removal from the Shed.<sup>448</sup> Early inventory sheets for the building note that cast iron columns were salvaged from the demolition of the earlier Sydney Terminal and reinstated to support a projecting canopy to the south of the building.<sup>449</sup>

<sup>441</sup> Artefact, 2021

<sup>442</sup> Artefact, 2021

<sup>443</sup> Weir Phillips Heritage, 2018. *Conservation Management Strategy, Former Inwards Parcels Shed Railway Square Sydney*; Urbis 2020. *Conservation Management Plan: Former Inwards Parcels Shed, 8-10 Lee Street, Haymarket*. Prepared for Vertical First Pty Ltd, 30 September 2020, p. 53.

<sup>444</sup> Artefact 2021. *Draft Central Precinct Renewal: Non-Aboriginal Heritage Study*. September 2021. Report to TfNSW, pp. 41-43.

<sup>445</sup> GML Heritage, 1999. *Inwards Parcels Shed, Sydney Terminal – Conservation Management Plan*. Report for Toga Building Company Pty Ltd; Urbis 2020. *Conservation Management Plan: Former Inwards Parcels Shed, 8-10 Lee Street, Haymarket*. Prepared for Vertical First Pty Ltd, 30 September 2020, p. 65.

<sup>446</sup> GML Heritage, 1999.

<sup>447</sup> Weir Phillips Heritage, 2018

<sup>448</sup> GML Heritage, 1999.

<sup>449</sup> GML Heritage, 1999.

However, more recent research indicates that the cast iron columns were instead utilised in the former Inwards Parcels Dock, which has since been demolished.<sup>450</sup>

Internally, the building was originally built as an open plan double height space. The floor plan was divided in the north into offices and two strong rooms, with an additional area designated for insured parcels.<sup>451</sup> Timber and steel columns supported the exposed roof rafters, with two rows running along the interior of the building, forming a nave. Several small brick bays were installed to provide additional support and were filled with fireplaces. The flooring comprised concrete poured into supporting iron sheeting formwork, forming the ceiling lining of the basement parcel area below. Contemporaneous with the construction of the Former Inwards Parcels Shed, a ramp was constructed to the west of the building in order to provide vehicular access from the Shed level down to Lee Street. The ramp was bounded by a brick retaining wall, additionally serving as the external walls for the basement area below the Former Inlands Parcels Shed, known as the Parcels Area.<sup>452</sup>

Originally, incoming parcels were delivered to the Inwards Parcels Shed using vans via the ramp to Lee Street and deposited on the loading dock along the western side of the shed. The parcels were brought inside via the sliding doors, sorted, and transferred to Platform 1 for transportation via trains through additional sliding doors along the south-eastern side of the shed. The Parcels Area, including the Small Parcels Bagging Room, below the shed connected to the tunnels beneath the station, allowing for transportation on other railway lines. The parcels that went through this process tended to stem from a booming mail-order system utilised by the department stores in Sydney to send parcels to regional NSW.<sup>453</sup>

The slow nature of the parcel sorting and delivery led to the employment of additional staff and alterations to the interior of the Inwards Parcels Office prior to 1913, in order to minimise delivery times and provide additional storage.<sup>454</sup> Ongoing extended waiting times for delivery collection led to further structural alterations to the Former Inwards Parcels Shed in 1920 in order to improve waiting times, although no details for the alterations are known.<sup>455</sup> In 1931, the use of the Inwards Parcels Shed was altered to an Outwards Parcels Shed, swapping roles with the then Outwards Parcels Shed.<sup>456</sup> This role appears to have continued at least until the 1960s (Figure 413).

It is unclear as to when the former Inwards Parcels Shed discontinued being used for its original function, but the last parcels train is recorded as leaving Sydney Terminal on 21 November 1988.<sup>457</sup> By 1996, the shed was used as an auction storage facility and was split into three sections: the Inwards Parcels Office; a cashier's office; and a lift lobby.<sup>458</sup> In 1999, the Inwards Parcels Shed was

<sup>450</sup> Urbis 2020. *Conservation Management Plan: Former Inwards Parcels Shed, 8-10 Lee Street, Haymarket*. Prepared for Vertical First Pty Ltd, 30 September 2020, p. 71.

<sup>451</sup> Urbis 2020. *Conservation Management Plan: Former Inwards Parcels Shed, 8-10 Lee Street, Haymarket*. Prepared for Vertical First Pty Ltd, 30 September 2020, p. 71.

<sup>452</sup> GML, 1999

<sup>453</sup> Urbis 2020. *Conservation Management Plan: Former Inwards Parcels Shed, 8-10 Lee Street, Haymarket*. Prepared for Vertical First Pty Ltd, 30 September 2020, p. 65.

<sup>454</sup> Weir Phillips Heritage, 2018

<sup>455</sup> Urbis 2020. *Conservation Management Plan: Former Inwards Parcels Shed, 8-10 Lee Street, Haymarket*. Prepared for Vertical First Pty Ltd, 30 September 2020, p. 72; The Sun, 5 January 1914, p. 5. Available online at: <http://nla.gov.au/nla.news-article221557191> (Accessed 29 October 2021).

<sup>456</sup> Weir Phillips Heritage, 2018

<sup>457</sup> The Sun, 'Railway Parcels Office', 25 Feb 1920, p.2. Available online at: <http://nla.gov.au/nla.news-article221380805> (Accessed 29 October 2021); Urbis 2020. *Conservation Management Plan: Former Inwards Parcels Shed, 8-10 Lee Street, Haymarket*. Prepared for Vertical First Pty Ltd, 30 September 2020, p. 72.

<sup>458</sup> Urbis 2020. *Conservation Management Plan: Former Inwards Parcels Shed, 8-10 Lee Street, Haymarket*. Prepared for Vertical First Pty Ltd, 30 September 2020, p. 72;

<sup>459</sup> Urbis 2020. *Conservation Management Plan: Former Inwards Parcels Shed, 8-10 Lee Street, Haymarket*. Prepared for Vertical First Pty Ltd, 30 September 2020, p. 77;

<sup>460</sup> Rappaport Pty Ltd & NSW Government Architect's Office, 2013. 3.18 YHA Railway Square (Former Inlands Parcel Shed), p. 1; Heritage Group State Projects & Department of Public Works & Services, 1996. Sydney/Central Station Conservation Management Plan. March 1996. p. 17.

set up with temporary site offices, storerooms and staffrooms for the development of the Henry Deane Park Plaza as part of the Central 2000 Strategic Asset Plan. The nearby Inwards Parcel Dock, Western Carriage Shed and dock awning demolished at this time. However, the access ramp and retaining wall were retained during the works.<sup>459</sup>

Following transformation of Henry Deane Plaza, the former Inwards Parcels Shed was converted into the Railway Square YHA in c.2003 to 2004, although the Development Application was submitted as early as 1999. This involved: the removal of partition walls, glass/louvre infills and roller doors; replacement of the roof lining, skylights, guttering and downpipes; the extension of existing and installation of new windows; installation of new skylights, lounge and dining areas, a new metal-framed entry, stairs and entry ramp, replica train carriages and a plunge pool; and removal of the infill slab and debris to the level of the rail track.<sup>460</sup> At this time, a number of the timbers were replaced with like for like elements due to degradation, while the timber-frame forming for southern wall was replaced with a steel frame due to its state of decay.<sup>461</sup> The YHA Railway Square signage was installed in 2004.

Since the opening of the YHA, the deck has been replaced, while the plunge pool was removed in 2015. Although the building is still used as the YHA, at the time of writing Atlassian has received permission to redevelop the site as part the Western Gateway Development as part of an State Significant Development Application.<sup>462</sup> As part of the Atlassian project, the building will be demolished and the materials salvaged, with only the timber frame being reused for reconstruction of the building.<sup>463</sup> The site is subject to the Western Gateway Sub-Precinct Design Guide. Further information can be located:

- <https://www.planning.nsw.gov.au/-/media/Files/DPE/Manuals-and-guides/Western-Gateway-Design-Guide.pdf>
- <https://pp.planningportal.nsw.gov.au/major-projects/projects/atlassian-office-and-hotel-development>

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<sup>459</sup> Urbis 2020. *Conservation Management Plan: Former Inwards Parcels Shed, 8-10 Lee Street, Haymarket*. Prepared for Vertical First Pty Ltd, 30 September 2020, p. 78; GML Heritage, 1999. *Inwards Parcels Shed, Sydney Terminal – Conservation Management Plan*. Report for Toga Building Company Pty Ltd.

<sup>460</sup> Urbis 2020. *Conservation Management Plan: Former Inwards Parcels Shed, 8-10 Lee Street, Haymarket*. Prepared for Vertical First Pty Ltd, 30 September 2020, p. 78.

<sup>461</sup> Urbis 2020. *Conservation Management Plan: Former Inwards Parcels Shed, 8-10 Lee Street, Haymarket*. Prepared for Vertical First Pty Ltd, 30 September 2020, p. 78.

<sup>462</sup> Urbis 2020. *Conservation Management Plan: Former Inwards Parcels Shed, 8-10 Lee Street, Haymarket*. Prepared for Vertical First Pty Ltd, 30 September 2020, p. 78.

<sup>463</sup> Urbis 2020. *Statement of Heritage Impact: Atlassian Central Development 8-10 Lee Street, Haymarket*. Prepared for Vertical First Pty Ltd, 30 September 2020.





Figure 411: 1906 photograph of the western façade of the Former Inwards Parcels Shed. The second Central Station is still extant to the right of frame while the present Central Station is to the left of frame. The Parcels Post Office has not been constructed; however, the area has been boarded up around the site.<sup>464</sup>

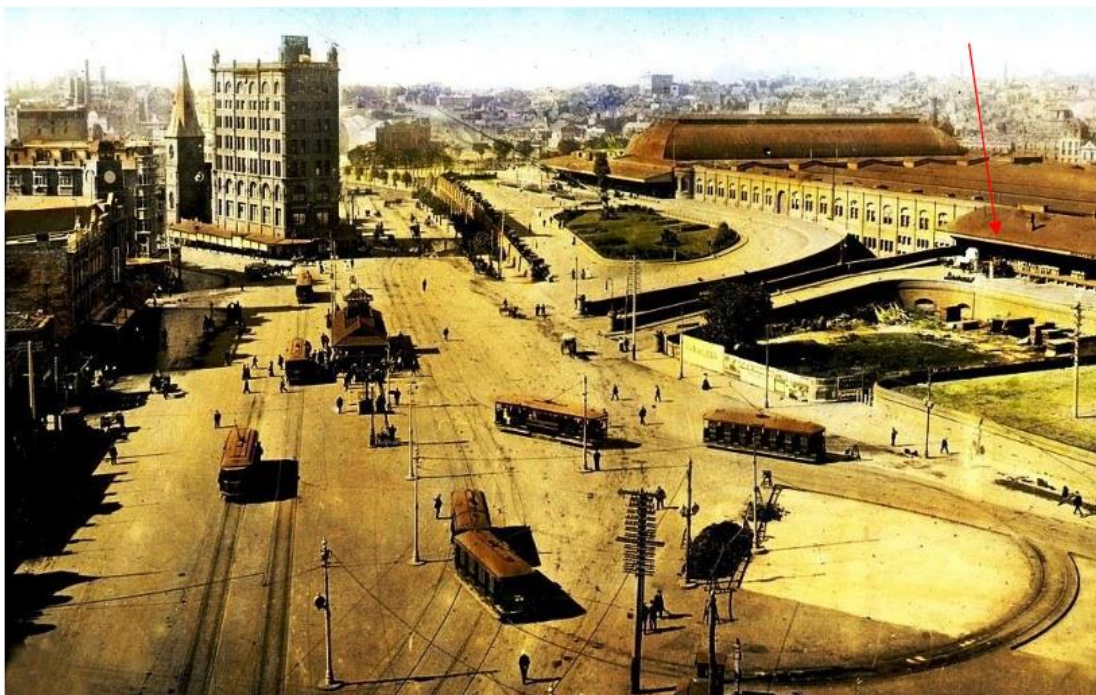


Figure 412: c.1906-1912 photograph of Railway Square, showing the Former Inwards Parcels Shed on the right side of the frame (indicated by red arrow).<sup>465</sup>

<sup>464</sup> State Library of New South Wales. *Panorama of Sydney Railway Station, old and new*, 4 August 1906 / photographer unknown. 4 photographic prints (7 panels) - panorama, gelatin silver - 26.2 x 135 cm. SPF/3060

<sup>465</sup> Flickr in Urbis 2020. *Conservation Management Plan: Former Inwards Parcels Shed, 8-10 Lee Street, Haymarket*. Prepared for Vertical First Pty Ltd, 30 September 2020, Figure 85.



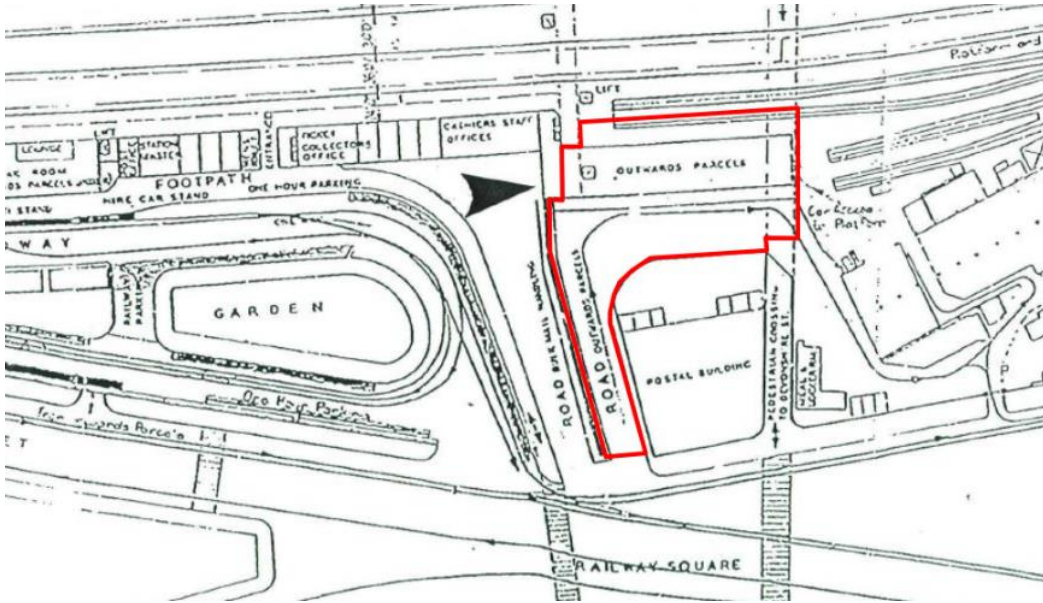


Figure 413: 1965 Plan of Sydney Terminal and Central Station, demonstrating the use of the Inwards Parcels Shed for outwards parcels (the Parcels Area, Inwards Parcels Shed and the ramp outlined in red).<sup>466</sup>



Figure 414: Inside the Former Inwards Parcels Shed, date unknown.<sup>467</sup>



Figure 415: Outside the Former Inwards Parcel Shed, date unknown.<sup>468</sup>

<sup>466</sup> SRA Archives, GML CMP 1999, Figure 2.5 in Urbis 2020. *Conservation Management Plan: Former Inwards Parcels Shed, 8-10 Lee Street, Haymarket*. Prepared for Vertical First Pty Ltd, 30 September 2020, Figure 85.

<sup>467</sup> Sydney Trains Archives, date unknown. ID: 17420\_a014\_a014000283.

<sup>468</sup> ARHS, Sydney Trains Archives, date unknown. ID: 033266.



Figure 416: Worker inside the Former Inwards Parcel Shed, date unknown.<sup>469</sup>



Figure 417: Workers on the deck of the Former Inwards Parcel Shed, date unknown.<sup>470</sup>



<sup>469</sup> ARHS, Sydney Trains Archives, date unknown. ID: 123720.

<sup>470</sup> ARHS, Sydney Trains Archives, date unknown. ID: 124731.



**Figure 418: A 1928 photograph from the Western Forecourt, showing the northern elevation of the Former Inwards Parcels Shed and the brick retaining wall along the ramp of Upper Carriage Lane.<sup>471</sup>**

**Figure 419. Interior of the Inwards Parcel Shed <sup>472</sup>**

### 3.18.2 Site Description and Condition

The Former Inwards Parcels Shed, now the YHA Railway Square, is bounded by Upper Carriage Lane to the west, Ambulance Avenue (Lower Carriage Lane) to the north, Platform 1 of Sydney Terminal to the east and Henry Deane Plaza to the south. The L-shaped loading ramp along Upper Carriage Lane to the west of the building provides vehicular access from the Shed level down to Lee Street. More broadly, the Former Inwards Parcels Shed is located to the east of Railway Square, to the west of the Intercity and Country platforms of Sydney Terminal (Platforms 1-13) and to the south of the Western Forecourt. The building is tucked in behind the Parcels Post Office (Adina Apartment Hotel).

The Former Inwards Parcels Shed comprises a parcel distribution shed, built in the railway/industrial vernacular style, oriented roughly north-east to south-west. The overall form of the building comprises a double-height one-storey rectilinear building with a gabled roof, with twelve bays along the main north-western elevation and two bays along the north-eastern elevation. The building is timber-framed, with external timber-framing and half-timbering. Although the corrugated iron roofing and cladding were replaced c.2003-2004, the use of corrugated iron has resulted in the continuation of the original materials of the building. A small number of original face-brick bays with chamfered sandstone plinths form piers along the north-western and south-eastern elevation, coinciding with the internal fireplaces and face-brick chimneys extant above the roof. The corrugated iron roof extends over the south-eastern and north-western sides of the building, forming timber-framed cantilevered awnings over the platform and loading dock flanking the building. Although the openings along the north-western and south-eastern elevations were originally sliding timber doors, these have largely been replaced with aluminium-framed multi-pane windows and doors, exhibiting fixed and hinged casement panes, dating to the c.2003-2004 renovation. Only two original timber-framed windows have been retained, located along the north-western elevation of the building, albeit with aluminium-framed extensions above these windows. The original loading platform, comprising a concrete dock with timber sleeper edging on concrete piers, is located along the north-western side of the building. A concrete ramp with rendered balustrade abuts the dock near the southern end. A concrete ramp and set of stairs also abut the ramp at the northern end. The original skylights between the concrete piers of the dock have been replaced with corrugated iron panels. A timber valance has been retained on the north-eastern end of the north-eastern awning above the loading dock. Asphalt lines the surface of Upper Carriage Lane.

A two-storey amenities building is located near the north-eastern end of the Former Inwards Parcels Shed and a single-storey dining and lounge area and timber deck are located at the south-western end of the building. Both additions exhibit steel framing, cladding and aluminium-framed windows and doors. The amenities block is connected to the Former Inwards Parcels Shed via a glazed-roofed hallway, resulting in the removal of a section of the awning along the south-eastern side of the buildings. The dining/lounge extension abuts the north-western awning. Four replica train carriages used as dormitories are located along the former platform on the south-western side of the building,

<sup>471</sup> Fairfax Corporation 1928. *Crowds watching the Light Horsemen guard of honour on Field Marshall Lord Edmund Allenby's arrival at Sydney Central Station, Sydney, 18 January 1928*, 1. Accessed online at: <http://nla.gov.au/nla.obj-163216372> (2 November 2021).

<sup>472</sup> *Interior of the Inwards Parcels Shed*. Circa 1973. In McKillop, Ellsmore and Oakes. *A Century of Central*. Redfern: Australian Railway Historical Society New South Wales Division, 2008.

between the building and the south-western end of Platform 1 of Sydney Terminal. The concrete platform, resting on brick piers, was refinished in c.2003-2004. Timber platforms lead to the train carriage dormitories and frosted glass screens obscure direct views between Platform 1 and the YHA area.

Internally, timber posts and steel columns support the exposed roof rafters, with two rows running along the interior of the building, forming a nave and side aisles. The freestanding timber posts are surmounted by timber struts supporting the exposed roof structure of rafters and purlins, connected to the timber posts with steel plates and straps. Several small brick bays with fireplaces provide additional structural support.<sup>473</sup> The flooring comprises concrete poured into supporting iron sheeting formwork, forming the ceiling lining of the basement parcel area below.<sup>474</sup> The south-western platform awning framing exhibits an original/early timber sign, which reads: 'No Thoroughfare/ Public Not Allowed on this Platform'.

A mezzanine level dating to the c.2003-2004 renovation runs along the length of the building, with double-height spaces retained at the northern and southern ends of the shed. Staircases at the southern and northern ends lead to the mezzanine. The interior is divided into a number of spaces by partitions, including dormitories within the central area and northern end, accessed by a central hallway, on both levels. A reception area, offices and service rooms are located at the southern end on the ground floor, while a laundry room and dormitories are located at the southern end of the first floor. The reception area exhibits polished concrete flooring, while the floors in the dormitories are lined with carpet. Four of the original timber sliding doors have been retained within the interior of the building.

The exterior of the building appears to generally be in good condition. The brickwork, timberwork, sandstone, corrugated iron, windows and doors are in good condition, with no discernible defects. The internal fit-out appears to be in good condition.

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<sup>473</sup> GML CMP 1999.

<sup>474</sup> GML CMP 1999.





**Figure 420. Eastern end of Ambulance Avenue, showing west wing extension building in background and entrance to Former Inward Parcels Shed basement level. (Source: Artefact Heritage, 2021.)**



**Figure 421. External view of YHA Hostel extension.<sup>475</sup>**



**Figure 422. External view of YHA Hostel, facing former Parcels Office, now Adina Hotel.<sup>476</sup>**

<sup>475</sup> Artefact, 2021

<sup>476</sup> Artefact, 2021



**Figure 423. External view of YHA Hostel car park.**<sup>477</sup>



**Figure 424. External view of YHA Hostel along deck.**<sup>478</sup>

### 3.18.3 Statement of Significance

The YHA Railway Square (Former Inlands Parcel Shed) was built in c.1906 has historical significance, forming part of the development of the third Sydney Terminus. It is historically associated with the Government Architect Gorrie Blair, who designed the Former Inwards Parcel Shed, and Walter Liberty Vernon, who supervised the construction. The Former Inwards Parcel Shed is also historically significant for the imperative role it played in the receipt of parcels across NSW during the start of the twentieth century. As such, the Former Inwards Parcel Shed has associative significance pertaining to the Australian Post mailing company and other contractors who used the office area such as Messrs Whitehead and Co.

Due to various modifications which occurred, particularly during the early 2000's when the space was converted into Railway Square YHA, the Former Inwards Parcel Shed has lost most of its original fabric, but retains its original timber columns and timber roof structure.

The YHA Railway Square (Former Inlands Parcel Shed) has been assessed as having local heritage significance.

### 3.18.4 Gradings of Significance and Integrity

*The following table should be read in conjunction with the gradings of significance outlined in the site specific CMP prepared by Urbis for the Former Inwards Parcel Shed.*

**Table 41: Gradings of Significance and Integrity for the YHA Railway Square (Former Inwards Parcel Shed)**

Element	Grading	Integrity
<b>YHA Railway Square Overall</b>	Moderate	Moderate
<b>Views and Vistas</b>	Moderate	Moderate
<b>Context and Setting</b>	High	Moderate
<b>Parcels Shed Yard</b>	Moderate	Moderate
<b>Roof and Awning</b>	Moderate	Moderate

<sup>477</sup> Artefact, 2021

<sup>478</sup> Artefact, 2021

Element	Grading	Integrity
Timber Columns and Trusswork	High	High
Remnant Brick Wall and Sandstone Plinth	High	Moderate
Corrugated Iron Cladding	Moderate	Moderate
Platform	Moderate	Moderate
Doors, Windows and Hardware	Little	N/A
Floors and Paving	Little	N/A
Ceilings and Cornices	Little	N/A
Carriage Accommodation	Little	N/A
Contemporary Awning and Glazed Barrier	Little	N/A
Fitout	Little	N/A
Introduced Services; Mechanical, Electrical, Lighting and Data	Intrusive	N/A

### 3.18.5 Site Specific Conservation Policies

The following site specific conservation policies should read in conjunction with the site specific Draft CMP written by Urbis and the Western Gateway Sub-Precinct Design Guide.

**Table 42: Site Specific Conservation Policies for the for the YHA Railway Square (Former Inwards Parcel Shed)**

Policy	Policy Discussion and Guidelines
1. Follow Overall Conservation Policies	See Section 15 of this CMP for Conservation Policies
2. Implement Fabric Guidelines	See Section 15.4.4 of this CMP.
3. Item Specific Policy: Heritage Process	When considering change follow the Heritage Flow Charts that form Appendix C of this CMP: A) Major Works Heritage Process Flow Chart; or B) Minor Works Heritage Process Flow Chart.

Policy	Policy Discussion and Guidelines
4. Item Specific Policy: Heritage Listing	<p>See Section 3 in this CMP for Legislative Context, and Section 14.4 for Statutory Context.</p> <p>Ensure appropriate statutory approvals or exemptions are obtained prior to change on the site because:</p> <ul style="list-style-type: none"> <li>• The site is within the overall listing for <b>Central Station</b> on the State Heritage Register under the <i>NSW Heritage Act 1977</i> for Sydney Terminal and Central Railway Stations Group.</li> <li>• The site is within the overall listing of the <b>Central Railway Station and Sydney Terminal Group</b> on Transport Asset Holding Entity's (TAHE) Heritage and Conservation Register under Section 170 of the <i>NSW Heritage Act 1977</i>.</li> <li>• The area is within the Railway Square/Central Station Special Character Area under the Sydney DCP 2012.</li> </ul>
5. Item Specific Policy: Masonry Conservation	<p>Conservation of the sandstone and brick facades should be undertaken as part of the cyclical maintenance of the building. Particular care and inspection should be undertaken for any projecting or decorative elements (such as parapets, dentils and mouldings) which has a higher risk of public injury. These should be regularly inspected to make sure the risk of falling is minimised and mitigated.</p> <p>A stone and brick strategy for the facades should be prepared to guide the conservation of the masonry.</p> <p>A strategy for the protection of the masonry facades during any major works should be undertaken by heritage architectural specialists and qualified stonemasons with heritage experience prior to the commencement of any works.</p>
6. Item Specific Policy: Fabric	<p>Specific elements for conservation include:</p> <ul style="list-style-type: none"> <li>• Sandstone and brick facades</li> <li>• Timber structure, including half-timbering, timber purlins, internal posts, struts and exposed rafters</li> <li>• Timber window and door joinery</li> <li>• Steel internal structure</li> </ul>
7. Item Specific Policy: Use	<p>Promote compatible use for the building which allows for demonstration of the former industrial use of the interior including large areas of exposed roof trusses.</p>
8. Item Specific Policy: Development	<p>Prior to the commencement of any proposed development or works, photographic archival records of the Former Inwards Parcel Shed should be undertaken.</p> <p>Ensure the original external colour schemes and finishes are documented and archivally recorded.</p>
9. Item Specific Policy: Alterations and Additions	<p>External alterations and additions to the Former Inlands Parcel Shed should be avoided. Do not paint the sandstone or masonry facades.</p> <p>The open area/loading dock adjacent to the Former Inlands Parcel Shed currently used for parking can be used for sympathetic new development. New development should be of a scale which is appropriate for the site, taking into consideration physical and visual impacts on the surrounding structures and fabric of significance, particularly the Former Inwards Parcel Shed, Western Forecourt and West Wing Extension.</p> <p>Demolition of or changes to the existing internal lightweight fitout for YHA use is permissible and should be allowed without formal approval provided no significant fabric is impacted on by the change.</p>



Policy	Policy Discussion and Guidelines
10. Item Specific Policy: Interpretation	<p>See Sections 9 and 14.8 'Heritage Interpretation' in this CMP.</p> <p>Prepare a heritage interpretation strategy, based on the precinct-wide Central Precinct Heritage Interpretation Strategy, specific to the Former Inwards Parcel Shed as part of Central Precinct. Particular focus should be given to the former landscape and cultivation of the space.</p> <p>Following this, develop and implement a detailed site-specific heritage interpretation plan, aligned with the themes and recommendations of the heritage interpretation strategy.</p>
11. Item Specific Policy: Vistas	<p>Ensure vistas to the Former Inwards Parcel Shed outlined in Section 7.6 of this CMP are not obscured. These include vistas from:</p> <ul style="list-style-type: none"> <li>• Western Forecourt and Railway Colonnade Drive</li> <li>• Ambulance Avenue</li> <li>• Henry Deane Plaza</li> <li>• Former Parcels Post Office (Adina Apartment Hotel)</li> <li>• West Wing Extensions</li> </ul>
12. Item Specific Policy: Short term uses and changes.	<p>Do not allow negative impacts on significant fabric, spatial qualities and setting for short term gain. E.g. commercial signage.</p>

## 3.19 FORMER PARCELS POST OFFICE



**Figure 425. Former Parcels Post Office (now Adina Apartment Hotel).**<sup>479</sup>



**Figure 426. Former Parcels Post Office (now Adina Apartment Hotel) as viewed from Railway Square.**<sup>480</sup>

### 3.19.1 History of the Former Parcels Post Office

In 1910 construction began on a large Parcels Post Office, located to the west of the Inwards Parcel Shed and Parcels Area, however the early work was limited to excavation. The design of the building is credited to the Government Architect of the time George McRae, although the initial design for the building was in fact prepared by Gorrie McLeish Blair, with large portions of the building's design having been designed or approved by preceding Government Architects, Walter Liberty Vernon and E.L.Drew.<sup>481</sup> Construction on the building was slow, with development halted by the requirement for substantial excavation in the area to construct the foundations for the building, especially in response to the addition of an extra two storeys to the design.<sup>482</sup> The building was designed to be connected to the new Sydney Terminal through tunnels which ran under the Parcels Shed and were accessible through the large sandstone archway located along the eastern side of the Parcels Area ramp's retaining wall. The Parcel Post Office was designed to serve as the main parcel sorting for the station and rail network – distributing large volumes of parcels in the early 20th century. Its location in such close proximity to the station indicative of the importance of the railway to the delivery of post and parcels to the state and country at large.<sup>483</sup>

Although Gorrie McLeish Blair from the Government Architects Office designed the building in two stages, it was constructed as one. Government Architect, Walter Liberty Vernon oversaw the first stage of design in 1910, while the second stage, from 1912-1913, was supervised by Vernon's successor, George McRae. The first design featured oueil-de-boeuf windows of which the building would terminate at the cornice above. The second design stage saw an increase to the height of the building and the addition of an upper cornice and pediments. In this design, the cornice from Stage 1 was converted to both a string course and balustrading.

With the increase in mail traffic, the Parcels Post Office was used to support all the postal services at Central Station, and therefore playing a significant role in the promotion of the station as a hub for

<sup>479</sup> Artefact, 2021

<sup>480</sup> Artefact, 2021

<sup>481</sup> DPIE, 2006. *Former Parcels Post Office Including Retaining Wall, Early Lamp Post and Building Interiors*. SHI Inventory database. Retrieved 07/04/21 from: [environment.nsw.gov.au/heritageapp/ViewHeritageItemDetails.aspx?ID=2424235](http://environment.nsw.gov.au/heritageapp/ViewHeritageItemDetails.aspx?ID=2424235)

<sup>482</sup> DPIE, 2006.

<sup>483</sup> GML, 1999.

state-wide railway postal services. In order to facilitate the increasing operations, extensions to the building were proposed in the 1920s. It was suggested that trusses would be used to extend the second and third floors over the existing railway parcels office at Central, however this concept never eventuated.

The Parcels Post Office was constructed using techniques that had never been used in Sydney before. While the frame of the building was assembled from concrete encased, I-beams and stanchions, the external façade was created from load bearing masonry walls. This allowed the creation of large, open interior spaces that were suited to the building's function. Due to the building regulations at the time, the expression of the frame on the building's exterior was not permitted, and therefore the load bearing walls were designed and constructed to act as an external skin. A hierarchy of materials was employed at Central Station, where sandstone was used for the most important buildings. As a result, the Parcel Post Office was considered secondary to the main public buildings and was therefore constructed from brick, with minimal sandstone detailing.

Upon completion, the Parcels Post Office spanned across multiple floors. It contained a basement that was used as the sorting room and originally linked to the subway tunnel system of the Main Terminus Building, and there was a spiral staircase which connected it to the supervisor's room above. The ground floor housed a strong room, vehicle dock and toilets as well as a public area, which was separated from the parcel's office by a long counter. Primary mail activities occurred on the first, second and fifth stores and to accommodate this function an open plan design was employed on these levels. Detective galleries, which were used to monitor staff, were located on the first and second floors and offices were located on the fourth floor. Due to the nature of the workforce, the building only contained male toilets.

Multiple elements of the Parcels Post Office building were removed during the late 20th century. These included the exterior parcel chutes, which were located on the eastern façade and spanned the height of the façade, and an awning located above ground level. Due to falling debris from the masonry façade, this awning was removed as a safety precaution. At this time, the use of the building as a post office also ceased. After remaining unoccupied during the 1990s, the building was leased to TOGA Group in 2000. The Group intended to transform the former Parcels Post Office into one of the Medina Apartment Hotels, requiring the building to be significantly modified. A two-storey mansard roofed extension was added to the top of the building, facilitating the creation of loft apartments. The conversion from a warehouse to an apartment complex also resulted in significant alteration to the interior space and the relocation of the main entrance from the western façade, facing Lee Street, to the northern façade. The building has been renamed as Adina Apartment Hotel and continues to operate as a residential and short-stay accommodation building.

At the time of writing a State Significant Development Application (SSDA) has been approved for the redevelopment of the adjacent Inward Parcels Office (YHA Railway Square). Another two SSDAs were being prepared for the area known as the 'Western Gateway' which seek approval for the redevelopment of the Former Parcel Post Office and adjacent Henry Deane Plaza. These SSDAs are subject to the Western Gateway Sub-Precinct Design Guide. Further information can be located:

- <https://www.planning.nsw.gov.au/-/media/Files/DPE/Manuals-and-guides/Western-Gateway-Design-Guide.pdf>
- <https://pp.planningportal.nsw.gov.au/major-projects/projects/atlassian-office-and-hotel-development>



Figure 427: Railway Square with the Parcels Post Office in the front right and the Main Terminus Building beyond. Note the Clocktower was in constructions. <sup>484</sup>



Figure 428: Southern view of the Parcels Post Office under construction <sup>485</sup>



Figure 429: Parcel Post Office building c. 1989 prior to the removal of the awning. <sup>486</sup>



Figure 430: Parcel Post Office building date unknown. <sup>487</sup>

<sup>484</sup> City of Sydney Archives, Railway Square, 1914

<sup>485</sup> NSWGR, *Central Station and Broadway precinct, Sydney, NSW*. 1910s. Photography. ARHS via UON Living Histories Digital Archive. Reference: ARHSBox004\_0074. Accessed online at <https://livinghistories.newcastle.edu.au/>.

<sup>486</sup> City of Sydney Archives, id. A-00024063

<sup>487</sup> Sydney Trains Archives, date unknown. ID: FB\_IMG\_ND



### 3.19.2 Site Description and Condition

The former Parcels Post Office, now the Adina Apartment Hotel, is bounded by Lee Street to the west, the Upper Carriage Lane to the north, the YHA Railway Square (the former Inlands Parcel Shed) to the east and Henry Deane Plaza to the south. More broadly, the former Parcels Post Office is located to the east of Railway Square, to the west of the Intercity and Country platforms of Sydney Terminal (Platforms 1-13) and to the south of the Western Forecourt. The building is located on a prominent site at the corner of Lee Street and Ambulance Avenue, with entrances along Lee Street, Ambulance Avenue and Railway Square, and a private entrance within the garden and pool area to the east. The position and size of the building affords uninterrupted views from Railway Square, the Main Terminus Building and Pitt, Quay and George Streets. To the rear (eastern side) of the building is a garden and pool for patrons of the hotel, bounded by a brick retaining wall that curves around the ramp up to the Former Inwards Parcel Shed (now, the YHA Railway Square).

The former Parcels Post Office is a six storey Federation Academic Classical style composed of a four-storey stage and two-storey stage above, both dating to 1912-1913. A basement level is located below the ground floor and the fifth floor is surmounted by a two-storey c.2000 addition. To the rear, at the south-eastern corner of the building, is a c.2000 one-storey extension.

The original 1912-1913 building is predominantly constructed from exposed red bricks laid in English bond forming rusticated horizontal bands of brickwork and recessed vertically form articulated engaged piers. Sandstone detailing surrounds the windows, doors and rounded arches, as well as the stringcourses and quoins. The base of the building, including the basement, is constructed from large rusticated trachyte blocks, forming a plinth interspersed with thin windows for the basement below. The fourth and fifth floors continue the style of the first four storeys, but are stepped back from the elevations of the lower floors. Although the external masonry walls are load bearing, the building also contains a concrete-encased internal steel frame, with reinforced concrete slabs along each floor. The windows are primarily steel-framed, with bronze mullions and glazing bars along the elevations on the ground floor. The basement windows have been infilled with brickwork or are covered with scroll-shaped grilles. Glass brick windows are located beneath the bronze-framed windows on the ground floor of the northern and southern elevations, and glazed tiles with wrought ironwork have been used for the basement skylights.

The eastern, western northern and southern elevations comprise seven bays, although the end bays along all elevations comprise curved concave corners at the fourth and fifth floor level, and curved convex corners from ground to third floor level on the north-western and south-western, Lee Street facing corners of the building. The curved convex corners and the adjoining bays generally feature single multi-paned steel-framed windows on the ground to second floors and steel-framed *oeil-de-bouef* windows with sandstone edging along the third floor. At the fourth and fifth floor level, the curved corner bays are concave, forming exedras, and appear on all four corners of the building, featuring steel-framed multi-paned double windows with sandstone surrounds. The central three bays along the northern, western and southern elevations display triple multi-light steel-framed windows along the first to fifth floors, surmounted by sandstone entablature featuring dentilled cornices along the top of the third floor. Sandstone balustrades appear above the cornices and in front of the windows on the fourth floor along the southern and northern elevations, forming a parapet at the top of the third floor. The western elevation displays double multi-paned steel framed windows along the first to fifth floors, aside from the central bay, which features triple steel-framed windows. All four elevations display a banded rusticated arch above a Diocletian window along the central bay on the fourth floor. The elevations along the fifth floor exhibit decorative sandstone relieving arches along the second and sixth bays and entablature featuring stone cornices and ornamental corbels along the central bays.

The western elevation displays two recesses: one on the ground floor serving as the original entrance; and one spanning the first to third floors, both of which are located in the central bay. The ground floor exedra features a simple Romanesque rusticated sandstone arch surmounted by a carved sandstone British Coat of Arms, with the British Monarch's motto 'DIEU ET MON DROIT' engraved on a scroll beneath. The first to third floor exedra is flanked by iconic columns and surmounted by entablature with a dentil cornice and columned balustrade. A central stone along the entablature features an embossed 'G|R' (George Regent) over a carved wreath. The metal letters reading 'Post Office' on the northern and western elevations have been retained in situ. The ground floor elevations feature tripartite windows with multi-paned upper elements and an arched central multi-paned window or timber door. These elements along the southern elevation lead into three retail/food service tenancies. The ground floor of the eastern elevation exhibits three segmental arched windows with painted timber framing and a contemporary glass door.

The c.2000 rear extension at the south-eastern corner of the building is cement-rendered in bands to extend the lines of the banded rustication along the western façade, with black marble cladding along the base of the wall and large multi-paned windows with inset doors, providing access into the retail tenancies within the extension. Glass and steel cantilevered awnings cover the entrances into the tenancies. The mansard roof of the c.2000 two-storey addition above the fifth floor of the building is clad in sheet metal panels interspersed by dormer windows. The rooms within the top floor lead onto a tiled balcony area divided by translucent glass barriers.

The majority of the building is currently used by the Adina Apartment Hotel (formerly Medina Hotel), converted into apartments in c.2000, with a boardroom and reception area on the ground floor. Retail/food service tenancies occupy the southern end of the ground floor. Although the original elevations, floor slabs, columns and majority of the fenestration has been retained, most of the original interior fabric, including partitions and joinery, has been removed, leaving little evidence of the original layout.<sup>488</sup>

The exterior of the building appears to generally be in good condition. The brickwork, trachyte blocks, windows and doors are in good condition, with no discernible defects. The sandstone elements are generally in good condition, aside from black staining and spalling on exposed elements, such as the cornices, British Coat of Arms and stringcourses. The interior was not inspected and therefore, the condition is unknown.



**Figure 431. External view of Post Office building as seen from Pitt Street.**<sup>489</sup>



**Figure 432. External view of Post Office Building as seen from Lee Street entrance.**<sup>490</sup>

<sup>488</sup> Rappaport Pty Ltd & NSW Government Architect's Office, 2013. 3.19 Parcels Post Office (Medina Hotel), p. 1; Heritage Group State Projects & Department of Public Works & Services, 1995. *Central 2000 Statement of Heritage Impact*. August 1995. p. 17.

<sup>489</sup> Artefact, 2021

<sup>490</sup> Artefact, 2021



Figure 433. External view from Post Office Building looking towards Lee Street / George St.<sup>491</sup>



Figure 434. External view of Post Office Building looking towards Railway Square.<sup>492</sup>



Figure 435. External view of Post Office Building as seen from Railway Square.<sup>493</sup>



Figure 436. External view of Post Office Building signage.<sup>494</sup>



Figure 437. External view of Post Office Building loading dock entry.<sup>495</sup>



Figure 438. Part of former fence/signage to Parcels Office on retaining wall.<sup>496</sup>

<sup>491</sup> Artefact, 2021

<sup>492</sup> Artefact, 2021

<sup>493</sup> Artefact, 2021

<sup>494</sup> Artefact, 2021

<sup>495</sup> Artefact, 2021

<sup>496</sup> Artefact, 2021





Figure 439. View of lamp post.<sup>497</sup>

### 3.19.3 Statement of Significance

*This Statement of Significance has been written in conjunction with Urbis who has written the site specific CMP for the Former Parcel Post Office.<sup>498</sup>*

The former Parcels Post Office is of state heritage significance for its historic, associative, aesthetic and representative values and for its rarity.

The Parcels Post Office building is historically significant for its association with the development of Central Station and the third Sydney Terminus (albeit constructed a decade later) and connections with the NSW rail and postal services in the early 1900s. The building also holds historical significance for its imperative role in the promotion of Central Station and as the main office in New South Wales for state-wide postal services during the early twentieth century. In the early 1900s, the majority of parcels were sent by rail, and the location of the site adjacent to the station reflects the importance of shipping by rail. The Parcels Post Office also worked in conjunction with the inward and outward parcel platforms in the western yard precinct (which includes the former inwards parcels platform, now the YHA).

The Former Parcels Post Office has associative significance pertaining to the Australia Post mailing company. As well as in serving as the main parcels sorting office between 1913 and 1960s, until the office's relocation to Redfern. Designed by the Government Architects Office (GAO), the building has also been assessed to have associative significance on a local level, for its association with the GAO broadly and more specifically Gorrie McLeish Blair, to whom the design was attributed, and Walter Liberty Vernon, who was the Government Architect at the time.

The building is designed in the Federation Free Academic Classical architectural style and is a centrepiece in the locality. The facades of the building are highly intact, therefore exhibiting high aesthetic significance. The building has a number of distinct external features that are attributed to the style, including the contrasting brick and rusticated stone, the giant order Ionic columns to the principal façade, the heavy sandstone entablature, pediments and oeil de boeuf windows. Due to various modifications during the twentieth century, multiple elements of the original Parcels Office were removed, these include: the exterior parcel chutes and the main awning. The interior is restrained in its original form and was modified c.2000 in conjunction with the Hotel (Adina) conversion including structural modifications for a new lift core. As a result, the interior has lost significance.

<sup>497</sup> Artefact, 2021

<sup>498</sup> Urbis, *Draft Former Parcel Post Office Conservation Management Plan*, 2022



The former Parcels Post Office building may also have some technical significance and research value as a relatively early example of the use of partial steel framework I-beams and stanchions encased in concrete to maximise internal floor areas.

As one of only two large metropolitan parcels offices constructed (the other being the Melbourne Mail Exchange) in the early twentieth century, the site has been assessed as rare. The size and prominence of the building is indicative of the significance of both the postal service and the railways in the early twentieth century however the connection between the two departments is uniquely represented in the Parcel Post Office building.

The former Parcels Post Office is representative of expansion and growth in the early twentieth century. However, the scale and grandeur of the building does differentiate the building from the built of postal buildings constructed during the period. The building is also representative of the Federation Free Classical architectural style of civic buildings designed in the GAO and of the work of Gorrie McLeish Blair.

The Parcels Post Office Building is of high heritage significance in the context of the Central Railway Station Group.

### 3.19.4 Gradings of Significance and Integrity

*The following table should be read in conjunction with the gradings of significance outlined in the site specific CMP prepared by Urbis for the Former Parcel Post Office.*

**Table 43: Gradings of Significance and Integrity for the Former Parcels Post Office**

Element	Grading	Integrity
<b>Parcels Post Office (Adina Hotel) Overall</b>	High	High
<b>Views and Vistas</b>	Exceptional	Low
<b>Context and Setting</b>	Exceptional	Moderate
<b>Landscaped Pool Area</b>	Little	N/A
<b>Brick Retaining Wall</b>	High	High
<b>Glass Brick Pavement Lights</b>	High	High
<b>Façades</b>	High	High
<b>Fenestration</b>	High	High
<b>Bronze Glazing Bars</b>	High	High
<b>Roof Addition</b>	Little	N/A
<b>Doors and Hardware</b>	Little	N/A
<b>Floors and Tiling</b>	Little	N/A
<b>Skirtings, Architraves and Linings</b>	Little	N/A
<b>Ceilings and Cornices</b>	Little	N/A
<b>Applied Finishes</b>	Intrusive	N/A
<b>Fitout</b>	Intrusive	N/A

Element	Grading	Integrity
Introduced Services; Mechanical, Electrical, Lighting & Data	Intrusive	N/A

### 3.19.5 Site Specific Conservation Policies

The following site specific conservation policies should read in conjunction with the site specific Draft CMP written by Urbis and the Western Gateway Sub-Precinct Design Guide.

**Table 44: Site Specific Conservation Policies for the for the Former Parcels Post Office**

Policy	Policy Discussion and Guidelines
1. Follow Overall Conservation Policies	See Section 15 of this CMP for Conservation Policies.
2. Implement Fabric Guidelines	See Section 15.4.4 of this CMP
3. Item Specific Policy: Heritage Process	<p>When considering change follow the Heritage Flow Charts that form Appendix C of this CMP:</p> <p>A) Major Works Heritage Process Flow Chart; or B) Minor Works Heritage Process Flow Chart.</p>
4. Item Specific Policy: Heritage Listing	<p>See Section 3 in this CMP for Legislative Context, and Section 14.4 for Statutory Context.</p> <p>Ensure appropriate statutory approvals or exemptions are obtained prior to change on the site because:</p> <ul style="list-style-type: none"> <li>The site is within the overall listing for <b>Central Station</b> on the State Heritage Register under the <i>NSW Heritage Act 1977</i> for Sydney Terminal and Central Railway Stations Group.</li> <li>The site is within the overall listing of the <b>Central Railway Station and Sydney Terminal Group</b> on Transport Asset Holding Entity's (TAHE) Heritage and Conservation Register under Section 170 of the <i>NSW Heritage Act 1977</i>.</li> <li>The area is within the Railway Square/Central Station Special Character Area under the Sydney DCP 2012.</li> </ul>
5. Item Specific Policy: CMP and other Management documents.	<p>Prior to any major program of change, the Conservation Management Plan for this building should be updated. Specifically examine the building's role in the Railway Square precinct. A Draft Conservation Management Plan for the building is currently being prepared by Urbis, due for completion 2022. Refer to the Draft CMP for specific design guidance in relation to the building.</p> <p>Prepare and implement an overall Commercial Tenancy and Signage Strategy for the site. Consider including other issues such as disability access and outdoor seating.</p> <p>These documents may allow for site specific exemptions under the NSW Heritage Act 1977.</p> <p>A cyclical maintenance plan should be prepared to guide the conservation and maintenance of the building.</p>

Policy	Policy Discussion and Guidelines
6. Item Specific Policy: Masonry Conservation	<p>Conservation of the sandstone and brick facades should be undertaken as part of the cyclical maintenance of the building. Particular care and inspection should be undertaken for any projecting or decorative elements (such as parapets, dentils and mouldings) which has a higher risk of public injury. These should be regularly inspected to make sure the risk of falling is minimised and mitigated.</p> <p>A stone and brick strategy for the facades should be prepared to guide the conservation of of the masonry.</p> <p>A strategy for the protection of the masonry facades during any major works should be undertaken by heritage architectural specialists and qualified stonemasons with heritage experience prior to the commencement of any works.</p>
7. Item Specific Policy: Fabric	<p>Specific elements for conservation include:</p> <ul style="list-style-type: none"> <li>• Sandstone and brick facades</li> <li>• Sandstone detailing including entablature, cornices, carvings and balustrades</li> <li>• Trachyte plinth</li> <li>• multi-paned steel-framed windows and steel-framed <i>oeil-de-bouef</i> windows</li> <li>• 'Post Office' signage</li> </ul>
8. Item Specific Policy: Development	<p>As part of the Western Forecourt and entrance to Central Station, the context and setting of the Former Parcels Post Office within the Central Station Precinct is of exceptional significance. Any new development in this area should be respectful of this and aim to minimise impacts and enhance the existing context and setting.</p> <p>Proposed new development should refer and respond to the Draft Parcels Post CMP prepared by Urbis 2022 for policies and guidelines in relation to new development in and adjacent to the Former Parcels Post Office.</p> <p>Prior to the commencement of any proposed development or works, photographic archival records of the Former Parcels Post Office should be undertaken.</p> <p>Ensure the original external colour schemes and finishes are documented and archivally recorded.</p>
9. Item Specific Policy: Alterations and additions	<p>Where possible, further external alterations and additions to the building should be avoided. Vertical additions located directly above the building on the roof should be carefully considered and minimised.</p> <p>Do not paint the sandstone or masonry facades. Alterations and additions to the original or early external fabric should be avoided. If unavoidable, these changes should not impact significant fabric. Alterations to the later single storey addition to the east, pool area and mansard roof are permissible, but should be designed to minimise impacts to adjacent significant fabric.</p> <p>Changes to external colours and finishes of window/door joinery, steelwork or roofing material should be sympathetic to the heritage building and consider where possible the implementation of original or early schemes.</p> <p>Internal alterations to the building may be undertaken in a sympathetic manner, and should avoid further removal of original fabric and understanding of the original configuration of the interior.</p> <p>Future refurbishment should investigate avenues to recover significance by revealing more of the original use, original internal spatial configuration and detailing; and by interpreting original signage and lighting.</p>

Policy	Policy Discussion and Guidelines
10. Item Specific Policy: Use	Promote compatible uses for the building which retains a functioning grand main public entrance; provides street level activity
11. Item Specific Policy: Interpretation	<p>See Sections 9 and 14.8 'Heritage Interpretation' in this CMP.</p> <p>Prepare a heritage interpretation strategy, based on the precinct-wide Central Precinct Heritage Interpretation Strategy, specific to the Former Parcels Post Office as part of Central Precinct. Particular focus should be given to the former landscape and cultivation of the space.</p> <p>Following this, develop and implement a detailed site-specific heritage interpretation plan, aligned with the themes and recommendations of the heritage interpretation strategy.</p>
12. Item Specific Policy: Vistas	<p>Ensure the Parcels Post Office continues to be understood as a landmark building in a prominent streetscape location.</p> <p>Ensure significant vistas to the former Parcels Post Office outlined in Section 8.6 of this CMP are not obscured. These include vistas from:</p> <ul style="list-style-type: none"> <li>• George Street</li> <li>• Pitt Street</li> <li>• Lee Street</li> <li>• Quay Street</li> <li>• Ambulance Avenue</li> <li>• Railway Square</li> <li>• Western Forecourt and Railway Colonnade Drive</li> </ul>
13. Item Specific Policy: Short term uses and changes.	Do not allow negative impacts on significant fabric, spatial qualities and setting for short term gain. E.g. commercial signage.





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