



# DORAN DRIVE PRECINCT

## MIXED USE DEVELOPMENT

2 MANDALA PARADE, CASTLE HILL NSW 2154

### DESIGN INTEGRITY REPORT

APRIL 2022  
REVISION 1



TURNER

## **CONTENTS**

- 1 Project Overview**
- 2 Site Wide Guidelines**
- 3 Doran Drive Precinct Character**
- 4 Doran Drive Precinct Guidelines**





*Proposed concept design - View from South West from metro station*

This Architectural Statement and SEPP65 Design verification response is prepared by Turner on behalf of Deicorp Projects Showgrounds Pty Ltd, to support the detailed design development and construction of the Development Application at 2 Mandala Parade, Castle Hill, known as Doran Drive Precinct. This report is to be read in conjunction with the Design Integrity Report, Design Excellence Report and Architectural drawing package.

The proposal consists of four residential towers above a highly articulated podium containing retail units, a supermarket, entry lobbies, loading facilities and community spaces. As a mixed-use development it provides 430 residential dwellings and 10935sqm of non-residential uses. The proposal also delivers the new publicly accessible Doran Drive Plaza, which forms the active heart of the precinct.

The proposal has been developed in collaboration with a comprehensive consultant team to address both strategic and detailed issues associated with the site and overall context.

The project has been the subject to a design integrity process throughout the design development period which requires a review by the Government Architect NSW (State Design Review Panel) to ensure design excellence principles and objectives are realised.

This report is intended to be read in conjunction with the approved Concept State Significant Development Application (SSD-9653), MOD approval for commercial car parking dated 10.12.21, the architectural drawings prepared by Turner, landscape drawings prepared by Urbis and the Environmental Impact Statement with appendices prepared by Gyde.

## 2. SITE WIDE GUIDELINES

### 2.2 Development Objectives

Objective	Team	Comment
<b>1. Place</b> <ul style="list-style-type: none"> <li>- Capture the unique opportunity of adjacency to the Castle Hill Showground event precinct</li> <li>- Celebrate the water movement, aspect and topography of the Site</li> <li>- Connect with and extend the green infrastructure</li> <li>- Respect and evolve the history and local character of the area</li> <li>- Centre the design aspiration around human experience and journey</li> </ul>	Turner	<p><b>Complies</b></p> <p>The site has a unique location, bounded by the station plaza to the south, Doran Plaza to the west, and the Showgrounds to the north. This ribbon of landscaped publicly accessible spaces creates a verdant green context.</p> <p>The Doran Drive Precinct sits on the edge of the Castle Hill showgrounds with expansive views to and from this large open space. The placement of the buildings within the site is cognisant of this relationship as they will have a major impact to the skyline when viewed from the Showground. The proposed built form facing the Showgrounds has a playful composition with a stepped form that integrates significant amounts of landscaping elements into the facades and roof terraces.</p> <p>Landscaping is a key component of the proposal, with generous landscaped open areas within the public domain, podium courtyard and communal rooftop terraces, which is embellished further with planters dressing the edge of lift lobbies and apartment balconies. This holistic approach to greening the proposal is in keeping the desired character of the 'Garden Shire.'</p> <p>The Public Art concept includes a water feature and is a celebration of water movement, located in the centre of the new Doran Drive Plaza. Refer to the Landscape Design Report for detailed illustration and Public Art Strategy.</p> <p>A permeable ground plane allows for easy and intuitive pedestrian movement via Doran Drive Plaza to the Showground, beyond. A generous and wide internal connection through the centre of the retail podium provides for a further link from Mandala Parade to De Clambe Drive.</p> <p>The site has a substantial fall from east to west. This topography is a challenge but is also an opportunity for the retail centre as it allows direct access from the surrounding streets into both first two levels of retail. This extent of connectivity increases the permeability of the centre. The floor levels of the perimeter tenancies step to follow the adjacent streets, creating a continuously activated streetscape. The topography also allows for the supermarket to be predominantly subterranean where it has limited street interface, allowing for more fine grain retail tenancies at street level.</p> <p>The area to date has been characterised by a suburban streetscape of large single-family homes, predominantly of brick construction with a solid and robust aesthetic. The streets are wide and generous, with large setbacks and established trees along the street edge and within the private gardens. This green streetscape is one of the desired characteristics for the future precinct.</p> <p>The proposal champions brick as the predominant material, using a variety of warm brick colours that are reminiscent of the existing domestic architecture. The façade types vary as one moves around the edges the podium in a playful composition.</p> <p>Previously the surrounding area was characterised by poor public transport links, a car dependency, and limited services or retail offering. The area will now evolve into a transport-oriented development leveraging off the new train station, and associated bus and public transport links. It will enjoy a quality mix of retail, commercial, childcare, dining, and other uses. While the proposal is cognisant of the past character of the area, it also embraces its new role as the vibrant heart of the emerging community.</p>



## 2. Diversity

- Consider the changing needs of the community, supporting regional and local growth
- Ensure a suitable mix of uses for future needs
- Provide a mix of housing typologies to suit different household budgets and stages of life
- Provide a range of spaces within the public realm network to suit a variety of activities
- Plan for appropriate diversity in built form, articulation and materials in keeping with the desired quality and character

## Turner

### Diversity of Housing Mix:

The proposed mix shows a diversity of 1 bed [17.7%], 2 bed [72.3%] and 3 bed [10%] apartments. Within these apartment types, there is a range of size and layouts that offer a high level of choice and amenity. Most apartments allow for either a dedicated study, or media space, to facilitate working from home and studying. Storage has been carefully integrated into the apartment plans, with an additional storage cage provided for every apartment in the basement car park.

Refer to the large north facing family friendly 3 bed apartment types to the upper levels of Buildings A & C, with views across the adjacent Showgrounds. Also refer to the two-storey duplex typology along Andalusian Way that adds further to the diversity of choice.

The proposal includes adaptable and liveable apartment types in all buildings. There are also provisions for affordable housing within the development.

### Diversity of Public and Communal Open Spaces:

The proposed Doran Drive Plaza is designed as a highly flexible space that can accommodate multiple uses. A generous 6m pedestrian hardscaped thoroughfare runs along the eastern edge of the plaza, connecting the metro station to the Showground. The retail tenancies along this edge have direct level access which allows for outdoor dining to spill into this area and assist with all day activation. A new public artwork will be located in the middle of the plaza and reference the natural context and local history of the area, including a connection to country with the integration of a fire pit within this space. The proposal includes a lawn area at its northern end which will offer a soft fall space for families to gather in, and children to play in. The lawn is separate to the main pedestrian links through the space, located away from the traffic movements of the adjacent streets, and has good sightlines and passive surveillance from the adjacent retail and outdoor dining areas.

The main podium level communal open space has been designed with a range of uses and facilities, to meet the needs of all age groups and demographics within the development. A children's play area and large lawn area with BBQ facilities are in the centre of the space for gathering and play and a garden patch and lawn area is located at the north of the podium where individuals can enjoy views out over the adjacent Showgrounds. The remainder of the podium allows for large amounts of planting to offer a quality outlook for apartments overhead along with smaller more intimate areas for individuals to use for relaxation. The communal rooftop terraces at the upper levels have been deliberately designed with both active areas and more passive type spaces, for rest and relaxation, to avoid any acoustic implications to adjacent apartments. The communal open areas are secure and for residents only and are separate to any public thoroughfares.

### Diversity of Architectural Styles:

The four residential towers anchor the corners of the site. Building separation and gaps have been maximised within the context of the approved Concept Plan building envelopes. The towers therefore read as independent and slender built forms as one walks around the perimeter of the development. The design allows for variations in materiality, colouration, and detailing of each tower. The podium base has been broken up into a series of smaller built forms with varying façade types, stepped alignment, stepping height and indentation. This avoids a 'big box' retail outcome. The podium has integrated large amounts of glazing to maximise permeability, openness, and wayfinding.

Collectively the buildings sit comfortably together to give a coherent overall character and streetscape that is animated and engaging.

## 2.2 Development Objectives

Objective	Team	Comment
<b>3. Value</b> <ul style="list-style-type: none"> <li>- Leverage and support the new Metro Station infrastructure</li> <li>- Ensure quality design outcomes with public value alongside a commercially viable solution</li> <li>- Promote design and operational efficiency</li> <li>- Attract new investment and create jobs</li> <li>- Promote investment in public art and public realm</li> </ul>	Turner	<p><b>Complies</b></p> <p>The proposed mixed-use precinct is aligned with best practice urban design principles. It delivers a high density vibrant new precinct immediately beside the metro station. The development has been designed as permeable and inviting and can be easily accessed from the metro station. The development will become a major hub for the wider area, leverage from the proximity to public transport and therefore encouraging the use of the public transport infrastructure.</p> <p>Refer also to the separate Design Integrity report produced by Turner which outlines the Design Excellence process to date and how this collaborative and iterative process has informed the final design outcomes. The design team will continue to support the project vision, objectives, and design excellence benchmarks during the future detailed design development.</p> <p>The proposal will act as a catalyst for future development, as it will bring much needed services to the area including a supermarket, speciality retail, medical centre, childcare, and a dining precinct. It will also improve the urban fabric of the area with the delivery of Doran Drive Plaza. This plaza will be flexible public outdoor space including a new artwork as part of its comprehensive design.</p>
<b>4. Activation</b> <ul style="list-style-type: none"> <li>- Provide a platform for various events and active place making initiatives</li> <li>- Provide high quality, active and safe public realm and streets</li> <li>- Provide strong vistas and views to support wayfinding and legibility</li> </ul>	Turner	<p>Community, events, and place making initiatives have been integrated into the design of the scheme.</p> <p>Doran Drive Plaza has been designed to be a flexible publicly accessible space that can accommodate a range of active uses. The plaza has been designed with level connections back into the adjacent retail tenancies and main retail internal circulation. The plaza will have a major new artwork that references the character of the area with connection to context and local history. The public realm has been designed with high quality and low maintenance materials and finishes. The permeable ground floor and the highly activated facades will provide for a safe place with good passive surveillance.</p> <p>Deicorp, through their community sponsorship and charitable program 'Deicorp Community', has established a memorandum of understanding agreement with HCA. Through this agreement, Deicorp will continue to support HCA to operate two large community spaces free for a minimum of 5 years after completion. If during the operational period of the Centre, the agreement with HCA ceases, Deicorp will enter into an agreement with an alternative not-for-profit service provider to manage the space.</p> <p>The proposal allows for strong vistas to support wayfinding and legibility. The primary southwest entry into the retail at ground floor has been relocated to the corner of Mandala Parade and Doran Drive Plaza, directly opposite the metro station. This southwest entry from Mandala Parade is wide, 3 storeys high and fully glazed. This strong architectural statement will assist wayfinding and enhance the permeability of the retail. Internally the main circulation mall has been configured to align with a direct line of sight to De Clambe Drive beyond. The retail floorplate is centred around a large central three storey atrium, where the escalators and vertical circulation are located. This atrium space allows for visual connectivity between levels. Due to the modest size of the retail podium, the internal circulation has natural light from the northern and southern glazed entries, and from Doran Drive Plaza to the west, as well as from a skylight situated at podium level. This natural light from all directions, enhances the quality of the space, providing a retail experience that has a strong connection back to the surrounding public streets and landscape.</p>

## 2.2 Development Objectives

Objective	Team	Comment
<b>5. Sustainability</b> <ul style="list-style-type: none"> <li>- Promote greater use of public and active transport modes</li> <li>- Create social infrastructure to support the new community needs</li> <li>- Ensure maximum and equitable amenity and living comfort with solar access to both public and private space</li> <li>- Set aspirational benchmarks for future developers of the Site</li> </ul>	Turner/ Arup/ Deicorp	<b>Complies</b> <p>The proposed mixed-use precinct is aligned with best practice urban design principles. It delivers a high density vibrant new precinct immediately beside the metro station. The development has been designed as permeable and inviting with key entries located opposite the metro station, Doran Drive Plaza and De Clambe Drive.</p> <p>Cycling is encouraged by the provision of undercover secure lockup areas in the basement car park, that have easy connection back to the lifts to the retail levels and to the four residential towers. Quality end of trip facilities are provided near the secure bicycle lock-up area.</p> <p>Deicorp, through their community sponsorship and charitable program 'Deicorp Community', has established a memorandum of understanding agreement with HCA. Through this agreement, Deicorp will continue to support HCA to operate two large communities spaces free for a minimum of 5 years after completion. If during the operational period of the Centre, the agreement with HCA ceases, Deicorp will enter into an agreement with an alternative not-for-profit service provider to manage the space.</p> <p>The proposal meets the requirements of the ADG for solar access to apartments, and to communal and private open space. 100% of the new Doran Drive Plaza enjoys minimum 2hrs solar access between 9am and 3pm at the winter solstice (excluding of areas covered by awnings). The proposal is compliant with the building envelopes, required building separation and maximum building height of the approved concept plan as prepared by Cox/Oculus, and therefore the proposal does not have any greater impact on the surrounding public open areas.</p> <p>Deicorp is the future developer of the site, and they are committed to delivering a high-quality development that will be aligned with the aspirational benchmarks and overall site vision set out in the Urban Design Guidelines.</p> <p>Refer also to the separate ESD Reports prepared by Arup which accompany this application. These reports detail the sustainability initiatives that will be committed to as part of this proposal.</p>
<b>6. Process</b> <ul style="list-style-type: none"> <li>- Collaborate and engage effectively with our partners, stakeholders and communities</li> <li>- Prepare robust design guidelines to shape desired future outcomes of the precinct and provide more certainty for stakeholders and the community</li> <li>- Engage a highly capable and diverse team and facilitate setting up for success</li> </ul>	Turner	<b>Community Consultation</b> <p>As part of the Doran Drive Precinct State Significant Development Application (SSDA), Deicorp (the applicant) consulted with [key stakeholders/ through the community] during the detailed design development through a community engagement session. The session ran from 6th May – 31st May 2021.</p> <b>Authority Consultation</b> <p><b>Council</b> - Formal consultation has been undertaken with The Hills Shire Council as the site is located within this LGA. This consultation included an initial meeting on 22 March 2021 with the planning team and a second meeting on 28 February 2022.</p> <p><b>Government Architect</b> - Extensive consultation has been undertaken with GANSW in accordance with the requirements of the SEARs.</p> <p><b>Endeavour Energy</b> - JHA Consulting Engineers submitted an application for connection to Endeavour Energy for the site.</p> <p><b>Sydney Water</b> - Opal Water Management have been engaged as the Water Servicing Coordinator for the proposed development.</p> <p>For Further details, please refer to the EIS report and Community Consultation.</p>



## 2.3 Design Principles

Principles	Team	Comment
<p><b>1.</b></p> <p><i>Public domain is prioritised with arrival to the precinct via public open space, green links and the enhanced active transport network.</i></p>	Turner	<p><b>Complies</b></p> <p>The proposed Doran Drive Plaza is designed as a point of gathering, connectivity, and access to the site. The generous 6m pedestrian hardscaped thoroughfare and its flat and seamless design connects the metro station to the showground and works as a clear and wide arrival path to the precinct, also providing clear site lines through the proposed publicly accessible Plaza. This encourages a pedestrian thoroughfare from the Hills Showground Station through to Castle Hill Showground.</p> <p>The location of the pedestrian crossings has been coordinated between landscape, civil, traffic, and architectural drawings, providing the best connectivity throughout the precinct.</p> <p>Public transport stops are located directly at the eastern and southern sides of the plaza, creating a direct connection to the site through the public domain.</p>
<p><b>2.</b></p> <p><i>Existing and new landscapes are connected creating a cohesive network of green public domain that extends through the site and beyond its boundaries.</i></p>	Turner	<p><b>Complies</b></p> <p>The Urban Design Guidelines prepared by Cox Architecture and Oculus for the concept considered the relationship and future connections between the Showground Precinct and Cattai Creek. Doran Drive Precinct is separated from the Cattai Creek Corridor by Precinct West, which will form a separate future application. The Urban Design Guidelines identify the western edge of Precinct West as the future Cattai Creek Masterplan interface, as well as an opportunity for a future pedestrian link connecting the two. The site is required to work within the existing subdivision layout of the Showground Precinct, including roads, pedestrian pathways, cycle paths and bus stops etc that have already been constructed. The Urban Design Guidelines note that the public art in Doran Drive Plaza may integrate an interpretive water feature that reflects the site's connection to Cattai Creek, west of the site. Public art in the form of a water fountain is proposed, consistent with these Guidelines.</p> <p>The wider precinct consists up of several north-south streets and pedestrian connections that link from Carrington Road in the south, to the Showgrounds in the north. This offers pedestrians multiple routes for connecting to the Showgrounds to the north of our site.</p> <p>De Clambe Drive is the key east-west link that connects all of the wider precinct from Cattai Creek to the Showground Road in the east.</p> <p>The future Doran Drive Plaza will link the existing network of public spaces within the precinct, creating a green vein from the Station Plaza through Doran Drive Plaza to the Showgrounds beyond.</p> <p>The subject site for this proposal is a relatively small, and already offers north-south links along the eastern and western boundaries. This is further complimented by the wide and generous internal connection through the centre of the retail podium. This internal connection offers an additional straight route between the station entry and De Clambe Drive. It has been designed with unobstructed view lines through the site, and easy and intuitive wayfinding.</p> <p>Collectively the above elements offer a highly connected and permeable precinct, that has a series of consistent green landscaped ribbons that align with the desired character of the 'garden shire'.</p>

## 2.3 Design Principles

Principles	Team	Comment
<p>3.</p> <p><i>A strong and active heart is created by stitching together the Cattai Creek Corridor, Castle Hill Showground and the station plazas with the transport interchange and mixed-use core enhancing the civic nature of the site adjacent to the Metro.</i></p>	Turner	<p><b>Complies</b></p> <p>Refer to the Architectural plans, Design Integrity report, Architectural Statement and Design Excellence report. SDRP comments have been addressed, with the proposal amended to include SDRP feedback.</p>
<p>4.</p> <p><i>A rational figure ground is planned with public domain and streets framed by a built form edge that's appropriately scaled, articulated and activated creating a legible, safe and enjoyable pedestrian experience.</i></p>	Turner	<p><b>Complies</b></p> <p>Doran Drive Plaza is a high-performance flexible urban space with the capacity to facilitate a number of events including markets, public gatherings and retail spill out. Passive spaces are provided in the shade for visitors to sit. Pedestrian links are provided throughout – with two 6m wide North / South through site thoroughfares provided.</p> <p>Refer to the Design Excellence report and Landscape Report for detailed illustration</p>
<p>5.</p> <p><i>Amenity is maximised with solar gain, quality landscape and significant views via well planned site design and built form composition.</i></p>	Turner	<p><b>Complies</b></p> <p>The design adheres to the ADG. All apartments are ADG compliant. Solar, cross ventilation, communal open space, building separation requirements are met.</p> <p>The awnings have been refined and added where required to provide weather protection to all active frontage and outdoor dining spaces.</p> <p>Refer to the Architectural Statement prepared by Turner.</p>
<p>6.</p> <p><i>The Site's unique values are celebrated by strengthening physical and visual connection to the significant adjacent assets including Cattai Creek and Castle Hill Showground, and reflecting the character and heritage of the site and its surrounds.</i></p>	Turner/ CK Statham Eastwood	<p><b>Complies</b></p> <p>The public art for the proposed development will incorporate a water fountain within the centre of Doran Drive Plaza. Consistent with this Strategy, the Cattai Creek Preliminary Landscape Masterplan report acknowledges that permanent water sources were central to Aboriginal life.</p> <p>Cattai Creek has a deep historical significance and cultural values to the Bedjigal (Darug People). The creek is a location where many Aboriginal artefacts have been recorded. The creek would have been key to every day Bedjigal life providing fresh water, food, natural resources and used as a camping site and ceremonial spiritual place.</p> <p>Early mapping of the area suggests Cattai Creek would have been a permanent source of water for the Bedjigal (Darug People). The proposed water fountain seeks to reinforce the important role permanent water sources played in their lives.</p> <p>The fountain will also function as a bird bath, supported by companion planting which provides shelter and food for local birds. This will facilitate acknowledgement by people of our shared existence with birds. A number of local bird-watching and environmental community groups provided resources and insights into local bird habitats and populations in order to assist with the development of this strategy.</p> <p>Refer to the proposed Public Art Strategy and Landscape Report by Urbis</p>

## 2.4 Vision

Vision	Team	Comment
<p>1.</p> <p><i>"The Hills Showground Station Precinct will be a thriving local mixed-use centre; a walkable, lively place enhanced by strong connections to world class transport and the cultural and recreational destination of Castle Hill Showground. The precinct will provide diverse housing for different generations and lifestyles, framed by green open spaces that encourage connectivity, and will celebrate its views over Cattai Creek and the wider Garden Shire".</i></p>	Turner	<p><b>Complies</b></p> <p>The proposed development at the subject site, within the Doran Drive Precinct, has been designed to meet the vision as established in the Urban Design Guidelines. The proposal will provide a vibrant mixed-use precinct, bringing much needed amenity to the residents within the development and to the wider community. As well as the four proposed residential towers, the development has been designed to accommodate a wide range of other uses, including but not limited to, a supermarket, speciality retail, medical centre, childcare and a dining precinct.</p> <p>The retail levels of the podium have been designed to push all the services, loading dock, and back of house areas, into the middle of the floorplate, allowing for fine grain retail and other active uses around the perimeter of the development. The podium has been designed to be highly permeable and inviting, with extensive amounts of glazing to the facades. Clear lines of the sight can be drawn from Mandala Parade, Doran Drive Plaza, and De Clambe Drive into the central retail atrium and to the streets beyond.</p> <p>The publicly accessible Doran Drive Plaza has been designed to provide a flexible, generously landscaped public space that enjoys quality solar access. The plaza will link the ribbon of green spaces and plaza of the wider precinct. The plaza will allow for easy and direct movement of people from the metro station to the Showgrounds. The plaza links directly into the adjacent retail tenancies which allows for an exciting new dining precinct.</p> <p>The residential towers propose a mix of 1, 2 and 3 bedroom apartments, as well as larger 2 storey residents. Apartments are well designed, meet the requirements of the ADG, and are designed to maximise views and outlook. The proposal includes adaptable and liveable apartment types, located in all buildings. The proposal includes affordable housing as part this the diversity of housing.</p> <p>Collectively the above components allow for a vibrant new mixed-use centre, with a strong sense of place that will be enjoyable to live in and to visit.</p>



## 2.5 Site Character and Place

Doran Drive Precinct Character	Team	Comment
<p>1.</p> <p><i>Significant employment area as a new local centre providing business and services required by the community, with direct connection to the station.</i></p>	Turner	<p><b>Complies</b></p> <p>The proposal allows for over 10,000sqm of non-residential uses. The scheme has been designed to accommodate a wide range of other uses, including but not limited to, a supermarket, speciality retail, medical centre, childcare, and a dining precinct.</p>
<p>2.</p> <p><i>The main plaza acting as the active heart connecting the station to Castle Hill Showground, lined with fine grain retail and dining experiences.</i></p>	Turner/ Urbis	<p><b>Complies</b></p> <p>The retail levels of the podium have been designed to push all of the services, loading dock, and back of house areas, into the middle of the floorplate, allowing for fine grain retail and other active uses around the perimeter of the development. The podium has been designed to be highly permeable and inviting, with extensive amounts of glazing to the facades. Clear lines of the sight can be drawn from Mandala Parade, Doran Drive Plaza, and De Clambe Drive into the central retail atrium and to the streets beyond. Wayfinding has been designed to be easy and intuitive.</p> <p>The publicly accessible Doran Drive Plaza has been designed to provide a flexible, generously landscaped public space that enjoys quality solar access. The plaza will link the ribbon of other green spaces and plazas of the wider precinct. The plaza will allow for easy and direct movement of people from the metro station to the Showgrounds. The plaza links directly into the adjacent retail tenancies which allows for an exciting new dining precinct.</p>
<p>3.</p> <p><i>Its buildings with dense, urban character and active urban edges, with residential towers above promoting weekend and evening activity.</i></p>	Turner	<p><b>Complies</b></p> <p>The proposal will provide a vibrant mixed-use precinct, bringing much needed amenity to the area. This design is cognisant of its role as the new heart of the wider community. It is designed with an urban character that includes a diversity of architectural styles, and a varied palette of quality and low maintenance materials. While its character is urban and high activated, the design also embraces the past character of the area with the extensive use of brick, and integration of generous landscaping and large tree canopy coverage.</p> <p>The retail levels of the podium have been designed to push all of the services, loading dock, and back of house areas, into the middle of the floorplate, allowing for fine grain retail and other active uses around the perimeter of the development. The podium has been designed to be highly permeable and inviting, with extensive amounts of glazing to the facades.</p> <p>The publicly accessible Doran Drive Plaza has been designed to provide a flexible, generously landscaped public space that enjoys quality solar access. The plaza will link the ribbon of other green spaces and plazas of the wider precinct. The plaza will allow for easy and direct movement of people from the train station to the Showgrounds. The plaza links directly into the adjacent retail tenancies, allowing for an exciting new dining precinct.</p> <p>The residential towers allow for passive surveillance of the surroundings streets and Doran Drive Plaza, and therefore enhance the safety and useability of these spaces during the day and into the evening.</p>

## 2.6 Site Structure

Controls	Team	Comment
<p><b>1.</b></p> <p><i>To ensure that development occurs in a coordinated manner consistent with the Precinct vision and the development principles of housing diversity, employment opportunities, transit oriented development, quality infrastructure and open space and place making.</i></p>	Turner	<p><b>Complies</b></p> <p>Refer to the staging diagrams that form part of the architectural documentation. Stage 1 will deliver all of the basement parking, loading, services, retail and other non-residential uses within the podium, as well as the public domain works.</p> <p>Stages 2 and 3 then deliver the overhead residential towers independently from the podium base.</p> <p>Therefore, the staging will provide all of the amenity up front for the wider precinct and surrounding community. When residents do move into the residential towers overhead, they will already have the immediate benefit of a wide range of retail, commercial and other uses within the podium.</p>
<p><b>2.</b></p> <p><i>To provide a mix of housing, retail, employment, and services in appropriate and logical locations within the Precinct.</i></p>	Turner	<p><b>Complies</b></p> <p>The proposal for the Doran Drive Precinct is a high-density mixed-use precinct. Part of the future success of the development will be driven by the legibility of the uses and intuitive wayfinding. The various uses have been arranged in a logical manner within the site so that they are easy to find and use. The retail is located at the lower 3 levels, and has direct access at ground floor to Mandala Parade and Doran Drive Plaza, and at the upper ground floor to De Clambe Drive. The entries to the retail are wide and generous, with good sightlines into to the central retail atrium and to the streets beyond. The escalators and vertical circulation are located in the central 3 storey atrium which allows for visual connectivity between floors. The four residential towers each have a prominent main lobby entry at street level that have good sightlines to the public domain.</p>
<p><b>3.</b></p> <p><i>To locate higher scale residential apartments and commercial uses closest to the station, the Castle Hill Showground and Cattai Creek corridor to optimise access to station facilities as well as outlook and natural amenity.</i></p>	Turner	<p><b>Complies</b></p> <p>The proposal is compliant with the approved GFA, building envelopes, building heights, and required building separation. It therefore is aligned with the intent of the approval and will provide a vibrant, high density, mixed-use precinct immediately adjacent to the metro station.</p>
<p><b>4.</b></p> <p><i>To develop a local centre and main plaza in the area immediately surrounding the station to provide local shopping, employment opportunities and other services to support the incoming population and establish a vibrant and well-used public domain.</i></p>	Turner/ Urbis	<p><b>Complies</b></p> <p>This proposal is part of the transformation of the area which is evolving into a transport orientated community. The mixed-use development within the Doran Drive Precinct will include a supermarket, speciality retail, commercial, childcare, medical centre, a dining precinct and a large community facility, all immediately beside the new metro station. The four residential towers overhead will add a large resident population that will further activate the site at all times. The public domain and Doran Drive Plaza have been envisaged as a pedestrian centric environment, where quality landscaping will make this an enjoyable place to live and to visit. Collectively, these many elements will allow for a vibrant precinct that will activate this area around the metro station.</p>

## 2.7 External Interfaces

Objectives	Team	Comment
<p>a)</p> <p><i>To ensure the delivery of a consistent, cohesive and complementary broader Showground Precinct in terms of;</i></p> <ul style="list-style-type: none"> <li>- uses and activities</li> <li>- built form</li> <li>- public and private domain</li> <li>- movement and access</li> <li>- finishes and materials</li> </ul>	Turner	<p><b>Complies</b></p> <p>The proposal delivers a considered and cohesive mixed-use precinct that meets the vision of the Urban Design Guidelines.</p> <p>The proposal is compliant with the total GFA and with the desired split between residential and non-residential uses. The proposal is compliant with the approved building envelopes, required building separation and maximum building height.</p> <p>The proposal delivers the 1400sqm publicly accessible Doran Drive Plaza and other public domain works around the perimeter of the site. The proposal also meets the ADG requirements for communal open space and private open space.</p> <p>Part of the future success of the development will be driven by the legibility of the uses and intuitive wayfinding. The various uses have been arranged in a logical manner within the site, so that they are easy to find and use. The retail is located at the lower 3 levels, and has direct access at ground floor to Mandala Parade and Doran Drive Plaza, and at upper ground floor to De Clambe Drive. The four residential towers each have a prominent main lobby entry at street level that has good sightlines to the public domain.</p> <p>The materials and finishes are chosen from a palette of robust, high quality, and low maintenance materials. The design champions the use of brick to the podium base and to two of the four residential towers. The use of brick allows for a range of colours, textures and detailing that offer a quality streetscape and a highly articulated character for these buildings.</p>



## 2.7 External Interfaces

Controls	Team	Comment
<p><b>1.</b></p> <p><i>Any development application within the Precinct East, Doran Drive Precinct and Precinct West must demonstrate that its proposals have considered the relationship between its subject site and the sites external to the Hills Showground Station Precinct where adjacent. This includes:</i></p> <ul style="list-style-type: none"> <li><i>- Plans for Castle Hill Showground as at the time of the DA. It is noted that at the time of publication of these Urban Design Guidelines the Castle Hill Showground Masterplan was at Draft status but may be further progressed when this control is enforced.</i></li> <li><i>- Plans for Cattai Creek Corridor as at the time of the DA. It is noted that at the time of publication of this UDG, the Cattai Creek Corridor Revitalisation Project was in its infancy. A Draft Master Plan had not yet been published.</i></li> <li><i>- Development Applications within the rezoned residential areas along the south of Carrington Road and north-east of Showground Road</i></li> </ul>	<p><b>Turner / City Plan</b></p>	<p><b>Complies</b></p> <p>The proposal has been cognisant of its context and the emerging character of the area which is experiencing rapid change since the delivery of the new train station. The proposal reacts to this emerging context by embracing its status as the vibrant heart of the wider community. The higher density future developments being delivered as part of the surrounding Development Applications, including the rezoned residential areas along the south of Carrington Road and north-east of Showground Road, will add to the vibrancy of the area, and further enhance the use of public transport and the new train station.</p> <p>At its meeting on 25 August 2020, Council adopted the Master Plan for the Castle Hill Showground. The site is located immediately to the south of the Castle Hill Showground promenade area, across De Clambe Drive. The Promenade will form an activate transition between the Metro station, the adjoining Neighbourhood Centre, and the Showgrounds. In response, the proposed design is highly permeable with multiple pedestrian connections through the site connecting the metro station through to the Showground. The internal pedestrian connections include the plaza, internal arcade, and stairs down from the proposed communal open space. Overall, the proposal supports the masterplan by creating connections and an active frontage to the showground precinct.</p> <p>It is our understanding that the Cattai Creek Concept Master Plan is anticipated to be adopted at the Council Meeting on 27 July 2021. The site does not have a direct interface with the Cattai Creek Corridor, however detailed consideration has been given to the corridor in terms of biodiversity impacts and stormwater run-off. Specifically, Erosion and sediment control measures are to be implemented during the construction of the proposed development to minimise any potential impacts on the nearby waterway of Cattai Creek. The proposed development has also been designed to be integrated with the existing walking and cycling infrastructure and future links to Cattai Creek via future Showground Precinct West.</p> <p>Future development in these rezoned residential areas have been considered in the EIS including in relation to traffic and potential amenity impacts.</p>

## 2.

*These relationship considerations must include the following elements:*

- a. the quantum, location and operations of non-residential uses located along pedestrian desire lines, street interfaces, public open space interfaces and within podiums*
- b. the location, programme and function of public open spaces, the collective public and private domain character, finishes and materials, street furniture and fixtures, public art and interpretation*
- c. the relationships between the built form, site coverage, podiums, primary and secondary setbacks, building height, transition, separation, articulation, facades, finishes and materials*
- d. precinct-wide requirements for vehicular circulation, access, car parking, service vehicles and waste collection, pedestrian and cyclist connectivity*
- e. precinct-wide landform, cut and fill, topography, integrated water management, wind impacts and micro-climates*

*This control is to be demonstrated via the inclusion of key material within the development application that specifically describes these relationship matters, including:*

- a coordination diagram/plan and key section(s) where appropriate*
- associated commentary to effectively describe how the above matters are considered and the objectives achieved*
- summary of the consultation that has occurred with those in control of the design of the adjacent areas and how this has underpinned the resulting design approach*

Turner/  
Urbis/  
AECOM/  
Windtech

## Complies

a. The proposed non-residential uses have been located along pedestrian desire lines, street interfaces, public open space interfaces and within podium. Part of the future success of the proposed development will be driven by the legibility of the uses and intuitive wayfinding. The retail is located at the lower 3 levels, with direct access at Ground Floor to Mandala Parade and Doran Drive Plaza, and at Upper Ground Floor to De Clambe Drive. The entries to the retail are wide and generous, with good sightlines into the central retail atrium and streets beyond. The escalators and vertical circulation are located in the central 3 storey atrium void, allowing for a visual connectivity between floors. Fine grain retail wraps the street edge interface, each stepped to follow the steep topography allowing for direct entries and consistent activation. The four residential towers have a prominent lobby entry at street level with good sightlines to the public domain.

b. The location, programme and function of public open spaces, the collective public and private domain character, finishes and materials, street furniture and fixtures, and public art have been carefully considered, and collectively will achieve a quality design outcome that offers a high level of amenity for those who live in, work at, and visit the development. The above items will create a strong and memorable sense of place that is connected to the natural context and the local history.

c. The proposal is compliant with the approved building envelopes, maximum building heights, and the required building separation. The podium is compliant with the zero setback for active uses, and with the secondary setback to the towers overhead. The podium is broken up into a series of façade components of varying materiality and character that offer a human scale at street level. The four residential towers each have their own architectural identity, with works together cohesively as a grouping of buildings.

d. The surrounding streets are already delivered and in use. Within the site boundary, the proposal will complete the new Doran Drive Plaza and any further public domain works. Car parking is all located in the basement, with a carefully integrated car park entry located on De Clambe Drive. The loading dock, waste management, services and retail back of house have been pushed into the centre of the floorplate further minimising impact on the façade activation, accessed from Andalusian Way where there are no conflicts with active uses. The public domain has been envisaged as a pedestrian centric environment. Good sightlines and comprehensive passive surveillance allow for a safe place to live, work and visit at any time during the day. Cyclist enjoy secure lock up areas within the weather protection of the basement car park, with quality end of trip facilities adjacent.

e. The precinct-wide landform, cut and fill, and topography are already established by the completed street network within the precinct. The steep topography is a challenge but is also an opportunity as it allows for direct access from the surrounding street into both the ground floor and upper ground floor retail levels. This permeability and connection greatly boost the viability of the upper retail levels. Refer to the civil report regarding the integrated water management. Refer to the wind report for the wind impacts and micro-climates within the site. The proposal has managed the impacts of wind with the inclusion of either continuous awnings, indented entries or covered colonnades to all active uses at street level. This is complemented by extensive landscaping and planting, including 50% tree canopy coverage to Doran Drive Plaza, and the existing street trees that lines the perimeter of the development. Apartment balconies are generally indented and therefore protected by side sweeping winds.

## 2.8 Design Excellence

Objectives	Team	Comment
<p><b>a)</b>  <i>To support the Design Excellence Strategy in outlining how design excellence will be achieved through the roles and responsibilities of each stakeholder, at each stage of the project lifecycle in which design can be controlled to maintain design integrity and achieve design excellence.</i></p>	Turner	<p><b>Noted</b>  Refer also to the separate Design Integrity Report produced by Turner which outlines the Design Excellence process to date and how this collaborative and iterative process has informed the final design outcomes.</p>
<p><b>b)</b>  <i>To support the project vision, objectives and design excellence benchmarks during detailed design development.</i></p>	Turner	<p><b>Noted</b>  The design team will continue to support the project vision objectives and design excellence benchmarks during the future detailed design development.</p>



## 2.8 Design Excellence

Control	Team	Comment
<p><b>1.</b></p> <p><i>Urban design and landscape architects are to be selected from the NSW Government Architect's 'Prequalification Scheme for Strategy and Design Excellence' or to collaborate with a pre-qualified architect.</i></p>	Turner/ Deicorp	<p><b>Complies</b></p> <p>Deicorp have chosen a highly qualified and talented Design Team. Consultants have been chosen based on their Prequalification Scheme for Strategy and Design Excellence and/or due to collaborate with the pre-qualified architect. Collectively this team has the experience and ability to deliver best practice design outcomes for this site.</p>
<p><b>2.</b></p> <p><i>All built form development across more than one lot is required to demonstrate architectural diversity in development outcomes. The use of multiple architects, which includes both established and emerging architectural firms, is encouraged.</i></p>		<p><b>NA</b></p> <p>This application only relates to one lot, ie the Doran Drive Precinct. The proposal does not include the other eastern and western precincts.</p>
<p><b>3.</b></p> <p><i>All residential and non-residential development is to adhere to the Urban Design Guidelines.</i></p>	Turner	<p><b>Complies</b></p> <p>The proposal embraces the desired vision and character for this precinct, complying with the Urban Design Guidelines.</p>
<p><b>4.</b></p> <p><i>Development is to address the principles of Crime Prevention Through Environmental Design (CPTED). Note: Consideration shall also be given to The Hills Shire Council's Policy Designing Safer Communities, Safer by Design Guidelines (June 2002).</i></p>	Barker Ryan Stewart	<p><b>Complies</b></p> <p>The proposed development is supported by a Crime Prevention through Environmental Design Assessment authored by Barker Ryan Stewart (v7, 14/07/2021). The report has been prepared in accordance with the Crime Prevention Through Environmental Design (CPTED) guidelines prepared by the NSW Police in conjunction with the Department of Planning. The report incorporates a comprehensive assessment of local crime statistics, the site and proposed built form. Further, the assessment includes CPTED recommendations in accordance with the four crime prevention principles of Surveillance, Access Control, Territorial Reinforcement and Space/ Activity Management.</p> <p>Section 1.4 of the CPTED Report details compliance and/or consistency with The Hills Shire Council's Policy Designing Safer Communities, Safer by Design Guidelines (June 2002). The assessment confirmed the proposed development is consistent with the guidelines and no requirement was identified for re-design of any built form or landscape elements of the proposal.</p>
<p><b>5.</b></p> <p><i>All future detailed SSDAs are required to submit a CPTED Assessment</i></p>	Barker Ryan Stewart	<p><b>Complies</b></p> <p>CPTED Submitted with the SSDA</p>
<p><b>6.</b></p> <p><i>All development is to comply with the sustainability mandatory targets and use best endeavours to achieve the stretch goals as outlined in the ESD report and ESD Requirements Tool.</i></p>	Deicorp	<p><b>Complies</b></p> <p>The Sustainability Measures have been integrated into the development.</p>
<p><b>7.</b></p> <p><i>Residential flat buildings are to meet the requirements for adaptable housing within Part B Section 5 Residential Flat Buildings of The Hills DCP 2012.</i></p>	Turner/ Deicorp	<p><b>Complies</b></p> <p><b>10% Adaptable housing has been proposed.</b></p>
<p><b>8.</b></p> <p><i>All types of residential accommodation are to consider flexibility in the design to allow adaptation to meet the changing needs of residents due to ageing or disability.</i></p>	Turner/ Morris Goding	<p><b>Noted</b></p> <p>The proposal meets the requirements for liveable and adaptable housing.</p>

## 2.9 Diversity and Inclusion

### Affordable Housing

Objectives	Team	Comment
<p>a)</p> <p><i>To ensure that the development promotes social and economic integration while providing households on lower incomes access to housing opportunities.</i></p>	Turner	<p><b>Complies</b></p> <p>The proposal allows for the required percentage of affordable housing within the development.</p>
<p>b)</p> <p><i>To ensure consistency in the material treatment of affordable and market dwellings.</i></p>	Turner	<p><b>Noted</b></p> <p>This guideline will be adhered to during the design development phase prior to the construction certificate</p>
Controls		Comment
<p>1.</p> <p><i>A minimum of 5% of the number of dwellings delivered must be Affordable Housing. The location(s) and configuration(s) of affordable housing within the Hills Showground Station Precinct is flexible as long as the 5% minimum is met, and may occur on any or all of the associated development lots.</i></p>	Turner	<p><b>Complies</b></p> <p>The proposal will deliver 5% of the number of dwellings as Affordable Housing for 10 years.</p>
<p>2.</p> <p><i>Affordable Housing must be integrated into the overall development with no discernible difference in quality when compared to market housing.</i></p>	Turner	<p><b>Noted</b></p> <p>This guideline has been addressed and affordable housing are distributed across the buildings and integrated with no discernible difference.</p>
<p>3.</p> <p><i>The provision of Affordable Housing must conform with the State Environmental Planning Policy (Affordable Rental Housing), 2009 (ARHSEPP).</i></p>	Turner	<p><b>Noted</b></p> <p>The provision of Affordable Housing will conform with the referenced State Environmental Planning Policy (Affordable Rental Housing), 2009 (ARHSEPP).</p>
<p>4.</p> <p><i>Car parking for affordable housing is to be in accordance with Sections 3.2.13, 4.2.12, and 5.2.16 Car Parking and Access of this document consistent with State Environmental Planning Policy (Affordable Rental Housing) 2009.</i></p>	Turner	<p><b>Noted</b></p> <p>Refer to the separate traffic report which accompanies this submission and outlines the parking rates. Car parking for affordable housing will be in accordance with the referenced section 4.2.12 Car Parking and Access</p>

## 2.9 Diversity and Inclusion

### Liveable and Adaptable Housing

Objectives	Team	Comment
<p>a)</p> <p><i>To encourage flexibility in design to allow people to adapt their home as their needs change.</i></p>	Turner/ Morris Goding	<p><b>Complies</b></p> <p>The proposal meets the requirements for liveable and adaptable housing. Refer to the Turner drawings:</p> <p>DA-810-001_ Pre and Post Adaptable Layouts 1</p> <p>DA-810-002_ Pre and Post Adaptable Layouts 2</p> <p>DA-810-101_ Livable Apartment Layouts</p> <p>Refer also the separate access report which accompanies this submission, which outlines compliance with liveable and adaptable housing, and with the DDA.</p>
<p>b)</p> <p><i>To ensure the provision of homes that are easier to access, navigate and live in, and more cost effective to adapt when life's circumstances change.</i></p>	Turner/ Morris Goding	<p><b>Complies</b></p> <p>The proposal meets the requirements for liveable and adaptable housing.</p>
<p>c)</p> <p><i>To ensure a sufficient proportion of dwellings include accessible layouts and features to accommodate changing requirements of residents due to ageing or disability.</i></p>	Turner/ Morris Goding	<p><b>Complies</b></p> <p>The proposal meets the requirements for liveable and adaptable housing.</p>
Controls		Comment
<p>1.</p> <p><i>A minimum of 20% of apartments are to achieve a 'Design and As-Built' Livable Housing Australia accreditation at silver level or above.</i></p>	Turner/ Morris Goding	<p><b>Complies</b></p> <p>The proposal meets the requirements for liveable and adaptable housing. Refer to the Turner drawings:</p> <p>DA-810-001_ Pre and Post Adaptable Layouts 1</p> <p>DA-810-002_ Pre and Post Adaptable Layouts 2</p> <p>DA-810-101_ Livable Apartment Layouts</p> <p>Refer also the separate access report which accompanies this submission, which outlines compliance with liveable and adaptable housing, and with the DDA.</p>
<p>2.</p> <p><i>Residential flat buildings and multi-dwelling housing are to meet the requirements for adaptable housing within part B Section 5 Residential Flat Buildings of The Hills DCP 2012.</i></p>	Turner	<p><b>Noted</b></p>
<p>3.</p> <p><i>Residential flat buildings and multi dwelling housing are to comply with the standards under the Disability Discrimination Act and Building Code of Australia.</i></p>	Turner/ Morris Goding	<p><b>Complies</b></p> <p>Refer to the separate access report which accompanies this submission which outlines compliance with liveable and adaptable housing, and with the DDA.</p>

## 2.9 Diversity and Inclusion

### Dwelling Typologies & Mix

Objectives	Team	Comment
<p>a)</p> <p><i>To provide housing choice to suit different demographics, living needs and household budgets.</i></p>	Turner	<p><b>Complies</b></p> <p>The proposed mix shows a diversity of 1 bed [17.7%], 2 bed [72.3%] and 3 bed [10%] apartments. Within these apartment types, there is a range of size and layouts that offer a high level of choice and amenity. Most apartments allow for a either a dedicated study or media space, to facilitate working from home and studying. Storage has been carefully integrated into the apartment plans, with an additional storage cage provided for every apartment in the basement car park.</p> <p>Refer to the large north facing family friendly 3 bed apartment types to the upper levels of Buildings A &amp; C, with views across the adjacent Showgrounds. Also refer to the two-storey duplex typology along Andalusian Way that add further to the diversity of choice.</p> <p>The proposal includes adaptable and liveable apartment types in all buildings. There are also provisions for affordable housing within the development.</p>
<p>b)</p> <p><i>To guide appropriate locations for differential typologies given their proximity to infrastructure and mixed-use areas, as well as adjacent residential areas and open space.</i></p>	Turner	<p><b>Complies</b></p> <p>The various uses have been carefully located within this development to remove any conflicts. The apartments within the Doran Drive Precinct are elevated, located from podium level and above, where they can ensure visual and acoustic privacy, and outlook over the surrounding public opens spaces to the south, west and north, as well as the large generously landscaped central courtyard.</p>
Controls		Comment
<p>1.</p> <p><i>Apartment mix is to be provided in accordance with Table 1: Excerpt Table 1 regarding Doran Drive precinct, (Urban Design Guidelines, Pg35)</i></p> <p>a) <i>No more than 25% of the total number of dwellings (to the nearest whole number of dwellings) contained in the development are to be studio or 1-bedroom dwellings, or both.</i></p> <p>b) <i>At least 10% of the total number of dwellings (to the nearest whole number of dwellings) contained in the development are to be 3 or more-bedroom dwellings.</i></p>	Turner	<p><b>Complies</b></p> <p>The proposed mix shows a diversity of 1 bed [17.7%], 2 bed [72.3%] and 3 bed [10%] apartments.</p>
<p>2.</p> <p><i>Any variation to the apartment mix controls must be supported by a market demand assessment prepared by a suitably qualified professional.</i></p>	Turner	<p><b>Noted</b></p> <p>The proposed mix shows a diversity of 1 bed [17.7%], 2 bed [72.3%] and 3 bed [10%] apartments.</p>
<p>3.</p> <p><i>Townhouse apartments are to be provided at a minimum along the new internal street to Precinct East where 12m high (3 storey) envelopes are stipulated.</i></p>		<p><b>N/A</b></p> <p>The proposal only related to the Doran Drive Precinct and does not relate to the separate Precinct East.</p>

## 2.10 Connectivity

### 2.10.1 Road Hierarchy

Objectives	Team	Comment
<p>a)</p> <p><i>To respond to the role and function of existing roads within the broader Showground Station Precinct.</i></p>	Turner	<p><b>Complies</b></p> <p>The surrounding roads are already fully delivered and in use.</p>
<p>b)</p> <p><i>To provide appropriate local access and services to the residential buildings and park.</i></p>	Turner	<p><b>Complies</b></p> <p>The surrounding roads are already fully delivered and in use.</p>
Control		Comment
<p>1.</p> <p><i>A new local street with a 17m road reserve is to be provided within Precinct East as shown in Figure 30.</i></p>		<p><b>N/A</b></p> <p>The proposal only related to the Doran Drive Precinct and does not relate to the separate Precinct East.</p>
<p>2.</p> <p><i>The new local street is to be consistent with the street profile in Section 5.2.13.</i></p>		<p><b>N/A</b></p> <p>The proposal only related to the Doran Drive Precinct and does not relate to the separate Precinct East.</p>
<p>3.</p> <p><i>The new local street is to be two-way to provide vehicular access to the residential development lots, however the street is to be one-way from the development lot to Andalusian Way to reduce the amount of traffic using Andalusian Way to access Precinct East.</i></p>		<p><b>N/A</b></p> <p>The proposal only related to the Doran Drive Precinct and does not relate to the separate Precinct East.</p>
<p>4.</p> <p><i>An appropriate transition and connectivity is to be provided between the new public road and the existing roads to ensure a uniform and logical profile. This may be achieved through finishes, treatment and street trees.</i></p>		<p><b>N/A</b></p> <p>The proposal only related to the Doran Drive Precinct and does not relate to the separate Precinct East.</p>
<p>5.</p> <p><i>The design and construction of road infrastructure shall comply with The Hills Shire Council's Design Guidelines for Subdivision and Developments'.</i></p>	Turner	<p><b>Noted.</b></p> <p>The surrounding roads are already fully delivered and in use.</p>

## 2.10 Connectivity

### 2.10.2 Active Transport

Objectives	Team	Comment
<p>a)</p> <p><i>To reduce reliance on private motor vehicles for trips undertaken to/from, through and within the Precinct.</i></p>	Turner/ Urbis/	<p><b>Complies</b></p> <p>The proposal meets the objectives of the approved concept plan for the Doran Drive precinct.</p> <p>This area has traditionally been a car dependent community with poor public transport links and limited retail and services offering. This new proposal is part of the transformation of the area which is evolving into a transport orientated community, centred around the new metro station. The new North West metro line and improved bus connections now link this area to the wider Sydney region. The mixed-use development within the Doran Drive Precinct will bring a supermarket, speciality retail, commercial, childcare, medical centre, a dining precinct, and a large community facility, all immediately beside the new metro station. The public domain and Doran Drive Plaza have been envisaged as a pedestrian centric environment, where quality landscaping will make this an enjoyable place to live and to visit. Bicycle parking is located within the public domain, and in secure lock-up areas in the basement. End of trip facilities offer best practice facilities for cyclists, and users of other modes of transport.</p>
<p>b)</p> <p><i>To maximise public transport patronage and encourage walking, cycling and other forms of active transport.</i></p>	Turner	<p><b>Noted</b></p> <p>Due to its proximity to the station, bus stop, and the proposed high density mixed-use development within the subject site, the proposal will maximise public transport patronage and encourage walking, cycling and other forms of active transport.</p>
<p>c)</p> <p><i>To provide weather protected, active connections between the station, transport interchange, Doran Drive Plaza, Castle Hill Showground and the regional recreational resource of Cattai Creek via the existing footpath.</i></p>	Turner	<p><b>Complies</b></p> <p>The proposal allows for awnings as well as an internalised retail plaza for pedestrians in wet weather.</p>
<p>d)</p> <p><i>To complete the pedestrian and bicycle network, which includes a mixture of dedicated off-road routes, pedestrian and bicycle priority shareways and links to the existing active transport networks of the broader Precinct and the Garden Shire.</i></p>	Turner	<p><b>Noted</b></p>
<p>e)</p> <p><i>To consider the needs of the residents with particular consideration to access requirements, safety and security.</i></p>	Turner	<p><b>Complies</b></p> <p>Clearly defined pedestrian pathways have been provided around the perimeter of the development and within the proposed publicly accessible Doran Drive Plaza. The public domain on all sides of the subject site is pedestrian centric, and has carefully managed any integration of car park, loading, and other services requirements.</p>
<p>f)</p> <p><i>To ensure that appropriate pathways, with high levels of pedestrian amenity are provided for residents in the locality along identified desire lines in accordance with Council's ESD objective 9 (THDCP (2012) Part A Section 5.1).</i></p>	Turner	<p><b>Noted</b></p>
<p>g)</p> <p><i>To ensure provision is made for bicycle access and storage in accordance with Council's ESD objective 9.</i></p>	Turner/ Arup/ Varga Traffic Planning	<p><b>Noted</b></p> <p>Bicycle parking is located within the public domain, and in secure lock-up areas in the basement.</p>



## 2.10 Connectivity

### 2.10.2 Active Transport

Control	Team	Comment
<p><b>1.</b></p> <p><i>Active transport movement and access throughout the Precinct is to be provided in accordance with Figure 31.</i></p>	Turner	<p><b>Complies</b></p> <p>The proposal meets the requirements of figure 31 with clearly defined pedestrian routes around the perimeter of the site. A quality north-south link is provided through the new Doran Drive Plaza, connecting the metro station in the southwest of the site to the Showgrounds at the north of the site. The plaza will be accessible for all and fully compliant with DDA requirements.</p>
<p><b>2.S</b></p> <p><i>A potential shared zoned to supplement the one-way treatment on the local road is to be provided between the Precinct East Park and Andalusian Way, subject to TfNSW and Council approval, to prioritise pedestrian movements between Precinct East and the Station.</i></p>	Turner	<p><b>Noted.</b></p>
<p><b>3.</b></p> <p><i>Shared paths are to have a minimum width of 2.5m.</i></p>	Turner	<p><b>Noted</b></p>
<p><b>4.</b></p> <p><i>Access to dwellings should be direct and without unnecessary barriers. All external and internal pathways and ramps should conform to the requirements set out in Australian Standard 1428 Parts 1 and 2.</i></p>	Turner/ Morris Goding	<p><b>Complies</b></p> <p>All external and internal pathways and ramps conform to the requirements set out in Australian Standard 1428 Parts 1 and 2.</p>
<p><b>5.</b></p> <p><i>Clearly defined pedestrian pathways are to be provided between proposed developments and proposed footpaths along sub-arterial roads.</i></p>	Turner	<p><b>Complies</b></p> <p>Clearly defined pedestrian pathways have been provided around the perimeter of the development and within the proposed publicly accessible Doran Drive Plaza.</p>
<p><b>6.</b></p> <p><i>Developments are to have adequate lighting in common and access areas to ensure the safety of residents and property.</i></p>	Turner	<p><b>Noted</b></p> <p>The development will have adequate lighting in common and access areas to ensure the safety of residents and property. This extent of detail will be confirmed at the later design development and construction certificate phases.</p>
<p><b>7.</b></p> <p><i>Building and unit numbering and all signage is to be clear and easy to understand.</i></p>	Turner	<p><b>Noted</b></p> <p>Building and unit numbering and all signage will be clear and easy to understand. This extent of detail will be confirmed at the later design development and construction certificate phases.</p>
<p><b>8.</b></p> <p><i>Pathway locations must ensure natural surveillance of the pathway from primary living areas of adjoining units. Dwelling entries must not be hidden from view and must be easily accessible.</i></p>	Turner	<p><b>Complies</b></p> <p>Pathway locations ensure natural surveillance of the pathway from primary living areas of adjoining units. Dwelling entries will not be hidden from view and will be easily accessible.</p>
<p><b>9.</b></p> <p><i>Bicycle lockup facilities are to be provided close to the main entry to the building.</i></p>	Turner	<p><b>Complies</b></p> <p>Bicycle lockup facilities are provided within the main basement carpark and have easy access to the lifts that connect to the retail centre and to the four residential towers. End of trip facilities are located adjacent to the bicycle lock up, encouraging the use of cycling and other modes of transport other than the car.</p>

## 2.11 Public Domain and Landscape

### 2.11.1 Public Open Space

Objectives	Team	Comment
<p>a)</p> <p><i>To deliver a well-connected, accessible, high quality, diverse, multifunctional and flexible public open space.</i></p>	Turner/ Urbis	<p><b>Complies</b></p> <p>The proposed new Doran Drive Plaza will be universally accessible and compliant with DDA requirements. The plaza is 1400sqm and has been designed as a flexible space to meet the needs of all age groups and demographics. Refer to the landscape design by Urbis for more detail.</p>
<p>b)</p> <p><i>To reinforce primary connections between the Precinct, the Hills Showground Station, Castle Hill Showground and Cattai Creek.</i></p>	Turner	<p><b>Complies</b></p> <p>The design for the Doran Drive Plaza has allowed for clear sight lines, good lighting, and passive surveillance from adjoining tenancies and from apartments overhead. This creates a safe environment for all day use. The design for the plaza allows for a generous 6m wide hard landscaped throughfare along the eastern edge that provides a direct connection from the metro station to the Showgrounds. The public domain on all sides of the subject site is pedestrian centric, and has carefully managed any integration of car park, loading, and other services requirements. The proposal reinforces the connections between the Station, Showgrounds, and Cattai Creek.</p>
<p>c)</p> <p><i>To provide a central open space that can perform as the active community 'heart' over the Precinct.</i></p>	Turner/ Morris Goding	<p><b>Complies</b></p> <p>The proposed new Doran Drive Plaza will be universally accessible and compliant with DDA requirements.</p>
<p>d)</p> <p><i>To provide sufficient open space to suit the needs of the residents, employment community and visitor, and that complements the existing open space network.</i></p>	Turner/ Urbis	<p><b>Complies</b></p> <p>The proposal includes significant amounts of open space to meet the needs of the residents, those working within the development and those visiting. The publicly accessible Doran Drive Plaza measures 1400sqm and will provide amenity for the wider community. The main resident's podium level courtyard has been designed as flexible space that meets the needs of all age groups and demographics within the development. Children's play area is located at the southern end of the podium, a large lawn area with BBQ facilities is in the centre of the space for gathering and play and a garden patch and lawn area is located at the north of the podium where individuals can enjoy views out over the adjacent Showgrounds. Remaining areas include large amounts of planting to offer a quality outlook for apartments overhead, or smaller more intimate areas for residents to escape to for rest and relaxation. The communal rooftop terraces at the upper levels have been deliberately designed with both active areas and more passive type spaces, for rest and relaxation, to avoid any acoustic implications to adjacent apartments.</p> <p>Collectively this range of open spaces and landscaping allows for quality amenity for the new emerging precinct.</p>
<p>e)</p> <p><i>To ensure clear, legible and safe pedestrian and cycle connections, including links to the regional cycle network.</i></p>	Turner	<p><b>Complies</b></p> <p>The design of Doran Drive Plaza has allowed for clear sight lines, good lighting, and passive surveillance from adjoining tenancies and from apartments overhead. This creates a safe environment for all day use. The design for the plaza allows for a generous 6m wide hard landscaped throughfare along the eastern edge that provides a direct connection from the metro station to the Showgrounds. The public domain on all side of the subject site is pedestrian centric, and has carefully managed any integration of car park, loading, and other services requirements. Bike use is encouraged through the provision of bike racks within the public domain and secure lock-up areas in the basement car park, with associated end-of-trip facilities.</p>

## 2.11 Public Domain and Landscape

### 2.11.1 Public Open Space

Objectives	Team	Comment
<p><i>f)</i> To deliver an environmentally and socially sensitive and responsive design that ensures the environmental qualities of surrounding landscapes are maintained or enhanced.</p>	Turner/ Urbis	<p><b>Complies</b></p> <p>Refer to the landscape design by Urbis, which has integrated native tree and plant species that are appropriate for this location and in keeping with the surrounding context.</p>
<p><i>g)</i> To provide an emphasis on local character and a continuity of landscape.</p>	Turner/ Urbis	<p><b>Complies</b></p> <p>Refer to the landscape design by Urbis, which has integrated native tree and plant species that are appropriate for this location and in keeping with the surrounding context.</p>
<p><i>h)</i> To ensure the delivery of public art assets as part of an integrated public domain and public open spaces</p>	Turner/ CK Statham	<p><b>Complies</b></p> <p>Refer to the separate Public Art Strategy that accompanies this report. The proposal includes for a new Public Artwork within Doran Drive Plaza that will reference the natural context and local history.</p>
<p><i>i)</i> To retain the existing landscape qualities within public open spaces in terms of topography and existing trees.</p>	Turner/ Urbis	<p><b>Complies</b></p> <p>Refer to the landscape design by Urbis, which has integrated native tree and plant species that are appropriate for this location and in keeping with the surrounding context.</p>

## 2.11 Public Domain and Landscape

### 2.11.1 Public Open Space

Control	Team	Comment
<p><b>1.</b></p> <p><i>Two new publicly accessible open spaces are to be provided:</i></p> <p><i>a. An urban plaza in the mixed-use Doran Drive Precinct – Doran Drive Plaza</i></p> <p><i>b. A local park within the new residential neighbourhood of Precinct East – Precinct East Park.</i></p>	Turner	<p><b>Complies</b></p> <p>a. The proposal for the subject site includes the Doran Drive Plaza. The area and dimensions of the plaza match the requirements of the Urban Design Guidelines. The plaza will be publicly accessible, and designed as a flexible space with quality landscaping, public art, DDA compliant, and with level direct access into the ground floor retail tenancies of the proposed new development.</p> <p>b. N/A as this relates to the Eastern Precinct and not to the subject site.</p>
<p><b>2.</b></p> <p><i>A publicly accessible pedestrian link is to be provided between Precinct East Park and Showground Road with a minimum width of 8m.</i></p>		<p><b>N/A</b></p> <p>The proposal only relates to the subject site within the Drive Drive Precinct.</p>
<p><b>3.</b></p> <p><i>All new publicly accessible open spaces are to be universally accessible and compliant with the Disability Discrimination Act 1995 (DDA).</i></p>	Turner/ Morris Goding	<p><b>Complies</b></p> <p>The proposed new Doran Drive Plaza will be universally accessible and compliant with DDA requirements.</p>
<p><b>4.</b></p> <p><i>Buildings that interface directly with the publicly accessible open spaces are to be accessed from those spaces via active retail and commercial frontages or residential courtyards.</i></p>	Turner	<p><b>Complies</b></p> <p>The proposed ground floor retail tenancies have direct level access from the tenancies to the Doran Drive Plaza open space. This allows for outdoor dining to spill from the tenancies into this new plaza area, thus greatly boosting both day and night activation and passive surveillance of the space. There is also a main entry from the plaza into the internal retail circulation where users can access retail and commercial tenancies, as well as ancillary amenities such as toilets, etc.</p>
<p><b>5.</b></p> <p><i>Attractive, high quality outdoor spaces for children to play shall be integrated into the public domain where appropriate. Such spaces should allow for interactive play and include seating and shading.</i></p>	Turner/ Urbis	<p><b>Complies</b></p> <p>The proposed Doran Drive Plaza is designed as a highly flexible space that can accommodate multiple uses. The proposal includes a lawn area which will offer a soft fall space for families to gather in, and children to play in. The lawn is separate to the main pedestrian links through the space, is located away from the traffic movements in the adjacent streets, and has good sightlines and passive surveillance from the adjacent retail and outdoor dining areas.</p>
<b>Doran Drive Plaza</b>		
<p><b>6.</b></p> <p><i>Doran Drive Plaza is to be a minimum 1,400m<sup>2</sup> of publicly accessible open space located in accordance with Figure 34 and the drawings forming part of the Plans for Approval.</i></p>	Turner	<p><b>Complies</b></p> <p>The proposed area and dimensions of Doran Drive Plaza match the requirements of the Urban Design Guidelines.</p>

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**Precinct East Park**

7.

N/A

*Precinct East Park is to be a minimum 3,500m<sup>2</sup> of public open space located in accordance with Figure 34 and the drawings forming part of the Plans for Approval.*

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**2.11 Public Domain and Landscape**
**2.11.1 Public Open Space**
**Control**
**Team**
**Comment**
**Utility Infrastructure**

8.

JHA

**Complies**

*Electricity reticulation and telecommunications is to be provided underground for all urban development.*

Electricity and Telecommunications will all be underground for the development as part of the authority detailed design phase works.

9.

JHA

**Complies**

*Any existing above-ground electricity reticulation service is to be relocated underground during the construction stage, to the satisfaction of the relevant authority, with the exception of main transmission lines.*

Any overhead Endeavour Energy assets within the Doran precinct area can be located underground to reflect the overall Precinct arrangements.

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## 2.11 Public Domain and Landscape

### 2.11.2 Communal Open Space

Objectives	Team	Comment
<p>a)</p> <p><i>To provide additional amenity and recreational opportunities within the private domain for the residents of the Precinct.</i></p>	Turner	<p><b>Complies</b></p> <p>A large podium level communal open space is provided, as well as 3 other mid-level communal roof terraces. Collectively these provide quality facilities and recreational opportunities for the residents within the Doran Drive Precinct.</p>
Controls		
<p>1.</p> <p><i>Communal open space is to be provided in the form of private areas at ground level, podium and rooftop level in accordance with SEPP 65 Apartment Design Guide.</i></p>	Turner	<p><b>Complies</b></p> <p>Secure communal open areas are located at podium level, and on the mid-level rooftops. All buildings have direct access to the communal open areas. Collectively these communal open areas meet the requirements of the SEPP 65 Apartment Design Guide.</p> <p>The main communal open space is located at Level 2 on the podium and is over 1400sqm. It has a range of active and passive uses that meet the varied requirements of all age groups and demographics. This space enjoys quality solar access, due to the building separation along its northern edge. The additional rooftop communal terraces are envisaged as passive and active areas for relaxation, with quality solar access and views across the Showgrounds and surrounding precinct.</p>
<p>2.</p> <p><i>External (outside) communal open space areas are to be located and designed to:</i></p> <p><i>a. Be seen from the street between buildings</i></p> <p><i>b. Provide for active and passive recreation needs of all residents</i></p> <p><i>c. Provide landscaping</i></p> <p><i>d. Present as a private area for use by residents only</i></p> <p><i>e. Include passive surveillance from adjacent internal living areas and/or pathways</i></p> <p><i>f. Have a northerly aspect where possible</i></p> <p><i>g. Be in addition to any public thoroughfares.</i></p>	Turner	<p><b>Complies</b></p> <p>The residential towers anchor the 4 corners of the podium and allow for generous building separation along the northern and southern edges of the main podium level communal courtyard. De Clambe Drive and Mandala Parade will have direct views into the courtyard. The generous planting and trees within the main courtyard will have a beneficial impact to the streetscape and the greening of the development.</p> <p>The main podium level communal open space has been designed to provide a range of uses and facilities, that meet the needs all age groups and demographics within the development. Children's play area and a large lawn area with BBQ facilities is in the centre of the space for gathering and play and a garden patch and lawn area is located at the north of the podium where individuals can enjoy views out over the adjacent Showgrounds. Remaining areas include for large amounts of planting to offer a quality outlook for apartments overhead, or smaller more intimate areas for residents. The communal rooftop terraces at the upper levels have been deliberately designed with both active areas and more passive type spaces, for rest and relaxation, to avoid any acoustic implications to adjacent apartments. The communal open areas are secure and for residents only, separate to any public thoroughfares.</p>
<p>3.</p> <p><i>Internal communal open spaces are to be located within a larger courtyard accessible via ground floor residential courtyards.</i></p>	Turner	<p><b>Complies</b></p> <p>The proposal allows for two large community rooms. The first of these rooms is located on De Clambe Drive at street level, facing the Showgrounds. It was direct level access from the street. The room has an external north facing terrace area, accessed from the north facing fully glazed façade. The second of these rooms is located at Level 02, overlooking Mandala Parade and the metro station. The room is accessed from the commercial lobby and its associated commercial lift/stairs. The room enjoys substantial glazing to the western and southern sides, allowing for a strong visual connection to the street below.</p> <p>These rooms are part of the managed community facilities that will be shared with the wider community, refer to plan of management.</p>



## 2.11 Public Domain and Landscape

### 2.11.2 Communal Open Space

Controls	Team	Comment
<p><b>4.</b></p> <p><i>Communal open space is to provide a range of uses including seating, picnic facilities, play spaces, productive gardens and lawn areas amongst generous planting</i></p>	Turner	<p><b>Complies</b></p> <p>A range of active and passive programmes and uses have been provided to the communal open space including:</p> <p>Level 2</p> <ul style="list-style-type: none"> <li>- Large central flexible lawn, framed by trees with garden beds / screening to private open space. The lawn area provides spaces for picnics, gathering and exercise</li> <li>- Community / productive gardens</li> <li>- Informal play space with integrated seating</li> <li>- BBQ pavilion with tables and chairs</li> <li>- Intimate seating spaces are provided to the edges of the central lawn</li> </ul> <p>Level 6</p> <ul style="list-style-type: none"> <li>- BBQ facilities with tables and chairs</li> </ul> <p>Level 3, 8 and 9</p> <p>Series of communal and intimate seating spaces set within lush garden setting, areas to gather and enjoy the views to all directions.</p>
<p><b>5.</b></p> <p><i>Internal open space areas are to provide opportunities for larger communal gathering and/or active recreation (i.e. kitchen facilities, tables and chairs, small-scale gymnasium or health studio) where possible.</i></p>	Turner	<p><b>Complies</b></p> <p>The proposal allows for two large community rooms.</p> <p>The first of these rooms is located on De Clambe Drive at street level, facing the Showgrounds. It has direct level access from the street. The room is regular in shape, with adequate space to allow for kitchen facilities, WC, storage, etc.</p> <p>The second of these rooms is located in Building D at Level 02, overlooking Mandala Parade and the metro station. The room is accessed from the commercial lobby and its associated commercial lift/stairs. The room is regular in shape, with adequate space to allow for kitchen facilities, WC, storage, etc.</p> <p>These two rooms are part of the managed community facilities that will be shared with the wider community, refer to plan of management.</p>
<p><b>6.</b></p> <p><i>Communal open space in Precinct East is to be primarily at grade and read as a continuation of the adjacent public domain character in planting and materiality. Small trees suitable for the landscaped area provided are encouraged.</i></p>		N/A
<p><b>7.</b></p> <p><i>Podium and rooftop gardens across all precincts are to incorporate a minimum of 70% native planting for local character, however this may be supplemented with exotics for colour and variation, and edible species as part of vegetable or herb gardens.</i></p>	Turner	<p><b>Complies</b></p> <p>The Landscape planting for the site will have a minimum of 75% of indigenous / water sensitive planting species</p>
<p><b>8.</b></p> <p><i>Rooftop gardens must be adequately enclosed and accessible to occupants of the development.</i></p>	Turner	<p><b>Complies</b></p> <p>The proposed communal rooftop terraces all have lift access. The path of travel to the roof terraces and their doorways are all designed to be fully accessible. Pathways and paving have been designed to meet DDA requirements.</p>

## 2.11 Public Domain and Landscape

### 2.11.2 Communal Open Space

Controls	Team	Comment
<p><b>9.</b></p> <p><i>Roof features shall be designed to generate an interesting skyline and enhance views from adjoining developments and surrounding areas</i></p>	Turner	<p><b>Complies</b></p> <p>The Doran Drive Precinct has a substantial fall from east to west. The proposed buildings are compliant with the maximum building height, and their height matches the alignment of this steep topography. Each tower has a varied architectural expression, and their colour, materials, and detailing differ greatly to create a playful grouping of buildings and a highly articulated architectural response. The large podium courtyard and mid-level communal rooftop terraces allow for generous landscaping that provide a green verdant roofscape to those parts of the buildings. Collectively these design considerations generate an interesting skyline and streetscape.</p>
<p><b>10.</b></p> <p><i>The design of exterior private open spaces such as podium or rooftop gardens are to achieve amenity by addressing visual and acoustic privacy, safety, security and wind effects.</i></p>	Turner	<p><b>Complies</b></p> <p>All apartments that are directly adjacent to the main podium communal open space, or beside the communal rooftop terraces, are provided with minimum 1800mm high fences to their private terraces for visual and acoustic privacy. The landscape design also allows for generous deep planting adjacent to these fences as a green buffer. Active uses and places of gathering, such as BBQs, are located away from private terraces to avoid conflicts of use. Within the communal open areas, clear sightlines are provided, plus adequate lighting and wayfinding signage will be provided as part of the later detailed design development. Upper-level apartments provide appropriate passive surveillance of the communal open areas to discourage any anti-social behaviour.</p>
<p><b>11.</b></p> <p><i>The location and design of communal open space is to achieve direct sunlight to the principal usable part of the communal open space for a minimum of 2 hours between 9am and 3pm at the winter solstice (21 June).</i></p>	Turner	<p><b>Complies</b></p> <p>Please refer to Communal Area Shadow Diagrams DA-730-101 and DA-730-102 which show the compliance of 2 hours solar access to the principle usable communal area between 9am and 3pm at the winter solstice. The proposal more than adequately meets this control, due to the size and orientation of the large central podium courtyard, and due to the additional rooftop communal terraces at the upper levels.</p>

## 2.11 Public Domain and Landscape

### 2.11.3 Materials and Elements

Objectives	Team	Comment
<p>a)</p> <p><i>To emphasise local character and a continuity of landscape.</i></p>	Urbis	<p><b>Complies</b></p> <p>Feature granite paving is provided throughout. Paving in the public domain will be in accordance Council's standards for public domain works.</p>
<p>b)</p> <p><i>To provide robust and site appropriate materials and elements across the public domain.</i></p>	Urbis	<p><b>Complies</b></p> <p>Material, finishes, furniture and fixtures will be selected with consideration to whole of life costs, detailed and installed to minimize ongoing maintenance needs.</p>
<p>c)</p> <p><i>To respond to the materials and elements in the existing open spaces and streets in the broader Precinct.</i></p>	Urbis	<p><b>Complies</b></p> <p>Paving and furniture in the public domain will be in accordance Council's standards for public domain works.</p>
<p>d)</p> <p><i>To ensure that high quality public art is incorporated into the fabric of buildings in the public domain or other publicly accessible areas.</i></p>	Urbis / CK Statham	<p><b>Complies</b></p> <p>Public Art opportunities and location have been shown on the landscape plans</p>
Controls	Team	Comment
<p>1.</p> <p><i>All public domain materials and elements including walls, furniture and play equipment are to be durable and of a quality that can withstand public use and high pedestrian volumes.</i></p>	Urbis	<p><b>Complies</b></p> <p>Paving and furniture in the public domain will be in accordance Council's standards for public domain works</p>
<p>2.</p> <p><i>Where communal open space is at ground level, the materiality is to be complimentary to the adjacent public domain and may include additional materials such as brick and/or stone paving. Materiality of podium and rooftop communal open space is to relate to and complement the materiality of the building.</i></p>	Urbis	<p><b>Complies</b></p> <p>Feature granite paving is provided throughout. Paving in the public domain will be in accordance Council's standards for public domain works.</p> <p>The design strategy is to provide a durable and high-quality landscaped building setting with a consistency of quality and treatments across the site selected to compliment the character of the architecture. Consideration has been given to durability and practicality for ongoing maintenance.</p>
<p>3.</p> <p><i>Fencing may be provided to delineate private vs public space, however should provide clear views into the ground level private open space.</i></p>		<p><b>N/A</b></p> <p>There is no ground level private open space.</p>
<p>4.</p> <p><i>The fencing materials chosen must protect the acoustic amenity and privacy of courtyards.</i></p>	Turner/ Urbis	<p><b>Noted</b></p> <p>This guideline will be adhered to during the design development phase prior to the construction certificate.</p>
<p>5.</p> <p><i>All boundary fencing/ walls fronting a street shall be setback to permit landscaping, and shall include recesses and other architectural features.</i></p>	Turner/ Urbis	<p><b>Complies</b></p> <p>Feature cascading planters have been designed at key areas around the building perimeter, supplementing architectural recesses and other features.</p>
<p>6.</p> <p><i>All fencing or walls shall be combined and integrated with site landscaping.</i></p>	Turner/ Urbis	<p><b>Complies</b></p> <p>Planting is integrated with fencing and walls</p>

## 2.11 Public Domain and Landscape

### 2.11.3 Materials and Elements

Controls	Team	Comment
<p>7.</p> <p><i>Fencing details for the site, clearly showing the location, height and type of proposed fencing is to be submitted as part of the development application.</i></p>	Turner	<p>There is no residential use at ground floor therefore no fencing to terraces or balconies is required.</p> <p>There are gates &amp; fences in control areas like the main retail entries and the communal podium stair entry. These gates &amp; fences will be designed to ensure enough privacy and security where required. Final design during the design development phase prior to the construction certificate.</p> <p>The podium level private terraces space has 1800mm high fences on all sides. Final design during the design development phase prior to the construction certificate.</p>
<p>8.</p> <p><i>Lighting is to be provided to all public spaces and connections for interest, wayfinding and safety purposes. Lighting shall be designed to minimise glare and light pollution and is to be aesthetically pleasing, functional and relates to intended night time use and activity.</i></p>	JHA	<p>All publically accessible pathways and connections to be lit to the agreed pedestrian category AS1158.3.1. Lighting within this standard ensures that light is provided on the horizontal plane and vertical (for facial recognition). The selected light category is verified against traffic and crime statistics to ensure the space is lit to an acceptable level for safety and ease of wayfinding.</p> <p>Lighting will also be designed at Design development to meet AS4282 and will be simulated to ensure the design is compliant. Obtrusive light parameters apply to ensure obtrusive light is controlled to avoid upward spill light and spill light into residential dwellings.</p> <p>Lighting design will be designed to best practice to ensure spaces are not overlit and will be visually comfortable. The proposed lighting is fit for purpose but to also provide an ambience that encourages social interaction and use of space</p>
<p>9.</p> <p><i>Outdoor seating, bins, drink fountains and other furniture items are to be provided in consultation with, and to the satisfaction of, The Hills Shire Council, unless these elements are otherwise integrated within landscape design and treatments and that appropriate vehicular access is provided to facilitate maintenance.</i></p>	Urbis	<p><b>Complies</b></p> <p>These elements have been integrated within the landscaping design.</p>
<p>10.</p> <p><i>Signage is to be provided in accordance with Sydney Metro's Northwest Wayfinding Strategy.</i></p>	Turner	<p><b>Noted</b></p> <p>Signage related to wayfinding to the station and other destinations within the wider precinct to be coordinated post DA.</p>

## 2.11 Public Domain and Landscape

### 2.11.4 Planting and Trees

Objectives	Team	Comment
<p>a)</p> <p><i>To reinforce the Garden Shire context of the Precinct through extensive and diverse planting across public and private open space.</i></p>	Urbis	<p><b>Complies</b></p> <p>Native and exotic planting is provided to both the private and public spaces.</p>
<p>b)</p> <p><i>To emphasise local character and a continuity of landscape.</i></p>	Urbis	<p><b>Complies</b></p> <p>Most of the street trees have been retained and native planting is extended throughout the development which provides a continuity of landscape character.</p>
<p>c)</p> <p><i>To respond to the existing planting across open spaces and streets in the broader Precinct.</i></p>	Urbis	<p><b>Complies</b></p> <p>Most of the existing street trees have been retained and native planting is extended throughout the development which provides a continuity of landscape character.</p>
<p>d)</p> <p><i>To retain existing trees within open space where possible.</i></p>	Urbis	<b>N/A</b>
<p>e)</p> <p><i>To enable survival and growth of new trees to achieve a large scale.</i></p>	Urbis	<p><b>Complies</b></p> <p>The proposal is compliant with ADG and Council Guidelines around soil depth and volume to ensure the growth of new trees.</p>
<p>f)</p> <p><i>To increase canopy cover and biodiversity.</i></p>	Urbis	<p><b>Complies</b></p> <p>50% Tree canopy is provided to the publicly accessible plaza  - 45% Tree canopy is provided to the communal spaces  - Proposed shrub and trees species to enhance biodiversity</p>
<p>g)</p> <p><i>To enable the unique history and heritage of the site to be reflected through plant species and garden design.</i></p>	Urbis	<p><b>Complies</b></p> <p>The environmental theme requires that the planting palette draw from the existing local vegetation communities.</p>
Controls	Team	Comment
<b>Species</b>		
<p>1.</p> <p><i>Planting design is to be appropriate for the intended location and function. Tree and plant species selection must take into account a number of factors including:</i></p> <ul style="list-style-type: none"> <li><i>a. Climate/microclimate</i></li> <li><i>b. Size requirements/constraints</i></li> <li><i>c. Form</i></li> <li><i>d. Native/exotic</i></li> <li><i>e. Density of foliage</i></li> <li><i>f. Growth rate</i></li> <li><i>g. Availability</i></li> <li><i>h. Maintenance (i.e. leaf fall, fruit drop) and safety (i.e. branch drop)</i></li> <li><i>i. Other considerations such as interpretation, which is outlined in the Interpretation Strategy for the precinct and must be used to guide decisions.</i></li> </ul>	Urbis	<p><b>Complies</b></p> <p>The planting palette has been carefully selected in direct response to a variety of site-specific conditions. The planting design will meet the following objectives:</p> <ul style="list-style-type: none"> <li>- To increase the number of indigenous species planted in Hills shire region</li> <li>- To eliminate the use of noxious weeds of potentially invasive species in developments</li> <li>- To use plants in such a way to foster energy efficient development that relies on passive energy principles for heating and cooling</li> <li>- To reduce maintenance and water consumption through appropriate species selection</li> <li>- To create buffer zones and add to existing areas of remnant vegetation with locally indigenous species including supplementary River-Flat Eucalypt Forest on Coastal Floodplains planting.</li> <li>- The Landscape planting for the site will have a minimum of 75% of indigenous / water sensitive planting species</li> </ul> <p>The northern and southern boundary planting will be comprised of a native palette inspired by the sites ecological setting. The planting scheme will create separation from the adjoining site to the south and tie into the existing parkland to the north. Species will include Acmena, Eucalyptus and Melaleuca.</p>

## 2.11 Public Domain and Landscape

### 2.11.4 Planting and Trees

Controls	Team	Comment
<b>Species</b>		
<p>2.</p> <p><i>Drought tolerant plant species, and species that enhance habitat and ecology, are to be prioritised.</i></p>	Urbis	<p><b>Complies</b></p> <p>The Landscape planting for the site will have a minimum of 75% of indigenous / water sensitive planting species which are drought tolerant plant species, and species that enhance habitat and ecology</p>
<p>3.</p> <p><i>Native ground covers and grasses are to be used in garden beds and path surrounds. Turf is to be confined to useable outdoor areas.</i></p>	Urbis	<p><b>Complies</b></p> <p>Native ground covers and grasses will be planted within garden beds and lawn to the usable outdoor spaces.</p>
<p>4.</p> <p><i>Native species are to be used for the Water Sensitive Urban Design rain garden areas.</i></p>	Urbis	<p><b>Complies</b></p> <p>The Landscape planting for the site will have a minimum of 75% of indigenous / water sensitive planting species</p>
<p>5.</p> <p><i>Tree and plant species are to reflect the four themes outlined in the Hills Showground Station Precinct Interpretation Strategy 2019, which are:</i></p> <p><i>a. Aboriginal Cultural Heritage and History</i></p> <p><i>b. Resistance and Rebellion</i></p> <p><i>c. Agriculture and Orchardring</i></p> <p><i>d. Pride in the Hills.</i></p>	Urbis	<p><b>Complies</b></p> <p>The Landscape tree and plant palette responds to the four above themes, please refer to our planting schedule</p>



6.

*Consultation with The Hills Shire Council and with the Aboriginal community (as per Landcom's Draft Aboriginal Reconciliation Action Plan) is required to inform species choices.*

Deicorp

Complies

A copy of Landcom's Draft Aboriginal Reconciliation Action Plan is not available. However, consultation was undertaken with The Hills Shire Council during the design process. In accordance with the Urban Design Guidelines, the proposed tree and plant species reflect the following four themes:

- a. Aboriginal Cultural Heritage and History
- b. Resistance and Rebellion
- c. Agriculture and Orchardring
- d. Pride in the Hills.

The Draft Aboriginal Reconciliation Action Plan has wider applicability elsewhere in the precinct. Notwithstanding, the proposed species selection has been informed by the four design themes discussed above.

Under the Connecting with Country Draft Framework created by the Government Architects of NSW an Aboriginal Artist has been engaged to create integrated Aboriginal artworks into the project. As Indigenous artists with cultural heritage connections to the traditional land of the Darug people, the artists will elevate and create awareness of stories of the First Nation People, and to display these stories in a way that promotes reconciliation and awareness to Connecting With Country.

Through shared knowledge and learning between local Aboriginal Elders, Aboriginal advisors, consultants, and the Aboriginal community as well as with project design team and The Hills Shire Council, the artist will create artworks that gives all who visit the artworks site access to learn and understand the local First Nation people.

7.

*Doran Drive Plaza is to utilise a combination of native and exotic species suited to its urban character with a minimum of 50% native species, including a grove of deciduous trees reflecting the area's history with orchards as outlined in Section 4.2.1. Public Art and Interpretation.*

Urbis

Complies

8.

*Precinct East Park and the pedestrian link are to have a palette of predominantly native plants (minimum 70%) that includes existing trees. Exotic deciduous trees (to a max 30% of total park trees) may be incorporated into the park as a feature and to provide solar access in winter.*

N/A

## 2.11 Public Domain and Landscape

### 2.11.4 Planting and Trees

Controls	Team	Comment
<p><b>9.</b></p> <p><i>Communal open spaces are to contain predominantly native species (minimum 70%) at ground level. Edible and exotic feature species are acceptable on podium and rooftop gardens.</i></p>	Urbis	<p><b>Complies</b></p> <p>Refer to Landscape package prepared by Urbis for further detail.</p>
<b>Existing Trees</b>		
<p><b>10.</b></p> <p><i>All existing trees within Precinct East Park that are of arboricultural significance are to be retained and protected, where possible.</i></p>		N/A
<b>New Planting and Trees</b>		
<p><b>11.</b></p> <p><i>Planting in the public domain is to be combined and layered in a way that provides visual interest, reduces visible areas of mulch, is easily maintained, provides screening where appropriate and maintains clear sight lines where needed.</i></p>	Urbis	<p><b>Complies</b></p> <p>Refer to Landscape package prepared by Urbis for further detail.</p>
<p><b>12.</b></p> <p><i>Layered planting is to be used to define the public and private interface with residential buildings, and to provide filtered views and adequate screening to balconies and front courtyards of garden apartments while maintaining visual surveillance of the public domain.</i></p>	Turner/ Urbis	<p><b>Complies</b></p> <p>Refer to Landscape package prepared by Urbis for further detail.</p>
<p><b>13.</b></p> <p><i>Large trees are to be planted in the new street in Precinct East, building setbacks and public spaces to provide scale and partial screening to built form in accordance with the Tree Canopy Cover controls contained in Section 2.16.4</i></p>		N/A
<p><b>14.</b></p> <p><i>The new street in Precinct East is to have a predominantly native palette of large trees for scale and shade.</i></p>		N/A
<p><b>15.</b></p> <p><i>New street trees within Precinct East to be planted at least at 10m intervals.</i></p>		N/A
<p><b>16.</b></p> <p><i>Where primary setbacks exceed 6m, deep soil areas are to be incorporated for large scale planting</i></p>	Urbis	<p><b>Complies</b></p> <p>The proposal is compliant with Council and Apartment Design Guidelines around soil depth and volume to ensure the growth of new trees.</p>

## 2.11 Public Domain and Landscape

### 2.11.4 Planting and Trees

#### Controls

#### Team

#### Comment

#### New Planting and Trees

17.

*Trees and planting are to be used to provide a comfortable microclimate, including shade for seating and footpaths and adverse wind amelioration. In particular, dense evergreen trees shall be provided and shrubs and hedging shall be considered to improve wind conditions in the pedestrian link, at building corners, and between Precinct East Park and Andalusian Way.*

Urbis

Complies

Trees species and planting have been selected and designed to provide a comfortable microclimate which seating benches collocated under trees. The proposal aims to increase Urban canopy & City greening improve Green infrastructure- with a 80% site coverage to mitigate Heat island effect.

18.

*Vegetation of the required size and nature is to be provided in locations identified in the wind assessment that is required to be submitted with a Development Application in accordance with Section 2.16.2.*

Windtech

19.

*Landscaped areas are to have a minimum width of 2m. Areas less than 2m in width will be excluded from the calculation of landscaped area.*

Urbis

Complies

Where possible garden beds are greater than 2m in width however there are some locations where they are less than 2m. Based on the plant selection, Urbis have advised these should still be calculated within the landscape areas.

20.

*Soft landscaping to the front of the terrace is to be a minimum of 40% of the setback area, contiguous, and a minimum of 2m in any direction.*

Urbis

N/A

21.

*Landscape design is to be integrated with water and stormwater management.*

Urbis

Complies

22.

*The incorporation of green walls and roofs into the design of commercial and residential buildings is encouraged. Where suitable, building facades should incorporate vertical landscaping features to soften the visual bulk of buildings and to improve streetscape appeal. Refer to Section 2.15 Sustainability for relevant controls.*

Urbis

Complies

Podium gardens are provided, there are cascading edge planters to the building.

23.

*Where roof gardens and green walls are provided, consideration should be given to the Urban Green Cover in NSW – Technical Guidelines, published by the Office of Environment and Heritage.*

N/A

**2.12 Culture**

<b>Objectives</b>	<b>Team</b>	<b>Comment</b>
<p><b>a)</b>  <i>To appropriately design and locate the development with sensitivity in order to minimise the likelihood of disturbance, impact or interface with any significant heritage items.</i></p>	Urbis	<p><b>Complies</b></p> <p>The subject site is not listed as a heritage item, however, it is located in the vicinity of two locally listed heritage items under The Hills Local Environmental Plan (LEP) 2019 including 'House', 128-132 Showground Road (Item No. 69) and 'House', 107 Showground Road (Item No. 68). It is also located directly to the south of the Castle Hill Showground which has previously been identified in the North West Rail Link Environmental Impact Statement: European Heritage Report (2012) as having heritage significance at a local level. Refer to Section 7 of the Heritage &amp; Archaeological Impact Statement prepared by Urbis for discussion.</p>
<p><b>b)</b>  <i>To narrate the different stages of historical land use in the study area and wider area through the design of built form, landscape and public art.</i></p>	GYDE	<p><b>Complies</b></p> <p>The implementation of the Heritage Interpretation Strategy prepared by GML (October 2019) during the detailed design and construction phases will facilitate the narration of the different stages of historical land use of the study area and wider area.</p>
<p><b>c)</b>  <i>To embrace and respond to Landcom's Draft Reconciliation Action Plan (RAP).</i></p>	GYDE	<p><b>N/A</b></p> <p>A copy of Landcom's Draft Aboriginal Reconciliation Action Plan is not available.</p>
<p><b>d)</b>  <i>To foster a strong sense of culture and community within the new residential and non-residential population.</i></p>	GYDE	<p><b>Complies</b></p> <p>The proposed development will aid in fostering a strong sense of culture and community between the new development and former Castle Hill Showground through the reinvigoration of the area which the development would afford. The proposal would generate opportunities for communication and public awareness of the Showground and its historical value within the Castle Hill area. This will further be facilitated by the implementation of the Heritage Interpretation Strategy prepared by GML (October 2019) during the detailed design and construction phases of the project.</p>
<p><b>e)</b>  <i>To connect and celebrate the non-Aboriginal and Aboriginal cultural heritage of the site and its context.</i></p>	GYDE	<p><b>Complies</b></p> <p>The implementation of the Heritage Interpretation Strategy prepared by GML (October 2019) during the design and construction phases of the project will aid in connecting and celebrating the non-Aboriginal and Aboriginal cultural heritage of the site and its context.</p>
<b>Controls</b>	<b>Team</b>	<b>Comment</b>
<p><b>1.</b>  <i>Development at, or within the vicinity, must have regard to any heritage items listed in Schedule 5 of THLEP.</i></p>	Urbis	<p><b>Complies</b></p> <p>The development has had regard to the heritage items listed under the Schedule 5 of the THLEP as addressed in Section 7.1.1 of the Heritage &amp; Archaeological Impact Statement prepared by Urbis.</p>
<p><b>2.</b>  <i>Future detailed design stages of the buildings should respond to the interface with the Castle Hill Showground site and provide an appropriate interface, through built form articulation and streetscape design in order to mitigate the impact on its setting.</i></p>	Turner/ Urbis	<p><b>Complies</b></p> <p>Future detailed design stages will follow the current design which responds to the interface with the Castle Hill Showground site and provide an appropriate interface.</p>

## 2.12 Culture

Controls	Team	Comment
<p><b>3.</b></p> <p><i>Cultural connections are to be developed during detailed design via the four interpretive themes and stories by incorporating high quality public art into the fabric of buildings in the public domain, other publicly accessible areas or open space areas, as well as other meaningful elements that underpin the themes as outlined below:</i></p> <p><i>Theme 1 - Aboriginal Cultural Heritage:</i></p> <ul style="list-style-type: none"> <li>- Use extensive native planting across the public domain.</li> <li>- Provide pedestrian and visual connections to Cattai Creek.</li> <li>- Provide art and interpretation in public spaces and in consultation with the local aboriginal community.</li> </ul> <p><i>Theme 2 - Resistance and Rebellion:</i></p> <ul style="list-style-type: none"> <li>- Provide historical interpretation in public spaces.</li> </ul> <p><i>Theme 3 - Agriculture and Orchardng:</i></p> <ul style="list-style-type: none"> <li>- Plant communal vegetable gardens and fruiting trees in communal open spaces.</li> </ul> <p><i>Plant a grove of flowering trees in Doran Drive Plaza.</i></p> <p><i>Theme 4 - Pride in the Hills:</i></p> <ul style="list-style-type: none"> <li>- Provide visual and physical connections to the adjacent Castle Hill Showground.</li> <li>- Provide public art and interpretation in public spaces.</li> <li>- Acknowledge the context of 'The Garden Shire' through extensive tree and understorey planting, planted setbacks to buildings, green roofs and podium gardens and visual connections to open spaces.</li> <li>- Provide space for public community events and gatherings in Doran Drive Plaza.</li> <li>- This is to be documented within the site-specific public art and interpretation plan that is also referenced in control number 10 of the Public Art and Interpretation section.</li> </ul>	GYDE	<p><b>Complies</b></p> <p>In relation to the cultural context, the proposed public art strategy (Appendix 19) has considered the history of the area. For example, the history of orchards and fruit-growing in the area is celebrated in the annual Orange Blossom festival, which takes place at the Castle Hill Showground annually. Tens of thousands of people attend the one-day festival, which features popup markets, public speaking competitions, pet shows, a senior's dance, a bonsai exhibition and plant sale, BMX competitions, classic car and bike displays, an art show, and other stalls and displays.</p> <p>Castle Hill is on the northern periphery of the Cumberland Plain, and was part of the Cumberland Plain woodlands. These were the lands of the Bediagal, Tugagal and Booroobirronggal people. The Aboriginal people of this area depended on the small animals, insects and edible roots that were found on the Cumberland Plan, and also ate mullet from the creeks. Weapons and tools were made from stalks of grass trees, timber, or tree roots tipped with stone and bone.</p> <p>Throughout the 19th century, the area developed into a fruit-growing region, and by the 1890s the Castle Hill district was home to many orchards, small farms and houses. Oranges became a staple crop of the area, and market gardens became common with the arrival of European migrants in the early 20th century and again after World War II. The Showground became a centre for agricultural and horse shows, and has retained its character as the surrounding development has evolved. In 2013, the Hills Entertainment Centre was demolished to make way for Showground Station.</p> <p>The proposed water-based artwork has drawn inspiration from the four themes in the Urban Design Guidelines. Refer to Appendix 19 for further detail.</p> <p>Under the Connecting with Country Draft Framework created by the Government Architects of NSW an Aboriginal Artist has been engaged to create integrated Aboriginal artworks into the project. As Indigenous artists with cultural heritage connections to the traditional land of the Darug people, the artists will elevate and create awareness of stories of the First Nation People, and to display these stories in a way that promotes reconciliation and awareness to Connecting With Country.</p> <p>Through shared knowledge and learning between local Aboriginal Elders, Aboriginal advisors, consultants, and the Aboriginal community as well as with project design team and The Hills Shire Council, the artist will create artworks that gives all who visit the artworks site access to learn and understand the local First Nation people.</p>
<p><b>4.</b></p> <p><i>Undertake appropriate and meaningful consultation and collaboration for the planning and production of Public Art and Interpretation as outlined in Control 4 of the Public Art and Interpretation section.</i></p>	Deicorp	<p><b>Complies</b></p>

## 2.13 Public Art and Interpretation

Objectives	Team	Comment
<p>a)</p> <p><i>To celebrate the Aboriginal and European heritage of the site and its surrounds.</i></p>	Deicorp	<p>Under the Connecting with Country Draft Framework created by the Government Architects of NSW an Aboriginal Artist has been engaged to create integrated Aboriginal artworks into the project. As Indigenous artists with cultural heritage connections to the traditional land of the Darug people, the artists will elevate and create awareness of stories of the First Nation People, and to display these stories in a way that promotes reconciliation and awareness to Connecting With Country.</p> <p>Through shared knowledge and learning between local Aboriginal Elders, Aboriginal advisors, consultants, and the Aboriginal community as well as with project design team and The Hills Shire Council, the artist will create artworks that gives all who visit the artworks site access to learn and understand the local First Nation people.</p>
<p>b)</p> <p><i>To add interest and local character to the public domain through public art and interpretation by including a range of public artwork types via an engagement of a Public Art Curator.</i></p>	Turner/ CK Statham	<p><b>Complies</b></p> <p>Refer to the separate Public Art Strategy that accompanies this report. The proposal includes for a new Public Artwork within Doran Drive Plaza that will reference the natural context and local history.</p>
<p>c)</p> <p><i>To select themes that reflect resonant and enduring ideas from the past and connect with values, interest and the kinds of experiences that has meaning and relevance to today's diverse communities.</i></p>	CK Statham	<p><b>Complies</b></p> <p>The Doran Drive Public Art Strategy lists (on Page 13) curatorial themes that are grouped under the overarching curatorial concept of 'Water Connects Us'. These themes support the curatorial concept, and reflect common experiences and concerns of the diverse communities who will work and reside in the vicinity of Doran Drive Plaza.</p>
<p>d)</p> <p><i>To engage diverse communities and artists in a shared creative dialogue through consultation with local experts including The Hills Shire Council, state records and representatives of the Aboriginal community to ensure public art and interpretation are a true expression of the lived history of the area.</i></p>	CK Statham	<p><b>Complies</b></p> <p>The Doran Drive Public Art Strategy was developed in collaboration with numerous local community groups and individual subject experts, and proposes ongoing community engagement through programming and events (Page 15).</p>
<p>e)</p> <p><i>To ensure public art:</i></p> <ul style="list-style-type: none"> <li>- <i>exemplifies artistic excellence and integrity, and is driven by curatorial merit;</i></li> <li>- <i>contributes to cultural identity and creates a distinctive sense of place for each precinct – both past and present;</i></li> <li>- <i>helps build stronger, more connected communities;</i></li> <li>- <i>can be enjoyed by people of varied ages, backgrounds and abilities;</i></li> <li>- <i>relates well to the built and natural environment, and is genuinely integrated into new development;</i></li> <li>- <i>is appropriate and safe in public contexts, and is durable and easily maintained;</i></li> <li>- <i>responds to the challenge of climate change through sustainable design and fabrication.</i></li> </ul>	CK Statham	<p><b>Complies</b></p> <p>The Doran Drive Public Art Strategy embodies all of these aims and objectives, seeking to embed public art at the heart of the precinct, for the benefit of humans and animals. Sustainable and water-sensitive design is a core aspect of the proposed public artwork (Page 14).</p> <p>Refer to discussion above and to the Public Art Strategy at Appendix 19 for further detail.</p>



## 2.13 Public Art and Interpretation

Controls	Team	Comment
<p><b>1.</b></p> <p><i>Deliver on the recommendations of Landcom's Hills Showground Station Precinct Heritage Interpretation Strategy (GML, 2019) and SMNWP Public Art Guidelines through the design and implementation of public art and interpretation within the public domain.</i></p>	CK Statham	<p><b>Complies</b></p> <p>The recommendations of Landcom's Hills Showground Station Precinct Heritage Interpretation Strategy (GML, 2019) and the SMNWP Public Art Guidelines provided direct inspiration in the development of the curatorial themes of the Doran Drive Public Art Strategy, and the artwork proposed by the Public Art Strategy clearly meets the aims and objectives of those documents. The details of how the Public Art Strategy was developed from the documents are detailed on Page 10 (relating to the Hills Showground Station Precinct Heritage Interpretation Strategy) and Page 9 (relating to the SMNWP Public Art Guidelines).</p> <p>The proposed Public Art Strategy has been developed in accordance with the Hills Showground Station Precinct – Heritage Interpretation Strategy (GML Heritage) and the Sydney Metro Northwest Places Public Art Guidelines. Both documents are addressed in the Public Art Strategy (Appendix 19)</p>
<p><b>2.</b></p> <p><i>Develop an Implementation Plan including:</i></p> <p><i>a. the engagement of a Public Art Curator and evaluation panel to determine the suitable public artwork type for each opportunity;</i></p> <p><i>b. the selection of a preferred procurement strategy;</i></p> <p><i>c. the undertaking of stakeholder consultation.</i></p>	CK Statham	<p><b>Complies</b></p> <p>A public art curator was engaged to develop the Doran Drive Public Art Strategy. The Public Art Strategy also contains recommendations for ongoing programming and community consultation.</p>
<p><b>3.</b></p> <p><i>Where indigenous artworks are to be included, undertake appropriate and meaningful consultation and collaboration for the planning and production of Public Art and Interpretation.</i></p> <p><i>a. Landcom's Draft Reconciliation Action Plan (RAP), which is expected to be completed late 2020 will include the development of the appropriate consultation required with Indigenous communities.</i></p> <p><i>b. In lieu of the completion of the Landcom RAP (expected in late 2020), the SMNWP Public Art Guidelines makes reference to the Arts Council of Australia "Protocols for Producing Indigenous Australian Visual Arts Guide." This Guide provides an appropriate process of consultation for engaging on and producing Aboriginal Art and is encouraged to be used during consultation for Indigenous public art and interpretation in the Doran Drive Plaza, Precinct East Park and the pedestrian link.</i></p> <p><i>c. The SMNWP Public Art Guidelines also outline the processes for genuine collaboration and consultation with other stakeholders including community groups and local council to assist in building on and delivering public art and interpretation. The steps provided in the SMNWP Public Art Guidelines are to be used for planning and delivery purposes.</i></p>	Deicorp	<p><b>A</b> A copy of Landcom's Draft Aboriginal Reconciliation Action Plan is not available.</p> <p><b>b.</b> In lieu of the Landcom's Draft Aboriginal Reconciliation Action Plan, the Connecting with Country Draft Framework created by the Government Architects of NSW has been adopted. An Aboriginal Artist has been engaged to create integrated Aboriginal artworks into the project. As Indigenous artists with cultural heritage connections to the traditional land of the Darug people, the artists will elevate and create awareness of stories of the First Nation People, and to display these stories in a way that promotes reconciliation and awareness to Connecting With Country.</p> <p><b>c. Complies</b></p> <p>The SMNWP Public Art Guidelines also outline the processes for genuine collaboration and consultation with other stakeholders including community groups and local council to assist in building on and delivering public art and interpretation. The steps provided in the SMNWP Public Art Guidelines are to be used for planning and delivery purposes.</p>

## 2.13 Public Art and Interpretation

Controls	Team	Comment
<p><b>4.</b></p> <p><i>Include and select themes and stories that celebrate and present the local character of the area. The key thematic framework for public art in SMNWP lies in the idea of a network represented by the connections we have to things that we value throughout our lives, including the relationships we develop with people, places and the environment, and the mutual effects we have on each other. The network consists of five key themes.</i></p> <ul style="list-style-type: none"> <li>- <i>Human and Environment: art with a focus on sustainability and our relationship to the environment.</i></li> <li>- <i>Art and Community: art that is participatory and socially engaged and fosters connectivity.</i></li> <li>- <i>Here and Far: art that transports us literally or through the imagination.</i></li> <li>- <i>Past and Present: art that connects us to stories specific to the site over time.</i></li> <li>- <i>Existing and New: art that celebrates renewed sites and development.</i></li> </ul> <p><i>The Hills Showground Station Precinct has further developed this with four sub-themes specifically related to the Aboriginal and European heritage interpretation for the precinct including:</i></p> <ul style="list-style-type: none"> <li>- <i>Theme 1 - Aboriginal Cultural Heritage</i></li> <li>- <i>Theme 2 - Resistance and Rebellion</i></li> <li>- <i>Theme 3 - Agriculture and Orchardng</i></li> <li>- <i>Theme 4 - Pride in the Hills</i></li> </ul>	<p><b>CK Statham / Eastwood</b></p>	<p><b>Complies</b></p> <p>See page 9 of the Doran Drive Public Art Strategy:</p> <p>The Sydney Metro Northwest Place Public Art Guidelines describe a thematic network for public art in the SMNWP - a network that expresses the connections we have to the things that we value in our lives, including the relationships we develop with people, places and the environment, and the mutual effects we have on each other. This concept of a network, and in particular its key themes Human and Environment, Art and Community, and Existing and New, have inspired the evolution of the curatorial themes for this Strategic document.</p> <p>See Page 10 of the Doran Drive Public Art Strategy, namely:</p> <p>The two themes that have been incorporated into the curatorial approach for this Public Art Strategy are Agriculture and Orchardng, and Pride in the Hills. These two themes touch on, respectively, the site's history in growing produce and connections with nature, and the prevalence of social gatherings and strength of community participation in the area.</p> <p>Cattai Creek has a deep historical significance and cultural values to the Bedjigal (Darug People). The creek is a location where many Aboriginal artefacts have been recorded. The creek would have been key to every day Bedjigal life providing fresh water, food, natural resources and used as a camping site and ceremonial spiritual place. Refer to Danny Eastwood's Connecting with Country Public Art Strategy.</p>
<p><b>5.</b></p> <p><i>The following suggested devices provide interpretive solutions that best fits within the scale of opportunity sites for public art and complements the surrounding built form. Device 1 - Surface inlays</i></p> <p><i>Device 2 - Lighting</i></p> <p><i>Device 3 - Public Art / Murals</i></p> <p><i>Device 4 - Branding and Naming</i></p> <p><i>Permanent artworks must be durable and weatherproof, fabricated to a high quality standard and seamlessly integrate into the landscape plan.</i></p>	<p><b>CK Statham</b></p>	<p><b>Complies</b></p> <p>The Doran Drive Public Art Strategy proposes a public artwork (Device 3) that complements the surrounding built form of Doran Drive and the Hills Showground Station Precinct. The proposed artwork would be designed to a very high standard, by an experienced artist and team, and has been developed in close collaboration with the project team landscape designers, to ensure seamless integration into the form and function of the Plaza.</p>

## 2.13 Public Art and Interpretation

Controls	Team	Comment
<p><b>6.</b></p> <p><i>The public art and interpretation in Doran Drive Plaza may integrate an installation of an interpretive water feature within the plaza that reflects the site's connection to Cattai Creek, and a deciduous tree grove referencing the area's previous use for orchards, along with the devices listed above. For further information on Doran Drive Plaza, refer to the Doran Drive Precinct Guidelines.</i></p>	CK Statham	<p><b>Complies</b></p> <p>The Doran Drive Public Art Strategy proposes a public artwork that is in the form of a public fountain. The artwork reflects and celebrates the nearby Cattai Creek and the green corridors of the area. The Strategy has been developed in close collaboration with project team landscape designers, and includes recommendations for companion plantings and species selection, in collaboration with Urbis and Danny Eastwood.</p>
<p><b>7.</b></p> <p><i>Public art is to be included within the Precinct East Park and is to include integrated public domain elements. This may be in the form of surface inlays, sculptural interactive play elements or other integrated elements that meet the objectives. For further information on the Precinct East Park refer to the Precinct East Guidelines.</i></p>		N/A
<p><b>8.</b></p> <p><i>Public art and interpretation is to be included within the Precinct East publicly accessible pedestrian link. For further information on the Precinct East Park refer to the Precinct East Guidelines.</i></p>	CK Statham	N/A
<p><b>9.</b></p> <p><i>Procure a Public Art Curator to manage the commission of public artists and designers, to make use of local knowledge, experience and understanding, as well as strengthening the local arts industry.</i></p>	CK Statham	<p><b>Complies</b></p> <p>A public art curator was engaged to develop the Doran Drive Public Art Strategy, and the curator has a strong interest in remaining involved with the project in forthcoming stages, including commissioning, design development, project management and programming, and developing further connections with local community groups, subject experts, artists and designers.</p>
<p><b>10.</b></p> <p><i>Provide a site specific public art and interpretation plan that adheres to the 'Implementation Plan' as outlined in the SMNWP Public Art Guidelines, and outlines how the objectives and controls in this section are or can be met.</i></p>	CK Statham/ Eastwood	<p><b>Complies</b></p> <p>The Doran Drive Public Art Strategy proposes a site-specific public artwork and methodology (Pages 3 and 22).</p> <p>Refer to Danny Eastwood's Connecting with Country Public Art Strategy, including objectives around:</p> <ul style="list-style-type: none"> <li>- Acknowledgment of Country</li> <li>- Waterways</li> <li>- Bedjigal (Darug) Language</li> <li>- Native Birds (Binyang) Bird Dreaming</li> </ul>

## 2.14 Integrated Water Management

Objectives	Team	Comment
a) <i>To carry out the recommendations of the IWCMS through the sustainable use and implementation of water treatment measures.</i>	AECOM	Complies
b) <i>To ensure the quality and integrity of urban waterways is maintained and enhanced through both construction and occupation phases of the development at an individual lot, overall development and regional scale; to minimise impact in erosion or pollution to receiving waterways.</i>	AECOM	Complies
c) <i>To utilise stormwater runoff to its fullest for non-potable purposes.</i>	AECOM	Complies
d) <i>To minimise risk to life and property from minor or major flooding to an acceptable level.</i>	ACE	Complies
e) <i>To protect and enhance the integrity and functionality of ecology and significant riparian corridors.</i>	Ecological Australia	Complies
Controls	Team	Comment
1. <i>Any water treatment measures provided are to be located within the development lot with provided access for inspection and maintenance. Water treatment measures are to consider gross pollutant traps and stormwater filtration devices at surface inlet pits and grated drains; linear bioretention in garden and tree beds; and along pedestrian and vehicle pathways are to provide for additional treatment of runoff from Precinct East.</i>	AECOM	Complies See Section 5.3 of report, Drawings CI-0621 and CI-0951 for proposed WSUD chamber with incorporated access for inspection and maintenance. Treatment measures include stormwater filtration devices. Litter baskets in pits have also been nominated where suitable.
2. <i>Erosion and sediment control is to be provided to all points where stormwater runoff can enter stormwater systems or where runoff may leave the construction site, and have a documented maintenance plan.</i>	AECOM	Complies For erosion and sediment control plan refer to CI-0031.
3. <i>A WSUD Management Plan is to be prepared that considers all placement and sizing of rainwater tanks to meet/improve rainwater capture, and reuse application and demand for other array of purposes such as: outdoor use, laundries and toilets. In addition to capture and re-use stormwater from roof areas as irrigation for planting; selected plant species are to withstand the local climate and require a low amount of watering.</i>	AECOM	Complies WSUD Management Plan considers rainwater re-use based on irrigation rates of landscape/turf and rainwater tank volume provided. Selected plant species by the landscape architect.

## 2.14 Integrated Water Management

Controls	Team	Comment
<p><b>4.</b></p> <p><i>All floor levels are to be above the 1% AEP level + 0.5m freeboard (i.e. above 83.6 mAHD at the intersection of De Clambe Drive and Carrington Road, and above 78.7mAHD at the detention basin). All garages/ carpark entrances are to be protected from inundation by flood waters up to the 1% AEP + 0.5m.</i></p>	AECOM	<p><b>Complies</b></p> <p>Doran Drive Precinct Lot is not flagged as at risk to flooding. Notwithstanding, the levels at the garages/carpark entrances are above 83.6 m AHD.</p> <p>See Flood report for more detail.</p>
<p><b>5.</b></p> <p><i>For development adjoining the Cattai Creek riparian corridor an interface adopted riparian width of 30m from the 'top of bank' is to be considered each side of the creek. A minimum 7.5m built form setback is required to be provided to the riparian corridor. Underground carparks are not permitted within 5m of the riparian corridor boundary for Precinct West.</i></p>		N/A
<p><b>6.</b></p> <p><i>Consideration is to be given through the detailed design process to the reuse of greywater or blackwater.</i></p>	AECOM	Noted. Not Applicable
<p><b>7.</b></p> <p><i>Where used, vegetated bioretention measures are to be integrated and coordinated with the landscape design to be aesthetically pleasing and contribute to the visual amenity.</i></p>		N/A
<p><b>8.</b></p> <p><i>Any water feature integrated into the design of Doran Drive Plaza is to be low water use.</i></p>	CK Statham	<p><b>Complies</b></p> <p>Refer to the separate Public Art Strategy that accompanies this report.</p>
<p><b>9.</b></p> <p><i>All stormwater drainage designs are to comply with the most up to date revision of Council's Design Guidelines Subdivision/Developments.</i></p>	AECOM	<p><b>Complies</b></p> <p>Stormwater drainage design for the subject lot in accordance with Hills Shire Council Design Guidelines. See Stormwater Management Plan Report Section 4.0 Stormwater Management Plan and Appendix A, B and C for Correspondence with Hills Council and DRAINS Model.</p>
<p><b>10.</b></p> <p><i>All developments are to implement an Erosion and Sediment Control Plan, prepared in accordance with 'Managing Urban Stormwater – Soils and Construction, to minimise land disturbance and erosion and control sediment pollution of waterways.</i></p>	AECOM	<p><b>Complies</b></p> <p>For erosion and sediment control plan refer to CI-0031.</p>
<p><b>11.</b></p> <p><i>All developments within the Precinct are required to manage the pollutant loads from each separate allotment as per the IWCMS prior to discharge to any adjoining drainage system.</i></p>	AECOM	<p><b>Complies</b></p> <p>Development allotment manages its own pollutant loads prior to discharging to the adjoining drainage system. See Section 5.0 Water Sensitive Urban Design Strategy of the Stormwater Management Report.</p>

## 2.14 Integrated Water Management

Controls	Team	Comment
<p><b>12.</b></p> <p><i>Water quality modelling undertaken to support development proposals within the Precinct shall utilise the latest version of MUSIC and be in line with the Draft NSW MUSIC Modelling Guidelines, Sydney Metropolitan Catchment Management Authority, 2010, utilising the modelling parameters in Tables 4 and 5.</i></p>	AECOM	<p><b>Complies</b></p> <p>Refer to Water Quality modelling has been undertaken using the latest version of MUSIC and in line with Draft NSW MUSIC Modelling Guidelines using the modelling parameters. See Section 5.4 MUSIC Modelling of the Stormwater Management Report.</p>
<p><b>13.</b></p> <p><i>For developments generating oils and grease, the additional objective of no visible oils for flows up to 50% of the one-year Average Recurrence Interval peak flow shall be achieved.</i></p>	AECOM	<p><b>Complies</b></p> <p>Refer to Development allotment driveway areas generally covered by roof area (not exposed) and not considered to generate oils and grease.</p>
<p><b>14.</b></p> <p><i>Water Sensitive Urban Design elements are to be designed and constructed in accordance with the following publications:</i></p> <p><i>a. Adoption Guidelines for Stormwater Biofiltration Systems – Cities as Water Supply Catchments, Sustainable Technologies (CRC for Water Sensitive Cities, 2015 or later)</i></p> <p><i>b. Australian Runoff Quality (Engineers Australia 2005)</i></p> <p><i>c. Water Sensitive Urban Design Technical Guidelines for Western Sydney (NSW Government Stormwater Trust and Upper Parramatta River Catchment Trust, May 2004).</i></p>	AECOM	<p><b>Complies</b></p> <p>Refer to See Stormwater Management Plan Report Section 5.0 and MUSIC model.</p>
<p><b>15.</b></p> <p><i>Rainwater tanks are to be provided with potable water trickle top-up with a back flow prevention device, complying with Sydney Water requirements.</i></p>	AECOM	<p><b>Complies</b></p> <p>Refer to See drawing CI-0952 for details of rainwater tank.</p>
<p><b>16.</b></p> <p><i>In accordance with the recommendations made in the publication “Guidance on the Use of Rainwater Tanks” (enHealth, Commonwealth Government 2004), diversion of the “first flush” of up to 180 litres is to be incorporated into the design of the rainwater tank and associated plumbing based on a minimum first flush of 1L/m<sup>2</sup> of roof area.</i></p>	AECOM	<p><b>Complies</b></p> <p>Refer to See drawing CI-0952 for details of rainwater tank.</p>



2.14 Integrated Water Management

Controls	Team	Comment
<p>17.</p> <p><i>The natural form, characteristics and function of waterways, including riparian land, are to be retained, restored, protected and enhanced wherever possible.</i></p>	AECOM	<p>Complies</p> <p>Refer to See Erosion and Sediment Control Plan - refer to CI-0031.</p>
<p>18.</p> <p><i>Waterway rehabilitation and construction works are to apply 'Best Practice' combination of soft and hard engineering techniques establishing a water sensitive, geomorphically stable, diverse and functional waterway corridor that addresses urban influences and considers the immediate waterway corridor and aquatic systems both upstream and downstream of a subject site.</i></p>	AECOM	<p>Complies</p> <p>Refer to See Erosion and Sediment Control Plan - refer to CI-0031.</p>

THE VISION

A CONTEXTUAL DESIGN  
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VIBRANT & FORWARD THINKING  
An active inviting heart for the emerging community



GREEN & SUSTAINABLE  
Extensively landscaped, 5 Star Green Star As-Built





## 2.15 Sustainability

Objectives	Team	Comment
<p>a)</p> <p><i>To minimise energy and water consumption during operation and occupation of the Precinct.</i></p>	ARUP	<p><b>Complies</b></p> <p>Refer to the BASIX, NABERS and Metering Section Page 16 of the ESD Report.</p>
<p>b)</p> <p><i>To reduce waste generation and increase treatment.</i></p>	ARUP	<p><b>Complies</b></p> <p>Refer to the Materials and Waste Section Page 17 of the ESD Report.</p>
<p>c)</p> <p><i>To provide resilience to climate change projections.</i></p>	ARUP	<p><b>Complies</b></p> <p>Full points achieved in adaptation and resilience, refer to Pg 14 of the ESD Report.</p>
<p>d)</p> <p><i>To reduce dependence on private transport.</i></p>	Turner	<p><b>Complies</b></p> <p>This area has traditionally been a car dependent community with poor public transport links, and limited retail and services offering. Residents previously had no choice but to drive to work, retail and for dining out. This new proposal is part of the transformation of the area which is evolving into a transport orientated community, centred around the new train station. The new North West train line and improved bus connections now link this area to the wider Sydney region. The mixed-use development within the Doran Drive Precinct will bring a supermarket, speciality retail, commercial, childcare, medical centre, a dining precinct, and a large community facility, all immediately beside the new train station. The public domain and Doran Plaza have been envisaged as a pedestrian centric environment, where quality landscaping will make this an enjoyable place to live and to visit. Bicycle parking is located within the public domain, and in secure lock-up areas in the basement. End of trip facilities offer best practice facilities for cyclists, and users of other modes of transport.</p>
<p>e)</p> <p><i>To increase biodiversity.</i></p>	ARUP	<p><b>Complies</b></p> <p>Refer to Page 21 Ecological Value in the ESD Report</p>
<p>f)</p> <p><i>To reduce urban heat and improve amenity and comfort via natural and passive means wherever possible.</i></p>	ARUP	<p><b>Complies</b></p> <p>Refer to Page 20 Urban Heat in the ESD Report</p>
<p>g)</p> <p><i>To meet the mandatory requirements outlined in Appendix K of the SSDA (SSD-9653) during detailed design, construction and operation, as they are carried out.</i></p>	ARUP	<p><b>Complies</b></p> <p>Refer to Table 5 and Appendix 7 in the ESD Report</p>
<p>h)</p> <p><i>To continually improve the opportunities in advancing design or construction measures by achieving the stretch goals.</i></p>	ARUP	<p><b>Complies</b></p> <p>Reflected under 'stretch goals' in Appendix 7 of the ESD Report</p>

## 2.15 Sustainability

Controls	Team	Comment
<p>1.</p> <p><i>Achieve minimum 5 star Green Star 'Design and As-Built' with full points in 'Adaptation and Resilience' and 'Heat Island Effect' credits.</i></p>	ARUP	<p><b>Complies</b></p> <p>Project Registered with the GBCA No. 5674DA - Doran Drive, Castle Hill</p> <p>Appendix 7 reflects 5-star commitment, of which the two credits will be achieved;</p> <ul style="list-style-type: none"> <li>- Full Green Star points for the 'Adaptation and Resilience' credit (Appendix 7)</li> <li>- 75% of the development plan area to be covered with green canopy, vegetation or landscaping/building elements that reduce the impact of heat island effect (Appendix 7) which meets the credit requirement</li> </ul>
<p>2.</p> <p><i>Achieve 4.5 star NABERS Energy rating (non-residential).</i></p>	ARUP	<p><b>Complies</b></p> <p>Complies, Refer to Appendix 7 of the report. Advising that project will achieve a 5 star rating</p>
<p>3.</p> <p><i>Achieve 5 star NatHERS Energy rating (residential).</i></p>	ARUP	<p><b>Complies</b></p> <p>Complies, Refer to Appendix 7 of the ESD report.</p>
<p>4.</p> <p><i>Achieve minimum BASIX ratings 25-35 Energy rating and 40 Water rating.</i></p>	ARUP	<p><b>Complies</b></p> <p>Complies, Refer to Appendix 7 states this will be achieved, evidence is in the ESD/BASIX report.</p>
<p>5.</p> <p><i>Provide separate metering energy across commercial and multi-unit tenancies.</i></p>	ARUP	<p><b>Complies</b></p> <p>Refer to Appendix 7 of the ESD report.</p>

## 2.15 Sustainability

Controls	Team	Comment
<b>6.</b> <i>Provide tree canopy cover in accordance with the controls outlined in Section 2.16.3. Tree Canopy Cover of the UDG.</i>	ARUP	<b>Complies</b> Refer to Table 5 – page 18 in the ESD Report with calculations stating that that 75% (calculated at ~81%) of the development plan area will be covered with green canopy, vegetation or landscaping/building elements that reduce the impact of heat island effect.
<b>7.</b> <i>Ensure the project does not engage in modern slavery practices.</i>	ARUP	<b>Complies</b> Refer to Table 5 – page 14 of the ESD Report.
<b>8.</b> <i>A minimum of 10% of total parking spaces are to have Electric Vehicle charging stations.</i>	ARUP	<b>Complies</b> The required number of Electric Vehicle charging stations have been located inside the basement car park. Refer to the Traffic Report and ESD Reports that forms part of this submission.
<b>9.</b> <i>Canopy trees are to be planted within street verges and medians to provide shade and reduce pavement surface temperatures. Understorey planting and permeable surfaces should also be provided where possible to reduce the extent of paved areas and to enhance the amenity of the streetscape environment.</i>	Urbis	<b>Complies</b> Refer to Landscape package prepared by Urbis for further detail.
<b>10.</b> <i>As a minimum, external landscape in or on the building (such as rooftop gardens, green walls, green or brown roofs) must be provided at a ratio of 15% of the development lot area. Vertical or horizontal landscapes are acceptable. Indigenous planting is to be used; this should be suitable local endemic species, as far as possible.</i>	ARUP	<b>Complies</b> Refer to Table 5 page 19 of the ESD Report
<b>11.</b> <i>Building designs are to:</i> <i>a. Maximise the use of natural light and cross ventilation;</i> <i>b. Reduce the reliance on mechanical heating and cooling through the use of eaves, awnings, good insulation and landscaping;</i> <i>c. Include energy efficient light fittings and water fittings;</i> <i>d. Allow for separate metering of water and energy usage for commercial and multi-unit tenancies.</i>	Turner/ ARUP	<b>Complies</b> The building design provides a 60.3% of apartments with natural cross ventilation and a 72.8% of apartments receiving a minimum 2 hours solar access between 9am and 3pm at the winter solstice.  The different façade treatments have been carefully designed to respond to the efficiency and reduce the reliance on mechanical heating and cooling, with the integration of projections in the façade, vertical elements to control the shading, colour palate and orientation.  Lighting and water fittings will be selected in the design development stage before construction certificate, and they will achieve the requested efficiency rating.
<b>12.</b> <i>Implement the key elements of the Integrated Water Cycle Management (IWCM) Strategy, including:</i> <i>a. Inclusion of rain water tanks</i> <i>b. Improvement of water quality treatment devices such as gross pollutant traps and filtration devices as well as linear bioretention in garden beds</i> <i>c. Selection of energy efficient equipment and fixtures.</i>	Arup	<b>Complies</b> Refer to Table 5 page 16. of the ESD report

## 2.16 Amenity

### 2.16.1 Solar Access

Objectives	Team	Comment
<p>a)</p> <p><i>To ensure solar access to public open spaces achieves a high level of amenity year round for those visiting, residing and working in the Precinct.</i></p>	Turner	<p><b>Complies</b></p> <p>The proposal complies with the building envelopes, maximum building heights, and required building separation of the Urban Design Guidelines, and therefore its effect on the adjacent public open spaces is equal to, or better than, the approved building massing of the original concept plan as prepared by Cox/Oculus.</p>
<p>b)</p> <p><i>To ensure that overshadowing from new development does not result in significant loss of sunlight and diminish the enjoyment of public and private open spaces.</i></p>	Turner	<p><b>Complies</b></p> <p>The proposal complies with the building envelopes, maximum building heights, and required building separation of the Urban Design Guidelines, and therefore its effect on the adjacent public open spaces is equal to, or better than, the approved building massing of the original concept plan as prepared by Cox/Oculus.</p>
Controls	Team	Comment
<p>1.</p> <p><i>Figure 62 prescribes the minimum proportions of the public spaces that are to achieve a minimum of 2 hours of sunlight between 9am and 3pm at the winter solstice (21 June). The minimum proportions are:</i></p> <p>a. <i>Doran Drive Plaza - 100% (excluding areas under awnings)</i></p> <p>b. <i>Station Plaza - 65%</i></p> <p>c. <i>Precinct East Park - 70%</i></p> <p>d. <i>Station Forecourt - 80%.</i></p>	Turner	<p><b>Complies</b></p> <p>The proposal complies with the building envelopes, maximum building heights, and required building separation of the Urban Design Guidelines and therefore its effect on the adjacent public open spaces is equal to, or better than, the approved building massing of the original concept plan as prepared by Cox/Oculus.</p> <p>Due to the shape and orientation of Doran Plaza, 100% of the space (excluding areas under awnings) receives minimum 2 hours solar access between 9am and 3pm at the winter solstice. This allows for a quality amenity and all day use of this important public space, which is located within our site boundary.</p>

## 2.16 Amenity

### 2.16.2 Wind and Weather Protection

Objectives	Team	Comment
<p>a)</p> <p><i>To allow for cooling summer breezes to move through the Precinct.</i></p>	Turner	<p><b>Complies</b></p> <p>The proposed building massing consists of 4 towers that anchor the corners of the site, with a large central podium of over 1400sqm. The podium is fully open to the north and south, allowing quality sunlight and cooling breezes into the heart of the development. This is further boosted by the gaps between the towers on the eastern and western sides.</p>
<p>b)</p> <p><i>To ensure pedestrian comfort in streets, and public and private open spaces.</i></p>	Turner	<p><b>Complies</b></p> <p>The proposal allows for continuous awnings, indented entries, or covered colonnades to all active uses along the north, south and western sides of the podium. A continuous line of street trees around the perimeter of the site further complements the shading and protection of the streetscape. Doran Plaza will have minimum 50% tree canopy coverage in line with best practice urban design.</p>
Controls		
<p>1.</p> <p><i>Secondary setbacks are to comply with the controls contained in Sections 3.2.4, 4.2.5 and 5.2.7 to mitigate wind down draft.</i></p>	Turner	<p><b>Complies</b></p> <p>Section 3.2.4 relates to the western precinct only and is therefore not applicable to the subject site or this proposal.</p> <p>Section 3.2.4 relates to the Doran Drive precinct only and is therefore applicable to the subject site or this proposal. A zero ground floor primary setback is provided, except where a 3m primary setback is required for outdoor dining zones. A minimum 3m setback is then provided above the line of the podium base. The façades are highly articulated with further stepping and indents, in line with the precinct's desired future character.</p> <p>Section 5.2.7 relates to the eastern precinct only and is therefore not applicable to the subject site or this proposal.</p>
<p>2.</p> <p><i>Wind and weather protection is to be provided at major entry points, active interfaces and dwelling/gathering spaces within the development lots and public domain.</i></p> <p>a. <i>All non-residential interfaces are to have a 2.5m awning and/or operable screening to protect pedestrians from the elements. This applies in all conditions, including where there may be a 3m colonnade inset – refer to Section 4.2.10 Active Use and Street Frontage Street of the Doran Drive Precinct Guidelines.</i></p> <p>b. <i>All building entry points are to have a 2.5m awning for the width of the entry.</i></p> <p>c. <i>All non-residential interfaces to communal open space on a podium are to have a 2.5m awning and/or operable screening for the length of that interface.</i></p> <p>d. <i>Localised screening is to be provided where longer duration activities are expected.</i></p> <p>e. <i>Wind screens or planting are to be provided within publicly accessible through-site links and at corners of buildings.</i></p> <p>f. <i>Awnings are to be designed to complement and integrate with the facade and the streetscape.</i></p>	Turner	<p><b>Complies, with qualifications:</b></p> <p>The proposal complies with the requirements of the Urban Design Guidelines, and has carefully integrated either 2.5m awnings, or 4m indented entries and covered colonnades to offer continuous weather protection to active frontages, particularly to the north, south and western elevations.</p> <p>The choice of awning, versus indented entries and covered colonnades, is based on what is spatially the most appropriate at that location in the podium. These elements have been considered in the context of wind, solar and other weather mitigation factors.</p> <p>All non-residential interfaces to communal open space on the podium (refer to the community room, and the childcare tenancy on the eastern side of the podium) have a minimum 2.5m projecting awning or slab to remove any direct relationship with the residential levels above. There is also a 2.5m screen for the length of the direct interface between these non-residential uses and the podium.</p> <p>The main communal podium area, and the communal rooftop terrace have substantial planting and trees to resolve wind issues, which are further complimented by solid screens where required by wind conditions.</p> <p>Awnings have been designed to complement and integrate with the façade and streetscape. The design of the awnings varies in material, colour, and shape to give provide articulation within the street edge. The design for awnings is cognisant of their hierarchy of importance e.g. the residential lobby entry awnings being further embellished to assist with wayfinding.</p>

## 2.16 Amenity

### 2.16.2 Wind and Weather Protection

Controls	Team	Comment
<p>3.</p> <p><i>It is recommended that private courtyards on the ground floor are protected from down draft by an awning and/or operable screening where the existing/future trees in the public and private domain facilitate an awning.</i></p>	Turner	<p><b>Noted</b></p> <p>The ground floor of the podium has non-residential uses only addressing the street edge. These active uses have either awnings, indented entries or colonnades to protect from down drafts and prevailing winds. This is complemented by street trees around the full perimeter of the site.</p> <p>Podium level private terraces are partly indented within the building form, and the remaining terrace space has 1800mm high fences on all sides. These terraces are further complimented by substantial trees and planting adjacent to the terraces which assist with side streaming winds, and visual privacy. This arrangement balances the needs for wind protection and visual privacy, with the need for quality solar access to meet ADG requirements.</p>
<p>4.</p> <p><i>Intertenancy screening, blade walls and recessed balconies are recommended where the communal area or private courtyard is elevated and subject to side-streaming winds.</i></p>	Turner	<p><b>Complies.</b></p> <p>The location of private open space has been considered and is located in appropriate positions on the floorplate. Balconies are generally indented within the building form and therefore protected from side streaming winds. Intertenancy screening and blade walls are provided when balconies are adjoining.</p>
<p>5.</p> <p><i>Buildings 8 or more storeys in height (or over 25 metres) require wind tunnel testing, irrespective of whether they are built to the street frontage or not, which demonstrates the following:</i></p> <p><i>a. In walkways and pedestrian transit areas and streets where pedestrians do not generally stop, sit, stand, window shop and the like, the gust equivalent mean (GEM) should not exceed 7.5 metres per second, with 5% probability of exceedance.</i></p> <p><i>b. In areas where pedestrians are involved in stationary short-exposure activities such as window shopping, standing or sitting (including areas such as bus stops, public open space and private open space, cafes) the gust equivalent mean (GEM) should not exceed 5.5 metres per second, with 5% probability of exceedance.</i></p> <p><i>c. In areas for stationary long-exposure activity, such as outdoor fine dining or outdoor amphitheatres, the gust equivalent mean (GEM) should not exceed 3.5 metres per second, with 5% probability of exceedance.</i></p> <p><i>d. The wind tunnel testing report is to be prepared by a suitably qualified engineer.</i></p>	Windtech	<p><b>Complies</b></p> <p>Refer to the Wind Tunnel Assessment Report. For results refer to section 5.2 of the report with figures of the criteria zones in section 5.3.</p>

## 2.16 Amenity

### 2.16.3 Tree Canopy Cover

Objectives	Team	Comment
<p>a)</p> <p><i>To maximise tree canopy cover in order to increase biodiversity, reduce urban heat and provide shade and amenity.</i></p>	Urbis	<p><b>Complies</b></p> <p>The development aims to create a Green Heart with an emphasis on tree Canopy, natural materials, and sustainability.</p>
Controls	Team	Comment
<p>1.</p> <p><i>In accordance with Figure 65, a minimum of 20% overall tree canopy cover is to be achieved within the Hills Showground Station Precinct (i.e. the combined canopy area of all trees contained within the entire site outlined in red).</i></p>	Urbis	<p><b>Complies</b></p> <ul style="list-style-type: none"> <li>- 50% Tree canopy is provided to the publicly accessible plaza</li> <li>- 45% Tree canopy is provided to the communal spaces</li> </ul>
<p>2.</p> <p><i>In accordance with Figure 65, a minimum of 40% tree canopy cover is to be achieved for the new public domain areas, including Precinct East internal street, pedestrian link and park.</i></p>	Urbis	<p><b>Complies</b></p> <ul style="list-style-type: none"> <li>- 50% Tree canopy is provided to the publicly accessible plaza</li> <li>- 45% Tree canopy is provided to the communal spaces</li> </ul>
<p>3.</p> <p><i>A minimum of 40% tree canopy cover is to be achieved in private open space areas of Precinct East.</i></p>	Urbis	N/A
<p>4.</p> <p><i>Tree planting and canopy cover is to be maximised within all other private open space areas, including building setbacks, communal open space and rooftop/podium gardens, where possible.</i></p>	Urbis	<p><b>Complies</b></p> <ul style="list-style-type: none"> <li>- 50% Tree canopy is provided to the publicly accessible plaza</li> <li>- 45% Tree canopy is provided to the communal spaces</li> </ul>
<p>5.</p> <p><i>Street trees are to be planted at 10m intervals to provide continuous canopy cover at expected mature sizes.</i></p>	Urbis	<p><b>Complies</b></p> <p>Existing street trees are to be retained and protected; new trees are proposed less than 10m intervals</p>
<p>6.</p> <p><i>Adequate soil depth and volumes to support tree growth are to be provided in accordance with SEPP 65 Apartment Design Guide.</i></p>	Urbis	<p><b>Complies</b></p> <p>The proposal is compliant with Council and Apartment Design Guidelines around soil depth and volume to ensure the growth of new trees.</p>



## 2.16 Amenity

### 2.16.4 Air Quality, Noise and Vibration

Objectives	Team	Comment
<p>a) <i>To ensure good amenity for future residents and workers within the development lots through the provision of appropriate interfaces and mitigation of air and noise pollutant impacts.</i></p>	Koikas Acoustics	<p><b>Complies</b> Koikas Acoustic was requested to prepare an acoustical (construction noise and vibration) report for the proposed mixed-use development. A quantitative construction noise impact assessment has been prepared to outlined mitigation measures which has been applied in the design. An air quality management plan has been prepared by EI Australia to provide overall guidance and direction for dust and odour management.</p>
<p>b) <i>To ensure development appropriately responds to noise and vibration impacts.</i></p>	Koikas Acoustics	<p><b>Complies</b> Refer to Construction Noise and Vibration Report</p>
<p>c) <i>To minimise land use conflicts arising from noise impacts between non-residential and residential land uses.</i></p>	Koikas Acoustics	<p><b>Complies</b> Refer to Acoustic Report</p>
<p>d) <i>To ensure that future residential development at the site satisfies the appropriate noise measures.</i></p>	Koikas Acoustics	<p><b>Complies</b> Refer to Acoustic Report</p>
Controls	Team	Comment
<b>Air Quality</b>		
<p>1. <i>The external facade of a residential building is to be located at least 10m from the roadway of Showground Road.</i></p>	Turner	<p><b>Complies</b> The subject site and proposal are located more than 10m from Showground Road.</p>
<p>2. <i>Balconies that directly face Showground Road on the ground floor and first floor of any building should be able to be closed (i.e. a winter garden or sun room). Windows on the balcony side should be openable/closable to allow the occupant to prevent direct air flow from Showground Road as desired.</i></p>		N/A
<b>Noise</b>		
<p>3. <i>Residential accommodation is to be designed to ensure that the following LAeq levels are not exceeded (measured with windows closed):</i></p> <p>a. 35 dB(A) for any bedrooms between 10pm and 7am</p> <p>b. 40 dB(A) for anywhere else in the accommodation (other than a garage, kitchen, bathroom or hallway) at any time.</p>	Koikas Acoustics	<p><b>Complies</b> Refer to Section 5.1.1 of the Acoustic Report</p>
<p>4. <i>Residential accommodation is to be designed to ensure that the following LAeq levels are not exceeded (measured with windows open):</i></p> <p>a. 45 dB(A) (1 hour) for bedrooms between 10pm and 7am</p> <p>b. 55 dB(A) (1 hour) for anywhere else in the accommodation (other than a garage, kitchen, bathroom or hallway) at any time.</p>	Koikas Acoustics	<p><b>Complies</b> Refer to Section 5.1.1 of the Acoustic Report</p>

## 2.16 Amenity

### 2.16.4 Air Quality, Noise and Vibration

Controls	Team	Comment
<b>Noise</b>		
<p><b>5.</b> <i>Site planning, building orientation and interior layout are to be used as tools to lessen noise intrusion as far as possible.</i></p>	Turner	<p><b>Complies</b> The loading Dock area, retail back-of-house, and the main plantrooms are located either in the basement, or along Andalusian Way. Buildings C &amp; D facing Andalusian Way, are set back substantially from the podium base at this location, which mitigates any direct noise or visual issues from the loading dock location.</p>
<p><b>6.</b> <i>Attenuation of noise at the source is preferred. Applicants are to indicate measures undertaken to mitigate the impact of noise upon adjacent residents and/or workers.</i></p>	Koikas Acoustics	<p><b>Complies</b> The Acoustic Report recommendations have been incorporated to design, refer to Section 5.3.</p>
<p><b>7.</b> <i>It is preferable that noise attenuation measures last for a minimum of 10 years or the life of the development proposal, before being upgraded to meet current standards as required.</i></p>	Koikas Acoustics	<p><b>Complies</b> The Acoustic Report recommendations have been incorporated to design, refer to Section 5.2.</p>
<p><b>8.</b> <i>A Noise Impact Assessment prepared by a suitably qualified consultant may be required when submitting a DA for a new development or the renovation of an existing development.</i></p>	Koikas Acoustics	<p><b>Complies</b> An Acoustic Assessment Report has been submitted as a part of the SSDA.</p>
<p><b>9.</b> <i>The provisions of State Environmental Planning Policy (Infrastructure) 2007 and Development near Rail Corridors and Busy Roads Interim Guideline must be taken into consideration to minimise impacts of busy roads and railway corridors on residential and other sensitive development.</i></p>	Koikas Acoustics	<p><b>Complies</b> The Acoustic Report recommendations have been incorporated to design, refer to Section 5.1.1.</p>
<b>General</b>		
<p><b>10.</b> <i>Development shall address all applicable recommendations contained in the Hills showground Station Precinct Noise Impact Assessment. (Renzo Tonin October 2019) Submitted as a part of the SSDA Package (SSD 9653)</i></p>	Koikas Acoustics	<p><b>Complies</b> The SSDA Acoustic Report is a more detailed and recent assessment, as such, supersedes the Renzo Tonin report The Acoustic Report recommendations have been incorporated to design.</p>
<p><b>11.</b> <i>Site Planning, building orientation and interior layout are to be used as tools to lessen noise intrusion as far as possible</i></p>	Turner	<p><b>Complies</b> The loading Dock area, retail back-of-house, and the main plantrooms are located either in the basement, or along Andalusian Way. Buildings C &amp; D facing Andalusian Way, are set back substantially from the podium base at this location, which mitigates any direct noise or visual issues from the loading dock location.</p>
<p><b>12.</b> <i>Attenuation of noise at the source is preferred. Applicants are to indicate measures undertaken to mitigate the impact of noise upon adjacent residents and/or workers</i></p>	Koikas Acoustics	<p><b>Complies</b> The Acoustic Report recommendations have been incorporated to design.</p>

#### 2.16.4 Air Quality, Noise and Vibration

Controls	Team	Comment
<b>General</b>		
<p><b>13.</b>  <i>It is preferable that noise attenuation measures last for a minimum of 10 years or the life of the development proposal, before being upgraded to meet current standards as required.</i></p>	Koikas Acoustics	<p><b>Complies</b>            The Acoustic Report recommendations have been incorporated to design.</p>
<p><b>14.</b>  <i>A Noise Impact Assessment prepared by a suitably qualified consultant may be required when submitting a development application for a new development or the renovation of an existing development.</i></p>	Koikas Acoustics	<p><b>Complies</b>            An Acoustic Report has been submitted with the SSDA.</p>
<p><b>15.</b>            The provisions of State Environmental Planning Policy (Infrastructure) 2007 and Development near Rail Corridors and Busy Roads Interim Guideline must be taken into consideration to minimise impacts of busy roads and railway corridors on residential and other sensitive development.</p>	Koikas Acoustics	<p><b>Complies</b>            An Acoustic Report Has been submitted with the SSDA which incorporates these measures.</p>
<b>Residential Uses</b>		
<p><b>16.</b>            Development for the purposes of residential accommodation shall comply with the following requirements:  <i>a. Appropriate measures shall be taken to ensure that the following internal traffic noise levels are not exceeded – Refer to Table 2 , Urban Design Guidelines, Pg60</i>  <i>b. Appropriate measures shall be taken to ensure that the following internal noise levels from “normal use” of the active street fronts and Castle Hill Showground (i.e. combined patron and music noise) are not exceeded – Refer to Table 3 , Urban Design Guidelines, Pg60</i>  <i>c. Appropriate measures shall be taken to ensure that the following cumulative impacts of simultaneous road traffic noise and patron and music noise (provided they are individually compliant with Controls 7a and 7b) are not exceeded – Refer to Table 4 , Urban Design Guidelines, Pg60</i></p>	Koikas Acoustics	<p><b>Complies</b>            Refer to Section 5.1 of the acoustic report</p>

## 2.16 Amenity

### 2.16.4 Air Quality, Noise and Vibration

Controls	Team	Comment
<b>Non-Residential Uses</b>		
<p><b>17.</b>  <i>Environmental noise intrusion from road traffic noise and operational noise to non-residential uses shall comply with the design internal sound levels of Australian/New Zealand Standard AS/NZS 2107:2016 “Acoustics – design sound levels and reverberation times for building interiors” (AS2107)</i></p>	Koikas Acoustics	<p><b>Complies</b>  Refer to Section 5.1.4 of the acoustic report</p>
<p><b>18.</b>  The use and internal fit-out of non-residential premises may be carried out by future operators and tenants in which case they will be subject to separate approval by the relevant determination authority. THDCP 2012 applies to the assessment of the use of non-residential premises, where applicable.</p>	Turner	<b>Noted</b>
<b>Mechanical Plant and Equipment</b>		
<p><b>19.</b>  External noise emissions from the use of mechanical plant and equipment and vehicles being driven on site shall comply with the requirements of relevant NSW EPA Noise Policy for Industry and any other guideline, policy or standard.</p>	Koikas Acoustics	<p><b>Complies</b>  Refer to Section 6.1.1 of the acoustic report</p>
<p><b>20.</b>  Acoustic assessment of mechanical services equipment is to be undertaken during the detailed design phase of the development to ensure that the cumulative noise of all equipment does not exceed the applicable noise criteria.</p>	Koikas Acoustics	<p><b>Complies</b>  Refer to Section 6.3 of the acoustic report</p>
<p><b>21.</b>  Fans shall be mounted on vibration isolators and balanced in accordance with Australian Standard 2625 ‘Rotating and Reciprocating Machinery –Mechanical Vibration’.</p>	Koikas Acoustics	<p><b>Complies</b>  Refer to Section 6.3 of the acoustic report</p>
<b>Vibration</b>		
<p><b>22.</b>  The provisions of Environmental Protection Authority’s Rail Infrastructure Noise Guideline (EPA, 2013) and Assessing Vibration: a technical guideline (DECC, 2006) must be taken into consideration to ensure acceptable vibration and ground-borne noise limits for spaces within the development.</p>	Koikas Acoustics	<p><b>Complies</b>  Refer to Section 5.1.5 of the acoustic report</p>

# 3. DORAN DRIVE PRECINCT CHARACTER

## 4.1.1 Built Form Character

The Design Team acknowledges the importance of an appropriate scale and built form to the lower levels, and for meaningful activation of the streetscapes. To that end, the design allows for continuous retail tenancies and residential lobbies to the north, south and western elevations.

The base avoids a single big box retail solution and has been designed as series of smaller scale buildings, ranging from two to four storeys, that are varied in character which combine to form the podium. Their palette is made up of a variety of brick colours that offer a robust, high quality and low maintenance finish. Collectively, this grouping of buildings creates a dynamic, fine-grain, and interesting street interface.

The four towers anchor the corners of the development, with the south-east and north-west towers either integrated or set back from the podium base. These diagonally

opposite towers have a strong horizontal emphasis in their façade design and building character.

As part of the desired diversity of architectural language and built form, the north-east and south-west towers are partly brought to ground. This gives a strong street identity, marks the primary entries to the precinct, and emphasises the vertical slender proportions of these two buildings. While both these towers are still partly wrapped by the podium, this varied approach to built form avoids the homogenous outcome of a continuous dominant podium with towers completely dislocated from the street below.



Perspective Image - Andalucian Way - Bespoke brick detailing



### 4.1.2 Material Character

Each of the buildings has an individual building expression that allows for a variance in architectural character within the proposal. With regards to composition, Buildings A and D sit above the podium and have a horizontal emphasis to the façade design. While buildings B & C partly come to ground and champion a vertical façade language.

Building A is predominantly glazed, in order to avail of the outlook and views over the verdant context of the Showgrounds. The vertical elements that frame the composition with natural and soft colours coming from natural species allow for a playful built form. Horizontal breaks define a base, middle and top to Building A.

Building B champions the use of brick with a notorious recess that emphasise the vertical components within the building. The facades are a balance between solid and glazed,

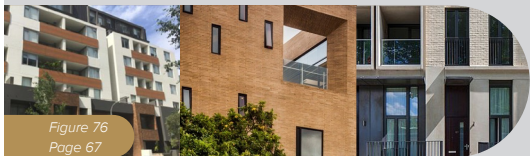
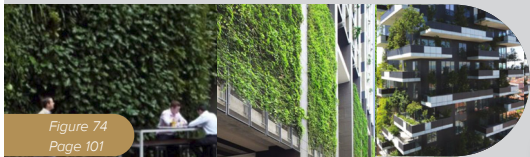
to allow for a sufficient solidity to the tower that is contrasting with the neighbouring buildings. The addition of a neutral dark material to the top of this building crowns it and empowers the variation to avoid the feeling of bulky forms.

Buildings C on the north east corner of the tower has a consistent use of brick to the vertical elements. This emphasises the towers’ vertical proportion which is in contrast to the wider and more horizontal building A adjacent. The addition of a neutral dark material to the top of this building crowns it and empowers the variation to avoid the feeling of bulky forms.

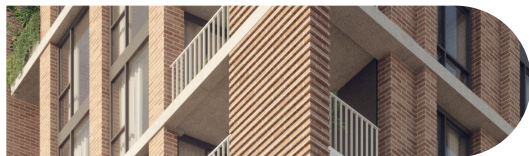
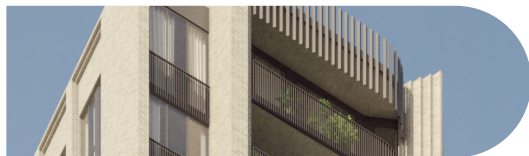
Building D has an expressed dark slab edge that projects out past the façade. The remainder of the façade is a recessive composition in soft colours coming from native species. This façade language provides for a strong horizontal graphic.

While the towers each have their own individual expression, they sit comfortably together as a cohesive and familial group of the buildings, tying together motifs from the existing local context. This includes; patterns of the existing brick vernacular suburban architecture, colours of endemic plant species such as the Gravellea Hills Jubilee and an emphasis on vista and connection to the adjacent Showground.

#### URBAN DESIGN GUIDELINES



#### THE PROPOSAL



Quality in the Facade

Playfulness in Form,  
articulation and/or colour,  
use of curves

Variety in materiality  
and textures

Bespoke detailing

### 4.1.3 Open Space and Landscape Character

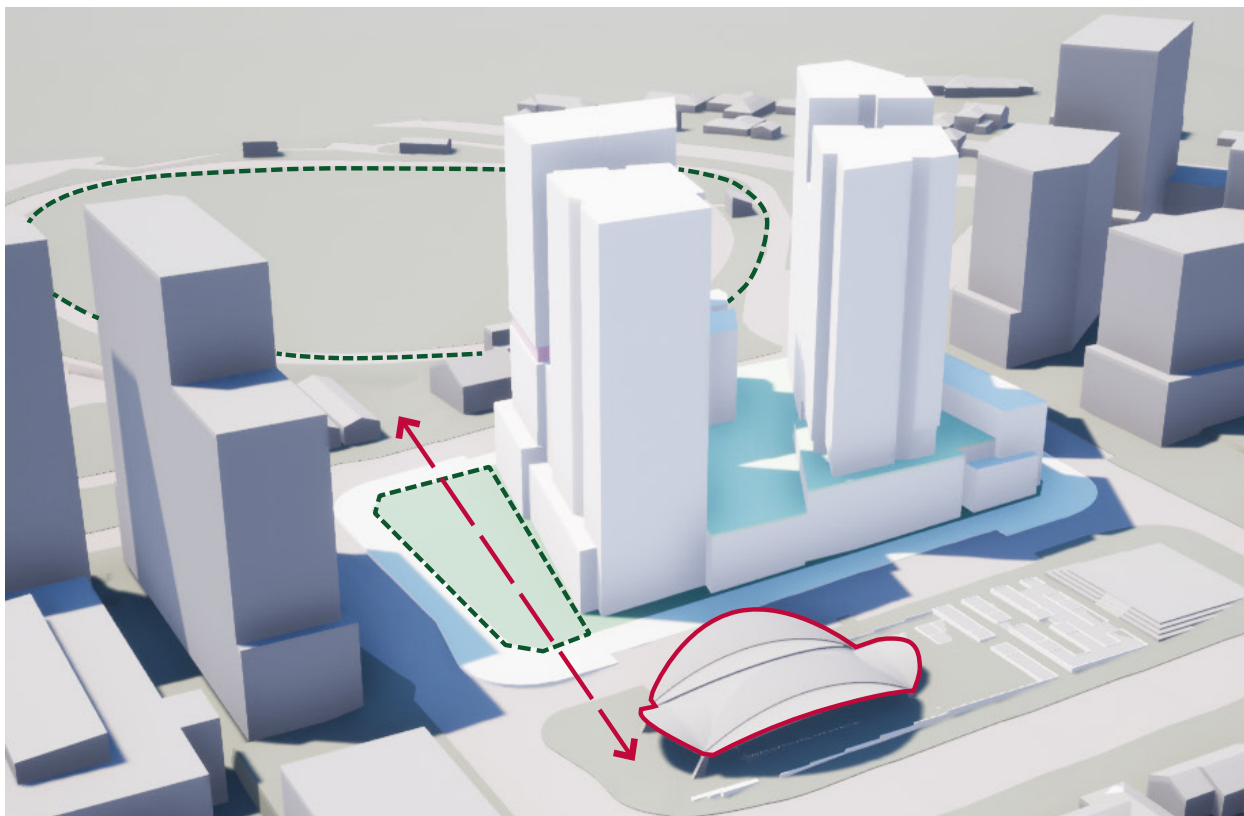
The publically accessible Doran Drive Plaza is structured around a series of interconnected new open spaces as an extension of the broader Green Grid. A diverse network of open spaces make a more connected place.

the open space network. Enhanced amenity is provided due to their location, including improved solar access and district views.

Refer to Landscape Design Report by Urbis for further detail.

A high performing public domain provides amenity and comfort for all users. The public domain strategy utilises and leverages the topography of the site to create a site-specific response that delivers a large publicly accessible park and a activated and permeable streetscape.

Private open space typologies provide increased greenery and amenity, connecting people to nature. Rooftop gardens on buildings increase communal access to open space and provide additional typologies to





## 4. DORAN DRIVE PRECINCT GUIDELINES

### 4.2.1 Doran Drive Plaza

Objective	Comment
<p><b>a.</b></p> <p><i>To provide an active plaza space as part of the community heart of the Precinct.</i></p>	<p><b>Complies</b></p> <p>The proposed publicly accessible Doran Drive Plaza provides a green space to form the active heart of the precinct. Its permeability from De Clambe Drive, Doran Drive and Mandala Parade encourages public activation, further supported by the on-grade active retail frontage facing the plaza. This is reinforced in the landscape design completed by Urbis.</p>
<p><b>b.</b></p> <p><i>To reinforce primary pedestrian connections between the Hills Showground Station and Castle Hill Showground.</i></p>	<p><b>Complies</b></p> <p>A 6m wide pathway is provided adjacent to Doran Drive Plaza, providing clear site lines through the proposed publicly accessible Plaza. This encourages a pedestrian thoroughfare from the Hills Showground Station through to Castle Hill Showground. There is a 4m wide proposed awning adjacent to the plaza to be shared between diners / retail and pedestrian thoroughfare.</p>
<p><b>c.</b></p> <p><i>To maintain clear and generous pedestrian access across all pedestrian desire lines between retail, transport and the Castle Hill Showground.</i></p>	<p><b>Complies</b></p> <p>Pedestrian permeability and connectivity between the metro station and Castle Hill Showground has informed the ground plane and retail layout of the proposed development. As well as a 6m wide pathway adjacent to the publicly accessible Doran Drive Plaza, an internal connection is provided between the retail entrance opposite the metro station at Ground Floor, to a secondary retail entrance provided at Level 01 at DeClambe Drive. This direct connection is on a key desire line for pedestrian movement, and has a series of voids that provide a clear sightline to the opposite side of the site. The connection enjoys substantial natural light from the north and south ends of the mall, as well as from west facing glazing to Doran Drive Plaza. A skylight is introduced at the ceiling of the top floor of the retail to enhance the natural lighting of the space.</p>
<p><b>d.</b></p> <p><i>To provide opportunities for outdoor dining along the building frontage.</i></p>	<p><b>Complies</b></p> <p>All of the proposed retail tenancies along Doran Drive Plaza have a direct and continuous level access to the publicly accessible Doran Drive Plaza, allowing for trade out zones and flexibility in use. This dining precinct will offer day and night activation of the new plaza. The other tenancies along the north and south edges of the site are stepped to follow the steep topography of the surrounding streets. The stepped slabs allow for an access point from these tenancies to the adjacent streets.</p>

#### 4.2.1 Doran Drive Plaza

Control	Comment
<p>1.</p> <p><i>Multiple comfortable seating options and gathering spaces are to be provided to cater to different individual needs and group settings.</i></p>	<p><b>Complies</b></p> <p>Refer to landscape documentation package, as prepared by Urbis Landscape Architects.</p>
<p>2.</p> <p><i>The Plaza is to include an engaging focal point in the form of a water feature, public art, sculptural pavilion or other urban element. Refer to SMNWP Guidelines to guide the decision and design of public art and interpretation features.</i></p>	<p><b>Complies</b></p> <p>Refer to landscape documentation package, as prepared by Urbis Landscape Architects.</p> <p>Refer to Public Art Strategy package prepared by CK Statham.</p>
<p>3.</p> <p><i>A minimum 6m wide pathway is to be provided adjacent Doran Drive.</i></p>	<p><b>Complies</b></p>
<p>4.</p> <p><i>A minimum 3m wide footpath is to be provided along the building frontage for uninterrupted pedestrian movement. A zone of up to 3m may be licensed for outdoor dining purposes, however this must not interrupt the pedestrian movement zone.</i></p>	<p><b>Complies</b></p>
<p>5.</p> <p><i>Adequate soil depths and volumes for trees and adequate soil depths for other types of planting such as shrubs, ground covers and turf are to be provided in accordance with Section 4P of the Apartment Design Guide.</i></p>	<p><b>Complies</b></p> <p>Refer to landscape documentation package, as prepared by Urbis Landscape Architects, in conjunction with soil specialist strategy report prepared by SESL, with 1500mm achieved beneath Doran Plaza.</p>
<p>6.</p> <p><i>The Plaza must be universally accessible and compliant with the Disability Discrimination Act.</i></p>	<p><b>Complies</b></p>
<p>7.</p> <p><i>Variation in paving texture and format is acceptable if it supports the character and layout of the Plaza.</i></p>	<p><b>Complies</b></p> <p>Refer to landscape documentation package, as prepared by Urbis Landscape Architects</p>
<p>8.</p> <p><i>High quality seating and furniture elements are to be integrated into the landscape design.</i></p>	<p><b>Complies</b></p> <p>Refer to landscape documentation package, as prepared by Urbis Landscape Architects</p>
<p>9.</p> <p><i>Public art and interpretation is to be incorporated into the design of Doran Drive Plaza in line with the SMNWP Guidelines and Hills Showground Station Precinct Heritage Interpretation Strategy (GML, 2019).</i></p>	<p><b>Complies with Qualifications</b></p> <p>Public Art Strategy package prepared by CK Statham and Danny Eastwood.</p>
<p>10.</p> <p><i>A combination of native and exotic species suited to the urban character of the Plaza are to be provided. This includes a minimum of 50% native species, and the a grove of deciduous trees reflecting the area's history with orchards.</i></p>	<p><b>Complies</b></p> <p>Refer to landscape documentation package, as prepared by Urbis Landscape Architects.</p>

#### 4.2.2 Communal Open Space

##### Objective

a.

*To provide additional amenity and recreational opportunities within the private domain for the residents of the Precinct.*

##### Comment

##### Complies

The large communal open space located on the podium, provides amenity to the residents within the development in accordance with the ADG. The proposed communal open space allows for generous greenery and amenity, including BBQ areas, play equipment for children, etc.

Additional communal open spaces are provided on the rooftop terraces for all buildings, with enhanced amenity due to their location, including improved solar access and district views.

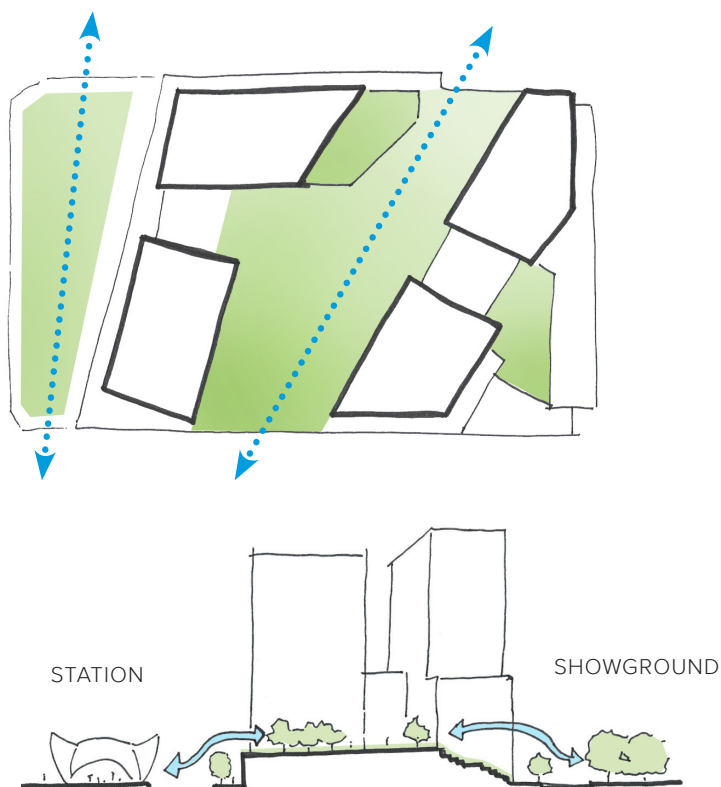
b.

*To locate and configure the communal open space to provide visual connection to and from the station plaza and Showground.*

##### Complies

The configuration of the communal podium landscaping and rooftop terraces provide generous district views, solar access and outlook. This green landscaping is immediately visible from both the station plaza and showgrounds with direct connectivity provided between the proposed podium and De Clambe Drive

The substantial communal podium landscaping, roof terraces and the publicly accessible Doran Drive Plaza collectively create a green network of the spaces that link the Station Plaza with the Showgrounds and Cattai Creek beyond.



**Bulk and Scale - Connectivity of Community Space**

#### 4.2.2 Communal Open Space

Control	Comment
<p>1.</p> <p><i>Communal open space is to form part of the Mandala Parade interface and overlook the public domain.</i></p>	Complies
<p>2.</p> <p><i>External access to the public domain from the communal podium spaces is to be provided where possible, subject to adherence to CPTED principles.</i></p>	Complies
<p>3.</p> <p><i>Communal open space is to be provided in the form of private areas at podium level in accordance with SEPP 65 Apartment Design Guide.</i></p>	Complies
<p>4.</p> <p><i>External (outside) communal open space areas are to be located and designed to:</i></p> <ul style="list-style-type: none"> <li>– <i>Be seen from the street between buildings (where possible)</i></li> <li>– <i>Provide for active and passive recreation needs of all residents</i></li> <li>– <i>Provide landscaping</i></li> <li>– <i>Present as a private area for use by residents only - Include passive surveillance from adjacent internal living areas and/ or pathways</i></li> <li>– <i>Have a northerly aspect (where possible) - Be in addition to any public thoroughfares.</i></li> </ul>	Complies
<p>5.</p> <p><i>Communal open space is to provide a range of uses including seating, picnic facilities, play spaces, productive gardens and lawn areas amongst generous planting.</i></p>	<p>Complies</p> <p>Refer to landscape documentation package, as prepared by Urbis Landscape Architects</p>
<p>6.</p> <p><i>Communal open space is to incorporate a minimum of 70% native planting for local character, however this may be supplemented with exotics for colour and variation, and edible species as part of vegetable or herb gardens.</i></p>	<p>Complies</p> <p>Refer to landscape documentation package, as prepared by Urbis Landscape Architects</p>
<p>7.</p> <p><i>The design of exterior communal open space areas is to achieve amenity by addressing visual and acoustic privacy, safety, security and wind effects.</i></p>	<p>Complies</p> <p>For wind effects refer to wind report prepared by Windtech</p>
<p>8.</p> <p><i>The location and design of communal open space is to achieve a minimum of 50% direct sunlight to the principal usable part of the communal open space for a minimum of 2 hours between 9am and 3pm at the winter solstice (21 June).</i></p>	<p>Complies</p> <p>Refer to DA-730-001, DA-730-101 and DA-730-102</p>

#### 4.2.3 Building Siting, Massing and Scale

Objective	Comment
<p><b>a.</b></p> <p><i>To protect and enhance the rich, distinctive and valued character of the area, particularly those elements that contribute to a sense of place and identity including the Castle Hill Showground and Station Plaza.</i></p>	<p><b>Complies</b></p> <p>The proposed built form and massing responds to and enhances the desired future character of the Hills Showground Station Precinct. The proposal recognises the necessity of an appropriate scale and built form of the podium to ensure the meaningful activation of the streetscapes. The design allows for continuous retail tenancies and residential lobbies to the north, south and western elevations. Combined with the proposed publicly accessible Doran Drive Plaza, this further strengthens the sense of place.</p>
<p><b>b.</b></p> <p><i>To provide building forms that reinforce the desired character of the area.</i></p>	<p><b>Complies</b></p> <p>The proposed massing and built form are compliant with the approved Concept Plan building envelopes. In embracing the desired design principles for the precinct, the overall massing of the site is broken down into a series of discernible components. The four towers anchor the corners of the site and compliment the more fine-grain nature of the active retail podium below.</p>
<p><b>c.</b></p> <p><i>To ensure building orientation maximises visual amenity and natural surveillance, taking advantage of any views to open space, public reserves and bushland.</i></p>	<p><b>Complies</b></p> <p>The proposed building massing and general orientation of the towers maximise resident's amenity achieving solar and cross-ventilation requirements as per the ADG, as well as maximising surrounding district views. The surrounding streets have passive surveillance from apartments overhead, as well as from the mixed-use podium where tenancies and lobby entries are located around the perimeter of the site.</p>
<p><b>d.</b></p> <p><i>To ensure towers are of a slender design to reduce perceived bulk and scale.</i></p>	<p><b>Complies</b></p> <p>The proposed towers have been designed with breaks between the buildings to provide increased amenity to the residents as well as to reduce the overall perceived bulk and scale of the development. Furthermore, differing building characters have been explored for each of the towers, via both materiality and façade design, collectively emphasising a series of slender building elements.</p>
<p><b>e.</b></p> <p><i>To ensure towers create an open, attractive and distinct skyline.</i></p>	<p><b>Complies</b></p> <p>The variation in the towers provide a varied and attractive contribution to the Hills Showground Station Precinct skyline. The articulation and breakup of the overall building massing allows for view lines to sky or to podium landscaping, and a sense of openness across the site.</p>
<p><b>f.</b></p> <p><i>To frame and define the streets and public open spaces with appropriately scaled built edge.</i></p>	<p><b>Complies</b></p> <p>The podium base avoids a single big box retail solution and has been designed as a series of smaller scale buildings, ranging from two to four storeys which vary in character. Their palette is made up of a variety of brick colours that offer a robust, high quality and low maintenance finish. Collectively, this grouping of buildings creates a dynamic, fine-grain, and interesting street interface.</p>
<p><b>g.</b></p> <p><i>To create a cohesive built environment through consistent and/or complementary elements of built form composition (eg relationships between podiums, modulation, proportions and the like).</i></p>	<p><b>Complies</b></p> <p>As part of the desired diversity of architectural language and built form, the north-east and south-west towers are partly brought to ground. This gives these towers a strong street identity, marking the primary approaches to the precinct. While both these towers are still partly wrapped by the podium, this varied approach to built form avoids the homogenous outcome of a continuous dominant podium with towers completely dislocated from the street below.</p>

#### 4.2.3 Building Siting, Massing and Scale

Control	Comment
<p>1.</p> <p><i>Development shall be designed to incorporate clearly defined ground floor street zone, podium and upper level elements.</i></p>	Complies
<p>2.</p> <p><i>Ground floor heights are to be a minimum of 4m (floor to floor) for all non-residential uses.</i></p>	Complies
<p>3.</p> <p><i>Streets are to be defined by a 4 storey street wall with a height of up to 19m (depending on the use) in accordance with Figure 141. The street wall is to respond to the topography of the site and may vary between buildings where appropriate.</i></p>	<p>Complies</p> <p>Podium massing is compliant with the approved Concept Plan building envelopes. The podium consists of a series of smaller scale buildings ranging from two to four storeys to allow for design variation and massing articulation. As a grouping of buildings this creates a dynamic, fine-grain, and interesting street interface, responding to both the steep topography of the site and the position of the towers over.</p>
<p>4.</p> <p><i>Tower forms above the 8<sup>th</sup> storey shall not exceed 40m in length and 24m in width and shall have floor plates of no more than 800m<sup>2</sup> GFA per floor.</i></p>	<p>Complies</p> <p>The towers are compliant with the approved Concept Plan building envelopes. Breaks have been incorporated into the building articulation to reduce the overall bulk and scale. This is emphasised further by the varying materiality of the four towers and indentation of the facades, allowing for the desired diversity in architectural language across the proposed mixed-use development.</p>
<p>5.</p> <p><i>Tower form is to be orientated to:</i></p> <ul style="list-style-type: none"> <li>a. <i>Reduce the perceived mass of the building</i></li> <li>b. <i>Provide solar access to station plaza as per the controls in Section 2.16.1</i></li> <li>c. <i>Provide privacy for both communal and private open space areas.</i></li> </ul>	<p>Complies</p> <p>The proposal is compliant with the approved Concept Plan building envelopes. The scheme is therefore careful to maintain the solar performance as established by the original Concept Plan and equals or betters its performance.</p> <p>The proposal balances the requirements for good passive surveillance of public and communal open space, while also allowing for sufficient privacy to resident's private open areas.</p>
<p>6.</p> <p><i>Towers above the street wall shall be orientated to maximise solar access to public and private spaces and habitable rooms, district views to the east, north and west and to minimise wind down draft.</i></p>	<p>Complies</p> <p>The towers are compliant with the approved Concept Plan building envelopes and have been designed to be compliant with the solar requirements for habitable rooms.</p> <p>Refer to wind report prepared by Windtech</p>
<p>7.</p> <p><i>Tower massing and scale is to consider possible future development on adjoining sites, including Precinct West, Precinct East and the Castle Hill Showground.</i></p>	<p>Complies</p> <p>The proposal provides a connection from the metro station through to Castle Hill Showground. The publicly accessible Doran Drive Plaza acts as a green heart of the Precinct supporting and providing further amenity for future development of adjoining sites.</p>

#### 4.2.4 Street Wall Height

##### Objective

a.

*To ensure that the height of the street walls make a significant contribution to the experience of place and add uniformity of character along particular streetscapes, or provide variations in areas where so desired.*

##### Comment

##### Complies

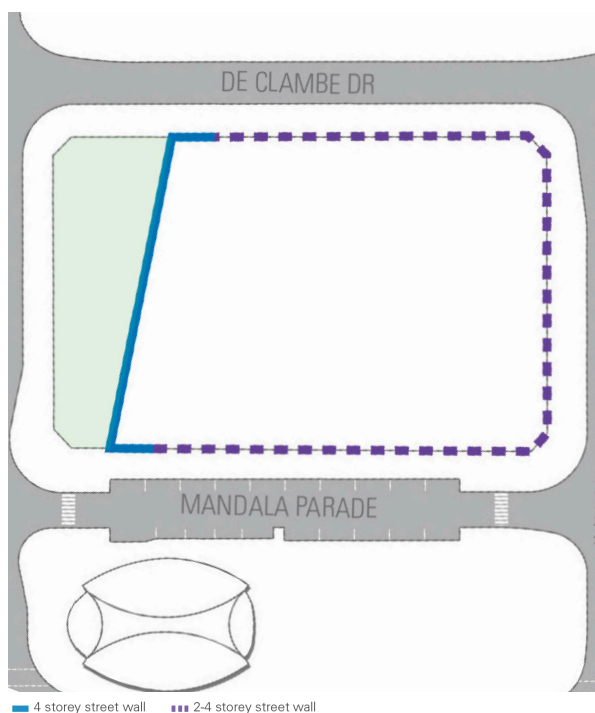
Podium massing is compliant with the approved Concept Plan building envelopes, with a series of smaller scale buildings ranging from two to four storeys. A varied palette of brick colours and detailing offers a robust, high quality and low maintenance finish to these lower levels. Collectively, this grouping of buildings creates a dynamic, fine-grain, and interesting street interface.

b.

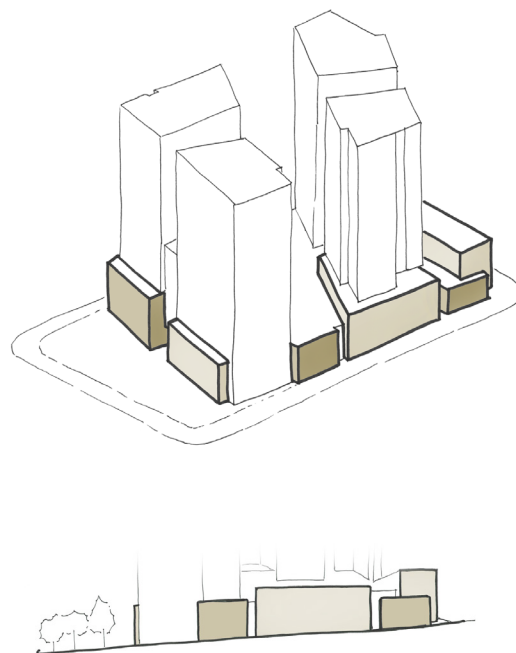
*To provide street wall heights that are a response to future conditions within, and adjoining the site and the desired future character of the streets and Character Areas.*

##### Complies

The articulation of the podium massing creates a dynamic ground plane which responds to the existing and future conditions of the site. The materiality is aligned with the desired future character, resulting in a series of fine grain streetscapes. Awnings have also been refined with the entries on Ground floor to increase permeability and quality street activation.



Excerpt from Urban Design Guidelines  
Pg108, Fig 141.



Facade Articulation - Stepped Human Scale Podium



#### 4.2.4 Street Wall Height

Control	Comment
<p><b>1.</b></p> <p><i>Provide a 4-storey street wall for Doran Drive Precinct to define the streets and public open spaces with an appropriately scaled built form.</i></p>	<p><b>Complies</b></p> <p>Refer to the more detailed response to the Urban Design Guide objectives. (4.2.4)</p>
<p><b>2.</b></p> <p><i>The southern, northern and eastern street walls of the Doran Drive Precinct may be a minimum of 2-storeys high and a maximum of 4-storeys high to allow for variation in the podium in response to topographical conditions on the site and the location of communal open space areas on the podium and potential relationships to the public domain.</i></p>	<p><b>Complies</b></p> <p>Podium massing is compliant with the approved Concept Plan building envelopes. The podium consists of a series of smaller scale buildings ranging from two to four storeys to allow for design variation and massing articulation. As a grouping of buildings this creates a dynamic, fine-grain, and interesting street interface, responding to both the steep topography of the site and the position of the towers over.</p>
<p><b>3.</b></p> <p><i>Where the podium does not exceed the 19m height limit, a mezzanine level of retail/commercial may be provided to assist in sleeving tenancies that have a large floor-to-floor height such as supermarkets. The mezzanine level should use similar finishes and materials as the ground floor and be connected via internal circulation to the ground floor tenancy addressing Doran Drive Plaza and De Clambe Drive.</i></p>	<p><b>Complies</b></p>

#### 4.2.5 Setbacks

Objective	Comment
<p><b>a.</b>  <i>To contribute to the human scale and visual experience of the street.</i></p>	<p><b>Complies</b>  A highly indented façade is applied around the ground floor plane of the proposed mixed-use development with a varied use of materials and detailing. Continuous awnings provide protection to pedestrians and F&amp;B diners. The proposed setbacks are as per the Urban Design Guidelines for this site. The south, north and west of the site are sleeved with continuous tenancies and lobby entry to provide meaningful activation. Tenancies and lobbies step to follow the steep falls of the surrounding streets to allow direct level access.</p>
<p><b>b.</b>  <i>To provide an intimate urban experience within active and non-residential streets where shops and food and beverage businesses can be easily viewed and 'spill out' onto the footpath.</i></p>	<p><b>Complies</b>  The proposed retail tenancies along Doran Drive Plaza have a direct level access to the publicly accessible Plaza, allowing for trade out zones and flexibility in use. This dining precinct will offer day and night activation of the plaza. Tenancies and lobbies along the norther and south edges of the podium step to follow the steep falls of the surrounding streets.</p>
<p><b>c.</b>  <i>To enhance the pedestrian experience through visual enclosure and scale of streets and provide access to sunlight.</i></p>	<p><b>Complies</b>  The proposed massing and built form are compliant with the approved Concept Plan building envelopes. The setback of the towers over from the publicly accessible Doran Drive Plaza allows for the required sunlight to this area.</p>
<p><b>d.</b>  <i>To define the public domain and create a consistent streetscape.</i></p>	<p><b>Complies</b>  The proposed retail tenancies along Doran Drive Plaza have a direct level access to the publicly accessible Plaza, allowing for trade out zones and flexibility in use. This dining precinct will offer day and night activation of the plaza, with a well defined built edge.  Tenancies and lobbies along the norther and south edges of the podium step to follow the steep falls of the surrounding streets.  The large supermarket is located in the centre of the site, generally below the adjacent street levels, and has a direct interface with the street edge in order to maintain a consistent edge of fine grain retail units around the perimeter of the podium.</p>
<p><b>e.</b>  <i>To reduce building bulk and scale and enable adequate sunlight access to the public domain.</i></p>	<p><b>Complies</b>  The proposed massing and built form are compliant with the approved Concept Plan building envelopes. The setback of the towers over allows for increased sunlight to the publicly accessible Doran Drive Plaza.</p>
<p><b>f.</b>  <i>To complement building mass and emphasise key design elements such as entrance points and respond to environmental conditions including solar access, noise, privacy and views.</i></p>	<p><b>Complies</b>  The proposal is compliant with the required numerics of the Urban Design Guidelines street set-backs.  As part of the façade articulation, the north-east and south-west towers are partly brought to ground to emphasise the primary approaches to the precinct, while elsewhere the towers are set back from the dominant podium base.  Awnings are continuous around the north, west and south the podium to offer protection to pedestrians and F&amp;B diners. 50% tree canopy coverage ensures shade and comfort levels within the public and communal open areas.</p>

4.2.5 Setbacks

Controls

1.

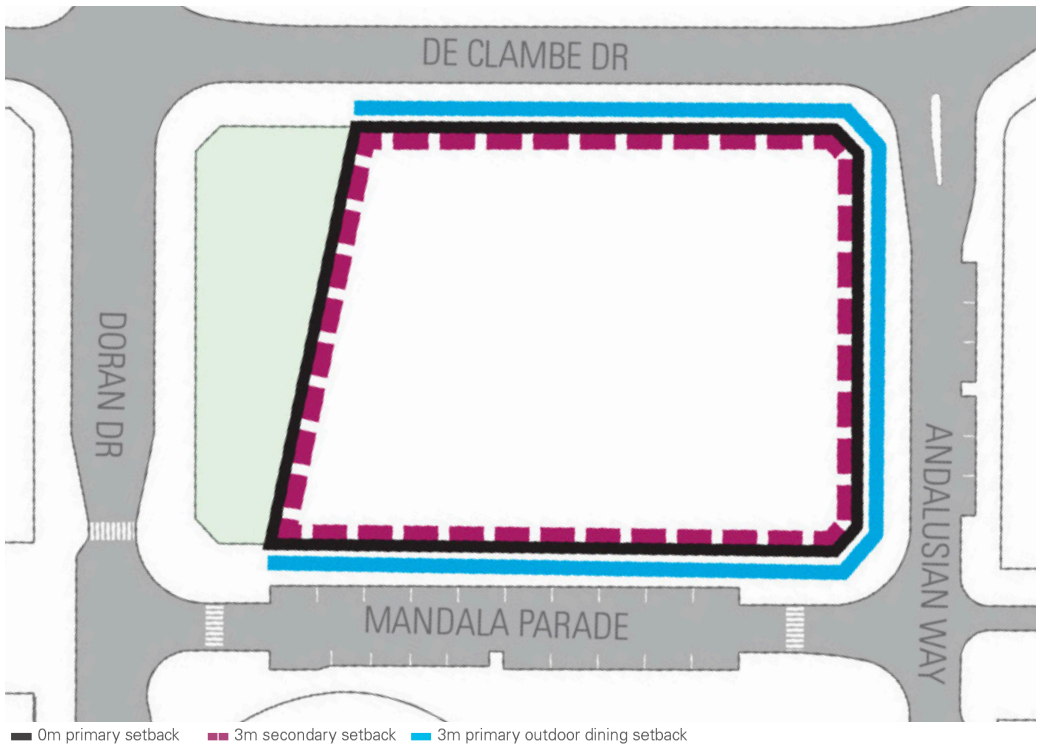
*In accordance with Figure 144, the minimum setback for a development to the boundary is:*

- a. 0m ground floor primary setback for all interfaces, except where a 3m primary setback is required for outdoor dining zones*
- b. 3m secondary setback above the podium.*

Comment

Complies

Setbacks are adhered to within the proposal.



Excerpt from Urban Design Guidelines  
Pg109, Fig 144.

#### 4.2.6 Building Height

Objective	Comment
<p><b>a.</b></p> <p><i>To locate density where most appropriate and in accordance with TOD fundamentals - eg in the mixed-use areas with greatest access to transport.</i></p>	<p><b>Complies</b></p> <p>The approved Concept Plan for The Hills Showgrounds Station Precinct recognises the strategic importance of this new emerging precinct as the active heart of the wider community. In line with best practice urban design, the Concept Plan envisages a vibrant mixed-use precinct. The densities proposed are in the context a transport orientated development, in close proximity to quality public transport links. The previous suburban, single use, and car focused urban character is being reimagined as a more dynamic urban quarter with high amenity and generous parks, plazas and green links. The proposed massing and built form is compliant with the approved Concept Plan building envelopes and has embraced the desired design principles for the precinct.</p>
<p><b>b.</b></p> <p><i>To provide an appropriate pedestrian scale built form to the public domain of Doran Drive Plaza, surrounding streets and the Station Plaza and forecourt.</i></p>	<p><b>Complies</b></p> <p>Podium massing is compliant with the approved Concept Plan building envelopes. Through the design of a series of smaller scale buildings, ranging from two to four storeys, a dynamic pedestrian focused streetscape is created with clearly defined entry points and wayfinding across the site.</p>
<p><b>c.</b></p> <p><i>To deliver a varied skyline across the Hills Showground Station Precinct and the broader Precinct.</i></p>	<p><b>Complies</b></p> <p>The variation in the character of the towers ensures a varied and attractive contribution to The Hills Showgrounds Station Precinct skyline. Each of the buildings differs in materiality, pattern and design language, on this steeply sloping site. Collectively they create a playful grouping of buildings.</p>
<p><b>d.</b></p> <p><i>To identify the heart of the Precinct at Doran Drive Plaza and the metro station.</i></p>	<p><b>Complies</b></p> <p>The proposed publicly accessible Doran Drive Plaza provides a generously landscaped space with 50% tree canopy coverage to form the active heart of the precinct. It's permeability from De Clambe Drive, Doran Drive and Mandala Parade encourage public activation from the adjoining precincts, with clear sightlines and well-defined wayfinding.</p>

#### 4.2.6 Building Height

Control	Comment
<p><b>1.</b></p> <p><i>A maximum height of 68m (21 storeys) is permitted for the towers.</i></p>	<p><b>Complies</b></p>
<p><b>2.</b></p> <p><i>A maximum height of 28m (6 storeys) for a small portion of the De Clambe Drive interface to provide adequate solar access to communal open space.</i></p>	<p><b>Complies</b></p> <p>The 6 stories proposed for the lower portion of tower A complies with the 28m height plane and ensures adequate solar access is provided to the proposed communal open spaces within the overall development, in accordance with the ADG.</p>
<p><b>3.</b></p> <p><i>A maximum height of 19m (4 storeys) is permitted for the podiums as per controls within Section 4.2.4. The inclusion of a mezzanine level within one of these storeys is permitted to assist in providing a finer grain sleeving of larger format retailing to the public domain as long as the total height of the podium does not exceed 19m.</i></p>	<p><b>Complies</b></p>





Perspective Image - Doran Drive Plaza



Perspective Image - Doran Drive Plaza - Artwork

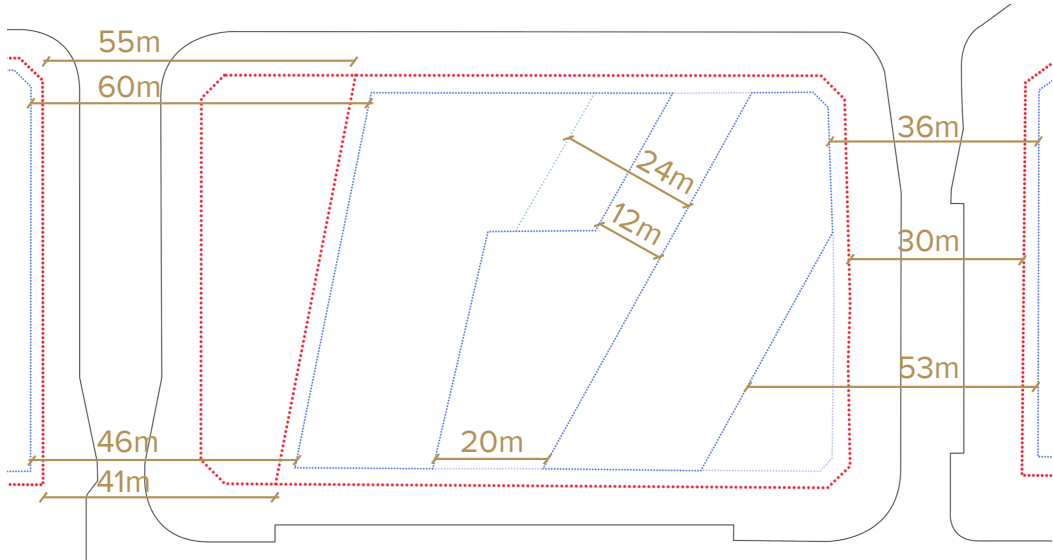


#### 4.2.7 Building Separation

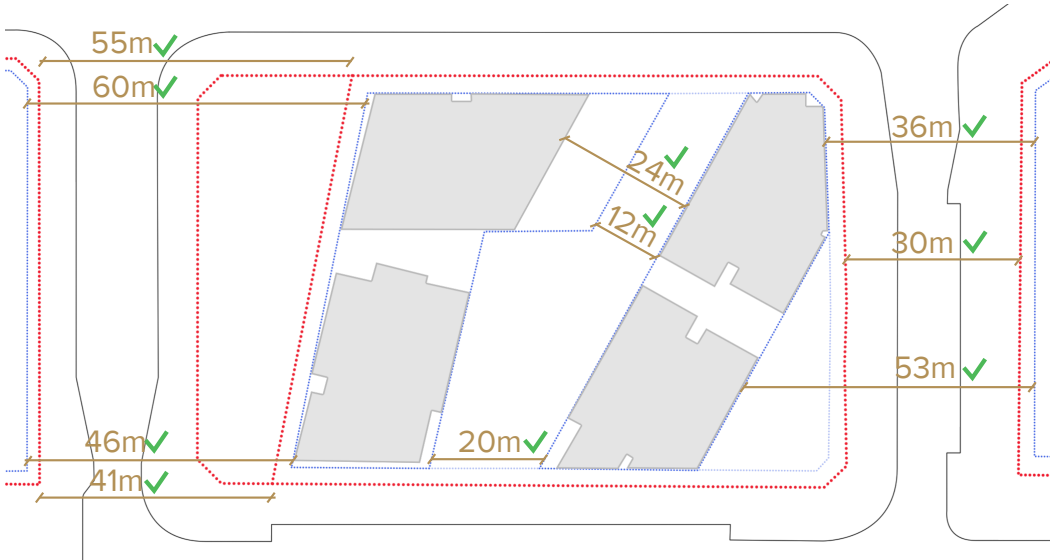
Objective	Comment
<p><b>a.</b></p> <p><i>To protect privacy and solar access to private and public spaces.</i></p>	<p><b>Complies</b></p> <p>The building separation aligns with the Urban Design Guidelines and ADG. The built forms meet solar access requirements to residential communal open spaces on podium and the publicly accessible Doran Drive Plaza at street level.</p>
<p><b>b.</b></p> <p><i>To ensure suitable open sky views are provided from key public infrastructure elements such as Doran Drive Plaza and the transport interchange.</i></p>	<p><b>Complies</b></p> <p>The breaks in the overall building massing allows for visual links and a sense of openness across the site. The towers are conceived as a series of smaller slender vertical elements within the streetscapes which allows for an overall reduced bulk and scale.</p>
<p><b>c.</b></p> <p><i>To provide building envelopes that enable design options that exceed minimum ADG requirements.</i></p>	<p><b>Complies</b></p> <p>Through the further shaping of the buildings within the provided envelopes within the Urban design Guidelines, increased solar amenity is achieved to both the proposed residential apartments and communal open spaces.</p>
<p><b>d.</b></p> <p><i>To provide generous building separation between the towers that frame Doran Drive Plaza and the transport interchange.</i></p>	<p><b>Complies</b></p> <p>The building separation adjacent to the publicly accessible Doran Drive Plaza achieves the dimensions and design expectations of the Urban Design Guidelines.</p>

#### 4.2.7 Building Separation

Control	Comment
<p><b>1.</b></p> <p><i>As shown in Figure 146, a minimum building separation of 12m and 24m is required for towers on the northern interface and a minimum building separation of 20m is required for towers on the southern interface.</i></p>	<p><b>Complies</b></p> <p>The towers are compliant with the approved Concept Plan building envelopes.</p>
<p><b>2.</b></p> <p><i>Separation between building envelopes within Precinct West and Doran Drive Precinct must be a minimum of:</i></p> <p><i>a. 41m at the southern end and 55m at the northern end for podiums</i></p> <p><i>b. 46m at the southern end and 60m at the northern end for towers.</i></p>	<p><b>Complies</b></p> <p>The towers are compliant with the approved Concept Plan building envelopes.</p>
<p><b>3.</b></p> <p><i>Separation between building envelopes within Doran Drive Precinct and Precinct East must be a minimum of:</i></p> <p><i>a. 30m for the podiums</i></p> <p><i>b. 36m at the northern end and 53m at the southern end for towers.</i></p>	<p><b>Complies</b></p> <p>The towers are compliant with the approved Concept Plan building envelopes.</p>



Concept Plan

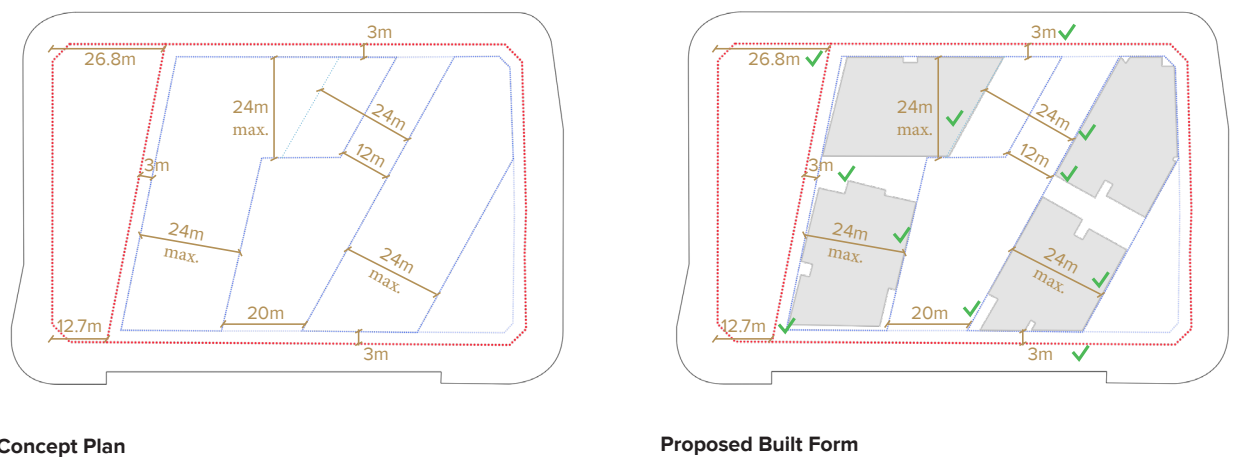


Proposed Built Form



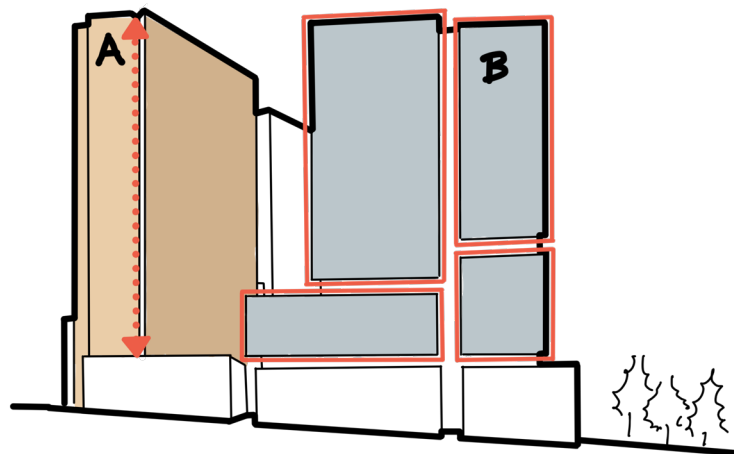
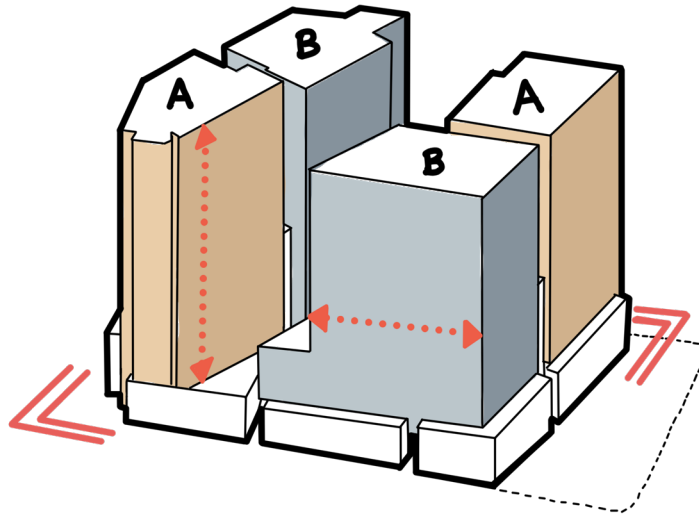
4.2.8 Building Envelopes	
Objective	Comment
<p><b>a.</b></p> <p><i>To prescribe a desired outcome for individual sites, and the Precinct as a whole, that delivers a level of certainty to Council and the community and retain a level of flexibility for innovation and diverse design outcomes in the future.</i></p>	<p><b>Complies</b></p> <p>The proposed built form and massing responds to and enhances the existing and proposed character of the Hills Showground Station Precinct. The proposal recognises the necessity of an appropriate scale and built form of the podium to ensure the meaningful activation of the streetscapes. Continuous retail tenancies and lobbies further encourage increased activation. The series of proposed community rooms also provide flexibility in use as the precinct continues to evolve.</p>
<p><b>b.</b></p> <p><i>To ensure good amenity is provided through appropriate building separation, setbacks and depths.</i></p>	<p><b>Complies</b></p> <p>In complying with the building massing as set out in the Urban design Guidelines, increased amenity is provided within the communal open space provided to residents. The configuration of the communal open space within the proposed podium and atop several rooftop spaces provide generous district views.</p>
<p><b>c.</b></p> <p><i>To ensure building depths support well-designed apartment layouts.</i></p>	<p><b>Complies</b></p> <p>The proposed building massing is compliant, achieving apartment layouts in accordance with the ADG</p>

4.2.8 Building Envelopes	
Control	Comment
<p><b>1.</b></p> <p>Development is to conform to the building envelopes outlined in Figure 147.</p>	<p><b>Complies</b></p>



#### 4.2.9 Building Articulation and Facades

Objective	Comment
<p><b>a.</b></p> <p><i>To ensure that developments are aesthetically pleasing, encourage creativity and diversity in design, incorporating architectural relief and modulation of facades to avoid a bulky or monotonous appearance.</i></p>	<p><b>Complies</b></p> <p>Each of the buildings has an individual building expression that allows for a variance in architectural character within the proposal. With regards to composition, Buildings A and D sit above the podium and have a horizontal emphasis to the façade design. While buildings B &amp; C partly come to ground and champion the use of brick within a vertical façade language.</p> <p>Building A is predominantly glazed, in order to avail of the outlook and views over the verdant context of the Showgrounds. The vertical elements that frame the overall composition are complimented with horizontal breaks to define a base, middle and top to Building A.</p> <p>Building B champions the use of brick with a notorious recess that emphasise the vertical components within the building. The facades are a balance between solid and glazed, to allow for a sufficient solidity to the tower that is contrasting with the neighbouring buildings. The addition of a neutral dark material to the top of this building crowns it and empowers the variation of form.</p> <p>Buildings C on the north east corner of the tower has a consistent use of brick to the vertical elements. This emphasises the towers' vertical proportion which is in contrast to the wider and more horizontal building A adjacent. The addition of a neutral dark material to the top of this building crowns it and empowers the variation to avoid the feeling of bulky forms.</p> <p>Building D has an expressed dark slab edge that projects out past the façade. The remainder of the façade is a recessive composition in soft colours coming from native species. This façade language provides for a strong horizontal graphic.</p> <p>While the towers each have their own individual expression, they sit comfortably together as a cohesive and familial group of the buildings, tying together motifs from the existing local context. This includes; patterns of the existing brick vernacular suburban architecture, colours of endemic plant species such as the Graviillea Hills Jubilee and an emphasis on vista and connection to the adjacent Showground.</p>
<p><b>b.</b></p> <p><i>To deliver a varied built form across both horizontal and vertical planes.</i></p>	<p><b>Complies</b></p> <p>A horizontal separation is achieved between the proposed podium and towers over, with breaks at key locations where the north-east and south-west towers are partly brought to ground. Vertical breaks are achieved within the podium with the design of a series of smaller scale buildings, ranging from two to four storeys. These breaks are extended up into the towers, providing building separation and increased amenity to the residential apartments.</p>
<p><b>c.</b></p> <p><i>To reinforce the intended neighbourhood character and enhance the pedestrian experience.</i></p>	<p><b>Complies</b></p> <p>The design of the podium allows for a series of smaller scale buildings ranging from two to four storeys. A varying palette of brick colour and detailing creates a differing and dynamic streetscape. The continuous tenancies and lobby entries around the perimeter of the site, allow for a fine-grain and interesting street interface.</p>
<p><b>d.</b></p> <p><i>To ensure that buildings address the existing streets.</i></p>	<p><b>Complies</b></p> <p>The articulation of the podium massing creates a dynamic ground plane which responds to the existing and future conditions of the site. The materiality is aligned with the desired future character, resulting in a series of fine grain streetscapes. Awnings have also been refined with the entries on Ground floor and with the stepping typography of the site to increase permeability and quality street activation.</p>

**Facade Articulation - Tower Forms**

4 Towers - 2 Types

A - Vertical Landmark, anchoring primary corner

B - Horizontal Composition, above podium

#### 4.2.9 Building Articulation and Facades

Controls	Comment
<p><b>1.</b></p> <p><i>Development shall be designed to incorporate ground floor street zone, podium and upper level elements that are clearly defined by horizontal articulation.</i></p>	Complies
<p><b>2.</b></p> <p><i>Corner buildings are to address both street and corner elements, to emphasize significant key intersections and enhance public domain legibility. Street corners shall be addressed by giving visual prominence to those parts of the building façade, such as a change in building articulation, material or colour, roof expression or height.</i></p>	<p>Complies</p> <p>Refer to the more detailed response to the Urban Design Guide objectives. (4.2.9)</p>
<p><b>3.</b></p> <p><i>Facade treatments are to create visual variety and interest while contributing to the continuity of the streetscape.</i></p>	Complies
<p><b>4.</b></p> <p><i>Walls should comprise a variety of colours to reduce monotony and add variety to the streetscape.</i></p>	Complies
Ground floor Street Zone Controls	Comment
<p><b>5.</b></p> <p><i>Ground level articulation is to ensure universal access to all tenancies and properties from the public domain. A mid-point in the topographical change may be used as a common access point where reduced terracing to the public domain is desired (eg colonnade or outdoor dining areas).</i></p>	Complies
<p><b>6.</b></p> <p><i>Provide architectural features in the façade that give human scale at ground floor level, such as entry porches, pergolas and so on.</i></p>	Complies
<p><b>7.</b></p> <p><i>A sense of address and visual interest from the street is to be provided through the use of insets and projections and, where relevant, the appearance of finer grain buildings, however ground floor recesses that undermine the safety of the public domain are to be avoided.</i></p>	<p>Complies</p> <p>Refer to the more detailed response to the Urban Design Guide objectives. (4.2.9)</p>
<p><b>8.</b></p> <p><i>Fine grain retail and commercial frontages are to be provided to ensure an interesting street edge and support human scale streetscapes. Finer detail to identify individual tenancies and different building levels are to be used to add richness to the architectural design.</i></p>	<p>Complies</p> <p>Refer to the more detailed response to the Urban Design Guide objectives. (4.2.9)</p>

#### 4.2.9 Building Articulation and Facades

##### Ground floor Street Zone Controls

##### Comment

9.

*Building entries are to be visually identifiable from the street frontage with clear sight lines and are to have direct address to the street. Separate entrances are required for commercial/ retail and residential uses. Lighting should be provided for safety at night.*

**Complies**

Refer to the more detailed response to the Urban Design Guide objectives.

10.

*Where an active frontage is required, a majority of the building frontage is to be transparent (i.e. windows and glazed doors). Clear glazing is to be provided to windows and doors.*

**Complies**

11.

*Security grilles may only be fitted internally behind the shopfront of any non-residential uses at ground level. They are to be transparent and fully retractable.*

**Complies**

Noted, this will be resolved later as part of detail design resolution.

12.

*A minimum 2.5m awning is to extend over the public domain where any active edge is prescribed by these Guidelines. This is to also extend beyond any outdoor dining areas zones as shown in Figure 161.*

**Complies**

13.

*Footpath awnings shall be designed to complement and integrate with the façade and the streetscape.*

**Complies**

14.

*Ventilation louvres and carpark entry doors are to be integrated into facade designs where located on street frontages.*

**Complies**

15.

*Services such as for fire protection, water and power distribution are not to intrude upon the pedestrian right of way, visually detract from the appearance of the development, and are to be screened from the street frontage with materials which are integrated with architectural expression of the development.*

**Complies**

16.

*Any visible carpark entries or walls should be comprised of more than one material and colour to enhance visual attractiveness and interest.*

**Complies**

17.

*Any ground level car park entries should be concealed or screened by planting from the street and public view, as much as possible.*

**Complies**

#### 4.2.9 Building Articulation and Facades

##### Podium Controls

##### Comment

18.

*Horizontal articulation of the podium facade is to be provided above 2 storeys.*

Complies

19.

*Horizontal articulation is to respond to the natural topography of the development lots with a maximum of two steps within each podium aligned with any breaks in the built form or tower elements above the podium.*

Complies

20.

*A constant podium height is required across individual buildings. Podium height may vary between buildings in response to topography.*

Complies

Refer to the more detailed response to the Urban Design Guide objectives.

21.

*A 1m wide notch is to be provided at regular interval for vertical podium façade articulation.*

Complies

Refer to the more detailed response to the Urban Design Guide objectives.

22.

*Podium facades shall avoid blank, featureless walls by patterning high quality architectural elements such as window bays, canopies and fenestration.*

Complies

Refer to the more detailed response to the Urban Design Guide objectives.

##### Upper Level Controls

##### Comment

23.

Building facades are to be vertically articulated to reduce the appearance of building bulk and to express the elements of the building's architecture.

Complies

24.

*Building facades are to be enhanced through the use of well-proportioned and balanced projections and recesses.*

Complies

25.

*Any towers longer than 50m between the podium and the 7th storey must be articulated through a minimum 3m and maximum 5m recess, inset or projection and treated with different materials and finishes.*

Complies

The towers are compliant with the approved Concept Plan building envelopes. Breaks have been incorporated into the building articulation to reduce the overall bulk and scale. This is emphasised further by the varying materiality of the four towers and indentation of the facades, allowing for the desired diversity in architectural language across the proposed mixed-use development.

26.

*Telecommunications, service structures, lift motor rooms and mechanical plants are to be integrated within the roof design and roof features to contribute to an attractive and interesting skyline for the precinct.*

Complies

26.

*Tower facades are to be articulated to be:*

- *Articulated to manage passive solar gain*
- *Well-glazed with functional windows where possible to reduce reliance on artificial cooling*
- *Designed with high-quality sustainable materials and finishes that promote building longevity*
- *Varied in design and articulation to promote visual interest.*

Complies

Refer to the more detailed response to the Urban Design Guide objectives. (4.2.9)





**Perspective Image - DeClambe Drive**



**Perspective Image - Doran Drive / Metro Station forecourt**



#### 4.2.10 Active Use and Street Frontage

Objective	Comment
<p><b>a.</b></p> <p><i>To create an active node around Doran Drive Plaza.</i></p>	<p><b>Complies</b></p> <p>Doran Drive Plaza Precinct is designed to be a welcoming and safe place for people to live in and visit. Legible and safe movement for all users is provided with a focus on pedestrian priority across the precinct. Loading and service areas are designed to be consolidated and discrete, minimising the potential for conflict between users.</p>
<p><b>b.</b></p> <p><i>To reinforce complementary uses and desired street character.</i></p>	<p><b>Complies</b></p> <p>A carefully considered strategy for services and parking/loading areas achieves the desired street activation across the site. The impact of services and parking/loading zones have been minimised by pushing these elements into the centre of the site, and away from the street edge. This key move allows the perimeter of the site to be predominantly sleeved with retail and commercial tenancies, community rooms, and residential lobbies.</p>
<p><b>c.</b></p> <p><i>To promote an exceptional pedestrian experience with active frontages.</i></p>	<p><b>Complies</b></p> <p>As a pedestrian priority environment, the proposal has carefully integrated ground level permeability. Safe movement, good connections and access are provided with well-defined routes and clear sightlines (day and night) so residents and visitors can see and be seen.</p>
<p><b>d.</b></p> <p><i>To ensure active uses are located in areas of high pedestrian activity and amenity.</i></p>	<p><b>Complies</b></p> <p>Pedestrian permeability and connection between the metro station and Castle Hill Showground has largely informed the ground plane and retail layout of the proposed development. As well as a 6m wide pathway adjacent to the publicly accessible Doran Drive Plaza, connectivity is provided between the retail entry opposite the metro station and the retail entrance at level 01 at De Clambe Drive.</p>
<p><b>e.</b></p> <p><i>To ensure ground floor uses activate the public domain and streets and provide passive surveillance.</i></p>	<p><b>Complies</b></p> <p>Visibility and surveillance of the public environment is maximised by providing public places that are overlooked from adjoining buildings, for 'eyes on the street' or 'natural surveillance'. Ground level retail, dining, community uses, and residential lobbies, and living areas within the neighbourhood streets provide for good activation and connectivity – both physically and visually – with the street network and public domain.</p>
<p><b>f.</b></p> <p><i>To encourage ground floor activities (uses such as local retail, business and/or community) to spill out into the public domain to create a vibrant streetscape and promote a sense of community.</i></p>	<p><b>Complies</b></p> <p>The proposed retail tenancies along Doran Drive Plaza have a direct level access to the publicly accessible Plaza, allowing for trade out zones and flexibility in use. This dining precinct will offer day and night activation.</p>
<p><b>g.</b></p> <p><i>To provide flexibility in allowing for permanent outdoor dining areas outside of the existing road reserve and footpath areas.</i></p>	<p><b>Complies</b></p> <p>The proposed retail tenancies along Doran Drive Plaza have a direct level access to the publicly accessible Plaza, allowing for trade out zones and flexibility in use. This dining precinct will offer day and night activation.</p>

#### 4.2.10 Active Use and Street Frontage

Control	Comment
<p>1.</p> <p>Active frontages are to be located on Doran Drive, De Clambe Drive, Mandala Parade and Andalusian Way for the extent identified in the Active Frontages diagram in Figure 157.</p>	<p><b>Complies, with qualifications</b></p> <p>Active Frontages have been maximised where possible along all streetscapes. Doran Drive Plaza, Mandala Parade and DeClambe Drive are sleeved with continuous retail tenancies, community rooms, and entry lobbies. Aligning with the Urban Design Guidelines, service and waste vehicles zones have been located along Andalusian Way. Notwithstanding this, increased activation has been provided where possible with the inclusion of a residential lobby and retail tenancies at the street corners of this eastern street frontage.</p>
<p>2.</p> <p>Active frontages may include one or a combination of the following:</p> <ul style="list-style-type: none"> <li>-Shop front</li> <li>-Business or retail premises</li> <li>-Café or restaurant with a street entrance</li> <li>-Community and civic uses with a street entrance               <ul style="list-style-type: none"> <li>-Recreation facilities with a street entrance.</li> </ul> </li> </ul>	<p><b>Complies</b></p>
<p>3.</p> <p>An active street frontage is not required for any part of a building that is used for any of the following:</p> <ul style="list-style-type: none"> <li>- Entrances and lobbies (including as part of mixed-use development)</li> <li>- Access for fire services</li> <li>- Vehicular access.</li> </ul>	<p><b>Complies</b></p>
<p>4.</p> <p>Large retail tenancies (above 500m<sup>2</sup> GFA) are to be screened by smaller tenancies for greater street activation and retail variety, where needed.</p>	<p><b>Complies</b></p> <p>The large supermarket tenancy has been located centrally and generally below the adjacent street levels. It has only a small direct interface with the street edge, maintaining a consistent edge of smaller fine grain retail units around the perimeter of the podium.</p>
<p>5.</p> <p>For larger developments, building entrances should be provided on each street frontage.</p>	<p><b>Complies</b></p>
<p>6.</p> <p>Retail and commercial uses at ground level are to be designed so that the ground floor for the primary entry area of the premises is at the same level as the finished footpath level of the adjacent street and/or open space.</p>	<p><b>Complies</b></p>
<p>7.</p> <p>Outdoor dining along the interface with De Clambe Drive, Mandala Parade and Andalusian Way is to be located in accordance with Figure 157 to ensure there are no conflicts with building entries, carpark and loading dock access.</p>	<p><b>Complies</b></p>
<p>8.</p> <p>Where a 3m outdoor dining zone is desired, it is to be provided in addition to the existing public domain.</p>	<p><b>Complies</b></p>
<p>9.</p> <p>Outdoor dining areas may be provided within Doran Drive Plaza in accordance with the controls contained in Section 4.2.10.</p>	<p><b>Complies</b></p>

#### 4.2.10 Active Use and Street Frontage

##### Control

##### Comment

10.

Complies

*Outdoor dining areas are to be adjacent to active edges that front streets with high pedestrian activity.*

11.

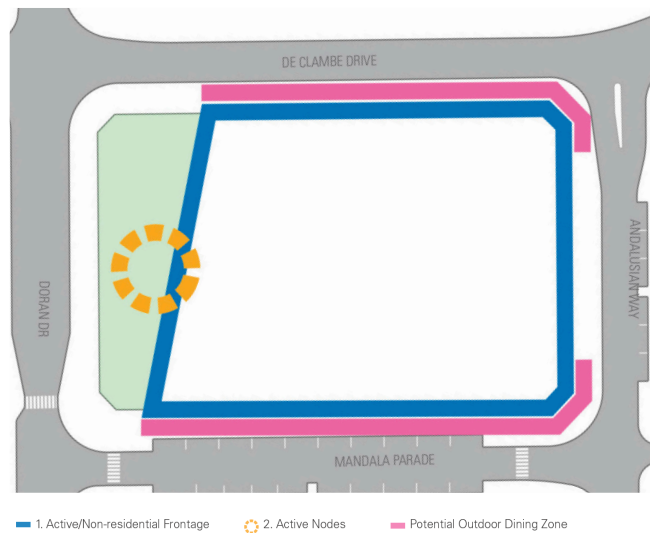
Complies

*Where more than one outdoor dining tenancy is desired along a single interface, the tenancies are to be continuous.*

12.

Complies

*Outdoor dining areas located on a street corner are to wrap around the corner to address both streets.*



Excerpt from Urban Design Guidelines  
Pg120, Fig 157.



Proposed Ground Plane Activation

#### 4.2.11 Street Interface

##### De Clambe Drive Interface

Objective	Comment
<p><b>a.</b></p> <p><i>To enhance the existing functions of De Clambe Drive where it interfaces with the Doran Drive Precinct development lot.</i></p>	<p><b>Complies</b></p> <p>The public domain along De Clambe Drive is enhanced by the extent of retail tenancies and active uses that line this street edge. This is further supported by the connectivity provided within the retail mall through to Doran Drive Plaza.</p>
<p><b>b.</b></p> <p><i>To maintain the existing functions of De Clambe Drive as a local road.</i></p>	<p><b>Complies</b></p> <p>Loading and service areas have been located away from De Clambe Drive, encouraging local vehicle movement only through to the residential or retail parking entry.</p>
<p><b>c.</b></p> <p><i>To maintain the width of the existing landscape verge and footpath.</i></p>	<p><b>Complies</b></p> <p>With the proposal of the proposed mixed-use development the landscape verge and footpath has been maintained along De Clambe Drive.</p>

##### Doran Drive Plaza Interface

<p><b>a.</b></p> <p><i>To enhance the existing functions of Doran Drive and Doran Drive Plaza.</i></p>	<p><b>Complies</b></p> <p>With 100% street activation along the publicly accessible Doran Drive Plaza interface, the existing public domain is enhanced through the inclusion of residential mall entrances and on grade potential retail trade-out zones.</p>
<p><b>b.</b></p> <p><i>To maintain the existing functions of Doran Drive as a local road and transport interchange.</i></p>	<p><b>Complies</b></p> <p>Loading and service areas have been located away from Doran Drive, retaining existing functions as a local road and transport interchange.</p>
<p><b>c.</b></p> <p><i>To maintain the width of the existing landscape verge and footpath.</i></p>	<p><b>Complies</b></p> <p>With the proposal of the proposed mixed-use development the landscape verge and footpath has been maintained along Doran Drive.</p>

##### Mandala Parade Interface

Objective	Comment
<p><b>a.</b></p> <p><i>To enhance the existing functions of Mandala Parade where it interfaces with the Doran Drive Precinct development lot.</i></p>	<p><b>Complies</b></p> <p>The public domain along Mandala Parade is enhanced by the retail tenancies and active uses that line this street edge. This is further supported through the north-south connection provided within the retail mall.</p>
<p><b>b.</b></p> <p><i>To reinforce the public open space of the station plaza through the location and visual connection to private open space within or on top of the podium on Doran Drive Precinct.</i></p>	<p><b>Complies</b></p> <p>Direct sight lines are created between the proposed communal open space within the podium and public open space adjacent to the station.</p>





Street Activation - Doran Drive Plaza



Street Activation - Doran Drive Plaza



#### 4.2.11 Street Interface

##### Andalusian Way Interface

Objective	Comment
<p><b>a.</b></p> <p><i>To enhance the existing functions of Andalusian Way where it interfaces with the Doran Drive Precinct development lot.</i></p>	<p><b>Complies</b></p> <p>Increased activation has been provided where possible with the inclusion of a residential lobby, plus retail tenancies at the street corners. Aligning with the Urban Design Guidelines, service and waste vehicles zones are located along this street frontage.</p>
<p><b>b.</b></p> <p><i>To maintain the existing functions of Andalusian Way as a local road.</i></p>	<p><b>Complies</b></p> <p>As Andalusian Way is a secondary street that is for local traffic only, service and waste vehicles zones are located along this street frontage in alignment with the Urban Design Guidelines.</p>
<p><b>c.</b></p> <p><i>To maintain the width of the existing landscape verge and footpath.</i></p>	<p><b>Complies</b></p> <p>With the proposal of the proposed mixed-use development the landscape verge and footpath has been maintained along Andalusian Way.</p>

#### 4.2.11 Street Interface

##### De Clambe Drive Interface

Control	Comment
<p><b>1.</b></p> <p>Development is to comply with the following interface controls:</p> <p>a. 0m primary setback for the first 4 storeys where there are no outdoor dining uses</p> <p>b. Minimum 3m inset via a colonnade for 2 storeys, or a 3m primary setback for the extent of the podium, where developments include outdoor dining uses</p> <p>c. 3m secondary setback above the podium, including where a colonnade or additional primary setback for outdoor dining has been provided.</p>	<p><b>Complies</b></p>
<p><b>2.</b></p> <p><i>A 2.5m awning is to be provided over the public domain.</i></p>	<p><b>Complies</b></p> <p>An awning with a minimum depth of 2.5m is provided over the public domain.</p>

##### Doran Drive Plaza Interface

Control	Comment
<p><b>1.</b></p> <p><i>Development is to comply with the following interface controls:</i></p> <p>a. 3-4 storey podium for the extent of the interface with Doran Drive Plaza to a height of 19m</p> <p>b. 0m primary setback for the height of the podium storeys where there are no outdoor dining uses.</p>	<p><b>Complies</b></p>
<p><b>2.</b></p> <p><i>A 2.5m awning is to be provided over the public domain.</i></p>	<p><b>Complies</b></p> <p>An awning with a minimum depth of 2.5m is provided over the public domain.</p>

4.2.11 Street Interface

Mandala Parade Interface

Control

1.

Development is to comply with the following interface controls:

- a. 0m primary setback
- b. 3m secondary setback above a minimum 2 storey podium.

Comment

Complies

Andalusian Way Interface

Control

1.

Development is to comply with the following interface controls:

- a. A maximum 4 storey podium for the extent of the interface with Andalusian Way
- b. 0m primary setback for the extent of the podium where there are no outdoor dining uses
- c. Minimum 3m inset via a colonnade for 2 storeys, or a 3m primary setback for the extent of the podium, where developments include outdoor dining uses
- d. 3m secondary setback above the podium, including where a colonnade or additional primary setback for outdoor dining has been provided
- e. Where outdoor dining areas are provide, they are to be located on the corners of Mandala Parade and De Clambe Drive.

Comment

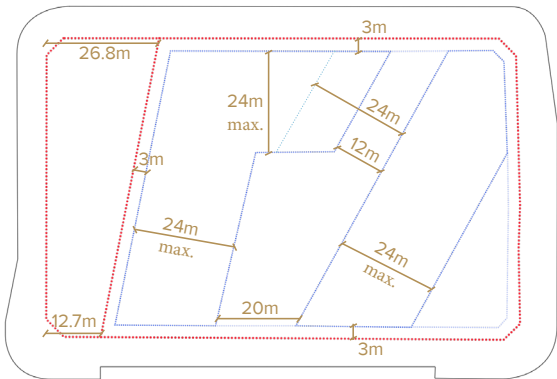
Complies.

2.

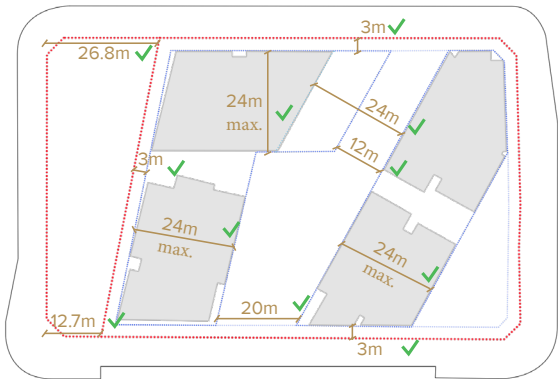
A 2.5m awning is to be provided over the public domain.

Complies

An awning with a minimum depth of 2.5m is provided over the public domain.



Concept Plan



Proposed Built Form



#### 4.2.12 Carparking and Access

##### Objective

a.

*To ensure access does not compromise the activity and pedestrian movements within the heart of the Precinct and the transport interchange on Doran Drive.*

##### Comment

##### Complies

A carefully considered strategy for services and parking/loading areas achieves the desired street activation across the site. The impact of services and parking/loading zones have been minimised by pushing these elements into the centre of the site, and away from the street edge. This key move allows the perimeter of the site to be predominantly sleeved with retail and commercial tenancies, community rooms, and residential lobbies.

b.

*To ensure entries and structures do not impinge upon pedestrian amenity and streetscape quality.*

##### Complies

Access to carparking and loading zones have been located away from primary retail and residential entry points, minimising impact on pedestrian amenity and overall streetscape quality.

c.

*To encourage car share spaces within residential flat buildings for the exclusive use of car share scheme vehicles.*

##### Complies

Carshare vehicle zones have been provided within B1 of the proposed mixed- use development.

d.

*To future proof the development via the provision of electric vehicle charging stations.*

##### Complies

Electric Vehicle charging stations have been provided across the retail and residential basement levels at a ratio of 10%.

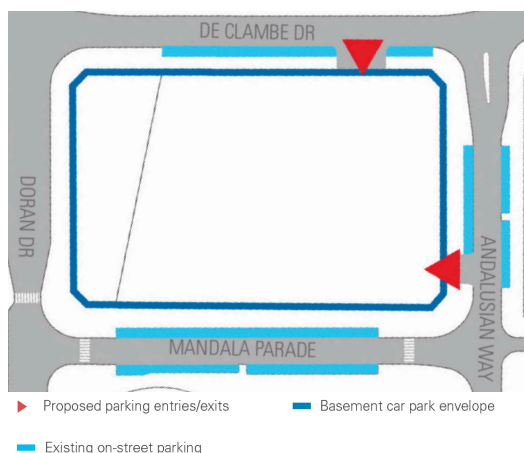
e.

*To ensure residential parking rates allow for flexibility to meet the future demographic needs and ongoing modal shift towards more sustainable transport outcomes.*

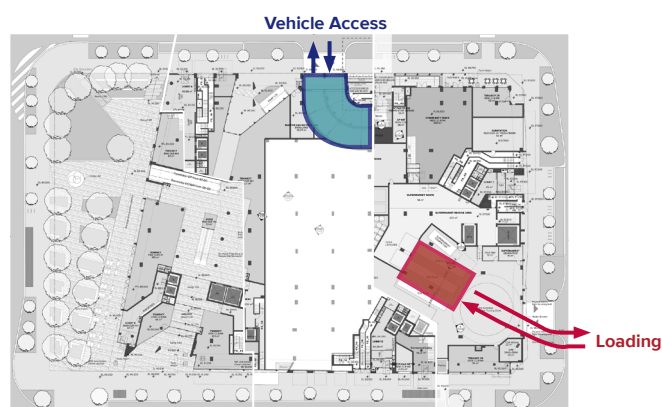
##### Complies

The proposal includes electric vehicle charging zones for all building users.

End of trip facilities for the non-residential uses are provided to encourage alternative means to travelling to the site.



Excerpt from Urban Design Guidelines  
Pg128, Fig 174.



Proposed car park and loading entrances

**4.2.12 Carparking and Access**

<b>Control</b>	<b>Comment</b>
<p><b>1.</b></p> <p><i>Residential carparking spaces are to be provided at the rates specified in Table 7. For any use not specified, the carparking rates in The Hills Development Control Plan 2012 (Part C Section 1 – Parking) shall apply.</i></p>	Complies
<p><b>2.</b></p> <p><i>Parking is to be provided and suitably located to enable shared parking between residential visitor parking and non-residential parking.</i></p>	Complies
<p><b>3.</b></p> <p><i>Secure, conveniently located bicycle parking facilities are to be provided at the rates specified in Table 8.</i></p>	Complies
<p><b>4.</b></p> <p><i>Driveways and vehicular access to car parks shall not be located on Doran Drive.</i></p>	Complies
<p><b>5.</b></p> <p><i>Vehicular access to car parks is to be limited to the eastern end of De Clambe Drive and along Andalusian Way. Access via Doran Drive and Mandala Parade is prohibited. Entry and exits are to be located in accordance with Figure 164</i></p>	Complies
<p><b>6.</b></p> <p><i>Driveways are to be appropriately set back from corners and intersections.</i></p>	Complies
<p><b>7.</b></p> <p><i>Driveways are to have a minimum width of 6 metres at the property boundary for a distance of 6 metres (measured along the centreline of the driveway) within the development to ensure easy entry/exit of vehicles.</i></p>	Complies
<p><b>8.</b></p> <p>Adequate vehicular entry, exit and circulation areas are to be provided. The design must:</p> <ul style="list-style-type: none"> <li>a. Provide safe environment for both pedestrians and vehicles using the site and surrounding road networks</li> <li>b. Ensure vehicular ingress and egress to the site is in a forward direction at all times</li> <li>c. Be designed to minimise the visual impact of hard paved areas.</li> </ul>	Complies
<p><b>9.</b></p> <p><i>Parking is to be underground and avoided within street setbacks. Where above ground parking cannot be avoided due to site conditions, it must be well integrated into the overall façade design and create a good relationship with the public domain.</i></p>	Complies
<p><b>10.</b></p> <p><i>Garages and parking structures are not to project forward of the building line into the public domain and are to be screened from the public domain by active uses.</i></p>	Complies

#### 4.2.12 Carparking and Access

Control	Comment
<b>11.</b> <i>Basement carpark or other structures are not to constrain the infrastructure or access easement to the metro services building.</i>	Complies
<b>12.</b> <i>Carparking shall not be located on the roof of buildings.</i>	Complies
<b>13.</b> <i>The location and means of access to customer carparking within a building is to be clearly visible.</i>	Complies
<b>14.</b> <i>Car share spaces are to be provided at a rate of one space per 150 car spaces for residential and one space per 80 car spaces for commercial.</i>	Complies
<b>15.</b> <i>Car share spaces are to be for the exclusive use of car share scheme vehicles, and included in the number of carparking spaces permitted on a site. The car share parking spaces are to be:</i> <ul style="list-style-type: none"> <li><i>a. Retained as common property by the Owners Corporation of the site, and not sold or leased to an individual owner/occupier at any time</i></li> <li><i>b. Made available for use by operators of car share schemes without a fee or charge</i></li> <li><i>c. Grouped together in the most convenient locations relative to carparking entrances and pedestrian lifts or access points</i></li> <li><i>d. Located in well-lit places that allow for casual surveillance</i></li> <li><i>e. Signposted for use only by car share vehicles</i></li> <li><i>f. Made known to building occupants and car share members through appropriate signage which indicates the availability of the scheme and promotes its use as an alternative mode of transport.</i></li> </ul>	Complies
<b>16.</b> <i>Development applications are to demonstrate how the car share parking space(s) is to be accessed, including where access is through a security gate. A covenant is to be registered with the strata plan advising of any car share parking space. The covenant is to include provisions that the car share parking space(s) cannot be revoked or modified without prior approval of Council.</i>	Complies
<b>17.</b> <i>A minimum of 10% of the total number of parking spaces are to have Electric Vehicle charging stations.</i>	Complies
<b>18.</b> <i>All garages/ carpark entrances must be protected from inundation by flood waters up to the 1% AEP + 0.5m.</i>	Complies
<b>19.</b> <i>End of trip facilities are to be provided where there are allocated bicycle parking facilities associated with commercial or retail development.</i>	Complies

#### 4.2.13 Service Vehicle and Waste Collection

##### Objective

###### Objective a.

*To provide a common zone for service vehicles and waste collection.*

##### Comment

###### Complies

Separate from the carparking access at DeClambe Drive, a common zone for service vehicles and waste collection has been provided on Andalusian Way.

###### Objective b.

*To provide a safe environment for pedestrians and vehicles using the road network.*

###### Complies

Access to service vehicle loading zones have been located away from primary retail and residential entry points, minimising impact on pedestrian amenity and overall streetscape quality. This strategy also avoids any overlap between car and truck movements.

#### 4.2.13 Service Vehicle and Waste Collection

##### Control

##### Comment

###### 1.

*On-site waste collection should be either at grade or via a basement and waste collection vehicles must be able to enter and exit the site in a forward direction.*

###### Complies

###### 2.

*Waste collection must occur from Andalusian Way as demonstrated in Figure 166.*

###### Complies

###### 3.

*Loading areas and vehicular access points for development are to be screened from public roads and public access points.*

###### Complies

###### 4.

*Loading areas and vehicular access point for development must avoid conflicts with pedestrian activity areas including waiting zones for bus, taxi and kiss and ride activities.*

###### Complies

###### 5.

*Service and waste collection vehicle zones must be sufficient dimensions to accommodate a standard 12.5m long HRV and allow for all access and manoeuvring to occur within the zone.*

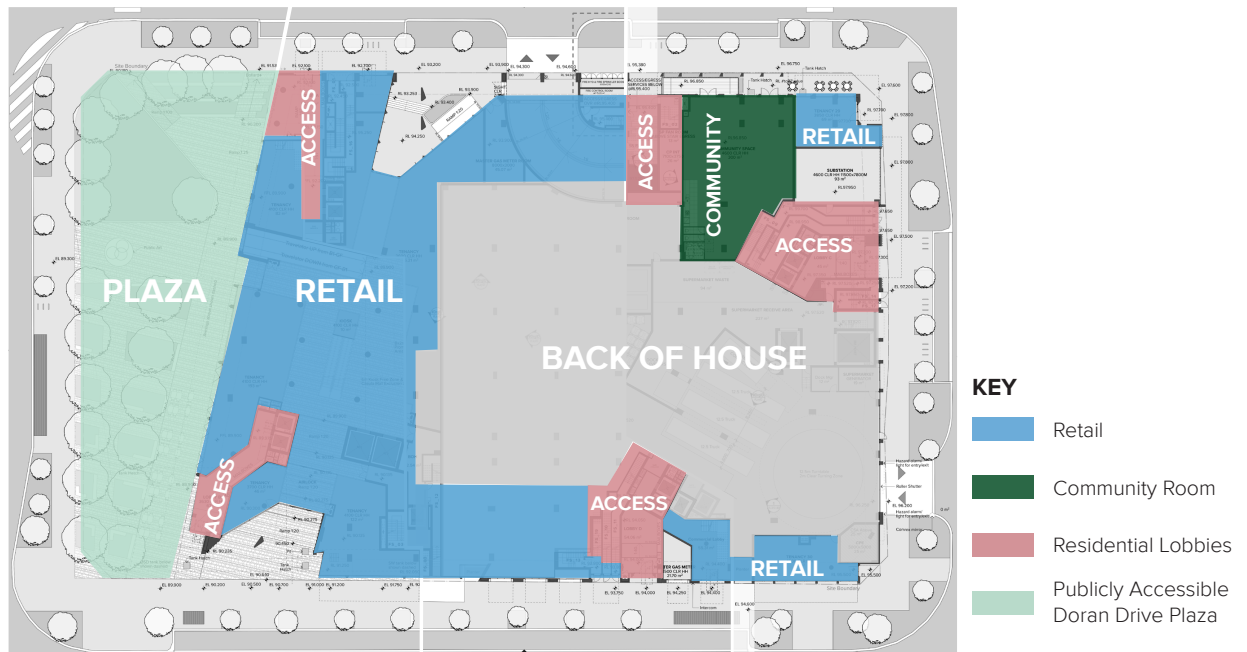
###### Complies

###### 6.

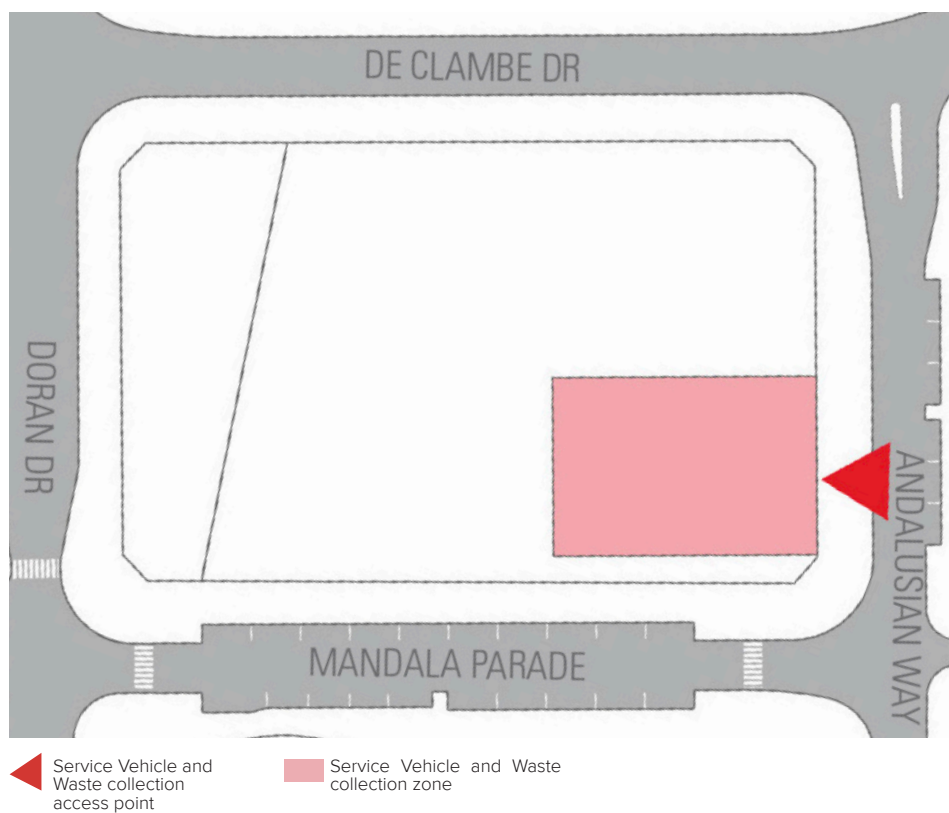
*Waste management shall comply with the waste management controls contained within Part B Section 5 - Residential Flat Buildings and Part B Section 6 - Business of The Hills DCP 2012.*

###### Complies

Refer to waste consultant waste management report.



**Diagrammatic Representation of Ground Level Uses / Program**



**Excerpt from Urban Design Guidelines  
Pg130, Fig 176.**





# DORAN DRIVE PRECINCT

## MIXED USE DEVELOPMENT

2 MANDALA PARADE, CASTLE HILL NSW 2154



TURNER